

SWL
PROJECT ID: 5897-00-70
WITH: N/A
COUNTY: SAUK

DEC 2014

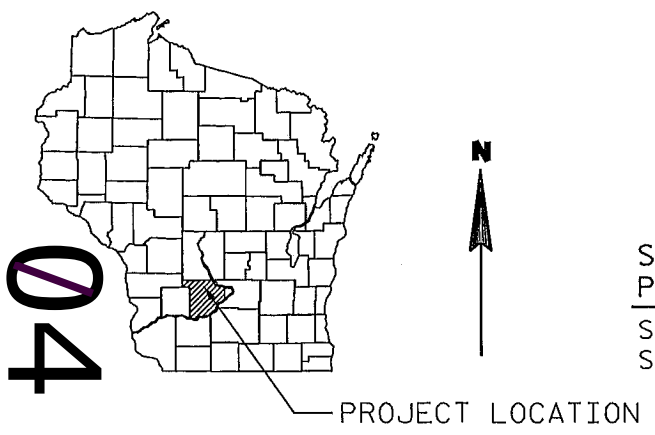
ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details (Includes Erosion Control Details)
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plot
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 80

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PLAN OF PROPOSED IMPROVEMENT
REEDSBURG - WISCONSIN DELLS
FAWN VALLEY DRIVE - IH 90
CTH H
SAUK COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5897-00-70	WISC 2014434	1



STATE PROJECT NUMBER
5897-00-70

DESIGN DESIGNATION

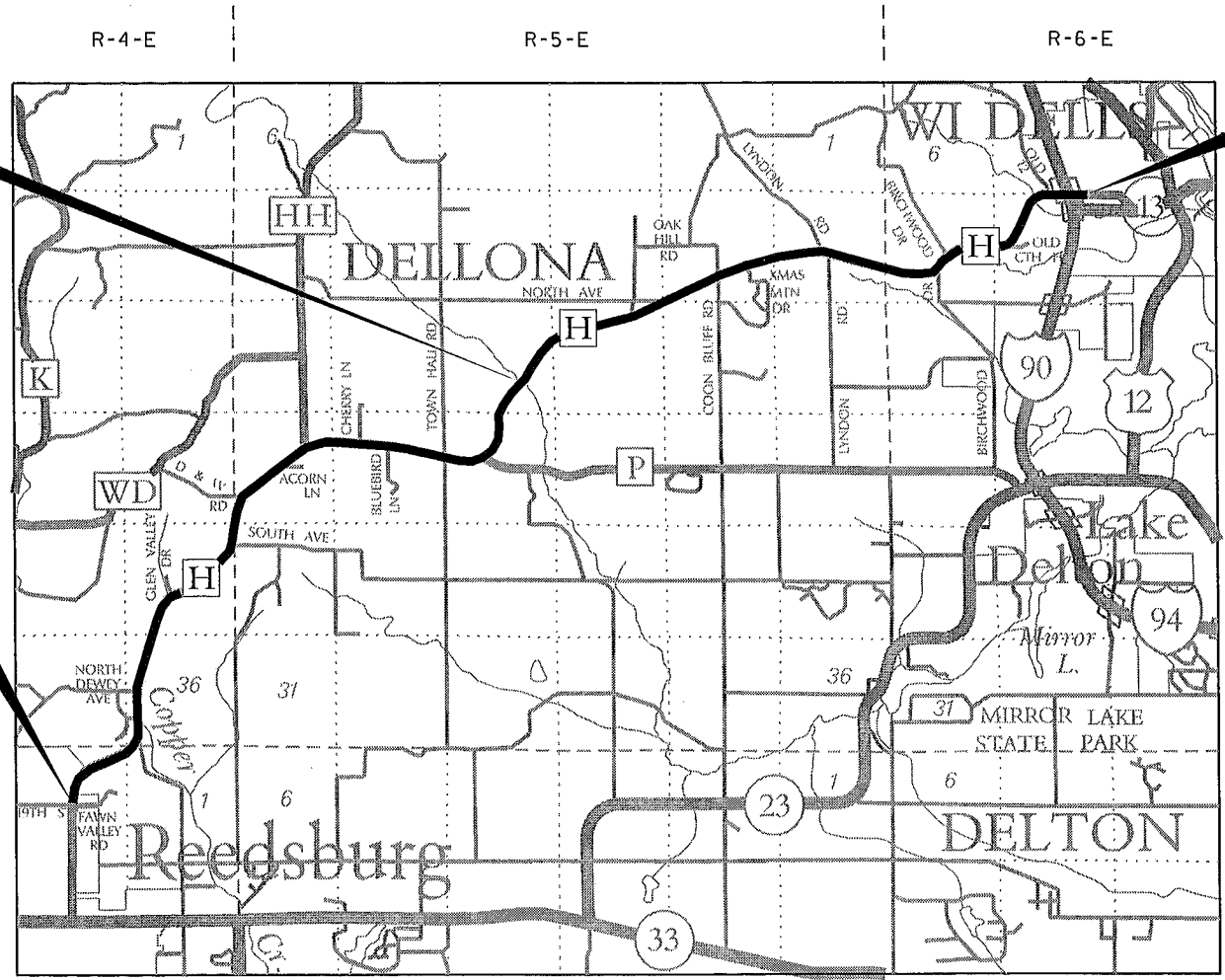
A.A.D.T. 2015	= 7600
A.A.D.T. 2035	= 9300
D.H.V. 2035	= 1330
D.D.	= 59/41
T.	= 10.3
DESIGN SPEED	= 55 MPH
ESALS	= 1,430,800

STRUCTURE
P-56-0070
STA 399+17.83 TO
STA 399+54.00

BEGIN PROJECT
STA 52+48.54
X = 586,629.34
Y = 265,465.80

END PROJECT
STA 698+68.81
X = 634,429.04
Y = 294,202.61

CONVENTIONAL SYMBOLS	
PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	
PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	



LAYOUT
SCALE 0 1 MI.

TOTAL NET LENGTH OF CENTERLINE = 12.238 MI.

"COORDINATES ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), 'SAUK' COUNTY."

ACCEPTED FOR
COUNTY OF SAUK

DATE: 7/14/2014
(Signature)
HIGHWAY COMMISSIONER
Title of Official

ORIGINAL PLANS PREPARED BY

MSA
PROFESSIONAL SERVICES
TRANSPORTATION • MUNICIPAL • REMEDIATION
DEVELOPMENT • ENVIRONMENTAL
1230 South Boulevard Baraboo, WI 53913
608-356-2771 1-800-362-4505 Fax: 608-356-2770

WISCONSIN
BOBBIL L. MAXWELL
39012-006 BARABOO WI
PROFESSIONAL ENGINEER
DATE: 7-6-14
(Professional Engineer)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	MSA PROFESSIONAL SERVICES
Designer	MSA PROFESSIONAL SERVICES
Management Consultant	KJOHNSON ENGINEERS
C.O. Examiner	

APPROVED FOR THE DEPARTMENT

DATE: 7/29/14
(Management Consultant Signature)

E

GENERAL NOTES

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

PAVEMENT REMOVAL WILL BE TO THE NEAREST JOINT OR A SAWED EDGE WILL BE REQUIRED AS DIRECTED BY THE ENGINEER.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY ARE TO BE FERTILIZED, SEEDED AND TEMPORARILY SEEDED AS DIRECTED BY THE ENGINEER.

ALL WASTE MATERIAL RESULTING FROM THE VARIOUS CONSTRUCTION OPERATIONS ADJACENT TO PAVEMENT UNDER TRAFFIC SHALL BE ENTIRELY REMOVED AND PROPERLY DISPOSED OF IMMEDIATELY OR AS DIRECTED BY THE ENGINEER.

THE LOW SIDE SHOULDER SLOPE ON SUPERELEVATION SECTIONS EQUALS SUPERELEVATION WHEN SUPERELEVATION IS GREATER THAN 4.00%. IF SUPERELEVATION IS LESS THAN OR EQUAL TO 4.00%, THEN THE LOW SIDE SHOULDER SLOPE IS 4.00%. HIGH SIDE SHOULDER SLOPE ON SUPERELEVATION SECTIONS EQUALS SUPERELEVATION.

THE QUANTITY OF THE ITEMS FOR EROSION PROTECTION INCLUDES AN UNDISTRIBUTED AMOUNT FOR PROTECTION, CONTROL AND ABATEMENT OF WATER POLLUTION RESULTING FROM SOIL EROSION. THE DISTRIBUTION AND LOCATION OF THESE MATERAILS ARE TO BE DETERMINED BY THE ENGINEER.

THE EXACT LOCATION OF PRIVATE ENTRANCES IS TO BE DETERMINED IN THE FIELD BY THE ENGINEER. ALL DRIVEWAYS ARE TO BE REPLACED IN KIND.

PURSUANT TO CHAPTER 59 OF THE WISCONSIN STATUTES, THE CONTRACTOR SHALL CAREFULLY MAKE A SEARCH FOR EVIDENCE OF A LANDMARK IN ALL AREAS WHERE SUCH A LANDMARK MAY EXIST.

HMA PAVEMENT TYPE E-3 TO BE PLACED IN TWO LAYERS. THE CONTRACTOR'S OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS. FOR THE 3½-INCH HMA PAVEMENT, THE LAYERS SHALL BE 1¾-INCHES WITH NOMINAL AGGREGATE SIZE OF 12.5MM. FOR THE 4-INCH HMA PAVEMENT, THE BOTTOM LAYER SHALL BE 2¼-INCHES WITH NOMINAL AGGREGATE SIZE OF 19MM, AND THE TOP LAYER SHALL BE 1¾-INCHES WITH NOMINAL AGGREGATE SIZE OF 12.5MM.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS.

HMA PAVEMENT CALCULATED BY USING 112 LB/SY/IN.

ABBREVIATIONS

AC	ACRES	FT	FOOT
AC	ASPHALT CEMENT	G.V.	GAS VALVE
ASPH	ASPHALT	INV	INVERT
AVG	AVERAGE	IP	IRON PIPE
ADT	AVERAGE DAILY TRAFFIC	JCT	JUNCTION
BAD	BASE AGGREGATE DENSE	LHF	LEFT HAND FORWARD
BM	BENCHMARK	L	LENGTH
CL	CENTERLINE	LS	LUMP SUM
CC	CENTER TO CENTER	NC	NORMAL CROWN
CIR	COLD IN-PLACE RECYCLING	N	NORTH
CONC	CONCRETE	Y	NORTH GRID COORDINATE
CSCP	CORRUGATED STEEL CULVERT PIPE	PC	POINT OF CURVATURE
CSM	CERTIFIED SURVEY MAP	PI	POINT OF INTERSECTION
CTH	COUNTY TRUNK HIGHWAY	PT	POINT OF TANGENCY
CULV	CULVERT	PL	PROPERTY LINE
CP	CULVERT PIPE	PE	PRIVATE ENTRANCE
C&G	CURB & GUTTER	R	RADIUS
D	DEGREE OF CURVE	REQD	REQUIRED
DHV	DESIGN HOURLY VOLUME	R/W	RIGHT-OF-WAY
DIA	DIAMETER	RHF	RIGHT HAND FORWARD
DWY	DRIVEWAY	SALV	SALVAGED
E	EAST	SHLDR	SHOULDER
X	EAST GRID COORDINATE	SDD	STANDARD DETAIL DRAWINGS
ELEV	ELEVATION	STA	STATION
EW	ENDWALL	SE	SUPERELEVATION
ENT	ENTRANCE	TAN	TANGENT
ESALS	EQUIVALENT SINGLE AXLE LOADS	TLE	TEMPORARY LIMITED EASEMENT
EXC	EXCAVATION	T	TRUCKS
EBS	EXCAVATION BELOW SUBGRADE	TYP	TYPICAL
EXIST	EXISTING	VERT	VERTICAL
FF	FACE TO FACE	VC	VERTICAL CURVE
FERT	FERTILIZER	VOL	VOLUME
FE	FIELD ENTRANCE	WV	WATER VALVE
FG	FINISHED GRADE	W	WELL

DESIGN CONTACT

MSA PROFESSIONAL SERVICES, INC.
ATTN: MS. BOBBIL MAXWELL
1230 SOUTH BOULEVARD
BARABOO, WI 53913
TELEPHONE: 608-355-8861
EMAIL: BMAXWELL@MSA-PS.COM

SAUK COUNTY, HWY COMMISSIONER
ATTN: STEVE MUCHOW
P.O. BOX 26
BARABOO, WI 53913
TELEPHONE: 608-355-4855
EMAIL: SMUCHOW@CO.SAUK.WI.US

DNR LIASON

WISCONSIN DEPARTMENT OF NATURAL RESOURCES
ATTN: ANDREW BARTA
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
TELEPHONE: 608-275-3308
EMAIL: ANDREW.BARTA@WISCONSIN.GOV

UTILITIES

*SANITARY SEWER
CITY OF WISCONSIN DELLS
ATTN: DAVE HOLZEM
DIRECTOR OF PUBLIC WORKS
300 LACROSSE STREET
P.O. BOX 655
WISCONSIN DELLS, WI 53965
608-253-2542
EMAIL: DHOLZEM@DELLSCITYGOV.COM

FIBER OPTIC
SAUK COUNTY
ATTN: TIM STIEVE
SAUK COUNTY COURTHOUSE ANNEX
510 BROADWAY
BARABOO, WI 53913
608-355-4419
EMAIL: TSTIEVE@CO.SAUK.WI.US

FIBER OPTIC
CHARTER COMMUNICATION
ATTN: HARLOW JARVIS
E10704 STATE HWY 33
BARABOO, WI 53913
608-448-2119
EMAIL: HARLOW.JARVIS@CHARTERCOM.COM

FIBER OPTIC
REEDSBURG UTILITY COMMISSION
ATTN: BRETT SCHUPPNER
501 UTILITY COURT
REEDSBURG, WI 53959
608-768-6435
BRETT.S@RUCLS.NET

FIBER OPTIC
WINDSTREAM KDL, INC.
ATTN: DENNIS RUESS
8531 COUNTY ROAD FF
WISCONSIN RAPIDS, WI 54494
608-512-5587
EMAIL: DENNIS.RUESS@WINDSTREAM.COM

TRANSMISSION LINES
ATC
ATTN: JIM OLSON
2 FEN OAK COURT
MADISON, WI 53718
608-877-3622
EMAIL: JROLSON@ATCLLC.COM

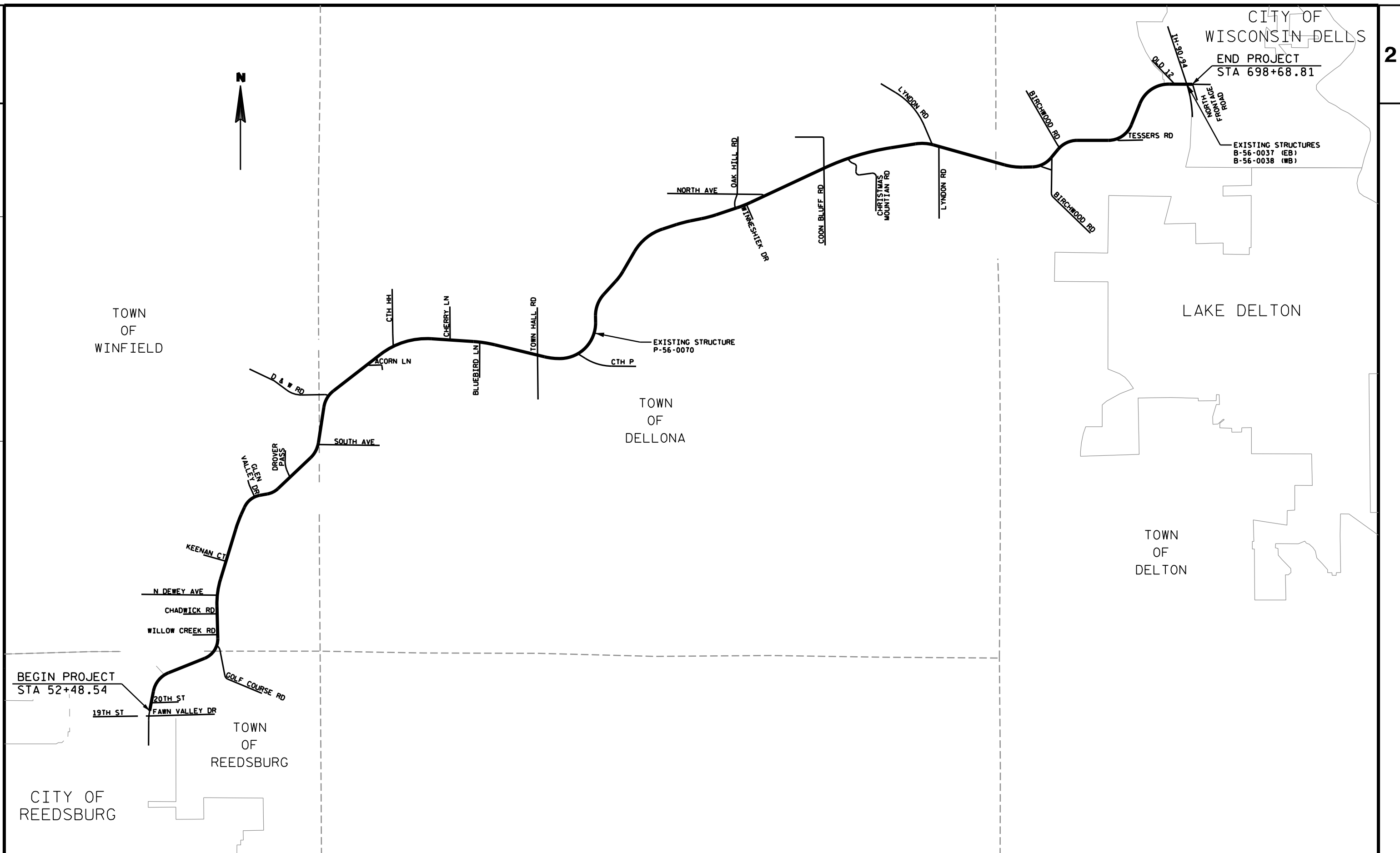
ELECTRIC & GAS
ALLIANT ENERGY
ATTN: TOM ORMSON
338 E. STATE STREET
MAUSTON, WI 53948
608-847-1319
EMAIL: TOMORMSON@ALLIANTENERGY.COM

TELEPHONE
FRONTIER COMMUNICATIONS
ATTN: JERRY MOORE
222 WEST WISCONSIN STREET
PORTAGE, WI 53901
608-742-9507
EMAIL: JEROLD.R.MOORE@FTR.COM

* NOT A DIGGER'S HOTLINE MEMBER



Dial 811 or (800) 242-8511
www.DiggersHotline.com



PROJECT NO: 5897-00-70

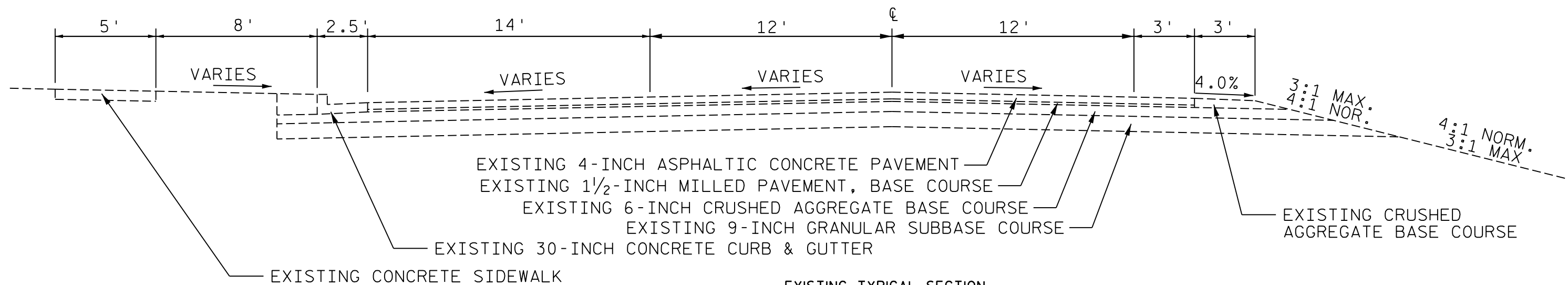
HWY: CTH H

COUNTY: SAUK

PROJECT OVERVIEW

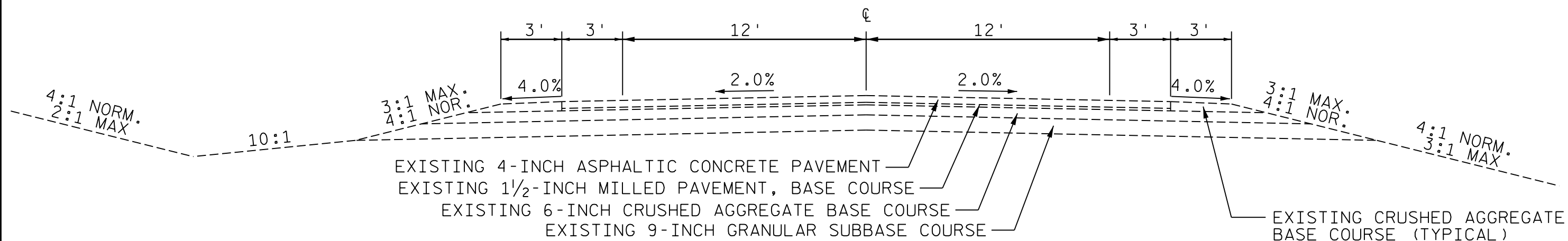
SHEET

E



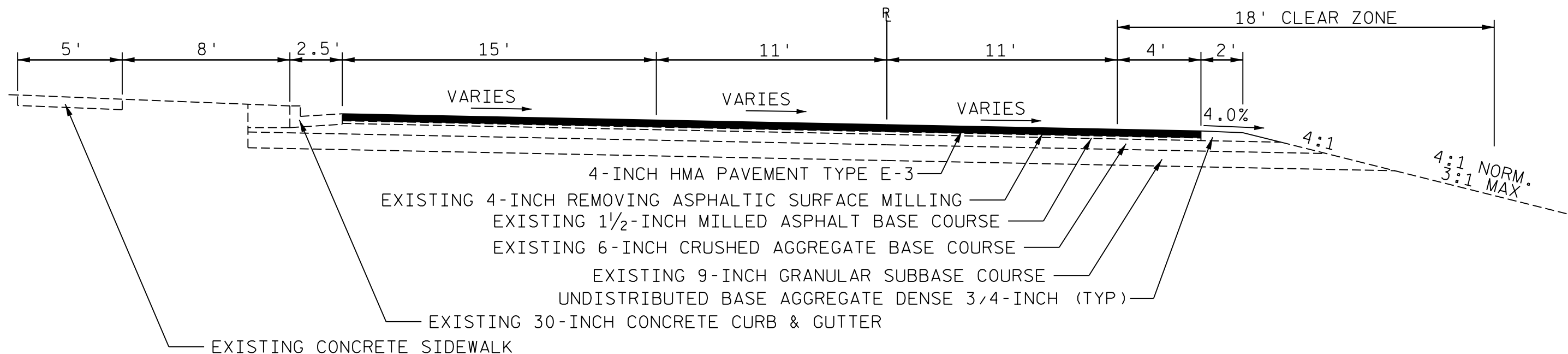
EXISTING TYPICAL SECTION

STA 52+48.54 - STA 58+61.53



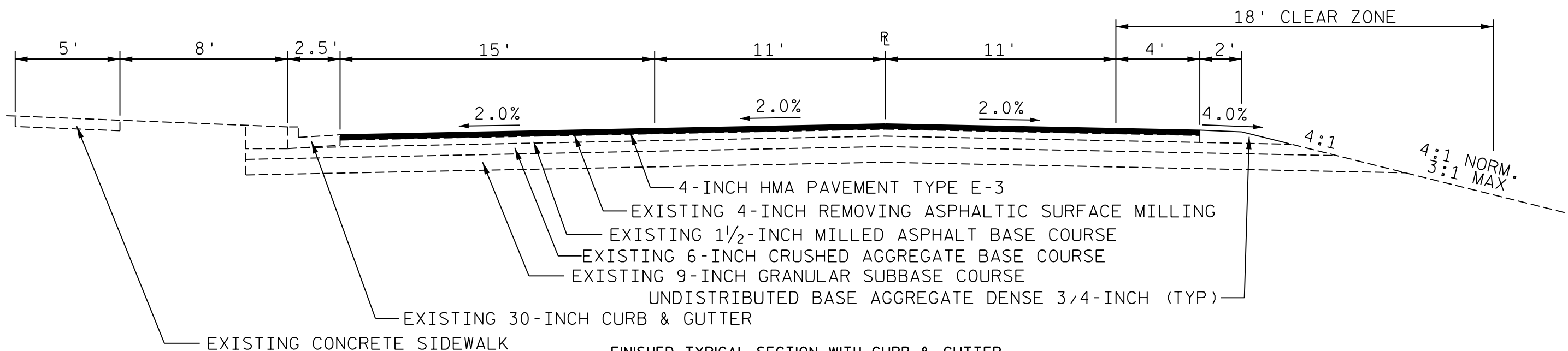
EXISTING TYPICAL SECTION

STA 58+61.53 - STA 698+68.81



FINISHED TYPICAL SUPERELEVATED SECTION

STA 52+48.54 - STA 52+72.86

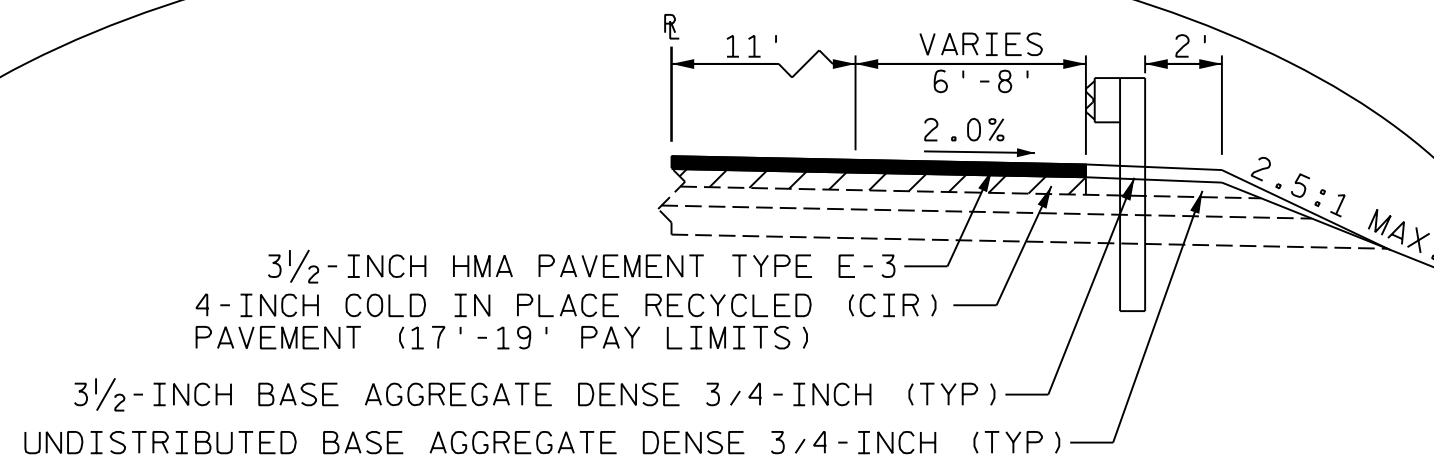


FINISHED TYPICAL SECTION WITH CURB & GUTTER

STA 52+72.86 - STA 58+61.53

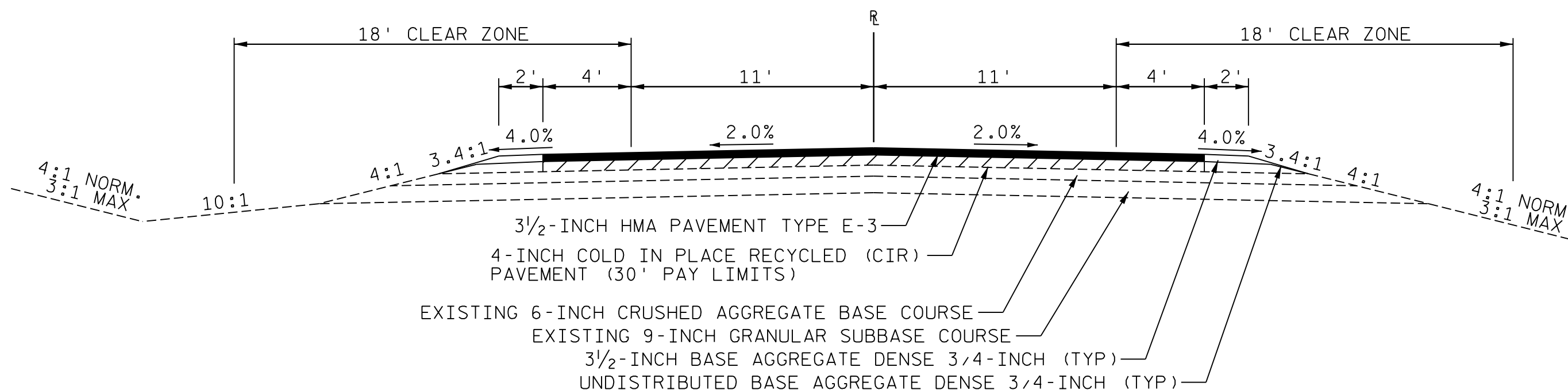
POST 1 LOCATIONS

STATION	OFFSET
77+43	19'-3 1/4" RT
83+55	19'-3 1/4" LT
86+24	19'-3 1/4" LT
83+03	19'-3 1/4" RT
142+74	19'-3 1/4" LT & RT
144+06	19'-3 1/4" LT & RT
224+66	19'-3 1/4" LT
238+45	19'-3 1/4" RT
239+77	19'-3 1/4" RT
238+54	19'-3 1/4" LT
239+73	19'-3 1/4" LT
304+02	19'-3 1/4" RT
305+34	19'-3 1/4" RT
304+00	19'-3 1/4" LT
305+32	19'-3 1/4" LT
321+16	19'-3 1/4" LT
322+47	19'-3 1/4" LT
321+12	19'-3 1/4" RT
322+44	19'-3 1/4" RT



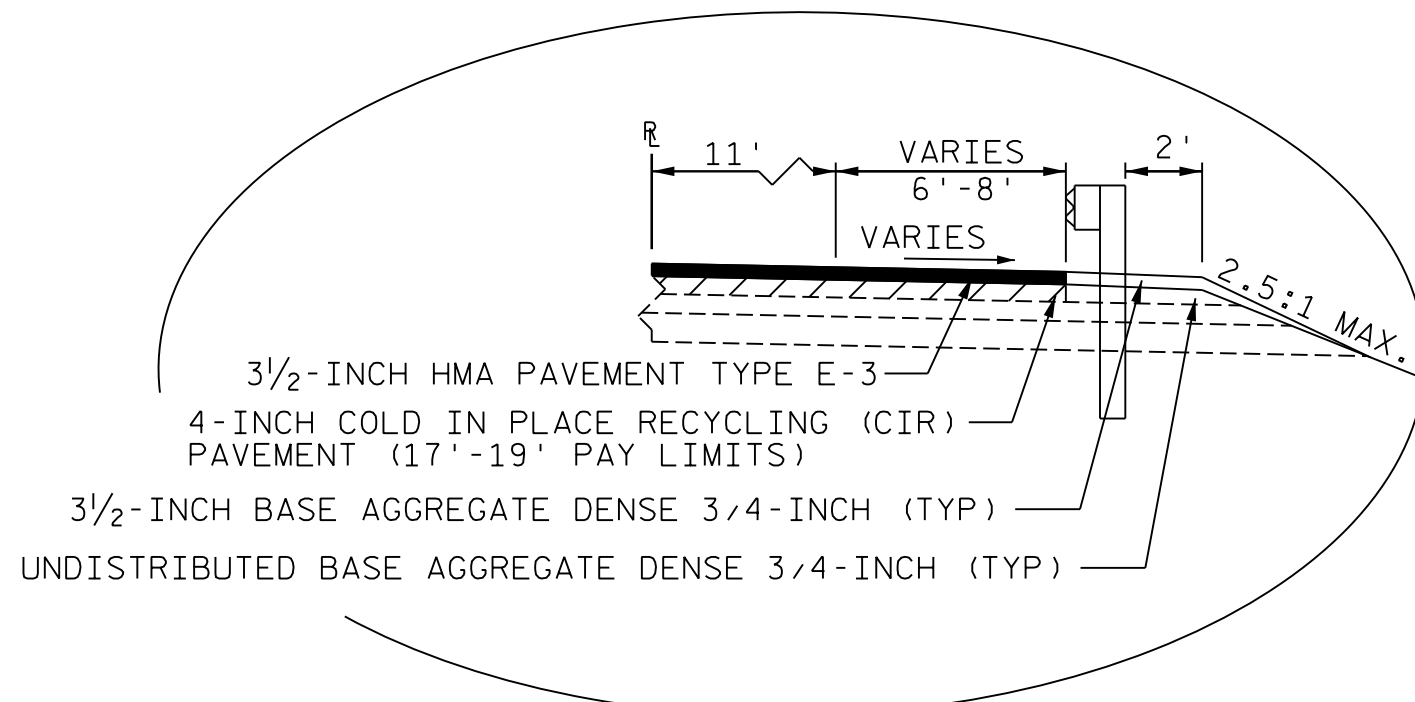
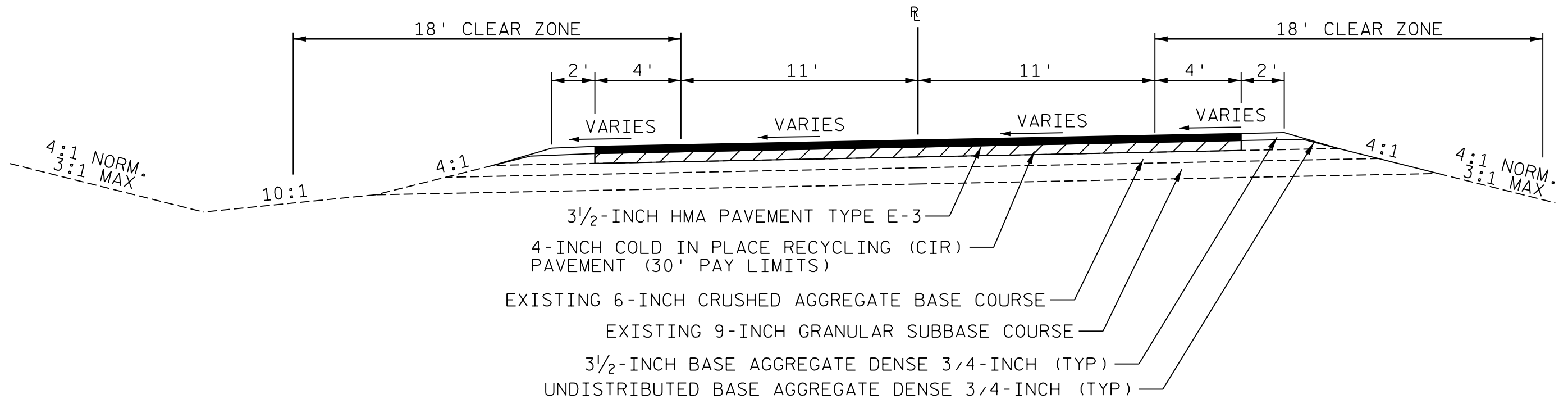
FINISHED TYPICAL SECTION WITH BEAM GUARD

STA 74+69.95 - STA 77+41 RT
STA 83+57 - STA 86+22 LT
STA 83+05 - STA 89+24.82 RT
STA 142+75 - STA 144+05 LT & RT
STA 224+72 - STA 226+35.40 LT
STA 238+50 - STA 239+72 RT
STA 238+56 - STA 239+71 LT
STA 304+04 - STA 305+32 RT
STA 304+02 - STA 305+30 LT
STA 321+17 - STA 322+46 LT
STA 321+14 - STA 322+42 RT



FINISHED TYPICAL SECTION

STA 58+61.53 - STA 59+95.82
STA 74+69.95 - STA 89+24.82
STA 104+18.94 - STA 115+38.50
STA 131+99.38 - STA 151+32.22
STA 175+51.34 - STA 175+78.28
STA 184+57.93 - STA 201+90.40
STA 213+08.98 - STA 226+35.40
STA 238+48.35 - STA 262+22.43
STA 291+10.70 - STA 306+01.85
STA 320+81.69 - STA 340+15.61
STA 390+18.22 - STA 394+89.42
STA 404+33.86 - STA 411+22.94
STA 431+54.78 - STA 434+10.23
STA 445+41.35 - STA 445+82.16
STA 457+70.88 - STA 462+93.85
STA 474+50.27 - STA 551+07.19
STA 562+72.62 - STA 593+02.30
STA 605+34.91 - STA 606+19.73
STA 619+20.63 - STA 620+94.18
STA 634+24.47 - STA 645+27.29
STA 662+12.61 - STA 669+28.11
STA 689+69.41 - STA 693+00.00

**FINISHED SUPERELEVATED TYPICAL SECTION**

STA 59+95.82 - STA 74+69.95
STA 89+24.82 - STA 104+18.94
STA 115+38.50 - STA 131+99.38
STA 151+32.22 - STA 175+51.34
STA 175+78.28 - STA 184+57.93
STA 201+90.40 - STA 213+08.98
STA 226+35.40 - STA 238+48.35
STA 262+22.43 - STA 291+10.70
STA 306+01.85 - STA 320+81.69
STA 340+15.61 - STA 390+18.22

STA 394+89.42 - STA 404+33.86
STA 411+22.94 - STA 431+54.78
STA 434+10.23 - STA 445+41.35
STA 445+82.16 - STA 457+70.88
STA 462+93.85 - STA 474+50.27
STA 551+07.19 - STA 562+72.62
STA 593+02.30 - STA 605+34.91
STA 606+19.73 - STA 619+20.63
STA 620+94.18 - STA 634+24.47
STA 645+27.29 - STA 662+12.61
STA 669+28.11 - STA 689+69.41

POST 1 LOCATIONS

STATION	OFFSET
62+47	19'-3 1/4" LT
63+78	19'-3 1/4" LT
62+21	19'-3 1/4" RT
63+64	19'-3 1/4" RT
74+11	19'-3 1/4" RT
89+85	19'-3 1/4" RT
94+84	19'-3 1/4" RT
98+27	19'-3 1/4" RT
99+77	19'-3 1/4" RT
101+58	19'-3 1/4" RT
115+48	19'-3 1/4" LT & RT
116+92	19'-3 1/4" LT & RT
168+99	19'-3 1/4" LT & RT
170+31	19'-3 1/4" LT & RT

POST 1 LOCATIONS

STATION	OFFSET
177+30	19'-3 1/4" RT
181+74	19'-3 1/4" RT
233+10	19'-3 1/4" LT
234+13	19'-3 1/4" LT
235+82	19'-3 1/4" LT
266+25	19'-3 1/4" LT & RT
267+57	19'-3 1/4" LT & RT
273+45	19'-3 1/4" LT
274+77	19'-3 1/4" LT
273+44	19'-3 1/4" RT
274+76	19'-3 1/4" RT
397+37	19'-3 1/4" RT
397+87	19'-3 1/4" RT
398+47	19'-3 1/4" LT

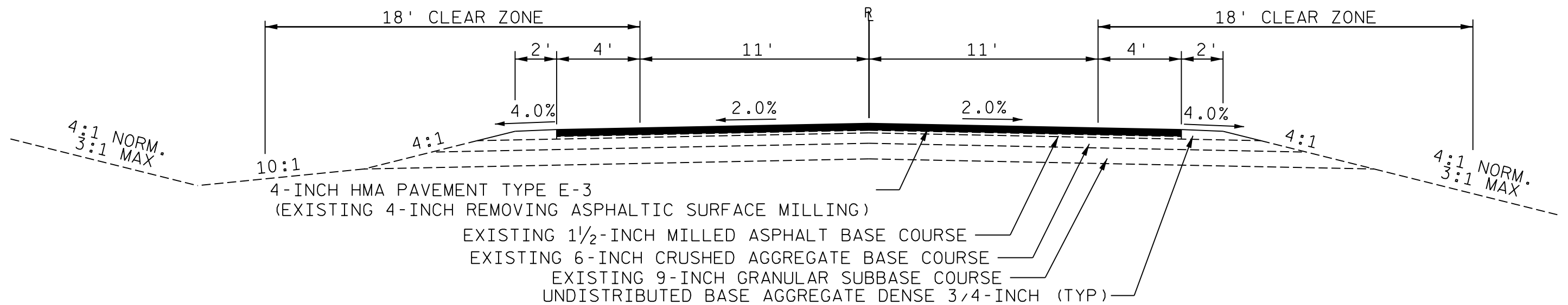
POST 1 LOCATIONS

STATION	OFFSET
400+23	19'-3 1/4" RT
400+73	19'-3 1/4" RT
400+88	19'-3 1/4" LT
397+97	19'-3 1/4" LT
401+38	19'-3 1/4" LT
678+16	19'-3 1/4" RT
681+09	19'-3 1/4" RT
679+44	19'-3 1/4" LT
681+75	19'-3 1/4" LT
694+64	19'-3 1/4" LT
694+76	19'-3 1/4" RT
697+70	19'-3 1/4" LT
697+82	19'-3 1/4" RT

FINISHED TYPICAL SUPERELEVATED SECTION WITH BEAM GUARD

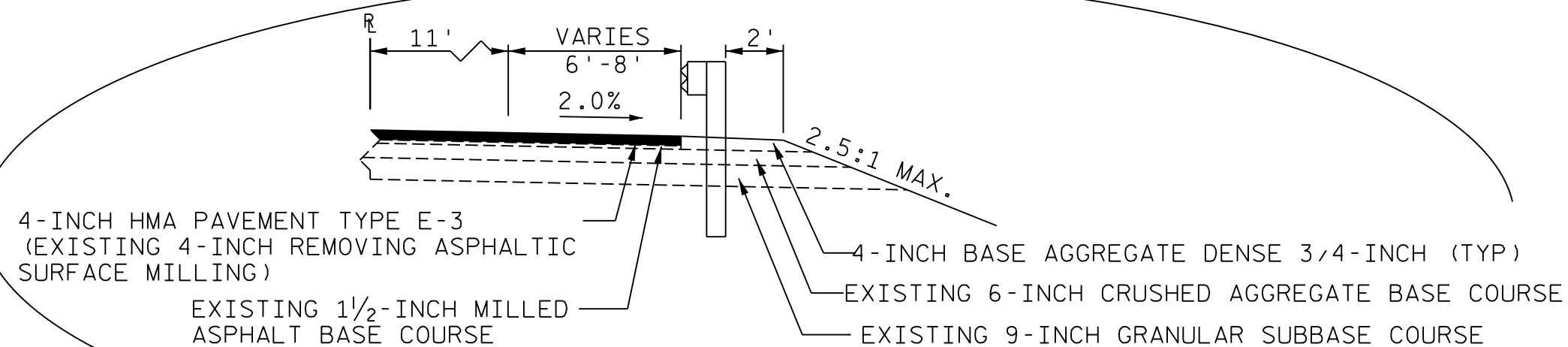
STA 62+50 - STA 63+75 LT
STA 62+25 - STA 63+60 RT
STA 74+13 - STA 74+69.95 RT
STA 89+24.82 - STA 89+83 RT
STA 94+88 - STA 98+23 RT
STA 99+80 - STA 101+55 RT
STA 115+50 - STA 116+90 LT & RT
STA 169+00 - STA 170+30 LT & RT
STA 177+36 - STA 181+68 RT

STA 226+35.40 - STA 233+04 LT
STA 234+18 - STA 235+77 LT
STA 266+27 - STA 267+55 LT & RT
STA 273+47 - STA 274+75 LT
STA 273+45 - STA 274+75 RT
STA 397+50 - STA 398+00 RT
STA 400+10 - STA 400+60 RT
STA 398+10 - STA 398+60 LT
STA 400+75 - STA 401+25 LT
STA 678+21 - STA 681+04 RT
STA 679+47 - STA 681+72 LT



FINISHED TYPICAL SECTION

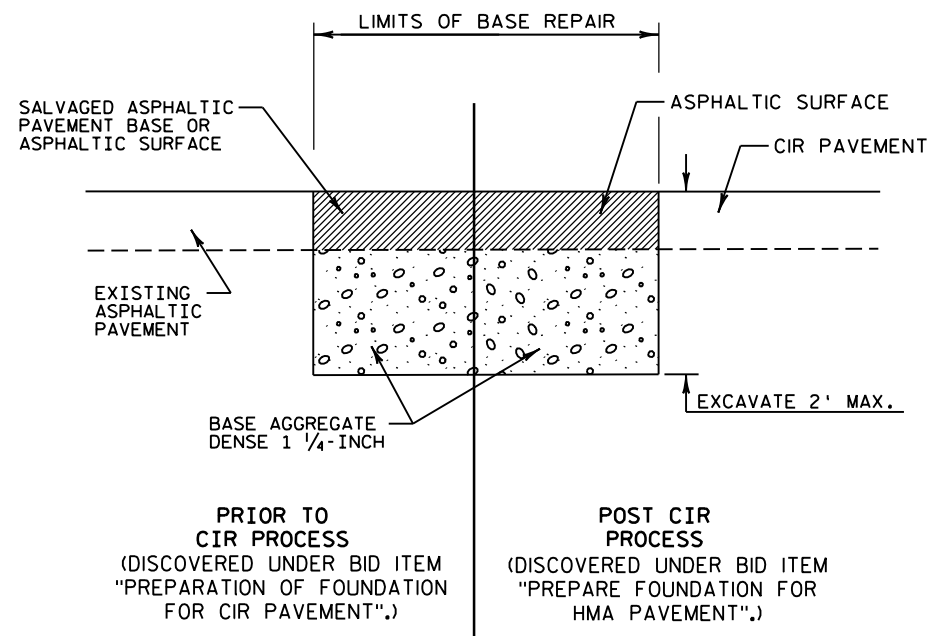
STA 693+00.00 - STA 698+68.81



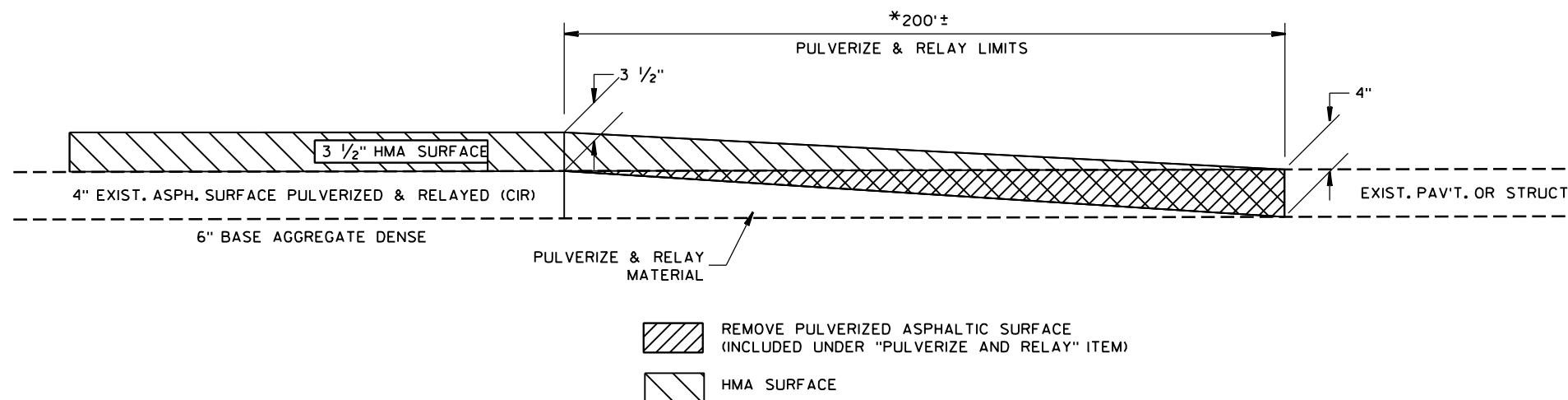
FINISHED TYPICAL SECTION WITH BEAM GUARD

STA 694+66 - STA 697+68 LT (REVERSED)
STA 694+78 - STA 697+80 RT

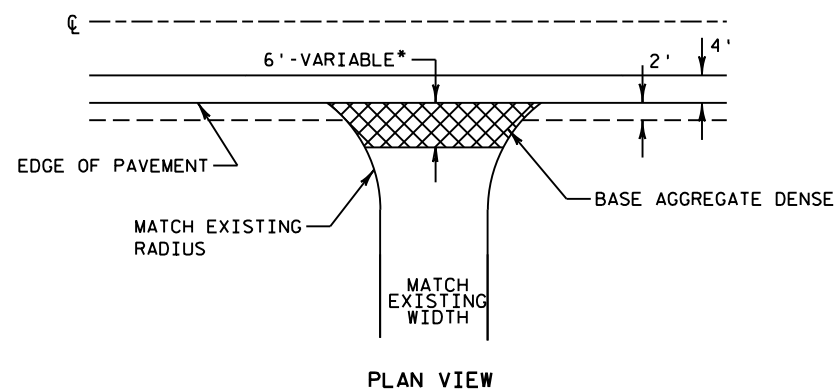
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**BASE REPAIR FOR CIR PAVEMENT**

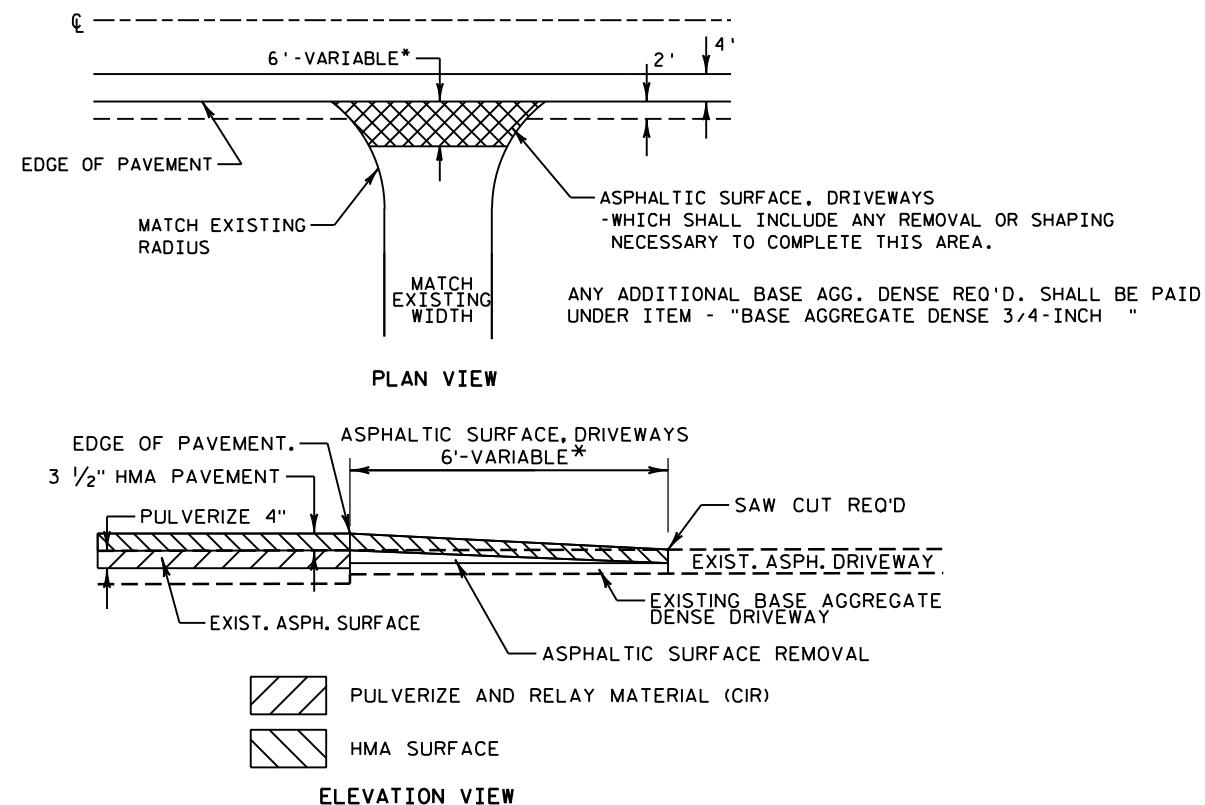
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**TYPICAL TRANSITION DETAIL****200' TRANSITIONS**

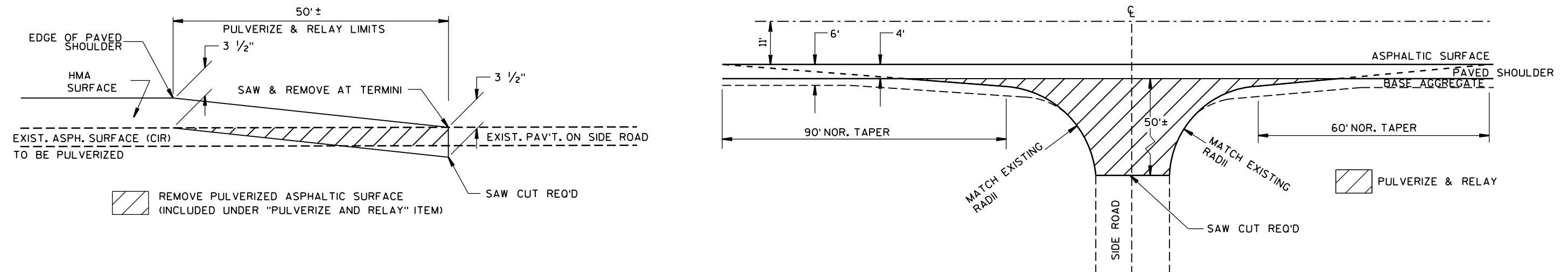
STA 58+61.53 - STA 60+61.53
 STA 397+17.83 - STA 399+17.83
 STA 399+54.00 - STA 401+54.00
 STA 691+00.00 - STA 693+00.00

**BASE AGGREGATE DRIVEWAY DETAIL**

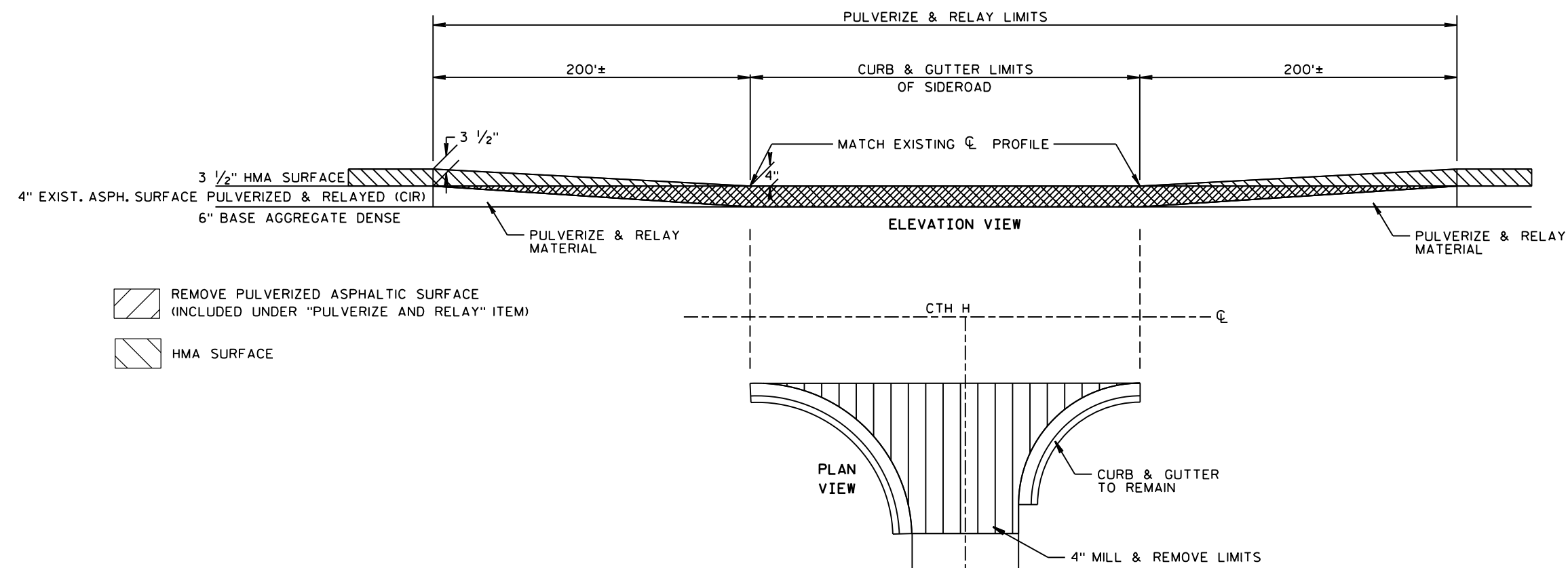
*EXACT DIMENSIONS TO BE DETERMINED
 BY ENGINEER IN THE FIELD

**ASPHALTIC DRIVEWAY DETAIL**

*EXACT DIMENSIONS TO BE DETERMINED
 BY ENGINEER IN THE FIELD

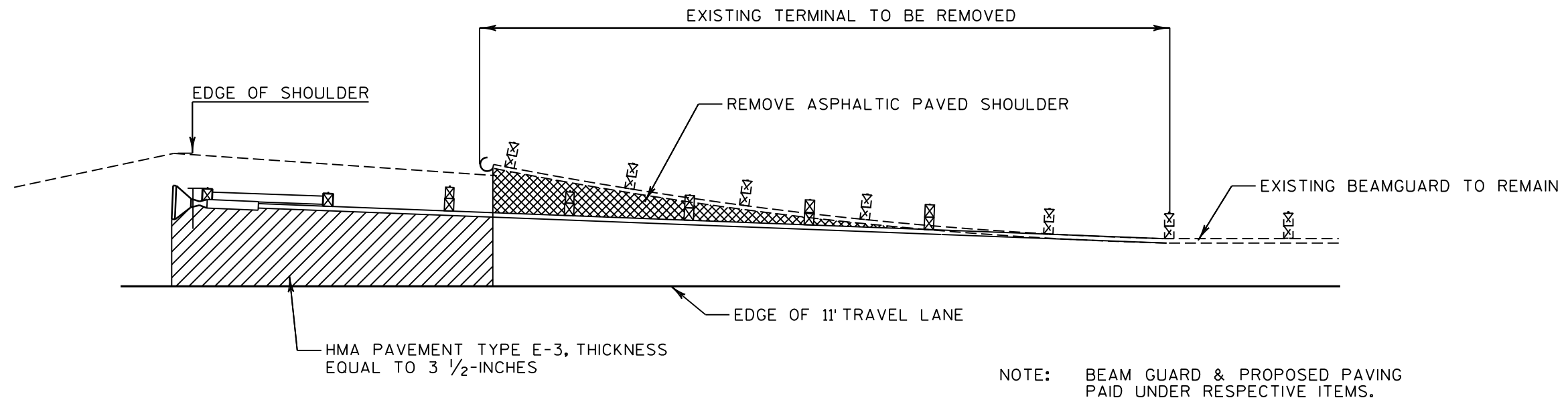


SIDEROAD DETAIL, WITHOUT CURB & GUTTER



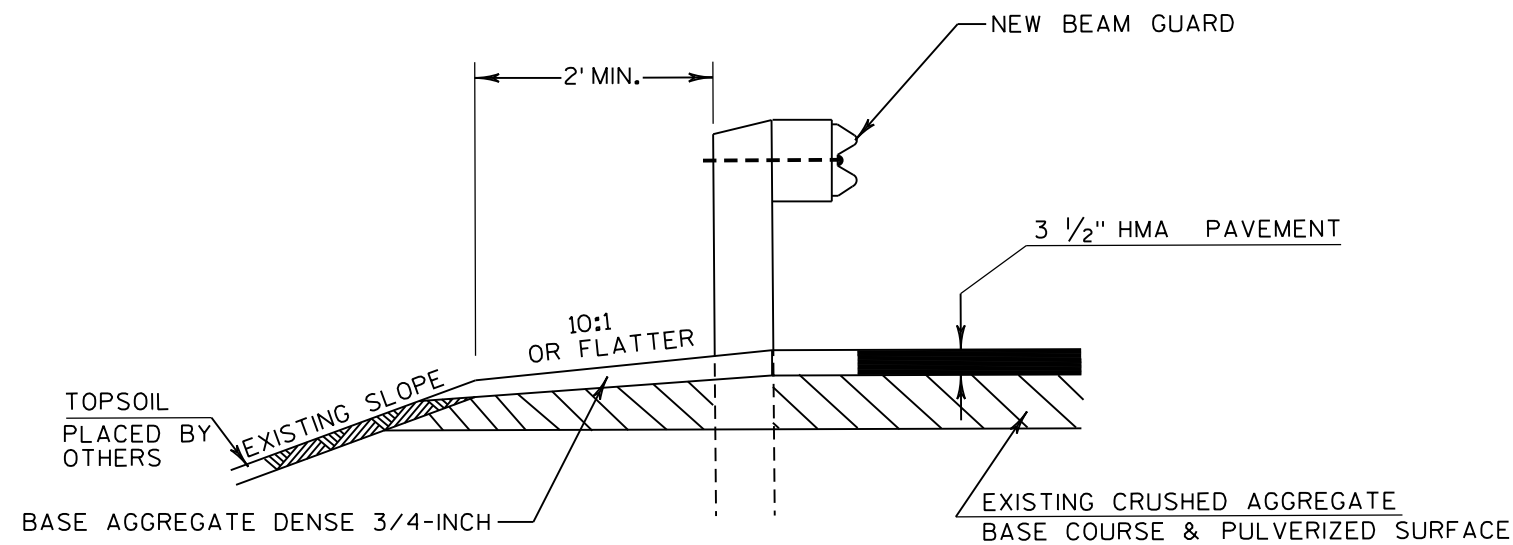
SIDEROAD DETAIL, WITH CURB & GUTTER
CTH P & WINNESHIEK DR INTERSECTIONS

STA 355+00 - STA 360+42
 STA 467+45 - STA 472+50



BEAM GUARD TERMINAL UPGRADE AT EXISTING BRIDGE (P-56-0070)

SEE S.D.D. "STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL".



