SWL JULY 2014

PROJECT ID: 5916-00-68
WITH: 5916-00-69, 5916-00-71, 5916-00-72

COUNTY: SAUK

ORDER OF SHEETS

TOTAL SHEETS = 12

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

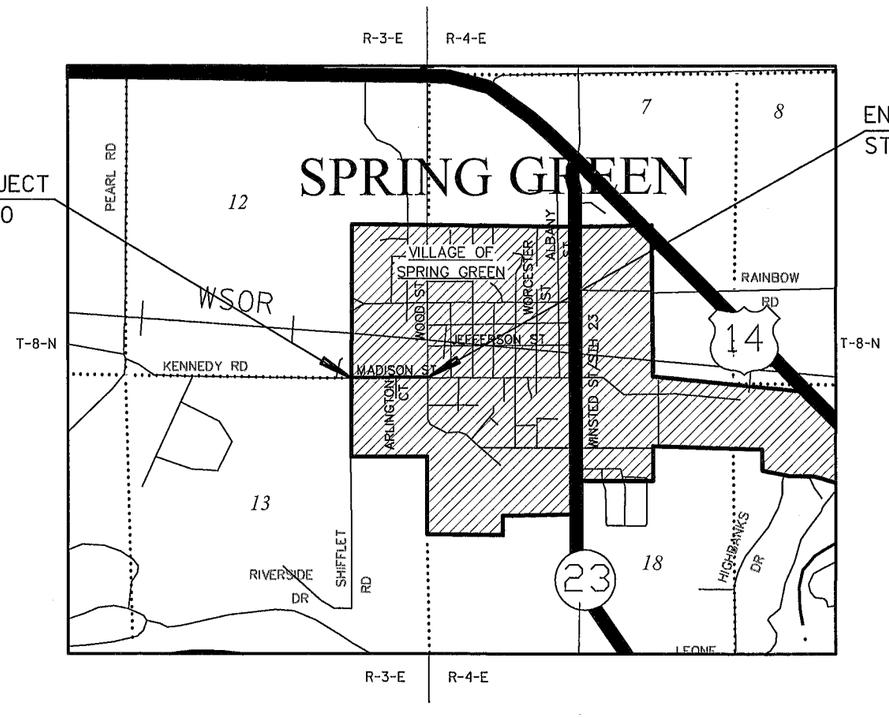
VILLAGE OF SPRING GREEN, WEST MADISON STREET

(SHIFFLET ROAD TO WOOD STREET)

LOCAL STREET
SAUK COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5916-00-68	———	—

STATE PROJECT NUMBER
5916-00-68



BEGIN PROJECT
STA. 50+00
Y= 129,732.36
X= 561,107.89

END PROJECT
STA. 65+15

DESIGN DESIGNATION

A.A.D.T.	2014	= 2730
A.A.D.T.	2034	= 3330
D.H.V.	2034	= 240
D.D.		= 62/38
T.		= 5.8%
DESIGN SPEED		= 30 MPH
ESALS		= 401,500

CONVENTIONAL SYMBOLS

PLAN
- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS/ ELECTRIC (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE
- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES
- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

LAYOUT
SCALE 0 1/2 MI.

TOTAL NET LENGTH OF CENTERLINE = 0.287 MI.

Coordinates on this plan are referenced to the Wisconsin County Coordinate System (WCCS), Sauk County.

ACCEPTED FOR
TOWN of SPRING GREEN
1-29-14 (Date) (TOWN CHAIRMAN)

ACCEPTED FOR
VILLAGE of SPRING GREEN
1/29/14 (Date) (Village President)

ORIGINAL PLANS PREPARED BY
JEWELL associates engineers, inc.
Engineers - Planners - Surveyors

WISCONSIN PROFESSIONAL ENGINEER
TODD L. DEIBERT
No. 37728
SPRING GREEN WIS.

1-24-2014 (Date) Todd L Deibert (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor: JEWELL ASSOCIATES ENGINEERS, INC.
Designer: JEWELL ASSOCIATES ENGINEERS, INC.
Management Consultant: KJOHNSON ENGINEERS, INC.

APPROVED FOR THE DEPARTMENT
DATE: 1/31/14
Kristen A. Johnson
Management Consultant Signature

E

FILE NAME: S34084 - Title Sheet

WISDOT/CADDS SHEET 10

2

LIST OF STANDARD ABBREVIATIONS

ABUT	Abutment
AC	Acre
AGG	Aggregate
AH	Ahead
<	Angle
ASPH	Asphaltic
AVG	Average
ADT	Average Daily Traffic
BAD	Base Aggregate Dense
BK	Back
BF	Back Face
BM	Bench Mark
BR	Bridge
C or C/L	Center Line
CC	Center to Center
CTH	County Trunk Highway
CR	Creek
CR	Crushed
CY or CU YD	Cubic Yard
CP	Culvert Pipe
C & G	Curb and Gutter
D	Degree of Curve
DHV	Design Hour Volume
DIA	Diameter
E	East
X	East Grid Coordinate
ELEC	Electric (al)
EL or ELEV	Elevation
ESALS	Equivalent Single Axle Loads
EBS	Excavation Below Subgrade
FF	Face to Face
FE	Field Entrance
F	Fill
FG	Finished Grade
FL or F/L	Flow Line
FT	Foot
FTG	Footing
GN	Grid North
HT	Height
CWT	Hundredweight
HYD	Hydrant
INL	Inlet
ID •	Inside Diameter
INV	Invert
IP	Iron Pipe or Pin
IRS	Iron Rod Set
JT	Joint
JCT	Junction
LHF	Left–Hand Forward
L	Length of Curve
LIN FT or LF	Linear Foot
LC	Long Chord of Curve
MH	Manhole
MB	Mailbox
ML or M/L	Match Line
N	North
Y	North Grid Coordinate
OD	Outside Diameter
PLE	Permanent Limited Easement
PT	Point
PC	Point of Curvature
PI	Point of Intersection
PRC	Point of Reverse Curvature
PT	Point of Tangency
POC	Point On Curve
POT	Point on Tangent
PVC	Polyvinyl Chloride
PCC	Portland Cement Concrete
LB	Pound
PSI	Pounds Per Square Inch
PE	Private Entrance
R	Radius
RR	Railroad
R	Range
RL or R/L	Reference Line
RP	Reference Point
RCCP	Reinforced Concrete Culvert Pipe
REQD	Required
RES	Residence or Residential
RW	Retaining Wall
RT	Right
RHF	Right–Hand Forward
R/W	Right–of–Way
R	River
RD	Road
RDWY	Roadway
SALV	Salvaged
SAN S	Sanitary Sewer
SEC	Section
SHLDR	Shoulder
SHR	Shrinkage
SW	Sidewalk
S	South
SQ	Square
SF or SQ FT	Square Feet
SY or SQ YD	Square Yard
STD	Standard
SDD	Standard Detail Drawings
STH	State Trunk Highways
STA	Station
SS	Storm Sewer
SG	Subgrade
SE	Superelevation
SL or S/L	Survey Line
SV	Septic Vent
T	Tangent
TEL	Telephone
TEMP	Temporary
TI	Temporary Interest
TLE	Temporary Limited Easement
t	Ton
T or TN	Town
TRANS	Transition
TL or T/L	Transit Line
T	Trucks (percent of)
TYP	Typical
UNCL	Unclassified
UG	Underground Cable
USH	United States Highway
VAR	Variable
V	Velocity or Design Speed
VERT	Vertical
VC	Vertical Curve
VOL	Volume
WM	Water Main
WV	Water Valve
W	West
WB	Westbound
YD	Yard

UTILITIES

ELECTRIC
ALLIANT ENERGY
142 S. CINCINNATI STREET
P.O. BOX 99
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CELL: (608) 214–4441
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TELEPHONE
FRONTIER COMMUNICATIONS
100 COMMUNICATIONS DRIVE
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ATTN: DANA GILLETT
EMAIL: dana.gillett@frontier.com

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CHARTER COMMUNICATIONS
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ATTN: STEVE HEGGE
EMAIL: steve.hegge@chartercom.com

WATER/SANITARY SEWER/STORM SEWER
VILLAGE OF SPRING GREEN
112 W. MONROE STREET
SPRING GREEN, WI 53588
CELL: (608) 588–4983
OFFICE: (608) 588–2335
ATTN: GREG WIPPERFURTH

GAS
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OFFICE: (608) 588–9702
ATTN: CHRIS WILHELM
EMAIL: chriswilhelm@alliantenergy.com

DIGGERS HOTLINE
Dial 811 or (800) 242-8511
www.DiggersHotline.com

* DENOTES UTILITY IS NOT A MEMBER OF DIGGERS HOTLINE

GENERAL NOTES

COORDINATES AND BEARINGS ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), SAUK COUNTY.

ELEVATIONS SHOWN ON THE PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88)

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

ALL RADII DIMENSIONS ON THE PLAN FOR CURB AND GUTTER ARE TO THE FLANGE OF THE CURB AND GUTTER.

THE COST OF CONNECTING WATER MAIN OR SANITARY SEWER TO EXISTING PIPE OR STRUCTURES SHALL BE INCIDENTAL TO THE COST OF INSTALLING THE WATER MAIN OR SANITARY SEWER.

WATER AND SANITARY SEWER SERVICES SHALL NOT BE DISCONNECTED UNTIL DIRECTED BY THE ENGINEER.

ALL WORK TO BE CONSTRUCTED PER GOVERNING CODES/ORDINANCES, AS AMENDED BY LOCAL AUTHORITIES. SAID CODES/ORDINANCES ARE HEREIN INCORPORATED INTO THESE DOCUMENTS. ALL CODE REQUIRED WORK TO BE INCLUDED IN CONTRACT SUM. REQUIRED CODES INCLUDE, BUT ARE NOT LIMITED TO:

- ADMINISTRATIVE CODE CHAPTER NR 811
- ADMINISTRATIVE CODE CHAPTER NR 110
- LOCAL CODES

GENERAL NOTES ARE INTENDED TO CLARIFY OR EMPHASIZE THE REQUIREMENTS OF THE DRAWINGS AND SPECIFICATIONS. IN THE EVENT OF A CONFLICT BETWEEN THE DRAWINGS OR SPECIFICATIONS AND THESE NOTES, THE CONTRACTOR SHALL COMPLY WITH THE MORE STRINGENT REQUIREMENT.

ALL WORK TO BE PERFORMED IN A MANNER SO AS TO HAVE A MINIMUM OF DISRUPTION AND DISTURBANCE WITH EXISTING OPERATIONS AND LOCAL ENVIRONMENT. NOISE AND DUST SHALL BE KEPT TO AN ABSOLUTE MINIMUM.

DO NOT SCALE DRAWINGS.

PERMIT ARE TO BE KEPT ON–SITE AT ALL TIMES.

DETAILS AND NOTES OF SIMILAR CONDITIONS ARE TYPICAL WHETHER OR NOT CALLED OUT AT ALL PLACES. REFERENCE TO ANY DETAIL OR DRAWINGS IS FOR CONVENIENCE ONLY AND DOES NOT LIMIT THE APPLICATION OF SUCH DETAIL OR DRAWING.

SYSTEMS SHOWN ON DRAWINGS ARE INTENDED TO BE FURNISHED, INSTALLED, AND TURNED OVER TO OWNER IN PROPER FUNCTIONING CONDITION. ALL WORK TO BE CONSIDERED IN CONTRACT SUM.

CONTRACTOR SHALL CHECK AND VERIFY ALL FIELD CONDITIONS AND DIMENSIONS WITH THE CONSTRUCTION DRAWINGS AT THE PROJECT SITE PRIOR TO CONSTRUCTION, ERECTION, AND/OR FABRICATION. CONTRACTOR SHALL INSPECT RELATED WORK AND ADJACENT SURFACES. CONTRACTOR SHALL REPORT ANY DISCREPANCIES BETWEEN THE CONTRACT DOCUMENTS AND THE EXISTING CONDITIONS WHICH SHALL PREVENT PROPER EXECUTION OF THIS WORK TO THE OWNER BEFORE PROCEEDING WITH THE WORK.

INSTALLATION OF ALL MATERIALS AND SYSTEMS SHALL BE IN ACCORDANCE WITH MANUFACTURER'S WRITTEN INSTRUCTIONS.

CONTACTS

DESIGN CONSULTANT:
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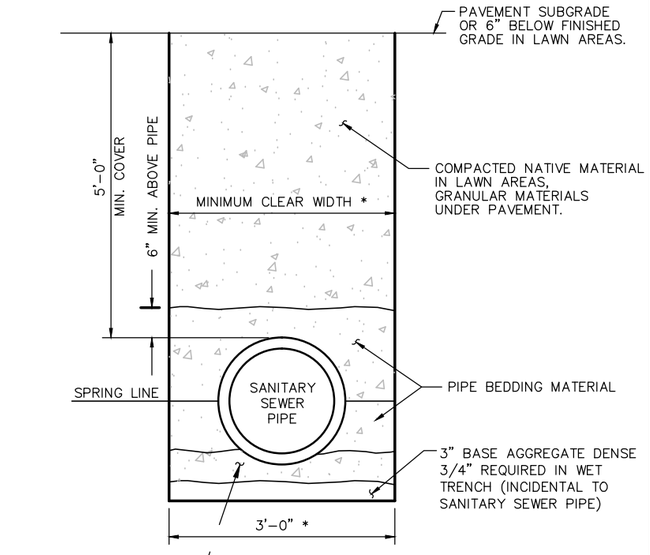
FAX: (608) 588–9322
E MAIL: ed.lilla@jewellassoc.com

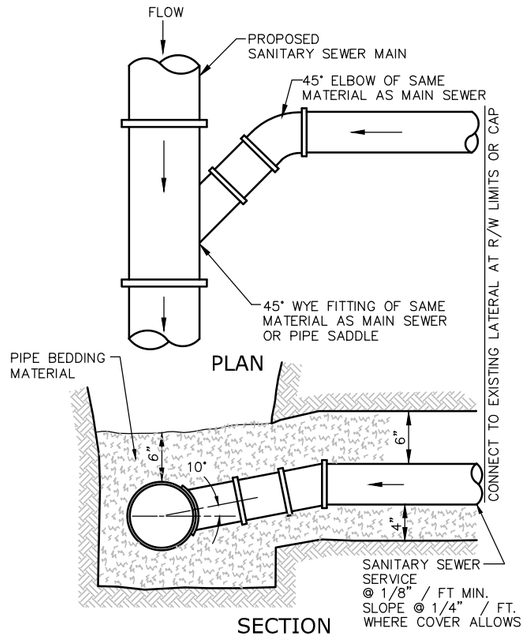
WDNR LIAISON:
STATE OF WISCONSIN
DEPT. OF NATURAL RESOURCES
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
ATTN: CATHY BLESER
PH: (608) 275–3308
E MAIL: catherine.bleser@wisconsin.gov

VILLAGE OF SPRING GREEN:
EUGENE HAUSNER, VILLAGE PRESIDENT
520 WORCESTER STREET
SPRING GREEN, WI 53588
PH: (608) 588–7780

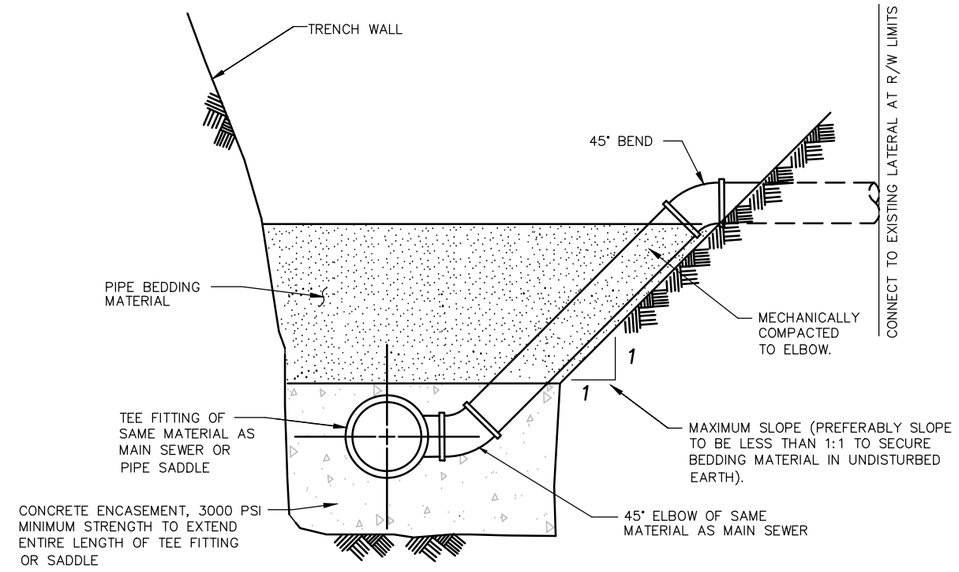
TOWN OF SPRING GREEN:
DENNIS POLIVKA, TOWN CHAIRPERSON
E3681 CTH JJ
SPRING GREEN, WI 53588
PH: (608) 335–3291



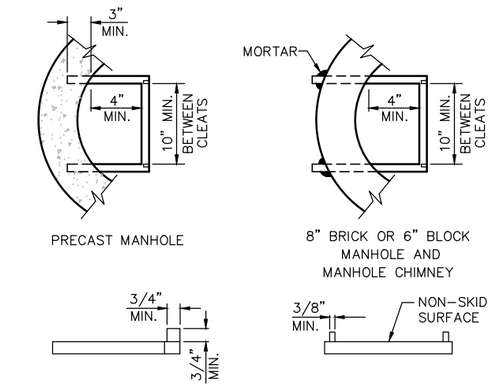
SANITARY SEWER TRENCH DETAIL



SANITARY SEWER LATERAL DETAIL



SANITARY SEWER RISER DETAIL



PROVIDE CERTIFIED TEST DATA THAT THE STEPS CAN WITHSTAND AN 800-POUND VERTICAL LOAD WITHOUT MORE THAN 3/8-INCH PERMANENT SET WHEN TESTED IN ACCORDANCE WITH SECTION 10 A.S.T.M. 498.

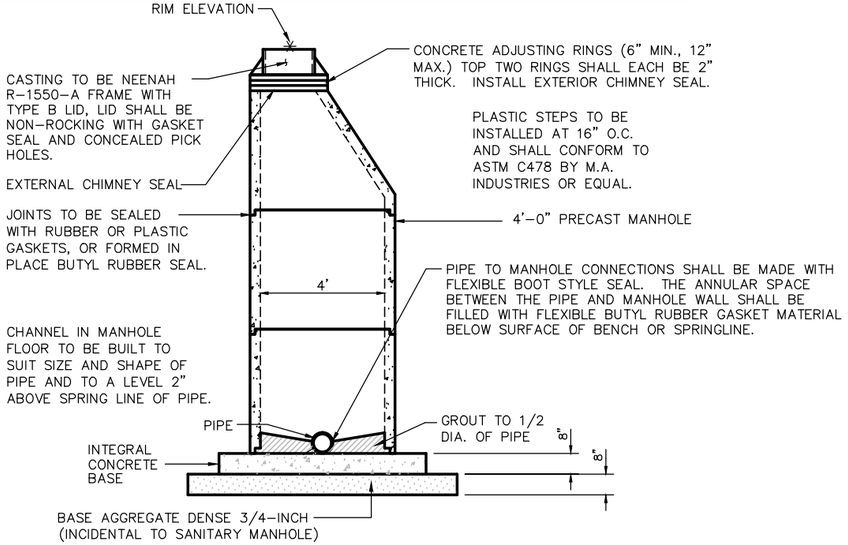
PROVIDE CERTIFIED TEST DATA THAT THE INSTALLED STEPS CAN WITHSTAND A HORIZONTAL LOAD OF 400 POUNDS WITH THE LOAD APPLIED OVER A WIDTH OF 3 1/2" AND CENTERED ON THE RUNG.

STEPS MUST BE EQUALLY SPACED VERTICALLY IN THE ASSEMBLED MANHOLE AT A MAXIMUM DISTANCE OF 16" ON CENTER.

STEPS SHALL BE FABRICATED OF 1/2" DIA. GRADE 60 STEEL REINFORCING ROD WITH MOLDED PLASTIC COVERING.

STEPS SHALL NOT BE INSTALLED IN THE PLASTIC RINGS.

MANHOLE STEPS DETAIL

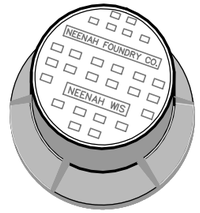


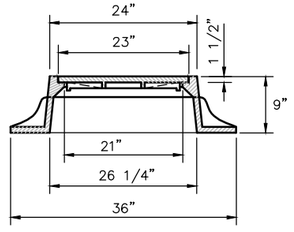
SANITARY MANHOLES 48" DIAMETER

ADJUST FRAME TO GRADE WITH CONCRETE RINGS OF VARIABLE THICKNESS MAXIMUM RING HEIGHT = 12" MINIMUM RING HEIGHT = 6"

CONCRETE AND STEEL REINFORCEMENT SHALL CONFORM TO DESIGNATION C-478 REQUIREMENTS OF ASTM SPECIFICATIONS.

TYPE 'B' LID DESIGN





NEENAH R-1550-A
SANITARY MANHOLE
FRAME, SOLID LID

HEAVY DUTY

NOTE: LID SHALL BE NON-ROCKING WITH GASKET SEAL AND CONCEALED PICKHOLES.

GENERAL NOTES

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMAN- SHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

FLATTOPS WITH 24-INCH DIAMETER ECCENTRIC OPENING SHALL BE USED ONLY ON SANITARY STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

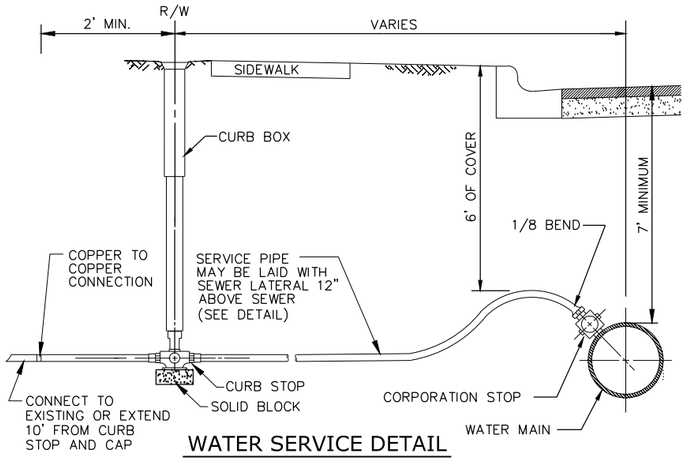
STEPS MEETING THE SPECIFIED REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

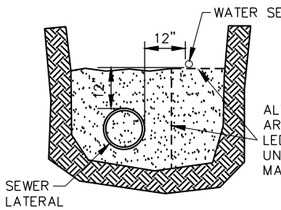
PRECAST REINFORCED CONCRETE RISERS MAY BE PLACED WITH TONGUE UP OR DOWN.

ALL PRECAST INLETS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M 199.

ALL STRUCTURES SHALL BE PLACED ON A BED OF 8" OF BASE AGGREGATE DENSE.



WATER SERVICE DETAIL



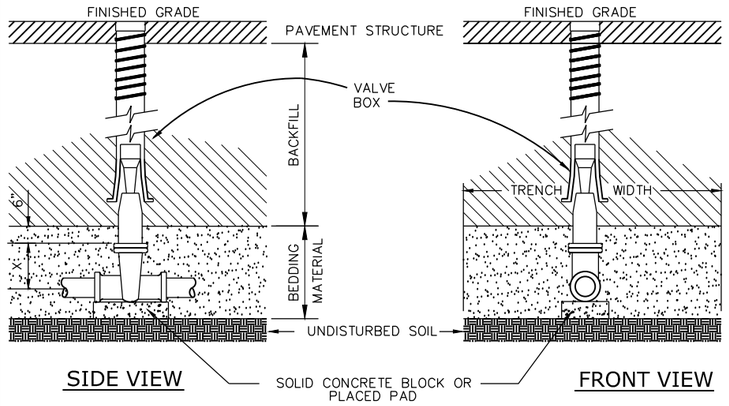
JOINT TRENCH INSTALLATION

NOTES:
- HORIZONTAL AND VERTICAL OFFSETS SHALL BE MADE WITH AN APPROVED PIPE BENDING TOOL. SHARP BENDS OR KINKS IN THE WATER SERVICE ARE NOT ALLOWED.

- VERTICAL OFFSETS SHALL BE MADE ON THE PROPERTY LINE SIDE OF THE CURB STOP.

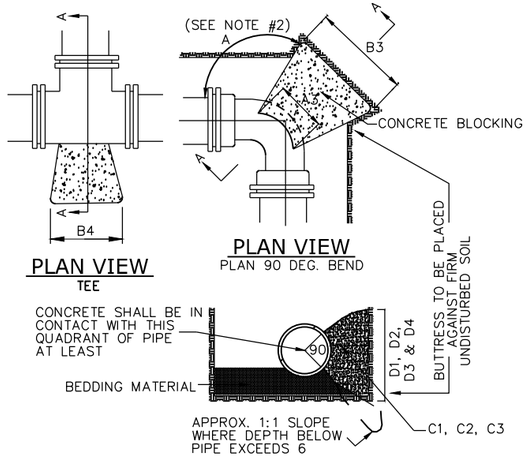
- CAST IRON CURB BOX SLEEVES SHALL BE INSTALLED WHERE CURB BOXES ARE INSTALLED IN CONCRETE OR ASPHALT SURFACES.

WATER SERVICE INSTALLATION DETAIL



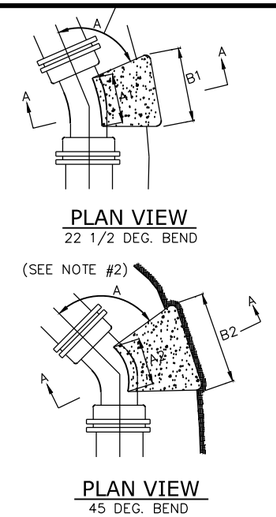
GATE VALVE BOX DETAIL

PIPE DIA. INCHES	X=SETTING INCHES
2	6
3	7
4	8
6	12
8	13
10	17
12	21
16	30



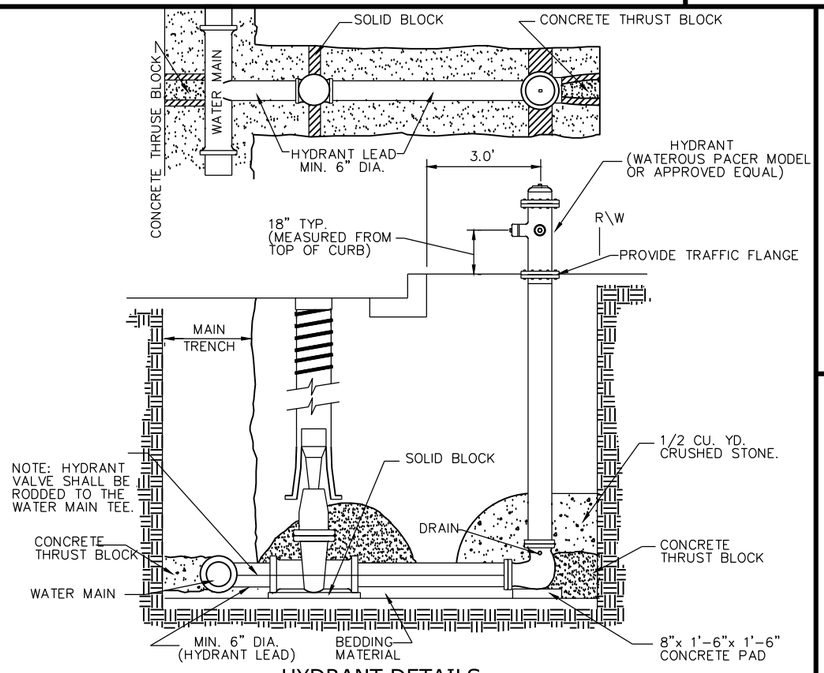
BUTTRESS FOR BENDS

BUTTRESS DIMENSIONS								
PIPE	22 1/2° BENDS		45° BENDS		90° BENDS		TEES	
SIZE	B1	D1	B2	D2	B3	D3	B4	D4
6"	1'-0"	1'-0"	1'-0"	1'-0"	1'-4"	1'-2"	1'-3"	1'-0"
8"	1'-0"	1'-0"	1'-4"	1'-2"	1'-10"	1'-6"	1'-6"	1'-4"
10"	1'-2"	1'-2"	1'-7"	1'-6"	2'-4"	1'-10"	2'-3"	2'-0"
12"	1'-4"	1'-4"	1'-10"	1'-10"	2'-9"	2'-3"	2'-3"	2'-0"
16"	1'-10"	1'-8"	2'-6"	2'-4"	3'-10"	2'-10"	3'-2"	2'-6"
20"	2'-4"	2'-0"	3'-3"	2'-10"	6'-0"	3'-4"	4'-0"	3'-0"
24"	2'-10"	2'-4"	4'-0"	3'-3"	6'-4"	3'-10"	5'-3"	3'-4"
30"	3'-6"	3'-0"	5'-4"	3'-10"	8'-0"	4'-8"	6'-3"	4'-3"



NOTES

1. MECHANICAL JOINT RESTRAINT FITTINGS MAY BE USED IN LIEU OF THRUST BLOCKS. MECHANICAL JOINT RESTRAINT SHALL BE MEGALUG OR EQUAL.
2. DIMENSIONS IN TABLE ARE BASED ON A WATER PRESSURE OF AT LEAST 150 PSI, AND ON EARTH RESISTANCE OF 2 TONS PER SQ. FT.
3. DIMENSION C1, C2, C3, SHOULD BE LARGE ENOUGH TO MAKE ANGLE "A" EQUAL OR LARGER THAN 45 DEG.
4. DIMENSION A1, A2, A3, SHOULD BE AS LARGE AS POSSIBLE WITHOUT INTERFERRING WITH M.J. BOLTS.
5. SHAPE OF BACK OF BUTTRESS MAY VARY AS LONG AS POUR IS AGAINST FIRM UNDISTURBED EARTH.



HYDRANT DETAILS

FOR: 8-INCH DUCTILE IRON PIPE
150 P.S.I. PRESSURE
20 FOOT NOMIMAL LAYING LENGTH

FITTING	LENGTH OF PIPE RESTRAINED BEYOND FITTING, FEET
90	20
45	10
22.5	5
TEE BRANCH	10
TEE RUN	10
DEAD END	40
VALVE	40 (BOTH SIDES)

NOTE:
A 20 FOOT RESTRAINED LENGTH WOULD REQUIRE ONE (1) RESTRAINED JOINT AT THE FITTING. IF LAYING LENGTH IS LESS THAN 20 FEET, ADDITIONAL JOINTS SHALL BE RESTRAINED UNTIL THE REQUIRED RESTRAINED LENGTH IS OBTAINED.

THRUST RESTRAINT TABLE

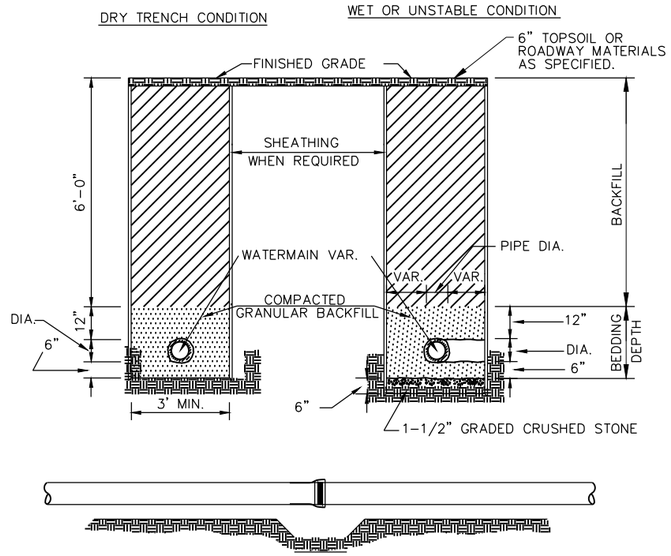
SERVICE PIPE	CORP. STOP	CURB STOP
1"	1"	1"
1-1/2"	1-1/2"	1-1/2"
2"	2"	2"

1. NO JOINTS ALLOWED WITHOUT PERMISSION OF THE ENGINEER.

NOTE:
IF 6 FEET MINIMUM COVER IS NOT AVAILABLE, THE NEW SERVICE PIPE SHALL BE INSULATED. CONNECTIONS SHALL BE TESTED FOR LEAKAGE PRIOR TO BACKFILL AFTER SUPPORT HAS BEEN COMPACTED. MINIMUM RADII ARE AS FOLLOWS: 1"= 6", 1-1/4" = 8", 1-1/2" = 10", 2" = 12". TAP AND SERVICE SHALL BE A MINIMUM OF 1"

TAP SERVICE PIPING

(COPPER)



WATER MAIN TRENCH SECTION

PROJECT NO: 5916-00-68	HWY: WEST MADISON STREET	COUNTY: SAUK	WATERMAIN DETAILS	SHEET 4	E

NOTES:
① CONTRACTOR MUST VERIFY SIZES AND LOCATIONS OF EXISTING SANITARY SEWERS, LATERALS, WATER MAINS, SERVICES, AND STORM SEWERS.
② SANITARY LATERALS SHALL BE 6-INCH DIA. PVC PIPE AND WATER SERVICES SHALL BE 1-INCH DIA. (MIN.) COPPER PIPE UNLESS OTHERWISE NOTED. REPLACE EXISTING LARGER SERVICES IN-KIND.
③ ALL CONNECTIONS TO EXISTING WATER MAINS SHALL BE MADE WITH RESTRAINED COUPLINGS UNLESS OTHERWISE NOTED.
④ WHEN NECESSARY, ADJUST WATER MAIN TO AVOID SANITARY SEWER LATERALS, AS DIRECTED BY ENGINEER.