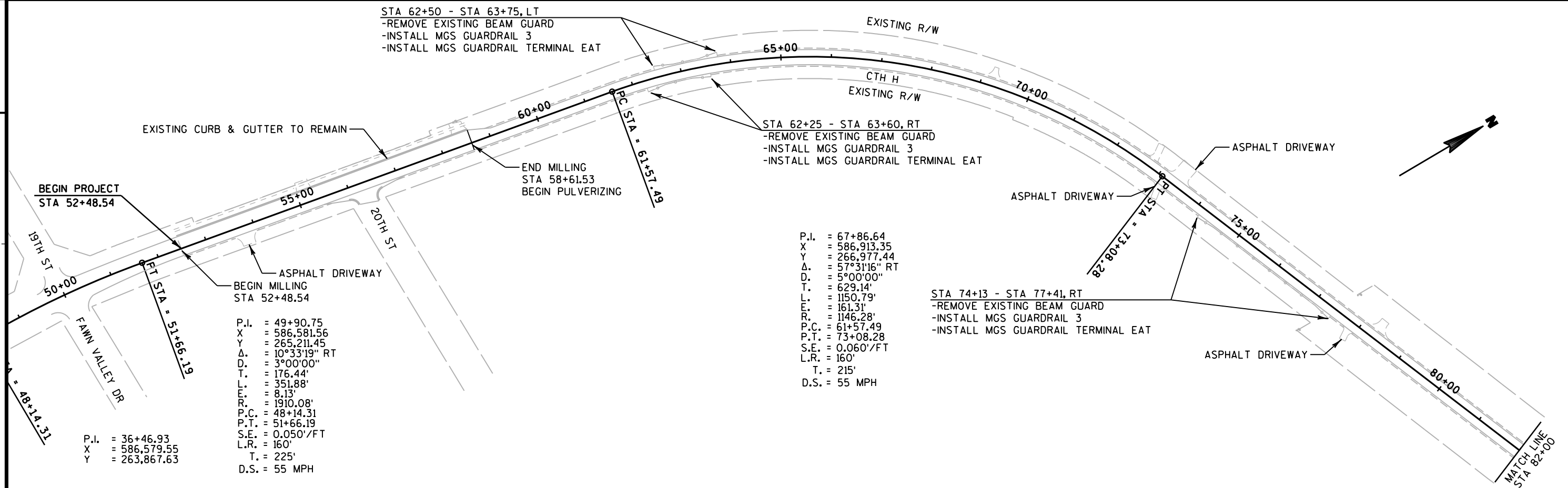
ASPHALTIC SHOULDER AT GUARD RAIL

RUNOFF COEFFICIENT TABLE

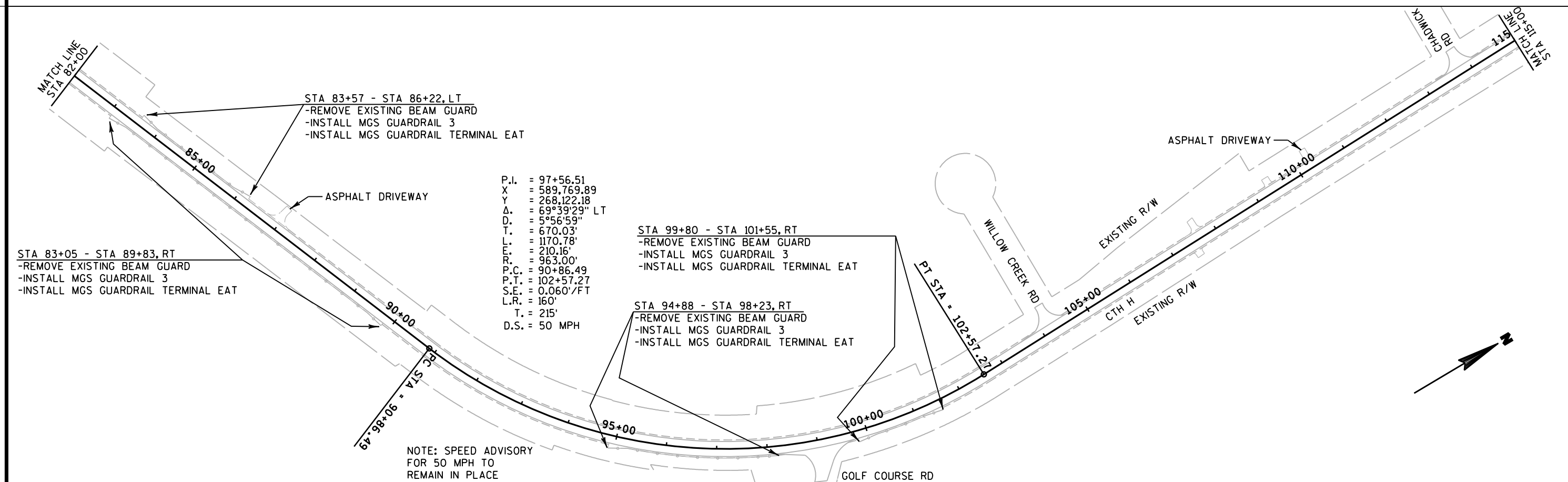
	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 50.4 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 50.4 ACRES

2



2



PROJECT NO: 5897-00-70

HWY: CTH H

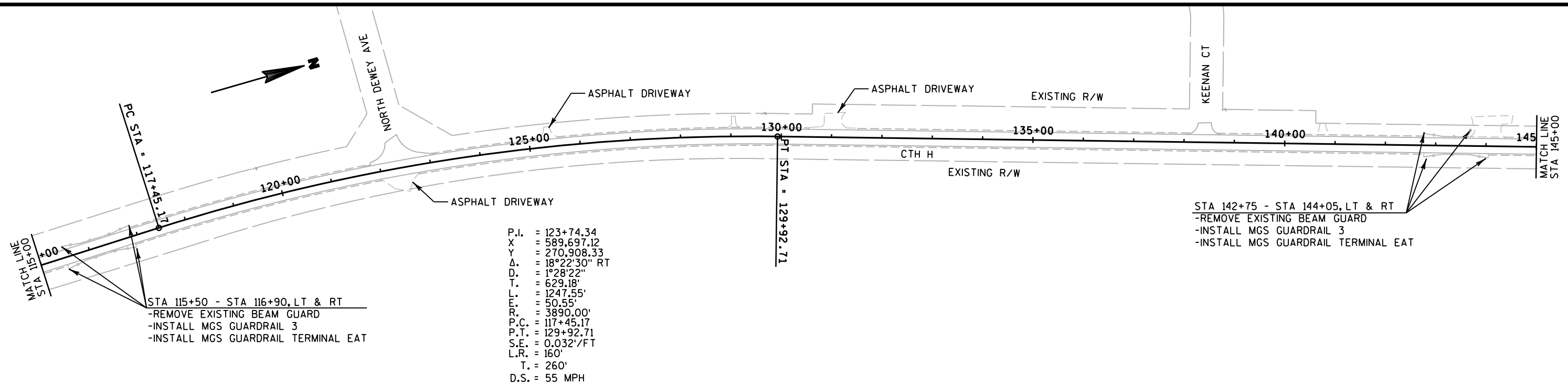
COUNTY: SAUK

PLAN DETAIL

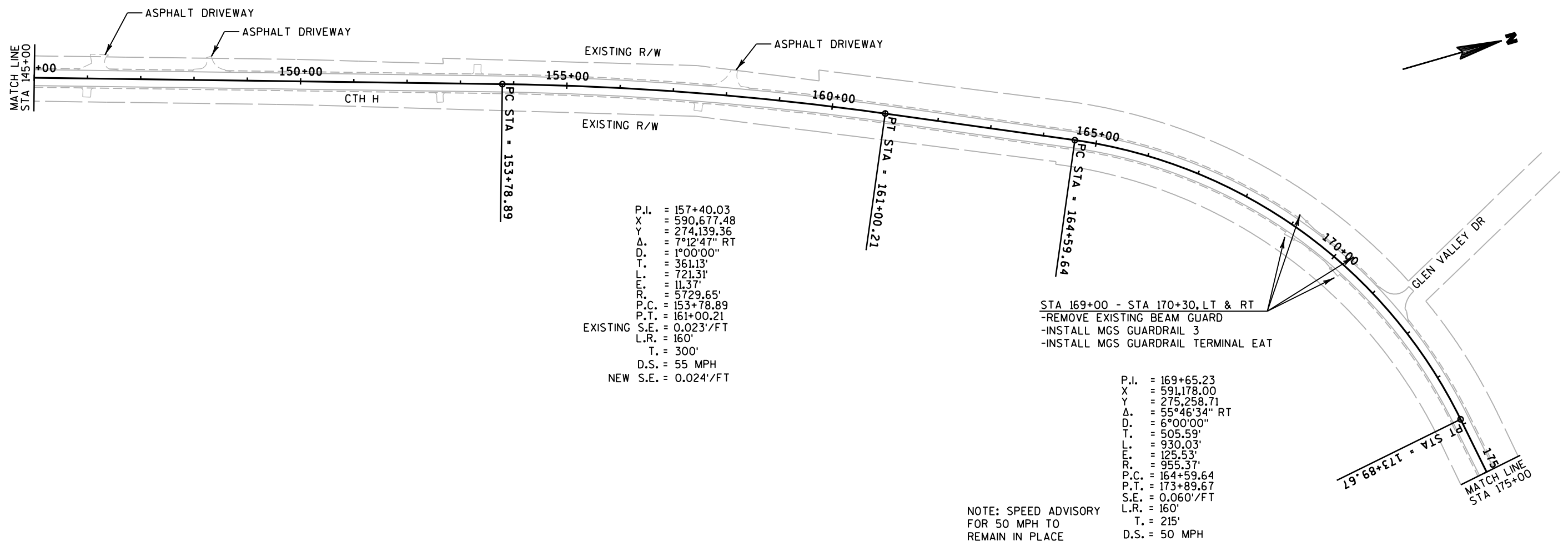
SHEET

E

2



2



PROJECT NO:5897-00-70

HWY:CTH H

COUNTY:SAUK

PLAN DETAIL

SHEET

E

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PLOT DATE : 7/16/2014

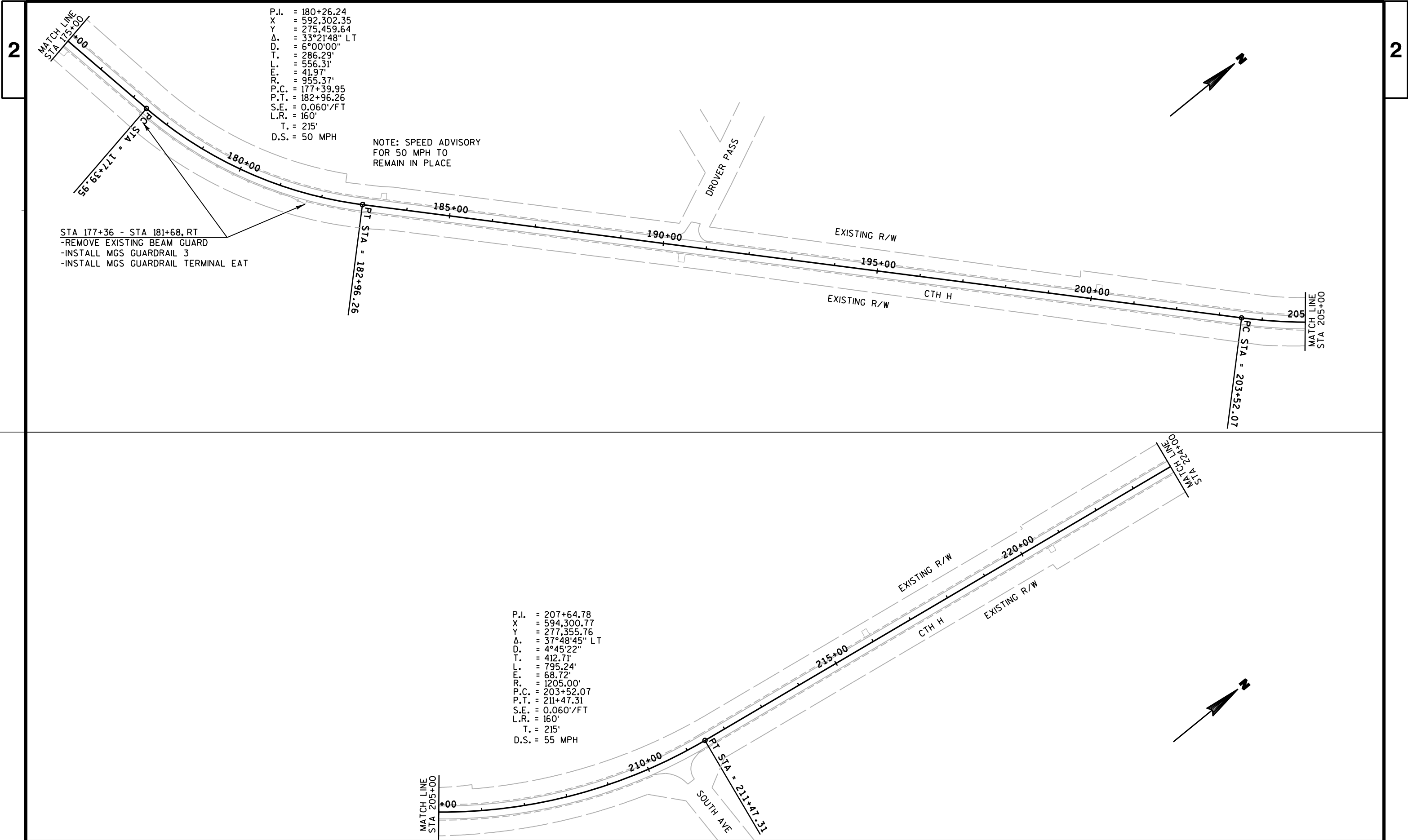
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PLOT NAME :

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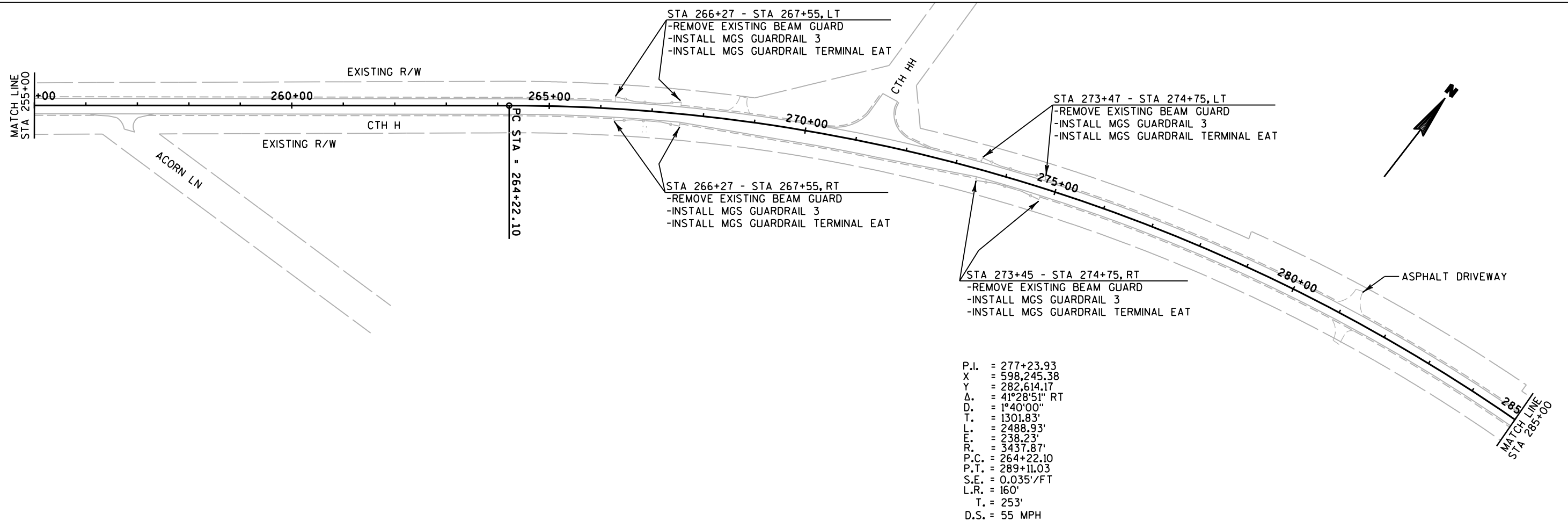
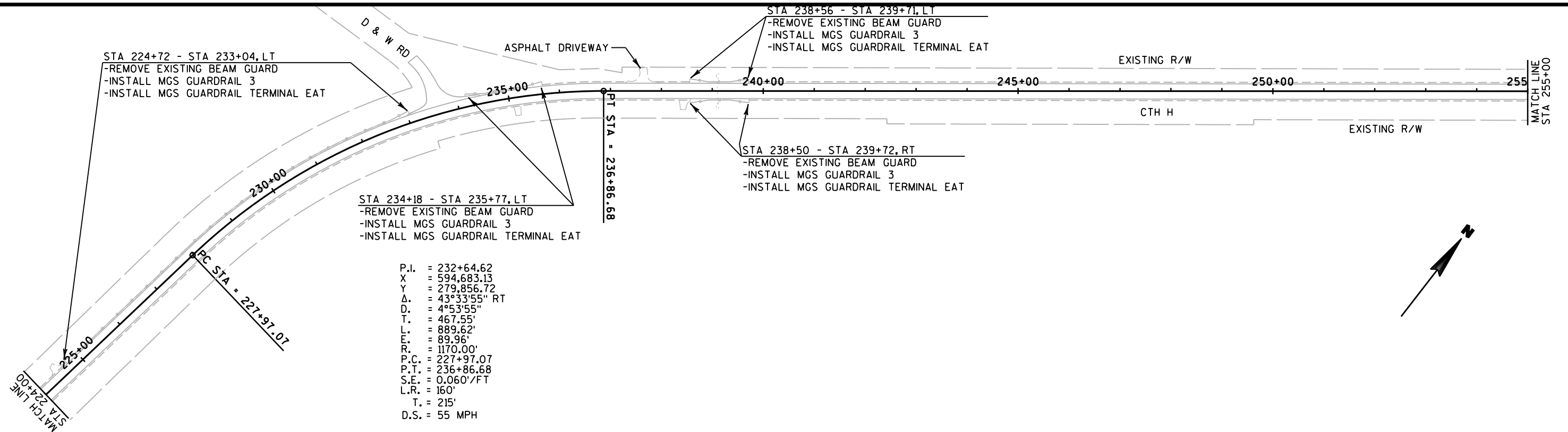
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021202_pd.dgn 7/16/2014 2:36:43 PM blee



2

2



PROJECT NO: 5897-00-70

HWY: CTH H

COUNTY: SAUK

PLAN DETAIL

SHEET

E

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PLOT DATE : 7/16/2014

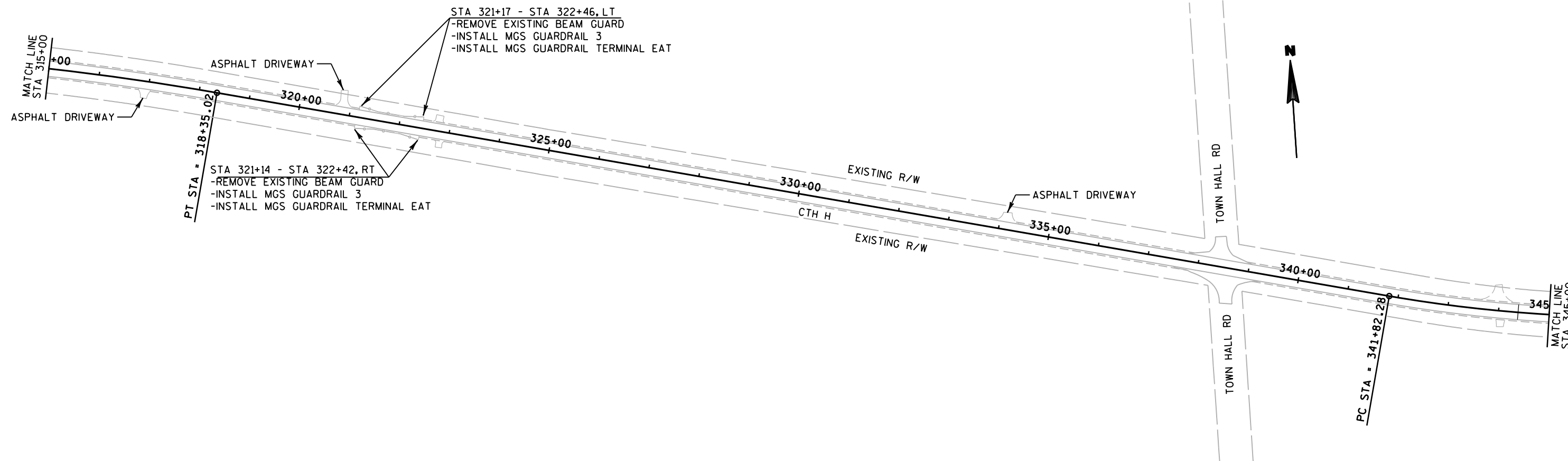
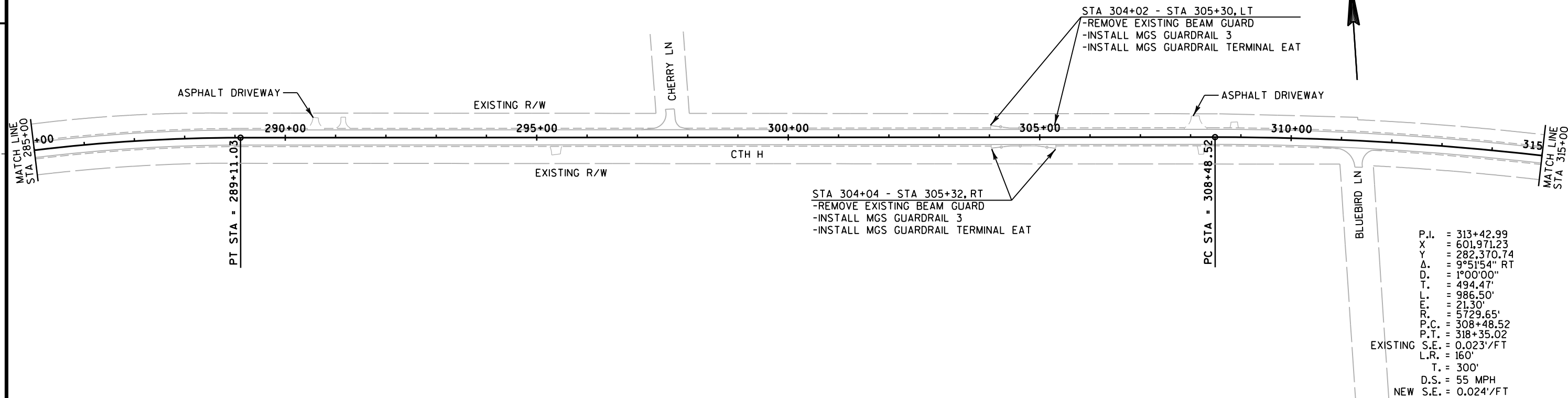
PLOT BY : blee

PLOT NAME :

PLOT SCALE : 1:2400

WISDOT/CADDs SHEET 42

021204_pd.dgn 7/16/2014 2:36:45 PM blee



2

P.I. = 345+78.45
X = 605,118.30
Y = 281,609.21
Δ = 16°07'04" LT
D. = 2°02'52"
T. = 396.17'
L. = 787.10'
E. = 27.91'
R. = 2797.98'
P.C. = 341+82.28
P.T. = 349+69.38
S.E. = 0.054'/FT
L.R. = 160'
T. = 220'
D.S. = 55 MPH

PC STA = 349+69.38
350+00
355+00

STA 355+00 - STA 360+42
SEE DETAIL "SIDEROAD DETAIL,
WITH CURB & GUTTER"

EXISTING R/W
CTH H
EXISTING R/W

P.I. = 365+38.78
X = 607,081.98
Y = 281,695.45
Δ = 87°54'52" LT
D. = 3°31'15"
T. = 1569.40'
L. = 2497.38'
E. = 633.40'
R. = 1627.60'
P.C. = 349+69.38
P.T. = 374+66.76
S.E. = 0.054'/FT
L.R. = 160'
T. = 220'
D.S. = 55 MPH

ASPHALT DRIVEWAY

370+00

ASPHALT DRIVEWAY

ASPHALT DRIVEWAY

ASPHALT DRIVEWAY

PC STA = 377+88.16
380+00
MATCH LINE
STA 380+00

2

P.I. = 383+48.58
X = 607,063.62
Y = 284,146.61
Δ = 42°43'56" RT
D. = 4°00'00"
T. = 560.41'
L. = 1068.38'
E. = 105.72'
R. = 1432.50'
P.C. = 377+88.16
P.T. = 388+56.55
S.E. = 0.057'/FT
L.R. = 160'
T. = 215'
D.S. = 55 MPH

385+00

PT STA = 388+56.55

390+00

CTH H

EXISTING R/W

EXISTING R/W

STRUCTURE P-56-0070
STA 399+17.83 TO STA 399+54.00

ASPHALT DRIVEWAY

395+00

P.I. = 399+62.71
X = 608,185.30
Y = 285,379.20
Δ = 12°04'35" LT
D. = 2°06'00"
T. = 288.62'
L. = 575.10'
E. = 15.22'
R. = 2728.52'
P.C. = 396+74.09
P.T. = 402+49.19
S.E. = 0.041'/FT
L.R. = 160'
T. = 238'
D.S. = 55 MPH

PC STA = 396+74.09

STA 397+97 - STA 398+47, LT
STA 400+88 - STA 401+38, LT
-REMOVE EXISTING BEAM GUARD TERMINALS
-INSTALL STEEL PLATE BEAM GUARD EAT

STA 397+37 - STA 397+87, RT
STA 400+23 - STA 400+73, RT
-REMOVE EXISTING BEAM GUARD TERMINALS
-INSTALL STEEL PLATE BEAM GUARD EAT

DELL CREEK

PT STA = 402+49.19

405+00

410+00
MATCH LINE
STA 410+00

PROJECT NO: 5897-00-70

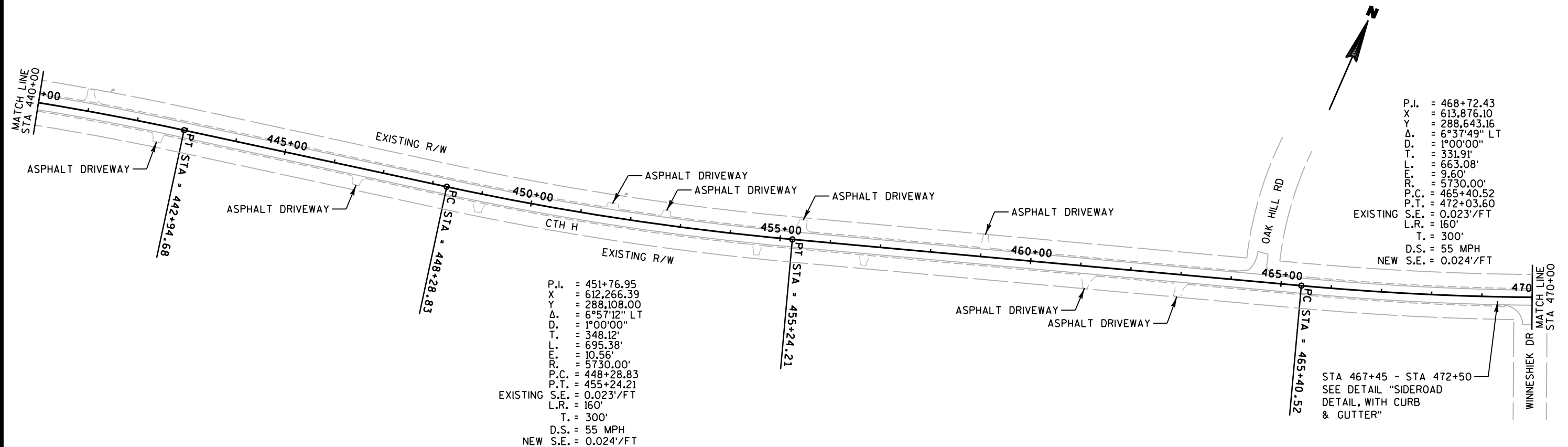
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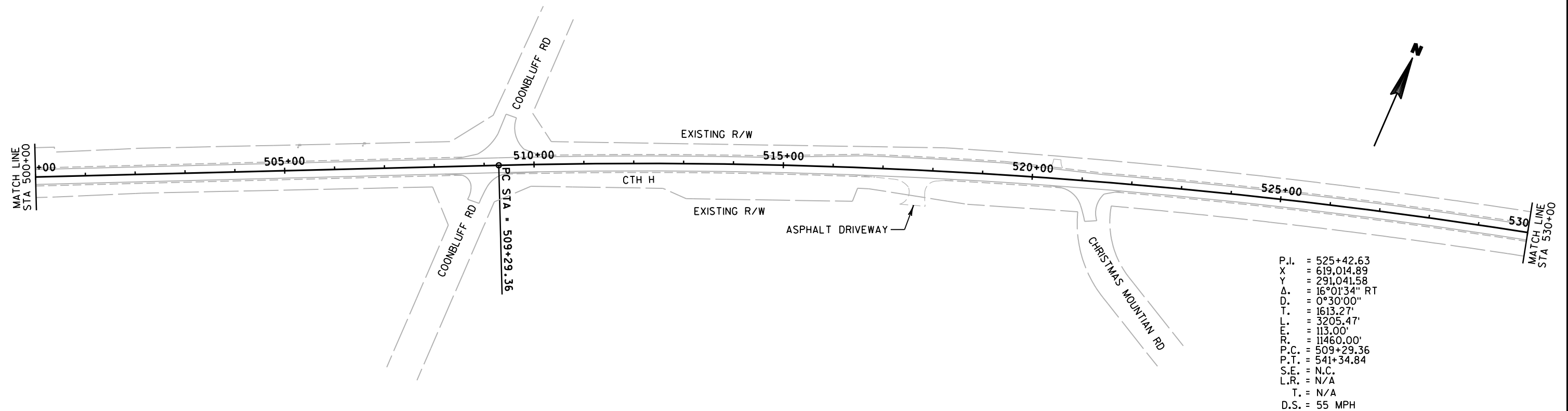
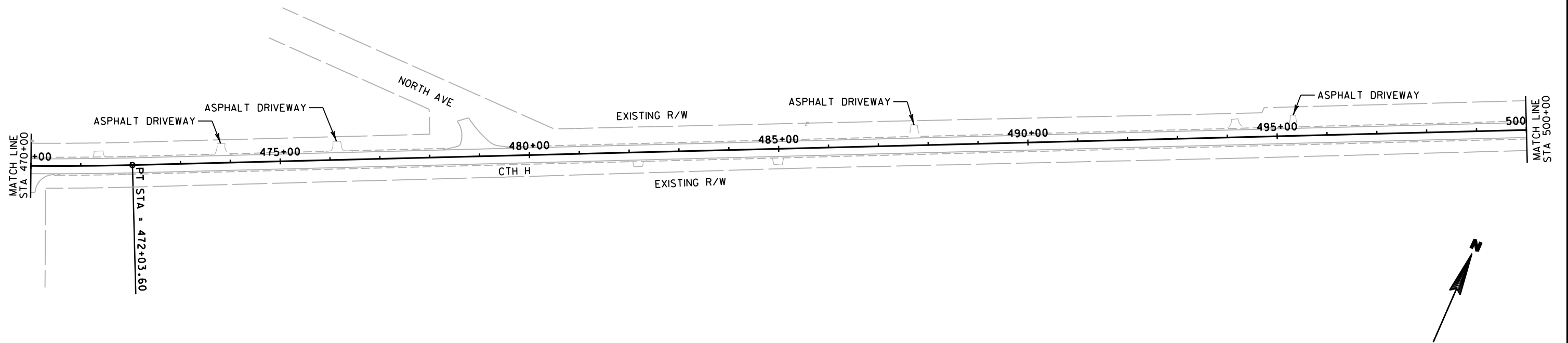
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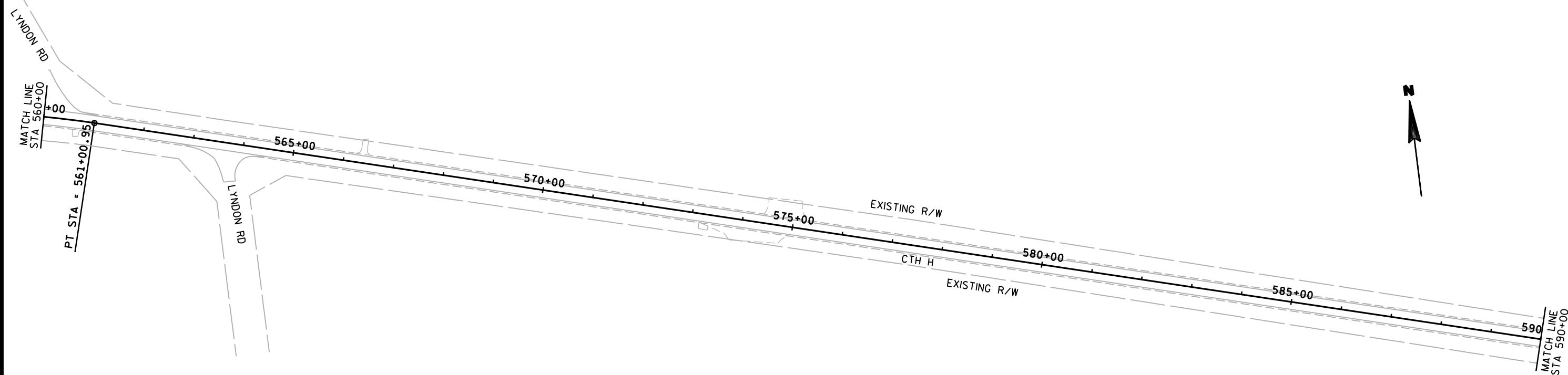
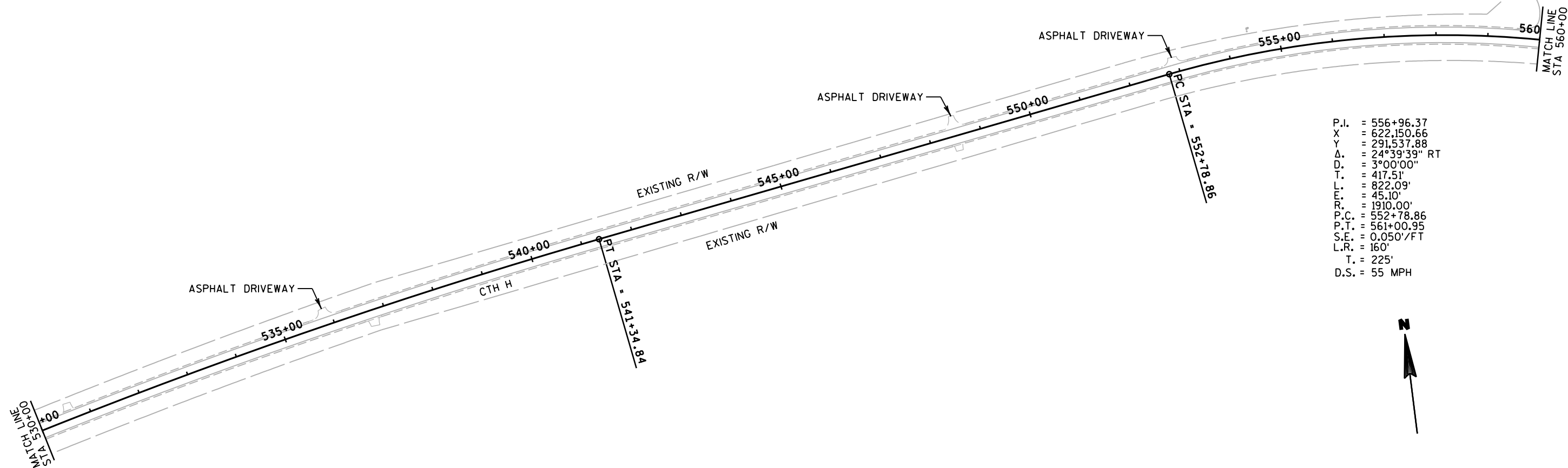
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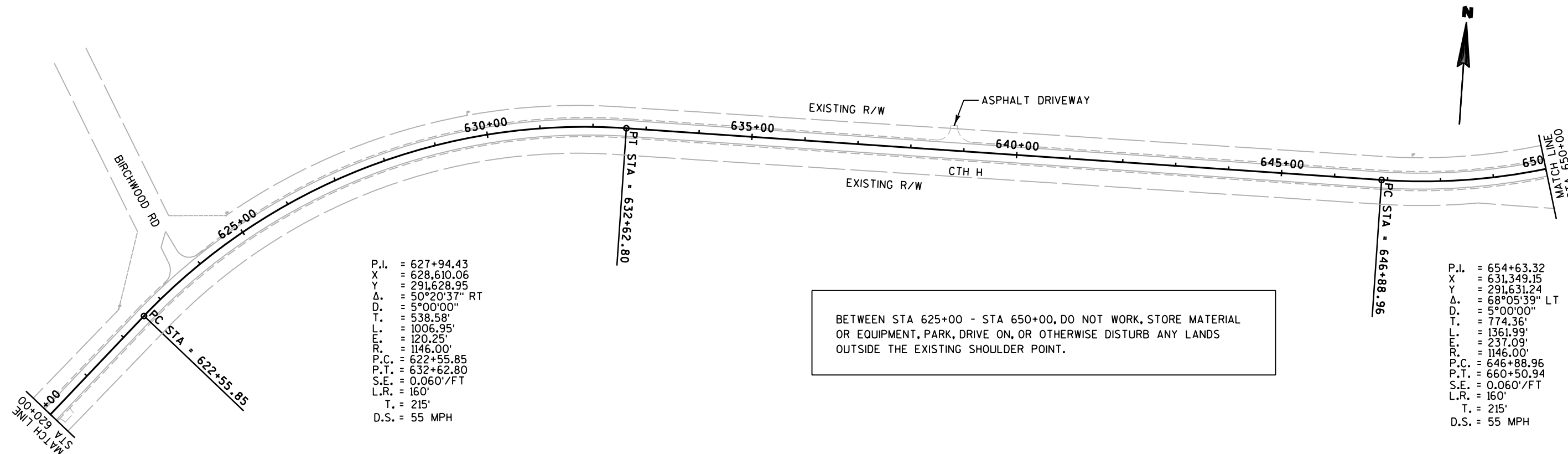
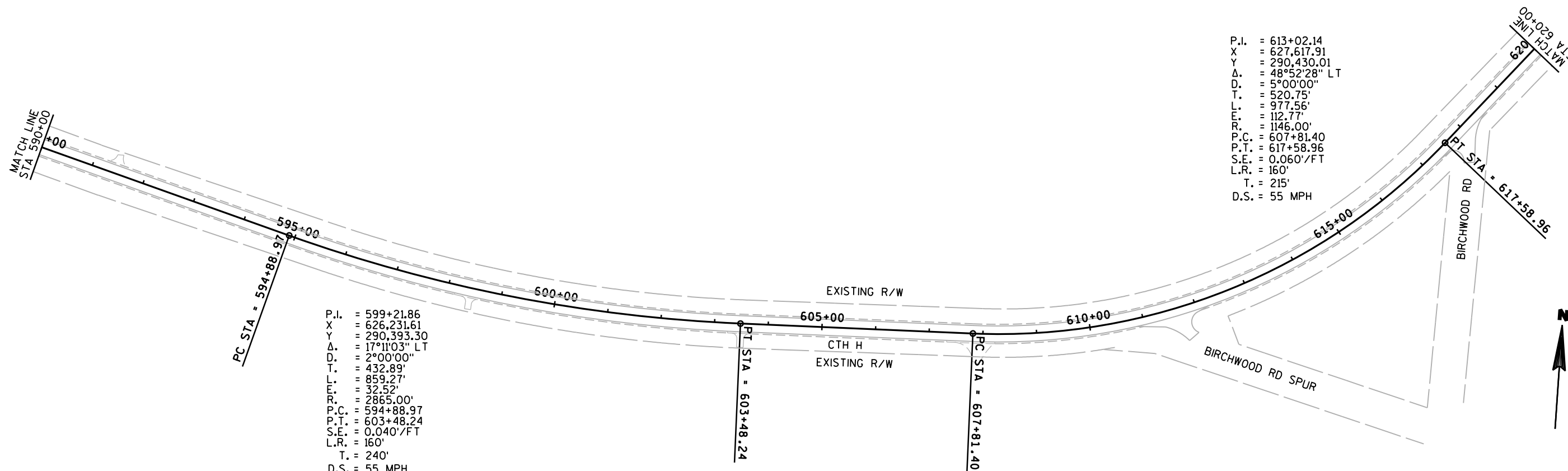
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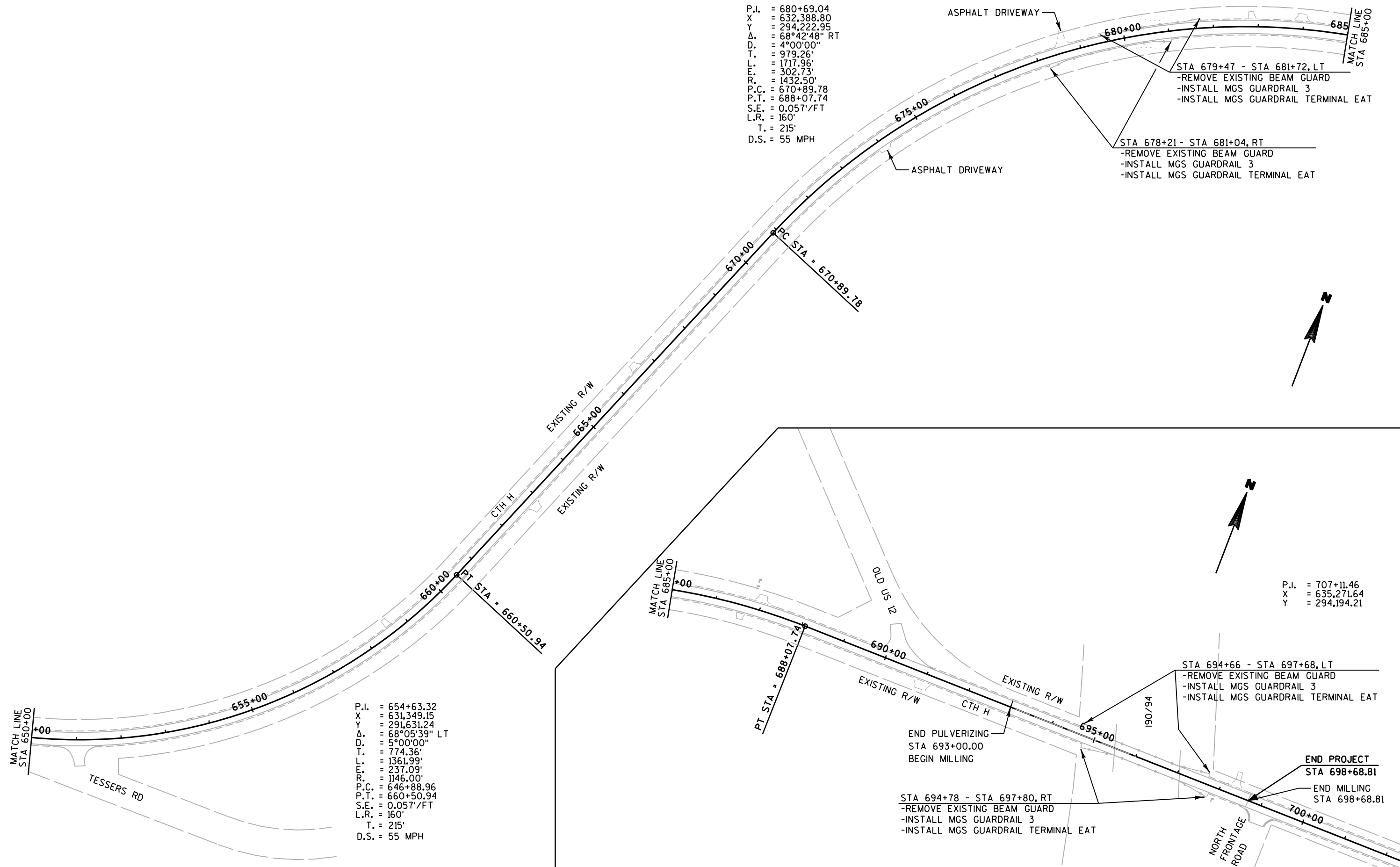
E











DATE 18SEP14		E S T I M A T E O F Q U A N T I T I E S			
LINE					5897-00-70
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0010	204. 0110	REMOVING ASPHALTIC SURFACE	SY	2, 118. 000	2, 118. 000
0020	204. 0120	REMOVING ASPHALTIC SURFACE MILLING	SY	5, 664. 000	5, 664. 000
0030	213. 0100	FINISHING ROADWAY (PROJECT) 01. 5897-00-70	EACH	1. 000	1. 000
0040	305. 0110	BASE AGGREGATE DENSE 3/4-INCH	TON	9, 427. 000	9, 427. 000
0050	305. 0500	SHAPING SHOULDERS	STA	617. 000	617. 000
0060	325. 0100	PULVERIZE AND RELAY	SY	7, 812. 000	7, 812. 000
0070	440. 4410. S	INCENTIVE IRI RIDE	DOL	48, 944. 000	48, 944. 000
0080	455. 0105	ASPHALTIC MATERIAL PG58-28	TON	2, 473. 000	2, 473. 000
0090	455. 0605	TACK COAT	GAL	16, 052. 000	16, 052. 000
0100	460. 1103	HMA PAVEMENT TYPE E-3	TON	44, 760. 000	44, 760. 000
0110	460. 2000	INCENTIVE DENSITY HMA PAVEMENT	DOL	28, 654. 000	28, 654. 000
0120	465. 0120	ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES	TON	306. 000	306. 000
0130	611. 8110	ADJUSTING MANHOLE COVERS	EACH	1. 000	1. 000
0140	611. 8120. S	COVER PLATES TEMPORARY	EACH	1. 000	1. 000
0150	614. 0370	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL	EACH	4. 000	4. 000
0160	614. 0920	SALVAGED RAIL	LF	6, 800. 000	6, 800. 000
0170	614. 2300	MGS GUARDRAIL 3	LF	3, 600. 000	3, 600. 000
0180	614. 2610	MGS GUARDRAIL TERMINAL EAT	EACH	60. 000	60. 000
0190	619. 1000	MOBILIZATION	EACH	1. 000	1. 000
0200	624. 0100	WATER	MGAL	945. 000	945. 000
0210	625. 0100	TOPSOIL	SY	100. 000	100. 000
0220	627. 0200	MULCHING	SY	100. 000	100. 000
0230	628. 1504	SILT FENCE	LF	1, 800. 000	1, 800. 000
0240	628. 1520	SILT FENCE MAINTENANCE	LF	1, 800. 000	1, 800. 000
0250	629. 0210	FERTILIZER TYPE B	CWT	25. 800	25. 800
0260	630. 0120	SEEDING MIXTURE NO. 20	LB	1, 105. 000	1, 105. 000
0270	630. 0200	SEEDING TEMPORARY	LB	1, 105. 000	1, 105. 000
0280	634. 0612	POSTS WOOD 4X6-INCH X 12-FT	EACH	13. 000	13. 000
0290	634. 0614	POSTS WOOD 4X6-INCH X 14-FT	EACH	109. 000	109. 000
0300	634. 0616	POSTS WOOD 4X6-INCH X 16-FT	EACH	10. 000	10. 000
0310	637. 2210	SIGNS TYPE II REFLECTIVE H	SF	417. 150	417. 150
0320	637. 2230	SIGNS TYPE II REFLECTIVE F	SF	529. 000	529. 000
0330	638. 2602	REMOVING SIGNS TYPE II	EACH	157. 000	157. 000
0340	638. 3000	REMOVING SMALL SIGN SUPPORTS	EACH	120. 000	120. 000
0350	642. 5001	FIELD OFFICE TYPE B	EACH	1. 000	1. 000
0360	643. 0100	TRAFFIC CONTROL (PROJECT) 01. 5897-00-70	EACH	1. 000	1. 000
0370	646. 0106	PAVEMENT MARKING EPOXY 4-INCH	LF	126, 199. 000	126, 199. 000
0380	646. 0126	PAVEMENT MARKING EPOXY 8-INCH	LF	150. 000	150. 000
0390	646. 0406	PAVEMENT MARKING SAME DAY EPOXY 4-INCH	LF	83, 887. 000	83, 887. 000
0400	647. 0566	PAVEMENT MARKING STOP LINE EPOXY 18-INCH	LF	38. 000	38. 000
0410	648. 0100	LOCATING NO-PASSING ZONES	MI	12. 240	12. 240
0420	649. 0200	TEMPORARY PAVEMENT MARKING REFLECTIVE PAINT 4-INCH	LF	167, 774. 000	167, 774. 000
0430	650. 8000	CONSTRUCTION STAKING RESURFACING REFERENCE	LF	63, 402. 000	63, 402. 000
0440	690. 0150	SAWING ASPHALT	LF	2, 236. 000	2, 236. 000
0450	ASP. 1TOA	ON-THE-JOB TRAINING APPRENTICE AT \$5. 00/HR	HRS	1, 600. 000	1, 600. 000
0460	ASP. 1TOG	ON-THE-JOB TRAINING GRADUATE AT \$5. 00/HR	HRS	1, 320. 000	1, 320. 000
0470	SPV. 0035	SPECIAL 01. BASE REPAIR FOR CIR PAVEMENT	CY	13, 825. 000	13, 825. 000
0480	SPV. 0060	SPECIAL 01. ADJUSTING WATER VALVES	EACH	3. 000	3. 000

DATE 18SEP14			E S T I M A T E O F Q U A N T I T I E S		
LINE					5897-00-70
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0490	SPV. 0105	SPECIAL 01. PREPARATION OF FOUNDATION FOR CIR PAVEMENT 5897-00-70	LS	1.000	1.000
0500	SPV. 0105	SPECIAL 02. PREPARE FOUNDATION FOR HMA PAVING 5897-00-70	LS	1.000	1.000
0510	SPV. 0180	SPECIAL 01. COLD-IN-PLACE RECYCLING (CIR) PAVEMENT PARTIAL DEPTH	SY	207,371.000	207,371.000
0520	SPV. 0195	SPECIAL 01. ASPHALT STABILIZING AGENT	TON	929.000	929.000

204.0110	REMOVING ASPHALTIC SURFACE		
204.0120	REMOVING ASPHALTIC SURFACE MILLING		
		REMOVING ASPHALTIC SURFACE	REMOVING ASPHALTIC SURFACE MILLING
STATION - STATION	LOCATION	SY	SY
52+48.54 - 58+61.53	C & G SECTION	---	2,991
357+75, RT	CTH P	---	366
470+00, RT	WINNESHIEK DR	---	237
693+00 - 698+68.81	UNDER BRIDGE	---	2,070
60+61.39 - 355+00.00	BEAM GUARD AREAS	567	---
397+50 - 400+60	BEAM GUARD AREAS	69	---
678+00.00 - 682.00.00	BEAM GUARD AREAS	44	---
53+80	RT	30	---
73+15	RT	17	---
73+40	LT	37	---
77+85	RT	12	---
87+00	LT	35	---
110+30	LT	18	---
122+35	RT	49	---
125+35	LT	26	---
131+00	LT	54	---
146+15	LT	38	---
148+30	LT	40	---
158+00	LT	45	---
237+65	LT	32	---
281+15	LT	35	---
290+60	LT	20	---
308+10	LT	25	---
316+95	RT	14	---
320+85	LT	21	---
334+15	LT	25	---
366+95	RT	35	---
370+60	RT	42	---
371+95	LT	41	---
379+70	RT	32	---
397+50	LT	29	---
416+00	LT	17	---
431+35	RT	30	---
431+45	LT	29	---
435+90	LT	29	---
437+10	LT	35	---
438+40	RT	19	---
442+45	RT	20	---
446+50	RT	19	---
451+65	LT	21	---
452+70	LT	15	---
455+40	LT	24	---
459+05	LT	17	---
461+15	RT	22	---
463+00	RT	18	---
473+80	LT	23	---
476+15	LT	27	---
487+70	LT	17	---
495+30	LT	20	---
517+65	RT	93	---
535+85	LT	34	---
548+50	LT	27	---
552+90	LT	29	---
638+80	LT	45	---
674+05	RT	26	---
678+60	LT	30	---
TOTAL		2,118	5,664

305.0500 SHAPING SHOULDERS
SPV.0035.01 BASE REPAIR FOR CIR PAVEMENT
SPV.0105.01 PREPARATION OF FOUNDATION FOR CIR PAVEMENT 5897-00-70
SPV.0105.02 PREPARE FOUNDATION FOR HMA PAVING 5897-00-70

STATION - STATION	LOCATION	SHAPING SHOULDERS STA	BASE** 1 1/4-INCH TON	ASPHALTIC** BASE TON	ASPHALTIC** SURFACE TON	BASE REPAIR FOR CIR CY	CIR PAVEMENT LS	HMA PAVING LS
60+61.53 - 355+00.00	CIR LOCATIONS	295	11,131	2,244	2,244	6,678	---	---
360+42.00 - 397+17.83	CIR LOCATIONS	37	1,362	275	275	817	---	---
401+54.00 - 467+45.00	CIR LOCATIONS	66	2,442	493	493	1,465	---	---
472+50.00 - 691+00.00	CIR LOCATIONS	219	8,109	1,635	1,635	4,865	---	---
PROJECT 5897-00-70	CIR LOCATIONS	---	---	---	---	---	1	1
TOTAL		617	23,044	4,647	4,647	13,825	1	1

** FOR INFORMATIONAL PURPOSES ONLY TO ESTIMATE FOR SPV.0035.01 BASE REPAIR FOR CIR PAVEMENT

325.0100 PULVERIZE AND RELAY
624.0100 WATER

LOCATION	PULVERIZE AND RELAY SY	WATER** * MGAL
58+61.53 - 60+61.53	694	2
355+00.00 - 360+42.00	2,151	7
397+17.83 - 399+17.83	743	2
399+54.00 - 401+54.00	745	2
467+45.00 - 472+50.00	1,683	6
691+00.00 - 693+00.00	667	2
GOLF COURSE RD	324	1
WILLOW CREEK RD	158	1
CHADWICK RD	170	1
NORTH DEWEY AVE	311	1
KEENAN CT	167	1
GLEN VALLEY DR	157	1
DROVER PASS	203	1
SOUTH AVE	272	1
D&W RD	264	1
ACORN LN	163	1
CTH HH	495	2
CHERRY LN	143	0
BLUEBIRD LN	162	1
TOWN HALL RD	535	2
OAK HILL RD	175	1
NORTH AVE	290	1
COONBLUFF RD	672	2
CHRISTMAS MOUNTAIN RD	281	1
LYNDON RD	622	2
BIRCHWOOD RD SPUR	256	1
BIRCHWOOD RD	424	1
TESSERS RD	202	1
OLD US 12	340	1
TOTAL		7,812 26

*RATE OF 10 GALS/TON OF PULVERIZED MATERIAL
** ADDITIONAL QUANTITIES LISTED ELSEWHERE

305.0110 BASE AGGREGATE DENSE 3/4-INCH

STATION - STATION	LOCATION	BASE TON
52+48.54 - 58+61.53	RT	2
58+61.53 - 693+00	RT & LT	7,427
693+00 - 698+68.81	RT & LT	46
DRIVEWAYS	RT & LT	83
UNDISTRIBUTED		1,869
TOTAL		9,427

440.4410.S INCENTIVE IRI RIDE

STATION - STATION	MILES	LANE MILES	INCENTIVE DOL
52+48.54 - 399+17.83	6.57	13.14	\$26,280
399+54 - 698+68.81	5.67	11.33	\$22,664
TOTAL			\$48,944

455.0105 ASPHALTIC MATERIAL PG58-28
455.0605 TACK COAT
460.1103 HMA PAVEMENT TYPE E-3
460.2000 INCENTIVE DENSITY HMA PAVEMENT

STATION - STATION	LOCATION	THICKNESS INCH	MATERIAL TON	TACK GAL	HMA TON	INCENTIVE DOL
52+48.54 - 60+61.39	MAINLINE	4	36	73	647	\$415
52+48.54 - 60+61.39	20TH ST	4	3	5	45	\$29
60+61.39 - 355+00.00	MAINLINE	3.5	1,064	2,466	19,329	\$12,371
60+61.39 - 355+00.00	SIDE ROADS	3.5	39	89	691	\$443
60+61.39 - 355+00.00	BEAM GUARD	3.5	27	61	474	\$304
355+00.00 - 360+42.00	MAINLINE	4	23	46	405	\$260
355+00.00 - 360+42.00	CTH P	4	10	19	165	\$106
360+42.00 - 397+17.83	MAINLINE	3.5	133	307	2,402	\$1,538
397+17.83 - 399+17.83	MAINLINE	4	9	17	150	\$96
397+17.83 - 399+17.83	BEAM GUARD	4	2	4	32	\$21
399+54.00 - 401+54.00	MAINLINE	4	9	17	150	\$96
399+54.00 - 401+54.00	BEAM GUARD	4	2	3	23	\$15
401+54.00 - 691+00.00	MAINLINE	3.5	1,041	2,413	18,912	\$12,104
401+54.00 - 691+00.00	SIDE ROADS	3.5	38	88	686	\$440
401+54.00 - 691+00.00	BEAM GUARD	3.5	3	5	39	\$25
691+00.00 - 698+68.81	MAINLINE	4	32	65	575	\$368
691+00.00 - 698+68.81	BEAM GUARD	4	2	4	35	\$23
TOTAL			2,473	5,682	44,760	\$28,654

** ADDITIONAL QUANTITIES LISTED ELSEWHERE

455.0605 TACK COAT
SPV.0180.01 COLD-IN-PLACE RECYCLING (CIR) PAVEMENT PARTIAL DEPTH
SPV.0195.01 ASPHALT STABILIZING AGENT

**			
STATION - STATION	TACK GAL	CIR SY	AGENT TON
60+61.53 - 355+00.00	5,009	100,173	449
360+42.00 - 397+17.83	613	12,253	55
401+54.00 - 467+45.00	1,099	21,970	98
472+50.00 - 691+00.00	3,649	72,975	327
TOTAL	10,370	207,371	929

** ADDITIONAL QUANTITIES LISTED ELSEWHERE
NOTE: TACK LAYER IS ON TOP OF CIR LAYER USING A RATE OF 0.05 GAL/SY

465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES

STATION	LOCATION	TON
53+80	RT	6
73+15	RT	4
73+40	LT	8
77+85	RT	3
87+00	LT	7
110+30	LT	4
122+35	RT	10
125+35	LT	6
131+00	LT	11
146+15	LT	8
148+30	LT	8
158+00	LT	9
237+65	LT	7
281+15	LT	7
290+60	LT	4
308+10	LT	6
316+95	RT	3
320+85	LT	5
334+15	LT	6
366+95	RT	7
370+60	RT	9
371+95	LT	9
379+70	RT	7
397+50	LT	6
416+00	LT	4
431+35	RT	6
431+45	LT	6
435+90	LT	6
437+10	LT	7
438+40	RT	4
442+45	RT	4
446+50	RT	4
451+65	LT	5
452+70	LT	4
455+40	LT	5
459+05	LT	4
461+15	RT	5
463+00	RT	4
473+80	LT	5
476+15	LT	6
487+70	LT	4
495+30	LT	4
517+65	RT	19
535+85	LT	7
548+50	LT	6
552+90	LT	6
638+80	LT	9
674+05	RT	6
678+60	LT	6
TOTAL		306

611.8110 ADJUSTING MANHOLE COVERS
611.8120.S COVER PLATES TEMPORARY
SPV.0060.01 ADJUSTING WATER VALVES

CATEGORY	STATION	LOCATION	ADJUST COVER EACH	COVER PLATES EACH	WATER VALVES EACH
020	690+00	RT	1	1	---
	691+00	OLD HWY 12	---	---	1
	695+00	WEST OF IH 90	---	---	1
	698+00	EAST OF IH 90	---	---	1
TOTAL CATEGORY 020			1	1	3

614.0920 SALVAGED RAIL

STATION - STATION	LOCATION	LF
62+25 - 63+60	RT	137.5
62+50 - 63+75	LT	125
74+13 - 77+41	RT	325
83+05 - 89+83	RT	675
83+57 - 86+22	LT	262.5
94+88 - 98+23	RT	337.5
99+80 - 101+55	RT	175
115+50 - 116+90	LT & RT	275
142+75 - 144+05	LT & RT	250
169+00 - 170+30	LT & RT	250
177+36 - 181+68	RT	437.5
224+72 - 233+04	LT	837.5
234+18 - 235+77	LT	162.5
238+56 - 239+71	LT	112.5
238+50 - 239+72	RT	125
266+27 - 267+55	LT & RT	250
273+45 - 274+75	RT	125
273+47 - 274+75	LT	125
304+02 - 305+30	LT	125
304+04 - 305+32	RT	125
321+17 - 322+46	LT	125
321+14 - 322+42	RT	125
397+50 - 400+00	RT	50
398+10 - 398+60	LT	50
400+75 - 401+25	LT	50
400+10 - 400+60	RT	50
678+21 - 681+04	RT	287.5
679+47 - 681+72	LT	225
694+66 - 697+68	LT	300
694+78 - 697+80	RT	300
TOTAL		6,800

614.0370 STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
614.2300 MGS GUARDRAIL 3
614.2610 MGS GUARDRAIL TERMINAL EAT

STATION - STATION	LOCATION	STEEL PLATE	MGS	MGS
		EAT EACH	GUARDRAIL LF	TERMINAL EACH
62+21 - 63+64	RT	---	37.5	2
62+47 - 63+78	LT	---	25	2
74+11 - 77+43	RT	---	225	2
83+03 - 89+85	RT	---	575	2
83+55 - 86+24	LT	---	162.5	2
94+84 - 98+27	RT	---	237.5	2
99+77 - 101+58	RT	---	75	2
115+48 - 116+92	LT & RT	---	75	4
142+74 - 144+06	LT & RT	---	50	4
168+99 - 170+31	LT & RT	---	50	4
177+30 - 181+74	RT	---	337.5	2
224+66 - 233+10	LT	---	737.5	2
234+13 - 235+82	LT	---	62.5	2
238+54 - 239+73	LT	---	12.5	2
238+45 - 239+77	RT	---	25	2
266+25 - 267+57	LT & RT	---	50	4
273+44 - 274+76	RT	---	25	2
273+45 - 274+77	LT	---	25	2
304+00 - 305+32	LT	---	25	2
304+02 - 305+34	RT	---	25	2
321+16 - 322+47	LT	---	25	2
321+12 - 322+44	RT	---	25	2
397+37 - 397+87	RT	1	---	---
397+97 - 398+47	LT	1	---	---
400+88 - 401+38	LT	1	---	---
400+23 - 400+73	RT	1	---	---
678+16 - 681+09	RT	---	187.5	2
679+44 - 681+75	LT	---	125	2
694+64 - 697+70	LT	---	200	2
694+76 - 697+82	RT	---	200	2
TOTAL		4	3600	60

624.0100 WATER
625.0100 TOPSOIL
627.0200 MULCHING
629.0210 FERTILIZER TYPE B
630.0120 SEEDING MIXTURE NO. 20
630.0200 SEEDING TEMPORARY

STATION - STATION	TOPSOIL SY	MULCH SY	FERTILIZER CWT	SEED NO 20 LB	TEMPORARY SEED LB	** WATER MGAL
UNDISTRIBUTED	100	100	25.8	1,105	1,105	919
TOTAL	100	100	25.8	1,105	1,105	919

** ADDITIONAL QUANTITIES LISTED ELSEWHERE

628.1504 SILT FENCE
628.1520 SILT FENCE MAINTENANCE

STATION - STATION	LOCATION	SILT FENCE LF	MAINTENANCE LF
		LF	LF
397+00 - 402+00	LT & RT	1000	1000
677+00 - 682+00	LT & RT	800	800
TOTAL		1800	1800

634.0612 POSTS WOOD 4x6-INCH X 12-FT
634.0614 POSTS WOOD 4x6-INCH X 14-FT
634.0616 POSTS WOOD 4x6-INCH X 16-FT
637.2210 SIGNS TYPE II REFLECTIVE H
637.2230 SIGNS TYPE II REFLECTIVE F

STATION	LOCATION	CODE	SIZE	TYPE H SF	TYPE F SF	12-FT EACH	14-FT EACH	16-FT EACH	COMMENTS
52+55	RT	W6-3	36" X 36"	---	9.0	1	---	---	---
53+30	LT	R2-1	24" X 30"	5.00	---	1	---	---	---
54+40	LT	R3-7R	30" X 30"	6.25	---	1	---	---	---
54+60	RT	W1-2R	30" X 30"	---	6.25	1	---	---	---
		W13-1	18" x 18"	---	2.25	---	---	---	---
56+55	RT	R1-1	30" X 30"	5.18	---	1	---	---	---
57+35	RT	R2-1	24" X 30"	5.00	---	1	---	---	---
57+40	LT	D9-2	24" X 24"	4.00	---	1	---	---	---
59+80	LT	I2-3	66" X 24"	11.00	---	2	---	---	POPULATION
60+65	LT	W3-5	36" X 36"	---	9.00	---	1	---	---
78+35	RT	R2-1	24" X 30"	5.00	---	---	1	---	---
83+65	RT	W1-2L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
83+65	LT	W1-2L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
91+30	RT	W2-2R	30" X 30"	---	6.25	---	1	---	---
93+10	LT	R2-1	24" X 30"	5.00	---	---	1	---	---
93+60	RT	W1-6L	48" X 24"	---	8.00	---	1	---	---
99+55	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
104+05	LT	R1-1	30" X 30"	5.18	---	1	---	---	---
109+20	LT	R2-1	24" X 30"	5.00	---	---	1	---	---
111+90	LT	W1-2R	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	1	---	---
112+00	RT	R2-1	24" X 30"	5.00	---	---	1	---	---
		W14-3	36" X 48"	---	6.00	---	---	---	---
113+65	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
118+30	LT	W14-3	36" X 48"	---	6.00	---	1	---	---
		W3-5	36" X 36"	---	9.00	---	1	---	---
122+05	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
137+30	RT	W14-3	36" X 48"	---	6.00	---	1	---	---

634.0612 POSTS WOOD 4x6-INCH X 12-FT
634.0614 POSTS WOOD 4x6-INCH X 14-FT
634.0616 POSTS WOOD 4x6-INCH X 16-FT
637.2210 SIGNS TYPE II REFLECTIVE H
637.2230 SIGNS TYPE II REFLECTIVE F

STATION	LOCATION	CODE	SIZE	TYPE H SF	TYPE F SF	12-FT EACH	14-FT EACH	16-FT EACH	COMMENTS
138+25	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
148+70	RT	W1-4R	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
156+30	LT	W14-3	36" X 48"	---	6.00	---	1	---	---
164+59.64	RT	W1-2R	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" x 18"	---	2.25	---	---	---	---
171+30	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
173+89.67	LT	W1-2L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" x 18"	---	2.25	---	---	---	---
177+39.95	RT	W1-2L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" x 18"	---	2.25	---	---	---	---
182+96.26	RT	W1-2R	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" x 18"	---	2.25	---	---	---	---
190+40	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
191+20	LT	W1-4R	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
194+10	RT	W1-2L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
		W14-3	36" X 48"	---	6.00	---	---	---	---
194+70	LT	W14-3	36" X 48"	---	6.00	---	1	---	---
		R2-1	24" X 30"	5.00	---	---	---	---	---
201+90	RT	W2-2R	30" X 30"	---	6.25	---	1	---	---
210+90	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
221+75	RT	W1-2R	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
222+50	LT	W2-2L	30" X 30"	---	6.25	---	1	---	---
225+75	LT	W1-2R	30" X 30"	---	6.25	---	1	---	---
226+45	RT	W2-2L	30" X 30"	---	6.25	---	1	---	---
233+45	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
242+70	RT	W14-3	36" X 48"	---	6.00	---	1	---	---
246+05	LT	W14-3	36" X 48"	---	6.00	---	1	---	---
		W1-2L	30" X 30"	---	6.25	---	---	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
256+20	RT	W1-2R	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
256+95	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
264+65	RT	J1-1	24" X 39"	6.50	---	---	1	---	---
		M2-1	21" X 15"	---	---	---	---	---	JCT
		M1-5A	24" X 24"	---	---	---	---	---	CTH HH
265+60	LT	W14-3	36" X 48"	---	6.00	---	1	---	---
266+45	RT	D1-1	60" X 15"	6.25	---	---	2	---	LEFT ARROW, LYNDON
268+85	RT	W14-3	36" X 48"	---	6.00	---	1	---	---
271+10	LT	R1-1	30" X 30"	5.18	---	1	---	---	---
		J1-2	48" X 39"	13.00	---	---	2	---	---
		M1-5A	24" X 24"	---	---	---	---	---	CTH H
		M6-4	21" X 21"	---	---	---	---	---	DOUBLE ARROW
		M1-5A	24" X 24"	---	---	---	---	---	CTH HH
		M4-6	24" X 12"	---	---	---	---	---	END
271+25	RT	J13-1	24" X 45"	7.50	---	---	1	---	---
		M1-5A	24" X 24"	---	---	---	---	---	CTH HH
		M6-1	21" X 21"	---	---	---	---	---	ARROW LEFT
271+25	RT	D1-2	72" X 30"	15.00	---	---	2	---	WIS DELLS LT, REEDSBURG RT
271+30	RT	W1-7	48" X 24"	---	8.00	---	---	1	---
272+30	LT	J13-1	24" X 45"	7.50	---	---	1	---	---
		M1-5A	24" X 24"	---	---	---	---	---	CTH HH
		M6-1	21" X 21"	---	---	---	---	---	ARROW RIGHT
286+25	LT	J1-1	24" X 39"	6.50	---	---	1	---	---
		M2-1	21" X 15"	---	---	---	---	---	JCT
		M1-5A	24" X 24"	---	---	---	---	---	CTH HH
291+75	RT	W14-3	36" X 48"	---	6.00	---	---	1	---

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634.0612 POSTS WOOD 4x6-INCH X 12-FT
634.0614 POSTS WOOD 4x6-INCH X 14-FT
634.0616 POSTS WOOD 4x6-INCH X 16-FT
637.2210 SIGNS TYPE II REFLECTIVE H
637.2230 SIGNS TYPE II REFLECTIVE F

STATION	LOCATION	CODE	SIZE	TYPE H SF	TYPE F SF	12-FT EACH	14-FT EACH	16-FT EACH	COMMENTS
296+55	LT	W1-2L	30" X 30"	---	6.25	---	1	---	---
297+50	LT	R1-1	30" X 30"	5.18	---	1	---	---	---
311+45	RT	R1-1	30" X 30"	5.18	---	1	---	---	---
325+75	RT	W2-1	30" X 30"	---	6.25	---	---	---	---
332+45	LT	W14-3	36" X 48"	---	6.00	---	---	1	---
334+00	RT	W1-4L	30" X 30"	---	6.25	---	1	---	---
338+20	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
338+80	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
348+10	RT	J1-1	24" X 39"	6.50	---	---	1	---	---
		M2-1	21" X 15"	---	---	---	---	---	JCT
		M1-5A	24" X 24"	---	---	---	---	---	CTH P
348+50	LT	W2-1	30" X 30"	---	6.25	---	1	---	---
350+35	RT	W2-2R	30" X 30"	---	6.25	---	---	1	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
351+25	RT	D1-2	78" X 30"	16.25	---	---	2	---	WIS DELLS LT, LAKE DELTON RT
353+40	RT	S3-1	36" X 36"	9.00	---	---	1	---	---
356+65	RT	J2-2	48" X 57"	19.00	---	---	---	2	---
		M1-5A	24" X 24"	---	---	---	---	---	CTH H
		M6-1	21" X 21"	---	---	---	---	---	ARROW STRAIGHT
		M1-5A	24" X 24"	---	---	---	---	---	CTH P
		M6-1	21" X 21"	---	---	---	---	---	ARROW RIGHT
357+50	LT	W1-7	48" X 24"	---	8.00	---	1	---	---
		D1-2	72" X 30"	15.00	---	---	2	---	REEDSBURG LT, WIS DELLS RT
358+20	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
358+20	RT	J2-2	48" X 57"	19.00	---	---	1	---	---
		M1-5A	24" X 24"	---	---	---	---	---	CTH H
		M6-4	21" X 21"	---	---	---	---	---	DOUBLE ARROW
		M1-5A	24" X 24"	---	---	---	---	---	CTH P
		M4-6	24" X 12"	---	---	---	---	---	END
358+50	LT	J2-2	48" X 57"	19.00	---	---	1	---	---
		M1-5A	24" X 24"	---	---	---	---	---	CTH H
		M6-1	21" X 21"	---	---	---	---	---	ARROW STRAIGHT
		M1-5A	24" X 24"	---	---	---	---	---	CTH P
		M6-1	21" X 21"	---	---	---	---	---	ARROW LEFT
362+80	RT	M1-5A	24" X 24"	4.00	---	---	1	---	CTH H
364+20	LT	W2-2L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
365+95	LT	S3-1	36" X 36"	9.00	---	---	1	---	---
367+00	LT	J1-1	24" X 39"	6.50	---	---	1	---	---
		M2-1	21" X 15"	---	---	---	---	---	JCT
		M1-5A	24" X 24"	---	---	---	---	---	CTH P
390+20	RT	W1-2L	30" X 30"	---	6.25	---	1	---	---
395+80	LT	W1-4L	30" X 30"	---	6.25	---	1	---	---
407+00	LT	W14-3	36" X 48"	---	6.00	---	---	1	---
408+70	RT	W14-3	36" X 48"	---	6.00	---	---	1	---
410+30	LT	W1-2R	30" X 30"	---	6.25	---	1	---	---
436+88	RT	R2-1	24" X 30"	5.00	---	---	1	---	---
		W14-3	36" X 48"	---	6.00	---	---	---	---
458+10	RT	W2-2L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
462+25	RT	W2-2R	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
464+35	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
470+20	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
470+90	LT	W2-2R	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
472+40	RT	W2-2L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
476+50	LT	W2-2L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---

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634.0612 POSTS WOOD 4x6-INCH X 12-FT
634.0614 POSTS WOOD 4x6-INCH X 14-FT
634.0616 POSTS WOOD 4x6-INCH X 16-FT
637.2210 SIGNS TYPE II REFLECTIVE H
637.2230 SIGNS TYPE II REFLECTIVE F

STATION	LOCATION	CODE	SIZE	TYPE H SF	TYPE F SF	12-FT EACH	14-FT EACH	16-FT EACH	COMMENTS
478+60	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
485+55	LT	W2-2R	30" X 30"	---	6.25	---	1	---	---
502+50	RT	W13-1	18" X 18"	---	2.25	---	---	---	---
		W2-1	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
504+00	LT	W14-3	36" X 48"	---	6.00	---	1	---	---
509+10	LT	R2-1	24" X 30"	5.00	---	---	---	---	---
		R1-1	30" X 30"	5.18	---	---	1	---	---
509+20	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
514+25	RT	W2-2R	30" X 30"	---	6.25	---	1	---	---
515+70	LT	W13-1	18" X 18"	---	2.25	---	---	---	---
		W2-1	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
521+45	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
545+50	LT	W2-2L	30" X 30"	---	6.25	---	1	---	---
553+40	RT	W13-1	18" X 18"	---	2.25	---	---	---	---
		R2-1	24" X 30"	5.00	---	---	---	1	---
		W14-3	36" X 48"	---	6.00	---	---	---	---
559+90	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
563+60	LT	W1-7	48" X 24"	---	8.00	---	1	---	---
564+00	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
568+10	RT	W14-3	36" X 48"	---	6.00	---	1	---	---
569+85	LT	W1-2L	30" X 30"	---	6.25	---	1	---	---
571+30	LT	W2-1	30" X 30"	---	6.25	---	1	---	---
584+60	RT	W13-1	18" X 18"	---	2.25	---	---	---	---
		W1-4L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
588+70	LT	W14-3	36" X 48"	---	6.00	---	---	1	---
611+90	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
618+05	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
623+55	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
638+65	RT	W1-4L	30" X 30"	---	6.25	---	1	---	---
639+75	LT	W13-1	18" X 18"	---	2.25	---	---	---	---
		W1-4L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
651+25	RT	R1-1	30" X 30"	5.18	---	---	1	---	---
689+95	LT	R1-1	30" X 30"	5.18	---	---	1	---	---
692+10	LT	R2-1	24" X 30"	5.00	---	---	1	---	---
694+00	RT	W3-5	36" X 36"	---	9.00	---	1	---	---
694+25	LT	W1-4L	30" X 30"	---	6.25	---	1	---	---
		W13-1	18" X 18"	---	2.25	---	---	---	---
TOTAL				417.15	529.00	13	109	10	

638.2602 REMOVING SIGNS TYPE II
638.3000 REMOVING SMALL SIGN SUPPORTS

STATION	LOCATION	SIGNS EACH	SUPPORTS EACH	DESCRIPTION
52+55	RT	1	1	TWO WAY TRAFFIC
53+30	LT	1	1	SPEED LIMIT 35 MPH
54+40	LT	1	1	RIGHT LANE MUST TURN RIGHT
54+60	RT	2	1	CURVE, 45 MPH
56+55	RT	1	1	STOP
57+35	RT	1	1	SPEED LIMIT 45 MPH
57+40	LT	2	1	HOSPITAL, ARROW
59+80	LT	1	2	REEDSBURG POPULATION
60+65	LT	1	1	REDUCE SPEED, 35 MPH
78+35	RT	1	1	SPEED LIMIT 45 MPH
83+65	RT	2	1	CURVE, 45 MPH
83+65	LT	2	1	CURVE, 45 MPH
91+30	RT	1	1	INTERSECTION
93+10	LT	1	1	SPEED LIMIT 45 MPH
93+60	RT	1	1	ARROW
99+55	RT	1	1	STOP
104+05	LT	1	1	STOP
109+20	LT	1	1	SPEED LIMIT 45 MPH
111+90	LT	2	1	CURVE, 45 MPH
112+00	RT	2	1	SPEED LIMIT 55 MPH, NO PASSING
113+65	LT	1	1	STOP
118+30	LT	2	1	NO PASSING, REDUCE SPEED, 45 MPH
122+05	LT	1	1	STOP
137+30	RT	1	1	NO PASSING
138+25	LT	1	1	STOP
148+70	RT	2	1	CURVES, 45 MPH
156+30	LT	1	1	NO PASSING
171+30	LT	1	1	STOP
190+40	LT	1	1	STOP
191+20	LT	2	1	CURVES, 45 MPH
194+10	RT	3	1	CURVE, 45 MPH, NO PASSING
194+70	LT	2	1	NO PASSING, SPEED LIMIT 55 MPH
201+90	RT	1	1	INTERSECTION
210+90	RT	1	1	STOP
221+75	RT	2	1	CURVE, 45 MPH
222+50	LT	1	1	INTERSECTION
225+75	LT	2	1	CURVE, 45 MPH
226+45	RT	1	1	INTERSECTION
233+45	LT	1	1	STOP
242+70	RT	1	1	NO PASSING
246+05	LT	3	1	NO PASSING, CURVE, 45 MPH
256+20	RT	2	1	CURVE, 45 MPH
256+95	RT	1	1	STOP
264+65	RT	1	1	JCT CTH HH
265+60	LT	1	1	NO PASSING
266+45	RT	1	1	LYNDON
268+85	RT	1	1	NO PASSING
271+10	LT	2	2	STOP, CTH H ARROW, CTH HH END
271+25	RT	1	1	CTH HH ARROW
271+30	RT	1	1	ARROW
272+30	LT	1	1	CTH HH ARROW
286+25	LT	1	1	JCT CTH HH
291+75	RT	1	1	NO PASSING
296+55	LT	2	1	CURVE, 45 MPH
297+50	LT	1	1	STOP
311+45	RT	1	1	STOP
325+75	RT	1	1	INTERSECTION
332+45	LT	1	1	NO PASSING
334+00	RT	1	1	CURVES
338+20	LT	1	1	STOP
338+80	RT	1	1	STOP
348+10	RT	1	1	JCT CTH P
348+50	LT	1	1	INTERSECTION
350+35	RT	1	1	INTERSECTION
351+25	RT	1	1	LAKE DELTON, WIS. DELLS

638.2602 REMOVING SIGNS TYPE II
638.3000 REMOVING SMALL SIGN SUPPORTS

STATION	LOCATION	SIGNS EACH	SUPPORTS EACH	DESCRIPTION
353+40	RT	1	1	SCHOOL BUS STOP AHEAD
356+65	RT	1	1	CTH H ARROW, CTH P ARROW
357+50	LT	2	1	ARROW, REEDSBURG, WIS. DELLS
358+20	RT	1	1	STOP
358+20	RT	1	1	JCT CTH H, CTH P END
358+50	LT	1	1	CTH H ARROW, CTH P ARROW
362+80	RT	1	1	CTH H
364+20	LT	2	1	INTERSECTION, 45 MPH
365+95	LT	1	1	SCHOOL BUS STOP AHEAD
367+00	LT	1	1	JCT CTH P
390+20	RT	1	1	CURVE
395+80	LT	1	1	CURVES
407+00	LT	1	1	NO PASSING
408+70	RT	1	1	NO PASSING
410+30	LT	2	1	CURVE, 45 MPH
436+88	RT	2	1	SPEED LIMIT 55 MPH, NO PASSING
458+10	RT	1	1	INTERSECTION
462+25	RT	1	1	INTERSECTION, 45 MPH
464+35	LT	1	1	STOP
470+20	RT	1	1	STOP
470+90	LT	2	1	INTERSECTION, 45 MPH
472+40	RT	2	1	INTERSECTION, 45 MPH
476+50	LT	2	1	INTERSECTION, 45 MPH
478+60	LT	1	1	STOP
485+55	LT	2	1	INTERSECTION, 45 MPH
502+50	RT	2	1	INTERSECTION, 45 MPH
504+00	LT	2	1	NO PASSING, SPEED LIMIT 55 MPH
509+10	LT	1	1	STOP
509+20	RT	1	1	STOP
514+25	RT	2	1	INTERSECTION, 45 MPH
515+70	LT	2	1	INTERSECTION, 45 MPH
521+45	RT	1	1	STOP
528+00	LT	2	1	CAUTION BLIND DRIVEWAY, 45 MPH
545+50	LT	2	1	INTERSECTION, 45 MPH
553+40	RT	2	1	SPEED LIMIT 55 MPH, NO PASSING
559+90	LT	1	1	STOP
563+60	LT	1	1	ARROW
564+00	RT	1	1	STOP
568+10	RT	1	1	NO PASSING
569+85	LT	1	1	CURVE
571+30	LT	2	1	INTERSECTION, 45 MPH
584+60	RT	2	1	CURVES, 45 MPH
588+70	LT	1	1	NO PASSING
611+90	RT	1	1	STOP
618+05	RT	1	1	STOP
623+55	LT	1	1	STOP
638+65	RT	2	1	CURVES, 45 MPH
639+75	LT	2	1	CURVES, 45 MPH
651+25	RT	1	1	STOP
689+95	LT	1	1	STOP
692+10	LT	1	1	SPEED LIMIT 55 MPH
694+00	RT	1	1	REDUCE SPEED, 35 MPH
694+25	LT	2	1	CURVES, 45 MPH
TOTAL		157	120	

646.0106 PAVEMENT MARKING EPOXY 4-INCH
646.0126 PAVEMENT MARKING EPOXY 8-INCH
646.0406 PAVEMENT MARKING SAME DAY EPOXY 4-INCH
647.0566 PAVEMENT MARKING STOP LINE EPOXY 18-INCH
649.0200 TEMPORARY PAVEMENT MARKING REFLECTIVE PAINT

STATION - STATION	LOCATION	EPOXY 4-INCH WHITE		EPOXY 8-INCH WHITE	SAME DAY EPOXY 4-INCH YELLOW		EPOXY STOP LINE 18-INCH WHITE	*TEMPORARY MARKING 4-INCH YELLOW		DESCRIPTION
		SOLID LF	SKIPS LF		SOLID LF	SKIPS LF		SOLID LF	SKIPS LF	
53+25 - 103+55	LT	5030	---	---	---	---	---	---	---	EDGE LINE
52+48 - 55+45	RT	297	---	---	---	---	---	---	---	EDGE LINE
55+45 - 55+95	RT	---	12	---	---	---	---	---	---	EDGE LINE 3' SKIPS
56+65 - 98+15	RT	4150	---	---	---	---	---	---	---	EDGE LINE
98+15 - 99+70	RT	---	39	---	---	---	---	---	---	EDGE LINE 3' SKIPS
52+48 - 100+00	CENTERLINE	---	---	---	9504	---	---	19008	---	NO PASSING
100+00 - 112+00	CENTERLINE	---	---	---	1200	300	---	2400	600	EASTBOUND PASSING
103+55 - 104+55	LT	---	24	---	---	---	---	---	---	EDGE LINE 3' SKIPS
104+55 - 113+45	LT	890	---	---	---	---	---	---	---	EDGE LINE
113+45 - 114+20	LT	---	18	---	---	---	---	---	---	EDGE LINE 3' SKIPS
99+70 - 210+05	RT	11,035	---	---	---	---	---	---	---	EDGE LINE
210+05 - 211+30	RT	---	33	---	---	---	---	---	---	EDGE LINE 3' SKIPS
112+00 - 118+00	CENTERLINE	---	---	---	---	150	---	---	300	PASSING BOTH WAY
114+20 - 121+65	LT	745	---	---	---	---	---	---	---	EDGE LINE
121+65 - 123+15	LT	---	36	---	---	---	---	---	---	EDGE LINE 3' SKIPS
118+00 - 126+00	CENTERLINE	---	---	---	800	200	---	1600	400	WESTBOUND PASSING
126+00 - 129+50	CENTERLINE	---	---	---	---	88	---	---	176	PASSING BOTH WAY
129+50 - 137+20	CENTERLINE	---	---	---	770	187	---	1540	374	EASTBOUND PASSING
123+15 - 138+05	LT	1490	---	---	---	---	---	---	---	EDGE LINE
138+50 - 138+75	LT	---	18	---	---	---	---	---	---	EDGE LINE 3' SKIPS
137+20 - 156+30	CENTERLINE	---	---	---	---	475	---	---	950	PASSING BOTH WAY
156+30 - 163+20	CENTERLINE	---	---	---	690	175	---	1380	350	WESTBOUND PASSING
138+75 - 170+95	LT	3220	---	---	---	---	---	---	---	EDGE LINE
170+95 - 171+85	LT	---	30	---	---	---	---	---	---	EDGE LINE 3' SKIPS
163+20 - 182+30	CENTERLINE	---	---	---	3820	---	---	7640	---	NO PASSING
171+85 - 190+00	LT	1815	---	---	---	---	---	---	---	EDGE LINE
190+00 - 191+10	LT	---	27	---	---	---	---	---	---	EDGE LINE 3' SKIPS
182+30 - 194+10	CENTERLINE	---	---	---	1180	300	---	2360	600	EASTBOUND PASSING
194+10 - 194+65	CENTERLINE	---	---	---	---	13	---	---	26	PASSING BOTH WAY
191+10 - 233+00	LT	4190	---	---	---	---	---	---	---	EDGE LINE
233+00 - 234+50	LT	---	36	---	---	---	---	---	---	EDGE LINE 3' SKIPS
194+65 - 206+80	CENTERLINE	---	---	---	1215	300	---	2430	600	WESTBOUND PASSING
206+80 - 233+75	CENTERLINE	---	---	---	5390	---	---	10780	---	NO PASSING
211+30 - 256+25	RT	4495	---	---	---	---	---	---	---	EDGE LINE
256+25 - 257+30	RT	---	27	---	---	---	---	---	---	EDGE LINE 3' SKIPS
234+50 - 270+35	LT	3585	---	---	---	---	---	---	---	EDGE LINE
270+35 - 272+45	LT	---	51	---	---	---	---	---	---	EDGE LINE 3' SKIPS
233+75 - 242+70	CENTERLINE	---	---	---	895	225	---	1790	450	EASTBOUND PASSING
242+70 - 246+05	CENTERLINE	---	---	---	---	88	---	---	176	PASSING BOTH WAY
246+05 - 256+20	CENTERLINE	---	---	---	1015	250	---	2030	500	WESTBOUND PASSING
256+20 - 265+45	CENTERLINE	---	---	---	825	200	---	1650	400	EASTBOUND PASSING
257+30 - 310+50	RT	5320	---	---	---	---	---	---	---	EDGE LINE
310+50 - 311+95	RT	---	36	---	---	---	---	---	---	EDGE LINE 3' SKIPS
265+45 - 269+10	CENTERLINE	---	---	---	730	---	---	1460	---	NO PASSING
269+10 - 271+05	CENTERLINE	---	---	---	195	50	---	390	100	WESTBOUND PASSING
269+55 - 271+45	RT	---	50	---	---	---	---	---	---	EDGE LINE 12.5' SKIPS
271+05 - 275+30	CENTERLINE	---	---	---	---	100	---	---	200	PASSING BOTH WAY
275+30 - 291+85	CENTERLINE	---	---	---	1655	413	---	3310	826	EASTBOUND PASSING
272+45 - 297+10	LT	2465	---	---	---	---	---	---	---	EDGE LINE
297+10 - 298+05	LT	---	24	---	---	---	---	---	---	EDGE LINE 3' SKIPS
298+05 - 337+80	LT	3975	---	---	---	---	---	---	---	EDGE LINE
337+80 - 339+05	LT	---	30	---	---	---	---	---	---	EDGE LINE 3' SKIPS
291+85 - 332+25	CENTERLINE	---	---	---	---	1000	---	---	2000	PASSING BOTH WAY
332+25 - 345+35	CENTERLINE	---	---	---	1310	325	---	2620	650	WESTBOUND PASSING
311+95 - 337+70	RT	2575	---	---	---	---	---	---	---	EDGE LINE
337+70 - 339+25	RT	---	39	---	---	---	---	---	---	EDGE LINE 3' SKIPS
339+25 - 357+05	RT	1780	---	---	---	---	---	---	---	EDGE LINE
339+05 - 464+00	LT	12495	---	---	---	---	---	---	---	EDGE LINE
464+00 - 465+25	LT	---	30	---	---	---	---	---	---	EDGE LINE 3' SKIPS
345+35 - 398+05	CENTERLINE	---	---	---	10540	---	---	21080	---	NO PASSING

646.0106 PAVEMENT MARKING EPOXY 4-INCH
646.0126 PAVEMENT MARKING EPOXY 8-INCH
646.0406 PAVEMENT MARKING SAME DAY EPOXY 4-INCH
647.0566 PAVEMENT MARKING STOP LINE EPOXY 18-INCH
649.0200 TEMPORARY PAVEMENT MARKING REFLECTIVE PAINT

STATION - STATION	LOCATION	EPOXY 4-INCH WHITE		EPOXY 8-INCH WHITE	SAME DAY EPOXY 4-INCH YELLOW		EPOXY STOP LINE 18-INCH WHITE	*TEMPORARY MARKING 4-INCH YELLOW		DESCRIPTION
		SOLID LF	SKIPS LF	LF	SOLID LF	SKIPS LF	LF	SOLID LF	SKIPS LF	
356+00 - 357+50	RT	---	---	150	---	---	---	---	---	TURN LANE
357+75	CTH P	---	---	---	30	---	20	60	---	
358+40 - 469+30	RT	11090	---	---	---	---	---	---	---	EDGELINE
469+30 - 470+60	RT	---	33	---	---	---	---	---	---	EDGELINE 3' SKIPS
398+05 - 407+00	CENTERLINE	---	---	---	895	225	---	1790	450	EASTBOUND PASSING
407+00 - 408+60	CENTERLINE	---	---	---	320	---	---	640	---	NO PASSING
408+60 - 417+85	CENTERLINE	---	---	---	925	225	---	1850	450	WESTBOUND PASSING
417+85 - 424+05	CENTERLINE	---	---	---	1240	---	---	2480	---	NO PASSING
424+05 - 436+30	CENTERLINE	---	---	---	1225	300	---	2450	600	EASTBOUND PASSING
436+30 - 503+90	CENTERLINE	---	---	---	---	1688	---	---	3376	PASSING BOTH WAY
470+00	WINNESHIEK DRIVE	---	---	---	---	---	18	---	---	
470+60 - 508+20	RT	3760	---	---	---	---	---	---	---	EDGELINE
508+20 - 510+10	RT	---	48	---	---	---	---	---	---	EDGELINE 3' SKIPS
465+25 - 478+25	LT	1300	---	---	---	---	---	---	---	EDGELINE
478+25 - 479+70	LT	---	36	---	---	---	---	---	---	EDGELINE 3' SKIPS
479+70 - 508+45	LT	2875	---	---	---	---	---	---	---	EDGELINE
508+45 - 510+45	LT	---	51	---	---	---	---	---	---	EDGELINE 3' SKIPS
503+90 - 521+05	CENTERLINE	---	---	---	3430	---	---	6860	---	NO PASSING
509+00	COONBLUFF RD, RT	---	---	---	40	---	---	80	---	
510+10 - 520+35	RT	1025	---	---	---	---	---	---	---	EDGELINE
520+35 - 521+80	RT	---	36	---	---	---	---	---	---	EDGELINE 3' SKIPS
510+45 - 559+50	LT	4905	---	---	---	---	---	---	---	EDGELINE
559+50 - 561+10	LT	---	39	---	---	---	---	---	---	EDGELINE 3' SKIPS
516+60 - 519+40	LT	---	75	---	---	---	---	---	---	EDGELINE 12.5' SKIPS
521+05 - 536+60	CENTERLINE	---	---	---	1555	388	---	3110	776	EASTBOUND PASSING
521+80 - 562+75	RT	4095	---	---	---	---	---	---	---	EDGELINE
562+75 - 564+50	RT	---	42	---	---	---	---	---	---	EDGELINE 3' SKIPS
536+60 - 545+45	CENTERLINE	---	---	---	---	225	---	---	450	PASSING BOTH WAY
545+45 - 556+40	CENTERLINE	---	---	---	1095	275	---	2190	550	WESTBOUND PASSING
556+40 - 556+80	CENTERLINE	---	---	---	80	---	---	160	---	NO PASSING
556+80 - 568+15	CENTERLINE	---	---	---	1135	275	---	2270	550	EASTBOUND PASSING
568+15 - 588+60	CENTERLINE	---	---	---	---	513	---	---	1026	PASSING BOTH WAY
563+75	LYNDON RD, RT	---	---	---	40	---	---	80	---	
561+10 - 623+15	LT	6205	---	---	---	---	---	---	---	EDGELINE
623+15 - 624+40	LT	---	30	---	---	---	---	---	---	EDGELINE 3' SKIPS
564+50 - 610+85	RT	4635	---	---	---	---	---	---	---	EDGELINE
610+85 - 612+25	RT	---	36	---	---	---	---	---	---	EDGELINE 3' SKIPS
588+60 - 599+65	CENTERLINE	---	---	---	1105	275	---	2210	550	WESTBOUND PASSING
599+65 - 698+70	CENTERLINE	---	---	---	19810	---	---	39620	---	NO PASSING
612+25 - 617+30	RT	505	---	---	---	---	---	---	---	EDGELINE
617+30 - 618+55	RT	---	30	---	---	---	---	---	---	EDGELINE 3' SKIPS
618+55 - 650+30	RT	3175	---	---	---	---	---	---	---	EDGELINE
650+30 - 651+55	RT	---	30	---	---	---	---	---	---	EDGELINE 3' SKIPS
624+40 - 689+65	LT	6525	---	---	---	---	---	---	---	EDGELINE
689+65 - 691+15	LT	---	36	---	---	---	---	---	---	EDGELINE 3' SKIPS
691+15 - 698+70	LT	755	---	---	---	---	---	---	---	EDGELINE
651+55 - 698+70	RT	4715	---	---	---	---	---	---	---	EDGELINE
SUBTOTAL		125,117	1,082	150	74,659	9,228	38	149,318	18,456	
TOTAL		126,199		150	83,887		38	167,774		

* INCLUDES TWO APPLICATIONS: ONE ON THE SAME DAY AS PAVING THE CIR LAYER, AND ONE ON SAME DAY AS PAVING THE LOWER HMA LAYER.

648.0100 LOCATING NO-PASSING ZONES

STATION - STATION	MI
52+48.54 - 698+68.81	12.24
TOTAL	12.24

650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE

STATION - STATION	LF
58+61.53 - 399+17.83	34,056
399+54.00 - 693+00.00	29,346
TOTAL	63,402

690.0150 SAWING ASPHALT

STATION	LOCATION	LF
53+80	RT	37
20TH ST	RT	38
73+15	RT	16
73+40	LT	37
77+85	RT	16
87+00	LT	30
GOLF COURSE RD	RT	30
WILLOW CREEK RD	LT	18
110+30	LT	19
CHADWICK RD	LT	20
NORTH DEWEY AVE	LT	25
122+35	RT	65
125+35	LT	29
131+00	LT	47
KEENAN CT	LT	27
146+15	LT	45
148+30	LT	43
158+00	LT	42
GLEN VALLEY DR	LT	14
DROVER PASS	LT	21
SOUTH AVE	RT	20
D&W RD	LT	24
237+65	LT	28
ACORN LN	RT	21
CTH HH	LT	44
281+15	LT	46
290+60	LT	27
CHERY LN	LT	18
308+10	LT	37
BLUEBIRD LN	RT	15
316+95	RT	19
320+85	LT	22
334+15	LT	29
TOWN HALL RD	RT & LT	47
366+95	RT	42
370+60	RT	49
371+95	LT	50
379+70	RT	32
397+50	LT	24
416+00	LT	22
431+35	RT	25
431+45	LT	27
435+90	LT	34
437+10	LT	40
438+40	RT	22
442+45	RT	21
446+50	RT	22
451+65	LT	24
452+70	LT	17
455+40	LT	27
459+05	LT	19
461+15	RT	28
463+00	RT	22
OAK HILL RD	LT	21
WINNESHIEK DR	RT	27
473+80	LT	23
476+15	LT	21
NORTH AVE	LT	27
487+70	LT	21
495+30	LT	19

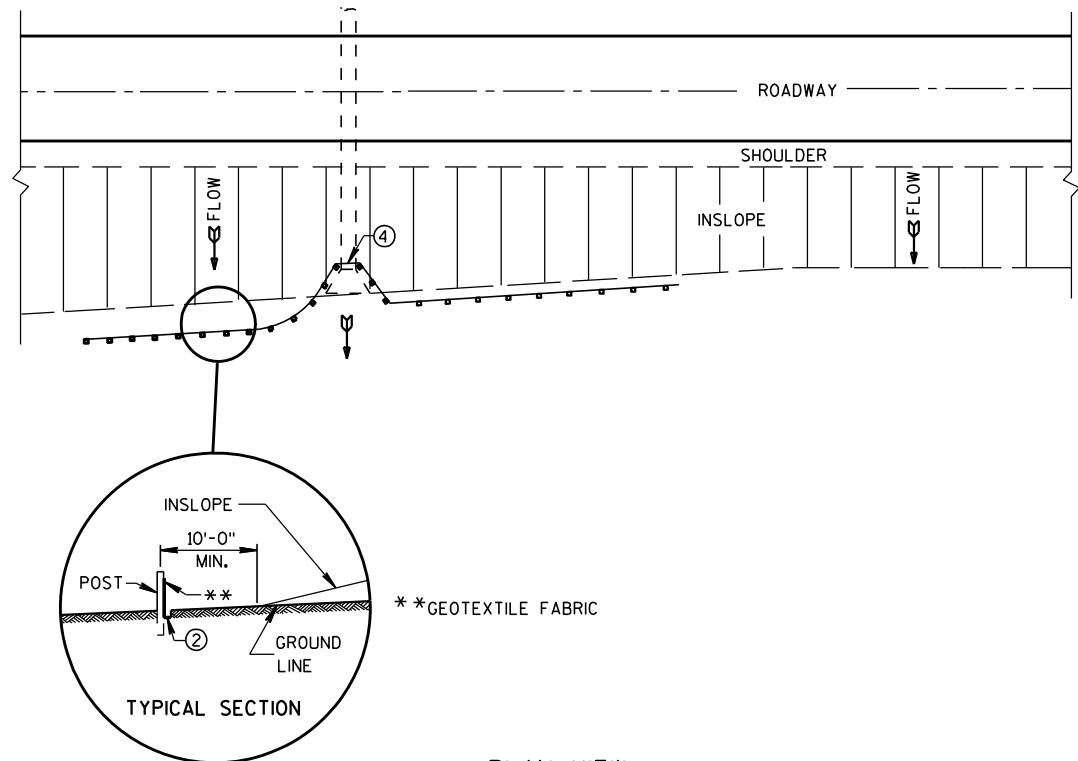
CONTINUED IN NEXT COLUMN

690.0150 SAWING ASPHALT

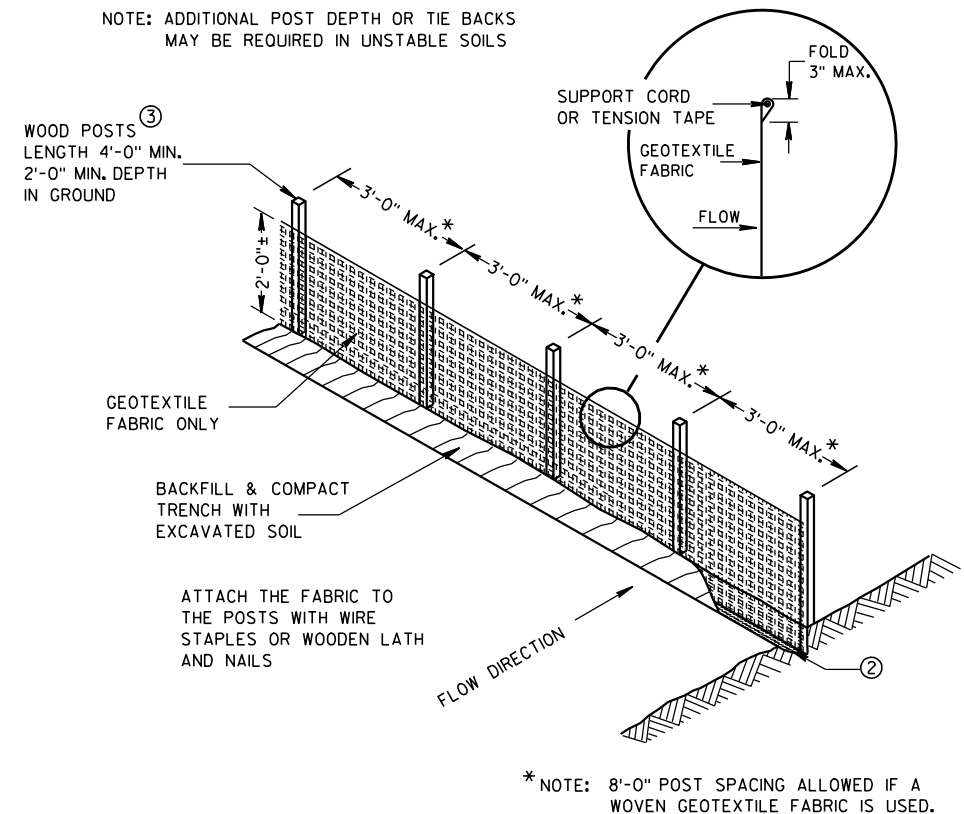
STATION	LOCATION	LF
COONBLUFF RD	RT & LT	60
517+65	RT	74
CHRISTMAS MOUNTIAN RD	RT	26
535+85	LT	28
548+50	LT	27
552+90	LT	25
LYNDON RD	RT & LT	55
BIRCHWOOD RD SPUR	RT	23
BIRCHWOOD RD	RT & LT	44
638+80	LT	43
TESSERS RD	RT	23
674+05	RT	27
678+60	LT	27
OLD US 12	LT	32
TOTAL		2236

Standard Detail Drawing List

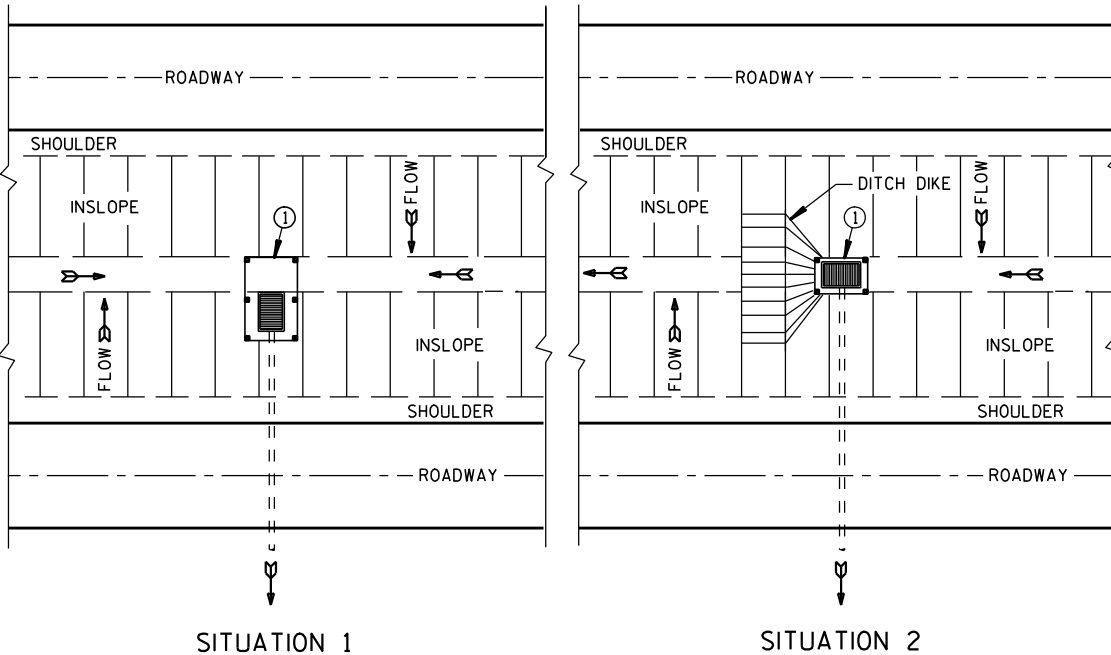
08E09-06	SILT FENCE
14B15-08A	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-08B	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B24-08A	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
14B24-08B	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
14B24-08C	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
14B42-03A	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-03B	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-03C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B44-02A	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-02B	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-02C	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
15C05-02	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C08-16B	PAVEMENT MARKING (INTERSECTIONS)
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C33-01	STOP LINE AND CROSSWALK PAVEMENT MARKING



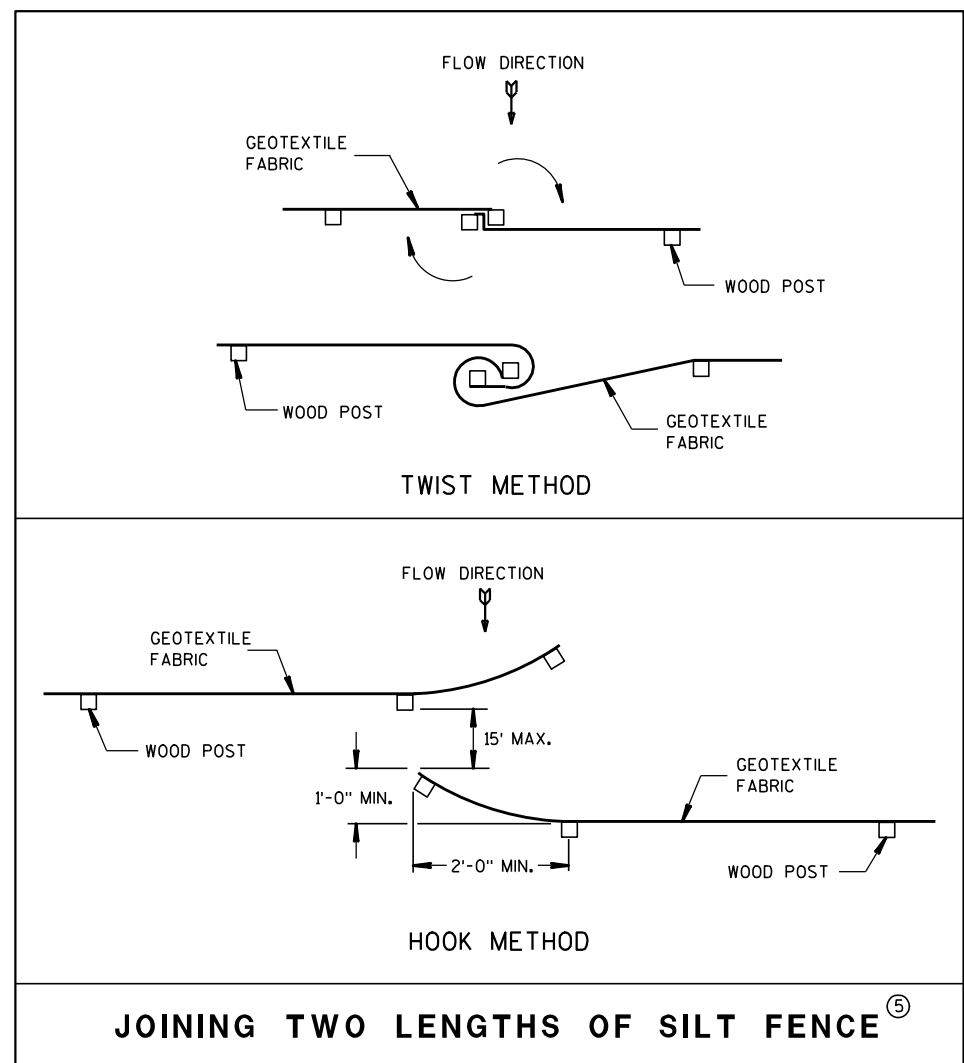
PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE



SILT FENCE



PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

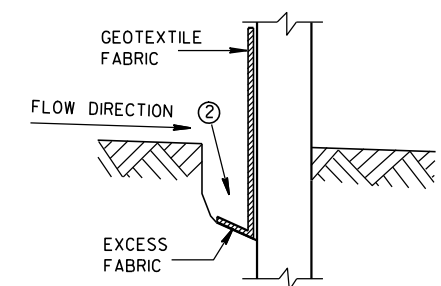


JOINING TWO LENGTHS OF SILT FENCE ⑤

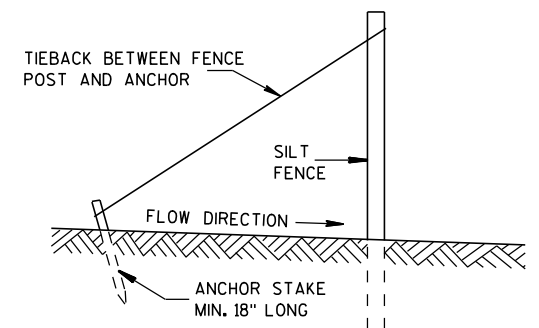
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



TRENCH DETAIL



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

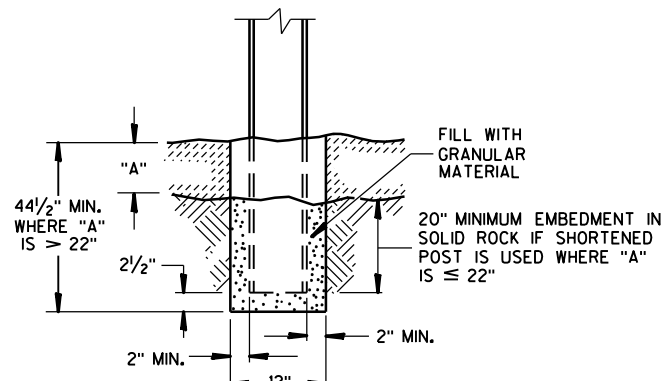
SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-29-05 DATE	/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

GENERAL NOTES

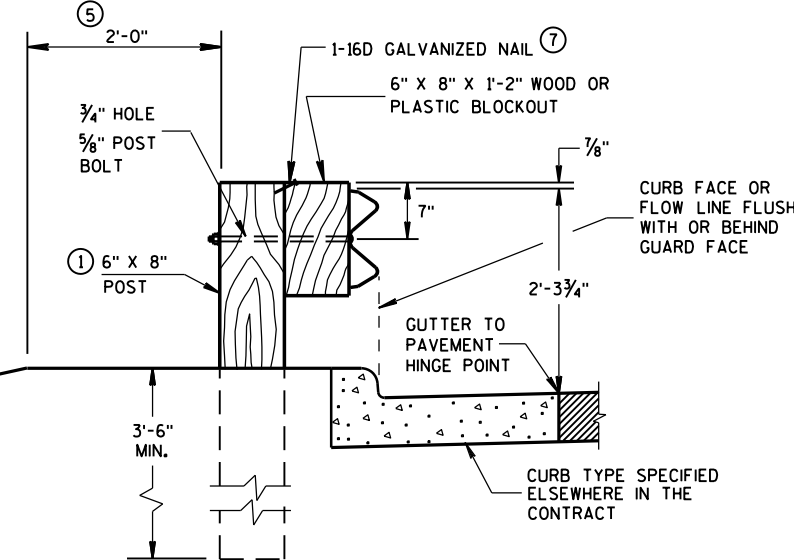
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, AND THE APPLICABLE SPECIAL PROVISIONS.