ROSE M - JEFFERY N ANDERSON & HUTCHINSON

ST JOHNS CATHOLIC CONG

LIMITS OF CEMETERY

SAN MH-105
STA:51+41.24 OFF:0.00
RIM:721.11

SAN MH-104
STA:55+32.49 OFF:0.00
RIM:723.61

MATCHLINE STA. 56+00

PHILWALD PARTNERS LLC

BEGIN PROJECT
STA. 50+00
Y= 129,732.36
X= 561,107.89

WALTER E & HANNELORE ALLEN

H JAMES & MURIEL A KLOTH

JAMES M BRICKL

GORDON E & MARY J NEEFE

SHIFFLET ROAD

FRONTIER COMMUNICATIONS (TYP.)

EXISTING HOSE BIBB.

FINISHED ℄ WEST MADISON STREET

CONTRACTOR TO CONNECT TO NEW SERVICE

REMOVE EXISTING 8" SANITARY SEWER (TYP.)

REMOVE EXISTING MANHOLE

REMOVE EXISTING HYDRANT, AUXILIARY VALVE, AND 6" LEAD

REMOVE EXISTING 8" D.I. WATER MAIN (TYP.)

8" RESTRAINED M.J. CAP OR PLUG

ALLIANT ENERGY (TYP.)

CAUTION

CHARTER COMMUNICATIONS (TYP.)

VILLAGE OF SPRING GREEN (TYP.)

1" WATER SERVICE (TYP.)

6" SANITARY LATERAL (TYP.)

CONNECT TO EXISTING (TYP.)

STA 51+94, 23.4' RT. HYDRANT, AUXILIARY VALVE, AND 6" LEAD (13 LF)

STA. 51+94, 10.4' RT. 8"X8"X6" TEE

STA. 51+92, 10.4' RT. 8" GATE VALVE & BOX

SAN P-106

SAN P-105

10' (TYP.)

PI STA = 50+93.03

PI STA = 10+49.97

PI STA = 51+75.43

N88°51'47"E

S89°59'52"E

N0°15'29"E

N4°31'26"W

S89°15'03"E

FINSIHED ℄ PROFILE WEST MADISON STREET

EXISTING ℄ PROFILE WEST MADISON STREET

SAN MH-105
SAN P-106: 716.20

PROPOSED STORM SEWER

7' COVER (MIN.)

SAN MH-104
SAN P-106: 714.64
SAN P-105: 714.54

387' of 10" PVC @ 0.40%

296' of 10" PVC @ 0.40%

8" RESTRAINED MJ CAP OR PLUG

8"X8"X6" HYDRANT TEE

8" GATE VALVE WITH VALVE BOX

EXISTING ℄ ELEVATION

FINISHED ℄ ELEVATION

8" D.I. WATER MAIN (TYP.)

MAINTAIN POSITIVE PROFILE TO AVOID HIGHPOINT IN WATERMAIN PRIOR TO EXISTING HYDRANT AT 59+60

50+00	50+20	50+40	50+60	50+80	51+00	51+20	51+40	51+60	51+80	52+00	52+20	52+40	52+60	52+80	53+00	53+20	53+40	53+60	53+80	54+00	54+20	54+40	54+60	54+80	55+00	55+20	55+40	55+60	55+80	56+00
722.61	722.29	721.98	721.67	721.38	721.21	721.16	721.11	721.06	721.11	721.19	721.26	721.32	721.51	721.70	721.86	722.00	722.14	722.28	722.42	722.56	722.69	722.80	722.90	723.00	723.10	723.23	723.35	723.40	723.45	723.51
722.61	722.33	722.06	721.79	721.52	721.31	721.17	721.08	721.06	721.10	721.20	721.36	721.59	721.88	722.22	722.54	722.82	723.07	723.28	723.45	723.59	723.69	723.76	723.79	723.79	723.75	723.68	723.57	723.46	723.35	723.24

PROJECT NO: 5916-00-68	HWY: WEST MADISON STREET	COUNTY: SAUK	SANITARY SEWER AND WATERMAIN PLAN	SHEET --5

NOTES: ① CONTRACTOR MUST VERIFY SIZES AND LOCATIONS OF EXISTING SANITARY SEWERS, LATERALS, WATER MAINS, SERVICES, AND STORM SEWERS.

② SANITARY LATERALS SHALL BE 6-INCH DIA. PVC PIPE AND WATER SERVICES SHALL BE 1-INCH DIA. (MIN.) COPPER PIPE UNLESS OTHERWISE NOTED. REPLACE LARGER SERVICES IN-KIND.

③ ALL CONNECTIONS TO EXISTING WATER MAINS SHALL BE MADE WITH RESTRAINED COUPLINGS UNLESS OTHERWISE NOTED.

④ WHEN NECESSARY, ADJUST WATER MAIN TO AVOID SANITARY SEWER LATERALS, AS DIRECTED BY ENGINEER.

COUNCIL OF SPRING GREEN
ST JOHNS CATHOLIC CHURCH

CARDINAL ST

N

MATCHLINE STA. 56+00

MATCHLINE STA. 62+00

CAP FOR FUTURE USE

REMOVE EXISTING 8" D.I. WATER MAIN (TYP.)

FINISHED ℄ WE MADISON STREET

REMOVE EXISTING MANHOLE

FRONTIER COMMUNICATIONS (TYP.)

ALLIANT ENERGY (TYP.) CAUTION

REMOVE EXISTING 8" VALVE AND VALVE BOX. SALVAGE TO VILLAGE

REMOVE EXISTING SANITARY SEWER (TYP.)

CAP FOR FUTURE USE

REMOVE AND REPLACE EXISTING MANHOLE

56+00 57+00 58+00 59+00 60+00 61+00 62+00

SAN P-105

SAN P-104

N0°15'50"E 201.51' S89°15'03"E

10' (TYP.)

8" D.I. WATER MAIN (TYP.)

CHARTER COMMUNICATIONS (TYP.)

SAN MH-103
STA:58+32.62 OFF:0.00
RIM:723.97

CONNECT TO EXISTING (TYP.)

EXISTING HYDRANT, AUXILIARY VALVE, AND 6" LEAD TO REMAIN.

CONNECT TO EXISTING 8" WATER MAIN WITH RESTRAINED COUPLING

VILLAGE OF SPRING GREEN (TYP.)

ALLIANT ENERGY (TYP.)

SAN MH-102
STA:61+32.64 OFF:0.01RT
RIM:723.49

STA. 56+25, 23.1' RT.
HYDRANT, AUXILIARY VALVE, AND 6" LEAD (13 LF)

STA. 56+25, 10.2' RT.
8"X8"X6" TEE

WILLARD NEEFE

JANET GEIGER

CAROL OLSON

STA. 59+41, 38.3' RT.
8"X8"X6" TEE

CHADD A MAXWELL

ALLIANT ENERGY (TYP.) CAUTION

ARLINGTON CT.

EXISTING 8" WATER MAIN

JOHN R & SUZANNE R SHARP

STA. 59+41, 10.1' RT.
8" GATE VALVE & BOX

STA. 59+44, 15.3' RT.
8" GATE VALVE & BOX

STA. 59+46, 38.0' RT.
ADJUST VALVE BOX

DAVID J & DONNA DRUCKREY

SAUK COUNTY HOUSING AUTH

726 724 722 720 718 716 714 712 710

EXISTING ℄ PROFILE
WEST MADISON STREET

FINISHED ℄ PROFILE
WEST MADISON STREET

SAN MH-103
SAN P-105: 713.34
SAN P-104: 713.24

7' COVER (MIN.)

SAN MH-102
SAN P-104: 712.04
SAN P-103: 711.94

PROPOSED STORM SEWER

8" GATE VALVE WITH VALVE BOX

HIGH POINT

8" D.I. WATER MAIN (TYP.)

8"X8"X6" HYDRANT TEE

8"X8"X6" TEE

SAN P-105 296' of 10" PVC @ 0.40%

SAN P-104 296' of 10" PVC @ 0.40%

SAN P-103 226' of 10" PVC @ 1.74%

EXISTING ℄ ELEVATION

FINISHED ℄ ELEVATION

723.51	723.63	723.75	723.86	723.97	724.07	724.13	724.20	724.26	724.32	724.39	724.48	724.57	724.60	724.61	724.62	724.61	724.61	724.55	724.46	724.36	724.31	724.25	724.18	724.10	724.04	724.00	723.93	723.84	723.77	723.71
723.24	723.13	723.04	722.99	722.99	723.05	723.16	723.30	723.45	723.59	723.74	723.88	724.03	724.17	724.32	724.45	724.54	724.58	724.56	724.48	724.36	724.23	724.10	723.96	723.83	723.70	723.57	723.44	723.31	723.19	723.11

56+00 56+20 56+40 56+60 56+80 57+00 57+20 57+40 57+60 57+80 58+00 58+20 58+40 58+60 58+80 59+00 59+20 59+40 59+60 59+80 60+00 60+20 60+40 60+60 60+80 61+00 61+20 61+40 61+60 61+80 62+00

PROJECT NO: 5916-00-68	HWY: WEST MADISON STREET	COUNTY: SAUK	SANITARY SEWER AND WATERMAIN PLAN	SHEET --6	E

CARDINAL ST

STA. 64+75, 1.7' LT.
ADJUSTING MANHOLE COVERS
RIM = 722.81
(EXISTING = 722.88)
ADD EXTERNAL CHIMNEY SEAL INCIDENTAL TO ADJUSTING MANHOLE COVERS

REMOVE EXISTING SANITARY SEWER (TYP.)

2-INCH WATER SERVICE

ALLIANT ENERGY (TYP.)

CAUTION

FRONTIER COMMUNICATION (TYP.)

VILLAGE OF SPRING GREEN (TYP.)

SAN MH-101
STA:63+62.90 OFF:0.37RT
RIM:723.27

CUT AND CAP EXISTING WATER SERVICE

STA. 64+69, 16.7' RT.
ADJUST VALVE BOX

STA. 64+98, 6.5' RT.
ADJUST VALVE BOX

STA. 65+14, 48.0' LT.
ADJUST VALVE BOX

STA. 65+14, 11.6' RT.
ADJUST VALVE BOX

STA. 64+75, 1.7' LT.
ADJUST MANHOLE COVER

ASAP PROPERTIES LLC

MATCHLINE STA. 62+00

EX SAN P-101

EX SAN P-102

SAN P-103

63+00

64+00

65+00

S89°15'03"E

N89°26'20"E

N01°17'28"E

N01°15'39"E 30.47

40.00

PI STA = 64+94.82

30.00

10' (TYP.)

REMOVE AND REPLACE EXISTING MANHOLE

1-INCH WATER SERVICE (TYP.)

REMOVE EXISTING 8" D.I. WATER MAIN (TYP.)

8" D.I. WATER MAIN (TYP.)

CHARTER COMMUNICATIONS (TYP.)

FINISHED ℄ WEST MADISON STREET

CONNECT TO EXISTING (TYP.)

REMOVE EXISTING 8" VALVE AND VALVE BOX

REMOVE WATER LATERAL RECONNECT SERVICE TO NEW WATERMAIN AS SHOWN

RICHARD A & PERRY L KNEEDLER

END PROJECT
STA. 65+15
Y= 129,717.15
X= 562,622.77

STA. 62+61.4, 23.0' RT.
HYDRANT AUXILIARY VALVE, AND 6" LEAD (13 LF)

STA. 62+66.7, 10.2' RT.
8"X8"X6" TEE

SAUK COUNTY HOUSING AUTH

HUGH C & PHYLLIS A STEELE

STA. 64+53, 8.1' RT.
8" GATE VALVE & BOX

NOTES:
① CONTRACTOR MUST VERIFY SIZES AND LOCATIONS OF EXISTING SANITARY SEWERS, LATERALS, WATER MAINS, SERVICES, AND STORM SEWERS.
② SANITARY LATERALS SHALL BE 6-INCH DIA. PVC PIPE AND WATER SERVICES SHALL BE 1-INCH DIA. (MIN.) COPPER PIPE UNLESS OTHERWISE NOTED. REPLACE EXISTING LARGER SERVICES IN-KIND.
③ ALL CONNECTIONS TO EXISTING WATER MAINS SHALL BE MADE WITH RESTRAINED COUPLINGS UNLESS OTHERWISE NOTED.
④ WHEN NECESSARY, ADJUST WATER MAIN TO AVOID SANITARY SEWER LATERALS, AS DIRECTED BY ENGINEER.

FINISHED ℄ PROFILE WEST MADISON STREET

EXISTING SANITARY MANHOLE

EXISTING ℄ PROFILE WEST MADISON STREET

PROPOSED STORM SEWER

7' COVER (MIN.)

8" D.I. WATER MAIN (TYP.)

CONNECT TO EXISTING 8" WATER MAIN WITH RESTRAINED COUPLING

EXISTING 8" D.I. WATER MAIN (TYP.) (CONTRACTOR TO VERIFY DEPTH, SIZE, AND LOCATION IN THE FIELD)

8"X8"X6" HYDRANT TEE

8" GATE VALVE WITH VALVE BOX

SAN MH-101
SAN P-103: 707.94
EX SAN P-101: 707.11
EX SAN P-102: 707.00

226' of 10" PVC @ 1.74%

SAN P-103

EXISTING 12" SANITARY SEWER PIPE TO REMAIN

EX SAN P-101

EXISTING ℄ ELEVATION

FINISHED ℄ ELEVATION

722 720 718 716 714 712 710 708 706

723.71 723.11 | 723.69 723.08 | 723.66 723.08 | 723.61 723.13 | 723.54 723.22 | 723.47 723.29 | 723.39 723.33 | 723.30 723.33 | 723.24 723.29 | 723.19 723.22 | 723.08 723.14 | 723.06 723.06 | 723.05 722.98 | 722.97 722.90 | 722.84 722.82 | 722.77 722.74 | 722.66 | 722.63 | 722.53 | 722.40 | 722.27 | 722.14 | 721.97 | 722.06

62+00 62+20 62+40 62+60 62+80 63+00 63+20 63+40 63+60 63+80 64+00 64+20 64+40 64+60 64+80 65+00 65+20 65+40 65+60 65+80 66+00 66+20 66+40 66+60 66+80 67+00 67+20 67+40 67+60 67+80 68+00

PROJECT NO: 5916-00-68	HWY: WEST MADISON STREET	COUNTY: SAUK	SANITARY SEWER AND WATERMAIN PLAN	SHEET 7

DATE 28APR14 E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	5916-00-68 QUANTITY
0090	204.0210	REMOVING MANHOLES	EACH	5.000	5.000
0520	611.8110	ADJUSTING MANHOLE COVERS	EACH	1.000	1.000
0540	619.1000	MOBILIZATION	EACH	0.200	0.200
1000	SPV.0060	SPECIAL 01. REMOVING HYDRANT	EACH	1.000	1.000
1010	SPV.0060	SPECIAL 02. REMOVING CURB STOP AND BOX	EACH	14.000	14.000
1020	SPV.0060	SPECIAL 03. SANITARY MANHOLE	EACH	5.000	5.000
1030	SPV.0060	SPECIAL 04. SANITARY MANHOLE CASTING	EACH	5.000	5.000
1040	SPV.0060	SPECIAL 05. HYDRANT AND AUXILIARY VALVE	EACH	3.000	3.000
1050	SPV.0060	SPECIAL 06. ADJUSTING VALVE BOXES	EACH	5.000	5.000
1060	SPV.0060	SPECIAL 07. WATER MAIN VALVE 8-INCH	EACH	4.000	4.000
1070	SPV.0060	SPECIAL 08. WATER SERVICE CURB STOP AND BOX 1-INCH	EACH	16.000	16.000
1080	SPV.0060	SPECIAL 09. WATER SERVICE CURB STOP AND BOX 2-INCH	EACH	1.000	1.000
1100	SPV.0090	SPECIAL 01. REMOVING WATER MAIN	LF	1,196.000	1,196.000
1110	SPV.0090	SPECIAL 02. WATER SERVICE LINE 1-INCH	LF	466.000	466.000
1120	SPV.0090	SPECIAL 03. WATER SERVICE LINE 2-INCH	LF	54.000	54.000
1130	SPV.0090	SPECIAL 04. REMOVING SANITARY SEWER	LF	1,072.000	1,072.000
1150	SPV.0090	SPECIAL 06. SANITARY SEWER PIPE 10-INCH	LF	1,206.000	1,206.000
1160	SPV.0090	SPECIAL 07. SANITARY LATERAL 6-INCH	LF	561.000	561.000
1170	SPV.0090	SPECIAL 08. WATER MAIN 8-INCH	LF	1,285.000	1,285.000
1180	SPV.0090	SPECIAL 09. HYDRANT LEAD 6-INCH	LF	39.000	39.000

REMOVING MANHOLES

STATION	LOCATION	204.0210 (EACH)
52+75	2.9' RT	1
55+24	2.6' RT	1
58+25	1.8' RT	1
61+25	0.7' RT	1
63+63	0.4' RT	1
	TOTAL	5

ADJUSTING MANHOLE COVERS

STATION	LOCATION	611.8110 ADJUSTING MANHOLE COVERS (EACH)
64+75	1.7' LT	1
	*TOTAL	1

SANITARY SEWER STRUCTURES

STRUCTURE NUMBER	STATION	LOCATION	SPV.0060.03 SANITARY MANHOLE (EACH)	SPV.0060.04 SANITARY MANHOLE CASTING (EACH)	N. INVERT ELEV.	S. INVERT ELEV.	E. INVERT ELEV.	W. INVERT ELEV.	RIM ELEV.	COMMENTS
SAN MH-101	63+63	0.4' RT	1	1	-	707.00	707.11	707.96	723.27	
SAN MH-102	61+33	0.0'	1	1	-	-	711.96	712.06	723.49	
SAN MH-103	58+33	0.0'	1	1	-	-	713.26	713.36	723.97	
SAN MH-104	55+32	0.0'	1	1	-	-	714.56	714.66	723.61	
SAN MH-105	51+41	0.0'	1	1	-	-	716.22	-	721.11	
		TOTAL	5	5						

NOTES: STATIONS AND OFFSETS ARE GIVEN TO THE CENTER OF THE STRUCTURE
INVERT ELEVATIONS ON PLAN ARE PIPE INVERTS

REMOVING SANITARY SEWER

FROM STATION	FROM LOCATION	TO STATION	TO LOCATION	SPV.0090.04 (L.F.)	COMMENTS
52+75	2.9' RT	55+24	2.6' RT	245	10"
55+24	2.6' RT	58+25	1.8' RT	296	10"
58+25	1.8' RT	61+25	0.7' RT	297	10"
61+25	0.7' RT	63+63	0.4' RT	234	10"
			TOTAL	1,072	

NOTES: PIPE LENGTHS ARE GIVEN FROM INNER WALL OF STRUCTURE TO
INNER WALL OF STRUCTURE

SANITARY SEWER PIPE 10-INCH

PIPE NUMBER	FROM STRUCTURE NUMBER	FROM STATION	FROM LOCATION	TO STRUCTURE NUMBER	TO STATION	TO LOCATION	SPV.0090.06 (L.F.)	SLOPE (%)	REMARKS
P103	SAN MH-101	63+63	0.4' RT	SAN MH-102	61+25	0.0' RT	227	1.74	PVC
P104	SAN MH-102	61+32	0.0' RT	SAN MH-103	58+32	0.0' RT	296	0.40	PVC
P105	SAN MH-103	58+32	0.0' RT	SAN MH-104	55+32	0.0' RT	296	0.40	PVC
P106	SAN MH-104	55+32	0.0' RT	SAN MH-105	51+41	0.0' RT	387	0.40	PVC
						TOTAL	1,206		

NOTES: PIPE LENGTHS ARE GIVEN FROM INNER WALL OF STRUCTURE TO INNER WALL OF STRUCTURE

SANITARY LATERAL 6-INCH

STATION	LOCATION	SPV.0090.07 (L.F.)	PROPERTY ADDRESS
52+55	RT	33	806 W. MADISON ST.
53+86	RT	33	800 W. MADISON ST.
54+25	RT	33	758 W. MADISON ST.
55+85	RT	33	734 W. MADISON ST.
56+60	RT	33	720 W. MADISON ST.
57+14	LT	33	COUNCIL OF SPRING GREEN ST. JOHN CATHOLIC CHURCH
57+27	RT	33	702 W. MADISON ST.
57+92	RT	33	660 W. MADISON ST.
59+67	RT	33	624 W. MADISON ST.
60+42	RT	33	600 W. MADISON ST.
60+67	LT	33	COUNCIL OF SPRING GREEN ST. JOHN CATHOLIC CHURCH
61+17	RT	33	600 W. MADISON ST.
61+94	RT	33	540 W. MADISON ST.
62+47	RT	33	520 W. MADISON ST.
62+55	LT	33	140 W. MADISON ST.
63+24	RT	33	206 W. MADISON ST.
64+00	RT	33	206 W. MADISON ST.
	TOTAL	561	

3

REMOVING HYDRANT

STATION	LOCATION	SPV.0060.01 (EACH)	COMMENTS
53+20	28.7' RT	1	17 LF LEAD
	TOTAL	1	

ADJUSTING VALVE BOXES

STATION	LOCATION	SPV.0060.06 (EACH)
59+46	38.0' RT	1
64+69	16.7' RT	1
64+98	6.5' RT	1
65+14	48.0' LT	1
65+14	11.6' RT	1
	TOTAL	5

WATER SERVICE LINES

STATION	LOCATION	SPV.0060.02 REMOVING CURB STOP AND BOX (EACH)	SPV.0060.08 CURB STOP AND BOX 1-INCH (EACH)	SPV.0090.02 SERVICE LINE 1-INCH (L.F.)	SPV.0060.09 CURB STOP AND BOX 2-INCH (EACH)	SPV.0090.03 SERVICE LINE 2-INCH (L.F.)	PROPERTY ADDRESS
52+59	RT	1	1	25	-	-	806 W. MADISON ST.
53+52	LT	-	1	40	-	-	CEMETARY
53+69	RT	1	1	25	-	-	800 W. MADISON ST.
54+20	RT	1	1	25	-	-	800 W. MADISON ST.
55+84	RT	1	1	25	-	-	734 W. MADISON ST.
56+52	RT	1	1	25	-	-	720 W. MADISON ST.
57+01	RT	1	1	25	-	-	720 W. MADISON ST.
57+06	LT	-	1	50	-	-	FARM FIELD
57+11	RT	1	1	25	-	-	702 W. MADISON ST.
57+74	RT	1	1	25	-	-	660 W. MADISON ST.
58+94	RT	1	1	25	-	-	300 ARLINGTON CT.
60+41	RT	1	1	25	-	-	600 W. MADISON ST.
60+58	LT	-	1	50	-	-	FARM FIELD
61+17	RT	1	1	25	-	-	600 W. MADISON ST.
62+48	LT	-	-	-	1	54	140 S. WOOD ST.
62+66	RT	1	1	25	-	-	520 W. MADISON ST.
64+34	RT	1	1	26	-	-	206 S. WOOD ST.
64+48	LT	1	-	-	-	-	140 S. WOOD ST.
	TOTAL	14	16	466	1	54	

3

WATER MAIN VALVE 8-INCH

STATION	LOCATION	SPV.0060.07 (EACH)
51+94	10.4' RT	1
59+41	10.1' RT	1
59+44	15.3' RT	1
64+53	8.1' RT	1
	TOTAL	4

REMOVING WATER MAIN

FROM STATION	FROM LOCATION	TO STATION	TO LOCATION	SPV.0090.01 (L.F.)	COMMENTS
52+72	11.9' RT	64+58	7.9' RT	1,186	8-INCH
59+44	9.7' RT	59+44	50.7' RT	10	8-INCH
			TOTAL	1,196	

WATER MAIN 8-INCH

FROM STATION	FROM LOCATION	TO STATION	TO LOCATION	SPV.0090.08 (L.F.)	COMMENTS
51+83	15.2' RT	64+58	7.9' RT	1,275	8-INCH
59+44	9.7' RT	59+44	19.7' RT	10	8-INCH
			TOTAL	1,285	

HYDRANT AND AUXILIARY VALVE
HYDRANT LEAD 6-INCH

STATION	LOCATION	SPV.0090.09 HYDRANT LEAD 6-INCH (L.F.)	SPV.0060.05 HYDRANT AND AUXILIARY VALVE (EACH)
51+94	23.4' RT	13	1
56+25	23.0' RT	13	1
62+61	23.0' RT	13	1
	TOTAL	39	3

PROJECT NO: 5916-00-68	HWY: WEST MADISON STREET	COUNTY: SAUK	WATER MAIN MISCELLANEOUS QUANTITIES	SHEET 10 E

Notes

Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

http://www.dot.wisconsin.gov

SWL JULY 2014

PROJECT ID: 5916-00-69
WITH: 5916-00-68, 5916-00-71 5916-00-72

COUNTY: SAUK

ORDER OF SHEETS

TOTAL SHEETS = 10

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5916-00-69	———	—

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

VILLAGE OF SPRING GREEN, EAST JEFFERSON STREET

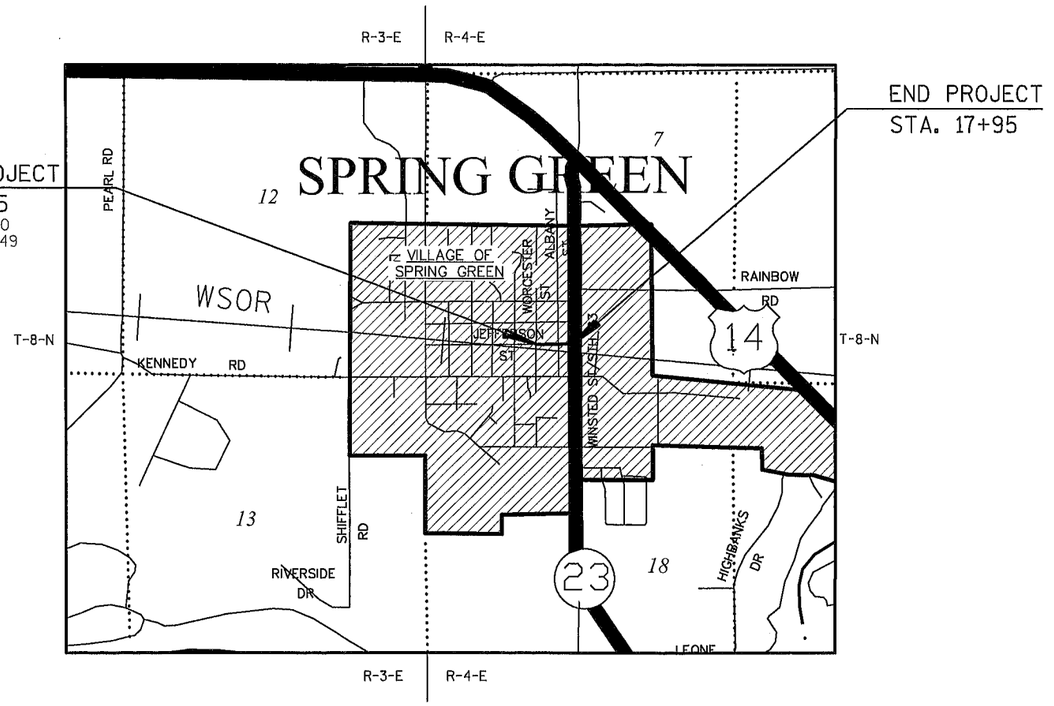
(WORCESTER STREET TO STH 23)

LOCAL STREET

SAUK COUNTY

STATE PROJECT NUMBER
5916-00-69

N



DESIGN DESIGNATION

A.A.D.T.	2014	= 1115
A.A.D.T.	2034	= 1360
D.H.V.	2034	= 120
D.D.		= 62/38
T.		= 6.5%
DESIGN SPEED		= 30 MPH
ESALS		= 182,500

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC — E —
- FIBER OPTIC — FO —
- GAS — G —
- SANITARY SEWER — SAN —
- STORM SEWER — SS —
- TELEPHONE — T —
- WATER — W —
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

LAYOUT

SCALE 0 1/2 MI.

TOTAL NET LENGTH OF CENTERLINE = 0.131 MI.

Coordinates on this plan are referenced to the Wisconsin County Coordinate System (WCCS), Sauk County.

ACCEPTED FOR

VILLAGE of SPRING GREEN

1-24-2014 (Date) (Village President)

ORIGINAL PLANS PREPARED BY

JEWELL associates engineers, inc.
Engineers - Planners - Surveyors

WISCONSIN PROFESSIONAL ENGINEER
TODD L. DEIBERT
No. 37728
SPRING GREEN WIS.

1-24-2014 (Date) Todd L. Deibert (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor JEWELL ASSOCIATES ENGINEERS, INC.
Designer JEWELL ASSOCIATES ENGINEERS, INC.
Management Consultant KJOHNSON ENGINEERS, INC.

APPROVED FOR THE DEPARTMENT

DATE: 1/31/14

Kirby A. Johnson
MANAGEMENT CONSULTANT SIGNATURE

E

FILE NAME: S34083 - Title Sheet

2

LIST OF STANDARD ABBREVIATIONS

ABUT	Abutment
AC	Acre
AGG	Aggregate
AH	Ahead
<	Angle
ASPH	Asphaltic
AVG	Average
ADT	Average Daily Traffic
BAD	Base Aggregate Dense
BK	Back
BF	Back Face
BM	Bench Mark
BR	Bridge
C or C/L	Center Line
CC	Center to Center
CTH	County Trunk Highway
CR	Creek
CR	Crushed
CY or CU YD	Cubic Yard
CP	Culvert Pipe
C & G	Curb and Gutter
D	Degree of Curve
DHV	Design Hour Volume
DIA	Diameter
E	East
X	East Grid Coordinate
ELEC	Electric (al)
EL or ELEV	Elevation
ESALS	Equivalent Single Axle Loads
EBS	Excavation Below Subgrade
FF	Face to Face
FE	Field Entrance
F	Fill
FG	Finished Grade
FL or F/L	Flow Line
FT	Foot
FTG	Footing
GN	Grid North
HT	Height
CWT	Hundredweight
HYD	Hydrant
INL	Inlet
ID •	Inside Diameter
INV	Invert
IP	Iron Pipe or Pin
IRS	Iron Rod Set
JT	Joint
JCT	Junction
LHF	Left–Hand Forward
L	Length of Curve
LIN FT or LF	Linear Foot
LC	Long Chord of Curve
MH	Manhole
MB	Mailbox
ML or M/L	Match Line
N	North
Y	North Grid Coordinate
OD	Outside Diameter
PLE	Permanent Limited Easement
PT	Point
PC	Point of Curvature
PI	Point of Intersection
PRC	Point of Reverse Curvature
PT	Point of Tangency
POC	Point On Curve
POT	Point on Tangent
PVC	Polyvinyl Chloride
PCC	Portland Cement Concrete
LB	Pound
PSI	Pounds Per Square Inch
PE	Private Entrance
R	Radius
RR	Railroad
R	Range
RL or R/L	Reference Line
RP	Reference Point
RCCP	Reinforced Concrete Culvert Pipe
REQD	Required
RES	Residence or Residential
RW	Retaining Wall
RT	Right
RHF	Right–Hand Forward
R/W	Right–of–Way
R	River
RD	Road
RDWY	Roadway
SALV	Salvaged
SAN S	Sanitary Sewer
SEC	Section
SHLDR	Shoulder
SHR	Shrinkage
SW	Sidewalk
S	South
SQ	Square
SF or SQ FT	Square Feet
SY or SQ YD	Square Yard
STD	Standard
SDD	Standard Detail Drawings
STH	State Trunk Highways
STA	Station
SS	Storm Sewer
SG	Subgrade
SE	Superelevation
SL or S/L	Survey Line
SV	Septic Vent
T	Tangent
TEL	Telephone
TEMP	Temporary
TI	Temporary Interest
TLE	Temporary Limited Easement
t	Ton
T or TN	Town
TRANS	Transition
TL or T/L	Transit Line
T	Trucks (percent of)
TYP	Typical
UNCL	Unclassified
UG	Underground Cable
USH	United States Highway
VAR	Variable
V	Velocity or Design Speed
VERT	Vertical
VC	Vertical Curve
VOL	Volume
WM	Water Main
WV	Water Valve
W	West
WB	Westbound
YD	Yard

UTILITIES

ELECTRIC
ALLIANT ENERGY
142 S. CINCINNATI STREET
P.O. BOX 99
SPRING GREEN, WI 53588
OFFICE: (608) 588–9702
ATTN: CHRIS WILHELM
EMAIL: chriswilhelm@alliantenergy.com

CABLE TV
CHARTER COMMUNICATIONS
315 KING STREET
DODGEVILLE, WI 53533
PH: (608) 576–2613
ATTN: STEVE HEGGE
EMAIL: steve.hegge@chartercom.com

GAS
ALLIANT ENERGY
142 S. CINCINNATI STREET
P.O. BOX 99
SPRING GREEN, WI 53588
OFFICE: (608) 588–9702
ATTN: CHRIS WILHELM
EMAIL: chriswilhelm@alliantenergy.com

WATER/SANITARY SEWER/STORM SEWER
VILLAGE OF SPRING GREEN
112 W. MONROE STREET
SPRING GREEN, WI 53588
CELL: (608) 588–4983
OFFICE: (608) 588–2335
ATTN: GREG WIPPERFURTH

FIBER OPTIC
CHARTER COMMUNICATIONS
315 KING STREET
DODGEVILLE, WI 53533
PH: (608) 576–2613
ATTN: STEVE HEGGE
EMAIL: steve.hegge@chartercom.com

WINDSTREAM KDL
13935 BISHOPS DRIVE
BROOKFIELD, WI 53005
PH: (262) 792–7938
ATTN: JIM KOSTUCH
EMAIL: james.kostuch@windstream.com

DIGGERS HOTLINE
Dial 811 or (800) 242-8511
www.DiggersHotline.com

* DENOTES UTILITY IS NOT A MEMBER OF DIGGERS HOTLINE

GENERAL NOTES

COORDINATES AND BEARINGS ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), SAUK COUNTY.