- ① W6 X 9 OR W6 X 8.5 STEEL POSTS AND NOTCHED PLASTIC BLOCKOUTS ARE ACCEPTABLE ALTERNATIVES FOR 6" X 8" WOOD POSTS WITH WOOD OR PLASTIC BLOCKOUTS. USE APPROVED NOTCHED PLASTIC BLOCKOUTS WITH STEEL POSTS. APPROVED PLASTIC BLOCKOUT DESIGNS MAY VARY FROM THIS TYPICAL DETAIL WHEN USED IN CONJUNCTION WITH STEEL POSTS. DO NOT MIX STEEL POSTS AND WOOD POSTS IN A SINGLE INSTALLATION.
- ② USE STRUCTURAL STEEL POSTS CONFORMING TO ASTM A 36. GALVANIZED POSTS ACCORDING TO AASHTO M 111 EITHER SET THE POSTS IN DRILLED HOLES OR DRIVE TO GRADE. REMOVE MUSHROOMING CAUSED BY DRIVING AND REPAIR DAMAGED SPELTER COATING ON GALVANIZED POSTS.
- ③ INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
- ④ USE EITHER WOOD OR APPROVED PLASTIC BLOCKOUTS ON WOOD POSTS.
- ⑤ IF THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING, W BEAM (LHW).
- ⑥ IF ROCK IS ENCOUNTERED DURING EXCAVATION, THE ENGINEER MAY APPROVE USING A 12 INCH DIAMETER POST HOLE EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE APPROXIMATELY 2 1/2 INCHES DEEP. CUT THE POSTS TO LENGTH AND PLACE IN THE HOLE. BACKFILL WITH MATERIAL EXCAVATED FROM THE HOLE AND COMPACT ADEQUATELY.
- ⑦ WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

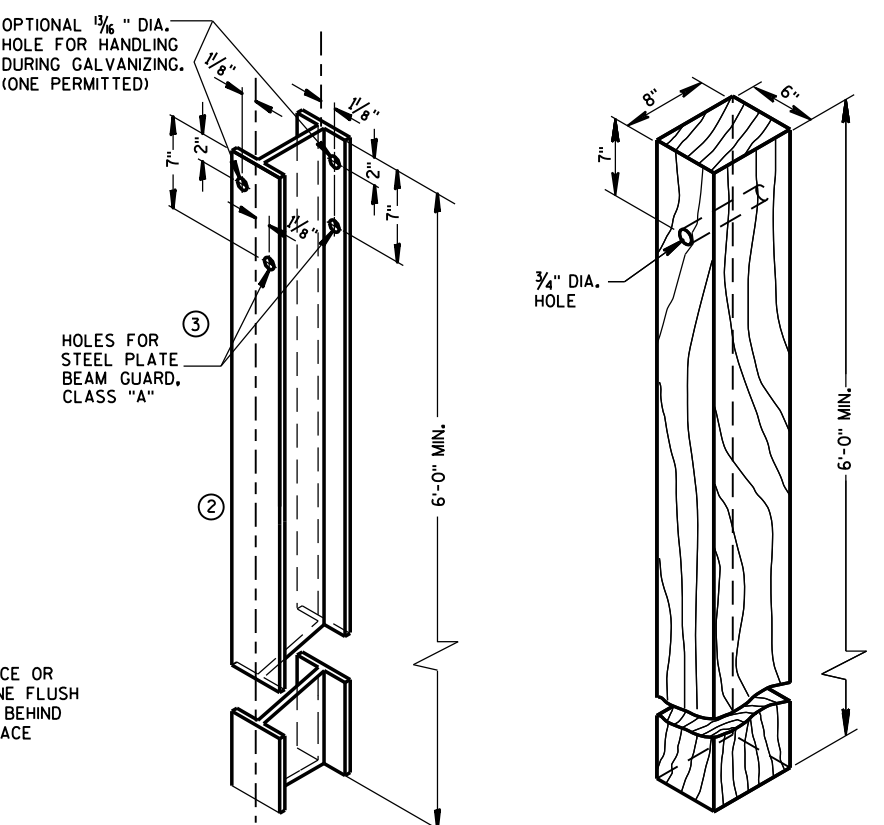
INSTALL BEAM GUARD SECTIONS AND ALL NECESSARY HARDWARE ACCORDING TO THE APPLICABLE PLAN AND CURRENT STANDARD AND SUPPLEMENTAL SPECIFICATIONS. ALL DIMENSIONS ARE SUBJECT TO MANUFACTURER'S TOLERANCES EXCEPT WHERE ALLOWABLE TOLERANCES ARE SHOWN.



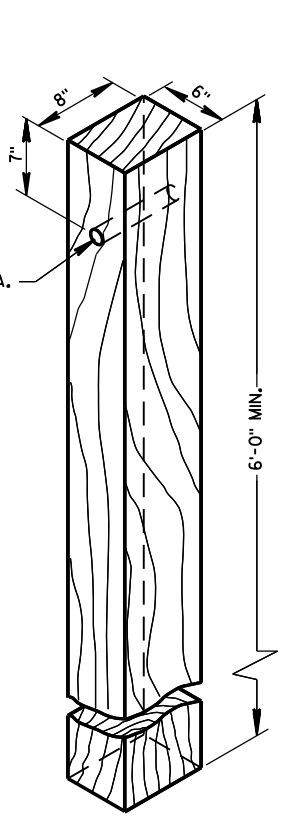
END VIEW
SETTING STEEL OR WOOD POST IN ROCK ⑥



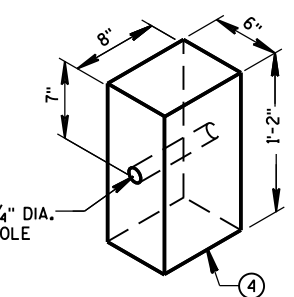
END VIEW
LOCATED ALONG A CURBED ROADWAY



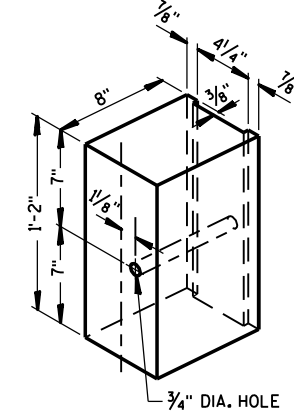
STEEL POST &
HOLE PUNCHING DETAIL
(W6 X 9) ①
ALL HOLES 1 3/8" DIAMETER EXCEPT AS NOTED



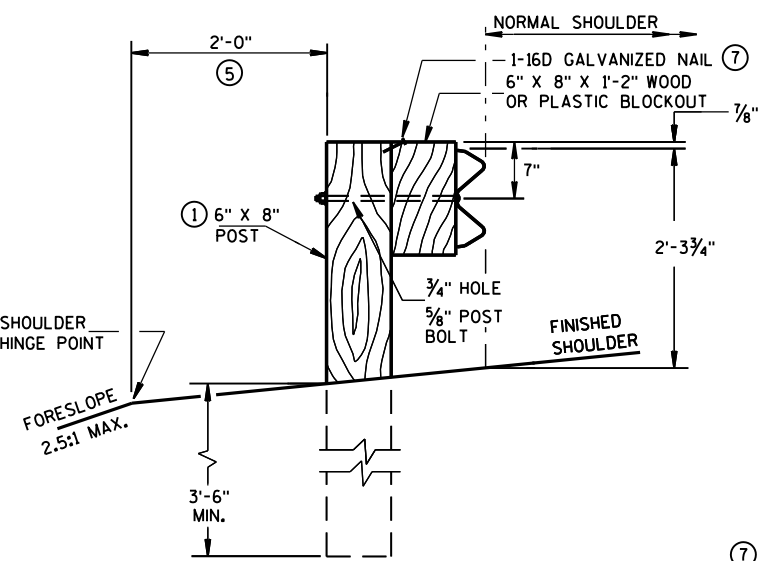
WOOD POST
(6" X 8") NOMINAL



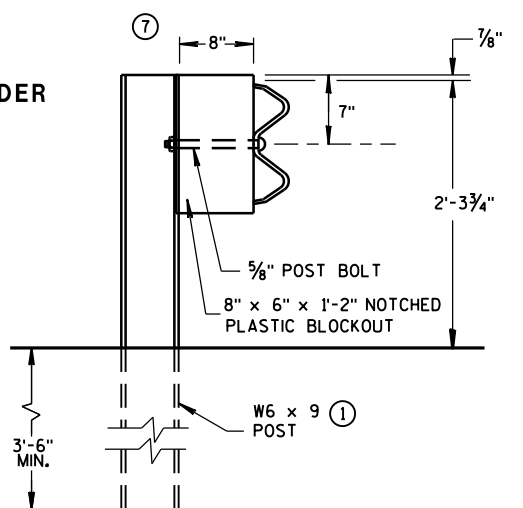
WOOD OR PLASTIC
BLOCKOUT FOR
WOOD POSTS



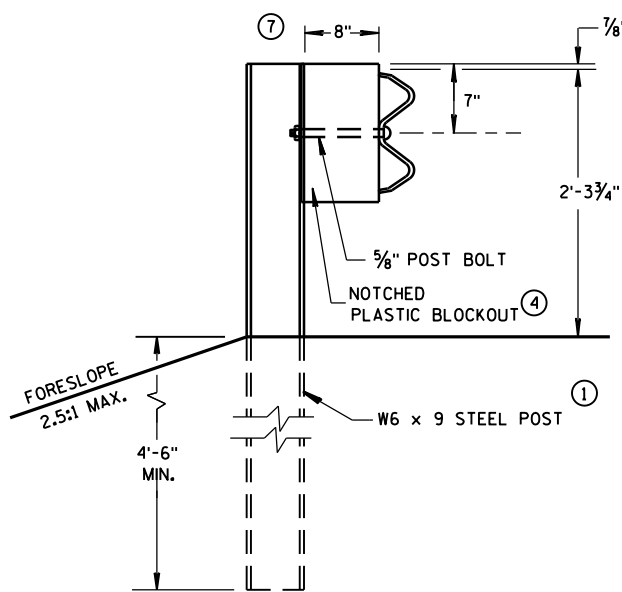
TYPICAL NOTCHED
PLASTIC BLOCKOUT
FOR STEEL POSTS ①



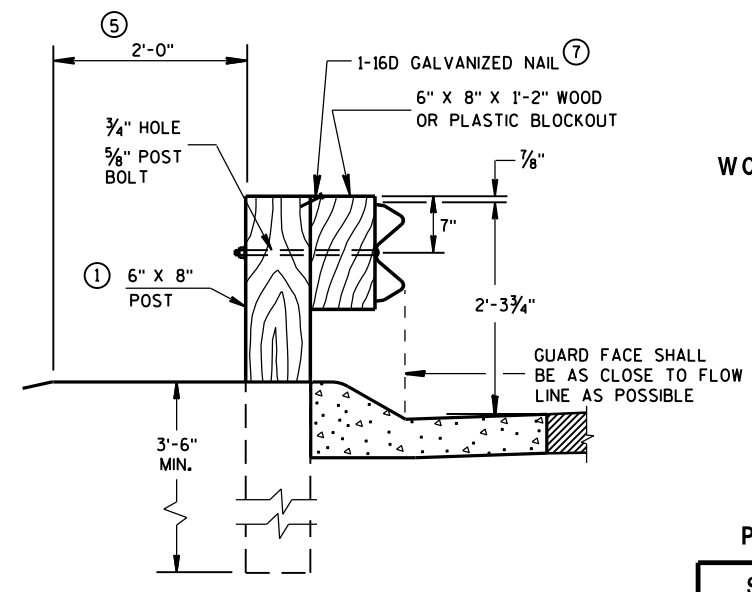
END VIEW
LOCATED ALONG A ROADWAY SHOULDER
STANDARD INSTALLATION



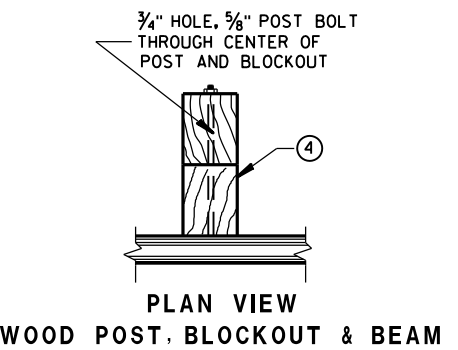
END VIEW
STEEL POST & NOTCHED
PLASTIC BLOCKOUT ALTERNATIVE
STANDARD INSTALLATION



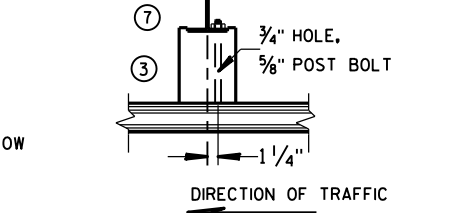
END VIEW
LONGER POST AT HALF
POST SPACING W BEAM
(LHW)



END VIEW
LOCATED ALONG A
MOUNTABLE CURBED ROADWAY



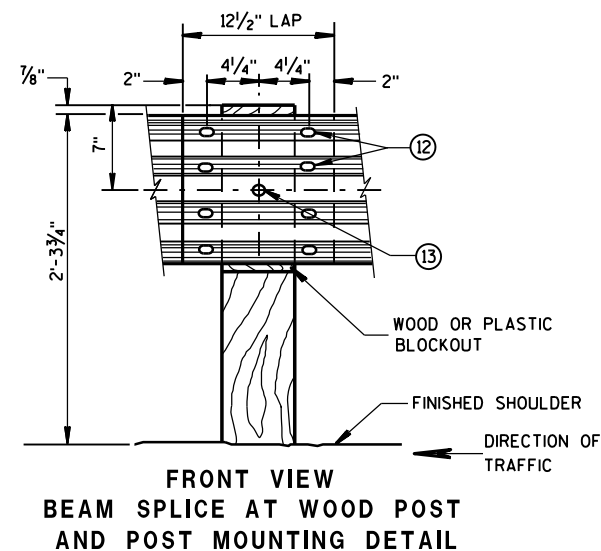
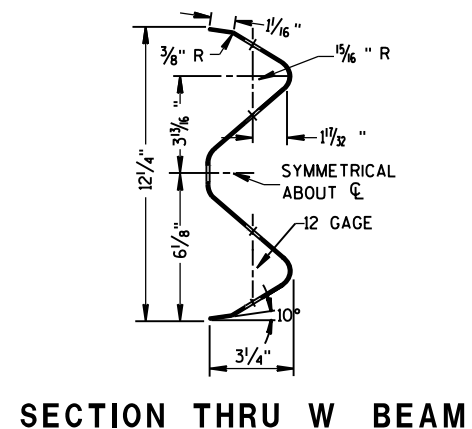
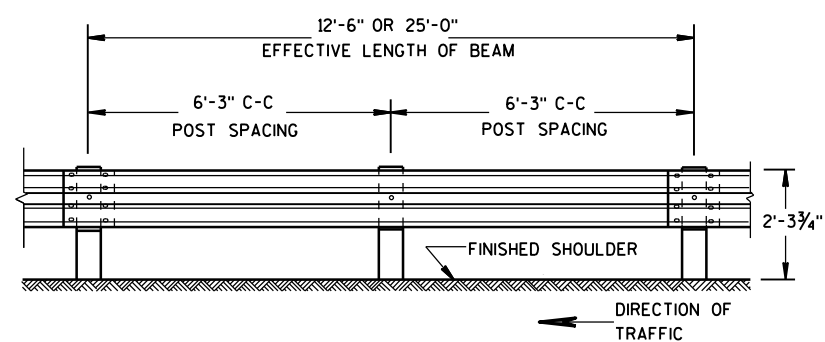
PLAN VIEW
WOOD POST, BLOCKOUT & BEAM



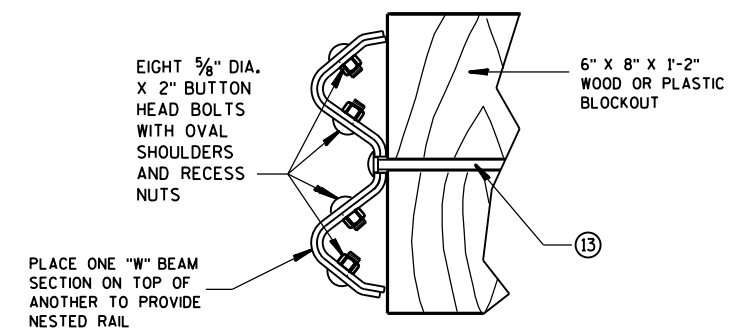
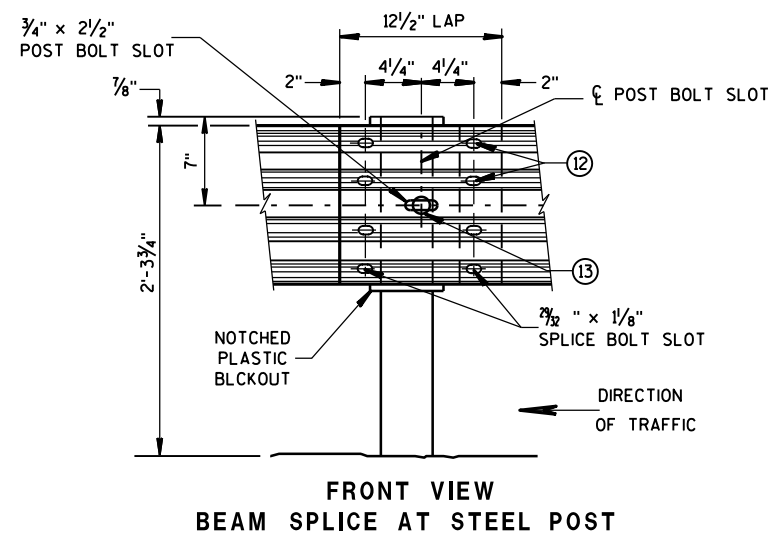
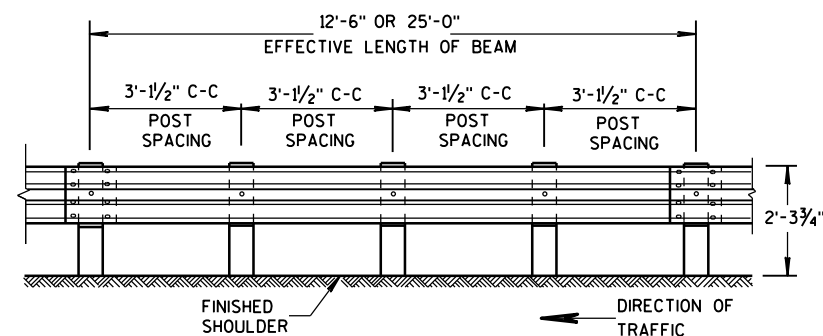
PLAN VIEW
STEEL POST, NOTCHED
PLASTIC BLOCKOUT & BEAM

STEEL PLATE BEAM GUARD,
CLASS "A"
INSTALLATION & ELEMENTS

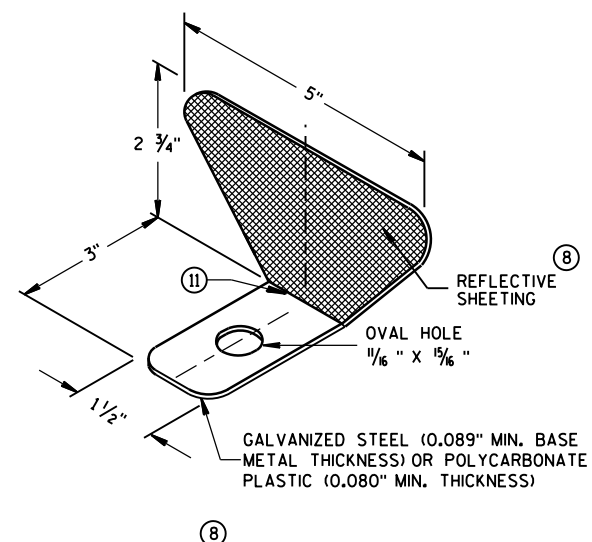
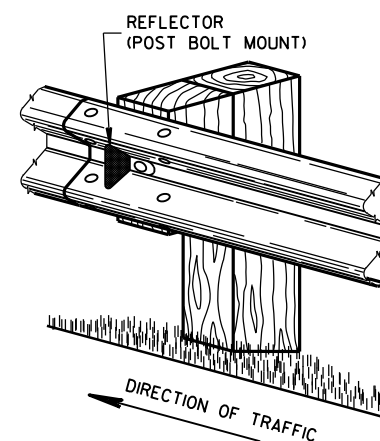
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



- ## GENERAL NOTES
- ⑧ PROVIDE SILVER REFLECTIVE SHEETING ON ALL REFLECTORS EXCEPT THOSE LOCATED ALONG THE LEFT EDGE OF ONE-WAY ROADWAYS, WHICH SHALL BE PROVIDED WITH YELLOW REFLECTIVE SHEETING. SHEETING IS TYPE H. SEE STANDARD SPECIFICATION 637.
 - ⑨ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.
 - ⑩ REVERSE EVERY OTHER REFLECTOR FOR 2-WAY VISIBILITY. THE CONTRACTOR MAY FURNISH TWO-SIDED REFLECTORS IN LIEU OF ONE-SIDED REFLECTORS.
 - ⑪ PROVIDE AN ANGLE OF BEND OF $90^{\circ} \pm 1^{\circ}$ FOR TWO-SIDED REFLECTORS.
 - ⑫ 8 - $\frac{5}{8}$ " ϕ X 2" BUTTON HEAD BOLTS WITH OVAL SHOULDERS & RECESS NUTS.
 - ⑬ $\frac{5}{8}$ " DIA. BUTTON HEAD BOLT AND RECESS NUT WITH $\frac{5}{8}$ " DIA. F844 FLAT WASHER UNDER NUT.



	BEAM GUARD LENGTH	REFLECTOR SPACING	NO. SURFACES REFLECTORIZED	MIN. NO. REFLECTORS
ONE WAY TRAFFIC	< 200' > 200'	50' C-C 100' C-C	1 1	3
TWO WAY TRAFFIC	< 200' > 200'	25' C-C 50' C-C	1 1 ⁽¹⁰⁾	6
TWO WAY TRAFFIC	< 200' > 200'	50' C-C 100' C-C	2 2 ⁽¹¹⁾	3



STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

BILL OF MATERIALS

NOTE NO.	QTY.	DESCRIPTION
①	4	WOOD BREAKAWAY TERMINAL POST: 5 1/2" X 7 1/2" X 3'-9"
②	**	STEEL TUBE: OPTION 1 - QUANTITY OF 4 TS 8" X 6" X 0.188", 4'-6" LONG OR OPTION 2 - QUANTITY OF 2 TS 8" X 6" X 0.188", 6'-0" AND 2 TS 8" X 6" X 0.188", 4'-6" LONG
③	2	SOIL PLATE: 2'-0" X 1'-6" X 1/4" **
④	4	WOOD BREAKAWAY CRT POST: 6" X 8" X 6'-0"
⑤	6	WOOD OFFSET BLOCKS: 6' X 8" X 1'-2"
⑥	1	PIPE SLEEVE: 2" X 5 1/2" STANDARD PIPE
⑦	1	BEARING PLATE
⑧	1	BCT CABLE ASSEMBLY
⑨	1	CABLE ANCHOR BOX
⑩	1	STRUT & YOKE
⑪	1	STEEL PLATE BEAM, END PANEL 12 GA. 13'-6 1/2" LONG FOR SKT-350, ET-2000 AND ET-2000 PLUS
⑫	3	STEEL PLATE BEAM: 12 GA. 13'-6 1/2"
⑬	1	ET-2000/ET-2000 PLUS GUARDRAIL EXTRUDER OR SKT-350 IMPACT HEAD: AS FURNISHED BY MANUFACTURER
⑭	1	0.040" ALUMINUM SHEET WITH REFLECTIVE SHEETING TYPE F PER SECTION 637 OF THE STANDARD SPECIFICATIONS
⑮	1	E.A.T. MARKER POST

GENERAL NOTES

FOLLOW MANUFACTURE'S BOLTING RECOMMENDATIONS, IF NONE ARE AVAILABLE, INSTALL 3/8" ϕ X 1'-6" BUTTON HEAD BOLTS AT ALL POSTS EXCEPT FOR POST 1.

(A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE (HPL), AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.

(B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED.

(C) THE 13 SLOT FIRST RAIL PANEL MAY BE USED IN LIEU OF THE 3 SLOT RAIL PANEL ON SKT-350 ONLY.

(D) THE TOP OF THE STEEL TUBE ON POSTS 1 THROUGH 4 SHALL NOT BE MORE THAN 3" ABOVE THE FINISH GROUND ELEVATION.

(E) THE CENTER OF THE UPPER 3 1/2" DIAMETER HOLE ON POST 5 THROUGH 8 SHALL BE 3/4" ABOVE THE FINISHED GROUND LINE.

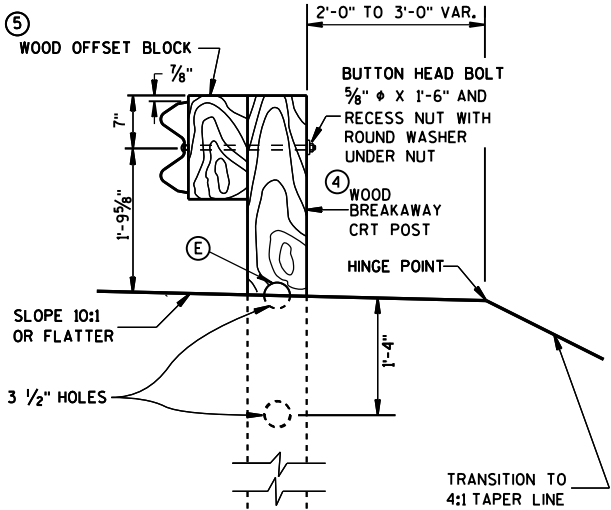
(F) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF-TAPPING SCREWS, ONE SCREW PER CORNER.

STEEL POSTS SHALL NOT BE ALLOWED FOR USE WITH ENERGY ABSORBING TERMINALS.

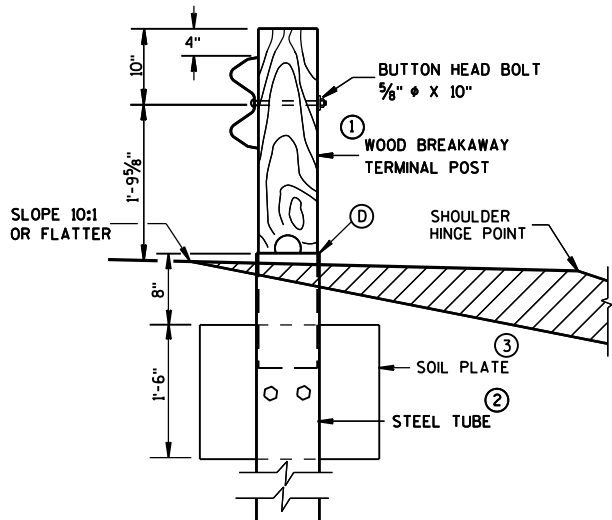
DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

* DO NOT ATTACH BLOCKOUTS TO POSTS 1 AND 2.

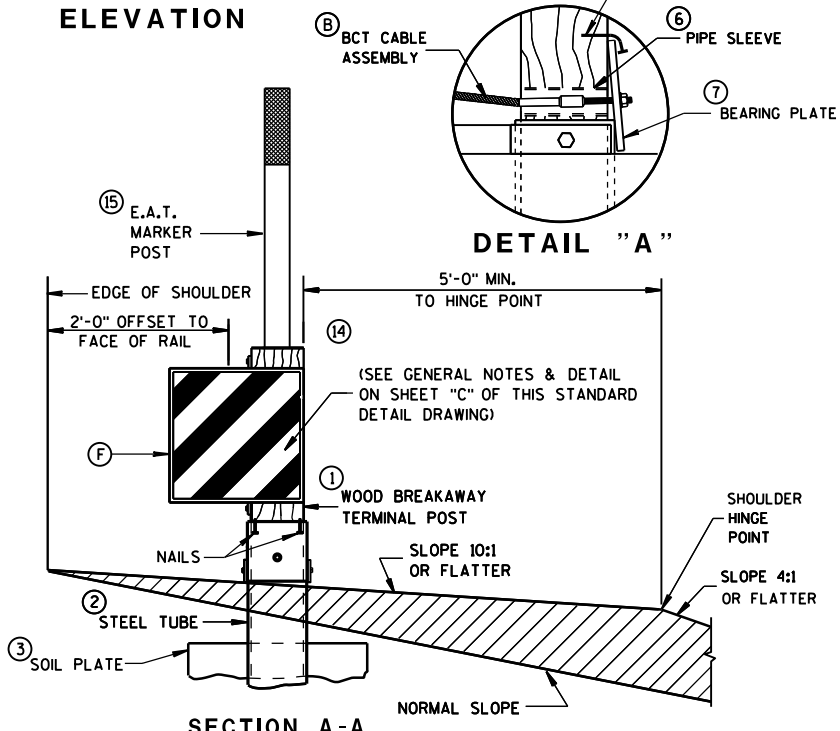
** SDD SHOWS 4 - 54 INCH STEEL TUBES WITH SOIL PLATES INSTALLED ON POST 1 AND POST 2. POST 3 AND 4 DO NOT NEED SOIL PLATES. AN ALTERNATIVE INSTALLATION WOULD CONSIST OF 2 - 72 INCH STEEL TUBES ON POST 1 AND POST 2 AND 54 INCH SOIL TUBES ON POSTS 3 AND 4. THE ALTERNATIVE INSTALLATION DOES NOT REQUIRE SOIL PLATES.



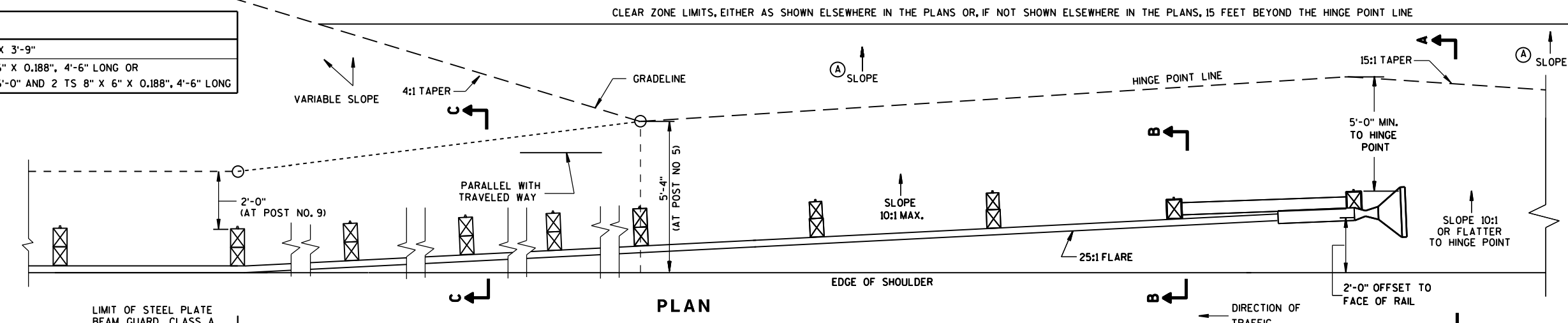
SECTION C-C
TYPICAL AT POST NOS. 6, 8



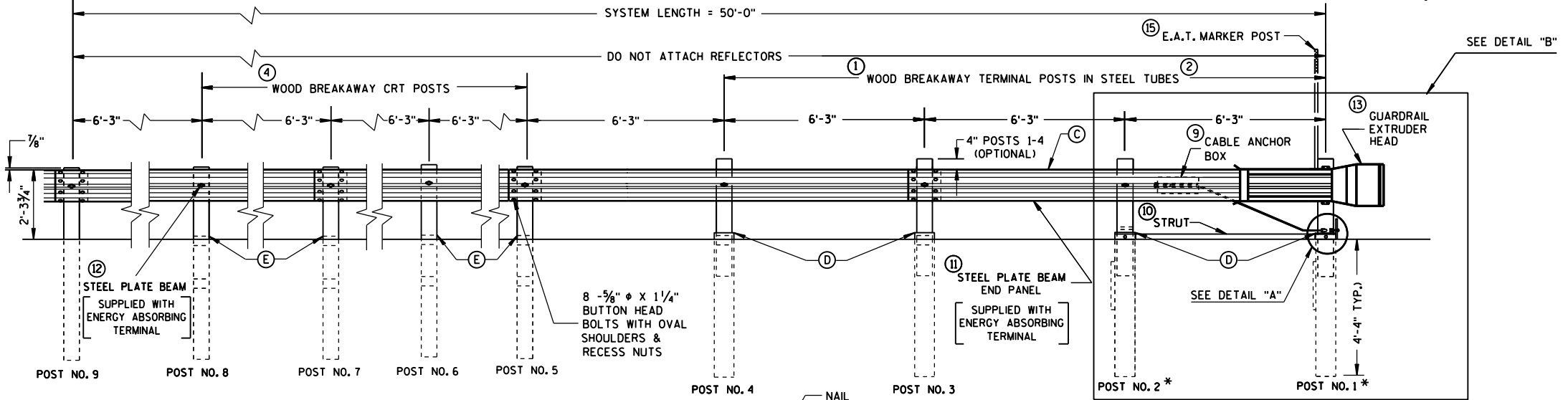
SECTION B-B
TYPICAL AT POST NO. 2 *



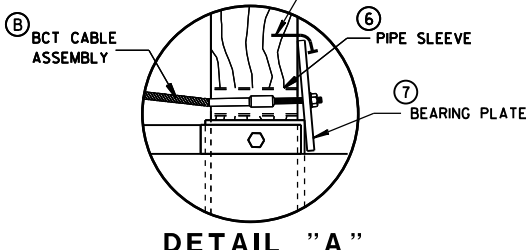
SECTION A-A
TYPICAL AT POST NO. 1 *



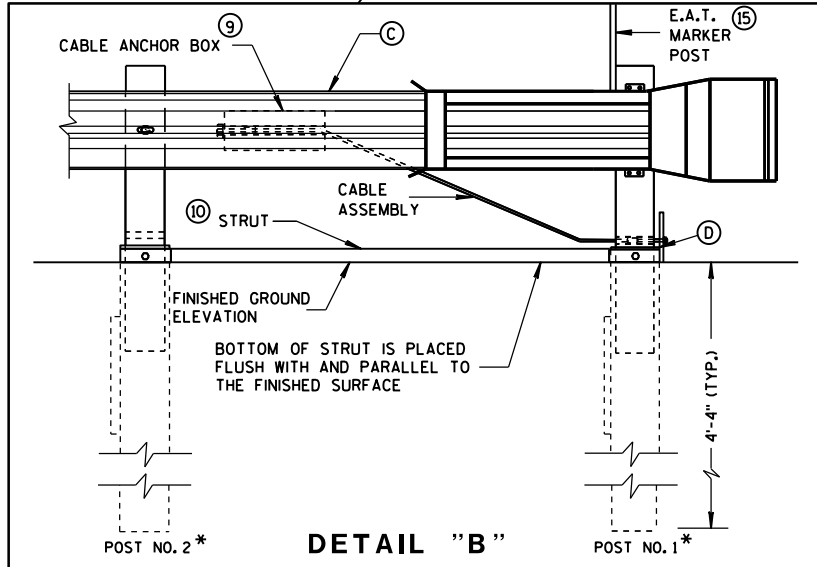
PLAN



ELEVATION



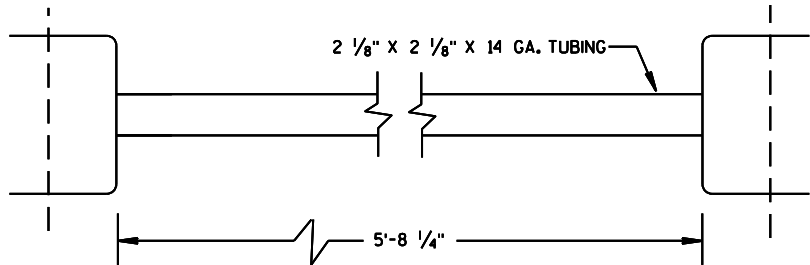
DETAIL "A"



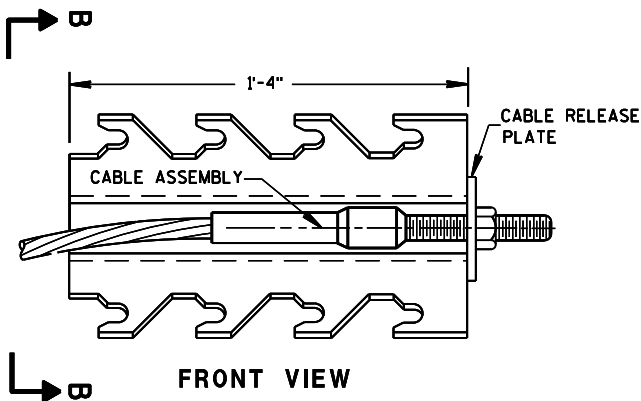
DETAIL "B"

STEEL PLATE BEAM GUARD
ENERGY ABSORBING TERMINAL

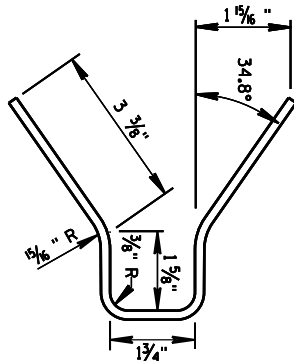
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



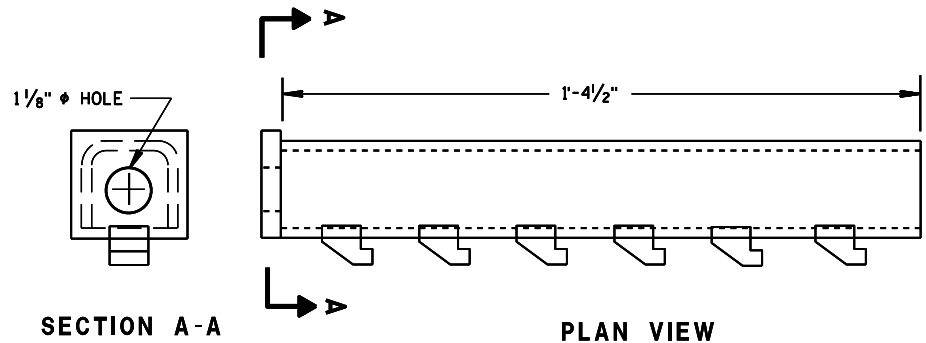
⑩ STRUT DETAIL (SKT-350)



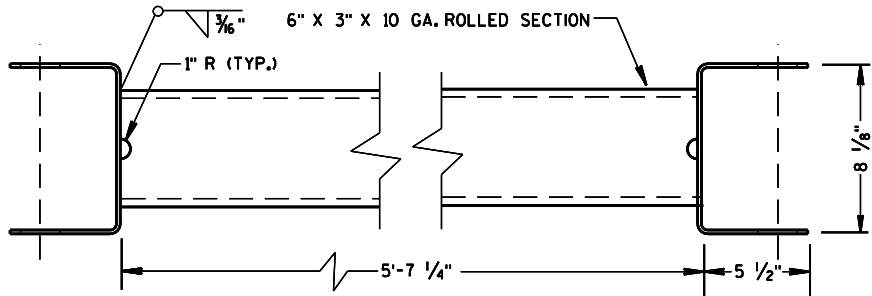
⑨ CABLE ANCHOR BOX (SKT-350)
(SKT-350)



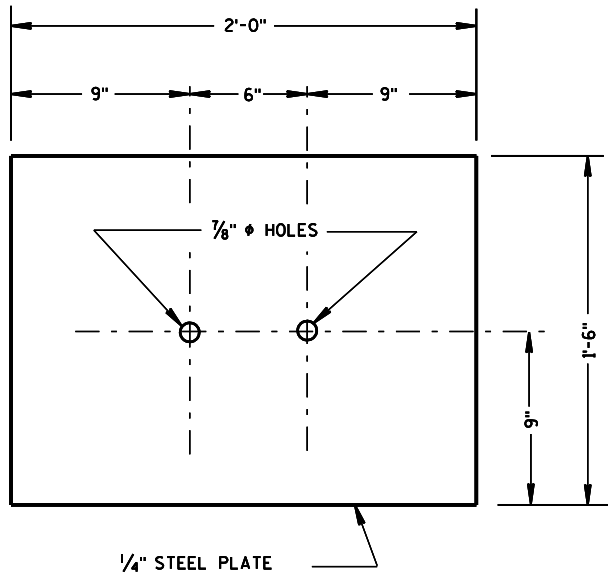
SECTION B-B



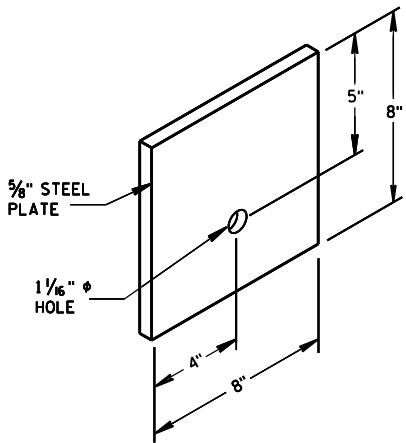
⑨ CABLE ANCHOR BOX (ET-2000/ET-2000 PLUS)



⑩ STRUT DETAIL (ET-2000/ET-2000 PLUS)
(ET-2000/ET-2000 PLUS)



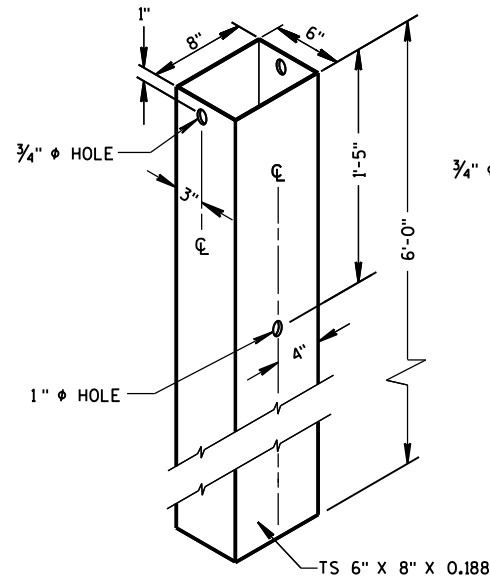
③ SOIL PLATE
(SKT-350, ET-2000/ET-2000 PLUS)



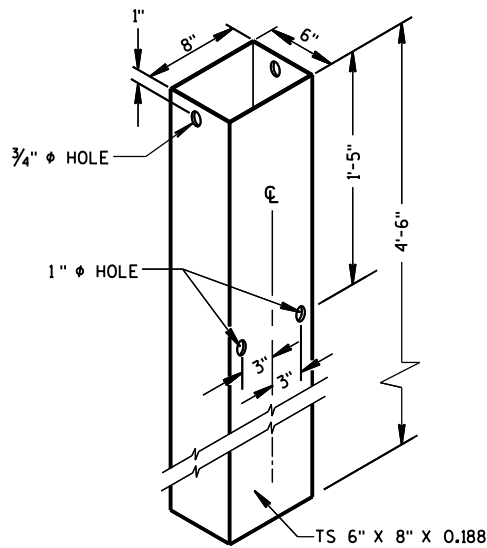
⑦ STEEL BEARING PLATE
(SKT-350, ET-2000/ET-2000 PLUS)

STEEL PLATE BEAM GUARD
ENERGY ABSORBING TERMINAL

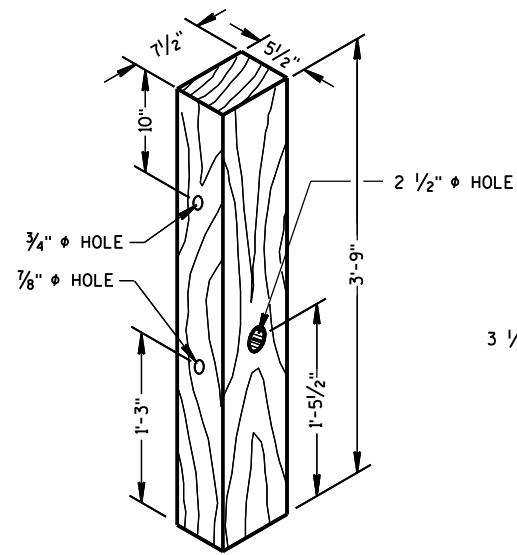
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



② **72" STEEL TUBE**
(POSTS NO. 1-4)

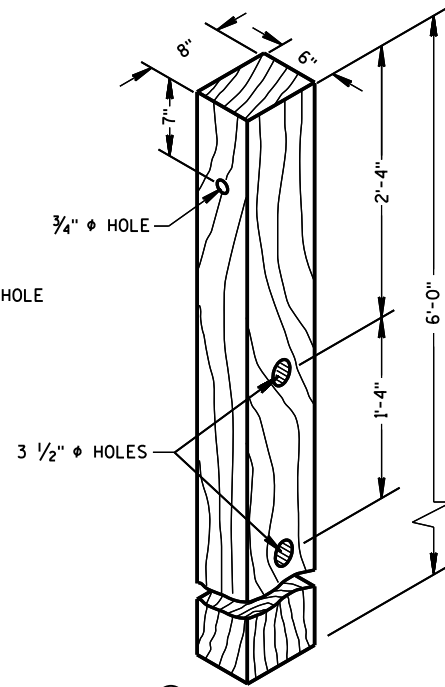


② **54" STEEL TUBE**
(POSTS NO. 1-4)



① **TERMINAL POST**
(POSTS NO. 1-4)

WOOD BREAKAWAY POSTS



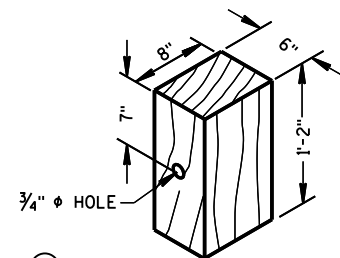
④ **CRT POST**
(POSTS NO'S 5-8)

GENERAL NOTES

WHEN ROCK IS ENCOUNTERED DURING EXCAVATION, A 12 INCH DIA. POST HOLE EXTENDING 20 INCHES DEEP INTO THE ROCK MAY BE USED IF APPROVED BY THE ENGINEER. GRANULAR MATERIAL SHALL BE PLACED IN THE BOTTOM OF THE HOLE APPROXIMATELY 2 1/2" INCHES DEEP TO PROVIDE DRAINAGE. THE SOIL TUBES SHALL BE FIELD CUT TO LENGTH, PLACED IN THE HOLE AND BACKFILLED WITH ADEQUATELY COMPACTED MATERIAL EXCAVATED FROM THE HOLE.

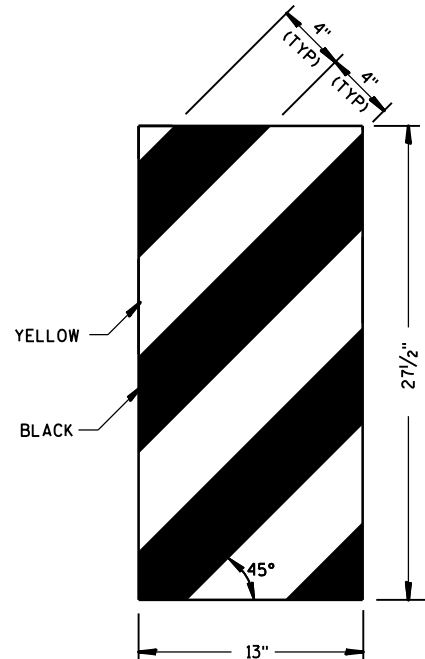
SEE APPROVED PRODUCTS LIST FOR ACCEPTABLE E. A. T. MARKER POST.

ⓐ 1/2" DIA. X 3" LAG BOLT WITH WASHER.

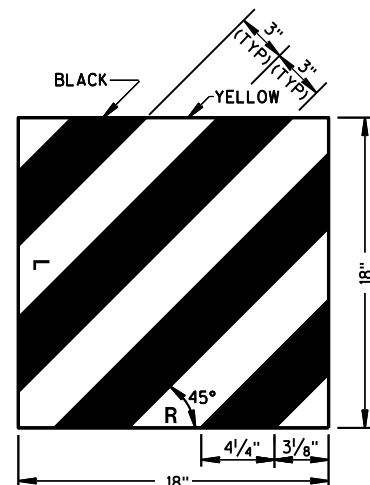


⑤ **WOOD OFFSET BLOCK**
REQ'D. AT ALL POSTS EXCEPT POST NO'S 1 & 2

TYPE H
YELLOW REFLECTIVE
SHEETING 3" X 9"
SEE STANDARD
SPECIFICATION 637

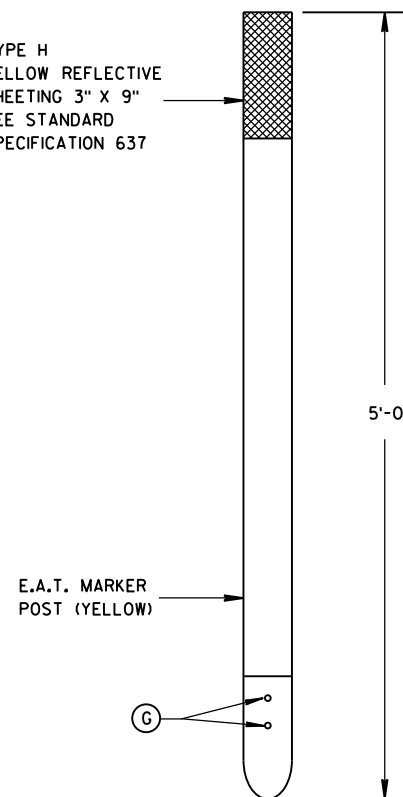


ET-2000 PLUS ONLY

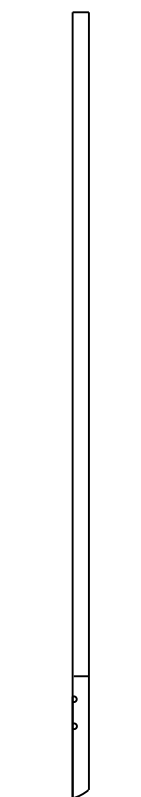


ET-2000 AND SKT-350

⑭ **REFLECTIVE SHEETING DETAILS**

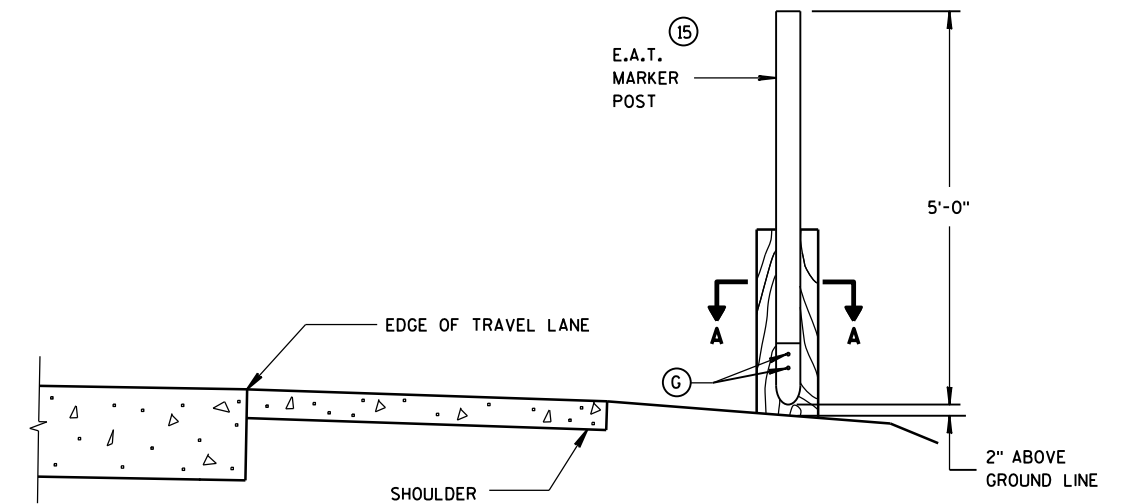


FRONT VIEW

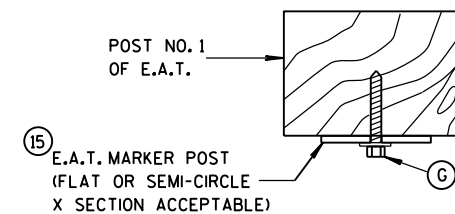


SIDE VIEW

⑮ **E.A.T. MARKER POST**



TYPICAL INSTALLATION OF E.A.T. MARKER POST BACKSIDE OF POST NO. 1
(E.A.T. AND RAIL REMOVED FOR CLARITY)



SECTION A-A

**STEEL PLATE BEAM GUARD
ENERGY ABSORBING TERMINAL**

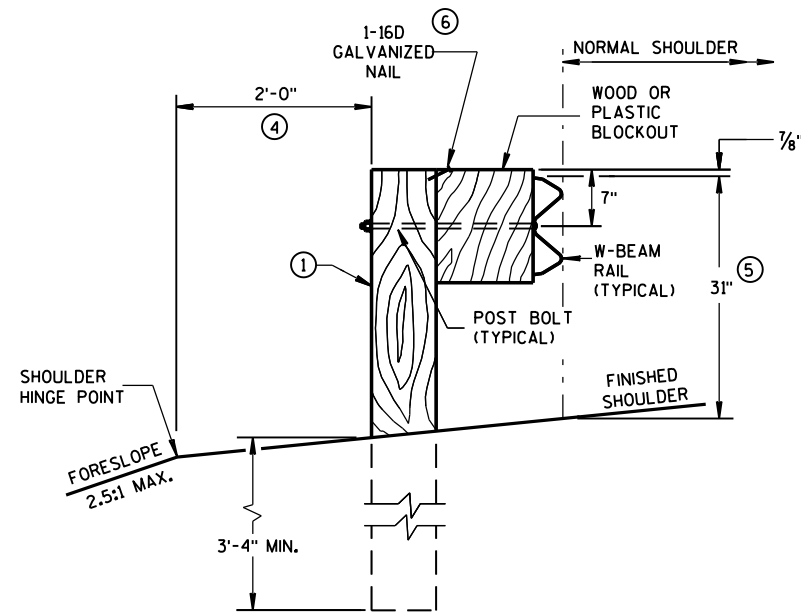
**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

APPROVED
June 2014
DATE
FHWA

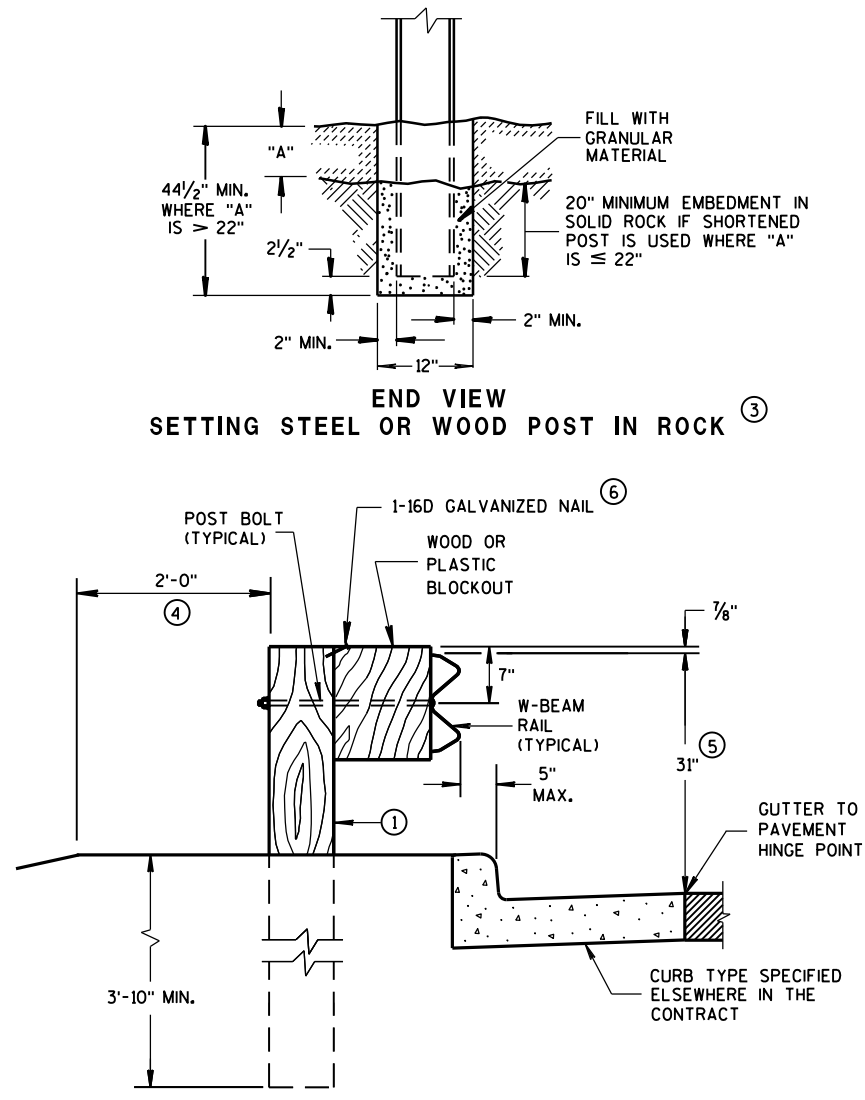
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

GENERAL NOTES

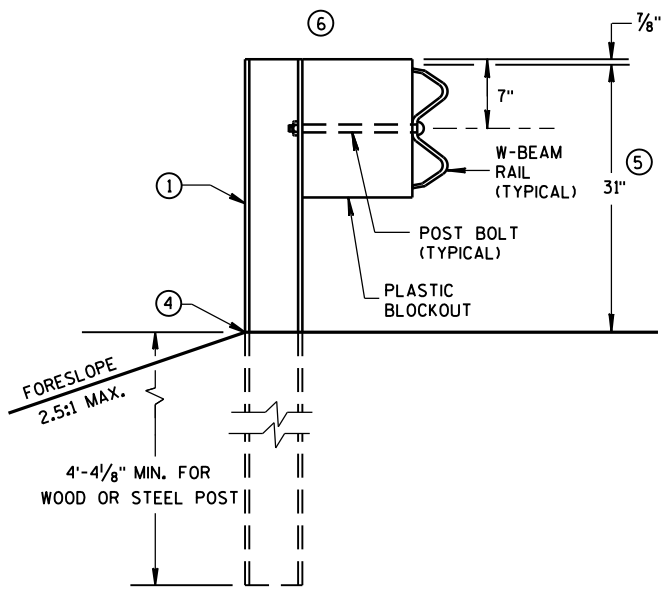
- ① WOOD OR STEEL POSTS (w6X9 OR w6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
- ② USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.
- ③ IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2 1/2 INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AND AMD INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.
- ④ WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).
- ⑤ FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS ± 1". FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 27 3/4" TO 32".
- ⑥ WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.



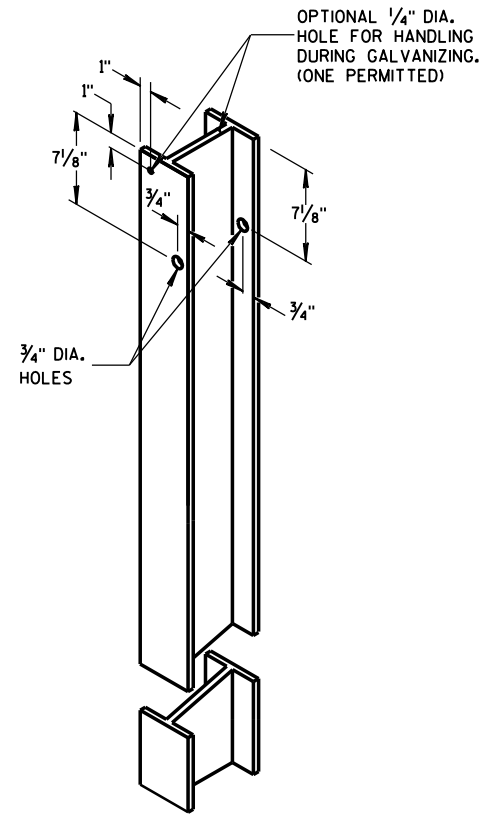
END VIEW
LOCATED ALONG A ROADWAY SHOULDER
STANDARD INSTALLATION



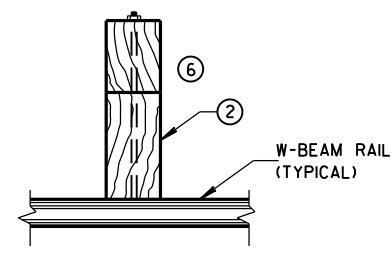
END VIEW
LOCATED ALONG A CURBED ROADWAY



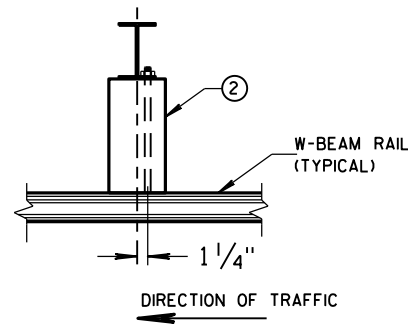
END VIEW
MGS LONGER POST AT HALFPST SPACING W BEAM (K)



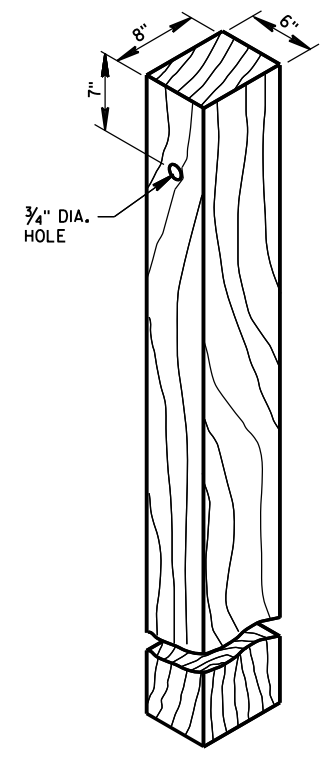
STEEL POST &
HOLE PUNCHING DETAIL
(w6X9) ①



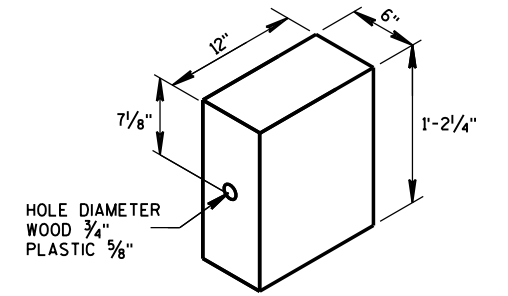
PLAN VIEW
WOOD POST,
BLOCKOUT & BEAM



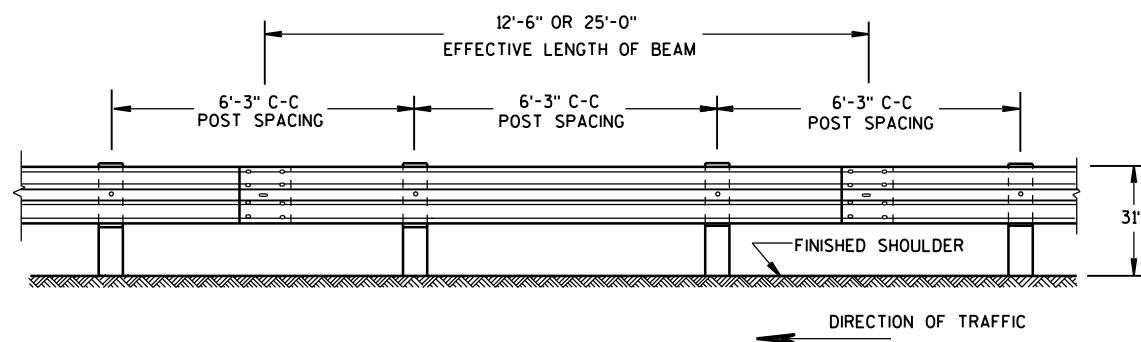
PLAN VIEW
STEEL POST,
PLASTIC BLOCKOUT & BEAM



WOOD POST
(6" X 8") NOMINAL ①

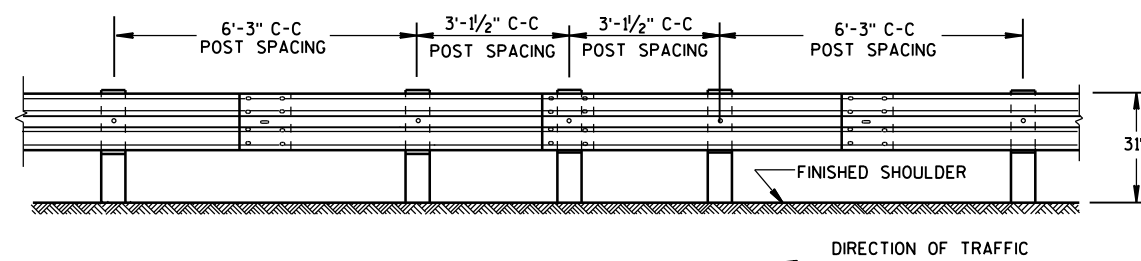


WOOD OR
PLASTIC BLOCKOUT ②



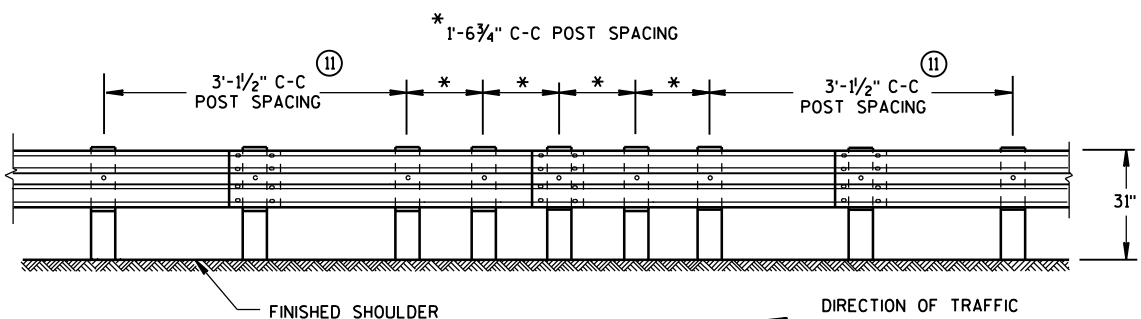
FRONT VIEW

POST SPACING STANDARD INSTALLATION



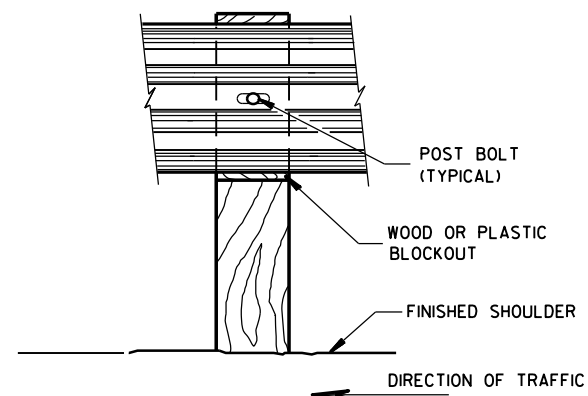
FRONT VIEW

HALF POST SPACING (HS) AND
HALF POST SPACING WITH LONGER POSTS (K)

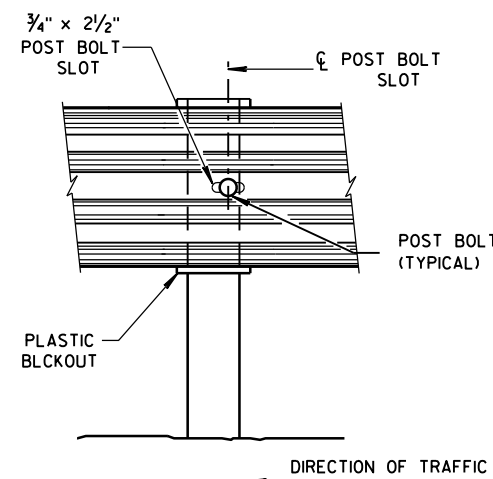


FRONT VIEW

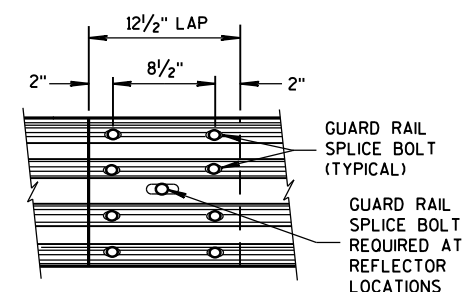
QUARTER POST SPACING (QS)



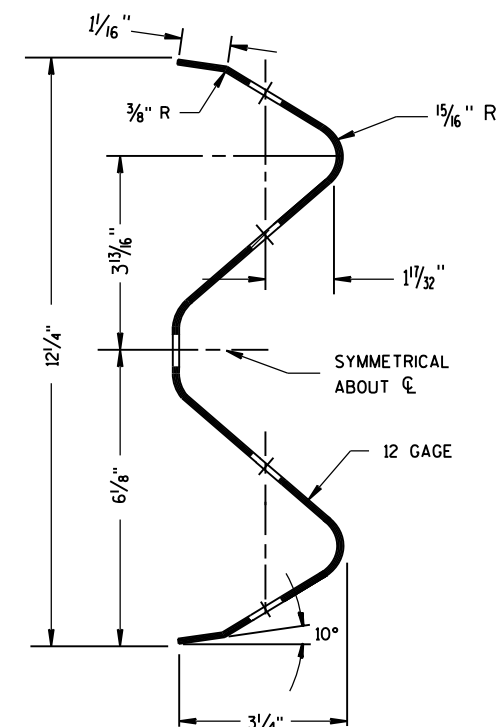
FRONT VIEW AT WOOD POST



FRONT VIEW AT STEEL POST



FRONT VIEW
MID-SPAN BEAM SPLICE



SECTION THRU W-BEAM RAIL

REFLECTOR SPACING ^⑧				
	BEAM GUARD LENGTH	REFLECTOR SPACING	NO. SURFACES REFLECTORIZED	MIN. NO. REFLECTOR
ONE WAY TRAFFIC	< 200' > 200'	50' C-C 100' C-C	1 1	3
TWO WAY TRAFFIC	< 200' > 200'	25' C-C 50' C-C	1 1 ^⑨	6
TWO WAY TRAFFIC	< 200' > 200'	50' C-C 100' C-C	2 2 ^⑩	3

**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

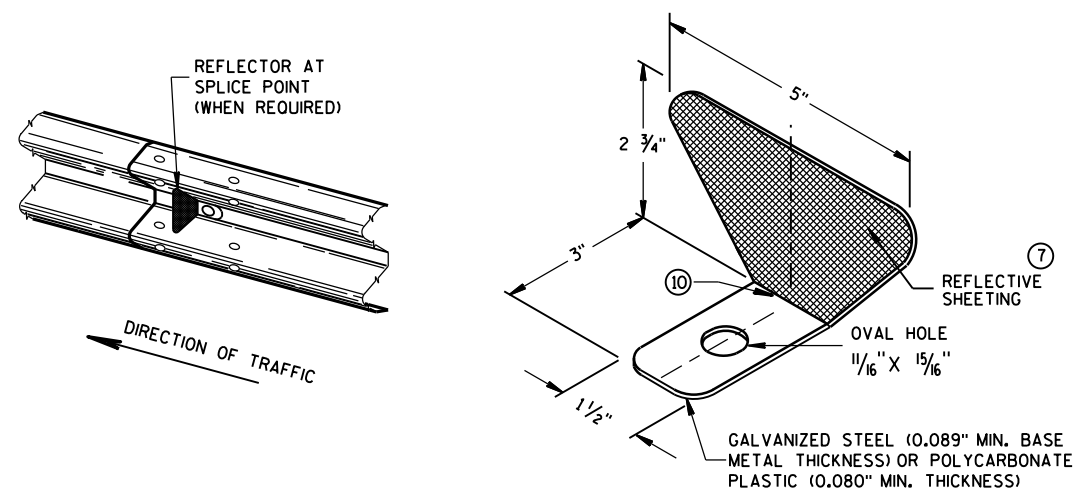
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

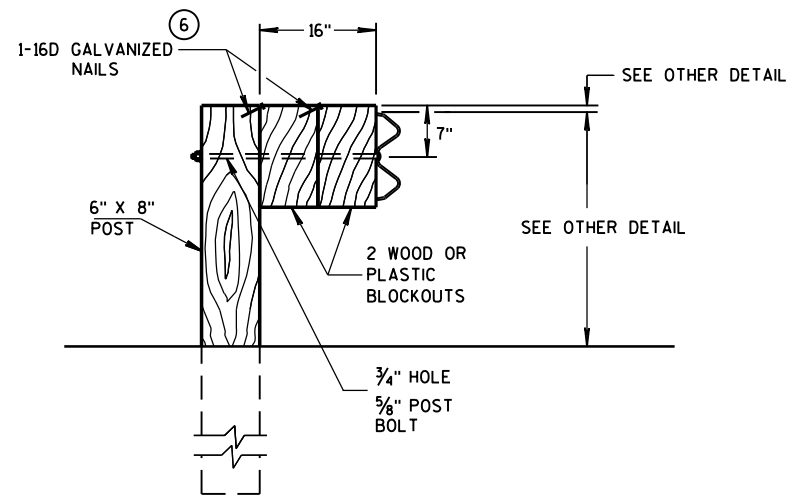
- ⑦ PROVIDE SILVER REFLECTIVE SHEETING ON ALL REFLECTORS EXCEPT THOSE LOCATED ALONG THE LEFT EDGE OF ONE-WAY ROADWAYS, WHICH SHALL BE PROVIDED WITH YELLOW REFLECTIVE SHEETING. SHEETING IS TYPE H. SEE STANDARD SPECIFICATION 637.
- ⑧ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL. RAIL SPLICE LOCATIONS ARE THE ONLY ACCEPTABLE LOCATIONS FOR REFLECTORS.
- ⑨ REVERSE EVERY OTHER REFLECTOR FOR 2-WAY VISIBILITY. THE CONTRACTOR MAY FURNISH TWO-SIDED REFLECTORS IN LIEU OF ONE-SIDED REFLECTORS.
- ⑩ PROVIDE AN ANGLE OF BEND OF $90^\circ \pm 1^\circ$ FOR TWO-SIDED REFLECTORS.
- ⑪ 25 FEET OF HALF POST SPACING IS REQUIRED ON APPROACH AND DEPARTURE ENDS OF QUARTER POST SPACING.

POST BOLTS ARE A 3/8" DIAMETER ASTM A307 GUARDRAIL BOLT. A POST BOLT REQUIRES 3/8" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT AND 3/8" DIAMETER F844 FLAT WASHER. POST BOLTS MAY BE LONGER IF MULTIPLE BLOCKOUTS ARE BEING USED.

GUARD RAIL SPLICE BOLTS ARE A 5/8" DIAMETER ASTM A307 GUARDRAIL
HEAD BOLT. A GUARDRAIL SPLICE BOLT REQUIRES 5/8" DIAMETER A563A DOUBLE
RECESSED (DR) HEAVY HEX NUT.

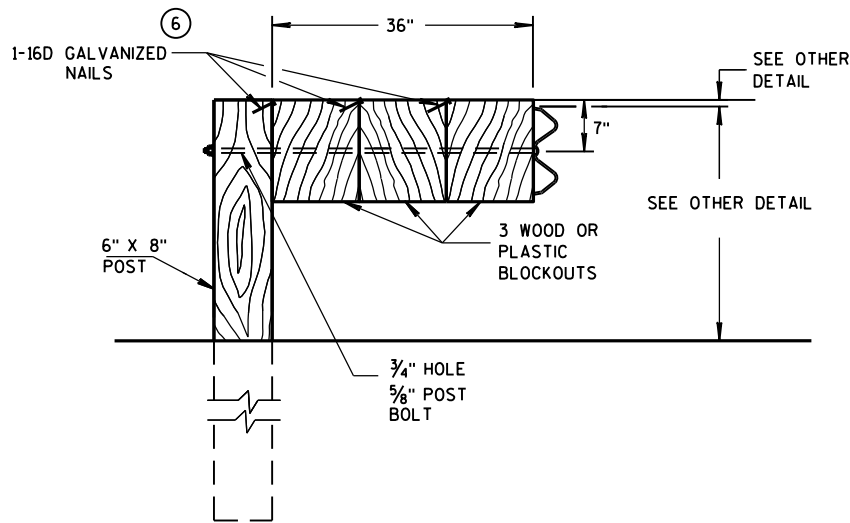


ONE SIDED REFLECTOR DETAIL AND TYPICAL INSTALLATION



DETAIL FOR 16" BLOCKOUT DEPTH

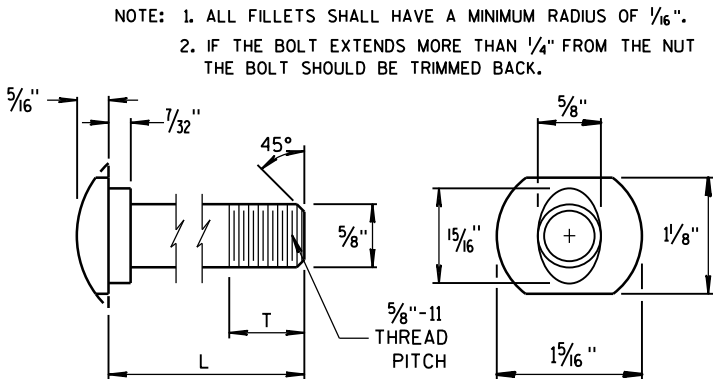
IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.



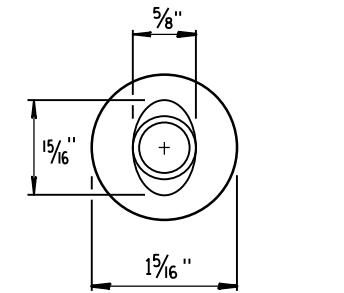
DETAIL FOR 36" BLOCKOUT DEPTH

NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.

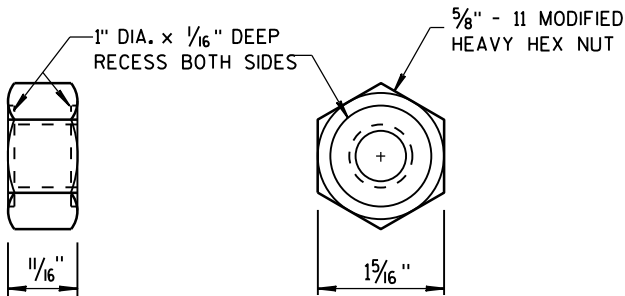
DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.



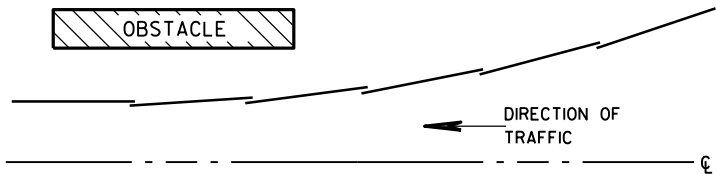
POST BOLT TABLE



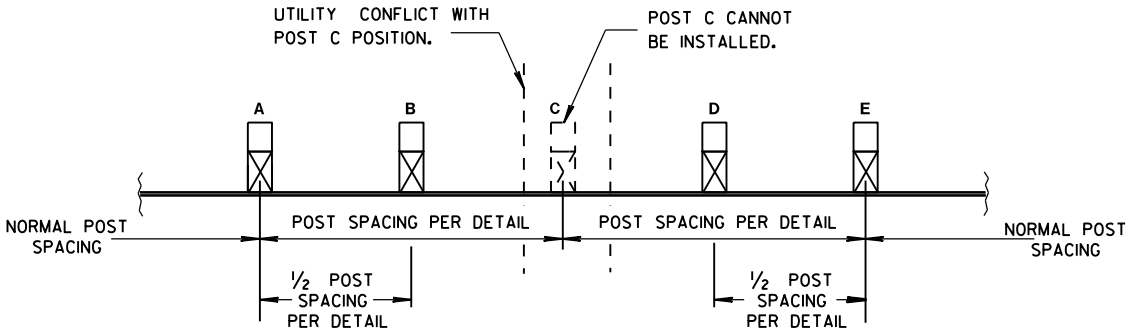
ALTERNATE BOLT HEAD



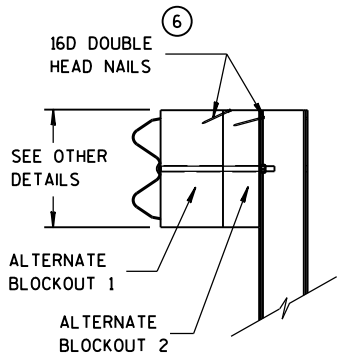
POST BOLT AND RECESS NUT



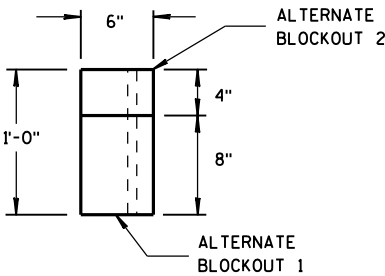
PLAN VIEW
BEAM LAPPING DETAIL



POST DRIVING FOR CONTINUOUS
UNDERGROUND OBSTRUCTION



SIDE VIEW



TOP VIEW

ALTERNATE WOOD
BLOCKOUT DETAIL

MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2014
DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

GENERAL NOTES

- (A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE (HPL), AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.
- (B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED.
- (C) DIFFERENT MANUFACTURES REQUIRE DIFFERENT PERFORATED W-BEAM RAIL END PANELS. SEE MANUFACTURES INFORMATION.
- (D) THE TOP OF THE STEEL TUBE ON POST 1 AND POST 2 SHALL NOT BE MORE THAN 3" ABOVE THE FINISH GROUND ELEVATION.
- (E) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF-TAPPING SCREWS, ONE SCREW PER CORNER.
- (G) 1/2" DIAMETER X 3" LONG LAG BOLT AND WASHER.
- (H) HARDWARE VARIES BETWEEN DIFFERENT MANUFACTURES. SEE MANUFACTURE'S DRAWING FOR INFORMATION.
- (I) DIMENSIONS MAY VARY. SEE MANUFACTURE'S INFORMATION.

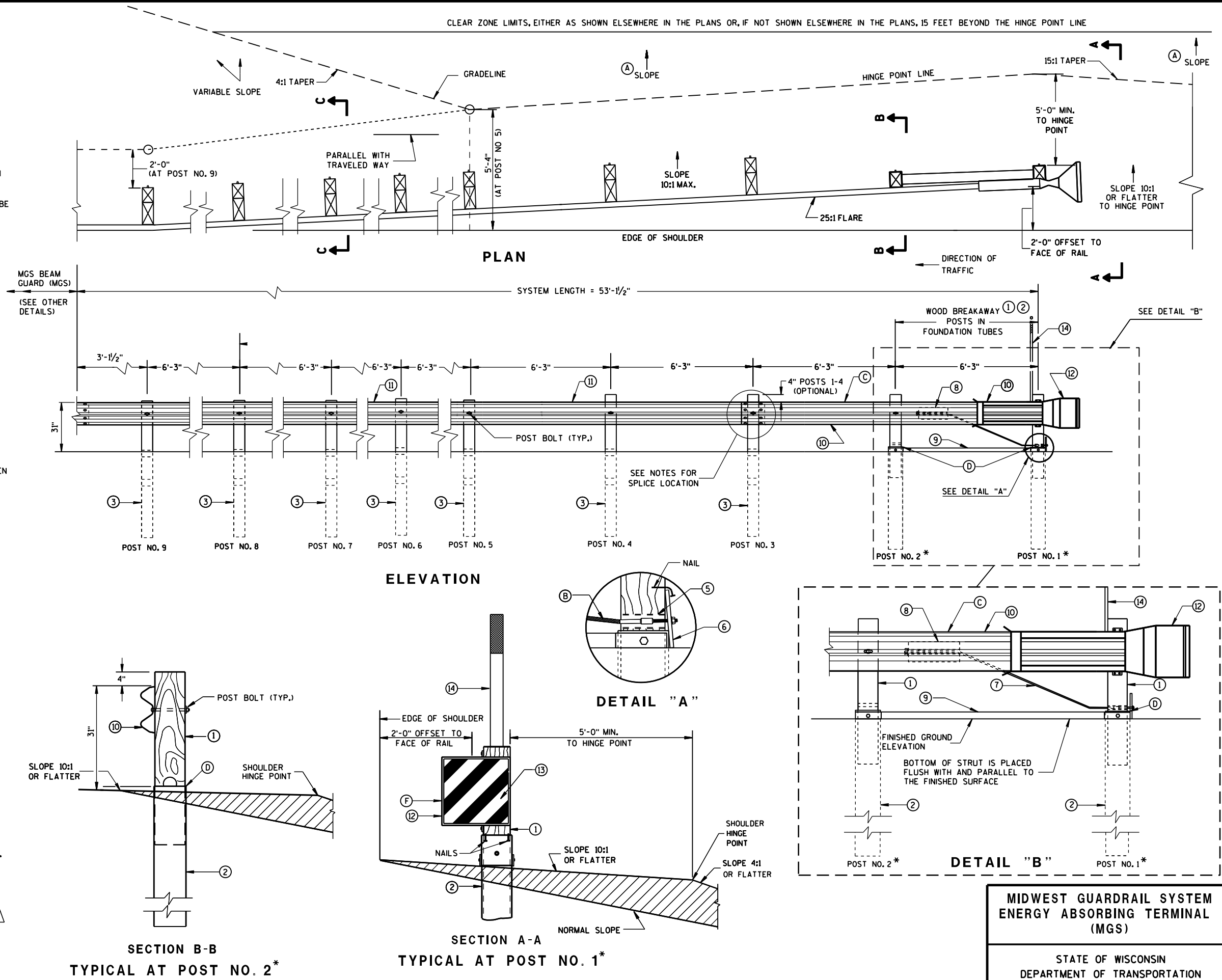
SEE SDD 14B42 FOR MORE INFORMATION.

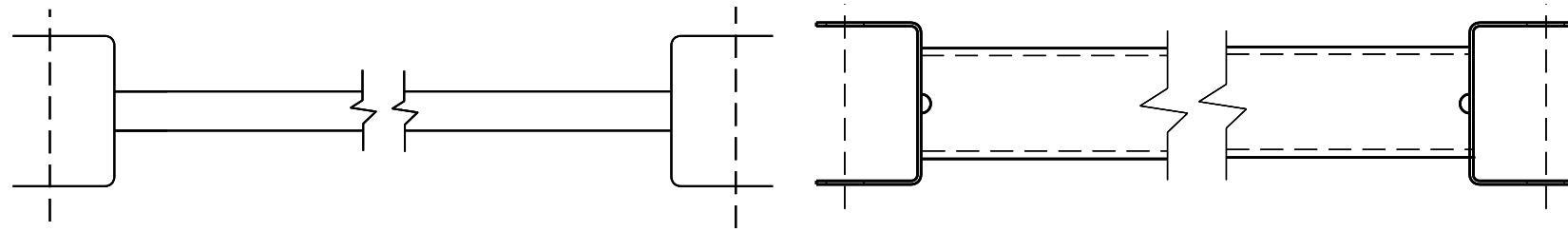
* DO NOT ATTACH BLOCKOUTS TO POSTS 1 AND 2.

DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

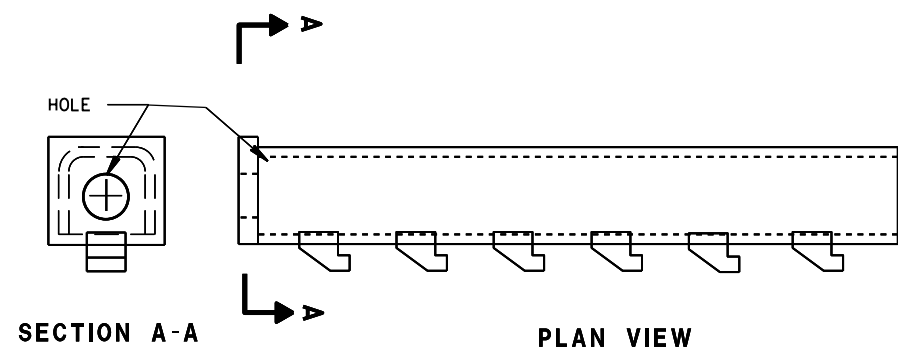
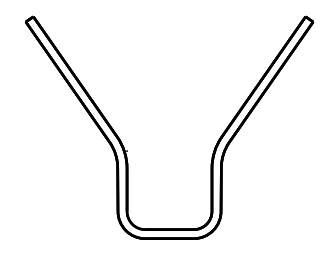
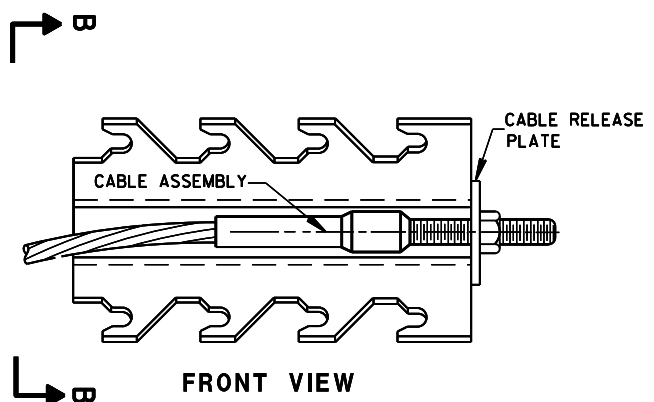
W-BEAM RAIL SPLICES ARE LOCATED AT POST NUMBER 3, AND BETWEEN POST 5 AND 6, BETWEEN POSTS 7 AND 8, AND MIDDLE OF THE SPAN AFTER POST 9.

THE CENTER OF THE UPPER 3/2" DIAMETER HOLE ON POST NUMBER 3 THROUGH POST 9 IS TO BE FLUSH WITH THE GROUND LINE UP TO A MAXIMUM OF 2" ABOVE GROUND LINE.





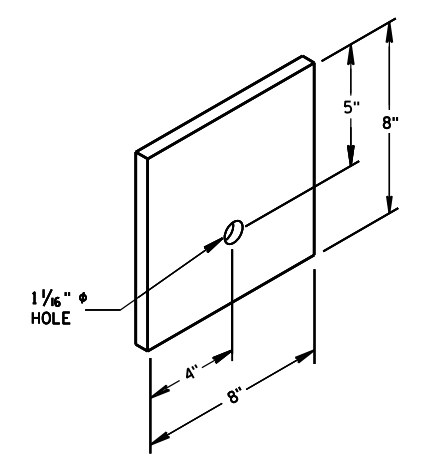
9 H
GENERIC GROUND STRUT



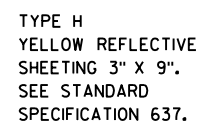
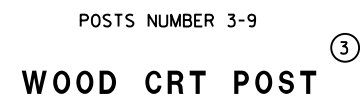
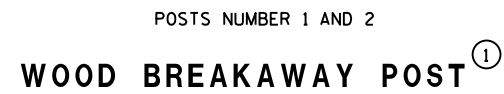
8 H
GENERIC ANCHOR CABLE BOX

BILL OF MATERIALS

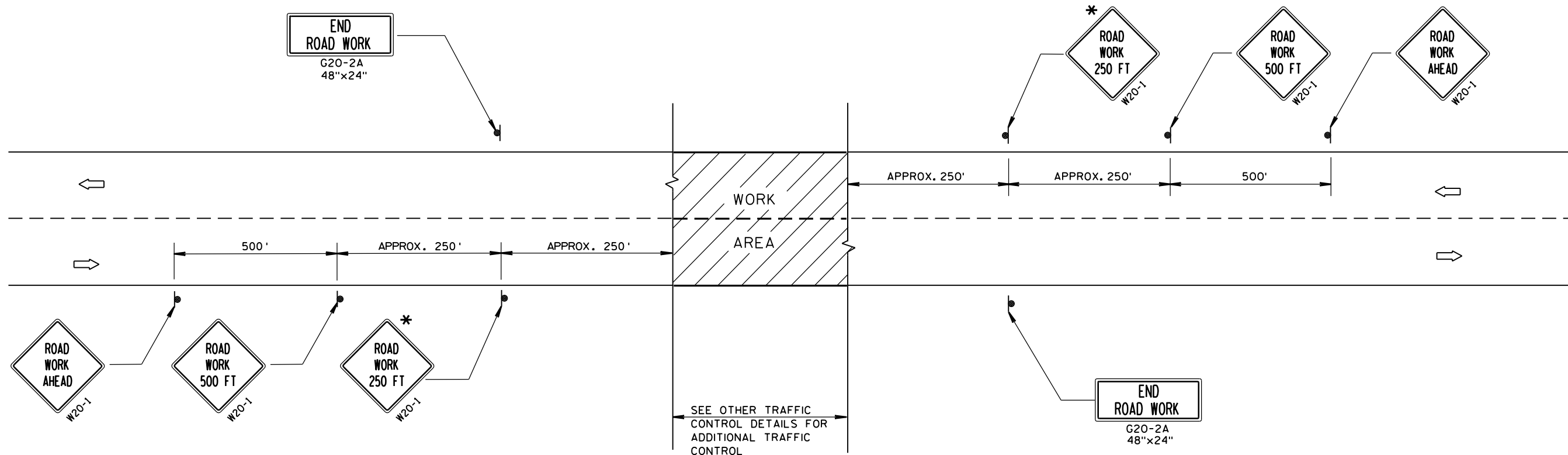
PART NO.	DESCRIPTION
MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUFACTURER'S DETAILS FOR MORE INFORMATION.	
①	WOOD BREAKAWAY POST
②	6" X 8" X 0.188", 6'-0" LONG FOUNDATION TUBE AT POSTS 1 AND 2
③	WOOD CRT
④	WOOD BLOCKOUT
⑤	PIPE SLEEVE
⑥	BEARING PLATE
⑦	BCT CABLE ASSEMBLY
⑧	ANCHOR CABLE BOX
⑨	GROUND STRUT
⑩	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.
⑪	STANDARD W-BEAM RAIL. MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.
⑫	END SECTION EAT
⑬	0.040" ALUMINUM SHEET WITH REFLECTIVE SHEETING TYPE F PER SECTION 637 OF THE STANDARD SPECIFICATIONS
⑭	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)



⑥
BEARING PLATE



MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2014	/S/ Jerry H. Zogg
DATE	ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

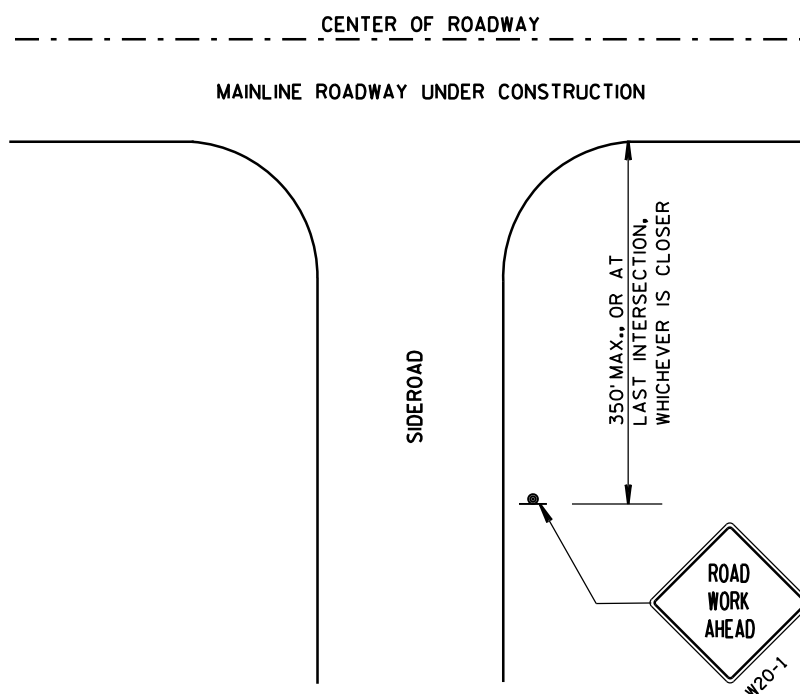
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



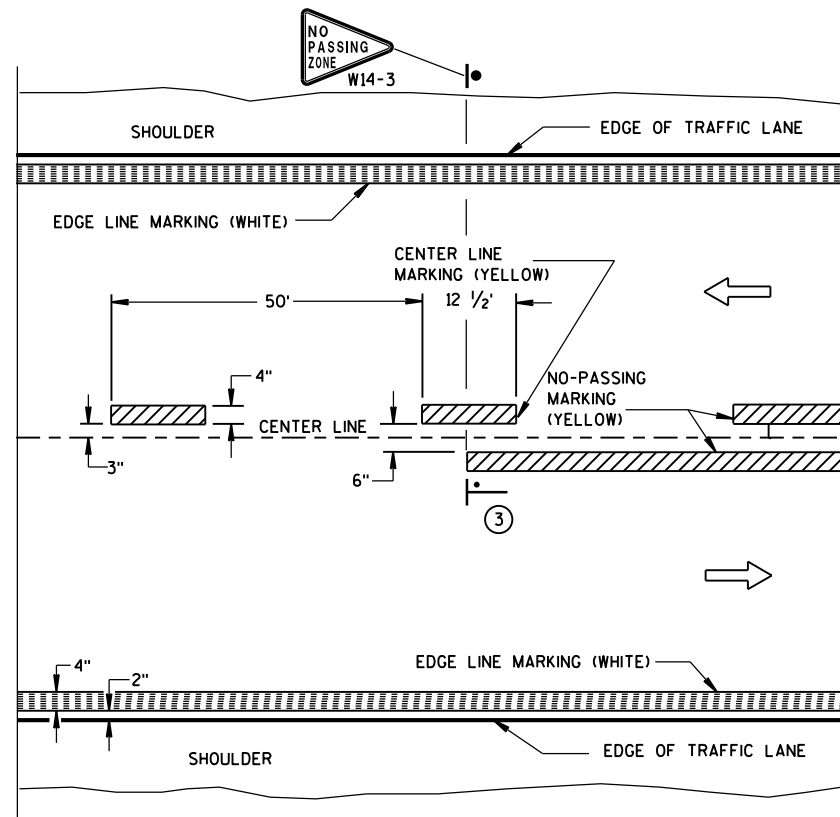
LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

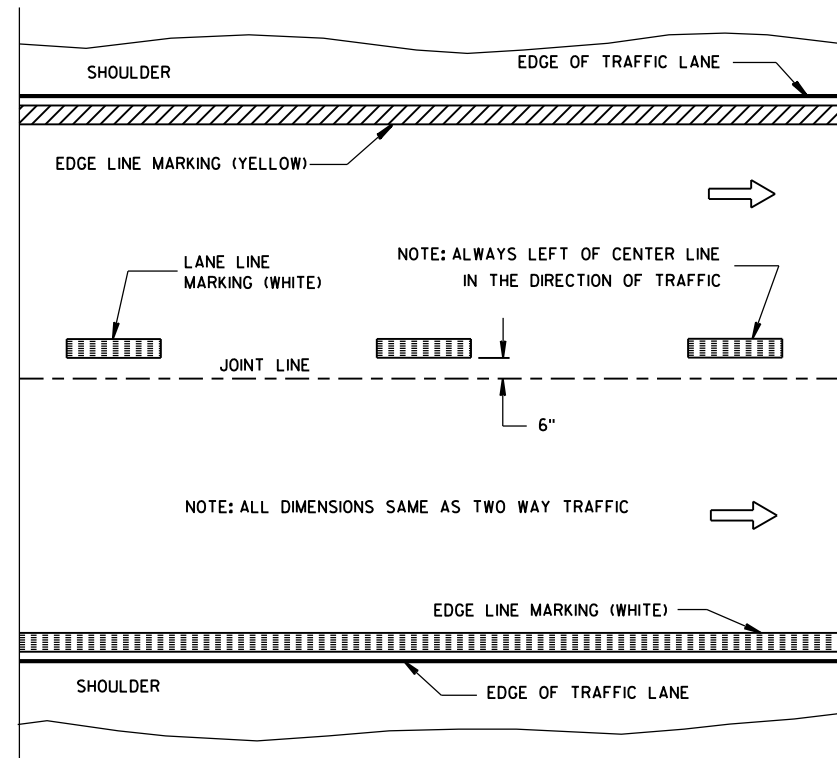
TRAFFIC CONTROL, ADVANCE
WARNING SIGNS 40 M.P.H.
OR LESS TWO-WAY UNDIVIDED
ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

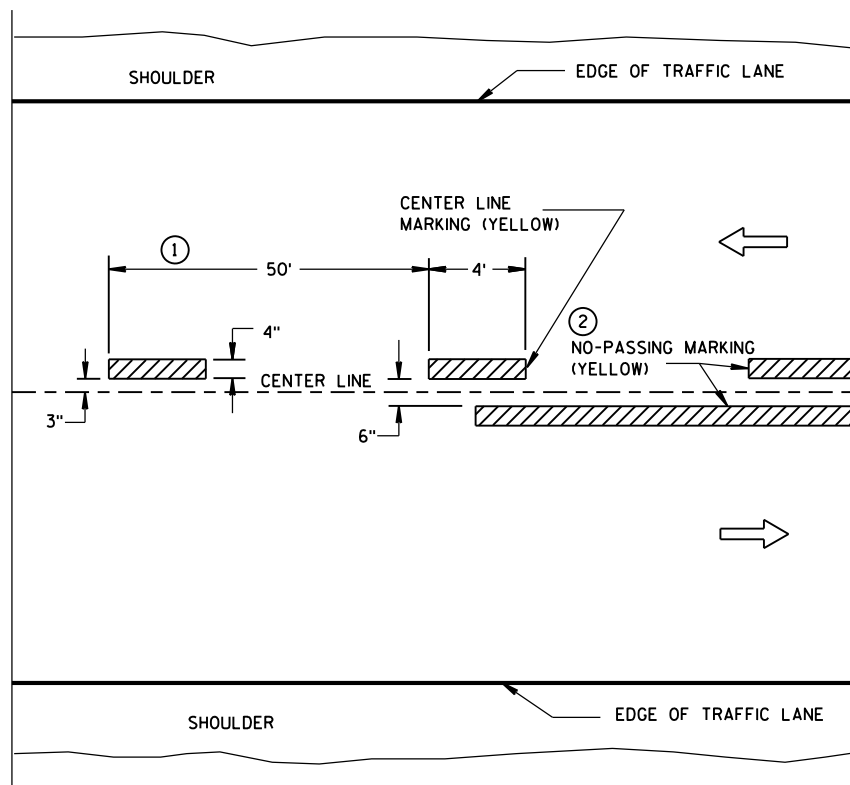


TWO WAY TRAFFIC

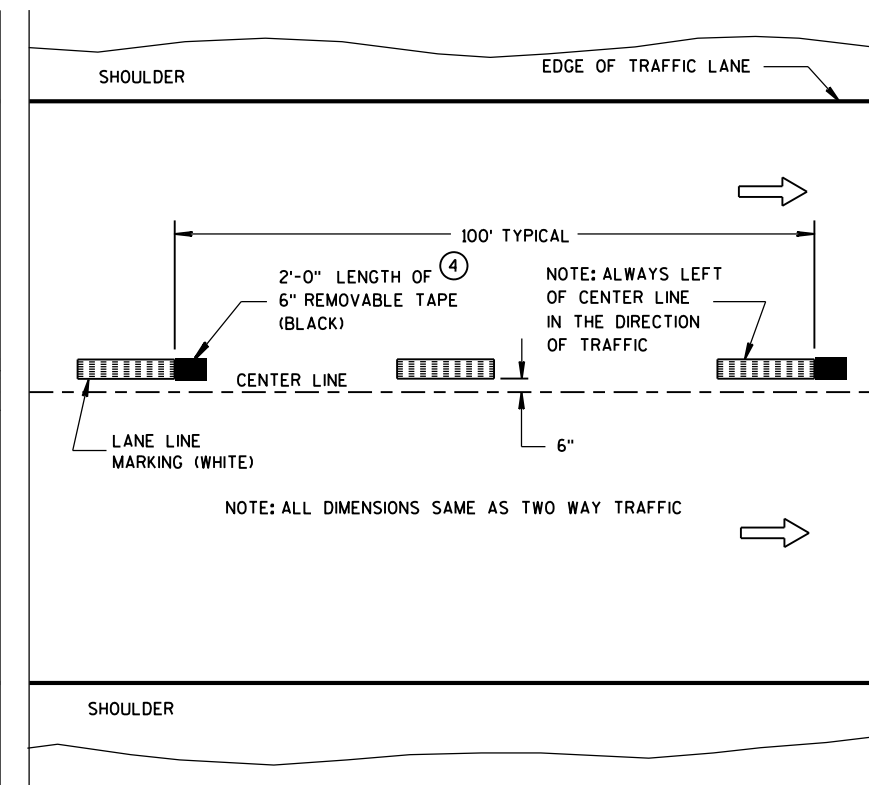


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING
(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

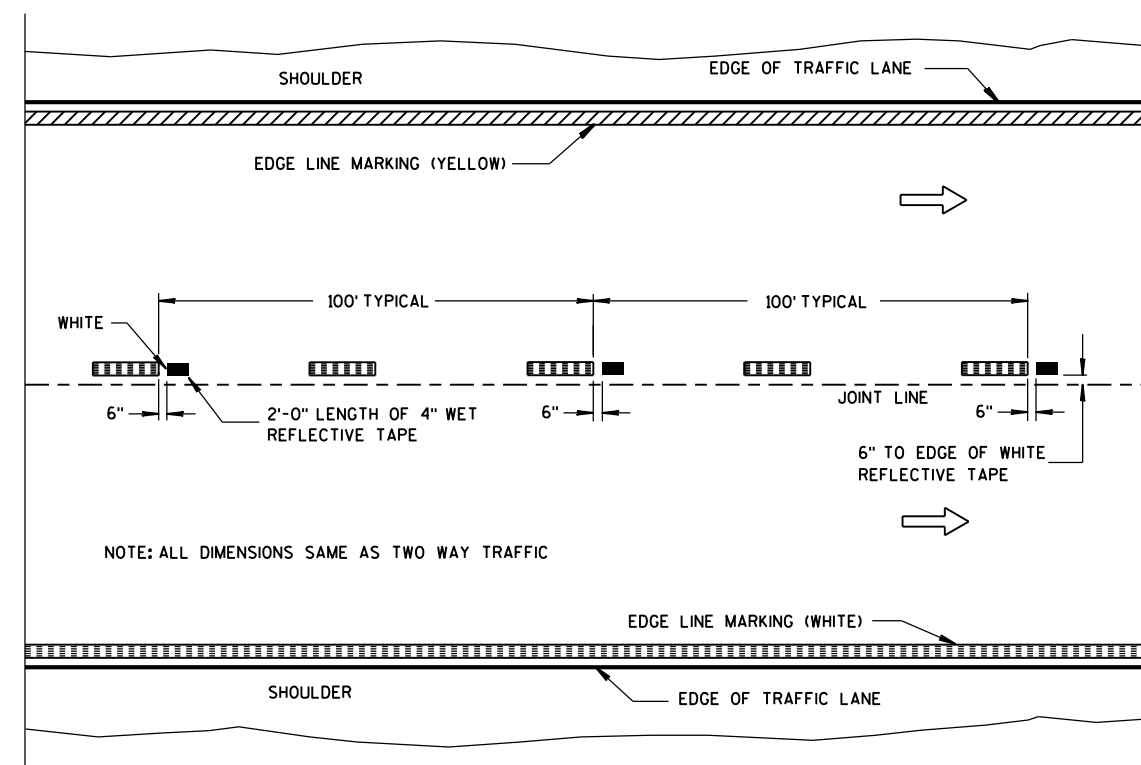
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- ② NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- ③ NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
- ④ CONCRETE ONLY.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL



**WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE**

LEGEND

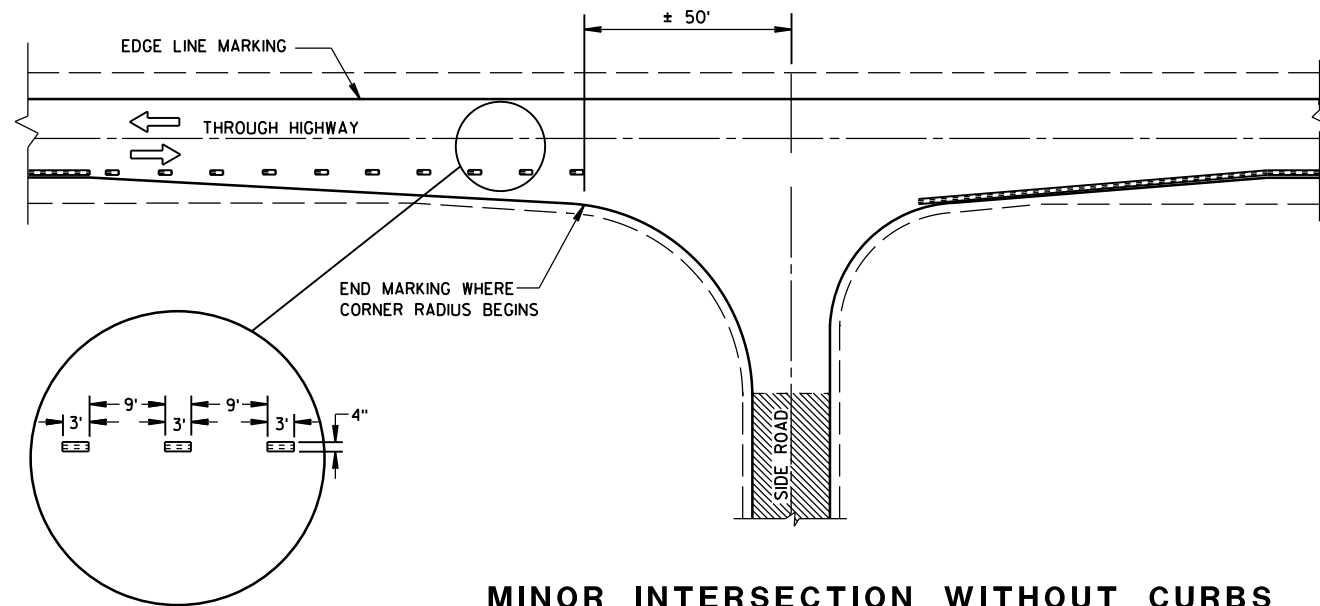
- "T" MARKING
- POST MOUNTED SIGN

PAVEMENT MARKING
(MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5-13-2013
DATE
FHWA

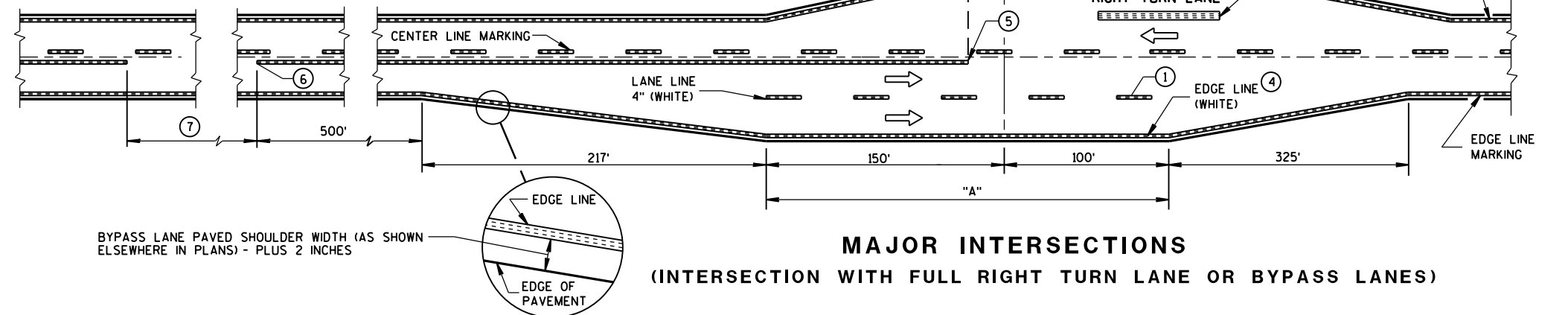
/S/ Travis Feltes
STATE TRAFFIC ENGINEER



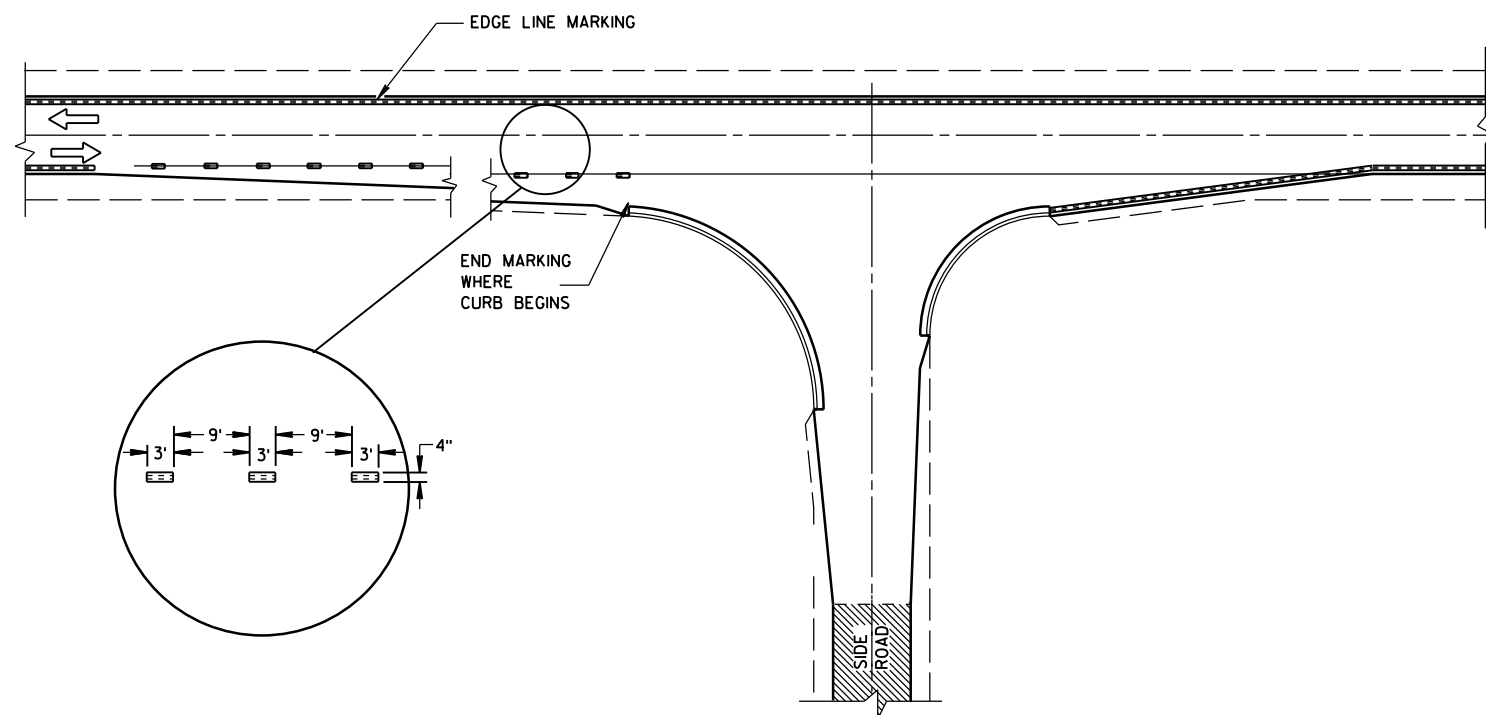
MINOR INTERSECTION WITHOUT CURBS

⑦

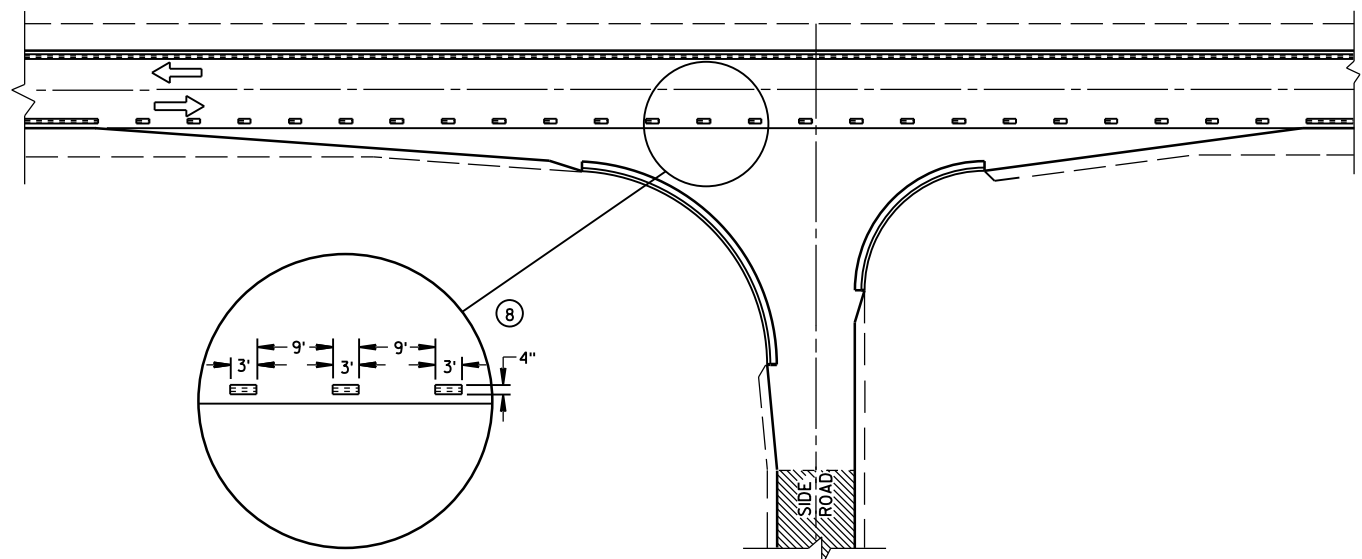
POSTED SPEED (MPH)	MINIMUM DISTANCE BETWEEN ZONES (FEET)
25 - 30	528
35 - 40	528
45 - 50	686
55	792



MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)



MINOR INTERSECTION WITH CURBS
(TYPICAL MARKING)



MINOR INTERSECTION WITH CURBS
③ (FOR SPECIAL CONDITIONS AS SPECIFIED)





GENERAL NOTES

- EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
 - ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
 - ③ ALTERNATIVE MARKING SHALL BE PROVIDED WHEN SPECIFIED IN THE CONTRACT. TYPICAL SITUATIONS WHERE THIS MARKING MAY BE REQUIRED ARE WHERE THE INTERSECTION IS ON A SHARP HORIZONTAL CURVE OR CREST VERTICAL CURVE IN AN UNLIGHTED AREA SUCH THAT THE EDGE LINE MAY BE MISLEADING TO THE MOTORIST OR DISAPPEAR FROM SIGHT.
 - ④ THE EDGE LINE IN THE TAPER AREAS OF THE BYPASS LANE AND THE BYPASS LANE SHALL BE LOCATED 1-FOOT FROM EDGE OF PAVEMENT TO THE OUTSIDE EDGE OF EDGE LINE.
 - ⑤ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.
 - ⑥ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
 - ⑦ IF THE DISTANCE BETWEEN 2 SUCCESSIVE NO-PASSING ZONES IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES, CONNECT THE 2 ZONES.
 - ⑧ 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

PAVEMENT MARKING
(INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

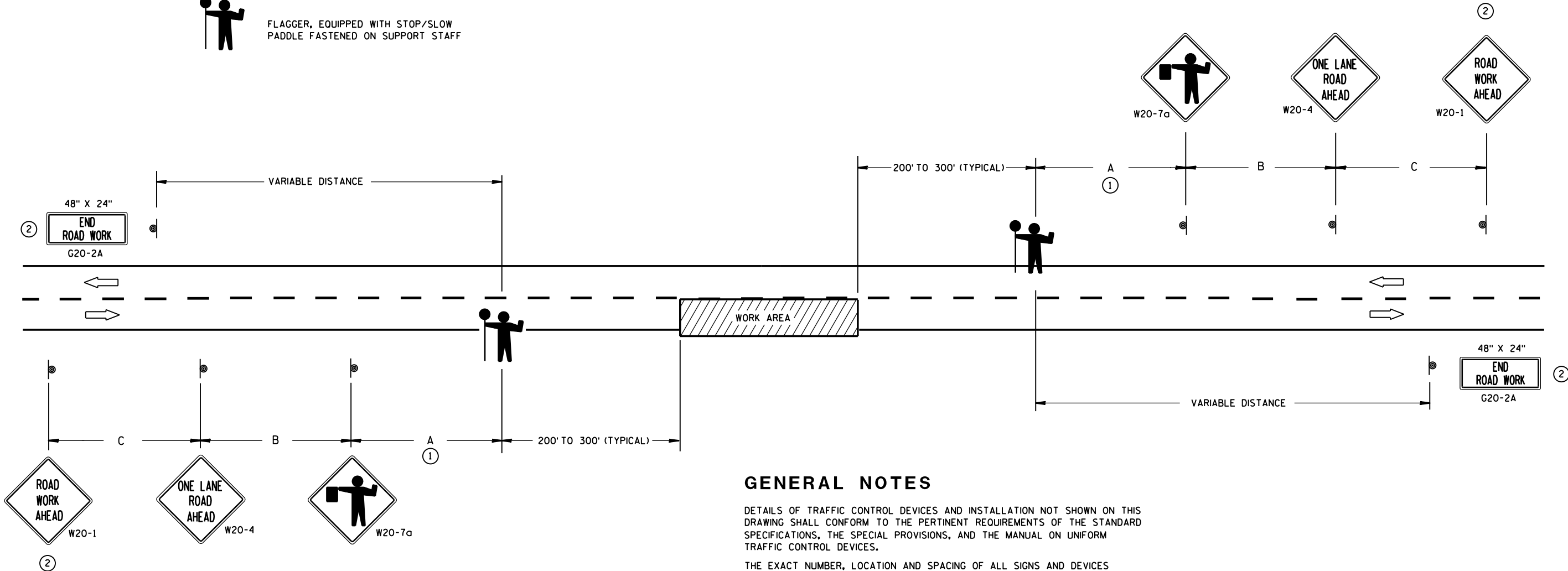
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

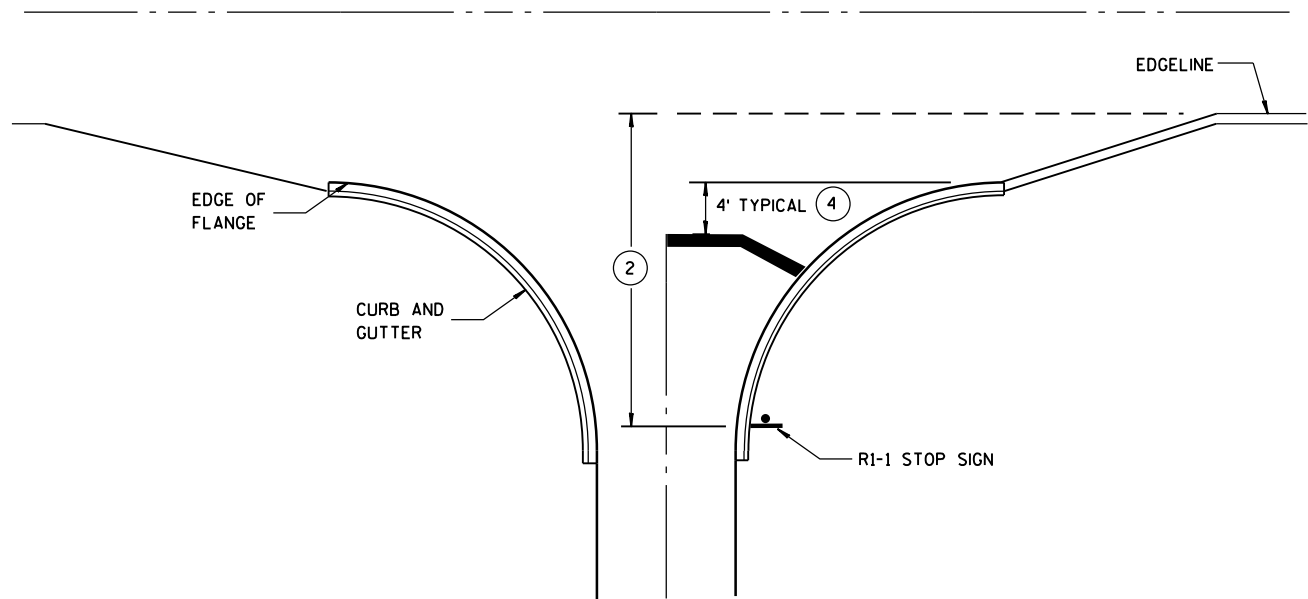
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

- ① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

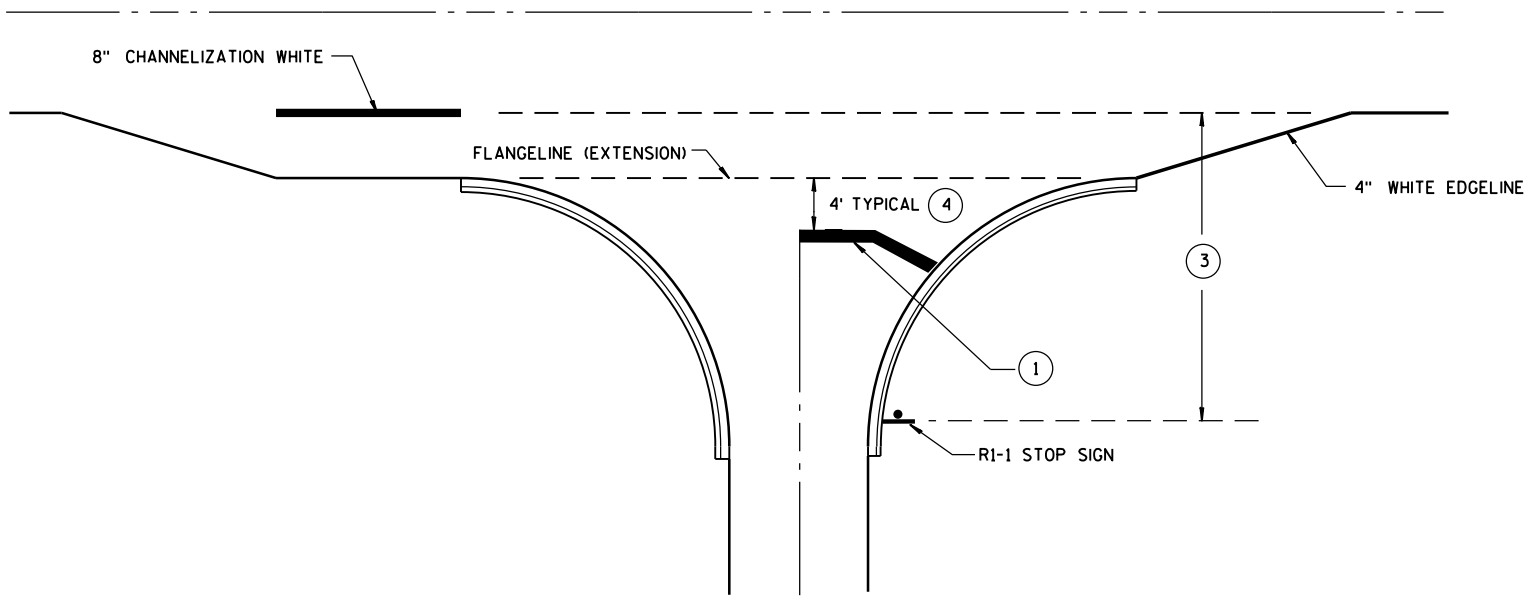
TRAFFIC CONTROL FOR LANE
CLOSURE (SUITABLE FOR
MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

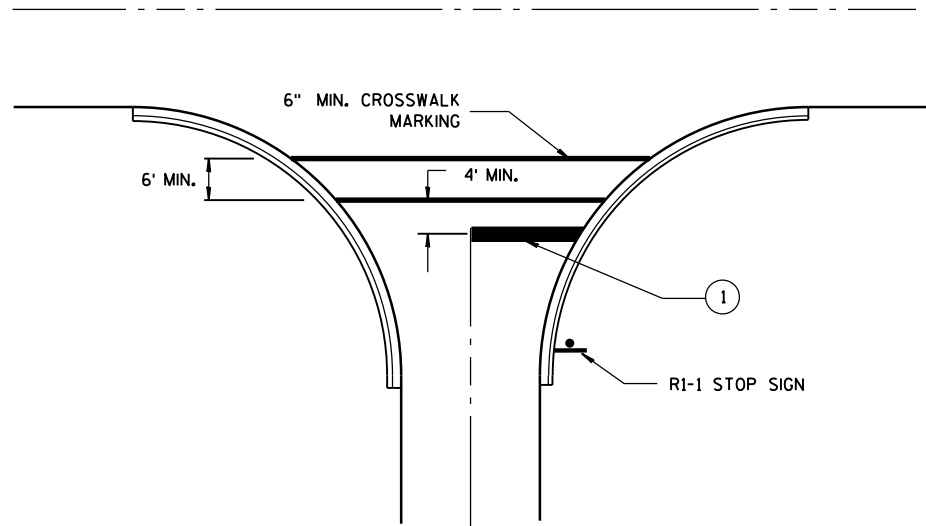
APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



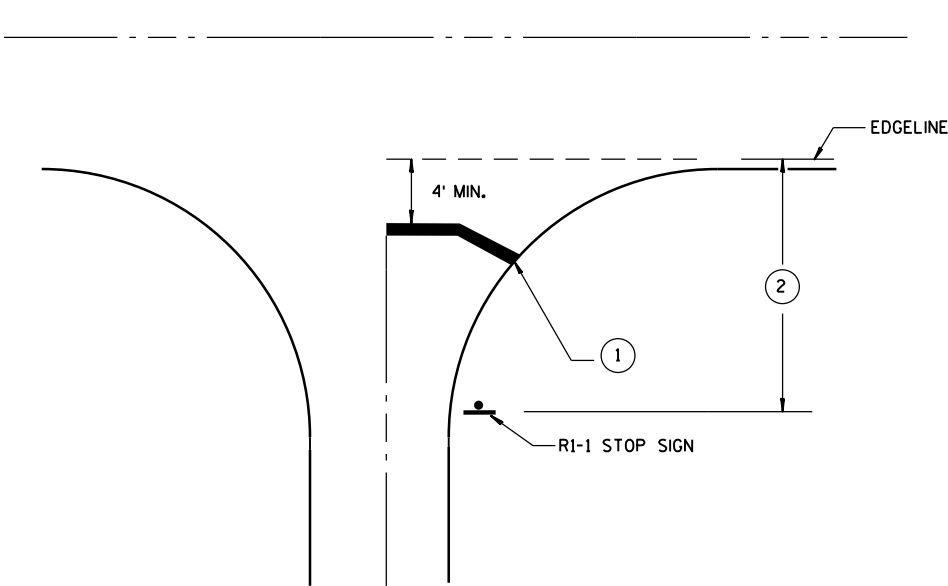
TYPICAL STOP LINE PAVEMENT MARKING
WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING
WITHOUT CURB AND GUTTER

GENERAL NOTES

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGE LINE THAN NO STOP LINE IS REQUIRED.
- ③ IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- ④ MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES.

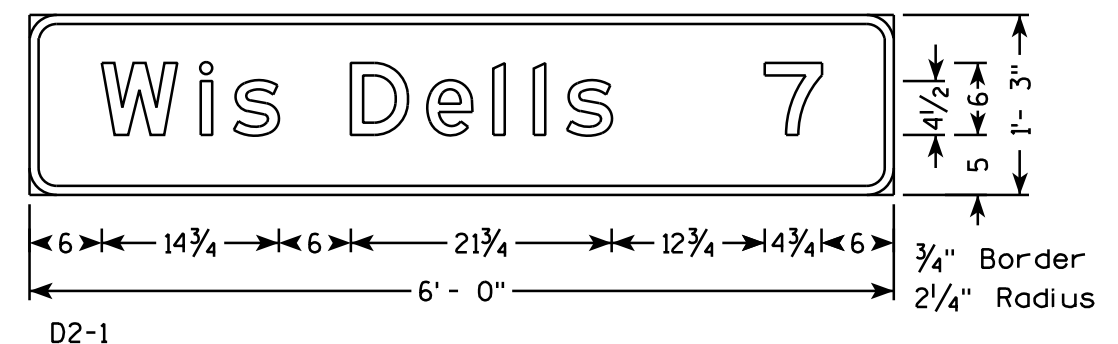
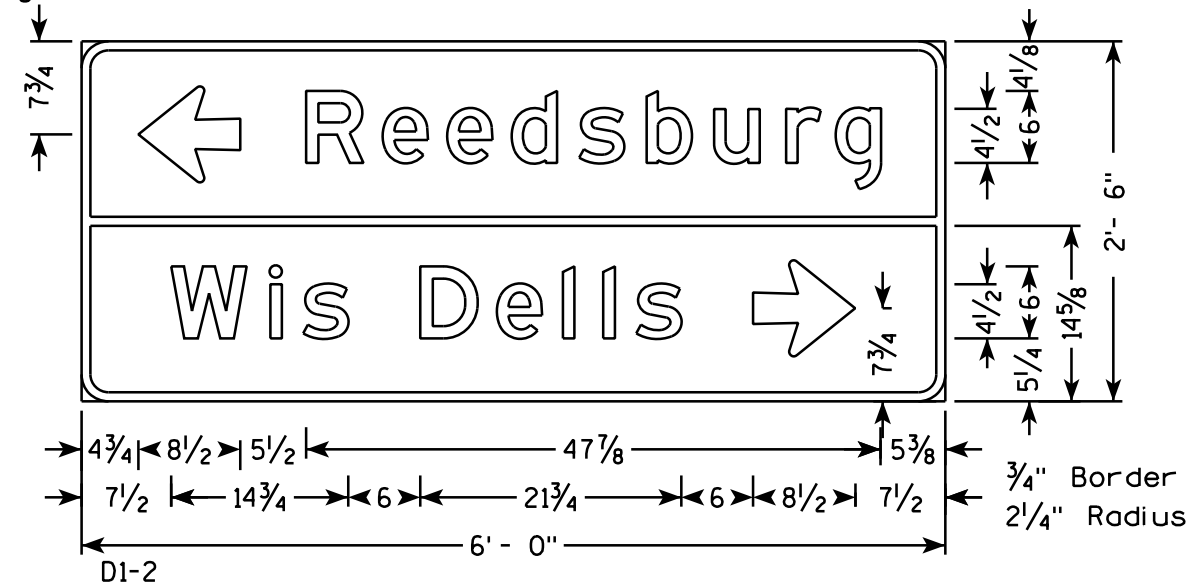
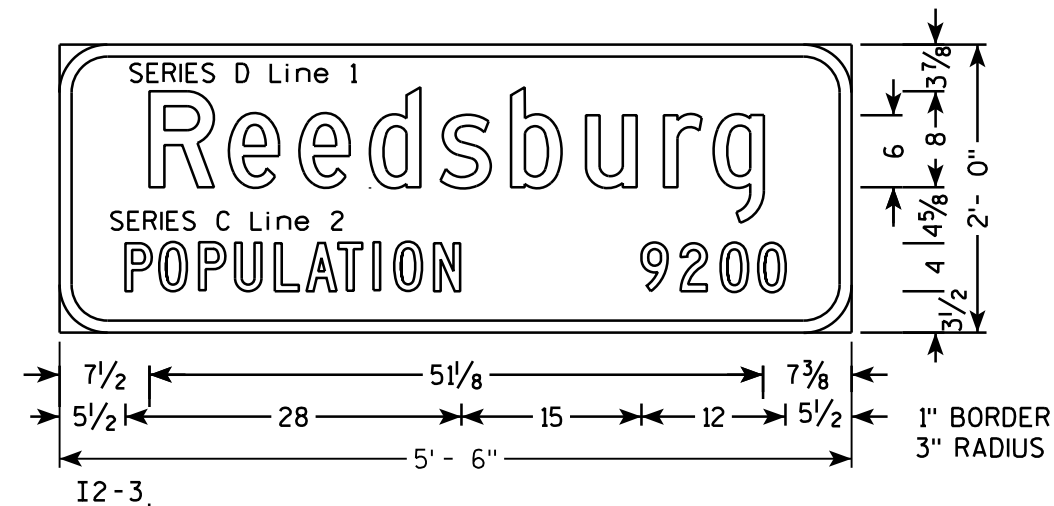
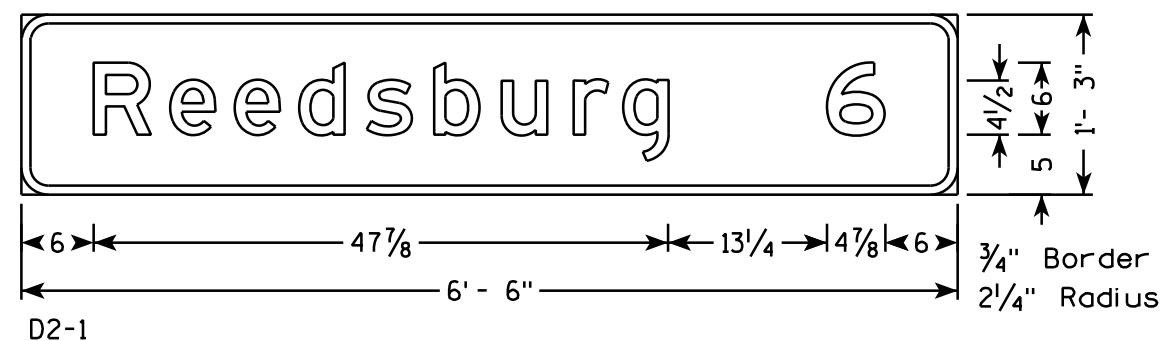
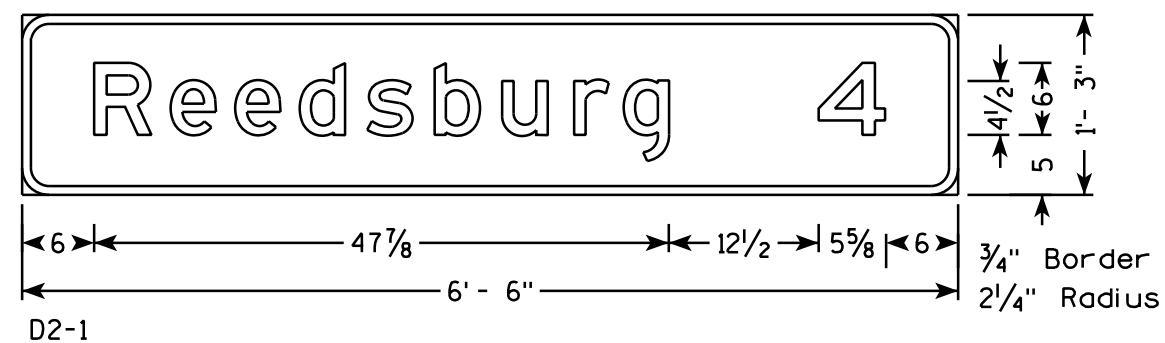
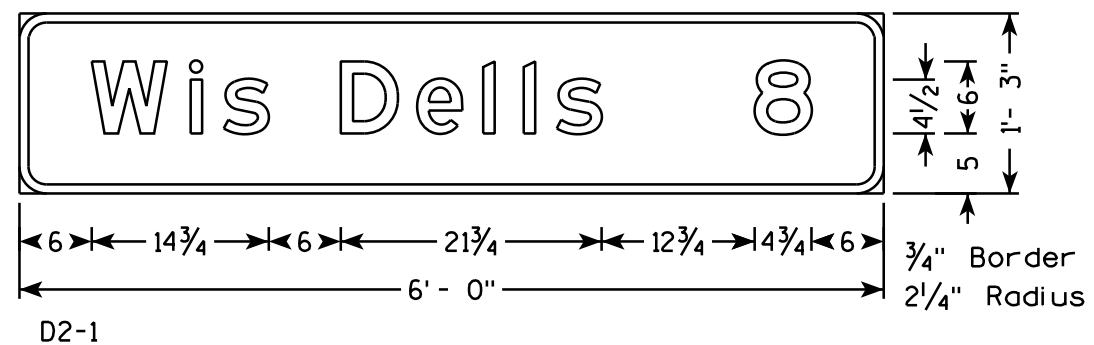
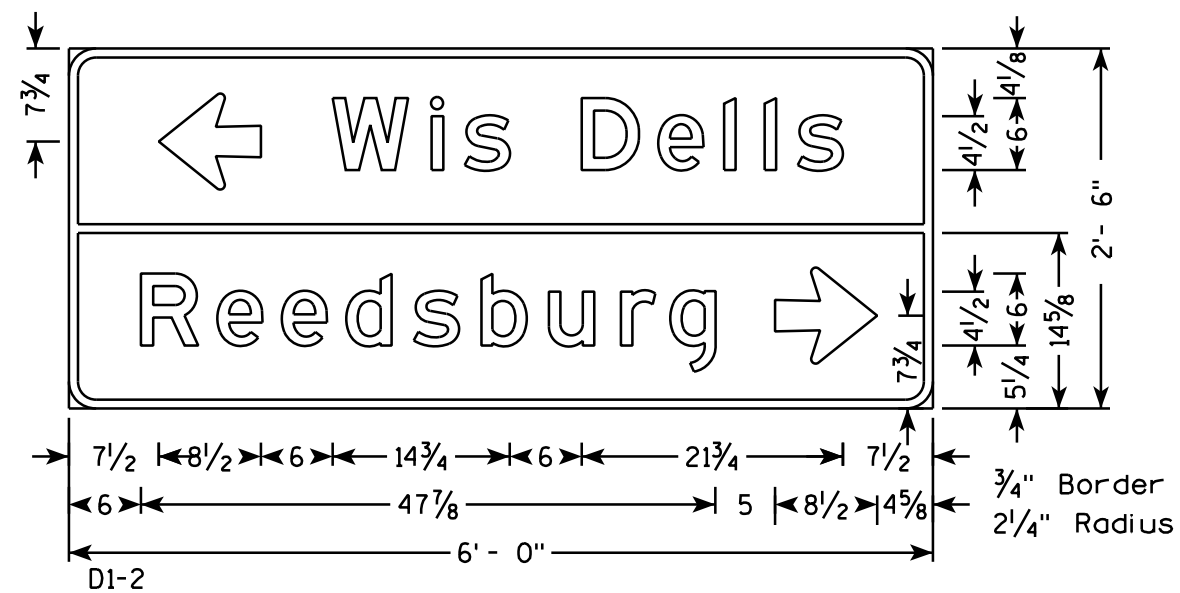
STOP LINE AND CROSSWALK
PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4/30/2013 DATE /S/ Travis Feltz
STATE TRAFFIC ENGINEER
FHWA

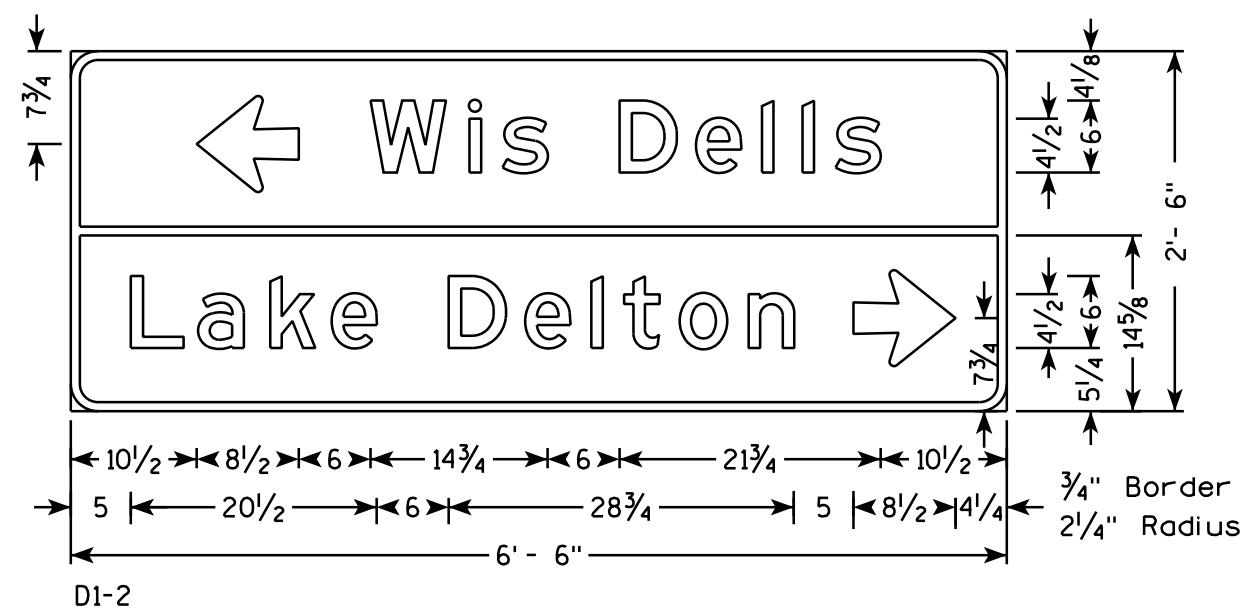
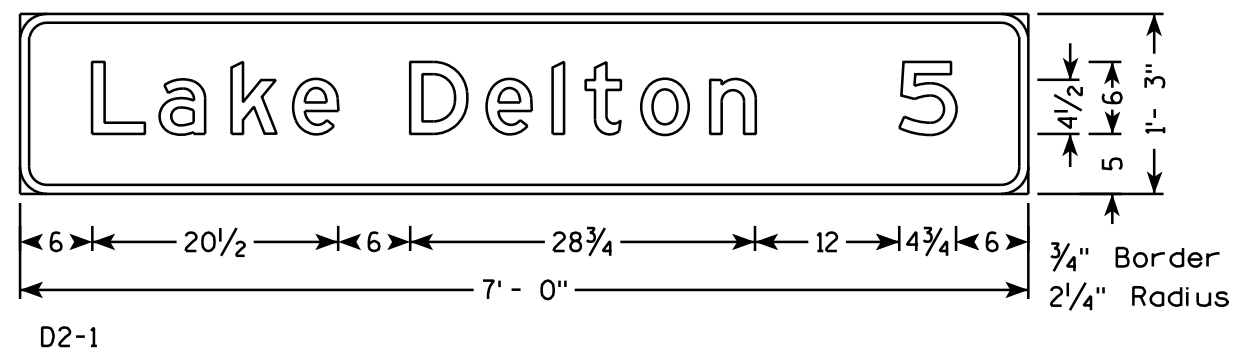
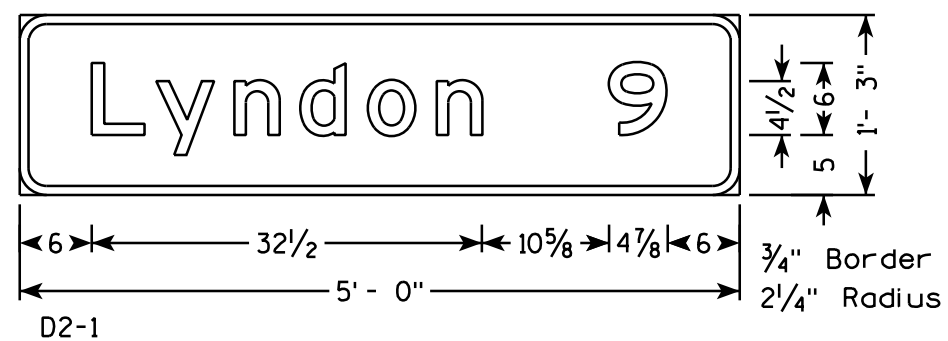
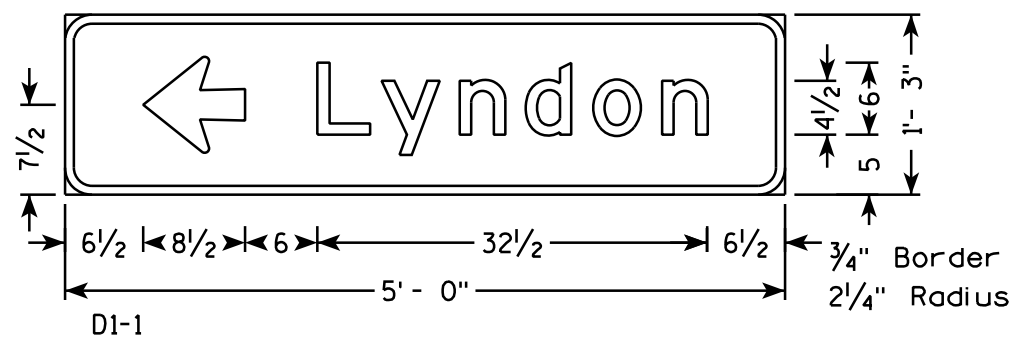
NOTES

1. All Signs Type II - Type H Reflective
2. Color:
Background - GREEN
Message - WHITE
3. Message Series - E except as Shown

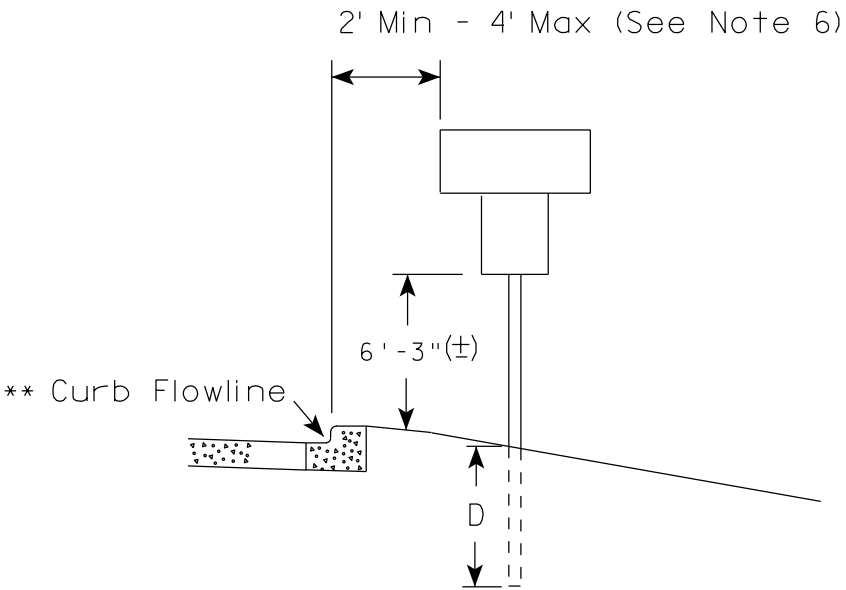
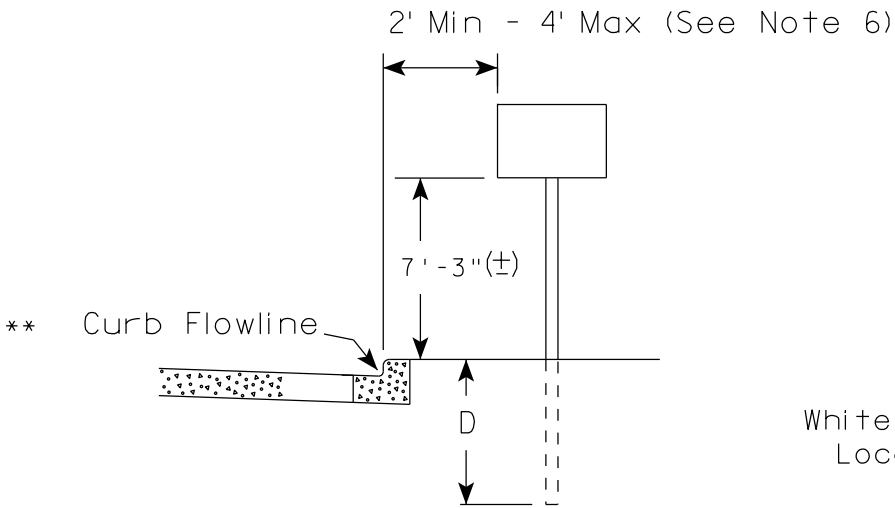


NOTES

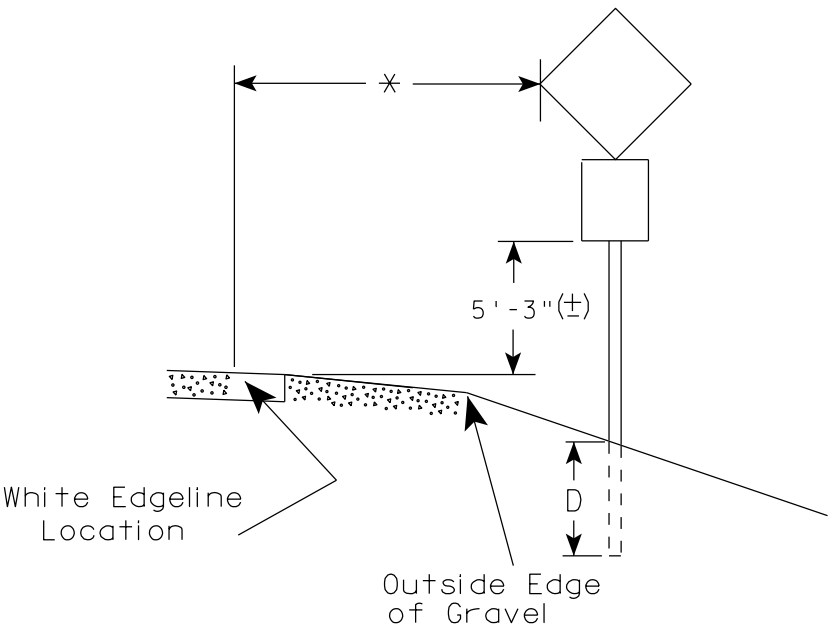
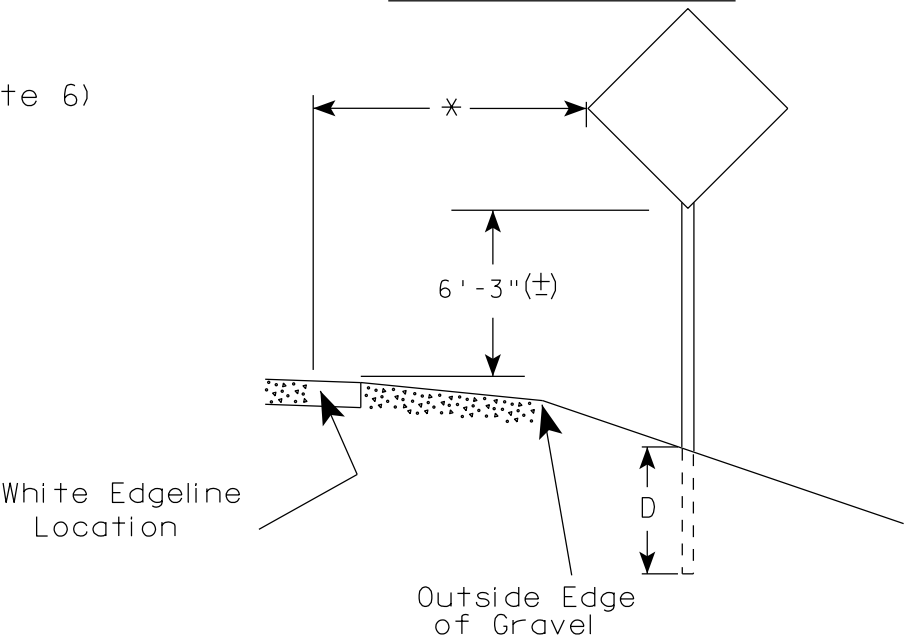
1. All Signs Type II - Type H Reflective
2. Color:
Background - GREEN
Message - WHITE
3. Message Series - E



URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet, 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'-3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series) & End of Rod Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (±).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

×× The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

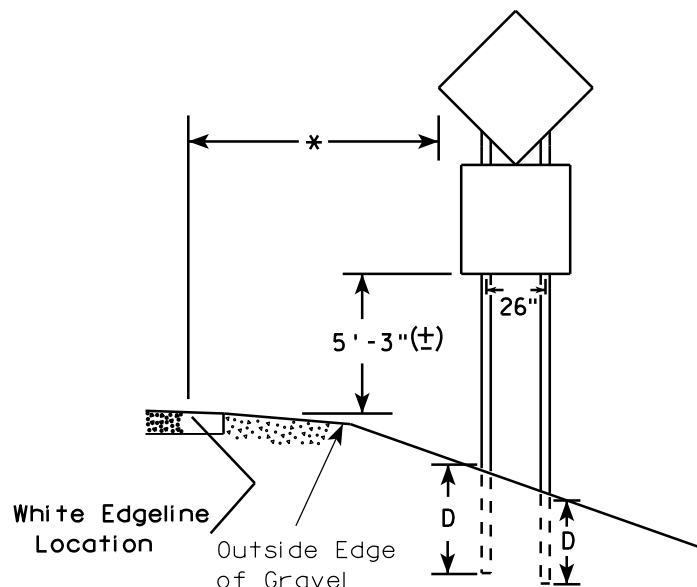
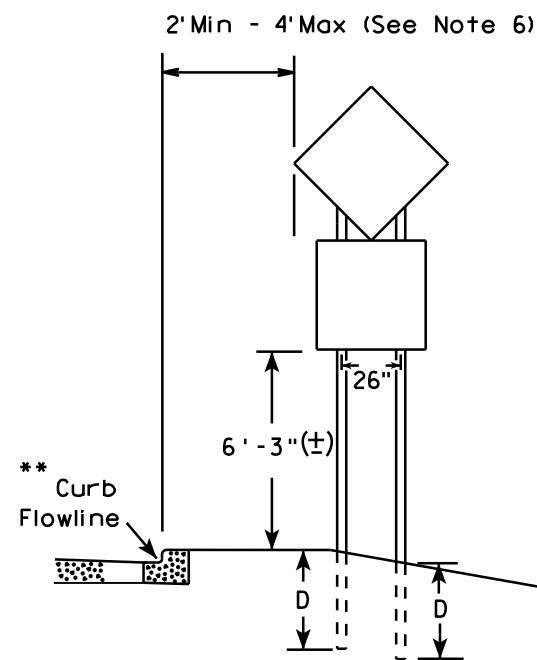
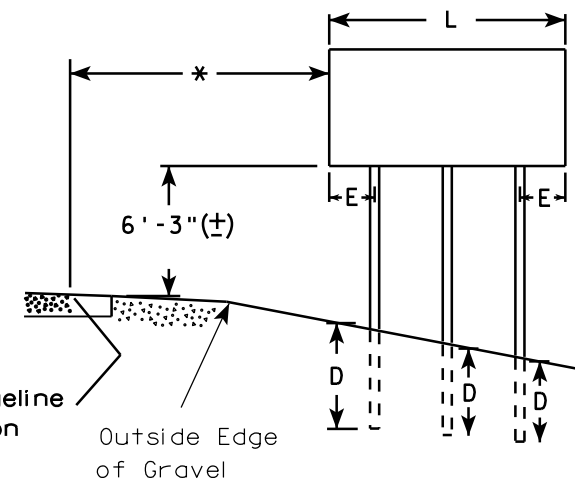
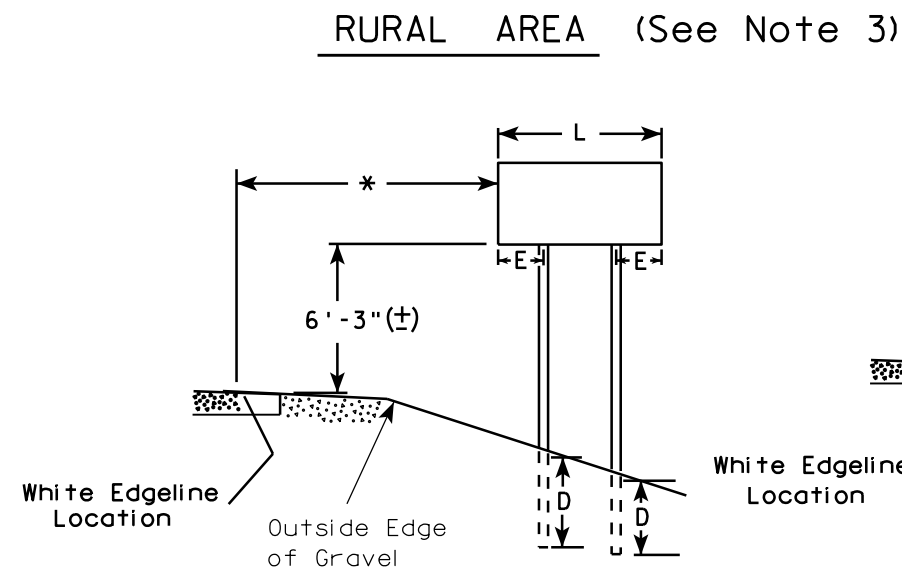
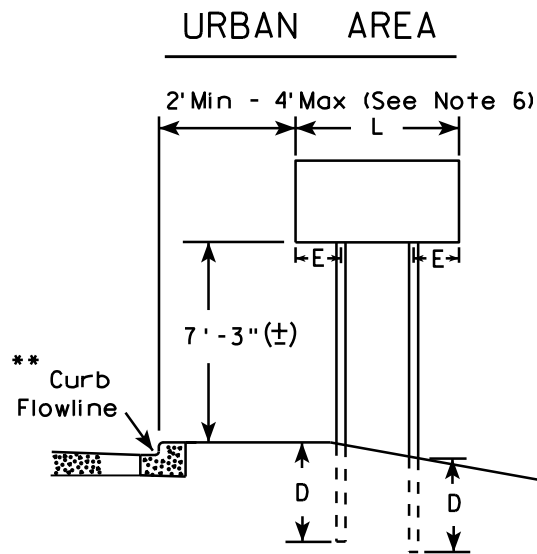
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 9/30/13 PLATE NO. A4-3.18



48" DIAMOND WARNING SIGN

48" DIAMOND WARNING SIGN

- GENERAL NOTES**
- For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
 - See tables below for required number of posts.
 - For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
 - The (±) tolerance for mounting height is 3 inches.
 - Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
 - Offset distance shall be consistent with existing signs or consistent throughout length of project.
 - Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
 - The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series) & End of Road Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width or 20 S.F. or less in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 120"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF TYPE II SIGNS
ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
for State Traffic Engineer

DATE 9/30/13

PLATE NO. A4-4.12

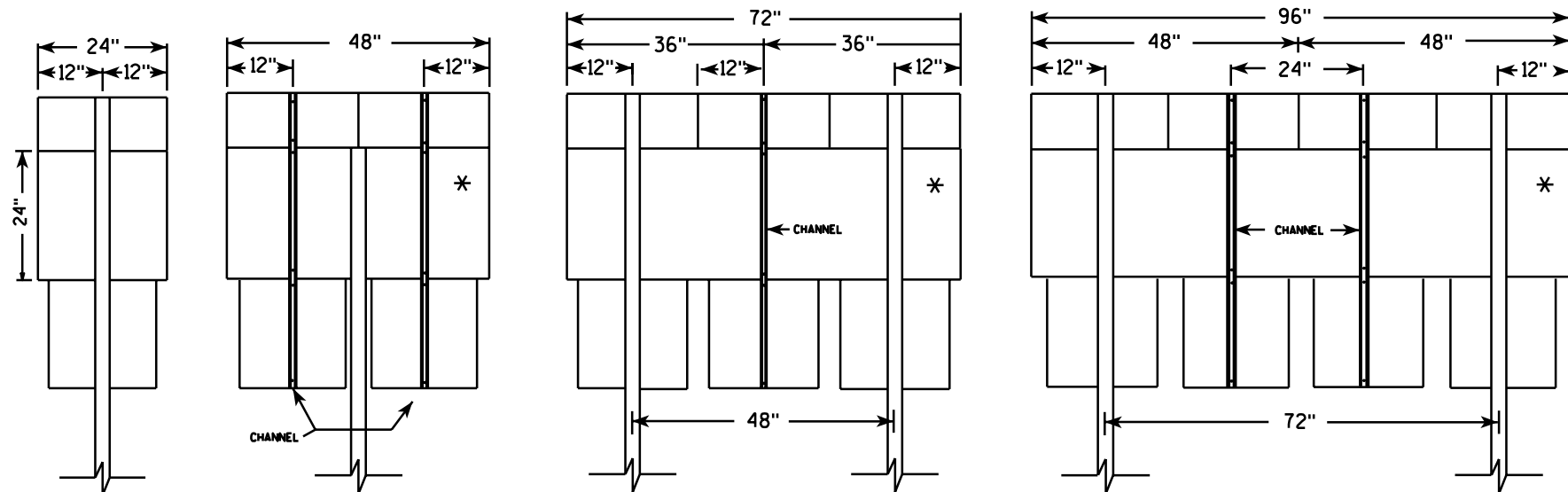
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



24" MARKER DETAIL

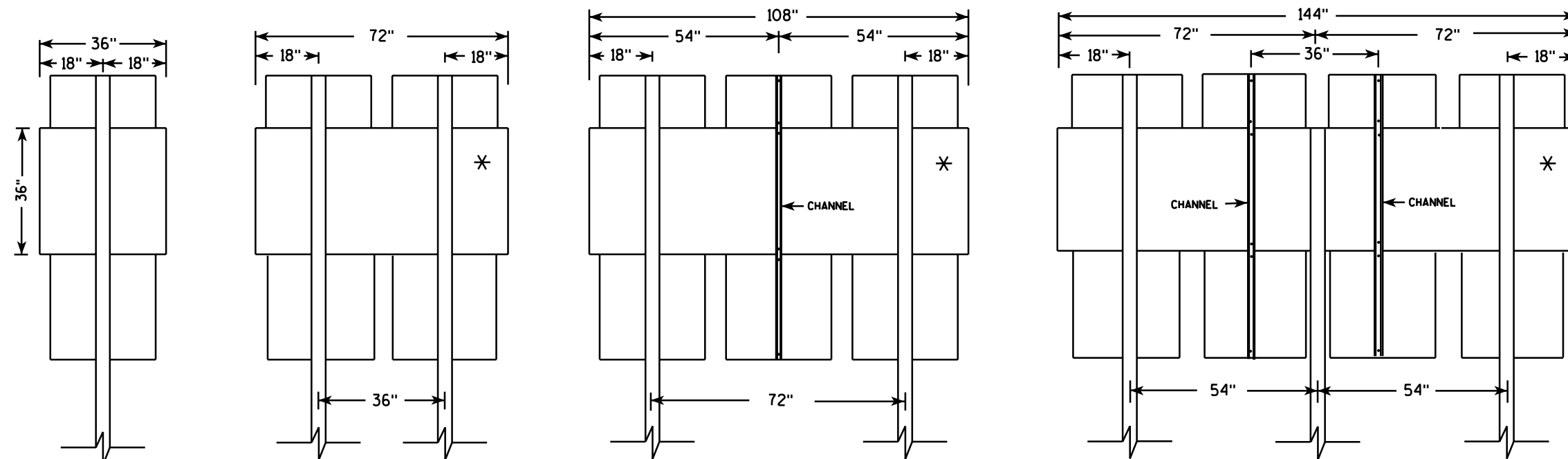
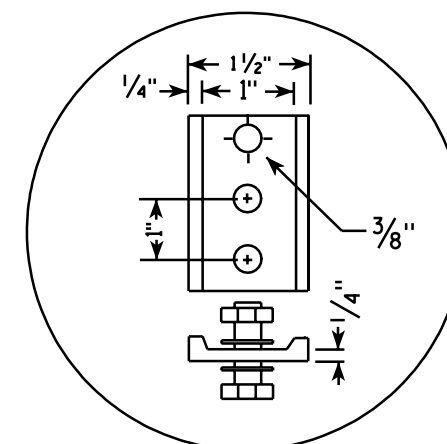
CHANNEL HARDWARE:

Aluminum Sign components: -1/4" x 3/4" bolt and 1/4" flat washers

Plywood Sign Components: -1/4" x 1 1/4" bolt and 1/4" flat washers

- NOTES:
1. Post spacing shall be according to this detail but post embedment depth shall be in accordance with A4-4.
 2. Channel material shall be as specified in Section 633 of Std. Specs. and weight shall be approx. 1.4 lbs/ft.
 3. Base material for a multiple marker head panel (*) shall be one piece high density overlay plywood. All other materials within the assembly can be either plywood or aluminum.