ELEVATIONS SHOWN ON THE PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88)

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

ALL RADII DIMENSIONS ON THE PLAN FOR CURB AND GUTTER ARE TO THE FLANGE OF THE CURB AND GUTTER.

THE COST OF CONNECTING WATER MAIN OR SANITARY SEWER TO EXISTING PIPE OR STRUCTURES SHALL BE INCIDENTAL TO THE COST OF INSTALLING THE WATER MAIN OR SANITARY SEWER.

WATER AND SANITARY SEWER SERVICES SHALL NOT BE DISCONNECTED UNTIL DIRECTED BY THE ENGINEER.

ALL WORK TO BE CONSTRUCTED PER GOVERNING CODES/ORDINANCES, AS AMENDED BY LOCAL AUTHORITIES. SAID CODES/ORDINANCES ARE HEREIN INCORPORATED INTO THESE DOCUMENTS. ALL CODE REQUIRED WORK TO BE INCLUDED IN CONTRACT SUM. REQUIRED CODES INCLUDE, BUT ARE NOT LIMITED TO:

- ADMINISTRATIVE CODE CHAPTER NR 811
- ADMINISTRATIVE CODE CHAPTER NR 110
- LOCAL CODES

GENERAL NOTES ARE INTENDED TO CLARIFY OR EMPHASIZE THE REQUIREMENTS OF THE DRAWINGS AND SPECIFICATIONS. IN THE EVENT OF A CONFLICT BETWEEN THE DRAWINGS OR SPECIFICATIONS AND THESE NOTES, THE CONTRACTOR SHALL COMPLY WITH THE MORE STRINGENT REQUIREMENT.

ALL WORK TO BE PERFORMED IN A MANNER SO AS TO HAVE A MINIMUM OF DISRUPTION AND DISTURBANCE WITH EXISTING OPERATIONS AND LOCAL ENVIRONMENT. NOISE AND DUST SHALL BE KEPT TO AN ABSOLUTE MINIMUM.

DO NOT SCALE DRAWINGS.

PERMIT ARE TO BE KEPT ON–SITE AT ALL TIMES.

DETAILS AND NOTES OF SIMILAR CONDITIONS ARE TYPICAL WHETHER OR NOT CALLED OUT AT ALL PLACES. REFERENCE TO ANY DETAIL OR DRAWINGS IS FOR CONVENIENCE ONLY AND DOES NOT LIMIT THE APPLICATION OF SUCH DETAIL OR DRAWING.

SYSTEMS SHOWN ON DRAWINGS ARE INTENDED TO BE FURNISHED, INSTALLED, AND TURNED OVER TO OWNER IN PROPER FUNCTIONING CONDITION. ALL WORK TO BE CONSIDERED IN CONTRACT SUM.

CONTRACTOR SHALL CHECK AND VERIFY ALL FIELD CONDITIONS AND DIMENSIONS WITH THE CONSTRUCTION DRAWINGS AT THE PROJECT SITE PRIOR TO CONSTRUCTION, ERECTION, AND/OR FABRICATION. CONTRACTOR SHALL INSPECT RELATED WORK AND ADJACENT SURFACES. CONTRACTOR SHALL REPORT ANY DISCREPANCIES BETWEEN THE CONTRACT DOCUMENTS AND THE EXISTING CONDITIONS WHICH SHALL PREVENT PROPER EXECUTION OF THIS WORK TO THE OWNER BEFORE PROCEEDING WITH THE WORK.

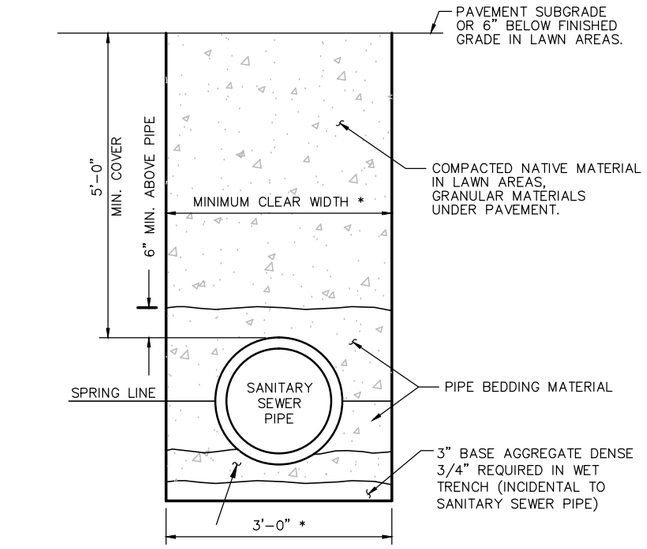
INSTALLATION OF ALL MATERIALS AND SYSTEMS SHALL BE IN ACCORDANCE WITH MANUFACTURER'S WRITTEN INSTRUCTIONS.

CONTACTS

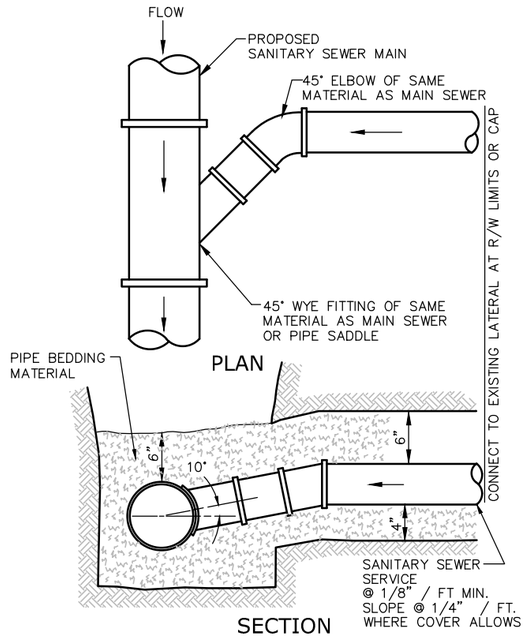
DESIGN CONSULTANT:
JEWELL ASSOCIATES ENGINEERS, INC.
560 SUNRISE DRIVE
SPRING GREEN, WI 53588
ATTN: FRED GRUBER, P.E., R.L.S.
PH: (608) 588–7484
FAX: (608) 588–9322
E MAIL: fred.gruber@jewellassoc.com

VILLAGE OF SPRING GREEN:
EUGENE HAUSNER, VILLAGE PRESIDENT
520 N. WORCESTER
SPRING GREEN, WISCONSIN 53588
PH: (608) 588–7780

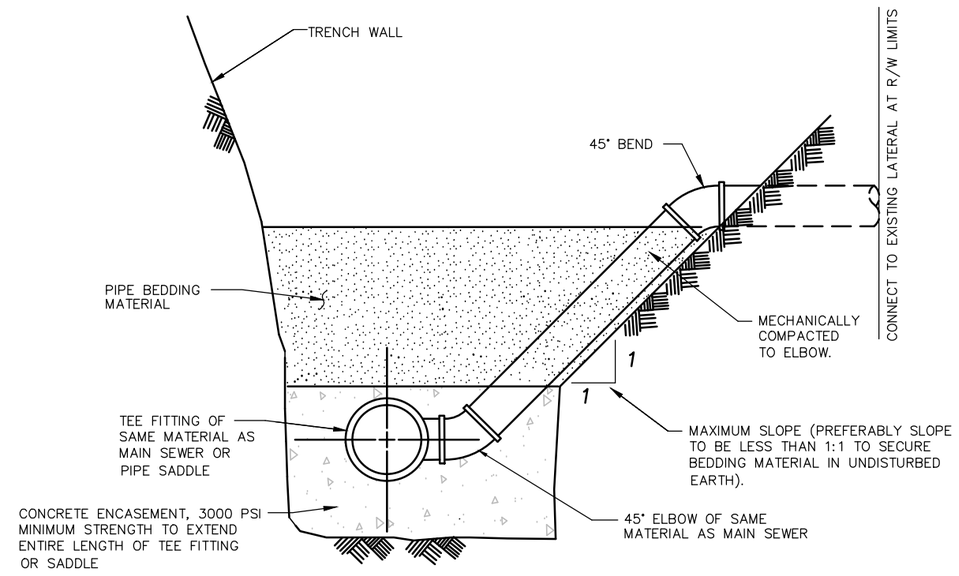
WDNR LIAISON:
STATE OF WISCONSIN
DEPT. OF NATURAL RESOURCES
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
ATTN: CATHY BLESER
PH: (608) 275–3308
E MAIL: catherine.bleser@wisconsin.gov



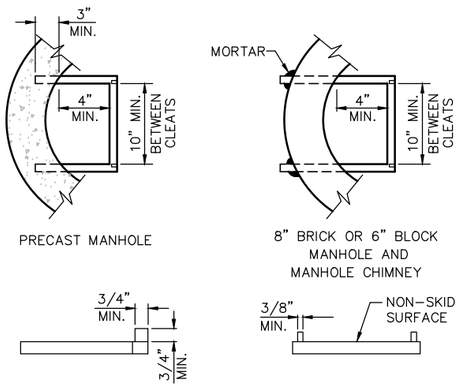
SANITARY SEWER TRENCH DETAIL



SANITARY SEWER LATERAL DETAIL



SANITARY SEWER RISER DETAIL



PROVIDE CERTIFIED TEST DATA THAT THE STEPS CAN WITHSTAND AN 800-POUND VERTICAL LOAD WITHOUT MORE THAN 3/8-INCH PERMANENT SET WHEN TESTED IN ACCORDANCE WITH SECTION 10 A.S.T.M. 498.

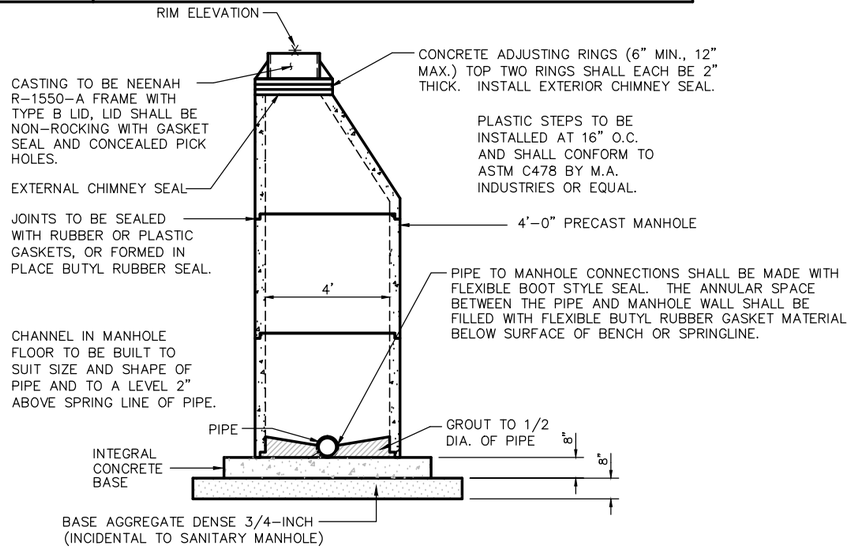
PROVIDE CERTIFIED TEST DATA THAT THE INSTALLED STEPS CAN WITHSTAND A HORIZONTAL LOAD OF 400 POUNDS WITH THE LOAD APPLIED OVER A WIDTH OF 3 1/2" AND CENTERED ON THE RUNG.

STEPS MUST BE EQUALLY SPACED VERTICALLY IN THE ASSEMBLED MANHOLE AT A MAXIMUM DISTANCE OF 16" ON CENTER.

STEPS SHALL BE FABRICATED OF 1/2" DIA. GRADE 60 STEEL REINFORCING ROD WITH MOLDED PLASTIC COVERING.

STEPS SHALL NOT BE INSTALLED IN THE PLASTIC RINGS.

MANHOLE STEPS DETAIL

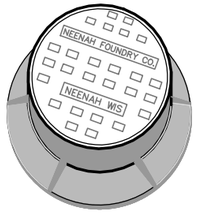


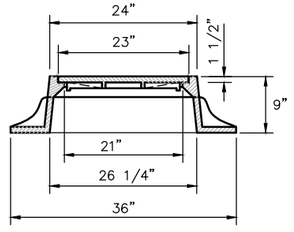
SANITARY MANHOLES 48" DIAMETER

ADJUST FRAME TO GRADE WITH CONCRETE RINGS OF VARIABLE THICKNESS MAXIMUM RING HEIGHT = 12" MINIMUM RING HEIGHT = 6"

CONCRETE AND STEEL REINFORCEMENT SHALL CONFORM TO DESIGNATION C-478 REQUIREMENTS OF ASTM SPECIFICATIONS.

TYPE 'B' LID DESIGN





NEENAH R-1550-A

SANITARY MANHOLE

FRAME, SOLID LID

HEAVY DUTY

NOTE: LID SHALL BE NON-ROCKING WITH GASKET SEAL AND CONCEALED PICKHOLES.

GENERAL NOTES

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMAN- SHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

FLATTOPS WITH 24-INCH DIAMETER ECCENTRIC OPENING SHALL BE USED ONLY ON SANITARY STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

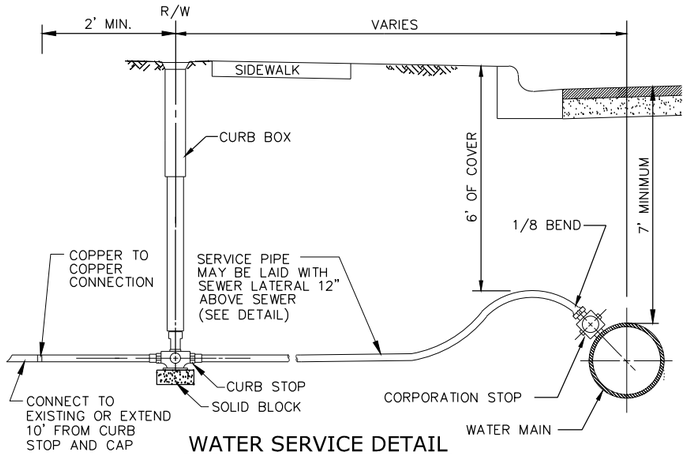
STEPS MEETING THE SPECIFIED REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

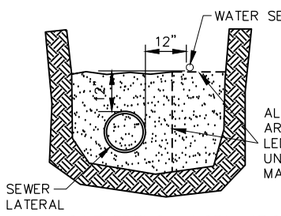
PRECAST REINFORCED CONCRETE RISERS MAY BE PLACED WITH TONGUE UP OR DOWN.

ALL PRECAST INLETS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M 199.

ALL STRUCTURES SHALL BE PLACED ON A BED OF 8" OF BASE AGGREGATE DENSE.



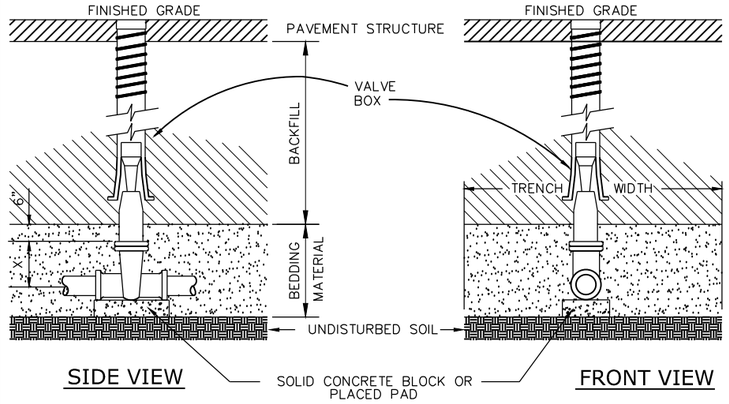
WATER SERVICE DETAIL



JOINT TRENCH INSTALLATION

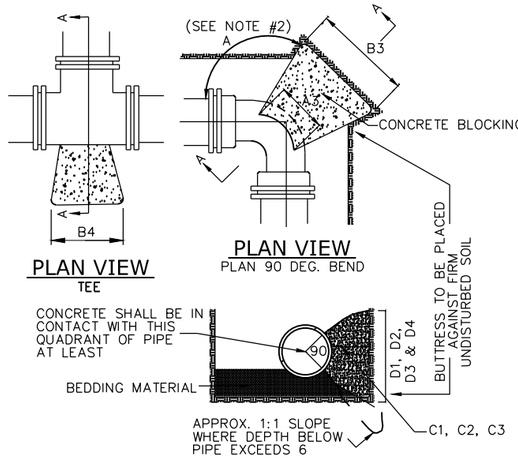
NOTES:
- HORIZONTAL AND VERTICAL OFFSETS SHALL BE MADE WITH AN APPROVED PIPE BENDING TOOL. SHARP BENDS OR KINKS IN THE WATER SERVICE ARE NOT ALLOWED.
- VERTICAL OFFSETS SHALL BE MADE ON THE PROPERTY LINE SIDE OF THE CURB STOP.
- CAST IRON CURB BOX SLEEVES SHALL BE INSTALLED WHERE CURB BOXES ARE INSTALLED IN CONCRETE OR ASPHALT SURFACES.

WATER SERVICE INSTALLATION DETAIL



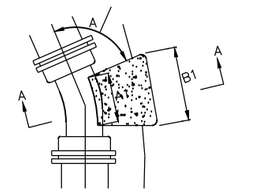
GATE VALVE BOX DETAIL

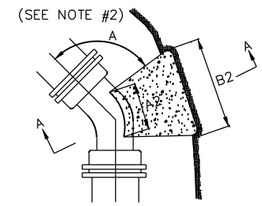
PIPE DIA. INCHES	X=SETTING INCHES
2	6
3	7
4	8
6	12
8	13
10	17
12	21
16	30



BUTTRESS FOR BENDS

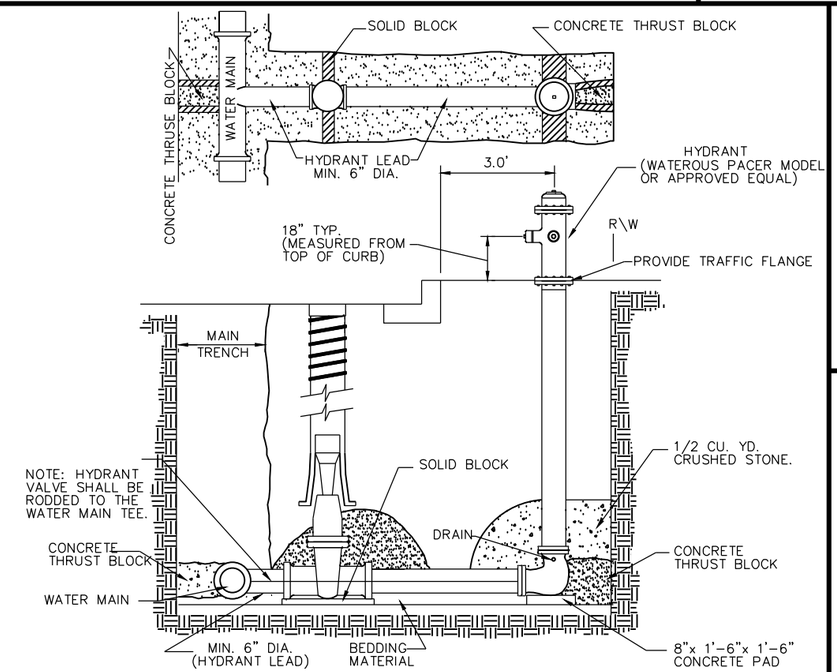
BUTTRESS DIMENSIONS								
PIPE	22 1/2° BENDS		45° BENDS		90° BENDS		TEES	
SIZE	B1	D1	B2	D2	B3	D3	B4	D4
6"	1'-0"	1'-0"	1'-0"	1'-0"	1'-4"	1'-2"	1'-3"	1'-0"
8"	1'-0"	1'-0"	1'-4"	1'-2"	1'-10"	1'-6"	1'-6"	1'-4"
10"	1'-2"	1'-2"	1'-7"	1'-6"	2'-4"	1'-10"	2'-3"	2'-0"
12"	1'-4"	1'-4"	1'-10"	1'-10"	2'-9"	2'-3"	2'-3"	2'-0"
16"	1'-10"	1'-8"	2'-6"	2'-4"	3'-10"	2'-10"	3'-2"	2'-6"
20"	2'-4"	2'-0"	3'-3"	2'-10"	6'-0"	3'-4"	4'-0"	3'-0"
24"	2'-10"	2'-4"	4'-0"	3'-3"	6'-4"	3'-10"	5'-3"	3'-4"
30"	3'-6"	3'-0"	5'-4"	3'-10"	8'-0"	4'-8"	6'-3"	4'-3"





NOTES

1. MECHANICAL JOINT RESTRAINT FITTINGS MAY BE USED IN LIEU OF THRUST BLOCKS. MECHANICAL JOINT RESTRAINT SHALL BE MEGALUG OR EQUAL.
2. DIMENSIONS IN TABLE ARE BASED ON A WATER PRESSURE OF AT LEAST 150 PSI, AND ON EARTH RESISTANCE OF 2 TONS PER SQ. FT.
3. DIMENSION C1, C2, C3, SHOULD BE LARGE ENOUGH TO MAKE ANGLE "A" EQUAL OR LARGER THAN 45 DEG.
4. DIMENSION A1, A2, A3, SHOULD BE AS LARGE AS POSSIBLE WITHOUT INTERFERRING WITH M.J. BOLTS.
5. SHAPE OF BACK OF BUTTRESS MAY VARY AS LONG AS POUR IS AGAINST FIRM UNDISTURBED EARTH.



HYDRANT DETAILS

FOR: 8-INCH DUCTILE IRON PIPE
150 P.S.I. PRESSURE
20 FOOT NOMIMAL LAYING LENGTH

FITTING	LENGTH OF PIPE RESTRAINED BEYOND FITTING, FEET
90	20
45	10
22.5	5
TEE BRANCH	10
TEE RUN	10
DEAD END	40
VALVE	40 (BOTH SIDES)

NOTE:
A 20 FOOT RESTRAINED LENGTH WOULD REQUIRE ONE (1) RESTRAINED JOINT AT THE FITTING. IF LAYING LENGTH IS LESS THAN 20 FEET, ADDITIONAL JOINTS SHALL BE RESTRAINED UNTIL THE REQUIRED RESTRAINED LENGTH IS OBTAINED.

THRUST RESTRAINT TABLE

SERVICE PIPE	CORP. STOP	CURB STOP
1"	1"	1"
1-1/2"	1-1/2"	1-1/2"
2"	2"	2"

1. NO JOINTS ALLOWED WITHOUT PERMISSION OF THE ENGINEER.

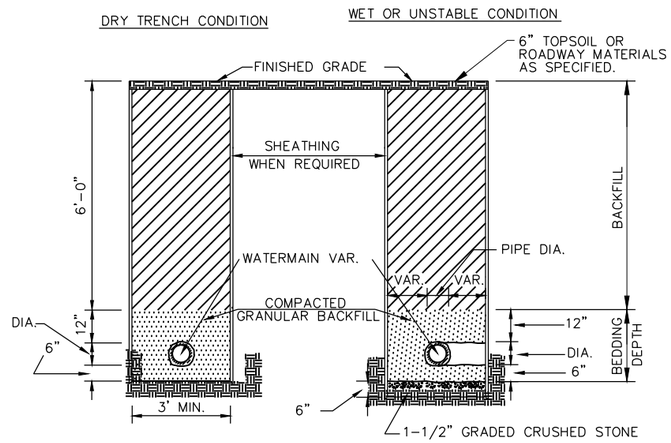
NOTE:
IF 6 FEET MINIMUM COVER IS NOT AVAILABLE, THE NEW SERVICE PIPE SHALL BE INSULATED. CONNECTIONS SHALL BE TESTED FOR LEAKAGE PRIOR TO BACKFILL AFTER SUPPORT HAS BEEN COMPACTED. MINIMUM RADII ARE AS FOLLOWS: 1"= 6", 1-1/4" = 8", 1-1/2" = 10", 2" = 12". TAP AND SERVICE SHALL BE A MINIMUM OF 1"

TAP SERVICE PIPING

(COPPER)

DRY TRENCH CONDITION

WET OR UNSTABLE CONDITION



WATER MAIN TRENCH SECTION

PROJECT NO: 5916-00-69	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	WATERMAIN DETAILS	SHEET 4	E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\CADD FILES\S34083 JEFFERSON CADD\S34083 JEFFERSON ROADWAY\DETAILS\WATER MAIN DETAILS.DWG PLOT DATE : 12/18/2013 4:38 PM PLOT BY : BALLWEG,THOMAS

2

HERBERT P KRITZ

C/O BMO HARRIS BANK NA
BANK OF SPRING GREEN

ROGER A BOWDEN

HERBERT P KRITZ

PECKHAM & NEIDER LLP

VILLAGE OF SPRING GREEN (TYP.)

STA 11+42, 23.7 LT.
REMOVE AND REPLACE HYDRANT, AUXILIARY VALVE, AND 6" LEAD (12 LF)

2-INCH WATER SERVICE

REMOVE EXISTING 8" D.I. WATER MAIN (TYP.)

ALLIANT ENERGY (TYP.)

CAUTION

VILLAGE OF SPRING GREEN (TYP.)

CONNECT TO EXISTING WATER MAIN

ALLIANT ENERGY (TYP.)

REPLACE 6" D.I. WATER MAIN WITH 8" D.I. WATER MAIN (34 LF)

REMOVE EXISTING VALVES

EXISTING 6" D.I. WATER MAIN TO REMAIN

STA. 14+78.5, 89.3' LT.
8"X6" REDUCER RESTRAINED

STA 15+09, 23.4 LT.
REMOVE AND REPLACE HYDRANT, AUXILIARY VALVE, AND 6" LEAD (12 LF)

REPLACE 4" D.I. WATER MAIN WITH 8" D.I. WATER MAIN (47 LF)

8" D.I. WATER MAIN (TYP.)

EXISTING 8" WATER VALVE TO REMAIN

ADJUST WATER VALVE BOX

22.5° BENDS

STA. 11+12.3', 16.2' LT.
22.5° BEND

STA. 11+23.5', 11.5' LT.
22.5° BEND

REMOVE EXISTING MANHOLE

N89°16'51"E

REMOVE EXISTING MANHOLE

ALLIANT ENERGY

CAUTION

MATCHLINE STA. 16+00

SAN P-3

SAN P-2

SAN MH-3
STA:11+61.52 OFF:13.76RT
RIM:724.83

FINISHED ℄ EAST JEFFERSON STREET

REPLACE EXISTING 8" SANITARY SEWER (TYP.)

2-INCH WATER SERVICE

CONTRACTOR MUST VERIFY EXISTING PIPE INVERT

SAN P-4

SAN MH-2
STA:14+79.69 OFF:13.71RT
RIM:724.25

STA. 14+97, 35.0' RT.
8"X4" REDUCER RESTRAINED

CONNECT TO EXISTING WATER MAIN

ZWETTLER & HUTTER INC

BEGIN PROJECT
STA. 11+05
Y= 130,312.49
X= 564,508.46

WORCESTER STREET

2-INCH WATER SERVICE

1-INCH WATER SERVICE (TYP.)

CONNECT TO EXISTING (TYP.)

STA. 14+77.5, 11.7' LT.
8"X8"X8" TEE

STA. 14+72.5, 11.7' LT
8" GATE VALVE & BOX

STA. 14+77.6, 16.7' LT
8" GATE VALVE & BOX

VILLAGE OF SPRING GREEN (TYP.)

ALBANY STREET

REPLACE 8" SANITARY SEWER (26 LF)

EXISTING 8" SANITARY SEWER TO REMAIN

EXISTING 4" WATER MAIN TO REMAIN

STA. 14+96.6, 9.7' LT.
8" GATE VALVE & BOX

STA. 15+01.6, 11.7' LT.
8" GATE VALVE & BOX

STA. 15+08.7, 11.7' LT.
8"X8"X8" TEE

HAHN PROPERTIES LLC

JEFFREY R OLSON

DAVIN PICKELL

PAMELA MEIER-HAMACHEK

PAMELA E CHEHADE

SUSAN M MC CLINTOCK

NOTES:
① CONTRACTOR MUST VERIFY SIZES AND LOCATIONS OF EXISTING SANITARY SEWERS, LATERALS, WATER MAINS, SERVICES, AND STORM SEWERS.
② SANITARY LATERALS SHALL BE 6-INCH DIA. PVC PIPE AND WATER SERVICES SHALL BE 1-INCH DIA. (MIN.) COPPER PIPE UNLESS OTHERWISE NOTED. REPLACE LARGER SERVICES IN-KIND.
③ ALL CONNECTIONS TO EXISTING WATER MAINS SHALL BE MADE WITH RESTRAINED COUPLINGS UNLESS OTHERWISE NOTED.
④ WHEN NECESSARY, ADJUST WATER MAIN TO AVOID SANITARY SEWER LATERALS, AS DIRECTED BY ENGINEER.

EXISTING ℄ PROFILE EAST JEFFERSON STREET

FINISHED ℄ PROFILE EAST JEFFERSON STREET

SAN MH-3
SAN P-3: 717.42

EXISTING STORM SEWER

PROPOSED STORM SEWER

7' COVER (MIN.)

CONNECT TO EXISTING WATER MAIN VALVE

EXISTING 8" D.I. WATER MAIN

22.5° BENDS

8"X8"X6" HYDRANT TEE

8" D.I. WATER MAIN (TYP.)

MAINTAIN SLOPE TO AVOID HIGHPOINT IN WATERMAIN

FINSIHED ℄ ELEVATION

EXISTING ℄ ELEVATION

SAN P-3 314' of 10" PVC @ 0.30%

SAN MH-2
SAN P-3: 716.48
SAN P-4: 716.58
SAN P-2: 716.38

PROPOSED STORM SEWER

PROPOSED STORM SEWER

8" GATE VALVE AND VALVE BOX

8" GATE VALVE AND VALVE BOX

REMOVE EXISTING SANITARY SEWER (TYP.)

SAN P-2 260' of 10" PVC @ 0.32%

8"X8"X6" HYDRANT TEE

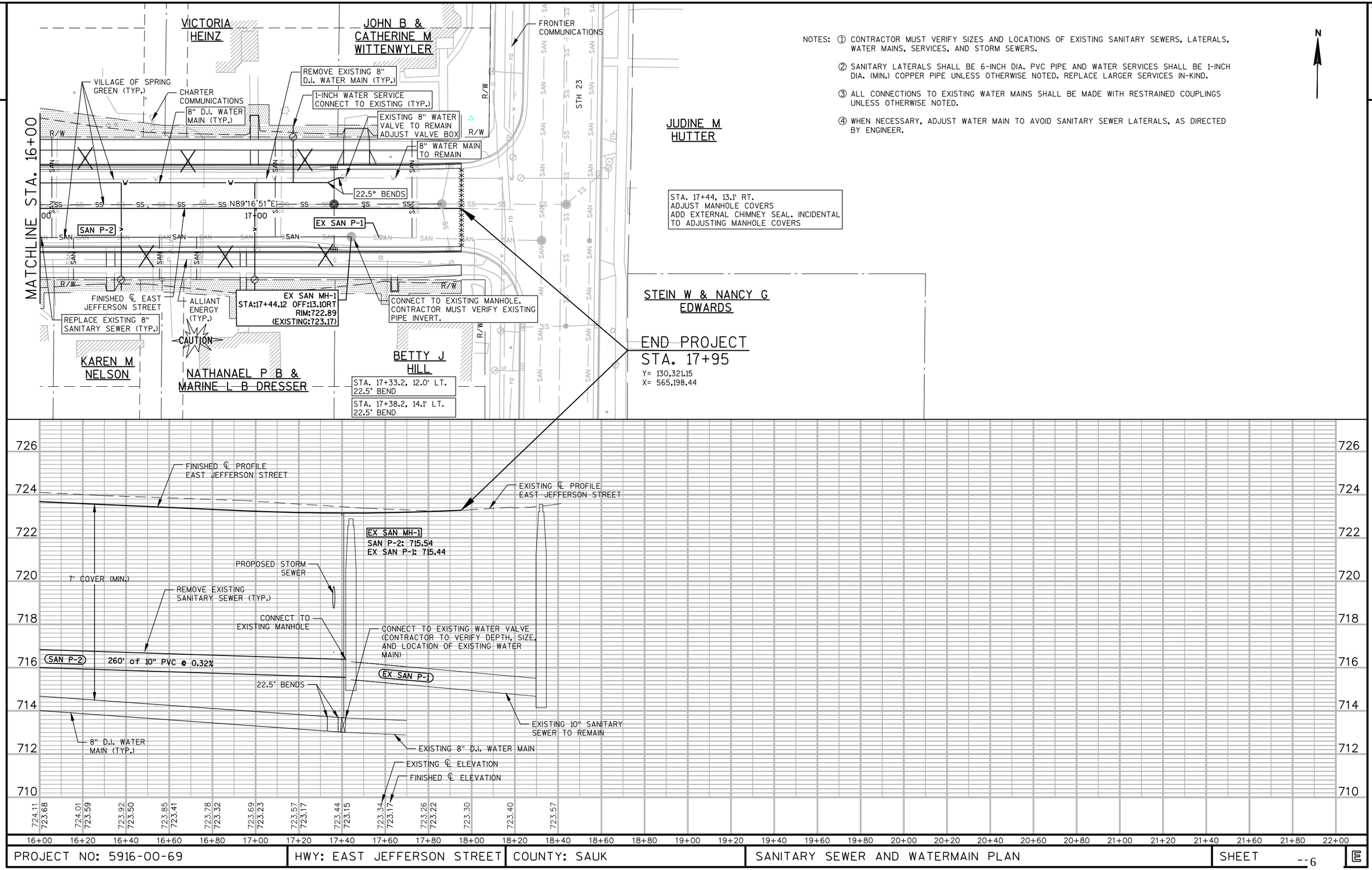
8"X8"X8" TEE

730 728 726 724 722 720 718 716 714

725.89 725.94 725.85 725.78 725.73 725.70 725.53 725.56 725.31 725.41 725.44 725.25 725.44 725.10 725.45 724.99 725.44 724.98 725.43 725.07 725.37 725.26 725.28 725.53 725.09 725.73 724.96 725.84 724.83 725.86 724.74 725.80 724.72 725.69 724.68 725.57 724.60 725.46 724.56 725.34 724.56 725.18 724.47 724.99 724.35 724.76 724.29 724.48 724.22 724.22 724.18 723.99 724.15 723.81 724.11 723.68

10+00 10+20 10+40 10+60 10+80 11+00 11+20 11+40 11+60 11+80 12+00 12+20 12+40 12+60 12+80 13+00 13+20 13+40 13+60 13+80 14+00 14+20 14+40 14+60 14+80 15+00 15+20 15+40 15+60 15+80 16+00

PROJECT NO: 5916-00-69	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	SANITARY SEWER AND WATERMAIN PLAN	SHEET --5



PROJECT NO: 5916-00-69	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	SANITARY SEWER AND WATERMAIN PLAN	SHEET --6

DATE 28APR14 E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	5916-00-69 QUANTITY
0090	204.0210	REMOVING MANHOLES	EACH	2.000	2.000
0520	611.8110	ADJUSTING MANHOLE COVERS	EACH	1.000	1.000
0540	619.1000	MOBILIZATION	EACH	0.120	0.120
1000	SPV.0060	SPECIAL 01. REMOVING HYDRANT	EACH	2.000	2.000
1010	SPV.0060	SPECIAL 02. REMOVING CURB STOP AND BOX	EACH	8.000	8.000
1020	SPV.0060	SPECIAL 03. SANITARY MANHOLE	EACH	2.000	2.000
1030	SPV.0060	SPECIAL 04. SANITARY MANHOLE CASTING	EACH	2.000	2.000
1040	SPV.0060	SPECIAL 05. HYDRANT AND AUXILIARY VALVE	EACH	2.000	2.000
1050	SPV.0060	SPECIAL 06. ADJUSTING VALVE BOXES	EACH	2.000	2.000
1060	SPV.0060	SPECIAL 07. WATER MAIN VALVE 8-INCH	EACH	4.000	4.000
1070	SPV.0060	SPECIAL 08. WATER SERVICE CURB STOP AND BOX 1-INCH	EACH	8.000	8.000
1080	SPV.0060	SPECIAL 09. WATER SERVICE CURB STOP AND BOX 2-INCH	EACH	3.000	3.000
1100	SPV.0090	SPECIAL 01. REMOVING WATER MAIN	LF	713.000	713.000
1110	SPV.0090	SPECIAL 02. WATER SERVICE LINE 1-INCH	LF	367.000	367.000
1120	SPV.0090	SPECIAL 03. WATER SERVICE LINE 2-INCH	LF	125.000	125.000
1130	SPV.0090	SPECIAL 04. REMOVING SANITARY SEWER	LF	595.000	595.000
1140	SPV.0090	SPECIAL 05. SANITARY SEWER PIPE 8-INCH	LF	26.000	26.000
1150	SPV.0090	SPECIAL 06. SANITARY SEWER PIPE 10-INCH	LF	574.000	574.000
1160	SPV.0090	SPECIAL 07. SANITARY LATERAL 6-INCH	LF	596.000	596.000
1170	SPV.0090	SPECIAL 08. WATER MAIN 8-INCH	LF	713.000	713.000
1180	SPV.0090	SPECIAL 09. HYDRANT LEAD 6-INCH	LF	24.000	24.000

REMOVING MANHOLES

STATION	LOCATION	204.0210 (EACH)
11+68	13.8' RT	1
14+76	13.7' RT	1
	*TOTAL	2

NOTES: STATIONS AND OFFSETS ARE GIVEN TO THE CENTER OF THE STRUCTURE.

ADJUSTING MANHOLE COVERS

STATION	LOCATION	611.8110 (EACH)
17+44.12	13.10' RT	1
	*TOTAL	1

SANITARY SEWER STRUCTURES

STRUCTURE NUMBER	STATION	LOCATION	SPV.0060.03 SANITARY MANHOLE (EACH)	SPV.0060.04 SANITARY MANHOLE CASTING (EACH)	N. INVERT ELEV.	S. INVERT ELEV.	E. INVERT ELEV.	W. INVERT ELEV.	RIM ELEV.	COMMENTS
EX SAN MH-1	17+44.12	13.10' RT	-	-	-	-	-	715.54	722.89	
SAN MH-2	14+79.69	13.71' RT	1	1	-	716.58	716.38	716.48	724.25	
SAN MH-3	11+61.62	13.76' RT	1	1	-	-	717.42	-	724.83	
		TOTAL	2	2						

NOTES: STATIONS AND OFFSETS ARE GIVEN TO THE CENTER OF THE STRUCTURE.
INVERT ELEVATIONS ON PLAN ARE PIPE INVERTS.

REMOVING SANITARY SEWER

FROM		TO		SPV.0090.04 (L.F.)	COMMENTS
STATION	LOCATION	STATION	LOCATION		
11+68	13.8' RT	14+76	13.7' RT	304	8"
14+76	13.7' RT	17+44	13.1' RT	265	8"
14+80	41.2' RT	14+80	13.7' RT	26	8"
			TOTAL	595	

NOTES: PIPE LENGTHS ARE GIVEN FROM INNER WALL OF STRUCTURE TO INNER WALL OF STRUCTURE

SANITARY SEWER PIPE

PIPE NUMBER	FROM STRUCTURE NUMBER	FROM STATION	FROM LOCATION	TO STRUCTURE NUMBER	TO STATION	TO LOCATION	SANITARY SEWER SPV.0090.05 8-INCH (L.F.)	SANITARY SEWER SPV.0090.06 10-INCH (L.F.)	SLOPE (%)	REMARKS
SAN-P2	EX SAN MH-1	17+44	13.1' RT	SAN MH-2	14+80	13.7' RT	-	260	0.32	PVC
SAN-P3	SAN MH-2	14+80	13.8' RT	SAN MH-3	11+62	13.8' RT	-	314	0.30	PVC
SAN-P4	SAN MH-2	14+80	13.8' RT	-	14+80	41.5' RT	26	-	0.40	PVC
						TOTAL	26	574		

SANITARY LATERAL 6-INCH

STATION	LOCATION	SPV.0090.07 (L.F.)	PROPERTY ADDRESS
11+78	RT	20	208 E. JEFFERSON ST.
11+93	LT	47	209 E. JEFFERSON ST.
12+10	RT	20	208 E. JEFFERSON ST.
12+81	LT	47	209 E. JEFFERSON ST.
12+87	RT	20	228 E. JEFFERSON ST.
13+49	RT	20	248 E. JEFFERSON ST.
13+67	LT	47	241 E. JEFFERSON ST.
13+79	RT	20	248 E. JEFFERSON ST.
14+18	RT	20	248 E. JEFFERSON ST.
14+36	LT	47	259 E. JEFFERSON ST.
15+34	LT	47	307 E. JEFFERSON ST.
15+70	RT	20	316 E. JEFFERSON ST.
15+79	RT	20	316 E. JEFFERSON ST.
16+07	LT	47	311 E. JEFFERSON ST.
16+13	RT	20	328 E. JEFFERSON ST.
16+56	RT	20	328 E. JEFFERSON ST.
16+74	RT	20	336 E. JEFFERSON ST.
17+12	LT	47	337 E. JEFFERSON ST.
17+75	LT	47	112 N. WINSTED ST.
	TOTAL	596	

3

REMOVING HYDRANT

STATION	LOCATION	SPV.0060.01 (EACH)	COMMENTS
11+42	23.6' LT	1	10 LF LEAD
15+09	23.5' LT	1	13 LF LEAD
	TOTAL	2	

ADJUSTING VALVE BOXES

STATION	LOCATION	SPV.0060.06 (EACH)
11+10	16.3' LT	1
17+41	14.2' LT	1
	TOTAL	2

WATER MAIN VALVE 8-INCH

STATION	LOCATION	SPV.0060.07 (EACH)
14+73	11.7' LT	1
14+78	16.7' LT	1
14+97	9.7' LT	1
15+02	11.7' LT	1
	TOTAL	4

WATER MAIN 8-INCH

FROM STATION	FROM LOCATION	TO STATION	TO LOCATION	SPV.0090.08 (L.F.)
11+10	16.3' LT	17+41	14.2' LT	631
14+78	11.1' LT	14+78	45.0' LT	34
14+97	11.7' LT	14+97	35.7' RT	48
			TOTAL	713

3

WATER SERVICE LINES

STATION	LOCATION	SPV.0060.02 REMOVING CURB STOP AND BOX (EACH)	SPV.0060.08 WATER SERVICE CURB STOP AND BOX 1-INCH (EACH)	SPV.0090.02 WATER SERVICE LINE 1-INCH (L.F.)	SPV.0060.09 WATER SERVICE CURB STOP AND BOX 2-INCH (EACH)	SPV.0090.03 WATER SERVICE LINE 2-INCH (L.F.)	PROPERTY ADDRESS
11+71	RT	1	-	-	1	50	208 E. JEFFERSON ST.
12+14	LT	1	-	-	1	25	209 E. JEFFERSON ST.
12+64	RT	1	1	50	-	-	228 E. JEFFERSON ST.
13+30	RT	1	1	50	-	-	238 E. JEFFERSON ST.
13+53	LT	1	1	26	-	-	241 E. JEFFERSON ST.
13+79	RT	1	-	-	1	50	248 E. JEFFERSON ST.
15+38	RT	-	1	55	-	-	306 E. JEFFERSON ST.
15+87	RT	-	1	55	-	-	316 E. JEFFERSON ST.
16+38	RT	-	1	55	-	-	328 E. JEFFERSON ST.
16+99	RT	1	1	50	-	-	336 E. JEFFERSON ST.
17+17	LT	1	1	26	-	-	337 E. JEFFERSON ST.
	TOTAL	8	8	367	3	125	

NOTE: 328, 318, AND 308 EAST JEFFERSON STREET HAVE WATER SERVICE FED FROM BEHIND THE HOMES. CAP NEW SERVICE FOR FUTURE CONNECTION. EXTEND NEW SERVICE 5' MINIMUM PAST CURB STOP.

REMOVING WATER MAIN

FROM STATION	FROM LOCATION	TO STATION	TO LOCATION	SPV.0090.01 (L.F.)	COMMENTS
11+10	16.3' LT	17+41	14.2' LT	631	8"
14+78	12.8' LT	14+78	45.0' LT	33	6"
14+97	12.9' LT	14+97	35.7' RT	49	6"
			TOTAL	713	

HYDRANT AND AUXILIARY VALVE, HYDRANT LEAD 6-INCH

STATION	LOCATION	SPV.0090.09 HYDRANT LEAD 6-INCH (L.F.)	SPV.0060.05 HYDRANT AND AUXILIARY VALVE (EACH)
11+42	23.7' LT	12	1
15+09	23.4' LT	12	1
	TOTAL	24	2

Wisconsin Department of Transportation

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through innovation and exceptional service.

http://www.dot.wisconsin.gov

SWL JULY 2014

PROJECT ID: 5916-00-71
WITH: 5916-00-68, 5916-00-69, 5916-00-72

COUNTY: SAUK

ORDER OF SHEETS

TOTAL SHEETS = 62

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

VILLAGE OF SPRING GREEN, EAST JEFFERSON STREET

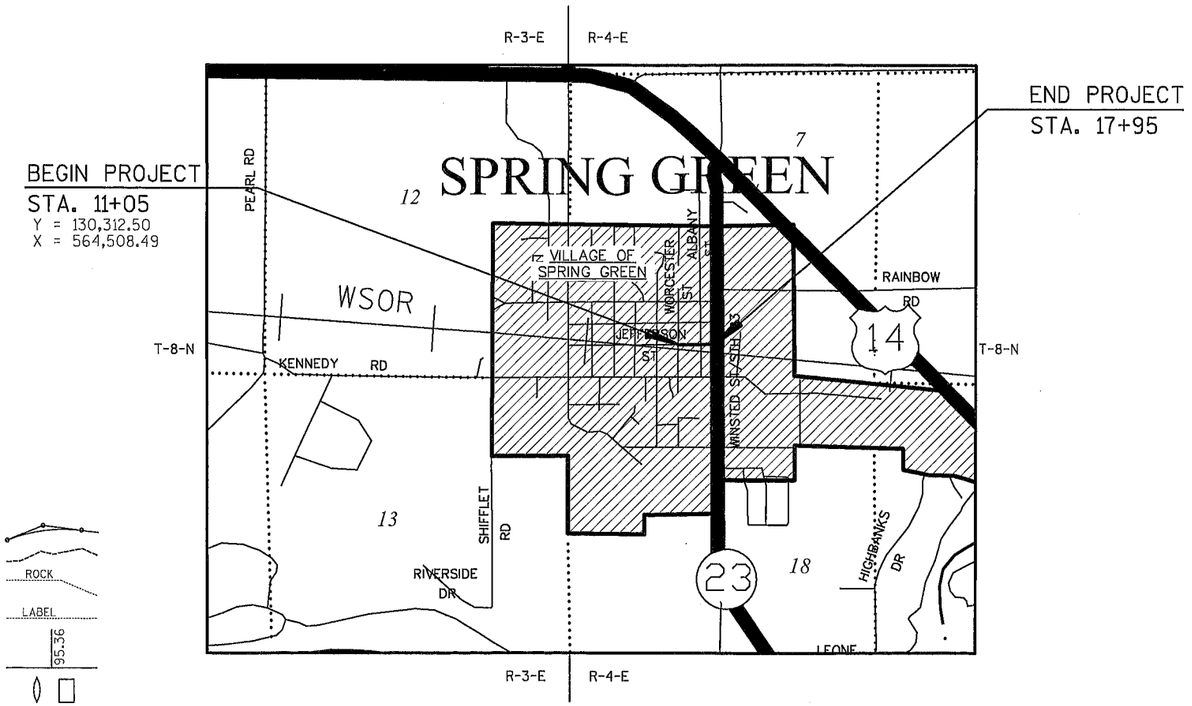
(WORCESTER STREET TO STH 23)

LOCAL STREET

SAUK COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5916-00-71	WISC 2014262	1

STATE PROJECT NUMBER
5916-00-71



DESIGN DESIGNATION

A.A.D.T.	2014	= 1115
A.A.D.T.	2034	= 1360
D.H.V.	2034	= 120
D.D.		= 62/38
T.		= 6.5%
DESIGN SPEED		= 30 MPH
ESALS		= 182,500

CONVENTIONAL SYMBOLS

PLAN
- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE
- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES
- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

LAYOUT
SCALE 0 1/2 MI.

TOTAL NET LENGTH OF CENTERLINE = 0.131 MI.

Coordinates on this plan are referenced to the Wisconsin County Coordinate System (WCCS), Sauk County.

ACCEPTED FOR
VILLAGE of SPRING GREEN
1-24-14 (Date) (Village President)

ORIGINAL PLANS PREPARED BY
JEWELL associates engineers, inc.
Engineers - Planners - Surveyors

WISCONSIN
ELLERY A. SCHAFFER
E-41742-6
SPRING GREEN, WI
PROFESSIONAL ENGINEER

1/24/2014 (Date) (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor JEWELL ASSOCIATES ENGINEERS, INC.
Designer JEWELL ASSOCIATES ENGINEERS, INC.
Management Consultant KJOHNSON ENGINEERS, INC.

APPROVED FOR THE DEPARTMENT
DATE: 1/31/14
Kirby A. Johnson
MANAGEMENT CONSULTANT SIGNATURE

E

FILE NAME: S34083 - Title Sheet

LIST OF STANDARD ABBREVIATIONS

ABUT	Abutment
AC	Acre
AGG	Aggregate
AH	Ahead
<	Angle
ASPH	Asphaltic
AVG	Average
ADT	Average Daily Traffic
BAD	Base Aggregate Dense
BK	Back
BF	Back Face
BM	Bench Mark
BR	Bridge
C or C/L	Center Line
CC	Center to Center
CTH	County Trunk Highway
CR	Creek
CR	Crushed
CY or CU YD	Cubic Yard
CP	Culvert Pipe
C & G	Curb and Gutter
D	Degree of Curve
DHV	Design Hour Volume
DIA	Diameter
E	East
X	East Grid Coordinate
ELEC	Electric (al)
EL or ELEV	Elevation
ESALS	Equivalent Single Axle Loads
EBS	Excavation Below Subgrade
FF	Face to Face
FE	Field Entrance
F	Fill
FG	Finished Grade
FL or F/L	Flow Line
FT	Foot
FTG	Footing
GN	Grid North
HT	Height
CWT	Hundredweight
HYD	Hydrant
INL	Inlet
ID	Inside Diameter
INV	Invert
IP	Iron Pipe or Pin
IRS	Iron Rod Set
JT	Joint
JCT	Junction
LHF	Left-Hand Forward
L	Length of Curve
LIN FT or LF	Linear Foot
LC	Long Chord of Curve
MH	Manhole
MB	Mailbox
ML or M/L	Match Line
N	North
Y	North Grid Coordinate
OD	Outside Diameter
PLE	Permanent Limited Easement
PT	Point
PC	Point of Curvature
PI	Point of Intersection
PRC	Point of Reverse Curvature
PT	Point of Tangency
POC	Point On Curve
POT	Point on Tangent
PVC	Polyvinyl Chloride
PCC	Portland Cement Concrete
LB	Pound
PSI	Pounds Per Square Inch
PE	Private Entrance
R	Radius
RR	Railroad
R	Range
RL or R/L	Reference Line
RP	Reference Point
RCCP	Reinforced Concrete Culvert Pipe
REQD	Required
RES	Residence or Residential
RW	Retaining Wall
RT	Right
RHF	Right-Hand Forward
R/W	Right-of-Way
R	River
RD	Road
RDWY	Roadway
SALV	Salvaged
SAN S	Sanitary Sewer
SEC	Section
SHLDR	Shoulder
SHR	Shrinkage
SW	Sidewalk
S	South
SQ	Square
SF or SQ FT	Square Feet
SY or SQ YD	Square Yard
STD	Standard
SDD	Standard Detail Drawings
STH	State Trunk Highways
STA	Station
SS	Storm Sewer
SG	Subgrade
SE	Superelevation
SL or S/L	Survey Line
SV	Septic Vent
T	Tangent
TEL	Telephone
TEMP	Temporary
TI	Temporary Interest
TLE	Temporary Limited Easement
t	Ton
T or TN	Town
TRANS	Transition
TL or T/L	Transit Line
T	Trucks (percent of)
TYP	Typical
UNCL	Unclassified
UG	Underground Cable
USH	United States Highway
VAR	Variable
V	Velocity or Design Speed
VERT	Vertical
VC	Vertical Curve
VOL	Volume
WM	Water Main
WV	Water Valve
W	West
WB	Westbound
YD	Yard

UTILITIES

ELECTRIC
ALLIANT ENERGY
142 S. CINCINNATI STREET
P.O. BOX 99
SPRING GREEN, WI 53588
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EMAIL: chriswilhelm@alliantenergy.com

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DODGEVILLE, WI 53533
PH: (608) 576-2613
ATTN: STEVE HEGGE
EMAIL: steve.hegge@chartercom.com

GAS
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ATTN: CHRIS WILHELM
EMAIL: chriswilhelm@alliantenergy.com

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VILLAGE OF SPRING GREEN
112 W. MONROE STREET
SPRING GREEN, WI 53588
CELL: (608) 588-4983
OFFICE: (608) 588-2335
ATTN: GREG WIPPERFURTH

FIBER OPTIC
CHARTER COMMUNICATIONS
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ATTN: STEVE HEGGE
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WINDSTREAM KDL
13935 BISHOPS DRIVE
BROOKFIELD, WI 53005
PH: (262) 792-7938
ATTN: JIM KOSTUCH
EMAIL: james.kostuch@windstream.com

DIGGERS HOTLINE
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www.DiggersHotline.com

* DENOTES UTILITY IS NOT A MEMBER OF DIGGERS HOTLINE

GENERAL NOTES

COORDINATES AND BEARINGS ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), SAUK COUNTY.

ELEVATIONS SHOWN ON THE PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88)

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

EXCAVATION BELOW SUBGRADE (EBS) IS NOT USED TO BALANCE YARDAGE, AND IS NOT SHOWN ON THE CROSS SECTIONS BUT IS MEASURED AND PAID FOR AS COMMON EXCAVATION. EXACT LOCATIONS OF EBS WILL BE DETERMINED BY THE ENGINEER.

DISTURBED AREAS SHOWN WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS ARE TO BE FERTILIZED (TYPE B), SEEDED (USE SEED MIX NO. 40) AND MULCHED AS DIRECTED BY THE ENGINEER.

WHEN THE QUANTITY OF THE ITEM OF BASE AGGREGATE DENSE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER IN THE FIELD.

EROSION CONTROL ITEMS SHALL BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER IN THE FIELD. SILT FENCE SHALL BE PLACED PRIOR TO CONSTRUCTION AND IN PLACE PRIOR TO STRUCTURE REMOVAL.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

FILL EXPANSION IS VARIABLE AND IS ESTIMATED AT 25%.

THE EXACT LOCATION OF PRIVATE AND COMMERCIAL ENTRANCES TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

REMOVAL OF ASPHALTIC SURFACES WHERE AN ABUTTING ASPHALTIC SURFACE IS TO REMAIN IN PLACE SHALL REQUIRE A SAWCUT MEETING THE APPROVAL OF THE ENGINEER IN THE FIELD.

ACCURACY OF INLET AND DISCHARGE ELEVATIONS FOR DRAINAGE STRUCTURES SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

CURB AND GUTTER ELEVATIONS ARE GIVEN ON THE FLANGE LINE, UNLESS OTHERWISE NOTED.

ALL RADII DIMENSIONS ON THE PLAN FOR CURB AND GUTTER ARE TO THE FLANGE OF THE CURB AND GUTTER.

EXISTING DRIVEWAYS SHALL BE RESTORED IN KIND AND THEIR LOCATION VERIFIED BY THE ENGINEER IN THE FIELD.

HMA PAVEMENT TYPE E-0.3 QUANTITIES WERE CALCULATED USING 115 LB/SY/IN.

4-INCHES OF HMA PAVEMENT TYPE E-0.3 SHALL BE CONSTRUCTED WITH A 1 3/4-INCH UPPER LAYER AND A 2 1/4-INCH LOWER LAYER. ASPHALTIC MATERIAL PG 64-28 SHALL BE USED ON THE UPPER LAYER AND ASPHALTIC MATERIAL PG 58-28 SHALL BE USED ON THE LOWER LAYER.

EXPANSION JOINTS SHALL BE CONSTRUCTED AT ALL RADII POINTS IN THE CURB & GUTTER.

THE COST OF CONNECTING CULVERT PIPE OR STORM SEWER TO EXISTING DRAINAGE STRUCTURES SHALL BE INCIDENTAL TO THE COST OF INSTALLING THE CULVERT OR STORM SEWER.

STORM SEWER ELEVATIONS, LENGTHS, AND LOCATIONS AS SHOWN ON THE PLANS AND CROSS SECTIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.

MISCELLANEOUS REMOVAL ITEMS REQUIRING RESTORATIONS OF CONCRETE OR ASPHALT DRIVEWAYS, SIDEWALKS, OR SIDE STREETS SHALL BE REMOVED TO AN EXISTING JOINT OR SAWED AS DETERMINED BY THE ENGINEER IN THE FIELD OR AS SHOWN ON THE PLANS.

TRANSVERSE JOINTS IN CONCRETE SIDEWALK SHALL BE CONSTRUCTED AT INTERVALS EQUAL TO THE WIDTH OF THE CONCRETE SIDEWALK, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

NO STORM SEWER SHALL BE REMOVED UNTIL DIRECTED BY THE ENGINEER.

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA= 1.20 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 1.12 ACRES

CONTACTS

DESIGN CONSULTANT:
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EUGENE HAUSNER, VILLAGE PRESIDENT
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WDNR LIAISON:
STATE OF WISCONSIN
DEPT. OF NATURAL RESOURCES
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
ATTN: CATHY BLESER
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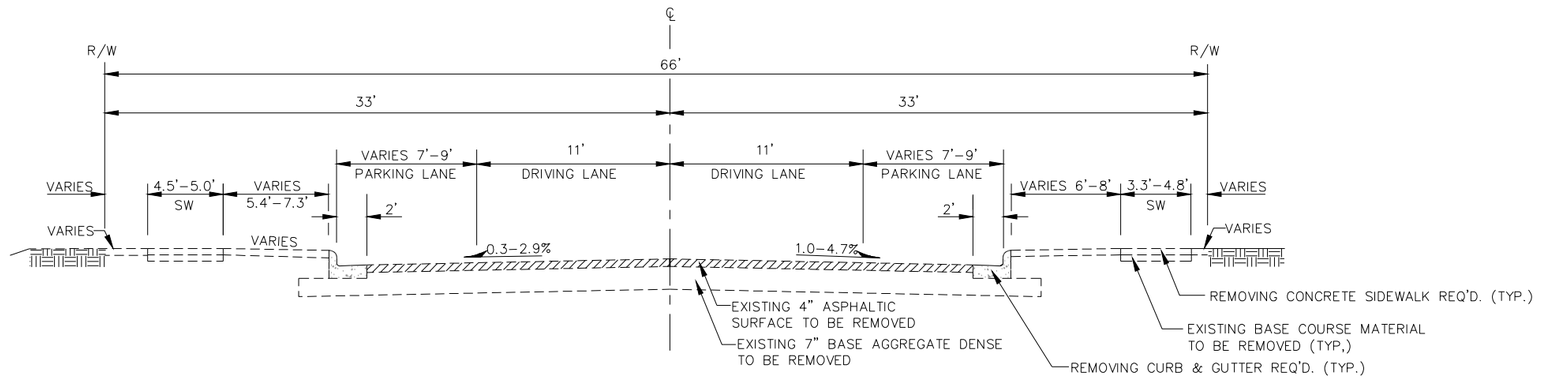
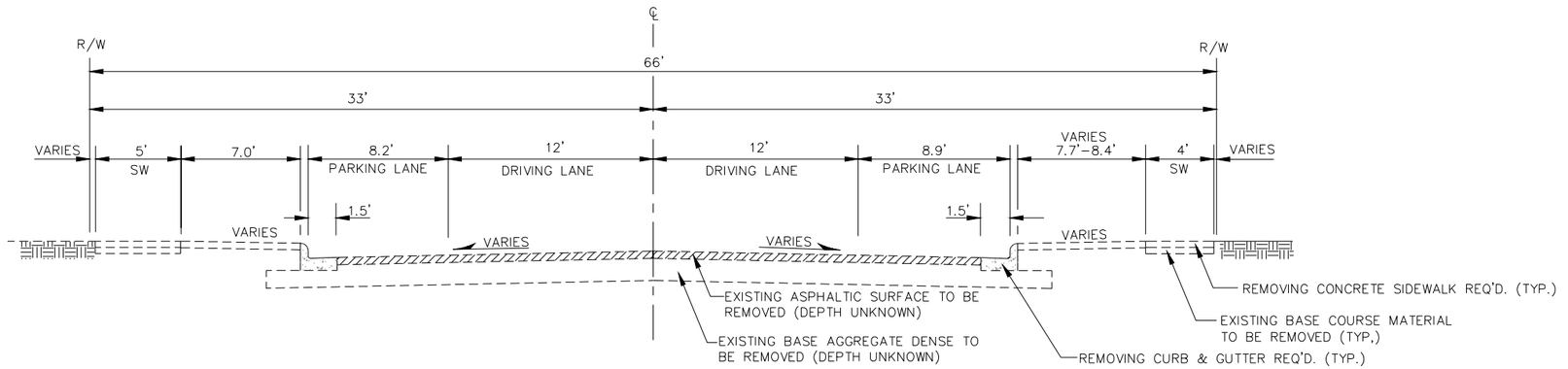
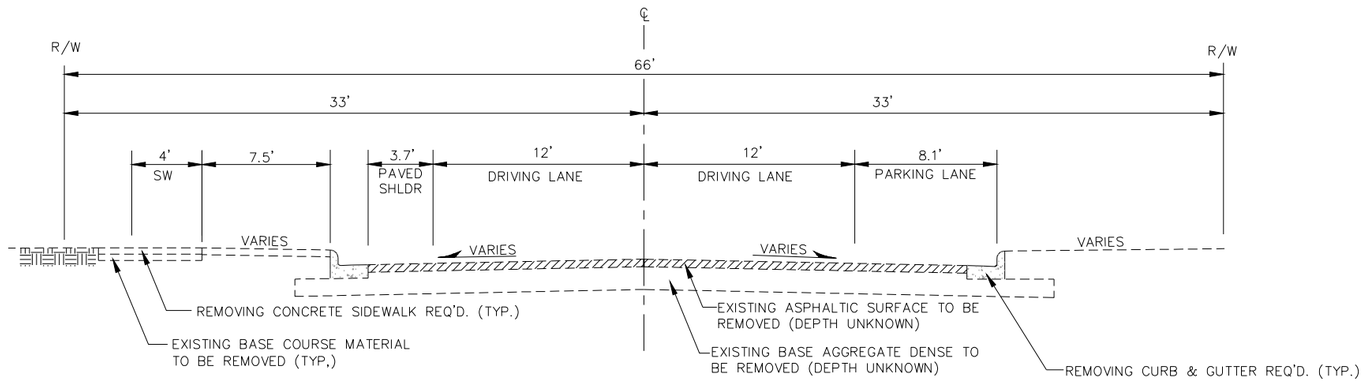
ORDER OF SECTION 2 SHEETS



TYPICAL EXISTING SECTION
(EAST JEFFERSON STREET)

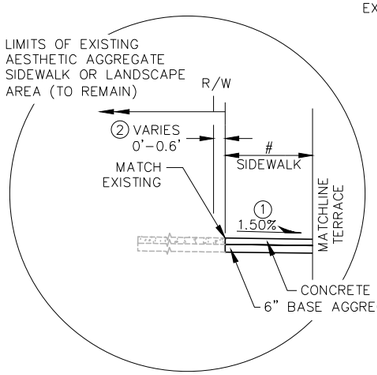


TYPICAL EXISTING SECTION
(NORTH ALBANY STREET)



TYPICAL EXISTING SECTION
(SOUTH ALBANY STREET)

PROJECT NO: 5916–00–71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	TYPICAL EXISTING SECTION	SHEET 3	E

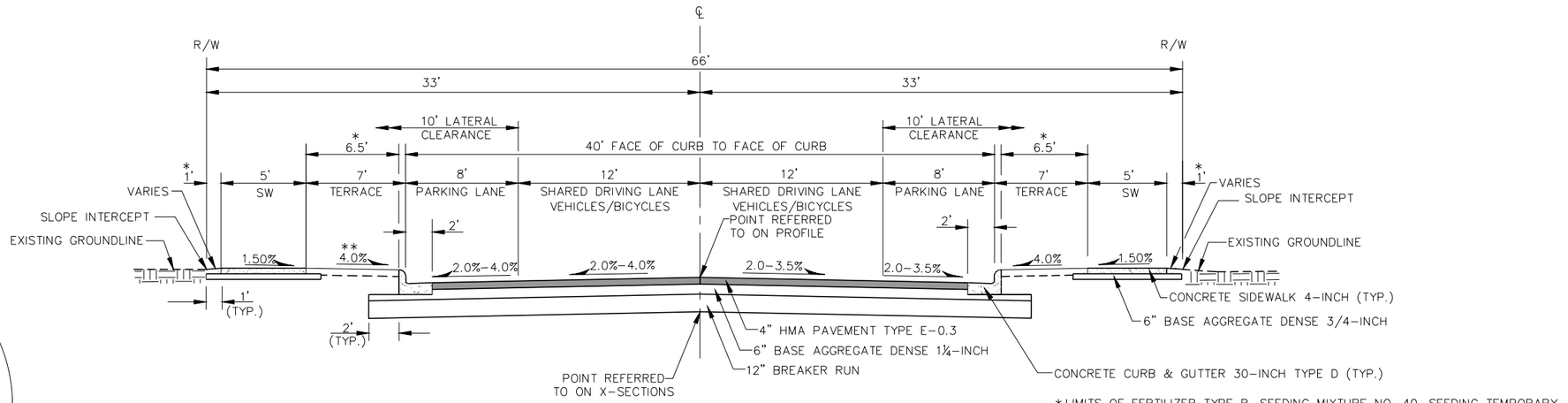


PARTIAL TYPICAL FINISHED SECTION

STA. – STA.	# FT.
11+40 – 12+48, LT.	6
13+71 – 14+42, LT.	5.4'