CHANNEL DETAIL



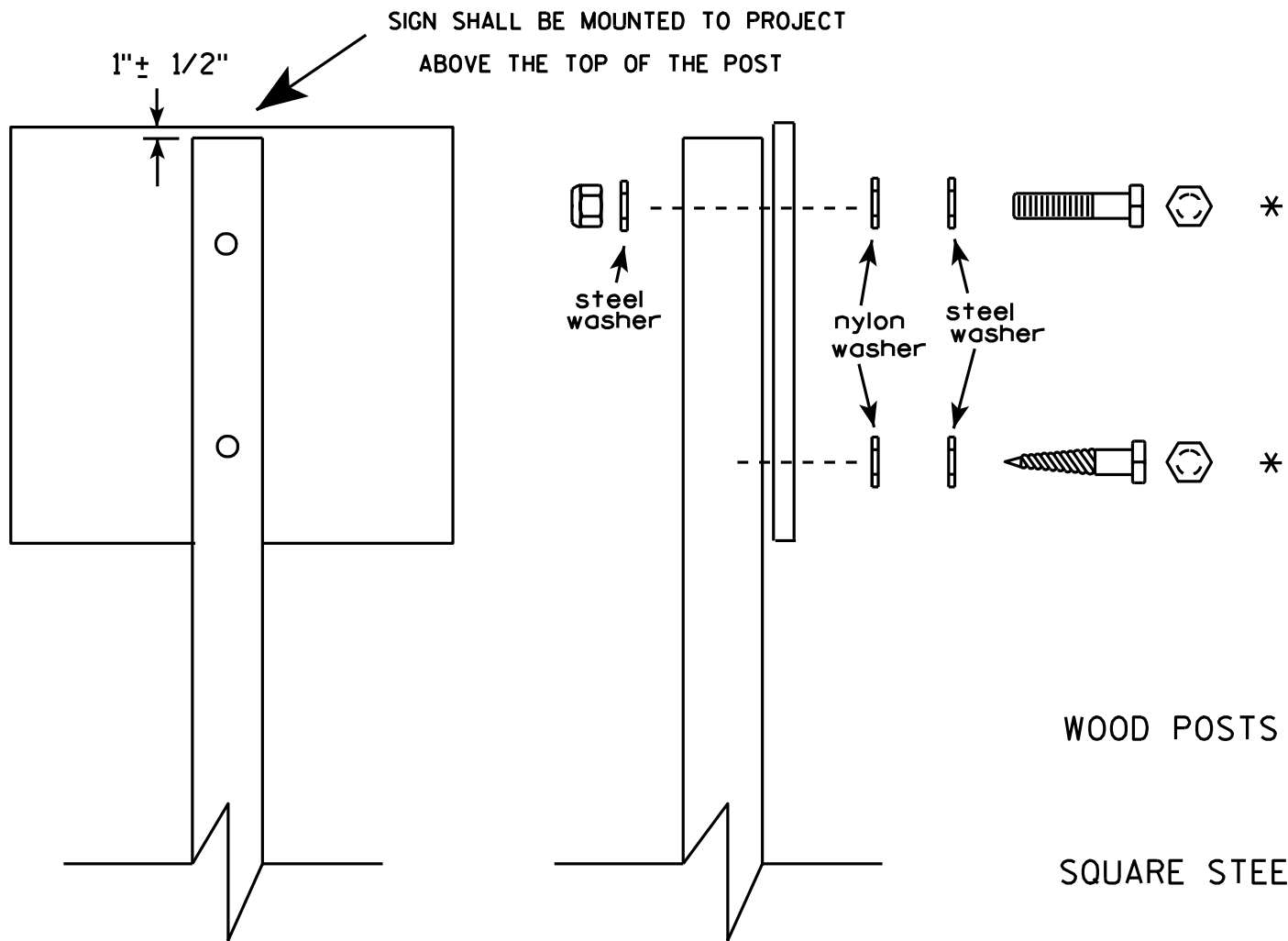
36" MARKER DETAIL

TYPICAL PANEL INSTALLATION FOR ASSEMBLIES	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Chester J. Spang</i> for State Traffic Engineer
DATE 10/28/96	PLATE NO. A4-5.4

PROJECT NO:

SHEET NO:

E

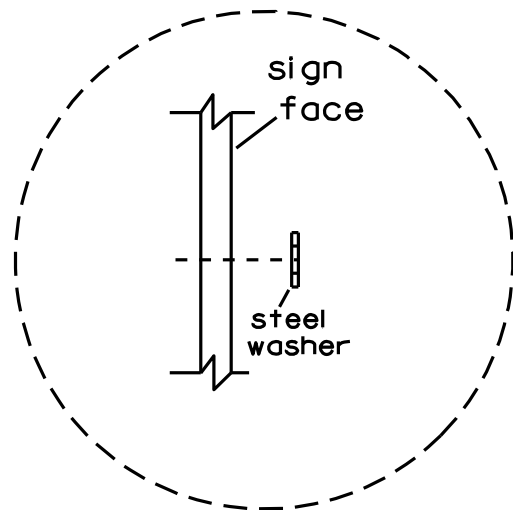


Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

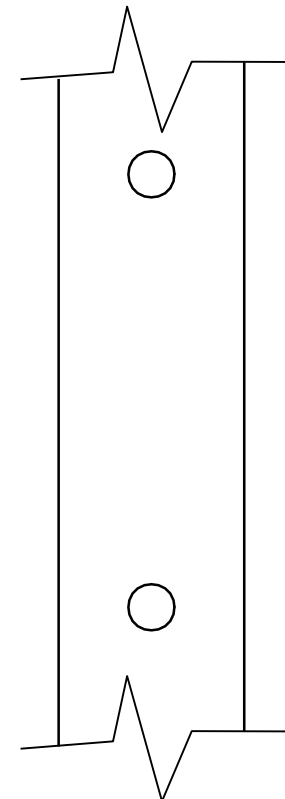
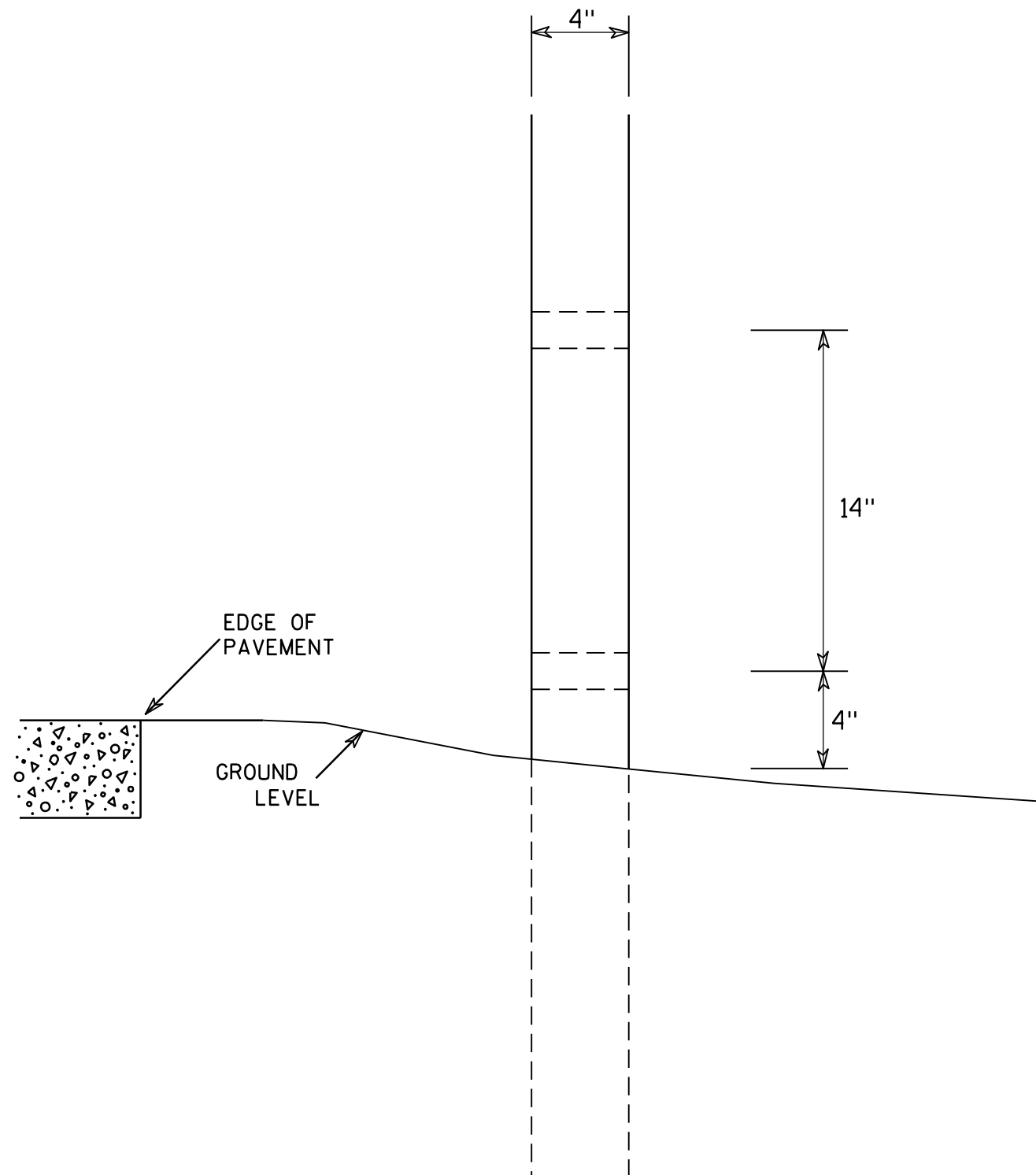
- WOOD POSTS (4" x 4" or 4" x 6")
LAG SCREWS - 3/8" X 3"
MACHINE BOLTS - 5/16" X 6-1/2" or 7" Length w/ nuts
- SQUARE STEEL POSTS (2" x 2")
MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts
RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON for all Type H signs.



Washer Placement when Sign Has Other Than Type H or Type F Face

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 3/23/10	PLATE NO. A4-8.7



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1 1/2" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

7

Metric equivalent
for this sign is:

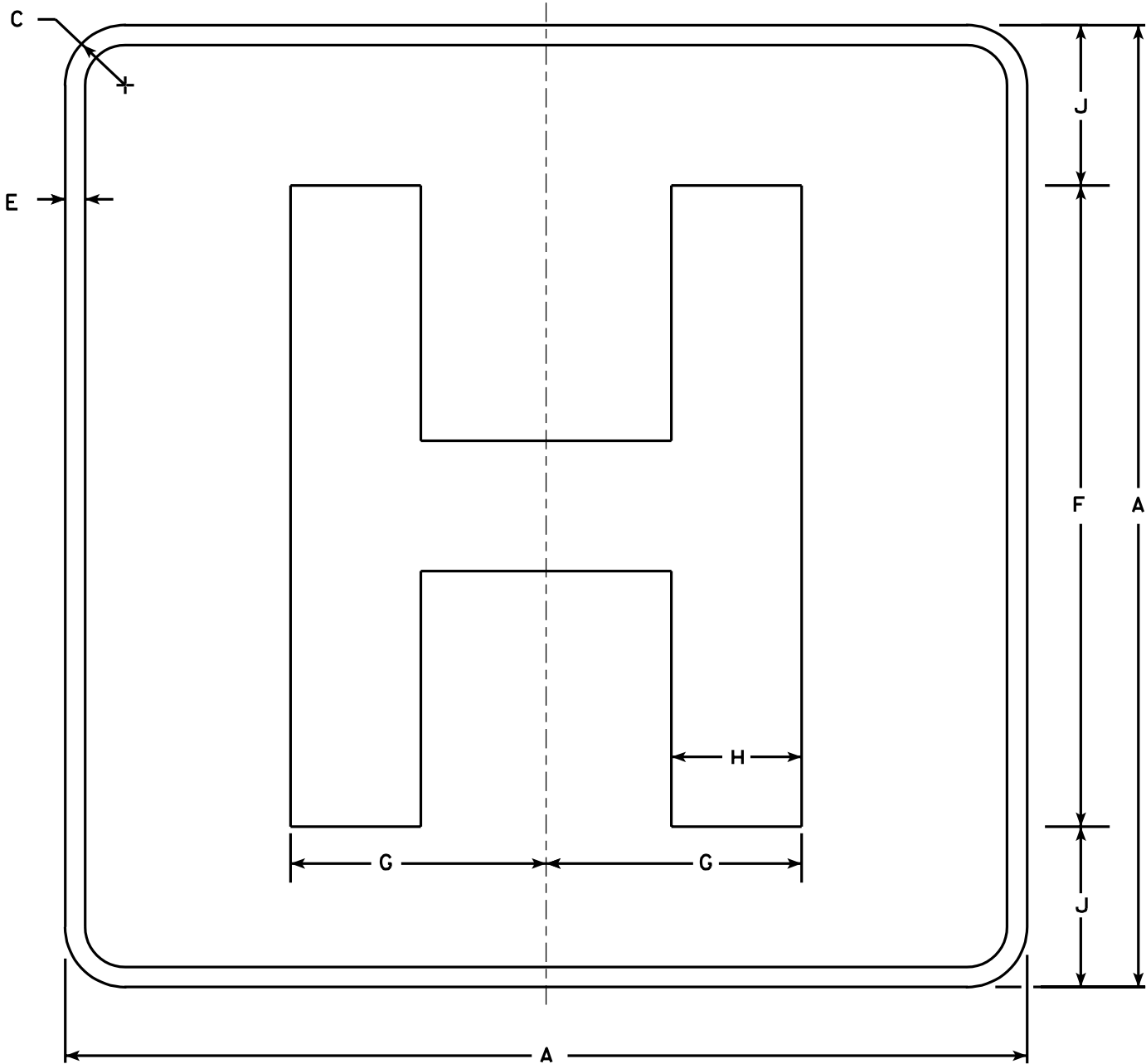
SIZE	
1	450 mmX 450 mm
2	600 mmX 600 mm
3	900 mmX 900 mm
4	X
5	X

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8		1/2	12	4 3/4	2 3/8		3																	4.0
2	24		1 1/2		1/2	16	6 3/8	3 1/4		4																	4.0
3	36		2 1/4		3/4	24	9 1/2	4 7/8		6																	9.0
4																											
5																											

PROJECT NO:

SHEET NO:

E

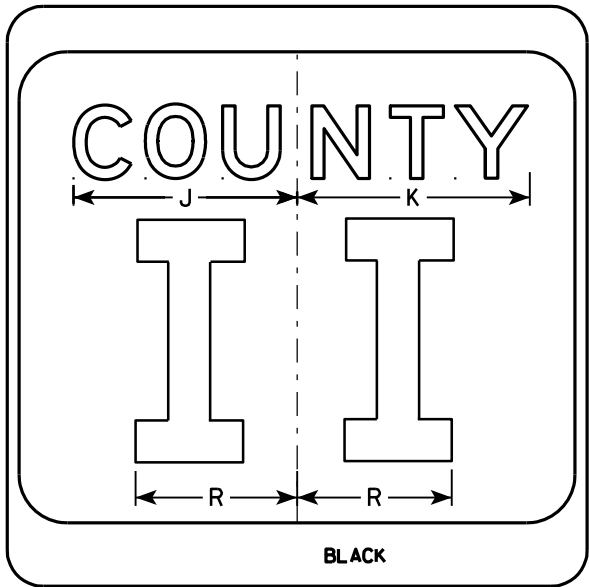
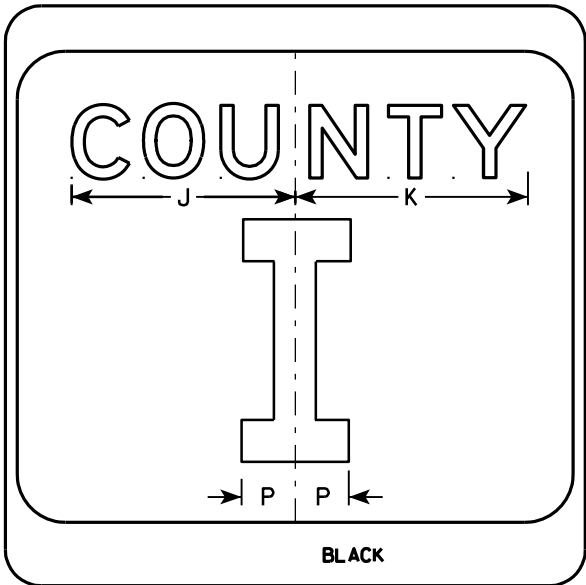
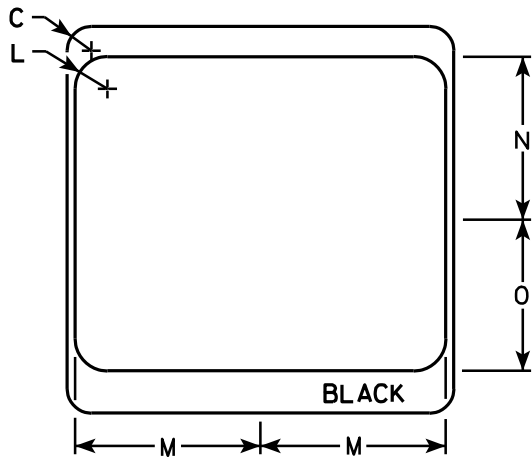
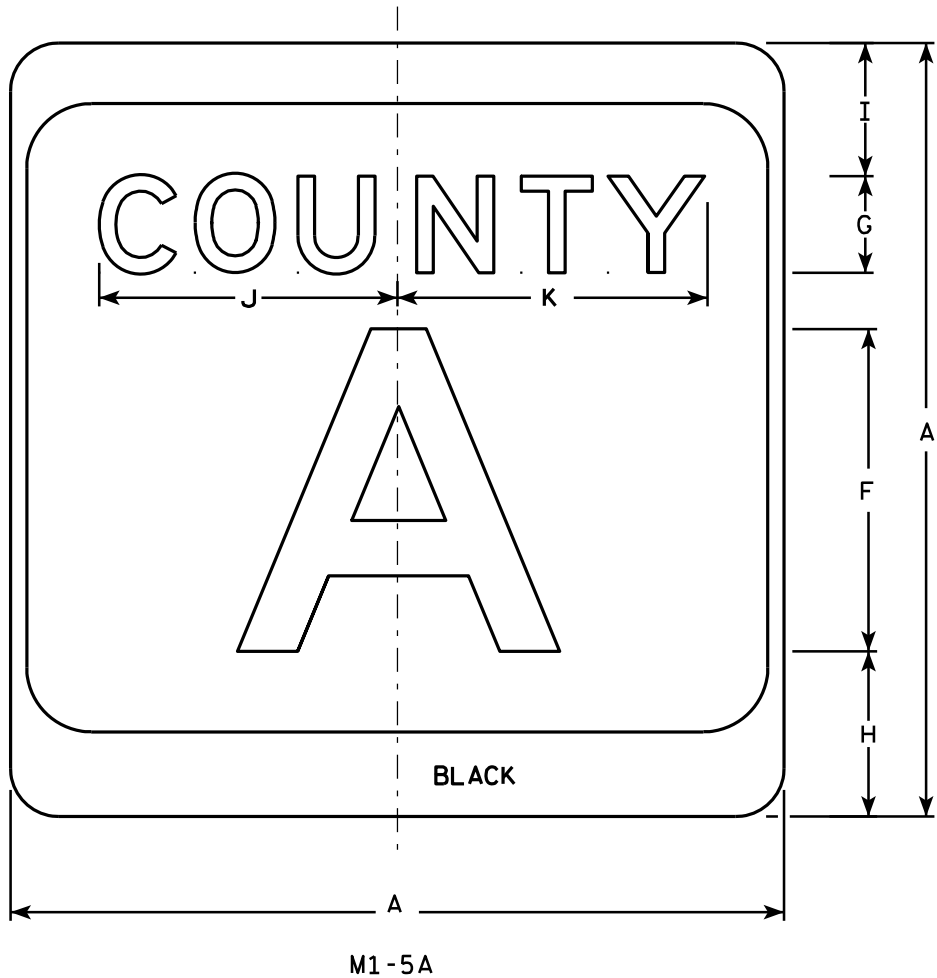


D9-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Blue
Message - White - Type H Reflective
3. Message Series - E Modified
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7



NOTES

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 7
Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.
Message Series D for 2 letters unless message is too big then Series C.
Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

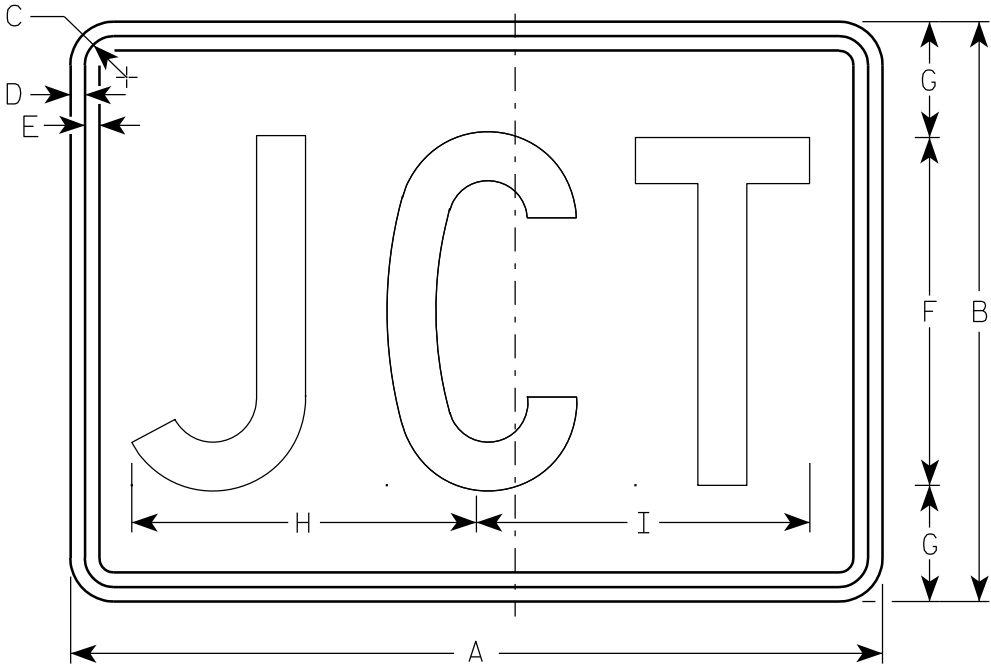
CTH MARKER	
M1-5A FOR ASSEMBLIES	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 9/27/11	PLATE NO. M1-5A.8

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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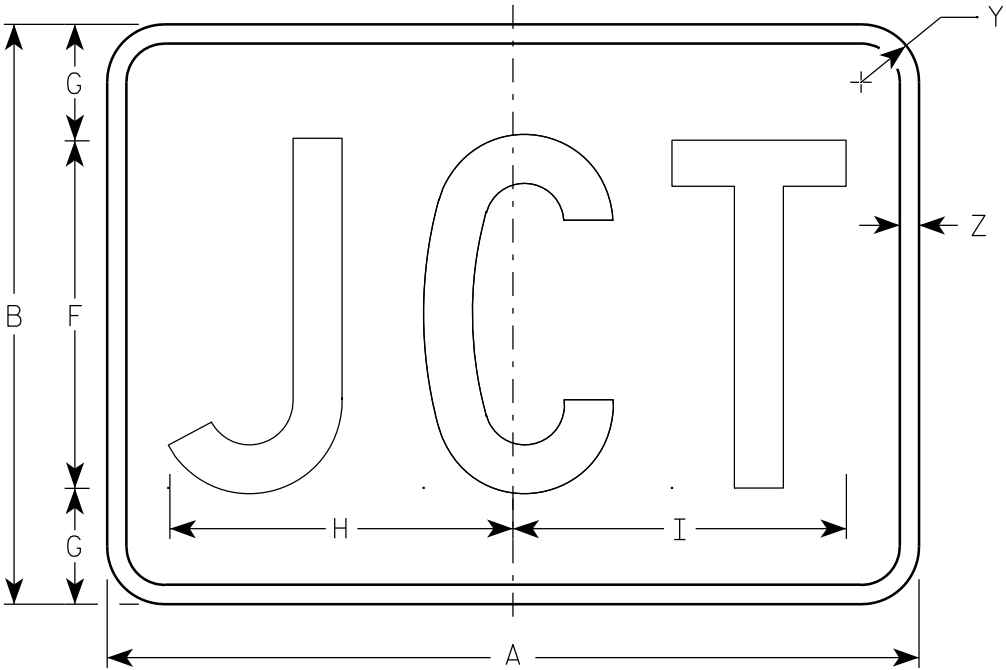
7

NOTES

1. Sign is Type II - See Note 5 - reference
WIS DOT Standard Specification for HIGHWAY
and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base
material is plywood but borders shall be rounded
as shown. When base material is metal, the
corners and borders shall be rounded.
5. M2-1 Background - White - Type H Reflective
(Detour or temporary Signs - Reflective)
Message - Black
MB2-1 Background - Blue
Message - White - Type H Reflective
(Detour or temporary Signs - Reflective)
MG2-1 Background - Green
Message - White - Type H Reflective
MK2-1 Background - Green
Message - White - Type H Reflective
MM2-1 Background - White - Type H Reflective
Message - Green
MN2-1 Background - Brown
Message - White - Type H Reflective
MR2-1 Background - Brown
Message - Yellow - Type H Reflective



M2-1
MK2-1
MM2-1
MR2-1



MB2-1
MG2-1
MN2-1

Metric equivalent
for this sign is:

SIZE	
1	
2	525 mm X 375 mm
3	750 mm X 525 mm
4	750 mm X 525 mm
5	750 mm X 525 mm

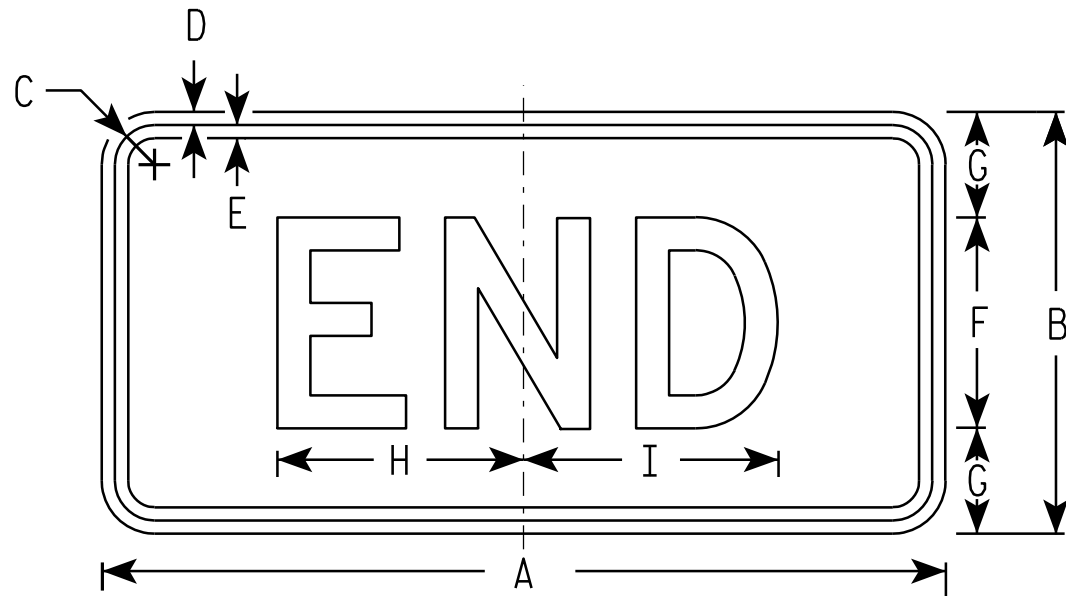
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20	0.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20

STANDARD SIGN
M2-1

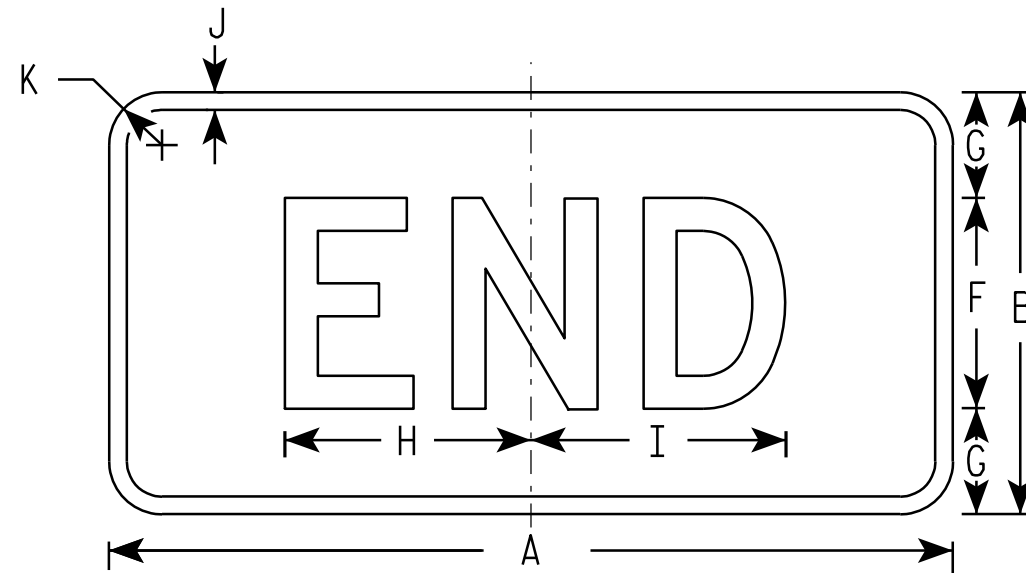
WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 3/16/10 PLATE NO. M2-1.10



M4-6
MK4-6
MM4-6
MR4-6



MB4-6
MG4-6
MN4-6

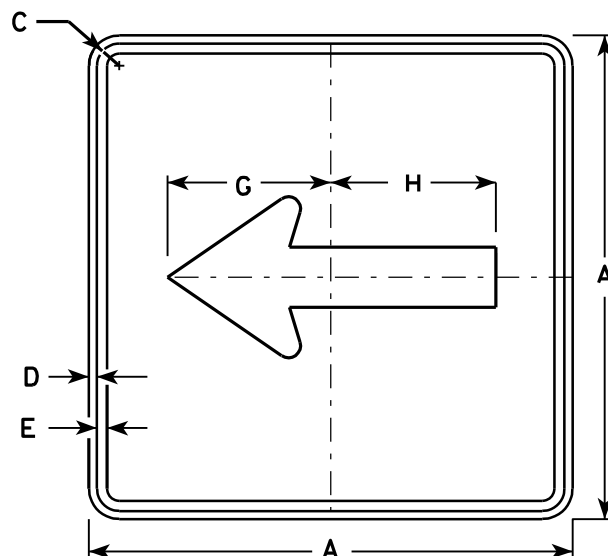
NOTES

- Sign is Type II - See Note 5 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - See note 5
Message - See note 5
- Message Series - D
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M4-6 Background - White - Type H Reflective
(Detour or temporary Signs - Reflective)
Message - Black
MB4-6 Background - Blue
Message - White - Type H Reflective
(Detour or temporary Signs - Reflective)
MG4-6 Background - Green
Message - White - Type H Reflective
MK4-6 Background - Green
Message - White - Type H Reflective
MM4-6 Background - White - Type H Reflective
Message - Green
MN4-6 Background - Brown
Message - White - Type H Reflective
MR4-6 Background - Brown
Message - Yellow - Type H Reflective

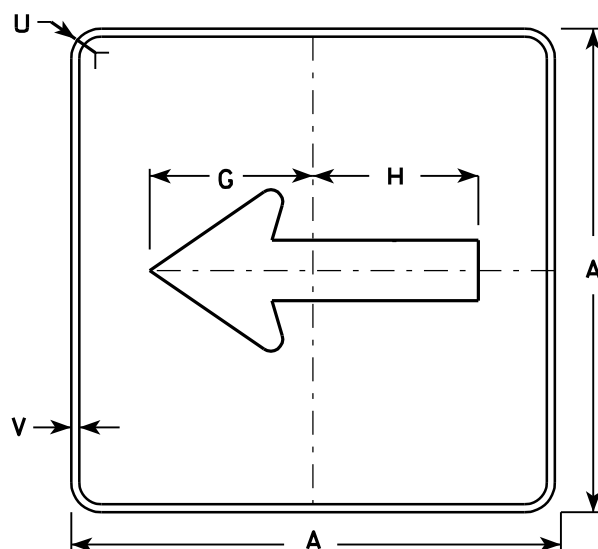
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	7	7 1/4	1/2	1 1/2																2.00
3	36	18	1 1/8	3/8	1/2	9	4 1/2	12	11 7/8	1/2	1 1/2																4.5
4	36	18	1 1/8	3/8	1/2	9	4 1/2	12	11 7/8	1/2	1 1/2																4.5
5	36	18	1 1/8	3/8	1/2	9	4 1/2	12	11 7/8	1/2	1 1/2																4.5

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

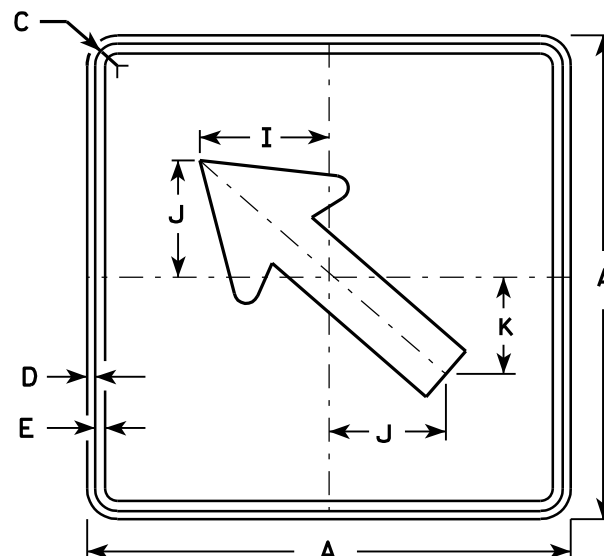
STANDARD SIGN M4 - 6	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 11/10/10	PLATE NO. M4-6.7



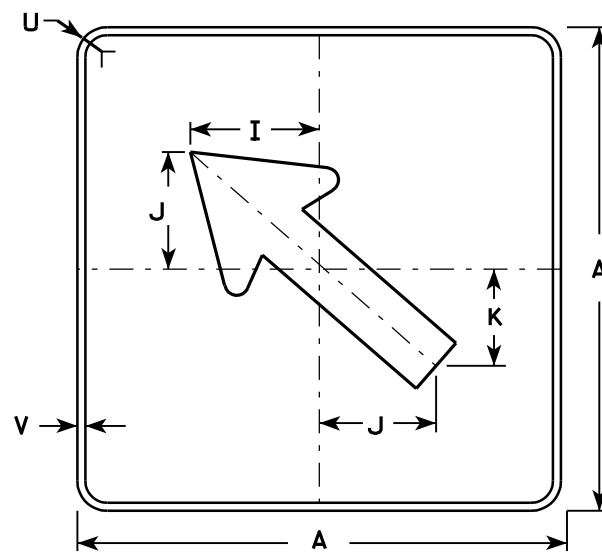
M6-1
MK6-1
MM6-1
MO6-1
MP6-1
MR6-1



MB6-1
MG6-1
MN6-1



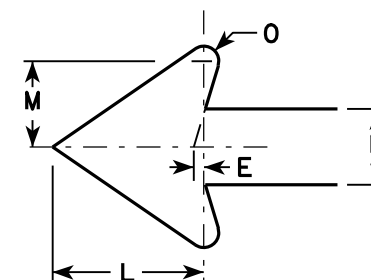
M6-2
MK6-2
MM6-2
MO6-2
MP6-2
MR6-2



MB6-2
MG6-2
MN6-2

NOTES

- Signs are Type II - See Note 4 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White - Type H Reflective Message - Black
 - MB6-1 and MB6-2 Background - Blue Message - White - Type H Reflective
 - MG6-1 and MG6-2 Background - Green Message - White - Type H Reflective
 - MK6-1 and MK6-2 Background - Green Message - White - Type H Reflective
 - MM6-1 and MM6-2 Background - White - Type H Reflective Message - Green
 - MN6-1 and MN6-2 Background - Brown Message - White - Type H Reflective
 - MO6-1 and MO6-2 Background - Orange - Type F Reflective Message - Black
 - MP6-1 and MP6-2 Background - White - Type H Reflective Message - Blue
 - MR6-1 and MR6-2 Background - Brown Message - Yellow - Type H Reflective



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN M6-1 & M6-2 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 7/29/13 PLATE NO. M6-1.13

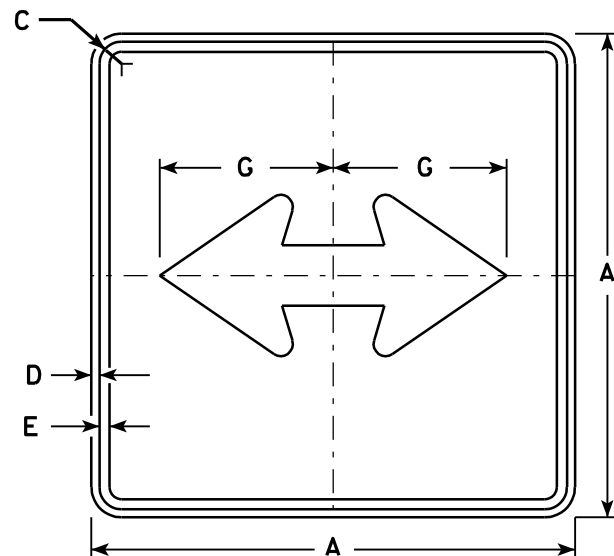
PROJECT NO:

HWY:

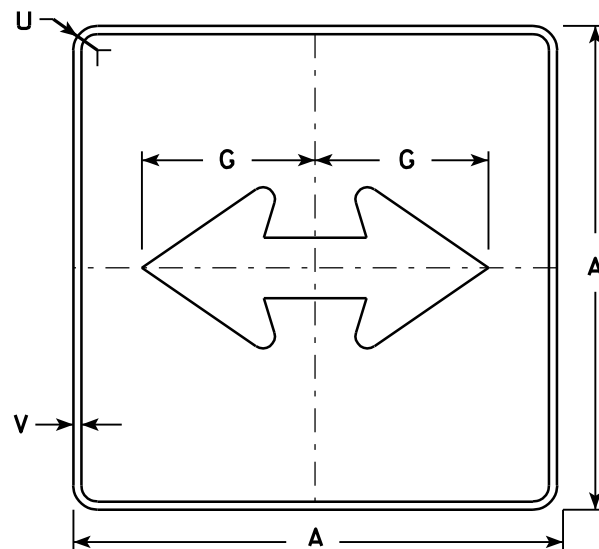
COUNTY:

SHEET NO:

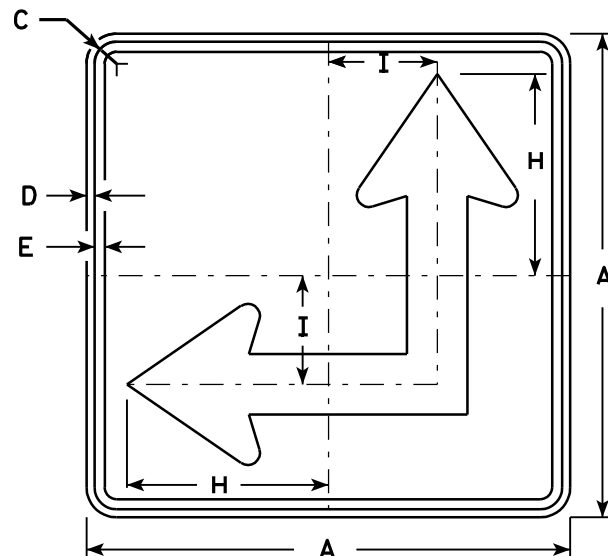
E



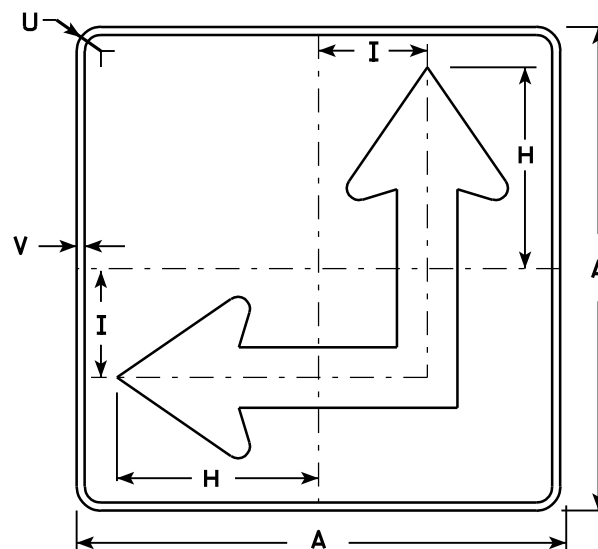
M6 - 4
MK6 - 4
MM6 - 4
MO6 - 4
MP6 - 4
MR6 - 4



MB6 - 4
MG6 - 4
MN6 - 4



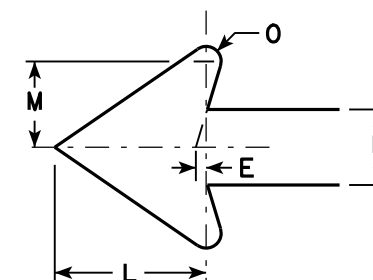
M6 - 6
MK6 - 6
MM6 - 6
MO6 - 6
MP6 - 6
MR6 - 6



MB6 - 6
MG6 - 6
MN6 - 6

NOTES

- Signs are Type II - See Note 4 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - See Note 4
Message - See Note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-4 and M6-6 Background - White - Type H Reflective
Message - Black
MB6-4 and MB6-6 Background - Blue
Message - White - Type H Reflective
MG6-4 and MG6-6 Background - Green
Message - White - Type H Reflective
MK6-4 and MK6-6 Background - Green
Message - White - Type H Reflective
MM6-4 and MM6-6 Background - White - Type H Reflective
Message - Green
MN6-4 and MN6-6 Background - Brown
Message - White - Type H Reflective
MO6-4 and MO6-6 Background - Orange - Type F Reflective
Message - Black
MP6-4 and MP6-6 Background - White - Type H Reflective
Message - Blue
MR6-4 and MR6-6 Background - Brown
Message - Yellow - Type H Reflective
- M6-6R same as M6-6L except arrow points ahead and right.



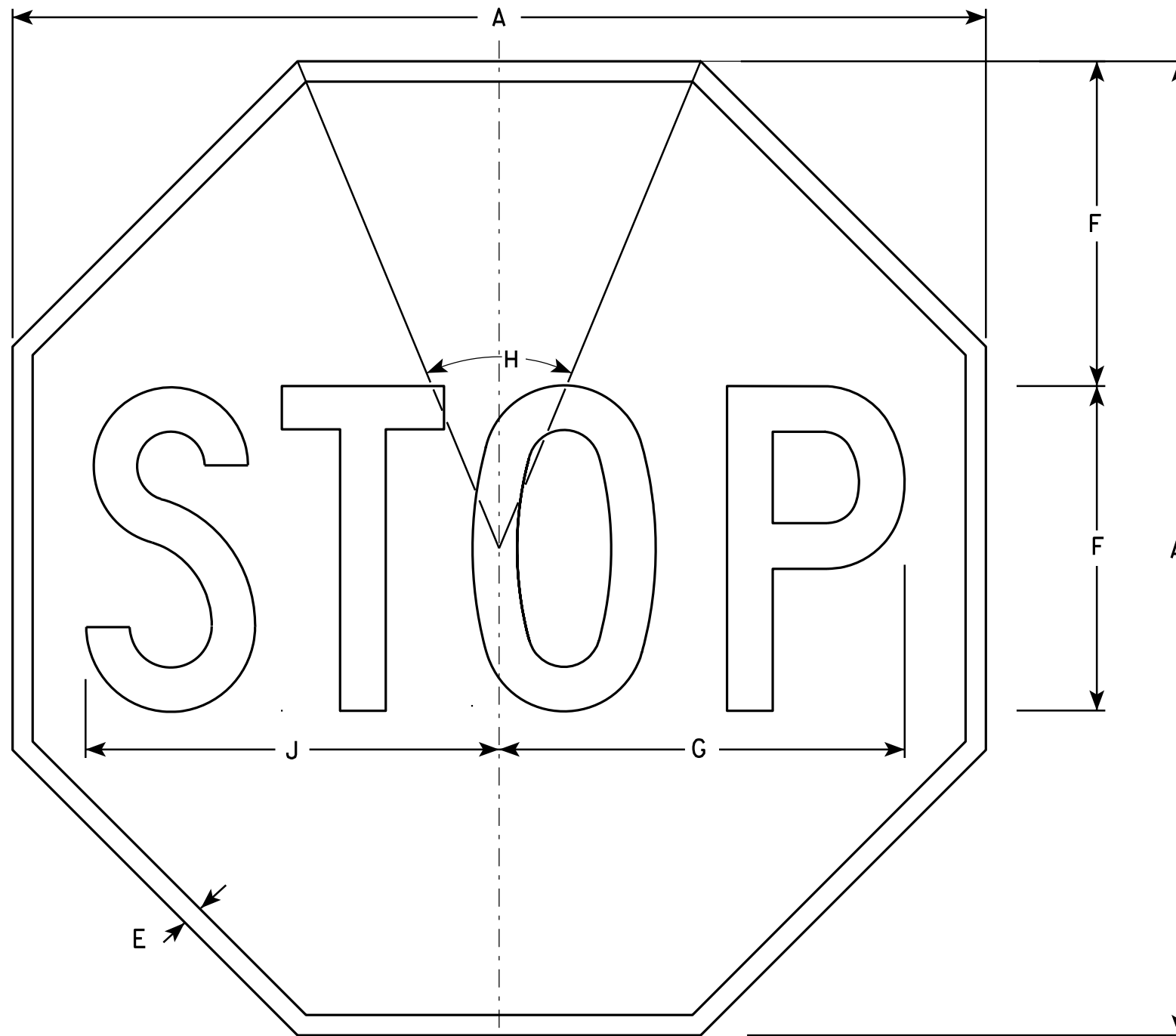
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	8 3/4	4 1/4			5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN
M6 - 4 & M6 - 6
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 7/29/13 PLATE NO. M6-4.8



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C

R1-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24				$\frac{3}{8}$	8	10	45°		10 $\frac{1}{4}$																	3.31
2S	30				$\frac{5}{8}$	10	12 $\frac{1}{2}$	45°		12 $\frac{3}{4}$																	5.18
2M	36				$\frac{3}{4}$	12	15	45°		15 $\frac{3}{8}$																	7.46
3	36				$\frac{3}{4}$	12	15	45°		15 $\frac{3}{8}$																	7.46
4	48				1	16	20	45°		20 $\frac{1}{2}$																	13.25
5	48				1	16	20	45°		20 $\frac{1}{2}$																	13.25
6	18				$\frac{3}{8}$	6	7 $\frac{3}{4}$	45°		7 $\frac{3}{4}$																	1.86
7	12				$\frac{1}{4}$	4	5	45°		5 $\frac{1}{8}$																	0.78

STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1.12

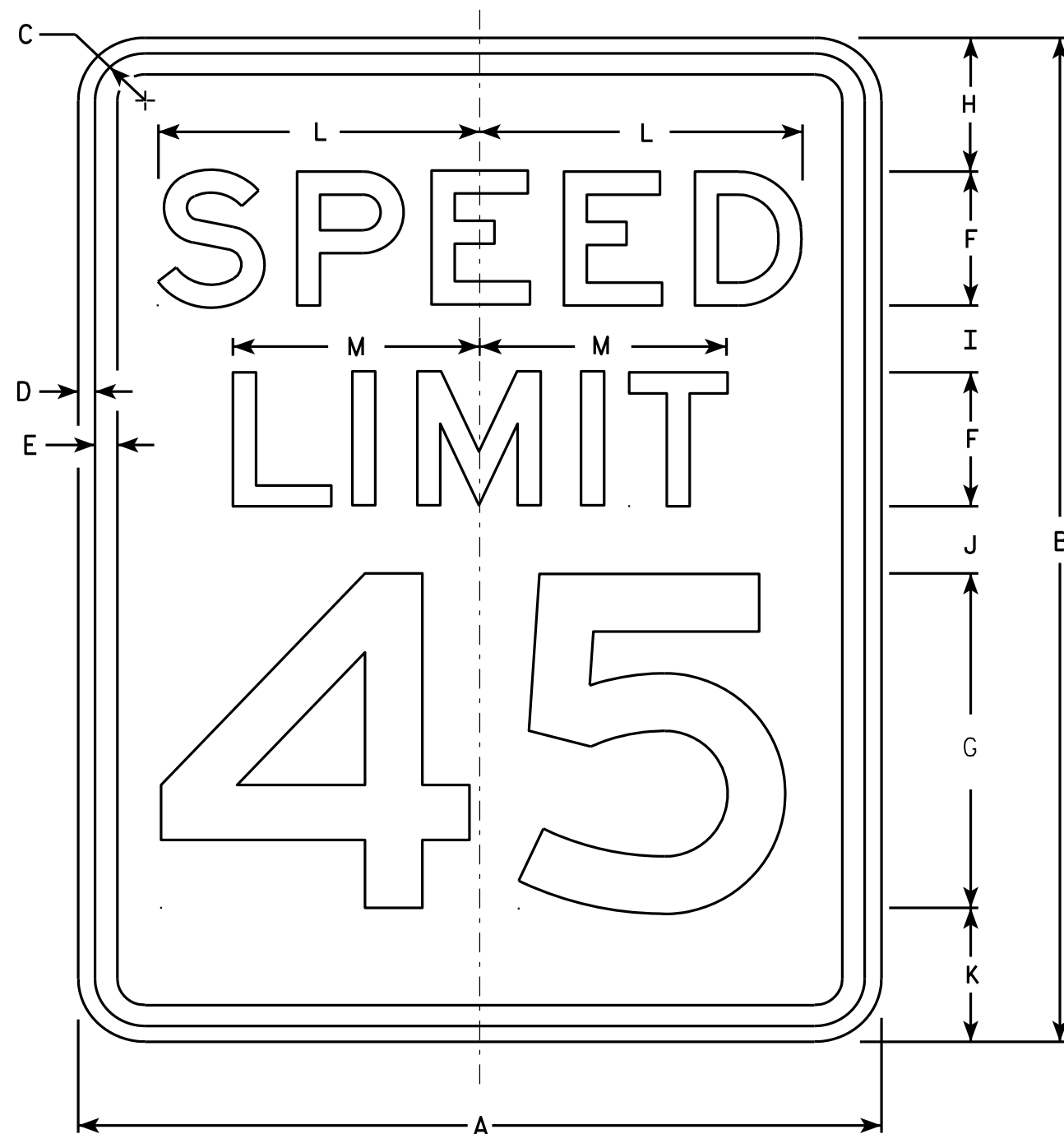
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



R2-1

NOTES

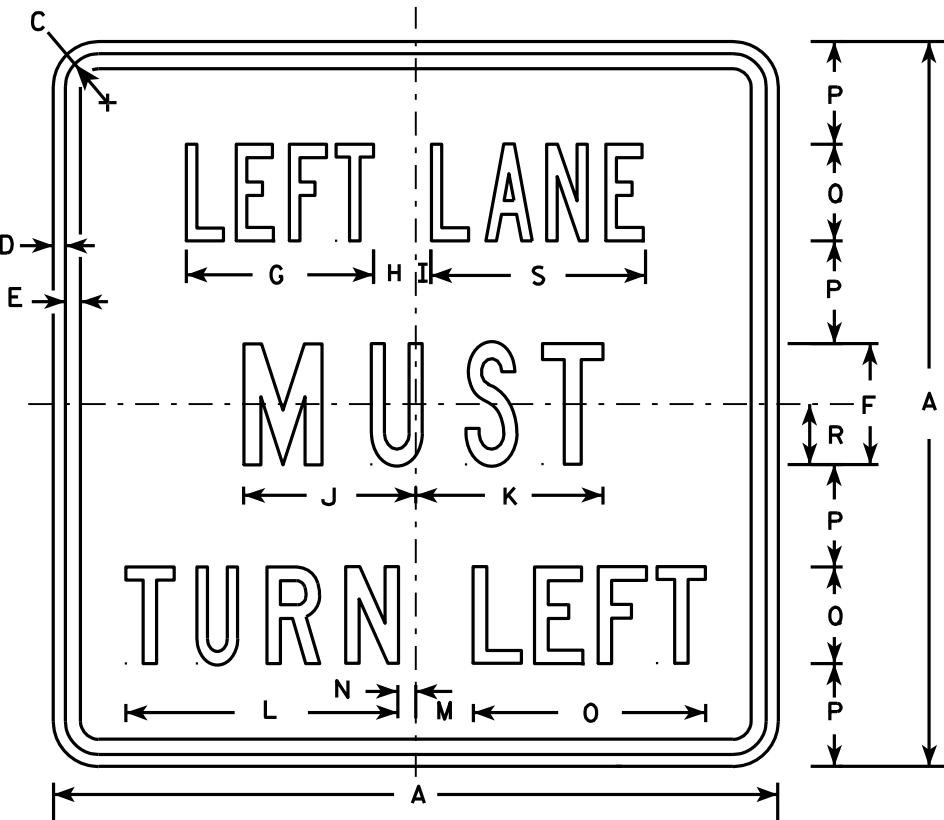
1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