① UNLESS NOTED OTHERWISE

② STA. 14+20 – STA. 14+42, LT. CONCRETE CURB PEDESTRIAN REQ'D

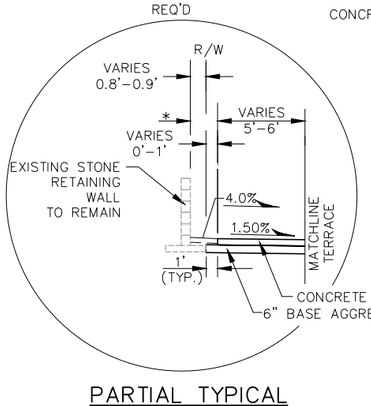


TYPICAL FINISHED SECTION

(EAST JEFFERSON STREET)

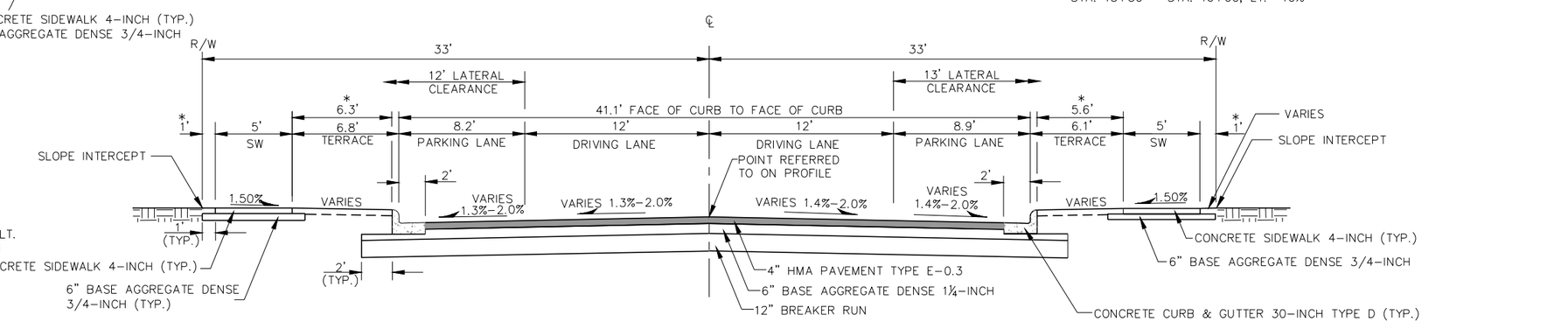
* LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, SEEDING TEMPORARY, MULCHING, AND TOPSOIL (AS DIRECTED BY ENGINEER)

** STA. 11+40 – STA. 12+48, LT. VARIES 4.0% – 13.0%
STA. 15+50 – STA. 16+00, LT. 10%



PARTIAL TYPICAL FINISHED SECTION

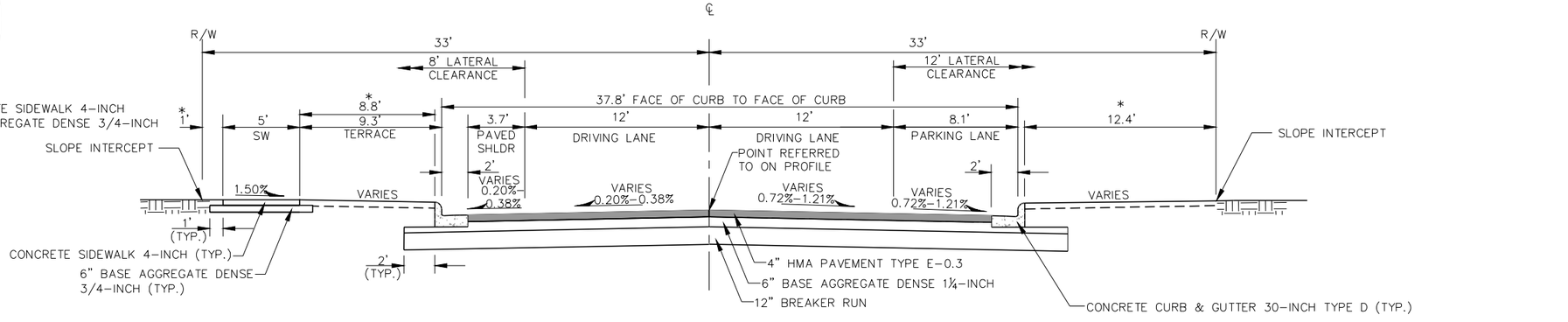
STA. 12+74 – STA. 12+92, LT.



TYPICAL FINISHED SECTION

(NORTH ALBANY STREET)

* LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, SEEDING TEMPORARY, MULCHING, AND TOPSOIL (AS DIRECTED BY ENGINEER)

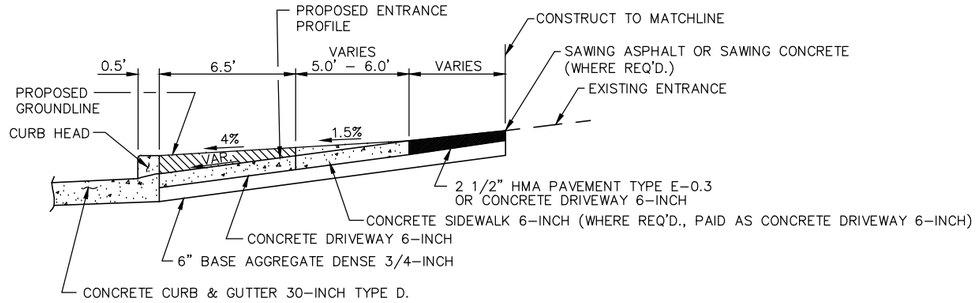


TYPICAL FINISHED SECTION

(SOUTH ALBANY STREET)

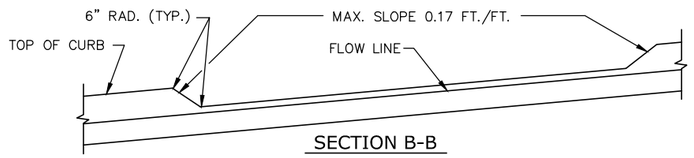
* LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, SEEDING TEMPORARY, MULCHING, AND TOPSOIL (AS DIRECTED BY ENGINEER)

PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	TYPICAL FINISHED SECTION	SHEET 4	E

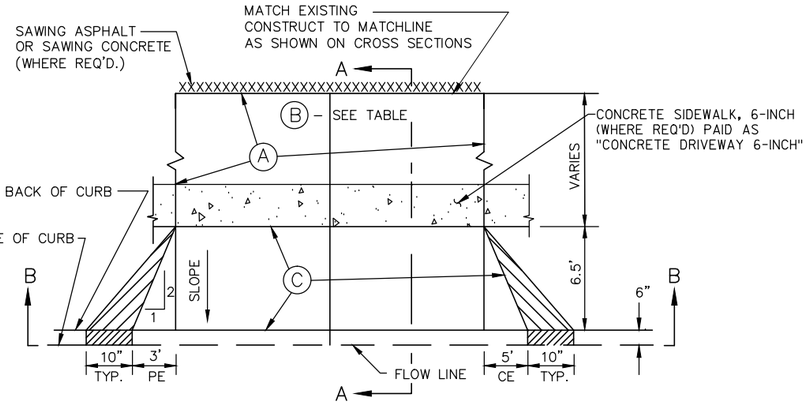


SECTION A-A

(AT CONCRETE SIDEWALK)

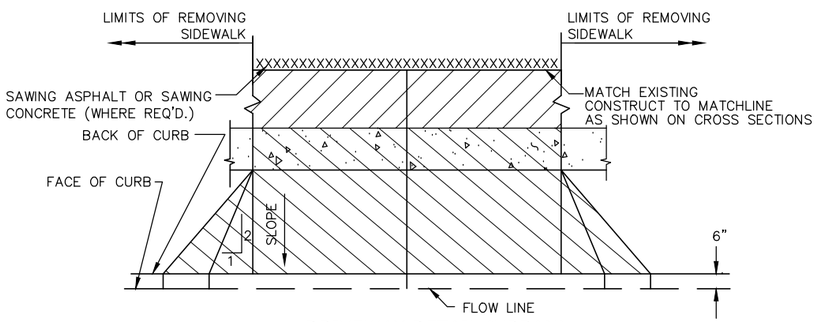


SECTION B-B



DRIVEWAY DETAIL (URBAN)

(WHERE REQ'D.)



DRIVEWAY DETAIL (URBAN)

(WHERE REQ'D.)

LIMITS OF REMOVING PAVEMENT

LIMITS OF REMOVING CONCRETE PAID UNDER REMOVING PAVEMENT BID ITEM OR REMOVING ASPHALT/AGGREGATE PAID UNDER EXCAVATION COMMON BID ITEM.

ENTRANCE DATA TABLE

STATION	LOCATION	TYPE	"A" PAVEMENT STRUCTURE	"B" WIDTH	MATCH EXISTING AT
12+61	LT.	C.E.	2 ½" HMA PAVEMENT TYPE E-0.3 OVER 6" BAD 3/4-INCH	VARIES 26' - 30.7'	38.0'
12+87	RT.	P.E.	2 ½" HMA PAVEMENT TYPE E-0.3 OVER 6" BAD 3/4-INCH	VARIES 6.1' - 12'	37.0'
13+06	LT.	P.E.	CONCRETE DRIVEWAY 6-INCH OVER 6" BAD 3/4-INCH	VARIES 7.1' - 12'	39.0'
14+09	LT.	C.E.	2 ½" HMA PAVEMENT TYPE E-0.3 OVER 6" BAD 3/4-INCH	21.4'	37.0'
17+47	LT.	P.E.	2 ½" HMA PAVEMENT TYPE E-0.3 OVER 6" BAD 3/4-INCH	10.6' - 12'	37.0'

Ⓐ

EXISTING	PROPOSED
BASE AGGREGATE DENSE 3/4-INCH	2 1/2" HMA PAVEMENT TYPE E-0.3 OVER 6" BASE AGGREGATE DENSE 3/4-INCH
ASPHALT	2 1/2" HMA PAVEMENT TYPE E-0.3 OVER 6" BASE AGGREGATE DENSE 3/4-INCH
CONCRETE	CONCRETE DRIVEWAY 6-INCH OVER 6" BASE AGGREGATE DENSE 3/4-INCH

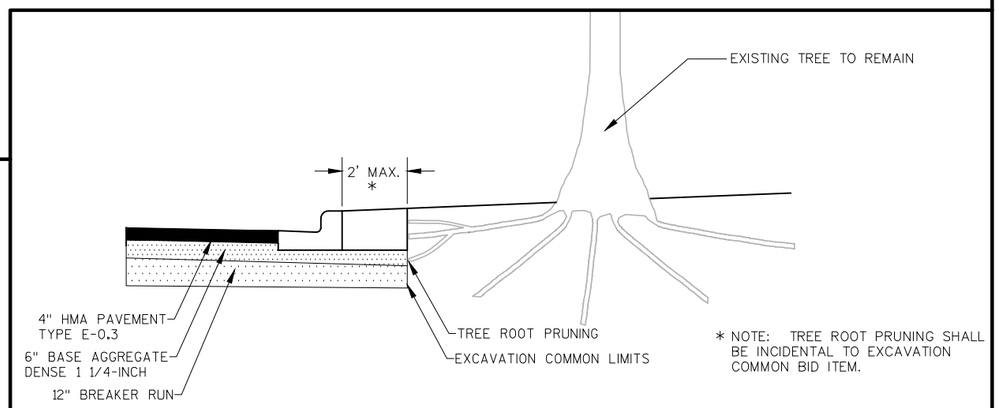
Ⓑ WIDTH

P.E.'S – MATCH EXISTING OR 12' MIN. – 24' MAX.

C.E.'S – MATCH EXISTING OR 35' MAX.

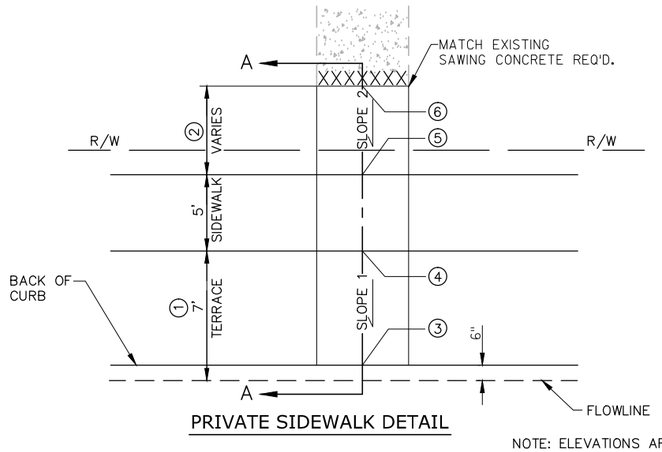
Ⓒ

EXISTING	PROPOSED
ALL TYPES	CONCRETE DRIVEWAY 6-INCH OVER 6" BASE AGGREGATE DENSE 3/4-INCH



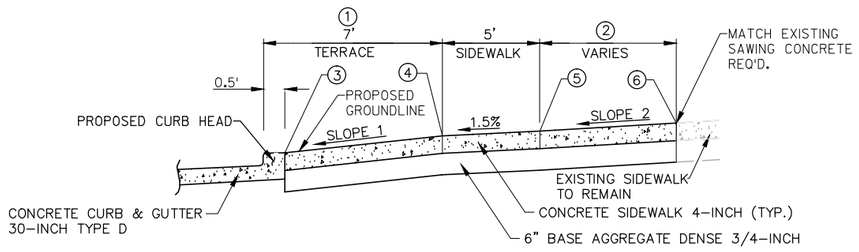
TREE PRESERVATION DETAIL

STA. 11+65, LT.
STA. 11+93, LT.
STA. 12+27, LT.
STA. 12+87, LT.
STA. 13+43, LT.
STA. 17+57, RT.
STA. 17+62, LT.
STA. 17+80, LT.



PRIVATE SIDEWALK DETAIL

NOTE: ELEVATIONS ARE TO ℄ OF SIDEWALK



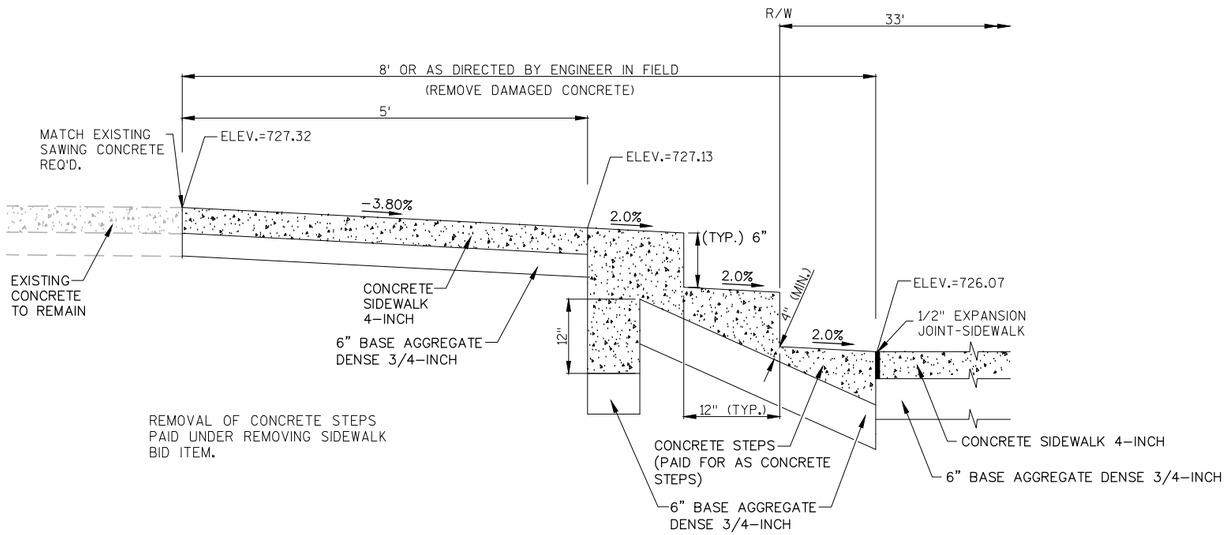
SECTION A-A

PRIVATE SIDEWALK DATA TABLE

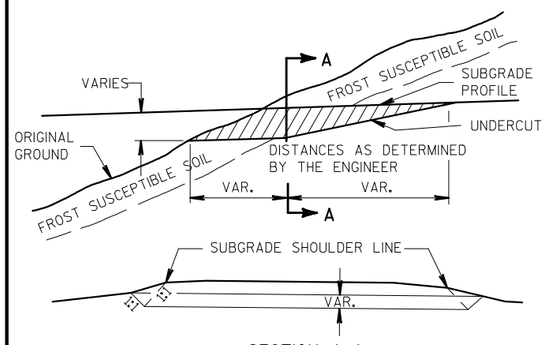
STATION	LOCATION	WIDTH "B" (FT.)	SAWCUT OFFSET FROM ℄ (FT.)	① APPLICABLE (Y/N)	② APPLICABLE (Y/N)	③	④	⑤	⑥	SLOPE 1	SLOPE 2
11+76	RT.	8.8'	40.0'	N	Y	-	-	725.31	725.69	-	+4.75%
12+68	RT.	3.9'	37.0'	N	Y	-	-	725.55	725.66	-	+2.20%
13+22	RT.	3.2'	47.0'	N	Y	-	-	726.03	725.45	-	-3.87%
#13+34	LT.	4.2'	40.0'	N	Y	-	-	-	-	-	-
14+01	RT.	5.0'	50.3'	Y	Y	725.40	725.66	725.74	725.29	+4.00%	-2.46%
15+44	RT.	4.6'	45.0'	N	Y	-	-	724.34	724.87	-	+4.08%
15+61	LT.	4.0'	59.0'	N	Y	-	-	724.73	726.94	-	-8.19%
15+93	LT.	2.9'	62.0'	N	Y	-	-	724.47	726.97	-	-8.33%
15+95	RT.	3.5'	41.3'	Y	Y	723.71	723.97	724.05	724.62	+4.00%	+6.13%
16+33	RT.	3.7	38.0'	Y	Y	723.55	723.81	723.89	724.25	+4.00%	+6.00%
16+94	RT.	2.8'	48.0'	Y	Y	723.28	723.54	723.62	724.73	+4.00%	+6.94%
16+99	LT.	3.7'	43.0'	Y	Y	723.26	723.52	723.60	724.13	-4.00%	-4.82%
17+64	RT.	2.8'	38.0'	N	Y	-	-	723.54	723.78	-	+4.00%
17+87	LT.	3.9'	38.3'	N	Y	-	-	723.56	723.62	-	-0.95%

SEE "CONCRETE STEP DETAIL" FOR ADDITIONAL INFORMATION"

NOTE: STA. 15+07, LT. – SEE INTERSECTION DETAILS FOR ADDITIONAL INFORMATION.

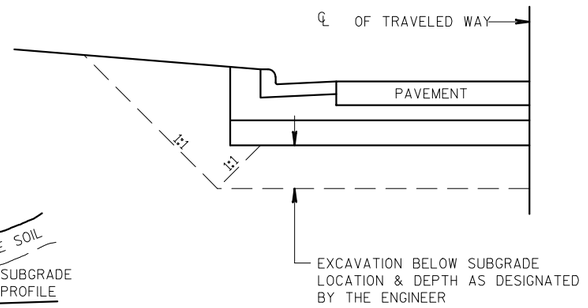


CONCRETE STEP DETAIL
(STA. 13+34, LT.)



SECTION A-A
CROSS SECTION SHOWING UNDERCUT

DETAIL FOR EXCAVATION BELOW SUBGRADE AT CUTS



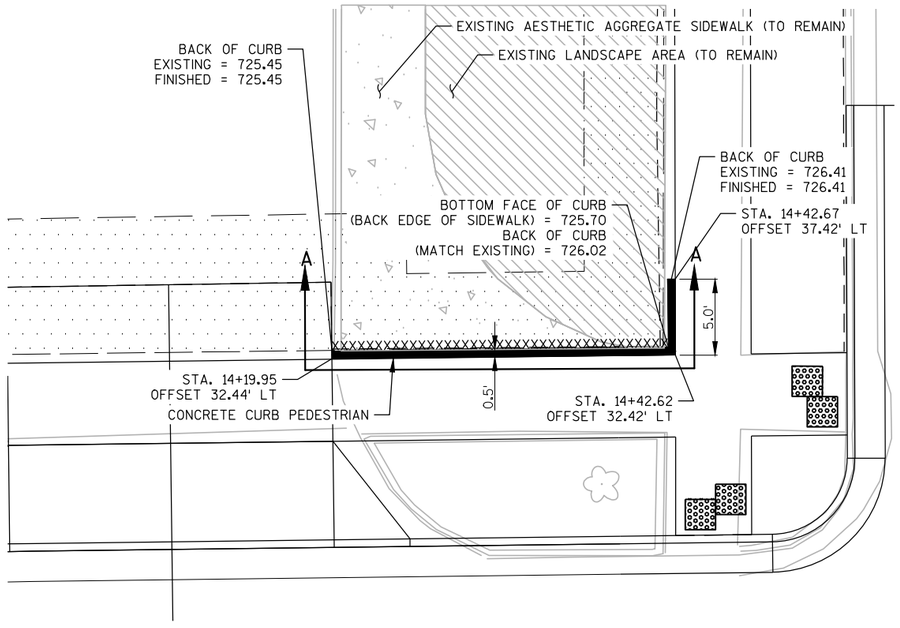
DETAIL FOR EXCAVATION BELOW SUBGRADE

NOTE: EXACT LOCATIONS AND EXTENT OF E.B.S. SECTIONS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

E.B.S. AREA TO BE BACKFILLED WITH BREAKER RUN.

THE FILL SECTION WITHIN 100' OF THE MOUTH OF THE CUT MUST BE KEPT 2' BELOW SUBGRADE UNTIL E.B.S. IS COMPLETED.

2

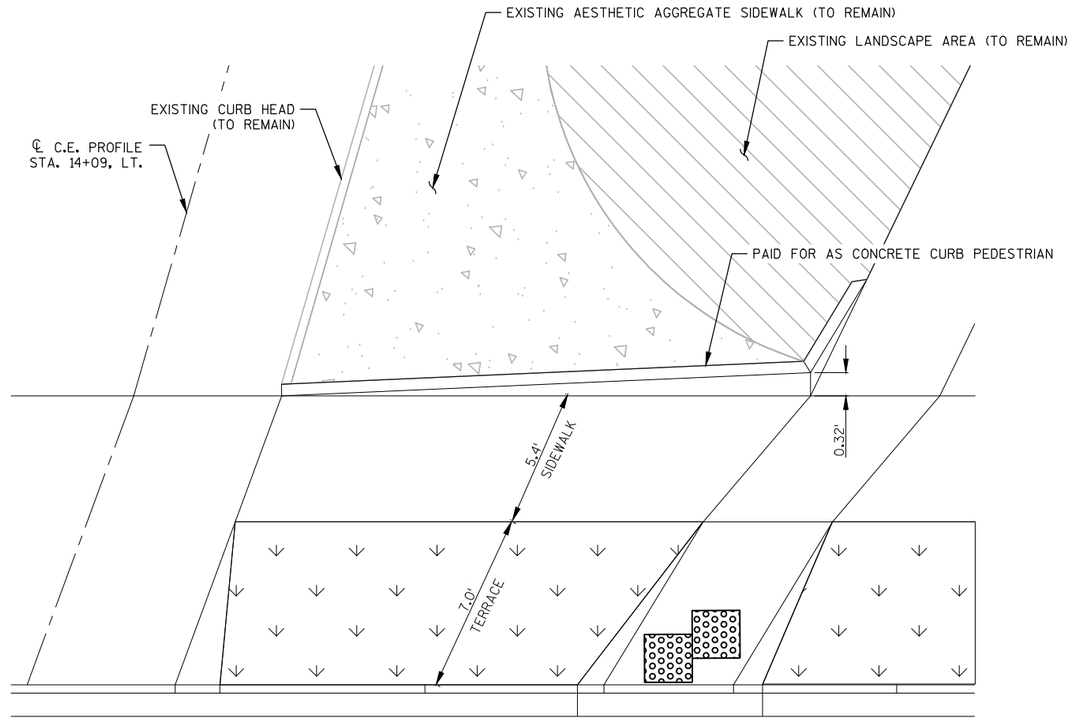


CONCRETE CURB PEDESTRIAN DETAIL

STA. 14+20 - STA. 14+42, LT.
STA. 100+73 - STA. 100+78, LT

NOTE: REMOVAL OF AESTHETIC AGGREGATE SIDEWALK FOR CONCRETE CURB PEDESTRIAN PLACEMENT IS INCIDENTAL TO EXCAVATION COMMON BID ITEM.

SEE EAST JEFFERSON STREET & ALBANY STREET INTERSECTION DETAIL FOR ADDITIONAL INFORMATION.



SECTION A-A

PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	CONSTRUCTION DETAILS	SHEET --7	E

S:±Projects±S34083 Village of SG Jefferson Street±CADD Files±S34083 Jefferson CADD±S34083 Jefferson Roadway±Intersection Details±Jefferson Street ALBANY Intersection Detail DRAWINGS.dwg

2

EXISTING CURB AND GUTTER TO REMAIN (TYP.)

EXISTING CONCRETE SIDEWALK TO REMAIN

EXISTING AESTHETIC AGGREGATE SIDEWALK (TO REMAIN)

EXISTING LANDSCAPE AREA (TO REMAIN)

STA. 12+61, LT.
CONSTRUCT C.E.
(ASPHALT)

EXISTING RETAINING WALLS TO REMAIN

STA. 12+74
33.0' LT.

STA. 13+06, LT.
CONSTRUCT P.E.
(CONCRETE)

EXISTING LANDSCAPE AREAS TO REMAIN

N

BUILDING

WORCESTER STREET

MATCH EXISTING AND SAWING ASPHALT / SAWING CONCRETE REQ'D

SLOPE INTERCEPTS

~ PARKING LOT ~

STA 11+05
45.0' LT.

20.0'

725.39

6.3'

R/W

725.98

725.91

STA. 11+37.92
30.5' LT.
Y=130,343.41
X=564,541.01

725.98

725.75

725.66

STA. 12+92
32.0' LT.

BUILDING

7.0'

10:1

5.6'

6.0'

CONCRETE CURB & GUTTER 30 INCH TYPE D. (TYP.)

R/W

R/W

R/W

725.23

R=12.5'

6.0'

5.0'

6.00%

8.30%

7.0'

13.5%

725.14

725.06

BEGIN PROJECT
STA. 11+05

EAST JEFFERSON STREET

2.0%

18.0'

FINISHED ℄ EAST JEFFERSON STREET

12+00

13+00

2.00%

2.00%

2.50%

2.50%

FG=725.28

MATCHLINE STA. 13+25

MATCH EXISTING AND SAWING ASPHALT STA. 11+05

724.86

724.95

3.0%

18.0'

7.0'

R=17.0'

R/W

R/W

R/W

724.85

6.0'

5.0'

1.0'

5.0'

1:5

7.5'

20.0'

725.00

STA. 11+41.39
35.0' RT.
Y=130,277.95
X=564,545.32

CONCRETE SIDEWALK 4-INCH (TYP.)

STA 11+05
45.0' RT.

MATCH EXISTING AND SAWING ASPHALT / SAWING CONCRETE REQ'D

EXISTING LANDSCAPE AREA TO REMAIN

STA. 12+87, RT.
CONSTRUCT P.E. (ASPHALT)

BUILDING

BUILDING

BUILDING

BUILDING

WORCESTER STREET

NOTE: STA. 11+40 - STA. 12+92, LT.
MATCH EXISTING AT PROPOSED BACK EDGE OF CONCRETE SIDEWALK 4-INCH (EXCEPT IN AREA OF DRIVEWAY CONSTRUCTION)

LEGEND

- TRAFFIC DIRECTION
- DIRECTION OF FLOW
- XXXXXXXXX SAWING ASPHALT/SAWING CONCRETE REQ'D
- CURB RAMP DETECTABLE WARNING FIELD - TYPE 1

EAST JEFFERSON STREET & WORCESTER STREET

PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	INTERSECTION DETAILS	SHEET --8	E

S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\CADD FILES\S34083 JEFFERSON CADD\S34083 JEFFERSON ROADWAY\INTERSECTION DETAILS\JEFFERSON STREET WORCESTER INTERSECTION DETAILS.DWG

2

N

BUILDING

EXISTING LANDSCAPE AREAS TO REMAIN

CONCRETE STEPS REQ'D SEE DETAIL

STA. 13+61 32.0' LT

STA. 13+71 32.4' LT.

STA. 14+09, LT. CONSTRUCT C.E. (ASPHALT)

EXISTING LANDSCAPE AREA TO REMAIN

EXISTING CURB HEAD TO REMAIN (PROPOSED DRIVEWAY TO MATCH EXISTING GRADES AT CURB HEAD)

STA. 14+48.87, 25.5' LT. (EAST JEFFERSON STREET)=
STA. 100+66.72, 25.87' LT. (NORTH ALBANY STREET)
Y=130,342.31
X=564,852.01

EL. 725.92

EL. 725.90

EL. 726.23

EL. 725.59

EL. 725.45

R/W

EXISTING AESTHETIC AGGREGATE SIDEWALK (TO REMAIN)

MATCH AT EXISTING JOINT

18.2

18.9'

BUILDING

R/W

END CONSTRUCTION
STA. 100+90

NORTH ALBANY STREET

MATCH EXISTING AND SAWING ASPHALT/SAWING CONCRETE REQ'D

20:1

10.0'

21.2'

10.0'

10:1

R/W

STA. 15+18
46.8' LT.
EL.=726.62

STA. 15+10.53, 35.0' LT. (EAST JEFFERSON STREET)=
STA. 100+76.87, 35.88' RT. (NORTH ALBANY STREET)
Y=130,352.58
X=564,913.55

8.25%

R/W

725.79

725.93

725.17

1.0'

5.0'

6.3'

18.2'

18.8'

SEE CONCRETE CURB PEDESTRIAN DETAIL

1.30%

2.00%

R=7.50'

R=17.0'

724.81

724.80

724.78

724.68

724.18

FINISHED ℄ NORTH ALBANY STREET

FINISHED ℄ EAST JEFFERSON STREET

5.0'

5.4'

7.0'

18.0'

3.00%

3.60%

4.00%

2.00%

90°36'15"

14+00

15+00

16+

F.C.=725.52

MATCHLINE STA. 13+25

MATCHLINE STA. 16+00
SEE PLAN & PROFILE SHEET
(STA. 16+00 - STA. 17+95)

CONCRETE CURB & GUTTER 30-INCH TYPE D (TYP.)

8°23'45"

3.40%

3.50%

724.79

724.52

724.01

FINISHED ℄ SOUTH ALBANY STREET

724.07

724.12

724.15

724.16

724.21

15.7'

18.1"

100.00

R=17.0'

R/W

3.89%

725.21

725.41

5.1'

CONCRETE SIDEWALK 4-INCH (TYP.)

STA. 14+41.13, 35.0' RT. (EAST JEFFERSON STREET)=
STA. 100+06.14, 32.78' RT. (SOUTH ALBANY STREET)
Y=130,281.72
X=564,845.03

STA. 15+08.97, 35.0' RT. (EAST JEFFERSON STREET)=
STA. 100+06.85, 35.06' RT. (SOUTH ALBANY STREET)
Y=103,282.57
X=564,912.87

BUILDING

MATCH EXISTING AND SAWING ASPHALT/SAWING CONCRETE REQ'D

SOUTH ALBANY STREET

BEGIN CONSTRUCTION
STA. 100+00

STA. 14+74.28 (EAST JEFFERSON STREET)=
STA. 100+41.49 (ALBANY STREET)

EAST JEFFERSON STREET & ALBANY STREET

LEGEND

- TRAFFIC DIRECTION
- DIRECTION OF FLOW
- XXXXXXXXX SAWING ASPHALT/SAWING CONCRETE REQ'D
- CURB RAMP DETECTABLE WARNING FIELD-TYPE 1
- CURB RAMP DETECTABLE WARNING FIELD - TYPE 2

NOTE: -ALL OTHER SIDEWALK LAYOUT INFORMATION IS PROVIDED IN "SIDEWALK DETAIL."

-CONSTRUCT STA. 13+71, LT. WITH A TERRACE CROSS SLOPE = 3.0% AND A SIDEWALK CROSS SLOPE = 1.0% TO MATCH EXISTING AT EXISTING CURB.

-STA. 13+71 - STA. 14+20, LT MATCH EXISTING AT PROPOSED BACK EDGE OF CONCRETE SIDEWALK 4-INCH

PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	INTERSECTION DETAILS	SHEET --9	E

S:\Projects\S34083 Village of SG Jefferson Street\CADD Files\S34083 Jefferson CADD\S34083 Jefferson Roadway\Intersection Details\Jefferson Street ALBANY Intersection Details.dwg

HERBERT P KRITZ

C/O BMO HARRIS BANK NA
BANK OF SPRING GREEN

ROGER A BOWDEN

HERBERT P KRITZ

PECKHAM & NEIDER LLP

VILLAGE OF SPRING GREEN (TYP.)

ADJUST EXISTING INLET

REMOVE AND REPLACE EXISTING MANHOLE

I-9
STA:11+95.95 OFF:18.00LT
RIM:724.60

P-15

I-10
STA:12+05.92 OFF:18.00LT
RIM:724.60

STMH-5
STA:11+27.41 OFF:-1.48LT
RIM:725.48

FINISHED ℄ EAST JEFFERSON STREET

REMOVE EXISTING INLET

I-6
STA:14+37.68 OFF:18.00LT
RIM:724.70

ALLIANT ENERGY (TYP.)

I-5
STA:14+56.48 OFF:-35.84LT
RIM:725.18

I-3
STA:14+93.52 OFF:-35.30LT
RIM:725.17

P-14

STMH-4
STA:12+05.94 OFF:-1.50LT
RIM:724.95
N89°16'51"E

P-12

REMOVE EXISTING MANHOLE

EX P-6

EX P-4

EX P-5

P-5

P-4

P-2

P-9

P-8

12+00

13+00

14+00

15+00

16+

P-16

P-17

P-10

STMH-3
STA:14+37.66 OFF:-1.54LT
RIM:725.32

ALLIANT ENERGY (TYP.)

CAUTION

REMOVE EXISTING INLET

I-12
STA:12+16.11 OFF:18.00RT
RIM:724.40

CONTRACTOR TO VERIFY EXISTING PIPE INVERTS

I-11
STA:12+06.04 OFF:18.00RT
RIM:724.41

ADJUST EXISTING INLET

ALLIANT ENERGY (TYP.)

REMOVE EXISTING STORM SEWER (TYP.)

I-7
STA:14+37.63 OFF:18.00RT
RIM:724.78

P-11

I-8
STA:14+42.50 OFF:38.69RT
RIM:724.60

P-18

I-13
STA:14+58.16 OFF:36.45RT
RIM:724.25

STMH-2
STA:15+01.45 OFF:-1.55LT
RIM:724.66

SLOPE INTERCEPTS

I-4
STA:14+92.50 OFF:30.88RT
RIM:724.10

ZWETTLER & HUTTER INC

BEGIN PROJECT
STA. 11+05
Y= 130,312.49
X= 564,508.46

WORCESTER STREET

HAHN PROPERTIES LLC

JEFFREY R OLSON

DAVIN PICKELL

PAMELA MEIER-HAMACHEK

ALBANY STREET

PAMELA E CHEHADE

SUSAN M MC CLINTOCK

VILLAGE OF SPRING GREEN (TYP.)

MATCHLINE STA. 16+00

EXISTING ℄ PROFILE EAST JEFFERSON STREET

FINSHED ℄ PROFILE EAST JEFFERSON STREET

EX P-6

EXISTING 18" STORM SEWER PIPE TO REMAIN

P-5 79' of 30" RCP @ 0.31%

STMH-5
EX P-5: 721.27
EX P-6: 721.27
EX P-4: 721.27
P-5: 720.58

STMH-4
P-5: 720.34
P-14: 720.88
P-4: 720.24
P-16: 720.88

P-4 232' of 30" RCP @ 0.31%

STMH-3
P-4: 719.52
P-10: 719.62
P-3: 719.42
P-12: 719.62

STMH-2
P-3: 719.04
P-9: 719.14
P-8: 719.14
P-2: 718.94

P-3
64' of 30" RCP @ 0.60%

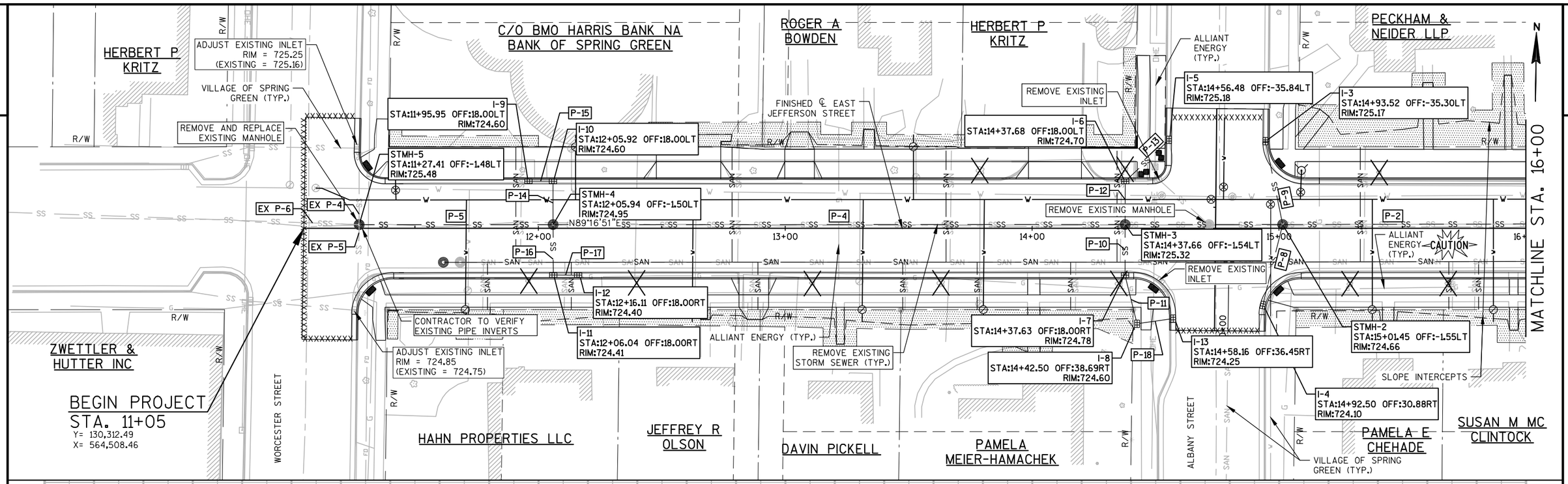
EXISTING ℄ ELEVATION
FINISHED ℄ ELEVATION

730 728 726 724 722 720 718 716 714

725.89 725.94 725.85 725.78 725.73 725.70 725.53 725.56 725.31 725.41 725.44 725.25 725.44 725.10 725.45 724.99 725.44 724.98 725.43 725.07 725.37 725.26 725.28 725.53 725.09 725.73 724.96 725.84 724.83 725.86 724.74 725.80 724.72 725.69 724.68 725.57 724.60 725.46 724.56 725.34 724.56 725.18 724.47 724.99 724.35 724.76 724.29 724.48 724.22 724.22 724.18 723.99 724.15 723.81 724.11 723.68

10+00 10+20 10+40 10+60 10+80 11+00 11+20 11+40 11+60 11+80 12+00 12+20 12+40 12+60 12+80 13+00 13+20 13+40 13+60 13+80 14+00 14+20 14+40 14+60 14+80 15+00 15+20 15+40 15+60 15+80 16+00

PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	STORM SEWER PLAN	SHEET -- 10



PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	STORM SEWER PLAN	SHEET -- 11

VICTORIA HEINZ

JOHN B & CATHERINE M WITTENWYLER

FRONTIER COMMUNICATIONS

VILLAGE OF SPRING GREEN (TYP.)

CHARTER COMMUNICATIONS

STMH-1
STA:17+36.12 OFF:-1.97LT
RIM:723.12

I-1
STA:17+36.12 OFF:18.00LT
RIM:722.77

STH 23

STA. 17+90, 18.6' LT.
ADJUST EXISTING INLET
RIM = 722.85
(EXISTING = 722.87)

STA. 17+90, 17.5' RT.
ADJUST EXISTING INLET
RIM = 722.85
(EXISTING = 722.75)

JUDINE M HUTTER

MATCHLINE STA. 16+00

FINISHED ℄ EAST JEFFERSON STREET

P-6

P-2

P-1

EX P-2

EX STMH-1
STA:17+85.95 OFF:-2.06LT
RIM:723.20

P-7

EX P-3

EX P-1

N89°16'51"E

17+00

STA. 17+86, 2.0' LT.
ADJUST EXISTING MANHOLE
RIM = 722.15
EXISTING = 722.22

I-2
STA:17+36.16 OFF:18.00RT
RIM:722.77

REMOVE EXISTING STORM SEWER (TYP.)

ALLIANT ENERGY (TYP.)

CAUTION

SLOPE INTERCEPTS

CONNECT TO EXISTING MANHOLE. INCIDENTAL TO REINFORCED CONCRETE STORM SEWER. CONTRACTOR MUST VERIFY EXISTING PIPE INVERTS.

STEIN W & NANCY G EDWARDS

END PROJECT
STA. 17+95
Y= 130,321.15
X= 565,198.44

KAREN M NELSON

NATHANAEL P B & MARINE L B DRESSER

BETTY J HILL

FINISHED ℄ PROFILE EAST JEFFERSON STREET

EXISTING ℄ PROFILE EAST JEFFERSON STREET

P-1
50' of 30" RCP @ 0.70%

EX P-1
26' of 30" RCP @ -0.64%

P-2
235' of 30" RCP @ 0.70%

STMH-1
P-2: 717.30
P-6: 718.75
P-1: 717.20
P-7: 718.75

EX STMH-1
P-1: 716.85
EX P-2: 720.00
EX P-1: 716.37
EX P-3: 720.00

CONNECT TO EXISTING MANHOLE

EXISTING 30" STORM SEWER TO REMAIN

EXISTING ℄ ELEVATION

FINISHED ℄ ELEVATION

I-1
P-6: 718.83

I-2
P-7: 718.85

P-6
17' of 12" RCP @ 0.50%

P-7
21' of 12" RCP @ -0.50%

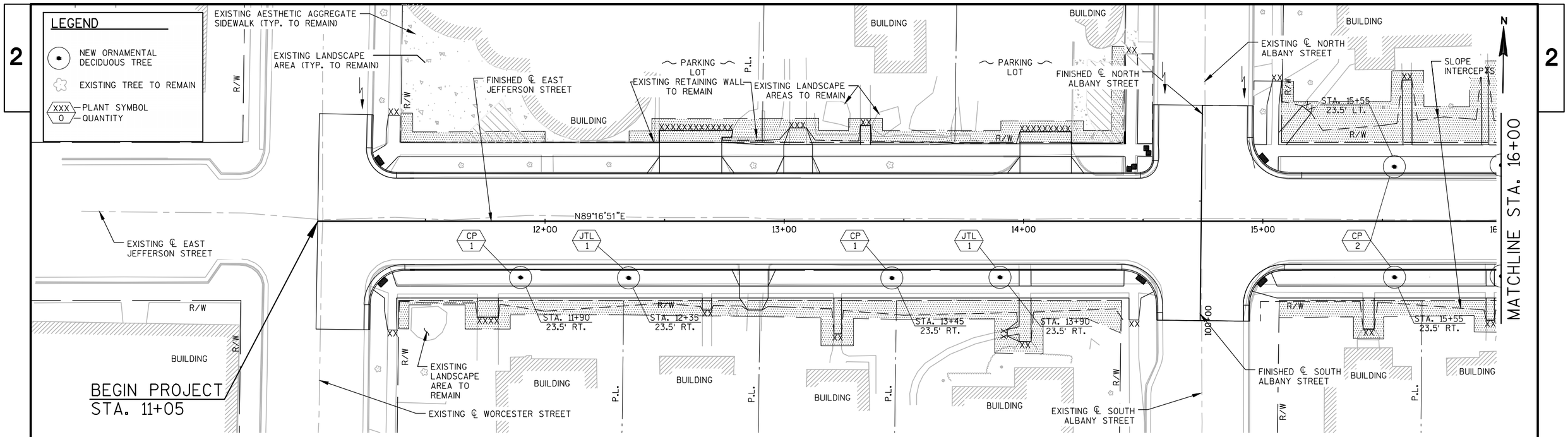
STMH-1
P-2: 717.30
P-6: 718.75
P-1: 717.20
P-7: 718.75

726
724
722
720
718
716
714
712
710

724.11 723.68 | 724.01 723.59 | 723.92 723.50 | 723.85 723.41 | 723.78 723.32 | 723.69 723.23 | 723.57 723.17 | 723.44 723.15 | 723.34 723.17 | 723.26 723.22 | 723.30 | 723.40 | 723.57

16+00 16+20 16+40 16+60 16+80 17+00 17+20 17+40 17+60 17+80 18+00 18+20 18+40 18+60 18+80 19+00 19+20 19+40 19+60 19+80 20+00 20+20 20+40 20+60 20+80 21+00 21+20 21+40 21+60 21+80 22+00

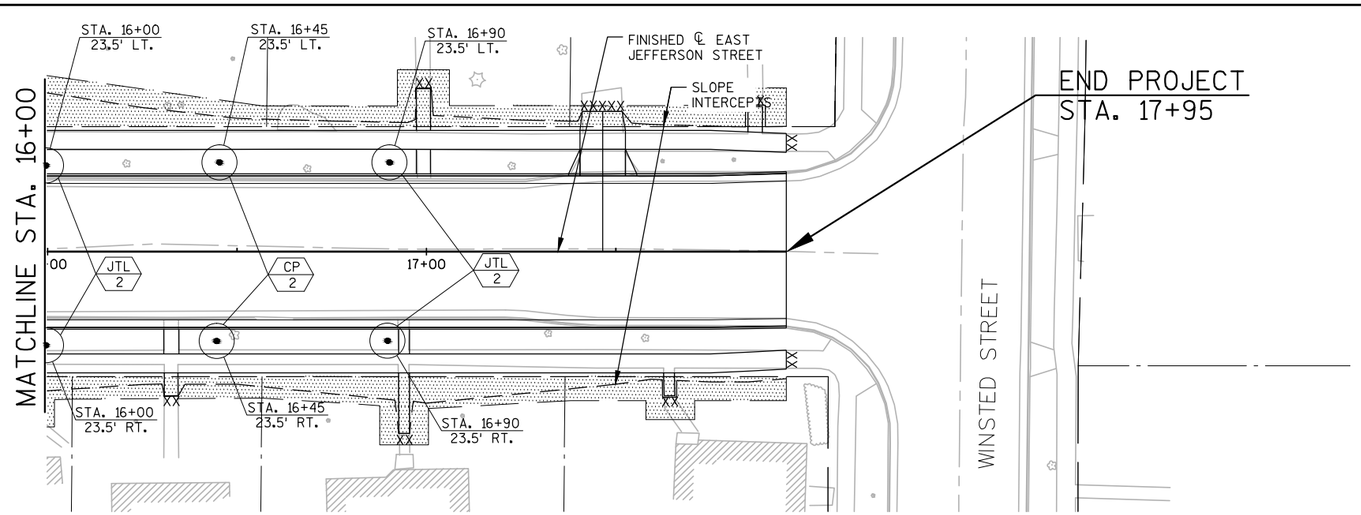
PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	STORM SEWER PLAN	SHEET 12



PLANTING DATA CHART

SYM.	COMMON NAME	SCIENTIFIC NAME	TYPE	AVE. MATURE HEIGHT	SIZE CAL. OR HEIGHT PLANTED	ROOT ZONE MODE	BALL/POT DIA. DEPTH	ROOT SPREAD	PLANT HOLE DIA. DEPTH.	BRACE OR GUY	FERT. UNITS REQ'D (1)	MULCH RING DIA.
	MEDIUM DECIDUOUS TREES											
JTL	LILAC, IVORY SILK JAPANESE TREE	SYRINGA RETICULATA 'IVORY SILK'	3T	15'-25'	2.5" cal	B&B	28 IN 19 IN	-	52 IN 19 IN	GUY	4	64 IN
CP	PEAR, CLEVELAND SELECT	PYRUS CALLERYANA 'CLEVELAND SELECT'	2T	25'-30'	2.5" cal	B&B	28 IN 19 IN	-	52 IN 19 IN	GUY	4	64 IN

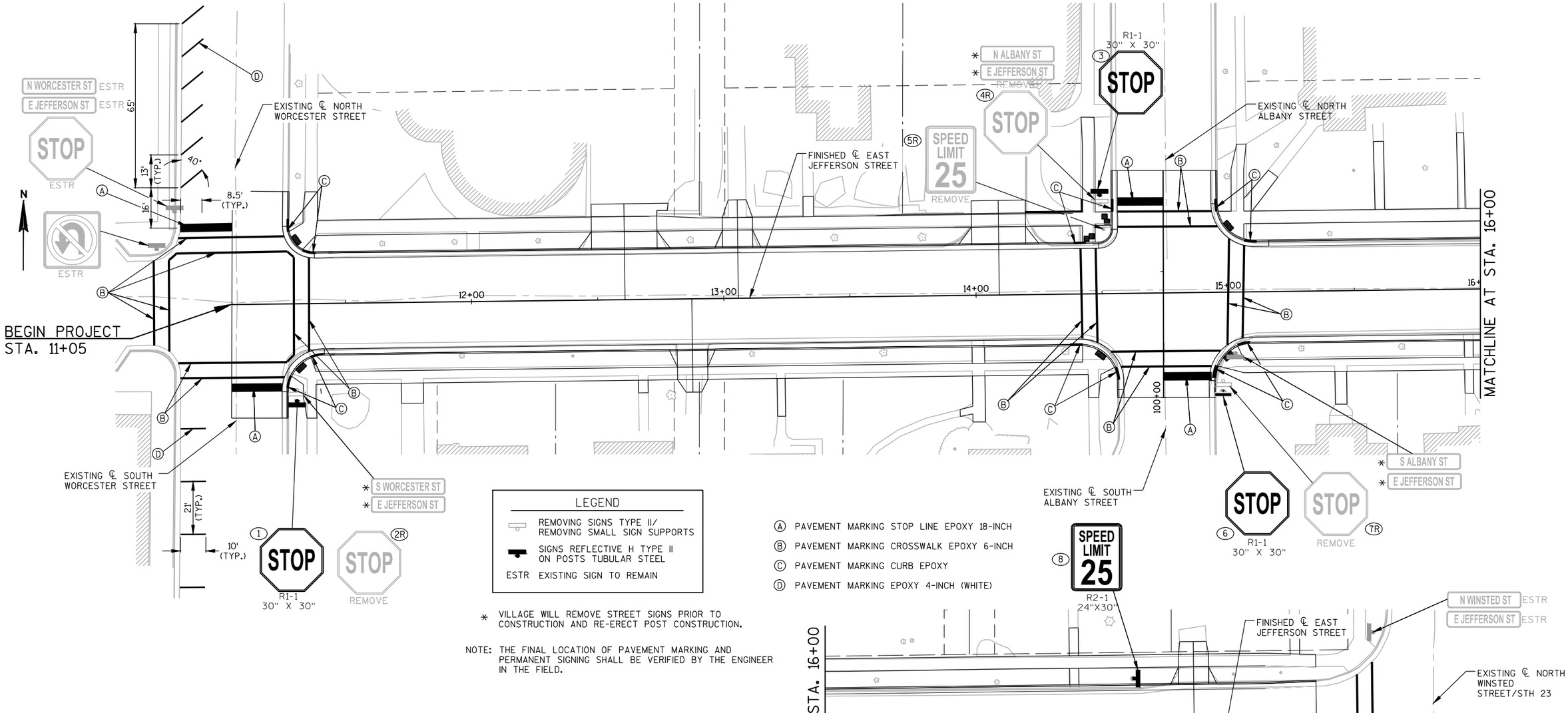
B&B = BALLED & BURLAPPED
(1) ONE FERTILIZER UNIT = 10 OZ. MICROPORE SLOW RELEASE POLYETHYLENE PACKET PER STANDARD SPECIFICATIONS.



NOTE: BOTH LILAC IVORY SILK JAPANESE TREE (JTL) AND PEAR, CLEVELAND SELECT (CP) SHALL BE PAID UNDER "MEDIUM DECIDUOUS TREES" BID ITEM.

PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	PLANTING PLAN	SHEET 13

2



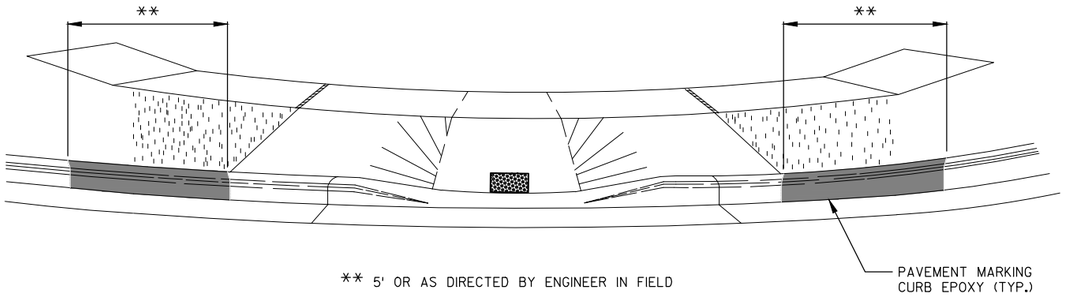
LEGEND

	REMOVING SIGNS TYPE II/ REMOVING SMALL SIGN SUPPORTS
	SIGNS REFLECTIVE H TYPE II ON POSTS TUBULAR STEEL
ESTR	EXISTING SIGN TO REMAIN

* VILLAGE WILL REMOVE STREET SIGNS PRIOR TO CONSTRUCTION AND RE-ERECT POST CONSTRUCTION.

NOTE: THE FINAL LOCATION OF PAVEMENT MARKING AND PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

Ⓐ PAVEMENT MARKING STOP LINE EPOXY 18-INCH
Ⓑ PAVEMENT MARKING CROSSWALK EPOXY 6-INCH
Ⓒ PAVEMENT MARKING CURB EPOXY
Ⓓ PAVEMENT MARKING EPOXY 4-INCH (WHITE)

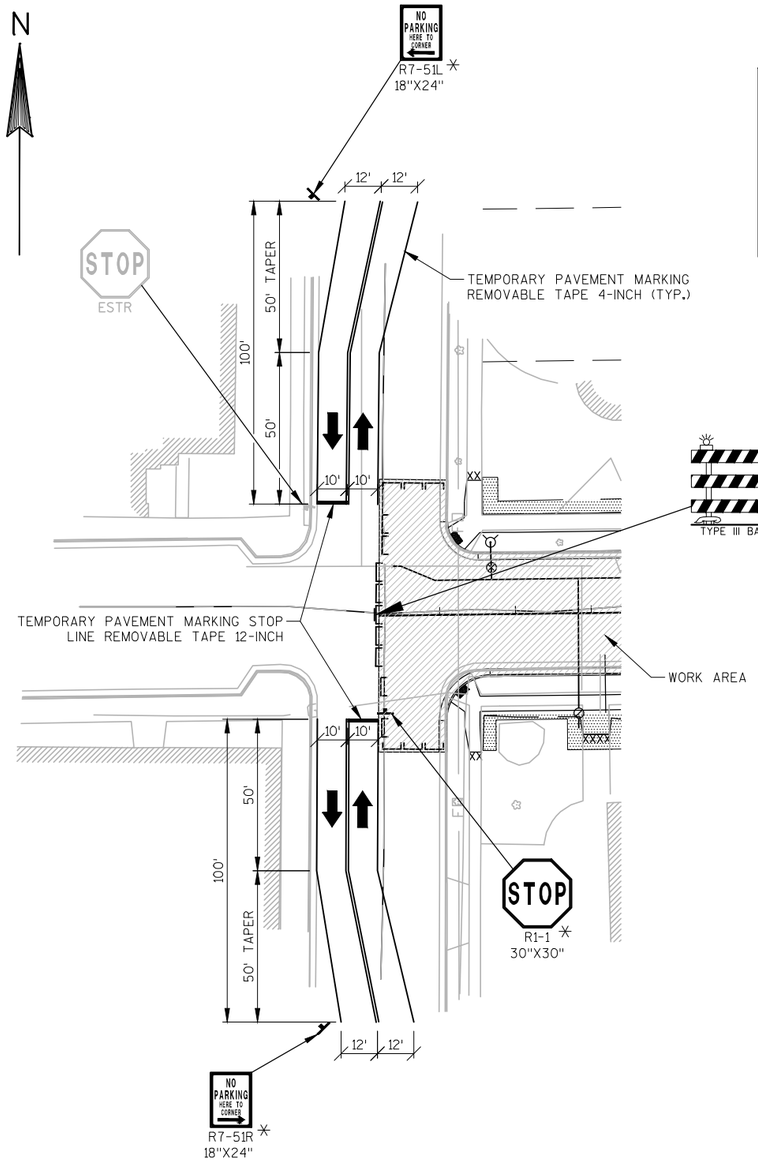


** 5' OR AS DIRECTED BY ENGINEER IN FIELD

PAVEMENT MARKING
CURB EPOXY DETAIL

PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	PAVEMENT MARKING AND PERMANENT SIGNING	SHEET --14	E

S:\Projects\S34083 Village of SG Jefferson Street\CADD Files\S34083 Jefferson CADD\S34083 Jefferson Roadway\Jefferson Street Pavement and Signing.dwg



LEGEND

- DIRECTION OF TRAFFIC
- WORK AREA
- TRAFFIC CONTROL SIGN
- TYPE III BARRICADE WITH/WITHOUT SIGN
- ESTR EXISTING SIGN TO REMAIN

GENERAL NOTES

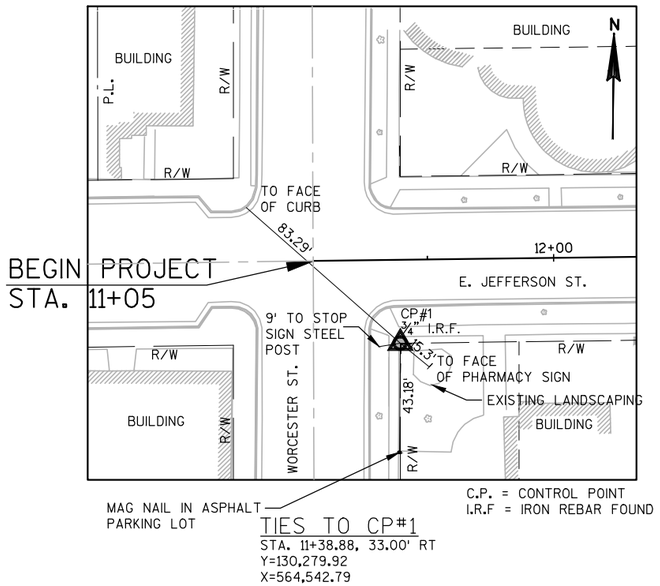
THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

PAYMENT OF TRAFFIC CONTROL BARRICADES TYPE III, TRAFFIC CONTROL WARNING LIGHTS TYPE A, AND TRAFFIC CONTROL SIGNS (ROAD CLOSED) ARE INCLUDED IN TRAFFIC CONTROL (PROJECT) BID ITEM.

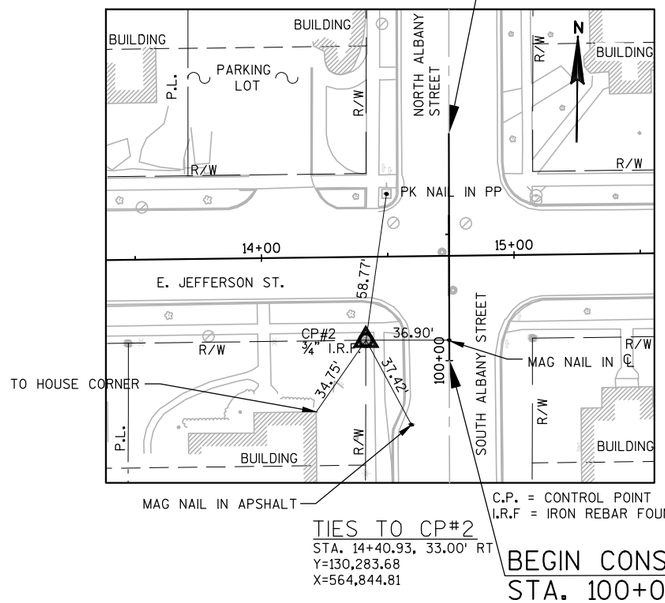
EAST JEFFERSON STREET & WORCESTER STREET INTERSECTION

* PAID FOR AS TRAFFIC CONTROL SIGN



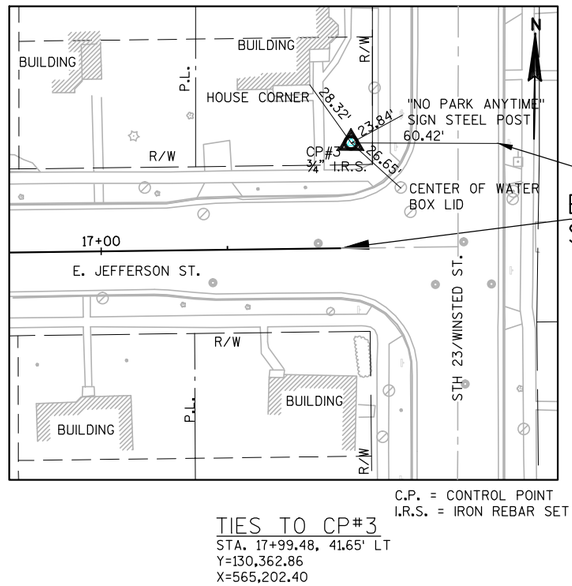
C.P. = CONTROL POINT
I.R.F = IRON REBAR FOUND

TIES TO CP#1
STA. 11+38.88, 33.00' RT
Y=130,279.92
X=564,542.79



C.P. = CONTROL POINT
I.R.F = IRON REBAR FOUND

TIES TO CP#2
STA. 14+40.93, 33.00' RT
Y=130,283.68
X=564,844.81

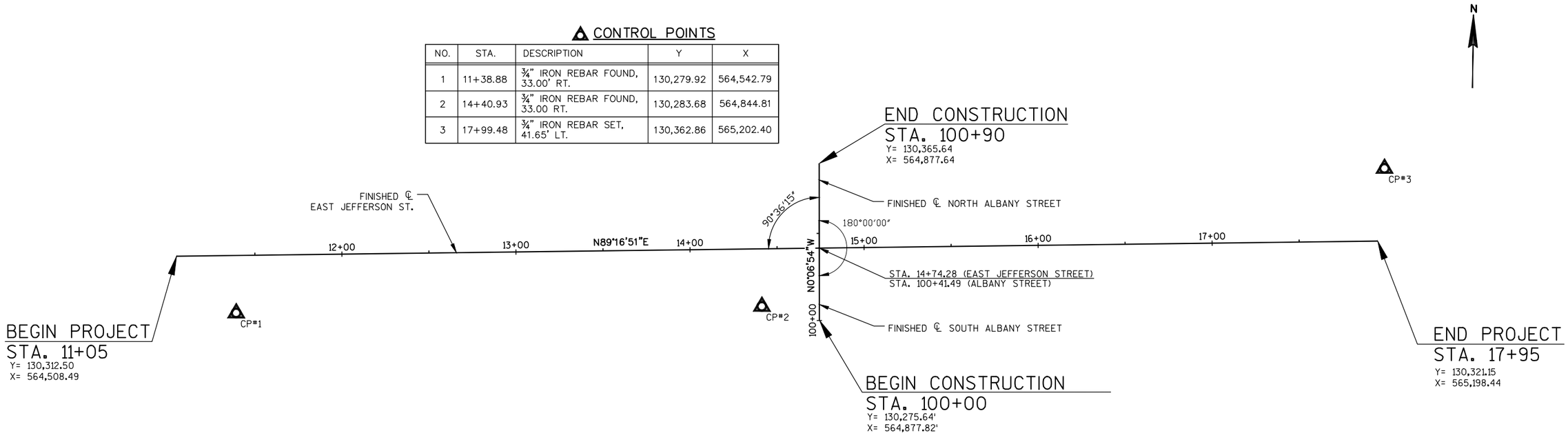


C.P. = CONTROL POINT
I.R.S. = IRON REBAR SET

TIES TO CP#3
STA. 17+99.48, 41.65' LT
Y=130,362.86
X=565,202.40

CONTROL POINTS

NO.	STA.	DESCRIPTION	Y	X
1	11+38.88	¾" IRON REBAR FOUND, 33.00' RT.	130,279.92	564,542.79
2	14+40.93	¾" IRON REBAR FOUND, 33.00 RT.	130,283.68	564,844.81
3	17+99.48	¾" IRON REBAR SET, 41.65' LT.	130,362.86	565,202.40



PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	TIES & ALIGNMENT PLAN	SHEET 16	E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\CADD FILES\S34083 JEFFERSON CADD\S34083 JEFFERSON ROADWAY\JEFFERSON ALIGNMENT AND TIE DIAGRAMS.DGN PLOTTING: 12/18/2013 3:27 PM PLOT BY : BALLWEG,THOMAS