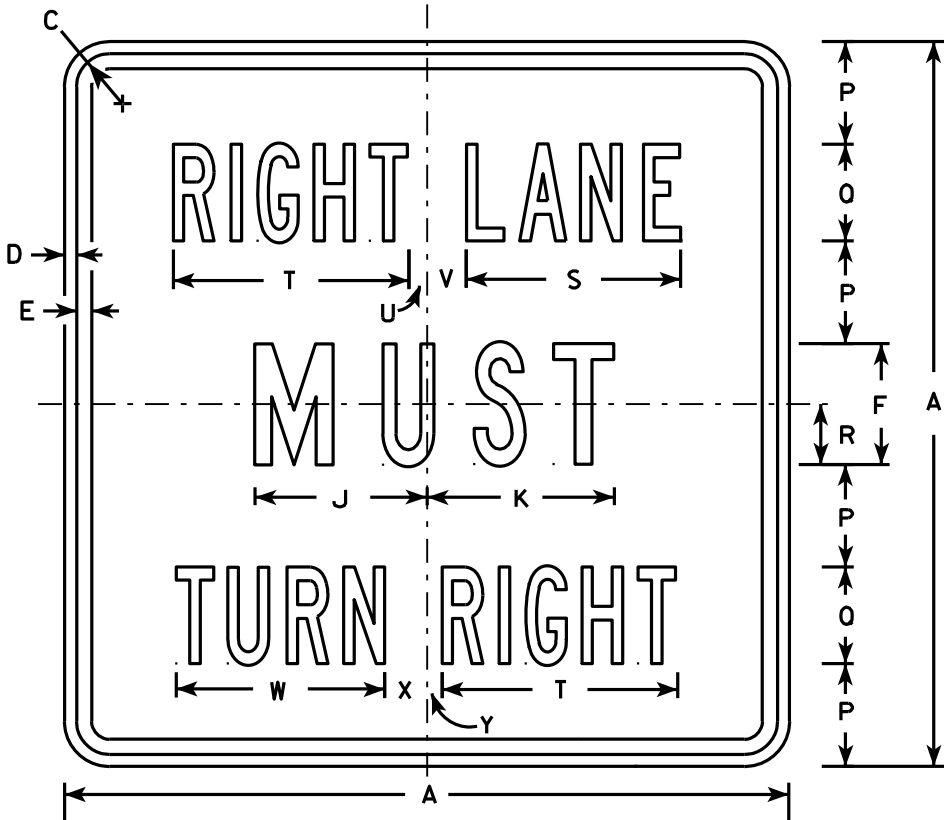
STANDARD SIGN R2-1

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
For State Traffic Engineer
DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: HWY: COUNTY: SHEET NO: E



R3-7L



R3-7R

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - Line 1 is Series B.
Line 2 is Series C.
Line 3 on plate R3-7R is Series B and Series C on plate R3-7L.
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	5	7 3/4	1 3/4	5/8	7 1/8	7 3/4	11 1/4	2 3/8	3/4	9 5/8	4 1/4	4	2 1/2	8 7/8	9 3/4	3/4	1 5/8	8 5/8	1 5/8	5/8		6.25
2S	30		1 3/8	1/2	5/8	5	7 3/4	1 3/4	5/8	7 1/8	7 3/4	11 1/4	2 3/8	3/4	9 5/8	4 1/4	4	2 1/2	8 7/8	9 3/4	3/4	1 5/8	8 5/8	1 5/8	5/8		6.25
2M	30		1 3/8	1/2	5/8	5	7 3/4	1 3/4	5/8	7 1/8	7 3/4	11 1/4	2 3/8	3/4	9 5/8	4 1/4	4	2 1/2	8 7/8	9 3/4	3/4	1 5/8	8 5/8	1 5/8	5/8		6.25
3	36		1 5/8	5/8	3/4	6	9 5/8	2	1 1/8	8 3/4	9	13 1/2	3 7/8	1 1/2	12 1/2	5	5	3	10 5/8	12	7/8	2 1/4	10 5/8	2 1/8	1		9.00
4	48		2 1/4	3/4	1	8	13 1/2	2 3/8	1 1/2	11 1/2	11 7/8	17 3/4	3 5/8	2 1/2	16 3/8	6 1/2	7	4	14 3/8	16 7/8	5/8	3 1/4	15 1/8	2 3/4	1 1/8		16.00
5																											

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

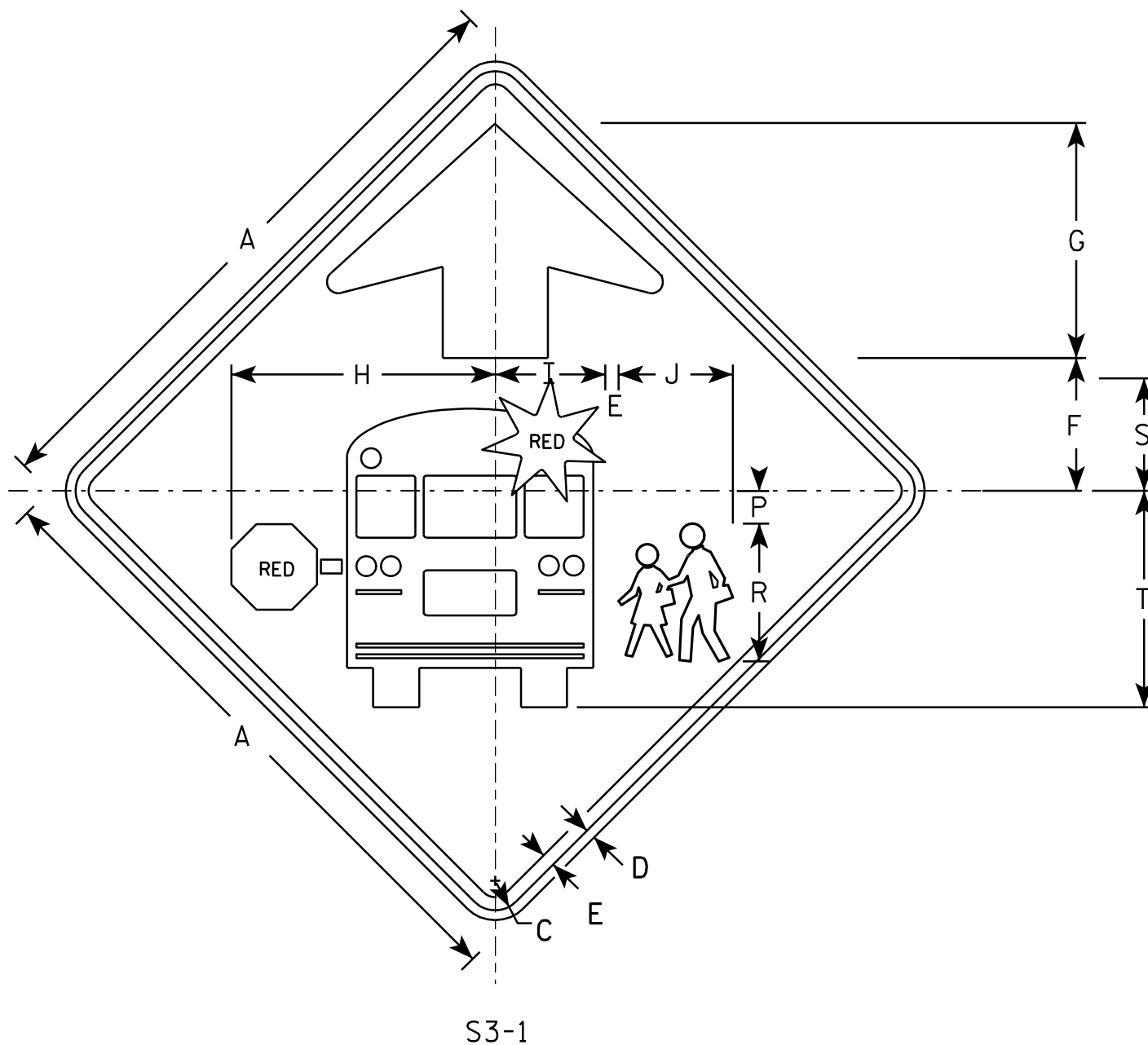
E

STANDARD SIGN
R3-7L & R3-7R

WISCONSIN DEPT OF TRANSPORTATION

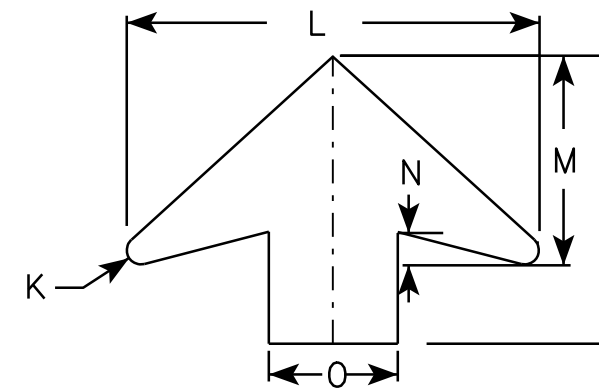
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/18/2011 PLATE NO. R3-7.3



NOTES

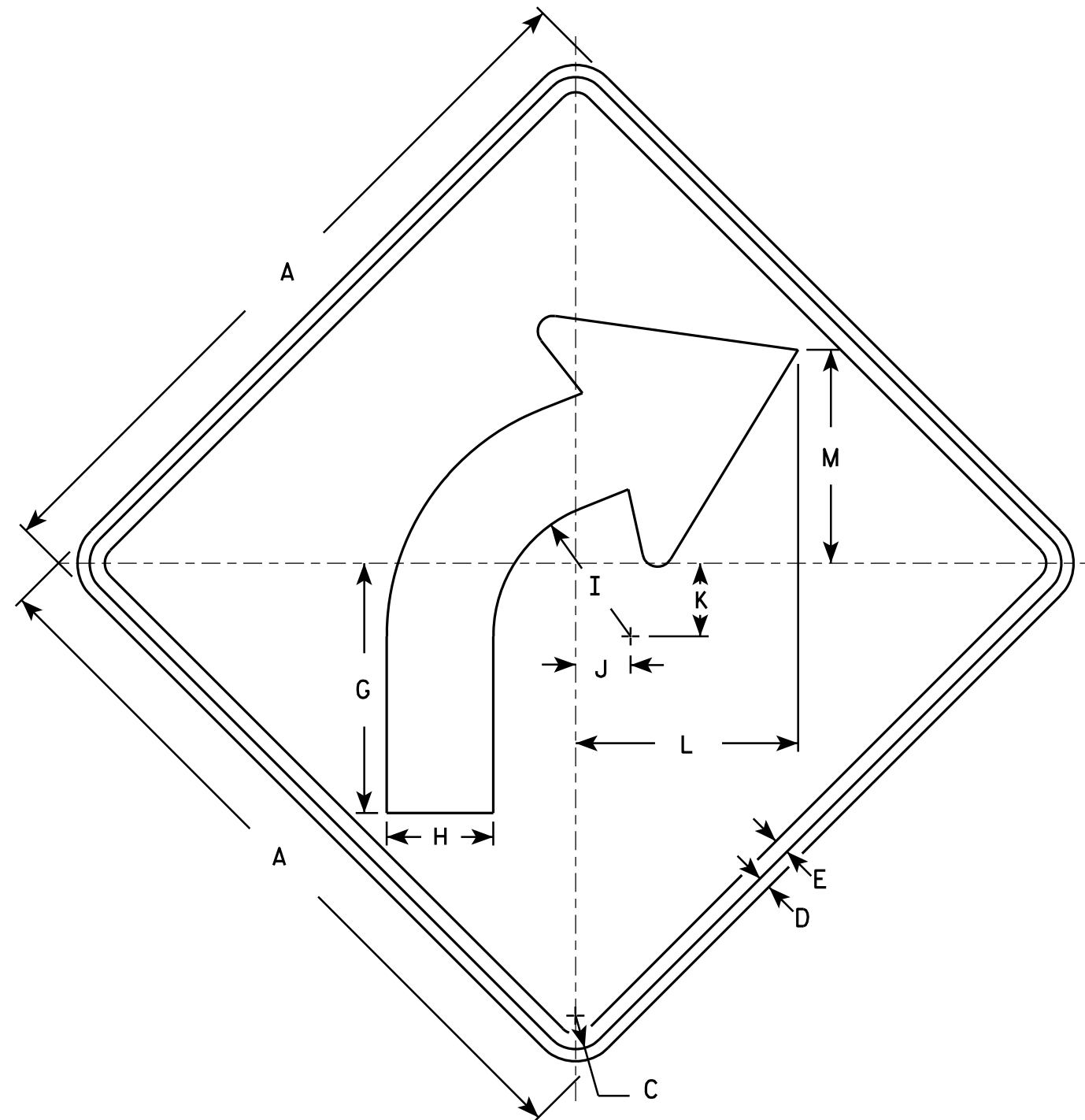
1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
 - Background - YELLOW-GREEN
 - Message - BLACK except as noted
 - Circles except PEDS- RED BACKGROUND
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

[illegible]

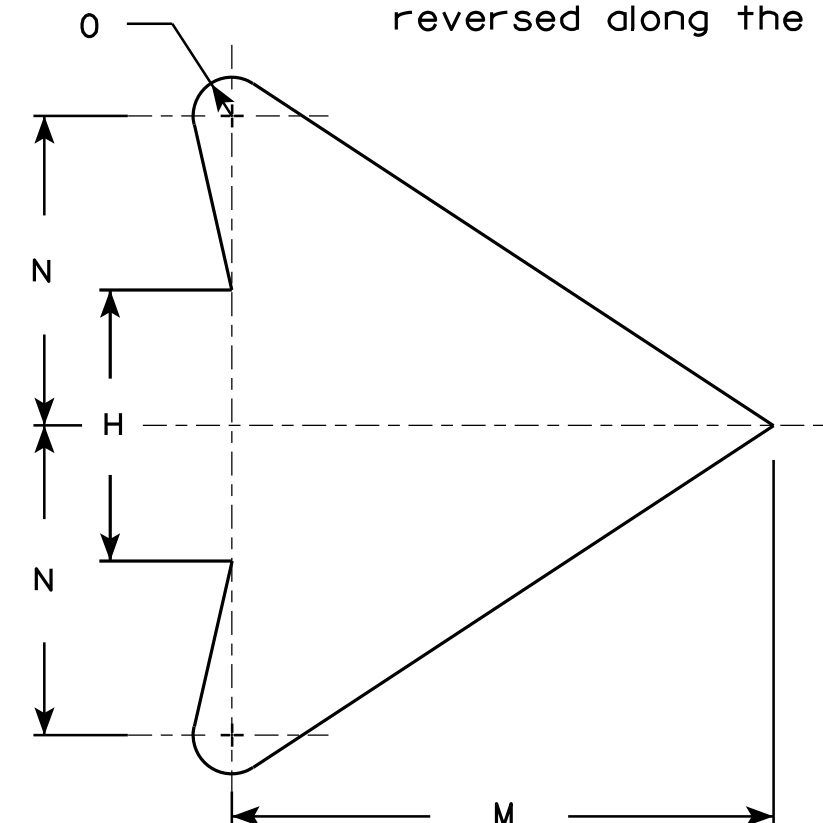
STANDARD SIGN	
S3-1	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<u>Matthew R. Rauch</u> for State Traffic Engineer
DATE <u>6/8/10</u>	PLATE NO. <u>S3-16</u>

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-2L is the same as W1-2R except the arrow is reversed along the vertical centerline.



W1-2R



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2												4.0
2S	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8												6.25
2M	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1												16.0

STANDARD SIGN W1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/15/12 PLATE NO. W1-2.10

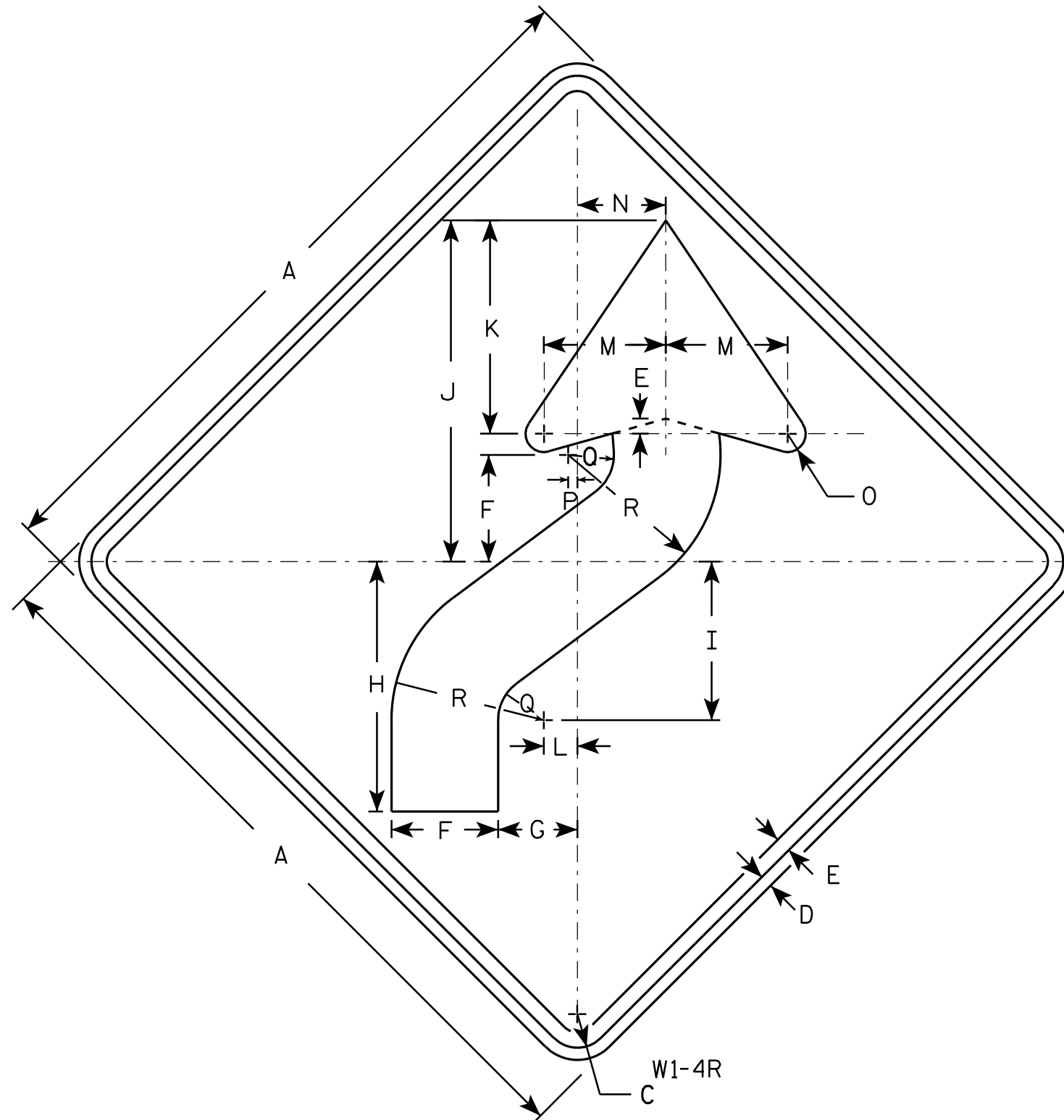
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



NOTES

- Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - Yellow
Message - Black
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- W1-4L is the same as W1-4R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	3 1/2	2 5/8	8 1/4	5 1/4	11 1/4	7	1 1/8	4	3	5/8	1/4	1 1/2	5									4.0
2S	30		1 3/8	1/2	5/8	4 3/8	3 1/4	10 1/4	6 1/2	14	8 3/4	1 3/8	5	3 5/8	3/4	3/8	1 7/8	6 1/4									6.25
2M	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
3	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
4	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

STANDARD SIGN

W1 - 4

WISCONSIN DEPT OF TRANSPORTATION

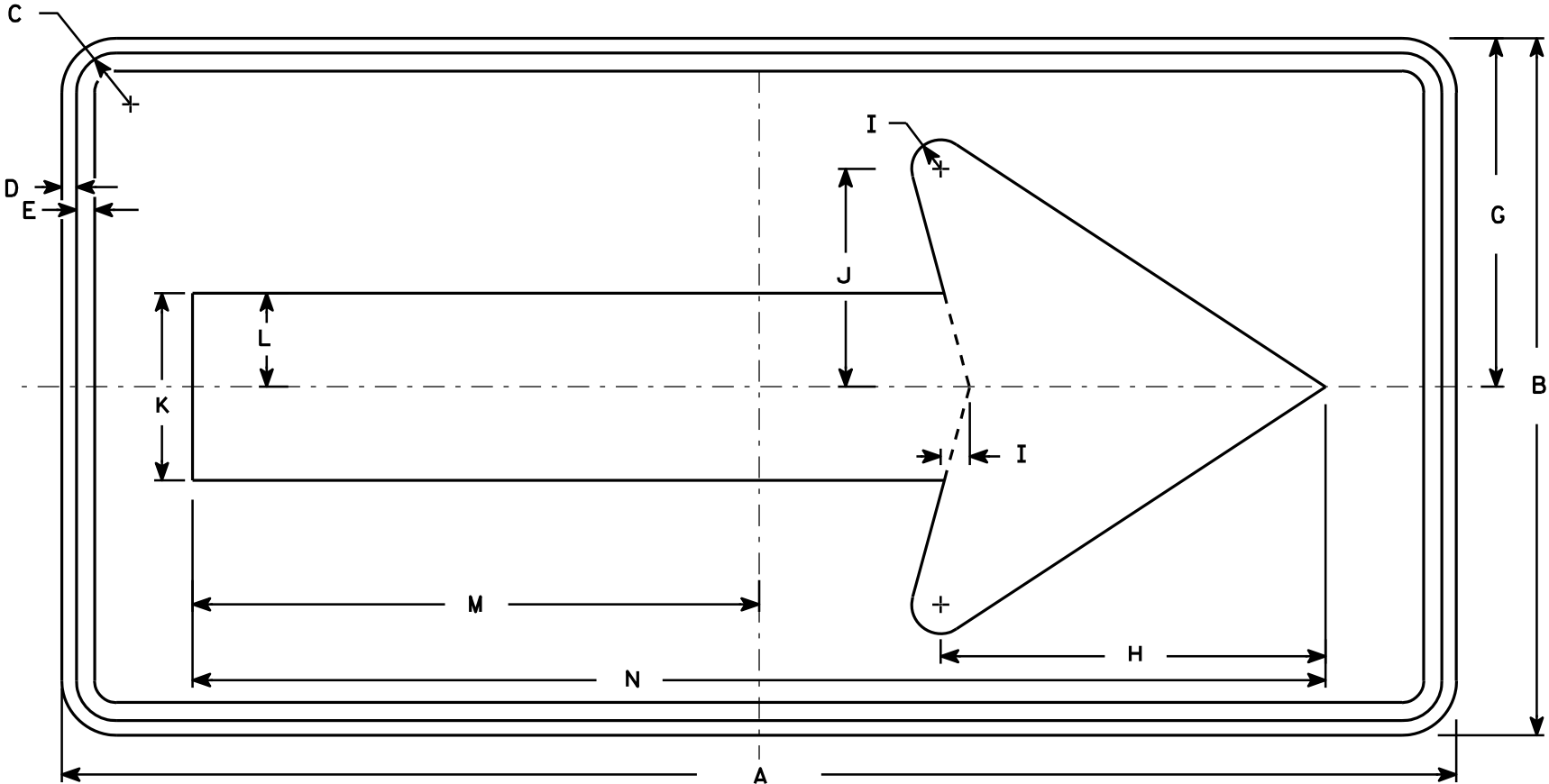
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/17/12 PLATE NO. W1-4.11

PROJECT NO: HWY: COUNTY: SHEET NO: E

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W1-6

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	3/8		9	10	3/4	5 5/8	4 3/4	2 3/8	14 5/8	29 1/4													4.5
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	96	48	2 1/4	3/4	1		24	26 1/2	2	15	13	6 1/2	39	78													32.0

STANDARD SIGN
W1-6

WISCONSIN DEPT OF TRANSPORTATION

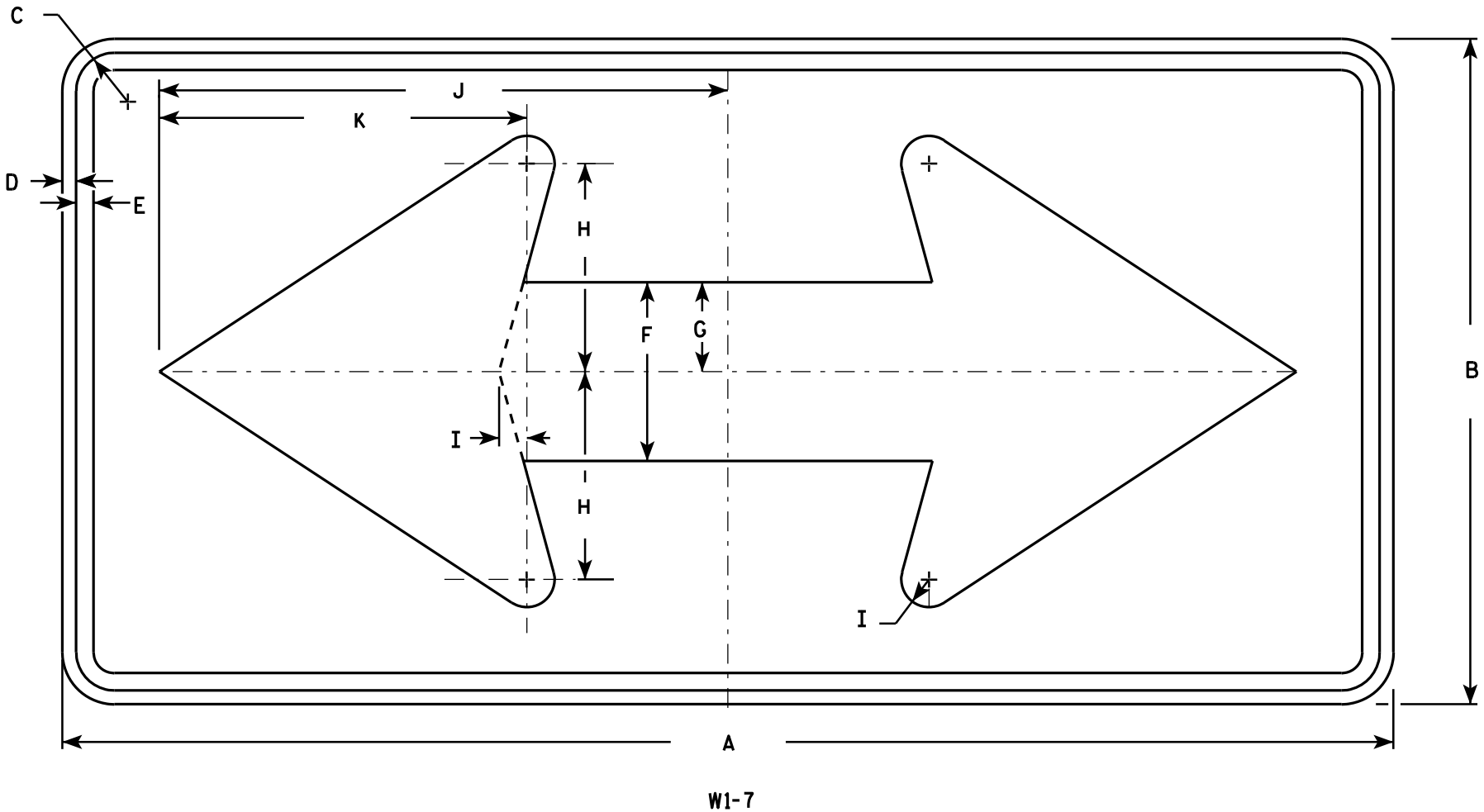
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/7/10 PLATE NO. W1-6.8

PROJECT NO:

HWY:

COUNTY:

SHEET NO:



NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

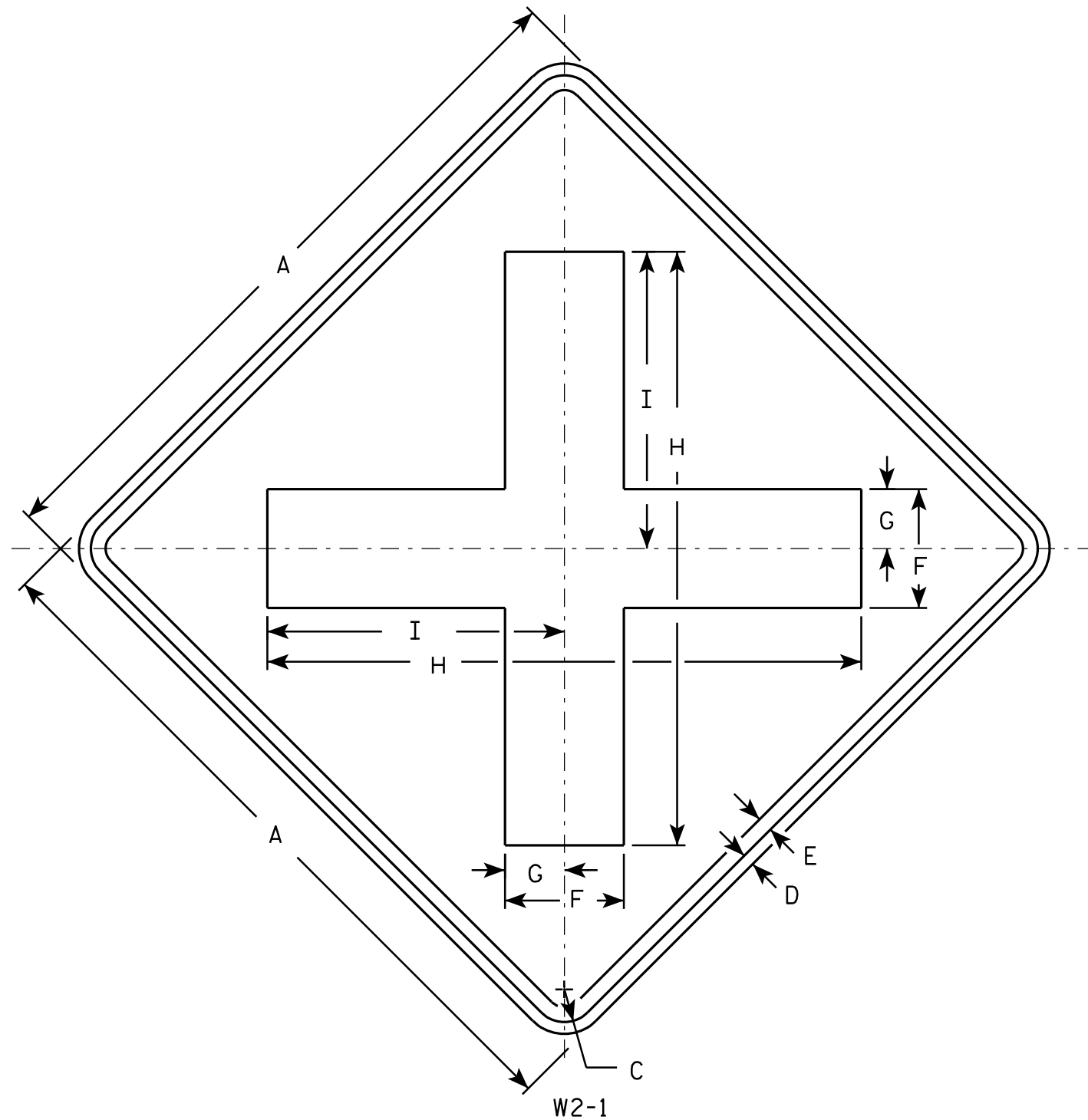
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	1/2	5	2 1/2	5 3/4	3/4	15 5/8	10 1/8																4.5
2S	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
2M	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
3	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
4	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
5	96	48	2 1/4	3/4	1	13	6 1/2	15	2	41	26 1/2																32.0

STANDARD SIGN
W1 - 7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-7.7



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	4	2	20	10																		4.0
2S	30		1 3/8	1/2	5/8	5	2 1/2	25	12 1/2																		6.25
2M	30		1 3/8	1/2	5/8	5	2 1/2	25	12 1/2																		6.25
3	36		1 5/8	5/8	3/4	6	3	30	15																		9.0
4	48		2 1/4	3/4	1	8	4	40	20																		16.0
5																											

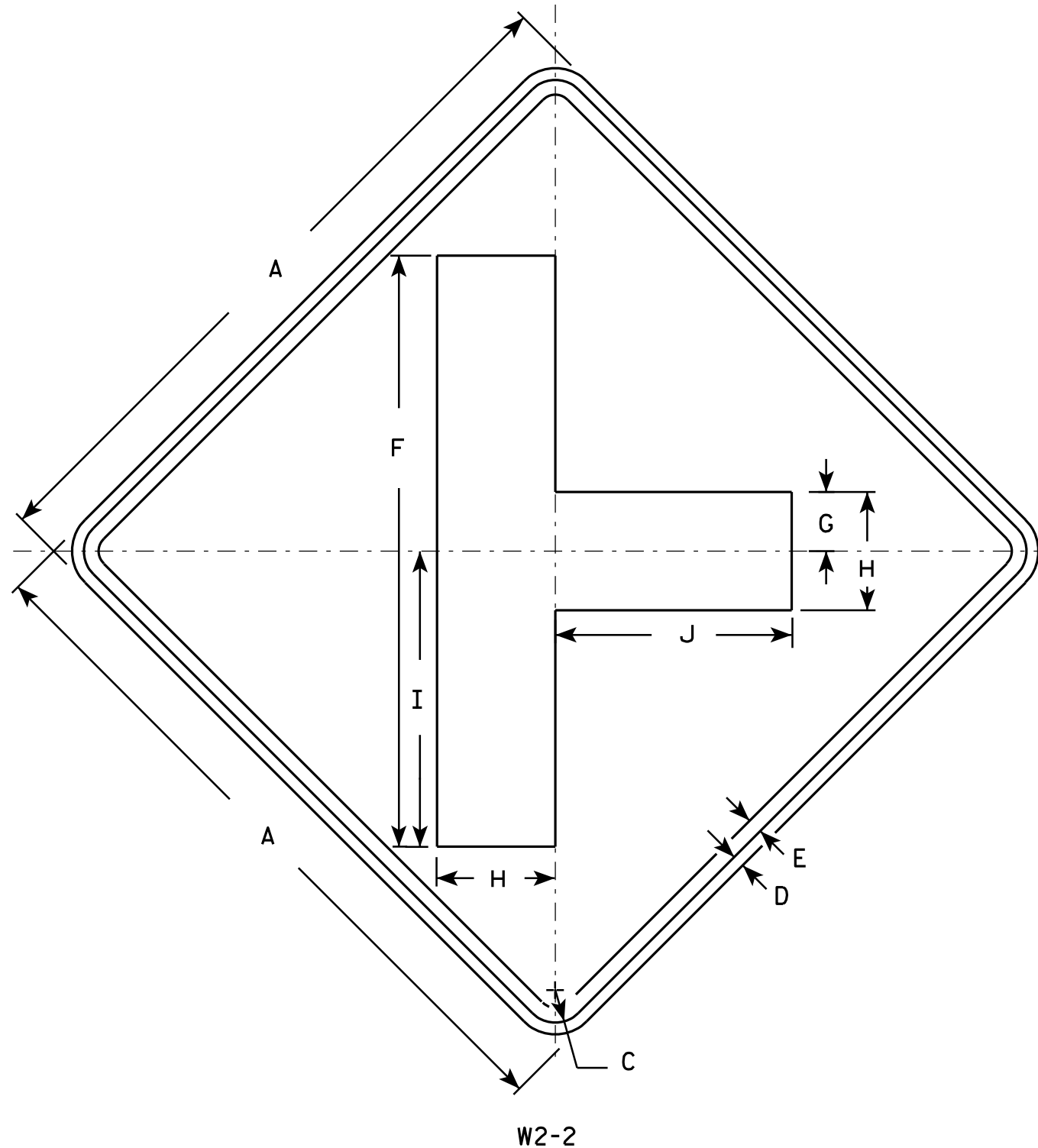
STANDARD SIGN W2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W2-1.9

PROJECT NO: HWY: COUNTY: SHEET NO: E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

W2-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	20	2	4	10	8																	4.0
2S	30		1 3/8	1/2	5/8	25	2 1/2	5	12 1/2	10																	6.25
2M	30		1 3/8	1/2	5/8	25	2 1/2	5	12 1/2	10																	6.25
3	36		1 5/8	5/8	3/4	30	3	6	15	12																	9.0
4	48		2 1/4	3/4	1	40	4	8	20	16																	16.0
5																											

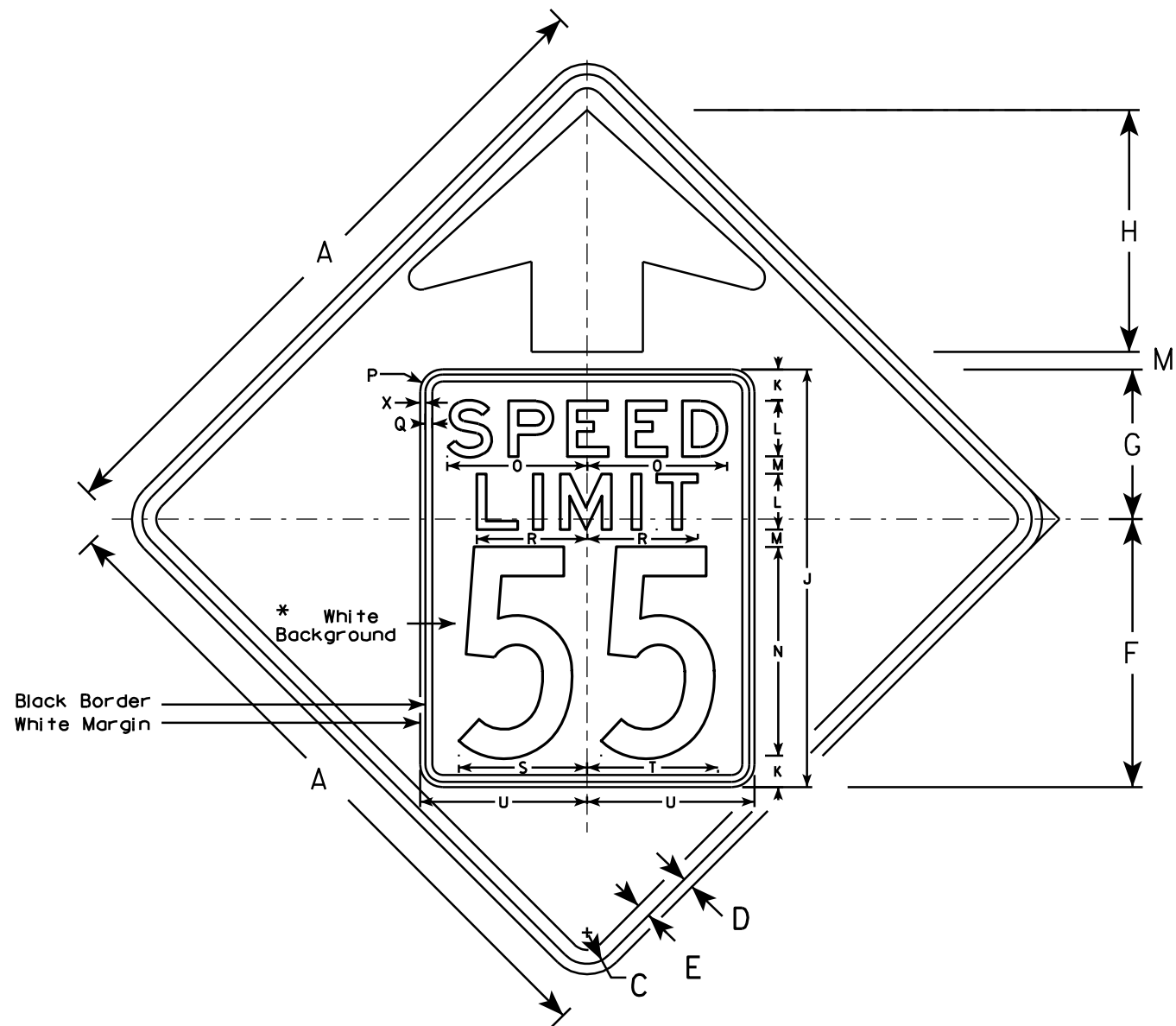
STANDARD SIGN W2-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W2-2.6

PROJECT NO: HWY: COUNTY: SHEET NO: E

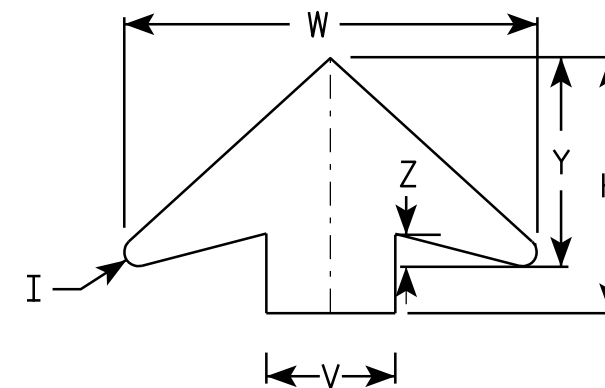


W3-5

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color: *
Background - YELLOW*
Message - BLACK
3. Message Series - C for numbers Series E for wording
4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

*Speed Limit Sign shall have a White Background



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36		1 5⁄8	5⁄8	3⁄4	14 1⁄2	9 1⁄2	11 1⁄2	5⁄8	24	2	3	1	12	7 1⁄8	1 1⁄2	3⁄8	5 3⁄4	7 1⁄4	7 1⁄8	9	6	19 1⁄4	3⁄8	9 3⁄4	1 5⁄8	9.0
2M	36		1 5⁄8	5⁄8	3⁄4	14 1⁄2	9 1⁄2	11 1⁄2	5⁄8	24	2	3	1	12	7 1⁄8	1 1⁄2	3⁄8	5 3⁄4	7 1⁄4	7 1⁄8	9	6	19 1⁄4	3⁄8	9 3⁄4	1 5⁄8	9.0
3	36		1 5⁄8	5⁄8	3⁄4	14 1⁄2	9 1⁄2	11 1⁄2	5⁄8	24	2	3	1	12	7 1⁄8	1 1⁄2	3⁄8	5 3⁄4	7 1⁄4	7 1⁄8	9	6	19 1⁄4	3⁄8	9 3⁄4	1 5⁄8	9.0
4	48		2 1⁄4	3⁄4	1	19 1⁄4	10 3⁄4	17 3⁄8	7⁄8	30	2 1⁄4	4	1 1⁄4	15	10	1 5⁄8	1⁄2	8	9 1⁄4	9 3⁄8	12	8	25 5⁄8	3⁄8	13	2	16.0
5	48		2 1⁄4	3⁄4	1	19 1⁄4	10 3⁄4	17 3⁄8	7⁄8	30	2 1⁄4	4	1 1⁄4	15	10	1 5⁄8	1⁄2	8	9 1⁄4	9 3⁄8	12	8	25 5⁄8	3⁄8	13	2	16.0

STANDARD SIGN

W3-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

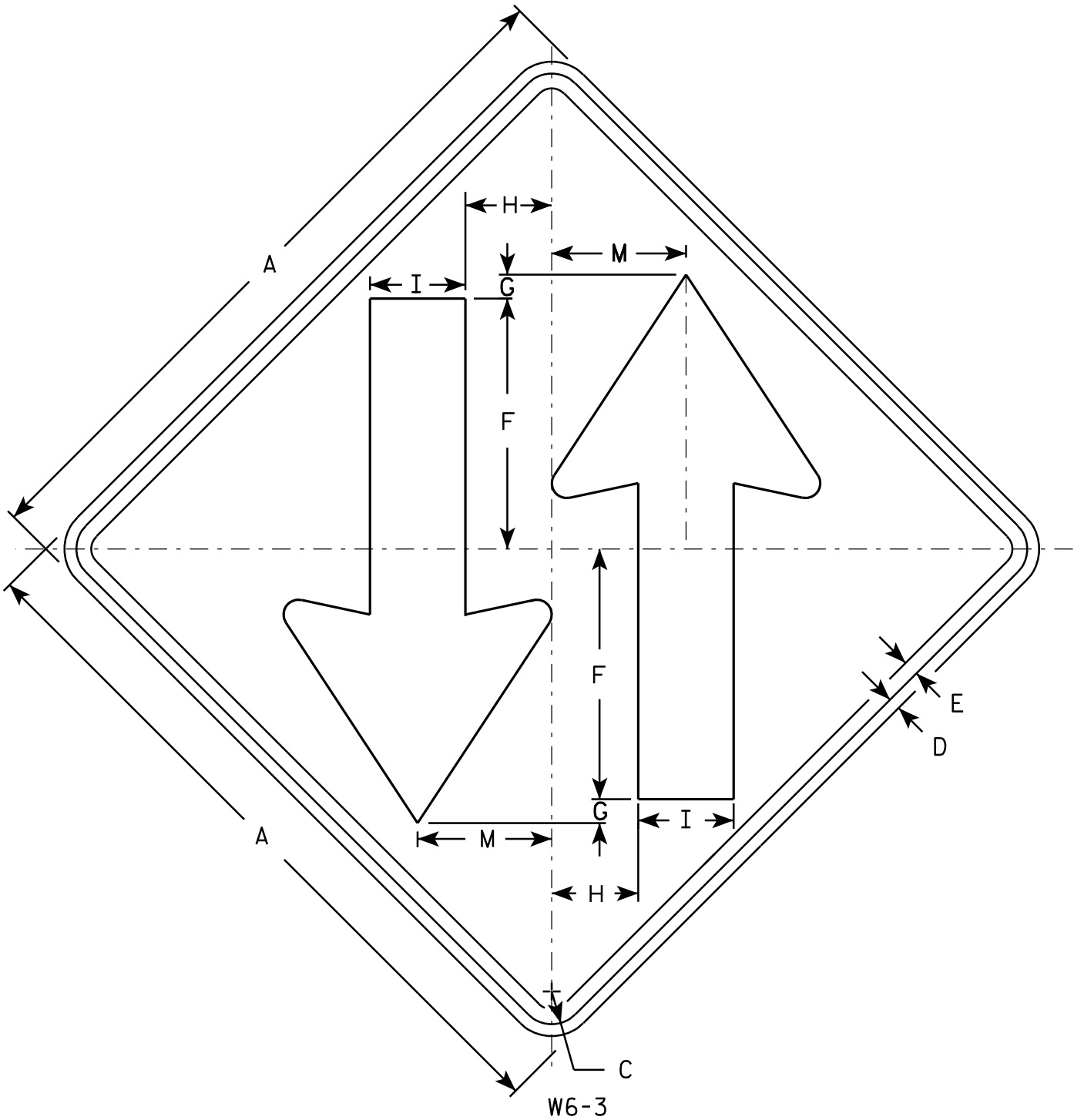
DATE 5/29/12 PLATE NO. W3-5.5

PROJECT NO:

SHEET NO:

E

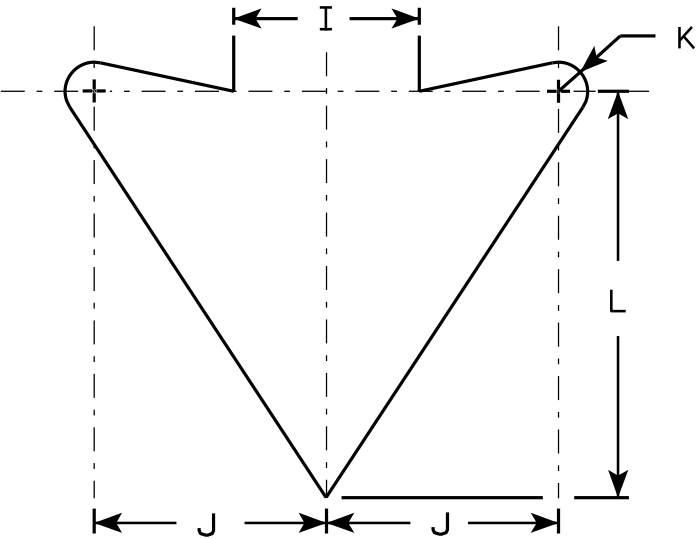
7



W6-3

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	10 1/2	1	3 5/8	4	5	5/8	8 3/4	5 5/8														6.25
2S	36		1 5/8	5/8	3/4	12	1	4 1/4	5	6	3/4	10 1/2	6 3/4														9.0
2M	36		1 5/8	5/8	3/4	12	1	4 1/4	5	6	3/4	10 1/2	6 3/4														9.0
3																											
4	48		2 1/4	3/4	1	15 1/2	1	6	6	8	1	14	9														16.0
5	48		2 1/4	3/4	1	15 1/2	1	6	6	8	1	14	9														16.0

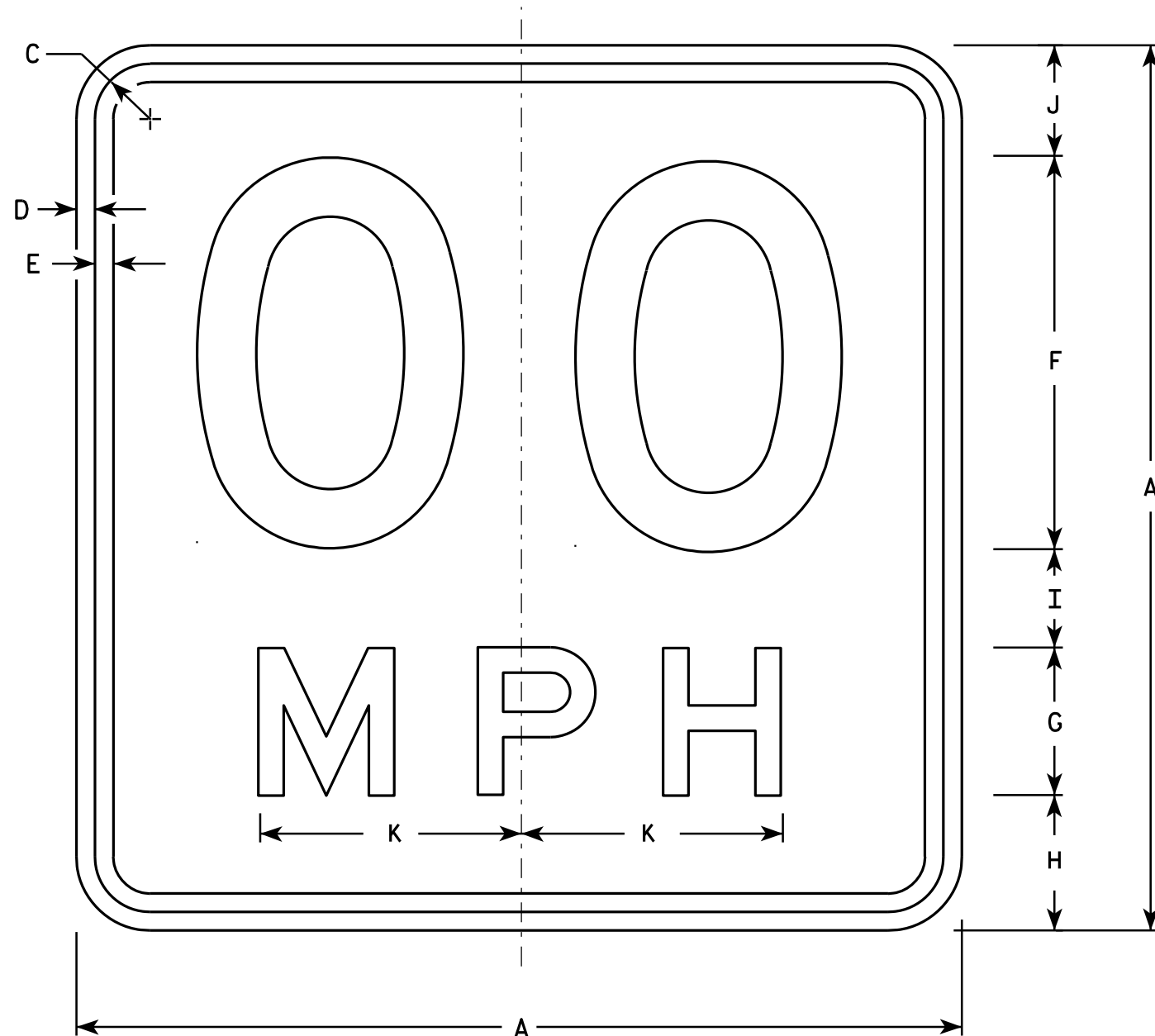
STANDARD SIGN
W6 - 3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 03/12/13 PLATE NO. W6-3.10

7



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
Line 2 is Series E

W13-1

- * For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

STANDARD SIGN

W13-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

PROJECT NO:

HWY:

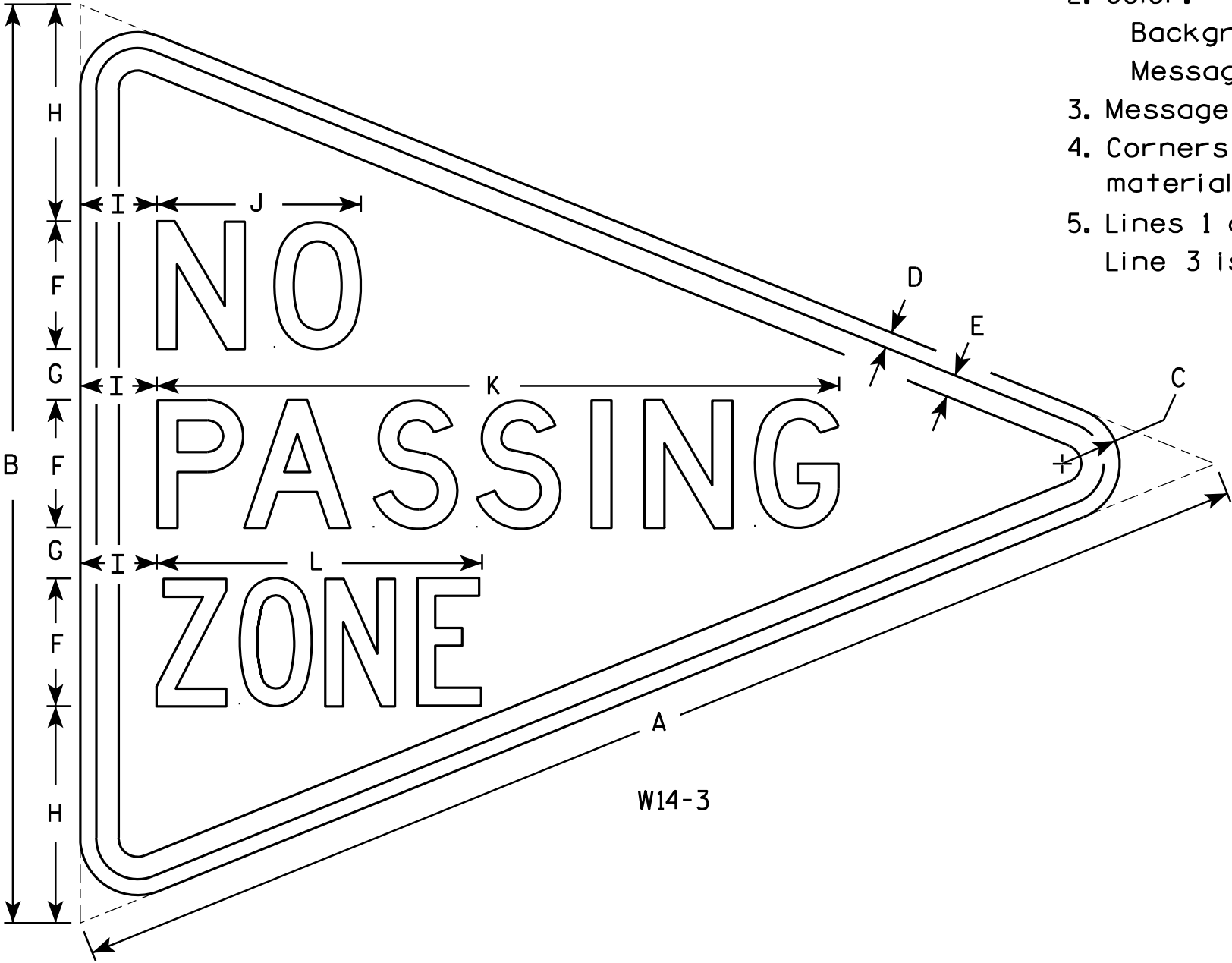
COUNTY:

SHEET NO:

E

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Message Series - See note 5
- 4. Corners and borders shall be rounded on all base materials for this sign.
- 5. Lines 1 and 2 are Series D.
Line 3 is series C.



W14-3

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
2M	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
3	64	48	3	3/4	1 1/4	6	3	12	4	10 3/4	33 5/8	16 1/2															10.7
4																											
5																											

STANDARD SIGN
W14-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 6/7/10 PLATE NO. W14-3.9

Notes



Wisconsin Department of Transportation

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