2

JEFFERSON STREET STATION LAYOUT

Station	Y	X	Remarks
11+05	130,312.50'	564,508.49'	Begin Project
11+50	130,313.06'	564,553.49'	-
12+00	130,313.69'	564,603.48'	-
12+50	130,314.31'	564,653.48'	-
13+00	130,314.94'	564,703.48'	-
13+50	130,315.57'	564,753.47'	-
14+00	130,316.20'	564,803.47'	-
14+50	130,316.82'	564,853.46'	-
15+00	130,317.45'	564,903.46'	-
15+50	130,318.08'	564,953.46'	-
16+00	130,318.71'	565,003.45'	-
16+50	130,319.33'	565,053.45'	-
17+00	130,319.96'	565,103.45'	-
17+50	130,320.59'	565,153.44'	-
17+95	130,321.15'	565,198.44'	End Project

SOUTH/NORTH ALBANY STREET STATION LAYOUT

Station	Y	X	Remarks
100+00	130,275.64'	564,877.82'	Begin Construction
100+50	130,325.64'	564,877.72'	-
100+90	130,365.64'	564,877.64'	End Construction

DATE 28APR14 — E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	5916-00-71 QUANTITY
0020	201.0120	CLEARING	ID	330.000	330.000
0040	201.0220	GRUBBING	ID	330.000	330.000
0060	204.0100	REMOVING PAVEMENT	SY	135.000	135.000
0070	204.0150	REMOVING CURB & GUTTER	LF	1,420.000	1,420.000
0080	204.0155	REMOVING CONCRETE SIDEWALK	SY	760.000	760.000
0090	204.0210	REMOVING MANHOLES	EACH	2.000	2.000
0100	204.0220	REMOVING INLETS	EACH	2.000	2.000
0110	204.0245	REMOVING STORM SEWER (SIZE) 01. 12-INCH	LF	59.000	59.000
0120	204.0245	REMOVING STORM SEWER (SIZE) 02. 15-INCH	LF	344.000	344.000
0130	204.0245	REMOVING STORM SEWER (SIZE) 03. 18-INCH	LF	314.000	314.000
0150	205.0100	EXCAVATION COMMON	CY	2,750.000	2,750.000
0160	213.0100	FINISHING ROADWAY (PROJECT) 01. 5916-00-71	EACH	1.000	1.000
0180	305.0110	BASE AGGREGATE DENSE 3/4-INCH	TON	490.000	490.000
0190	305.0120	BASE AGGREGATE DENSE 1 1/4-INCH	TON	1,340.000	1,340.000
0200	311.0110	BREAKER RUN	TON	2,840.000	2,840.000
0210	416.0160	CONCRETE DRIVEWAY 6-INCH	SY	135.000	135.000
0230	455.0105	ASPHALTIC MATERIAL PG58-28	TON	25.000	25.000
0240	455.0120	ASPHALTIC MATERIAL PG64-28	TON	21.000	21.000
0250	455.0605	TACK COAT	GAL	85.000	85.000
0260	460.1100	HMA PAVEMENT TYPE E-0.3	TON	750.000	750.000
0280	460.2000	INCENTIVE DENSITY HMA PAVEMENT	DOL	480.000	480.000
0320	601.0411	CONCRETE CURB & GUTTER 30-INCH TYPE D	LF	1,380.000	1,380.000
0330	601.0600	CONCRETE CURB PEDESTRIAN	LF	28.000	28.000
0340	602.0405	CONCRETE SIDEWALK 4-INCH	SF	7,600.000	7,600.000
0350	602.0505	CURB RAMP DETECTABLE WARNING FIELD YELLOW	SF	56.000	56.000
0360	602.1500	CONCRETE STEPS	SF	15.000	15.000
0370	608.0312	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 12-INCH	LF	243.000	243.000
0380	608.0315	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 15-INCH	LF	17.000	17.000
0400	608.0330	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 30-INCH	LF	660.000	660.000
0430	611.0535	MANHOLE COVERS TYPE J-SPECIAL	EACH	5.000	5.000
0440	611.0639	INLET COVERS TYPE H-S	EACH	12.000	12.000
0450	611.0645	INLET COVERS TYPE MS-A	EACH	1.000	1.000
0470	611.2005	MANHOLES 5-FT DIAMETER	EACH	2.000	2.000
0480	611.2006	MANHOLES 6-FT DIAMETER	EACH	3.000	3.000
0500	611.3230	INLETS 2X3-FT	EACH	12.000	12.000
0510	611.3901	INLETS MEDIAN 1 GRATE	EACH	1.000	1.000
0520	611.8110	ADJUSTING MANHOLE COVERS	EACH	1.000	1.000
0530	611.8115	ADJUSTING INLET COVERS	EACH	4.000	4.000
0540	619.1000	MOBILIZATION	EACH	0.260	0.260
0550	624.0100	WATER	MGAL	34.000	34.000
0560	625.0100	TOPSOIL	SY	1,950.000	1,950.000
0570	627.0200	MULCHING	SY	1,950.000	1,950.000
0580	628.1504	SILT FENCE	LF	185.000	185.000
0590	628.1520	SILT FENCE MAINTENANCE	LF	370.000	370.000
0600	628.1905	MOBILIZATIONS EROSION CONTROL	EACH	4.000	4.000
0610	628.1910	MOBILIZATIONS EMERGENCY EROSION CONTROL	EACH	3.000	3.000
0620	628.7005	INLET PROTECTION TYPE A	EACH	13.000	13.000
0630	628.7015	INLET PROTECTION TYPE C	EACH	19.000	19.000
0650	629.0210	FERTILIZER TYPE B	CWT	2.000	2.000
0660	630.0140	SEEDING MIXTURE NO. 40	LB	35.000	35.000

3

DATE 28APR14 E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	5916-00-71 QUANTITY
0670	630.0200	SEEDING TEMPORARY	LB	20.000	20.000
0680	632.0101	TREES (SPECIES, ROOT, SIZE) 01. LILAC, IVORY, SILK JAPANESE TREE B&B 2.5-INCH CAL.	EACH	6.000	6.000
0690	632.0101	TREES (SPECIES, ROOT, SIZE) 02. PEAR, CLEVELAND SELECT B&B 2.5-INCH CAL.	EACH	6.000	6.000
0700	632.9101	LANDSCAPE PLANTING SURVEILLANCE AND CARE CYCLES	EACH	3.000	3.000
0720	634.0814	POSTS TUBULAR STEEL 2X2-INCH X 14-FT	EACH	5.000	5.000
0740	637.2210	SIGNS TYPE II REFLECTIVE H	SF	25.720	25.720
0750	638.2602	REMOVING SIGNS TYPE II	EACH	5.000	5.000
0760	638.3000	REMOVING SMALL SIGN SUPPORTS	EACH	4.000	4.000
0770	642.5001	FIELD OFFICE TYPE B	EACH	0.500	0.500
0780	643.0100	TRAFFIC CONTROL (PROJECT) 01. 5916-00-71	EACH	1.000	1.000
0800	643.0900	TRAFFIC CONTROL SIGNS	DAY	180.000	180.000
0810	646.0106	PAVEMENT MARKING EPOXY 4-INCH	LF	110.000	110.000
0820	646.0600	REMOVING PAVEMENT MARKINGS	LF	290.000	290.000
0850	647.0456	PAVEMENT MARKING CURB EPOXY	LF	60.000	60.000
0860	647.0566	PAVEMENT MARKING STOP LINE EPOXY 18-INCH	LF	100.000	100.000
0870	647.0766	PAVEMENT MARKING CROSSWALK EPOXY 6-INCH	LF	740.000	740.000
0880	649.0400	TEMPORARY PAVEMENT MARKING REMOVABLE TAPE 4-INCH	LF	800.000	800.000
0890	649.1000	TEMPORARY PAVEMENT MARKING STOP LINE REMOVABLE TAPE 12-INCH	LF	20.000	20.000
0900	650.4000	CONSTRUCTION STAKING STORM SEWER	EACH	18.000	18.000
0910	650.4500	CONSTRUCTION STAKING SUBGRADE	LF	745.000	745.000
0920	650.5000	CONSTRUCTION STAKING BASE	LF	745.000	745.000
0930	650.5500	CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER	LF	1,410.000	1,410.000
0950	650.9910	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) 01. 5916-00-71	LS	1.000	1.000
0970	650.9920	CONSTRUCTION STAKING SLOPE STAKES	LF	745.000	745.000
0980	690.0150	SAWING ASPHALT	LF	310.000	310.000
0990	690.0250	SAWING CONCRETE	LF	150.000	150.000

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CLEARING & GRUBBING

STATION	LOCATION	201.0120 CLEARING (ID)	201.0220 GRUBBING (ID)
11+96	MAINLINE, RT.	21	21
12+41	MAINLINE, RT.	4	4
13+11	MAINLINE, RT.	13	13
13+63	MAINLINE, RT.	28	28
13+79	MAINLINE, LT.	36	36
14+35	MAINLINE, RT.	24	24
14+37	MAINLINE, LT.	32	32
15+29	MAINLINE, RT.	21	21
15+40	MAINLINE, LT.	11	11
15+69	MAINLINE, RT.	26	26
16+20	MAINLINE, LT.	13	13
16+49	MAINLINE, RT.	28	28
16+68	MAINLINE, LT.	16	16
16+85	MAINLINE, RT.	21	21
17+23	MAINLINE, LT.	21	21
17+33	MAINLINE, RT.	15	15
	TOTALS =	330	330

REMOVING PAVEMENT

STATION	LOCATION	204.0100 (SY)
12+61	MAINLINE, LT.	41
12+87	MAINLINE, RT.	18
13+06	MAINLINE, LT.	20
14+09	MAINLINE, LT.	41
17+47	MAINLINE, LT.	15
	TOTAL =	135

REMOVING CURB & GUTTER

STATION - STATION	LOCATION	204.0150 REMOVING CURB & GUTTER (LF)
11+25 - 14+57	MAINLINE, LT.	375
11+25 - 14+57	MAINLINE, RT.	366
14+93 - 17+95	MAINLINE, RT.	318
14+94 - 17+95	MAINLINE, LT.	323
12+87	MAINLINE, RT.	9
14+09	MAINLINE, LT.	29
	TOTAL =	1,420

NOTE: MAINLINE REMOVALS INCLUDE WORCESTER STREET AND ALBANY STREET REMOVALS.

REMOVING CONCRETE SIDEWALK

STATION - STATION	LOCATION	204.0155 (SY)
11+27 - 14+55	MAINLINE, RT.	157
11+27 - 14+58	MAINLINE, LT.	186
14+96 - 17+95	MAINLINE, LT.	166
14+96 - 17+95	MAINLINE, RT.	138
11+76	MAINLINE, RT.	10
12+01	MAINLINE, LT.	4
12+68	MAINLINE, RT.	3
13+22	MAINLINE, RT.	6
13+34	MAINLINE, LT.	4
14+01	MAINLINE, RT.	14
15+07	MAINLINE, LT.	9
15+44	MAINLINE, RT.	8
15+61	MAINLINE, LT.	9
15+93	MAINLINE, LT.	10
15+95	MAINLINE, RT.	8
16+33	MAINLINE, RT.	7
16+94	MAINLINE, RT.	8
16+99	MAINLINE, RT.	8
17+64	MAINLINE, RT.	2
17+87	MAINLINE, LT.	3
	TOTAL =	760

NOTE: MAINLINE REMOVALS INCLUDE WORCESTER STREET AND ALBANY STREET REMOVALS.

ALL BID ITEMS ARE CATEGORY 010 UNLESS OTHERWISE NOTED

REMOVING MANHOLES

STATION	LOCATION	204.0210 (EACH)
11+27	1.5' LT	1
14+72	1.5' LT	1
	TOTAL	2

REMOVING INLETS

STATION	LOCATION	204.0220 (EACH)
14+51	20.0' LT	1
14+51	21.7' RT	1
	TOTAL	2

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REMOVING STORM SEWER

FROM STATION	FROM LOCATION	TO STATION	TO LOCATION	REMOVING STORM SEWER 204.0245 12-INCH (L.F.)	204.0245 15-INCH (L.F.)	204.0245 18-INCH (L.F.)
11+27	1.5' LT	14+72	1.5' LT	-	344	-
14+51	20.0' LT	14+72	1.5' LT	28	-	-
14+51	21.7' RT	14+72	1.5' LT	31	-	-
14+72	1.5' LT	17+86	2.1' LT	-	-	314
			TOTAL	59	344	314

EARTHWORK SUMMARY

CATEGORY	FROM/TO STA	LOCATION	(1) 205.0100 COMMON EXCAVATION CUT (2) (CY)	205.0100 COMMON EXCAVATION EBS (3) (CY)	SALVAGED/ UNUSABLE PAVEMENT MATERIAL (CY) (4)	AVAILABLE MATERIAL (CY) (5)	205.0400 MARSH EXCAVATION (CY) (6)	205.0200 ROCK EXCAVATION (CY) (7)	REDUCED MARSH IN FILL (CY) FACTOR 0.6 (8)	REDUCED EBS IN FILL (CY) FACTOR 0.8 (9)	EXPANDED MARSH BACKFILL (CY) FACTOR 1.5 (10)	EXPANDED EBS BACKFILL (CY) FACTOR 1.5 (11)	EXPANDED ROCK (CY) FACTOR 1.1 (12)	UNEXPANDED FILL (CY)	EXPANDED FILL (CY) FACTOR 1.25 (13)	MASS ORDINATE +/- (CY) (14)	WASTE (CY)	208.0100 BORROW (CY)	COMMENT:
010	11+05 - 17+95	MAINLINE	2504	-	-	2504	-	-	-	-	-	-	-	72	90	2414	2414	-	
	100+00 - 100+23.50	SOUTH ALBANY STREET	82	-	-	82	-	-	-	-	-	-	-	0	0	82	82	-	
	100+59.50 - 100+90	NORTH ALBANY STREET	104	-	-	104	-	-	-	-	-	-	-	0	0	104	104	-	
	-	P.E., F.E., C.E.	60			60								0	0	60	60		
	TOTALS =		2750			2750								72	90	2660	2660		

BASE AGGREGATE DENSE

STATION - STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH (TON)	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH (TON)	311.0110 BREAKER RUN (TON)
11+05 - 17+95	MAINLINE	-	1,164	2,484
11+28 - 14+54	MAINLINE, LT.	87	-	-
11+28 - 14+54	MAINLINE, RT.	93	-	-
14+96 - 17+95	MAINLINE, LT.	81	-	-
14+96 - 17+95	MAINLINE, RT.	83	-	-
100+00 - 100+09	SOUTH ALBANY STREET, LT.	3	-	-
100+00 - 100+23.50	SOUTH ALBANY STREET	-	47	93
100+59.50 - 100+90	NORTH ALBANY STREET	-	53	106
100+74 - 101+00	NORTH ALBANY STREET, RT.	8	-	-
100+74 - 101+11	NORTH ALBANY STREET, LT.	11	-	-
-	P.E., C.E.	61	-	-
-	PRIVATE SIDEWALKS	37	-	-
-	UNDISTRIBUTED	26	76	157
	TOTALS =	490	1,340	2,840

CONCRETE DRIVEWAY 6-INCH

STATION	LOCATION	416.0160 CONCRETE DRIVEWAY 6-INCH (SY)
12+61	C.E. - MAINLINE, LT.	41
12+87	P.E. - MAINLINE, RT.	18
13+06	P.E. - MAINLINE, LT.	25
14+09	C.E. - MAINLINE, LT.	33
17+47	P.E - MAINLINE, LT.	18
	TOTAL =	135

HMA PAVEMENT

STATION - STATION	LOCATION	455.0105 ASPHALTIC MATERIAL PG 58-28 (TON)	455.0120 ASPHALTIC MATERIAL PG 64-28 (TON)	455.0605 TACK COAT (GAL)	460.1100 HMA PAVEMENT TYPE E-0.3 (TON)
11+05 - 17+95	MAINLINE	22	18	72	665
100+00 - 100+23.50	SOUTH ALBANY STREET	1	1	3	24
100+59.50 - 100+90	NORTH ALBANY STREET	1	1	3	31
-	P.E., C.E.	-	0.5	-	6
-	UNDISTRIBUTED	1	0.5	7	24
	TOTALS =	25	21	85	750

PROJECT NO: 5916-00-71 | HWY: EAST JEFFERSON STREET | COUNTY: SAUK | MISCELLANEOUS QUANTITIES | SHEET E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\CADD FILES\S34083 JEFFERSON CADD\S34083 JEFFERSON ROADWAY\MQ.DWG PLOT DATE: 4/18/2014 3:41 PM PLOT BY: STRINE, THERESA WISDOT/CADDS SHEET 42

ALL BID ITEMS ARE CATEGORY 010 UNLESS OTHERWISE NOTED

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CONCRETE CURB & GUTTER 30-INCH TYPE D

STATION - STATION	LOCATION	601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D (LF)	601.0600 CONCRETE CURB PEDESTRIAN (LF)	650.5500 CONSTRUCTION STAKING CURB & GUTTER AND CURB & GUTTER (LF)
11+25 - 14+56	MAINLINE, LT.	376	-	376
11+25 - 14+58	MAINLINE, RT.	363	-	363
14+20 - 14+43	MAINLINE, LT.	-	28	30
14+94 - 17+95	MAINLINE, LT.	323	-	323
14+92 - 17+95	MAINLINE, RT.	318	-	318
	TOTALS =	1,380	28	1,410

CONCRETE SIDEWALK 4-INCH

STATION - STATION	LOCATION	602.0405 (SF)
11+28 - 14+54	MAINLINE, LT.	1,617
11+28 + 14+54	MAINLINE, RT.	1,683
14+96 - 17+95	MAINLINE, LT.	1,474
14+96 - 17+95	MAINLINE, RT.	1,519
100+00 - 100+09	SOUTH ALBANY STREET, LT.	42
100+74 - 101+00	NORTH ALBANY STREET, RT.	127
100+74 - 101+11	NORTH ALBANY STREET, LT.	189
11+76	MAINLINE, RT.	71
12+68	MAINLINE, RT.	20
13+22	MAINLINE, RT.	48
13+34	MAINLINE, LT.	19
14+01	MAINLINE, RT.	139
15+07	MAINLINE, LT.	90
15+44	MAINLINE, RT.	60
15+61	MAINLINE, LT.	131
15+93	MAINLINE, LT.	86
15+95	MAINLINE, RT.	55
16+33	MAINLINE, RT.	48
16+94	MAINLINE, RT.	63
16+99	MAINLINE, LT.	65
17+64	MAINLINE, RT.	28
17+87	MAINLINE, LT.	26
	TOTAL =	7,600

CURB RAMP DETECTABLE WARNING FIELD YELLOW

STATION	LOCATION	602.0505 (SF)	REMARKS
11+30	MAINLINE, LT.	8	TYPE 1
11+31	MAINLINE, RT.	8	TYPE 1
14+45	MAINLINE, LT.	8	TYPE 2
14+50	MAINLINE, RT.	8	TYPE 1
15+00	MAINLINE, LT.	8	TYPE 1
14+99	MAINLINE, RT.	8	TYPE 1
100+71	NORTH ALBANY STREET, LT.	8	TYPE 2
	TOTAL =	56	

3

CONCRETE STEPS

STATION	602.1500 (S.F.)
13+34, LT.	15
TOTALS =	15

ADJUSTING MANHOLE COVER

STATION	LOCATION	611.8110 (EACH)
17+85.95	2.06' LT	1
	TOTAL	1

ADJUSTING INLET COVERS

STATION	LOCATION	611.8115 (EACH)
11+27	32.5' LT	1
11+26	33.3' RT	1
17+90	18.6' LT	1
17+90	17.5' RT	1
	TOTAL	4

WATER

LOCATION	624.0100 (MGAL)
PROJECT	34
TOTAL =	34

SILT FENCE

STATION - STATION	LOCATION	628.1504 SILT FENCE (LF)	628.1520 SILT FENCE MAINTENANCE (LF)
12+69 - 14+37	MAINLINE, RT.	150	300
-	UNDISTRIBUTED	35	70
	TOTALS =	185	370

PERMANENT SIGNING

SIGN	APPROX.	LOCATION	POSITION	SIGN	SIGN DESCRIPTION	ORDER LINES	SIZE IN X	637.2210 SIGNS TYPE II REFLECTIVE H SF	634.0814 POSTS TUBULAR STEEL 2X2-INCH X 14-FT 14-FT	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH
1	11+30	Mainline	Right	R1-1	Stop		30X30	5.18	1	---	---
2R	11+30	Mainline	Right	R1-1	Stop		30X30	---	---	1	1
3	100+75	North Albany Street	Left	R1-1	Stop		30X30	5.18	1	---	---
4R	100+75	North Albany Street	Left	R1-1	Stop		30X30	---	---	1	1
5R	100+67	North Albany Street	Left	R2-1	Speed Limit _MPH	25	24X30	---	---	1	---
6	100+05	North Albany Street	Right	R1-1	Stop		30X30	5.18	1	---	---
7R	100+05	North Albany Street	Right	R1-1	Stop		30X30	---	---	1	1
8	17+25	Mainline	Left	R2-1	Speed Limit _MPH	25	24X30	5.00	1	---	---
9R	17+97	Mainline	Right	R1-1	Stop		30X30	---	---	1	1
10	17+97	Mainline	Right	R1-1	Stop		30X30	5.18	1	---	---
						SHEET TOTALS		25.72	5	5	4

FINISHING ITEMS

STATION - STATION	LOCATION	625.0100 TOPSOIL (SY)	627.0200 MULCHING (SY)	629.0210 FERTILIZER TYPE B (CWT)	630.0140 SEEDING MIXTURE NO. 40 (LB)	630.0200 SEEDING TEMPORARY (LB)
11+05 - 17+95	MAINLINE	1,494	1,494	1	27	14
100+00 - 101+11	ALBANY STREET	65	65	0.5	1	1
-	UNDISTRIBUTED	391	391	0.5	7	5
	TOTALS =	1,950	1,950	2.0	35	20

PAVEMENT MARKING

STATION - STATION	LOCATION	DESCRIPTION	PAVEMENT MARKING 646.0106 EPOXY 4-INCH (LF)	647.0456 CURB EPOXY (LF)	647.0566 STOP LINE EPOX 18-INCH (LF)	647.0766 CROSSWALK EPOXY 6-INCH (LF)
10+77	MAINLINE	-	-	-	-	71
-	S WORCESTER ST	-	-	-	-	85
-	S WORCESTER ST	-	-	-	20	-
-	N WORCESTER ST	-	-	-	-	92
-	N WORCESTER ST	-	-	-	21	-
11+29	MAINLINE, LT.	NE QUARANT	-	10	-	-
11+29	MAINLINE, RT.	SE QUADRANT	-	10	-	-
11+33	MAINLINE	-	-	-	-	81
14+45	MAINLINE	-	-	-	-	73
15+00	MAINLINE	-	-	-	-	80
100+08	S ALBANY ST	-	-	-	19	-
100+15	S ALBANY ST	-	-	-	-	78
100+71	N ALBANY ST	-	-	-	-	77
100+77	N ALBANY ST	-	-	-	19	-
14+51	MAINLINE, RT.	SW QUADRANT	-	10	-	-
14+54	MAINLINE, LT.	NW QUADRANT	-	10	-	-
14+98	MAINLINE, LT.	NE QUARANT	-	10	-	-
14+98	MAINLINE, RT.	SE QUADRANT	-	10	-	-
18+07	MAINLINE	-	-	-	21	-
18+14	MAINLINE	-	-	-	-	103
-	S WORCESTER ST	ST PARKING - PERP	74	-	-	-
-	N WORCESTER ST	ST PARKING - DIAG	36	-	-	0
		TOTALS =	110	60	100	740

TRAFFIC CONTROL

LOCATION	DESCRIPTION	643.0900 TRAFFIC CONTROL SIGNS (DAYS)	646.0600 REMOVING PAVEMENT MARKINGS (LF)	649.0400 TEMPORARY PAVEMENT MARKING REMOVABLE TAPE 4-INCH (LF)	649.1000 TEMPORARY PAVEMENT MARKING STOP LINE REMOVABLE TAPE 12-INCH (LF)
S WORCESTER STREET	R7-51R	60	-	-	-
S WORCESTER STREET	R1-1	60	-	-	-
N WORCESTER STREET	R7-51L	60	-	-	-
S WORCESTER STREET	STREET PARKING - PERPENDICULAR	-	34	-	-
S WORCESTER STREET	CROSSWALK	-	92	-	-
N WORCESTER STREET	CROSSWALK	-	92	-	-
N WORCESTER STREET	STREET PARKING - DIAGONAL	-	72	-	-
S WORCESTER STREET	-	-	-	-	10
N WORCESTER STREET	-	-	-	-	10
S WORCESTER STREET	WHITE EDGELINE - TAPER	-	-	200	-
S WORCESTER STREET	DOUBLE YELLOW CENTERLINE - TAPER	-	-	200	-
N WORCESTER STREET	WHITE EDGELINE - TAPER	-	-	200	-
N WORCESTER STREET	DOUBLE YELLOW CENTERLINE - TAPER	-	-	200	-
	TOTALS =	180	290	800	20

STORM SEWER STRUCTURES

STRUCTURE NUMBER	STATION	LOCATION	RIM ELEVATION (FT)	611.0535 MANHOLE COVERS TYPE J-S (EACH)	611.0639 INLET COVER TYPE H-S (EACH)	611.0645 INLET COVER TYPE MS-A (EACH)	611.2005 MANHOLES 5-FT DIAM. (EACH)	611.2006 MANHOLES 6-FT DIAM. (EACH)	611.3230 INLETS 2X3-FT (EACH)	611.3901 INLETS MEDIAN 1 GRATE (EACH)	STRUCTURE DEPTH (FT)	650.4000 CONSTRUCTION STAKING STORM SEWER (EACH)	628.7005 INLET PROTECTION TYPE A (EACH)	628.7015 INLET PROTECTION TYPE C (EACH)
-	17+90	LT.	-	-	-	-	-	-	-	-	-	-	-	1
-	17+90	RT.	-	-	-	-	-	-	-	-	-	-	-	1
EX STMH-1	17+85.95	2.06' LT	723.20	-	-	-	-	-	-	-	-	-	-	-
I-1	17+36.12	18.00' LT	722.77	-	1	-	-	-	1	-	2.94	1	1	1
I-2	17+36.16	18.00' RT	722.77	-	1	-	-	-	1	-	2.92	1	1	1
I-3	14+94.52	35.30' LT	725.17	-	1	-	-	-	1	-	4.86	1	1	1
I-4	14+93.49	30.88' RT	724.10	-	1	-	-	-	1	-	3.80	1	1	1
I-5	14+55.46	35.84' LT	725.18	-	1	-	-	-	1	-	4.13	1	1	1
I-6	14+37.68	18.00' LT	724.70	-	1	-	-	-	1	-	3.92	1	1	1
I-7	14+37.63	18.00'RT	724.78	-	1	-	-	-	1	-	3.98	1	1	1
I-8	14+43.12	38.69' RT	724.60	-	-	1	-	-	-	1	4.55	1	1	1
I-9	11+95.95	18.00' LT	724.60	-	1	-	-	-	1	-	2.38	1	1	1
I-10	12+05.92	18.00' LT	724.60	-	1	-	-	-	1	-	2.63	1	1	1
I-11	12+06.04	18.00' RT	724.41	-	1	-	-	-	1	-	2.44	1	1	1
I-12	12+16.11	18.00' RT	724.40	-	1	-	-	-	1	-	2.28	1	1	1
I-13	14+57.15	36.45' RT	724.25	-	1	-	-	-	1	-	2.88	1	1	1
STMH-1	17+36.12	1.97' LT	723.12	1	-	-	-	1	-	-	4.67	1	-	-
STMH-2	15+01.45	1.55' LT	724.66	1	-	-	-	1	-	-	4.47	1	-	-
STMH-3	14+37.66	1.54' LT	725.32	1	-	-	-	1	-	-	4.65	1	-	-
STMH-4	12+05.94	1.50' LT	724.95	1	-	-	1	-	-	-	3.46	1	-	-
STMH-5	11+27.41	1.48' LT	725.48	1	-	-	1	-	-	-	3.65	1	-	-
-	10+85	LT.	-	-	-	-	-	-	-	-	-	-	-	1
-	10+85	RT.	-	-	-	-	-	-	-	-	-	-	-	1
-	11+27	LT.	-	-	-	-	-	-	-	-	-	-	-	1
-	11+27	RT.	-	-	-	-	-	-	-	-	-	-	-	1
PROJECT TOTALS				5	12	1	2	3	12	1		18	13	19

NOTES:
STATION AND OFFSET OF MANHOLE STRUCTURES ARE MEASURED FROM CENTER OF STRUCTURE.
STATION AND OFFSET OF INLET STRUCTURES ARE MEASURED TO FLANGE OF GUTTER.
ALL RIM ELEVATIONS ARE MEASURED TO THE FLANGE OF THE INLET.
STRUCTURE DEPTH (INLET) = RIM ELEVATION - INVERT LOWEST PIPE- 6 INCHES (RINGS) - 6 INCHES (CASTING HEIGHT).
STRUCTURE DEPTH (MANHOLE) = RIM ELEVATION - INVERT LOWEST PIPE- 6 INCHES (RINGS) - 9 INCHES (CASTING HEIGHT).

ALL BID ITEMS ARE CATEGORY 010 UNLESS OTHERWISE NOTED

SAWING ASPHALT/SAWING CONCRETE

STATION	LOCATION	690.0150 SAWING ASPHALT (LF)	690.0250 SAWING CONCRETE (LF)
11+05 - 11+25	MAINLINE	130	5
11+36	MAINLINE, RT.	-	4
11+37	MAINLINE, LT.	-	5
11+76	MAINLINE, RT.	-	9
12+61	MAINLINE, LT.	36	-
12+68	MAINLINE, RT.	-	4
13+06	MAINLINE, LT.	-	8
13+22	MAINLINE, RT.	-	3
13+34	MAINLINE, LT.	-	4
14+01	MAINLINE, RT.	-	8
14+09	MAINLINE, LT.	23	-
14+20 - 14+42	MAINLINE, LT.	-	22
15+07	MAINLINE, LT.	-	5
15+44	MAINLINE, RT.	-	5
15+61	MAINLINE, LT.	-	4
15+93	MAINLINE, LT.	-	4
15+95	MAINLINE, RT.	-	4
16+33	MAINLINE, RT.	-	4
16+94	MAINLINE, RT.	-	3
16+99	MAINLINE, LT.	-	4
17+47	MAINLINE, LT.	11	-
17+64	MAINLINE, RT.	-	3
17+87	MAINLINE, LT.	-	4
17+95	MAINLINE	38	15
100+00	SOUTH ALBANY STREET	35	9
100+90	NORTH ALBANY STREET	37	5
101+00	NORTH ALBANY STREET, RT.	-	4
101+11	NORTH ALBANY STREET, LT.	-	5
	TOTALS =	310	150

STORM SEWER PIPE

PIPE NUMBER	FROM STRUCTURE	TO STRUCTURE	INLET ELEVATION (FT)	DISCHARGE ELEVATION (FT)	% SLOPE	REINFORCED CONCRETE PIPE CLASS III STORM SEWER 608.0312 12-INCH L.F.	608.0315 15-INCH L.F.	608.0330 30-INCH L.F.
EX P-1	EX STMH-1	-	716.37	-	0.64	-	-	-
EX P-2	-	EX STMH-1	-	720.00	-	-	-	-
EX P-3	-	EX STMH-1	-	720.00	-	-	-	-
EX P-4	-	STMH-5	-	721.27	0.40	-	-	-
EX P-5	-	STMH-5	-	721.27	0.40	-	-	-
EX P-6	-	STMH-5	-	721.27	0.40	-	-	-
P-1	STMH-1	EX STMH-1	717.20	716.85	0.70	-	-	50
P-2	STMH-2	STMH-1	718.94	717.30	0.70	-	-	235
P-3	STMH-3	STMH-2	719.42	719.04	0.60	-	-	64
P-4	STMH-4	STMH-3	720.24	719.52	0.31	-	-	232
P-5	STMH-5	STMH-4	720.58	720.34	0.31	-	-	79
P-6	I-1	STMH-1	718.83	718.75	0.50	17	-	-
P-7	I-2	STMH-1	718.85	718.75	0.50	21	-	-
P-8	I-4	STMH-2	719.30	719.14	0.50	33	-	-
P-9	I-3	STMH-2	719.31	719.14	0.50	34	-	-
P-10	I-7	STMH-3	719.80	719.62	0.90	21	-	-
P-11	I-8	I-7	720.05	719.90	0.75	21	-	-
P-12	I-6	STMH-3	719.78	719.62	0.90	-	17	-
P-13	I-5	I-6	720.05	719.88	0.71	24	-	-
P-14	I-10	STMH-4	720.97	720.88	0.50	18	-	-
P-15	I-9	I-10	721.22	721.17	0.50	10	-	-
P-16	I-11	STMH-4	720.97	720.88	0.44	20	-	-
P-17	I-12	I-11	721.12	721.07	0.50	10	-	-
P-18	I-13	I-8	720.37	720.26	0.75	14	-	-
PROJECT TOTALS						243	17	660

NOTES:
PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURES.

TREES (SPECIES, ROOT, SIZE)

STATION	LOCATION	632.0101 TREES (SPECIES, ROOT, SIZE) 01. LILAC, IVORY, SILK JAPENESE TREE, B & B, 2.5-INCH CAL. (EACH)	02. PEAR, CLEVELAND SELECT, B & B, 2.5-INCH CAL. (EACH)
11+90	MAINLINE, RT.	1	-
12+35	MAINLINE, RT.	-	1
13+45	MAINLINE, RT.	1	-
13+90	MAINLINE, RT.	-	1
15+55	MAINLINE, LT.	1	-
15+55	MAINLINE, RT.	1	-
16+00	MAINLINE, LT.	-	1
16+00	MAINLINE, RT.	-	1
16+45	MAINLINE, LT.	1	-
16+45	MAINLINE, RT.	1	-
16+90	MAINLINE, LT.	-	1
16+90	MAINLINE, RT.	-	1
	TOTALS =	6	6

MOBILIZATION EROSION CONTROL

PROJECT	628.1905 MOBILIZATIONS EROSION CONTROL (EACH)	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL (EACH)
5916-00-01	4	3
TOTALS =	4	3

CONSTRUCTION STAKING

STATION - STATION	LOCATION	CONSTRUCTION STAKING 650.4500 SUBGRADE (LF)	650.5000 BASE (LF)	650.9910 SUPPLEMENTAL CONTROL 01.5916-00-71 (LS)	650.9920 SLOPE STAKES (LF)
11+05 - 17+95	MAINLINE	690	690	-	690
100+00 - 100+23.50	S ALBANY STREET	24	24	-	24
100+59.50 - 100+90	N ALBANY STREET	31	31	-	31
-	PROJECT	-	-	1	-
	TOTALS =	745	745	1	745

CONVENTIONAL ABBREVIATIONS

ACCESS POINT/ DRIVEWAY CONNECTION	AP
ACCESS RIGHTS	AR
ACRES	AC.
AND OTHERS	ET.AL.
BARN	B.
CENTERLINE	C/L
CERTIFIED SURVEY MAP	CSM
CORNER	COR.
CONVEYANCE OF RIGHTS	CR
DOCUMENT	DOC.
EASEMENT	EASE.
GARAGE	G.
HIGHWAY EASEMENT	H.E.
HOUSE	H.
HOUSE TRAILER	H.T.
LAND CONTRACT	LC
MONUMENT	MON.
PAGE	P.
PERMANENT LIMITED EASEMENT	PLE
PROPERTY LINE	PL
RECORDED AS	(100')
REFERENCE LINE	R/L
RELEASE OF RIGHTS	ROR
REMAINING	REM.
RIGHT-OF-WAY	R/W
SECTION	SEC.
SHED	S.
STATION	STA.
TEMPORARY LIMITED EASEMENT	TLE
VOLUME	V.

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE OR DELTA	DELTA
LENGTH OF CURVE	L
TANGENT	TAN

CONVENTIONAL SYMBOLS

FOUND SURVEY MONUMENT (WITH POINT NUMBER) o 1040
R/W MONUMENT o ● (SET)
R/W STANDARD △ ▲ (SET)
SIGN ISIGN
SECTION CORNER MONUMENT
SECTION CORNER SYMBOL
FEE (HATCH VARIES)
TEMPORARY LIMITED EASEMENT
PERMANENT LIMITED EASEMENT
R/W BOUNDARY POINT RWB20
PARCEL NUMBER (8)
UTILITY PARCEL NUMBER (92)
SIGN NUMBER (OFF PREMISE) 21-
BUILDING

PROPOSED R/W LINE
EXISTING H.E. LINE
PROPERTY LINE
LOT & TIE LINES
SLOPE INTERCEPTS
CORPORATE LIMITS
NO ACCESS (BY PREVIOUS ACQUISITION/CONTROL)
NO ACCESS (BY ACQUISTION)
NO ACCESS (BY STATUTORY AUTHORITY)
SECTION LINE
QUARTER LINE
SIXTEENTH LINE
EXISTING CENTERLINE
PROPOSED REFERENCE LINE
PARALLEL OFFSET
ENCROACHMENT E-D/TYPE

CONVENTIONAL UTILITY SYMBOLS

			NON COMPENSABLE	COMPENSABLE
WATER	W	SANITARY SEWER	SAN	
GAS	G	STORM SEWER	SS	
TELEPHONE	T			
OVERHEAD TRANSMISSION LINES	OH	POWER POLE		
ELECTRIC	E	TELEPHONE POLE		
CABLE TELEVISION	TV	TELEPHONE PEDESTAL		
FIBER OPTIC	FO	ELECTRIC TOWER		

NOTES

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, (SAUK) COUNTY, NAD 83 (2007) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

RIGHT-OF-WAY MONUMENTS ARE TYPE 2 MONUMENTS (TYPICALLY ¾" X 24" REBAR) AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER SURVEYS OF PUBLIC RECORD.

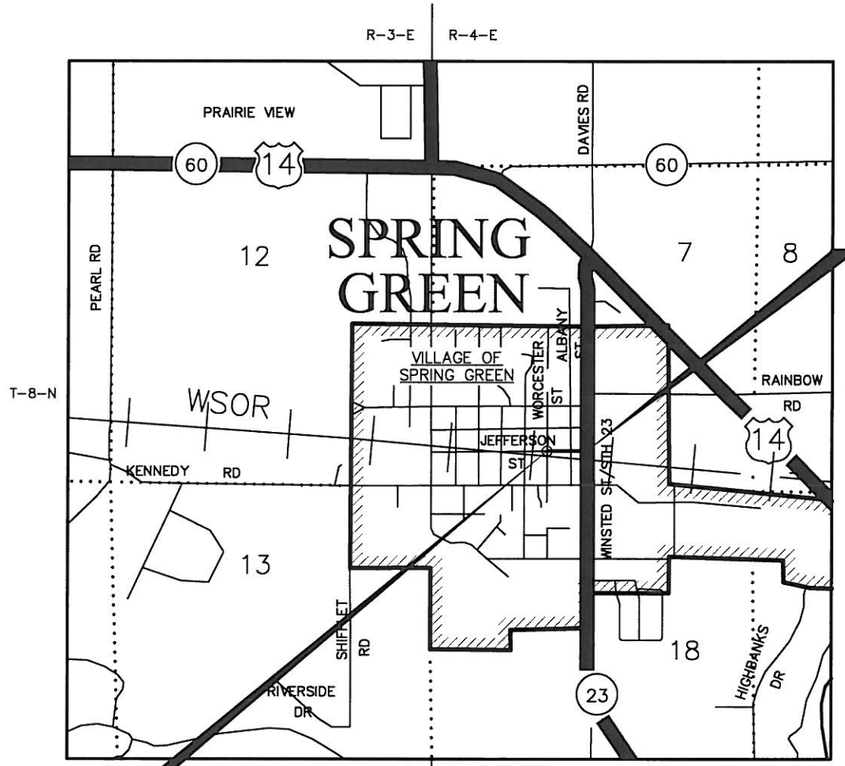
R/W PROJECT NUMBER	SHEET NUMBER	TOTAL SHEETS
5916-00-01	4.01	4
FEDERAL PROJECT NUMBER		

PLAT OF RIGHT-OF-WAY REQUIRED FOR

VILLAGE OF SPRING GREEN, EAST JEFFERSON STREET (WORCESTER STREET TO STH 23)

LOCAL STREET — SAUK COUNTY

CONSTRUCTION PROJECT NUMBER
5916-00-69/71



END RELOCATION ORDER
STA. 18+06.94
578.36' NORTH AND 34.49' WEST OF THE S¼ CORNER OF SECTION 7, T.8N., R.4E., VILLAGE OF SPRING GREEN, SAUK COUNTY, WI
Y = 130,321.30
X = 565,210.38

BEGIN RELOCATION ORDER
STA. 11+39.29
569.98' NORTH AND 702.09' WEST OF THE S¼ CORNER OF SECTION 7, T.8N., R.4E., VILLAGE OF SPRING GREEN, SAUK COUNTY, WI
Y = 130,312.93
X = 564,542.78

N

LAYOUT
SCALE 0 — 1/2 MI.

TOTAL NET LENGTH OF CENTERLINE = 0.126 MI.

REVISION DATE

JEWELL
associates engineers, inc.
Engineers - Surveyors - Architects

560 SUNRISE DRIVE
SPRING GREEN, WI 53588
PHONE : 608.588.7484
FAX : 608.588.9322

I HEREBY CERTIFY THAT THIS PLAT WAS MADE FOR THE VILLAGE OF SPRING GREEN AND IS CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

WISCONSIN
FREDERICK G. GRUBER
S-2282
SPRING GREEN WIS.
LAND SURVEYOR
1/9/14

APPROVED FOR THE VILLAGE OF SPRING GREEN
DATE 1/9/14 (NAME/TITLE)

FILE NAME : S:\Projects\S34083 Village of SG Jefferson Street\Right-of-Way\s34083 RW PLAT.dwg PLOT DATE : 1/9/2014 2:47 PM PLOT BY : Schaffer, Ellery PLOT NAME : PLOT SCALE : 1:2
WISDOT/CADDS SHEET 50

4

SCHEDULE OF LANDS & INTERESTS REQUIRED

SHEET NUMBER	PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	TOTAL ACRES	R/W ACRES REQUIRED			TOTAL ACRES REM.	T.L.E. ACRES
					NEW	EXISTING	TOTAL		
4.03	1	C/O BMO HARRIS BANK NA BANK OF SPRING GREEN	TLE	0.52	--	--	--	0.52	0.02
4.03	2	ROGER A BOWDEN	TLE	0.28	--	--	--	0.28	0.01
4.03	3	HERBERT P KRITZ	TLE	0.25	--	--	--	0.25	0.02
4.03 & 4.04	4	PECKHAM & NEIDER LLP	TLE	0.51	--	--	--	0.51	0.06
4.04	5	VICTORIA S HEINZ	TLE	0.27	--	--	--	0.27	0.01
4.04	6	JOHN B & CATHERINE M WITTENWYLER	TLE	0.14	--	--	--	0.14	0.01
4.04	7	BETTY J HILL	TLE	0.24	--	--	--	0.24	0.01
4.04	8	NATHANAEL P B & MARNIE L B DRESSER	TLE	0.27	--	--	--	0.27	0.02
4.04	9	KAREN M NELSON	TLE	0.17	--	--	--	0.17	0.01
4.03 & 4.04	10	SUSAN M MCCLINTOCK	TLE	0.14	--	--	--	0.14	0.01
4.03	11	PAMELA E CHEHADE	TLE	0.20	--	--	--	0.20	0.01
4.03	12	PAMELA MEIER-HAMACHEK	TLE	0.23	--	--	--	0.23	0.02
4.03	13	DAVIN PICKELL	TLE	0.26	--	--	--	0.26	0.01
4.03	14	JEFFREY R OLSON	TLE	0.26	--	--	--	0.26	0.01
4.03	15	HAHN PROPERTIES LLC	TLE	0.44	--	--	--	0.44	0.01

NOTE: AREAS SHOWN IN THE TOTAL ACRES COLUMN MAY BE APPROXIMATE AND ARE DERIVED FROM THE TAX ROLLS OR OTHER AVAILABLE SOURCES AND MAY NOT INCLUDE LANDS OF THE OWNER WHICH ARE NOT CONTIGUOUS TO THE AREA TO BE ACQUIRED. OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE VILLAGE OF SPRING GREEN.

4

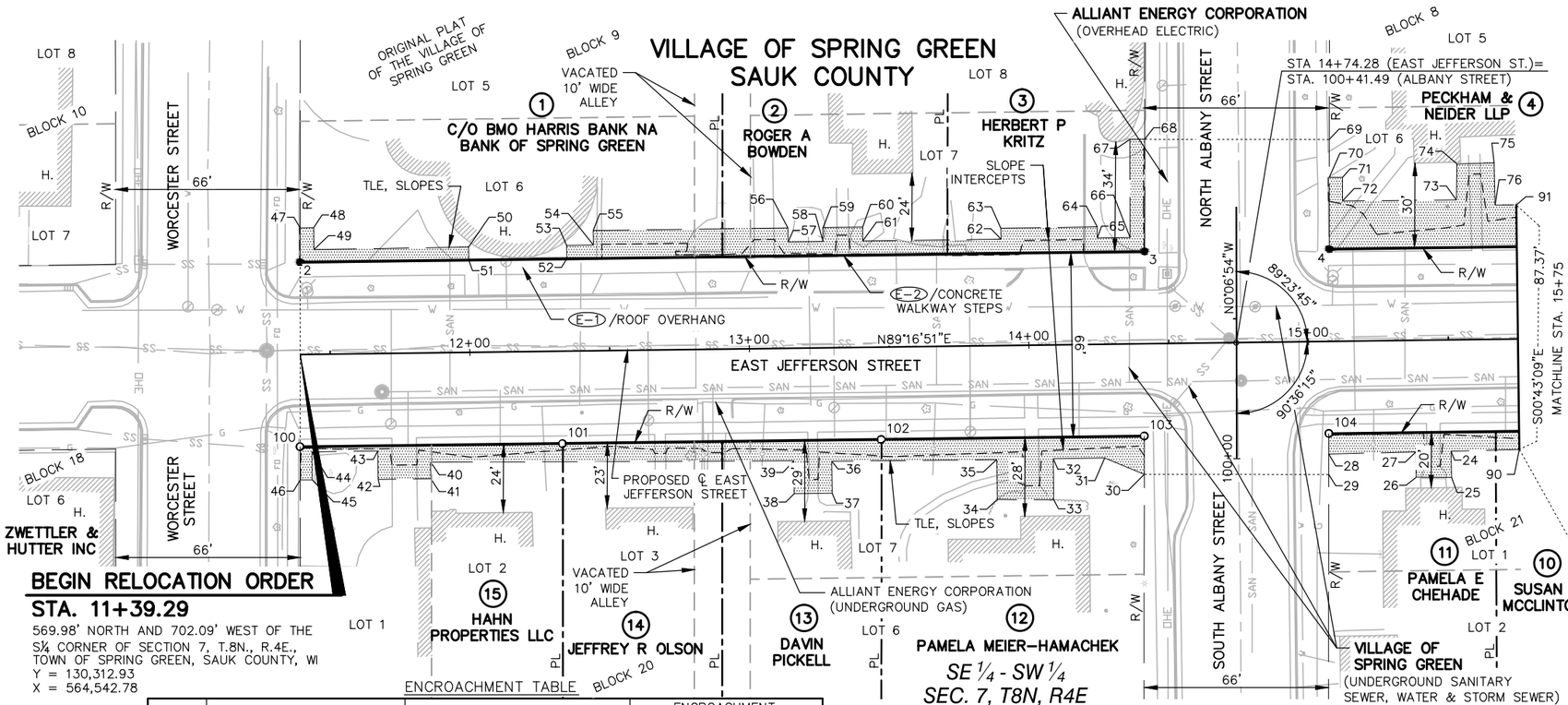
REVISION DATE	DATE	SCALE, FEET 0 50	HWY: EAST JEFFERSON STREET	STATE R/W PROJECT NUMBER: 5916-00-01	PLAT SHEET 4.02	
	GRID FACTOR N/A		COUNTY: SAUK	CONSTRUCTION PROJECT NUMBER: 5916-00-69/71	PS&E SHEET 24	E

LINE TABLE		
PT.# TO PT.#	BEARING	DISTANCE
90–24	S88°28'27"W	24.47'
24–25	S0°43'16"E	9.34'
25–26	N89°52'04"W	12.61'
26–27	N0°43'09"W	9.02'
27–28	S88°42'22"W	31.14'
28–29	S0°05'21"W	7.35'
29–30	N89°54'39"W	66.00'
30–31	N67°35'48"W	15.32'
31–32	S89°05'06"W	18.45'
32–33	S0°43'09"E	14.64'
33–34	N89°51'00"W	20.19'
34–35	N0°43'09"W	14.26'
35–36	S89°29'28"W	59.04'
36–37	S0°43'09"E	11.43'
37–38	S89°16'51"W	13.59'
38–39	N0°43'09"W	11.71'
39–40	S89°48'15"W	129.84'
40–41	S0°43'09"E	5.89'
41–42	S89°16'51"W	18.86'
42–43	N0°43'09"W	10.14'
43–44	S89°16'51"W	23.50'
44–45	S0°06'54"E	10.06'
45–46	S89°53'06"W	4.29'
46–100	N0°00'35"W	11.88'
100–2	N0°00'39"W	66.01'
2–47	N0°00'42"W	12.23'
47–48	N89°16'51"E	4.82'
48–49	S0°43'09"E	7.84'
49–50	N89°02'47"E	55.33'
50–51	S0°43'09"E	4.62'
51–52	N89°16'51"E	35.12'
52–53	N0°43'09"W	4.78'
53–54	N89°02'11"E	9.55'
54–55	N0°43'09"W	5.18'
55–56	N89°16'51"E	69.66'
56–57	S0°43'09"E	5.00'
57–58	N89°16'51"E	12.54'
58–59	N0°41'28"W	5.00'
59–60	N89°16'51"E	14.18'
60–61	S0°43'02"E	5.00'
61–62	N89°16'51"E	49.46'
62–63	N0°43'09"W	3.93'
63–64	N89°08'02"E	34.44'
64–65	S0°43'09"E	4.01'
65–66	N89°16'51"E	11.72'
66–67	N0°06'54"W	35.15'
67–68	N89°53'06"E	5.33'
68–69	S89°54'39"E	66.00'
69–70	S0°05'21"W	13.54'
70–71	N89°53'06"E	4.71'
71–72	S0°06'54"E	8.42'
72–73	N89°30'05"E	40.75'
73–74	N0°41'39"E	12.90'
74–75	N88°29'03"E	13.40'
75–76	S1°02'06"E	15.13'
76–91	N89°30'05"E	7.64'
91–90	S0°43'09"E	87.37'

COORDINATE TABLE – TLE POINTS				
PT.#	STATION	OFFSET	Y	X
24	15+50.54	39.81 R	130278.28	564954.49
25	15+50.54	49.15 R	130268.94	564954.61
26	15+37.93	48.96 R	130268.97	564942.00
27	15+37.93	39.93 R	130277.99	564941.88
28	15+06.79	40.25 R	130277.29	564910.75
29	15+06.68	47.60 R	130269.94	564910.74
30	14+40.69	46.67 R	130270.04	564844.74
31	14+26.60	40.65 R	130275.88	564830.58
32	14+08.16	40.71 R	130275.58	564812.13
33	14+08.16	55.35 R	130260.95	564812.31
34	13+87.97	55.05 R	130261.00	564792.12
35	13+87.97	40.78 R	130275.26	564791.94
36	13+28.93	40.57 R	130274.74	564732.91
37	13+28.93	52.00 R	130263.30	564733.05
38	13+15.33	52.00 R	130263.13	564719.46
39	13+15.33	40.29 R	130274.84	564719.31
40	11+85.49	39.11 R	130274.40	564589.47
41	11+85.49	45.00 R	130268.51	564589.55
42	11+66.63	45.00 R	130268.27	564570.69

COORDINATE TABLE – TLE POINTS				
PT.#	STATION	OFFSET	Y	X
43	11+66.63	34.86 R	130278.41	564570.56
44	11+43.13	34.86 R	130278.12	564547.06
45	11+43.02	44.93 R	130268.06	564547.08
46	11+38.74	44.88 R	130268.05	564542.79
47	11+39.85	45.23 L	130358.17	564542.78
48	11+44.67	45.23 L	130358.23	564547.60
49	11+44.67	37.39 L	130350.39	564547.70
50	12+00.00	37.62 L	130351.31	564603.02
51	12+00.00	33.00 L	130346.69	564603.08
52	12+35.12	33.00 L	130347.13	564638.19
53	12+35.12	37.78 L	130351.91	564638.13
54	12+44.66	37.82 L	130352.07	564647.68
55	12+44.66	43.00 L	130357.25	564647.62
56	13+14.32	43.00 L	130358.13	564717.27
57	13+14.32	38.00 L	130353.13	564717.33
58	13+26.86	38.00 L	130353.28	564729.87
59	13+26.87	43.00 L	130358.28	564729.81
60	13+41.05	43.00 L	130358.46	564743.99
61	13+41.05	38.00 L	130353.46	564744.05

COORDINATE TABLE – TLE POINTS				
PT.#	STATION	OFFSET	Y	X
62	13+90.51	38.00 L	130354.08	564793.51
63	13+90.51	41.93 L	130358.01	564793.46
64	14+24.95	42.01 L	130358.53	564827.90
65	14+24.95	38.00 L	130354.52	564827.95
66	14+36.67	38.00 L	130354.67	564839.67
67	14+37.05	73.14 L	130389.82	564839.60
68	14+42.38	73.09 L	130389.83	564844.93
69	15+08.37	72.15 L	130389.73	564910.93
70	15+08.18	58.61 L	130376.19	564910.90
71	15+12.90	58.56 L	130376.20	564915.61
72	15+12.81	50.14 L	130367.78	564915.63
73	15+53.56	49.99 L	130368.13	564956.38
74	15+53.88	62.88 L	130381.03	564956.54
75	15+67.28	63.07 L	130381.39	564969.93
76	15+67.36	47.93 L	130366.26	564970.20
90	15+75.00	39.46 R	130278.93	564978.95
91	15+75.00	47.90 L	130366.32	564977.84



ENCROACHMENT TABLE			
NUMBER	OWNER	LOCATION	ENCROACHMENT TYPE
E–1	C/O BMO HARRIS BANK NA BANK OF SPRING GREEN	STA. 12+00 – STA. 12+36, LT.	ROOF OVERHANG
E–2	ROGER A BOWDEN	STA. 13+32 – STA. 13+36, LT.	CONCRETE WALKWAY STEPS

COORDINATE TABLE – NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
1	18+06.06	33.00 R	130288.30	565209.91
2	11+39.70	33.00 L	130345.93	564542.78
3	14+41.81	33.00 L	130349.72	564844.87
4	15+07.82	33.00 L	130350.55	564910.87

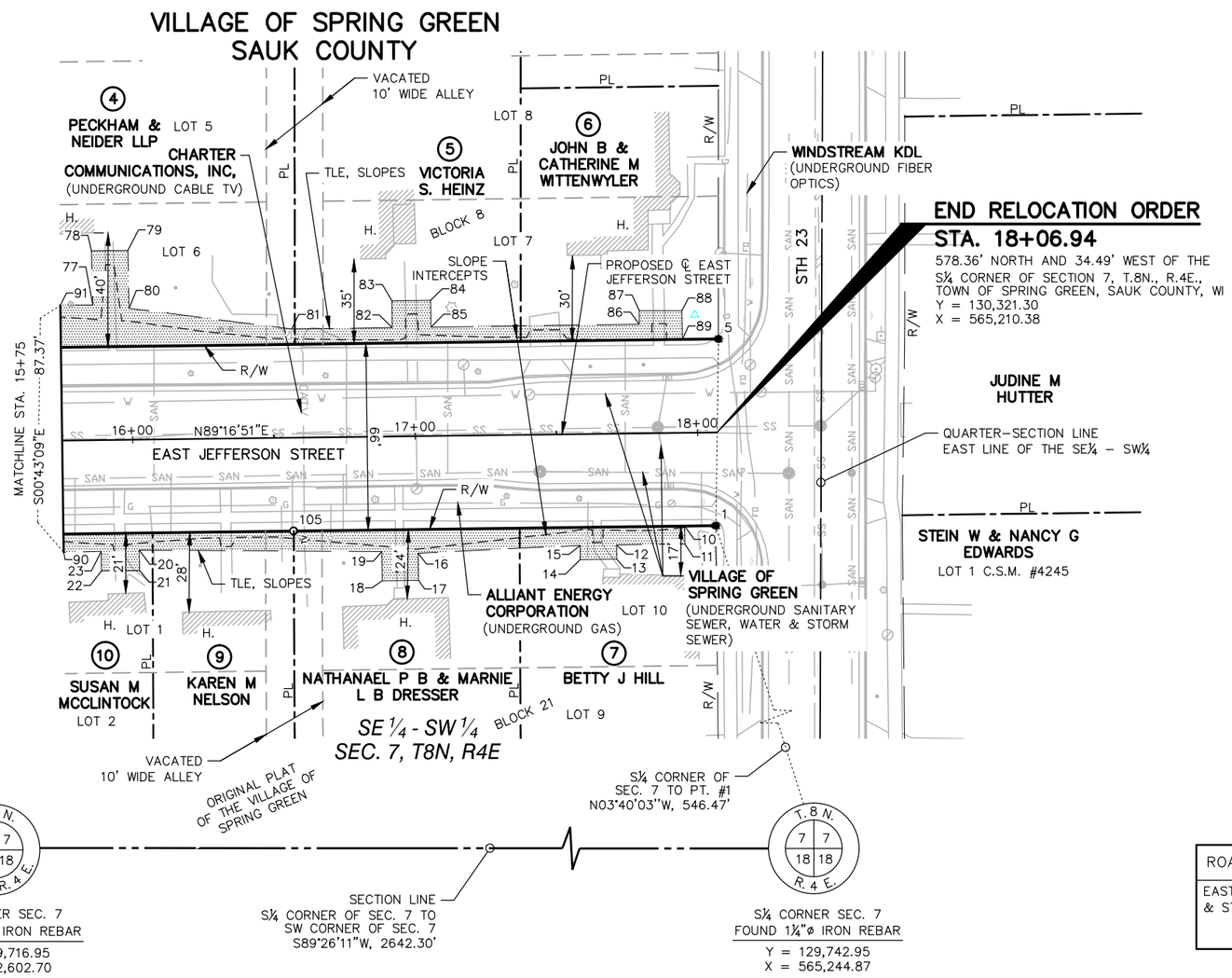
COORDINATE TABLE – FOUND SURVEY MONUMENTS					
PT.#	STATION	OFFSET	Y	X	DESCRIPTION
100	11+38.88	33.00 R	130279.93	564542.79	¾"ø REBAR
101	12+32.75	33.12 R	130280.99	564636.65	1¼ø REBAR
102	13+46.79	33.11 R	130282.42	564750.68	¾"ø REBAR
103	14+40.93	33.04 R	130283.68	564844.81	¾"ø REBAR
104	15+06.94	32.97 R	130284.57	564910.81	¾"ø REBAR

ROAD NAME	BASIS OF EXISTING R/W
EAST JEFFERSON STREET, WORCESTER STREET, SOUTH & NORTH ALBANY STREET	COUNTY RECORDS, WIS. STATUTES 82.31(2), & ORIGINAL PLAT OF THE VILLAGE OF SPRING GREEN

REVISION DATE	DATE	SCALE, FEET 0 50	HWY: EAST JEFFERSON STREET	STATE R/W PROJECT NUMBER: 5916–00–01	PLAT SHEET 4.03
	GRID FACTOR N/A		COUNTY: SAUK	CONSTRUCTION PROJECT NUMBER: 5916–00–69/71	PS&E SHEET 25 E

COORDINATE TABLE – NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
1	18+06.06	33.00 R	130288.30	565209.91
5	18+07.82	33.00 L	130354.30	565210.84

COORDINATE TABLE – FOUND SURVEY MONUMENTS					
PT.#	STATION	OFFSET	Y	X	DESCRIPTION
105	16+56.58	33.00 R	130286.42	565060.44	¾"ø REBAR



LINE TABLE		
PT.# TO PT.#	BEARING	DISTANCE
1–10	S89°16'51"W	11.05'
10–11	S0°43'09"E	6.34'
11–12	S89°16'14"W	24.21'
12–13	S0°43'09"E	5.00'
13–14	S89°16'14"W	12.88'
14–15	N0°43'09"W	5.00'
15–16	S87°14'54"W	57.45'
16–17	S0°43'09"E	9.62'
17–18	S89°16'51"W	12.80'
18–19	N0°43'09"W	10.36'
19–20	N89°39'26"W	85.90'
20–21	S0°43'09"E	7.25'
21–22	S89°29'47"W	13.46'
22–23	N0°43'09"W	6.97'
23–90	S88°28'27"W	13.37'
90–91	N0°43'09"W	87.37'
91–77	N89°30'05"E	11.44''
77–78	N1°15'55"W	19.14'
78–79	N89°16'51"E	12.86'
79–80	S1°13'49"E	20.45'
80–81	S82°40'55"E	58.00'
81–82	N89°13'11"E	35.64'
82–83	N0°43'09"W	9.52'
83–84	N89°16'51"E	13.70'
84–85	S0°43'09"E	9.51'
85–86	N89°13'11"E	73.82'
86–87	N0°32'27"W	4.80'
87–88	N89°58'23"E	15.10'
88–89	S0°43'09"E	10.19'
89–5	N89°16'51"E	12.82'
5–1	S0°48'18"W	66.02'

COORDINATE TABLE – TLE POINTS				
PT.#	STATION	OFFSET	Y	X
10	17+95.01	33.00 R	130288.16	565198.87
11	17+95.01	39.34 R	130281.82	565198.94
12	17+70.80	39.34 R	130281.51	565174.74
13	17+70.80	44.34 R	130276.51	565174.80
14	17+57.93	44.34 R	130276.35	565161.92
15	17+57.93	39.35 R	130281.35	565161.86
16	17+00.51	41.38 R	130278.59	565104.47
17	17+00.51	51.00 R	130268.97	565104.59
18	16+87.71	51.00 R	130268.81	565091.80
19	16+87.71	40.64 R	130279.17	565091.67
20	16+01.83	39.04 R	130279.68	565005.77
21	16+01.83	46.30 R	130272.43	565005.86
22	15+88.37	46.25 R	130272.31	564992.40
23	15+88.37	39.27 R	130279.28	564992.31
77	15+86.44	47.86 L	130366.39	564989.29
78	15+86.26	67.00 L	130385.52	564988.87
79	15+99.12	67.00 L	130385.69	565001.73
80	15+99.30	46.55 L	130365.24	565002.16
81	16+56.73	38.44 L	130357.85	565059.69
82	16+92.37	38.48 L	130358.34	565095.33
83	16+92.37	48.00 L	130367.86	565095.21
84	17+06.07	48.00 L	130368.03	565108.91
85	17+06.07	38.49 L	130358.52	565109.03
86	17+79.89	38.57 L	130359.53	565182.84
87	17+79.90	43.38 L	130364.33	565182.80
88	17+95.00	43.19 L	130364.33	565197.90
89	17+95.00	33.00 L	130354.14	565198.02
90	15+75.00	39.46 R	130278.93	564978.95
91	15+75.00	47.90 L	130366.29	564977.85

ROAD NAME	BASIS OF EXISTING R/W
EAST JEFFERSON STREET & STH 23	QUARTER SECTION LINE, COUNTY RECORDS, WIS. STATUTES 82.31(2), & R/W PLAT PROJECT # 5080–01–21 & THE ORIGINAL PLAT OF THE VILLAGE OF SPRING GREEN

REVISION DATE	DATE	SCALE, FEET	HWY: EAST JEFFERSON STREET	STATE R/W PROJECT NUMBER: 5916–00–01	PLAT SHEET 4.04	
	GRID FACTOR N/A	0 50	COUNTY: SAUK	CONSTRUCTION PROJECT NUMBER: 5916–00–69/71	PS&E SHEET 26	E

LEGEND

XXXXX SAWING ASPHALT

CURB RAMP DETECTABLE WARNING FIELD -TYPE 1

CURB RAMP DETECTABLE WARNING FIELD -TYPE 2

DIRECTION OF FLOW

SILT FENCE

TYPE A PROTECTION PLACED PRIOR TO CURB & GUTTER CONSTRUCTION AND TYPE C PLACED AFTER

EXISTING AESTHETIC AGGREGATE SIDEWALK (TYP. TO REMAIN)

EXISTING LANDSCAPE AREA (TYP. TO REMAIN)

VILLAGE OF SPRING GREEN (TYP.)

(TYPE C ONLY)

EXISTING ℄ EAST JEFFERSON STREET

ZWETTLER & HUTTER INC

BUILDING

BEGIN PROJECT
STA. 11+05
Y= 130,312.50
X= 564,508.49

CHARTER COMMUNICATIONS

EXISTING LANDSCAPE AREA TO REMAIN

T.L.E. REQUIRED (TYP.)

CONCRETE SIDEWALK 4-INCH (TYP.)

EXISTING ℄ WORCESTER STREET

HAHN PROPERTIES LLC

C/O BMO HARRIS BANK NA
BANK OF SPRING GREEN

STA. 12+61, LT.
CONSTRUCT C.E. (ASPHALT)

STA. 12+92
32.0' LT.

FINISHED ℄ EAST JEFFERSON STREET

EXISTING RETAINING WALL TO REMAIN

PARKING LOT

STA. 12+74
33.0' LT.

JEFFREY R OLSON
STA. 12+87, RT.
CONSTRUCT P.E. (ASPHALT)

ROGER A BOWDEN
STA. 13+06, LT.
CONSTRUCT P.E. (CONCRETE)

EXISTING LANDSCAPE AREAS TO REMAIN

DAVIN PICKELL

HERBERT P KRITZ

STA. 13+61
32.0' LT.

STA. 13+71
32.4' LT.

PARKING LOT

STA. 14+09, LT.
CONSTRUCT C.E. (ASPHALT)

FINISHED ℄ NORTH ALBANY STREET

ALLIANT ENERGY (TYP.)

CAUTION

STA. 14+74.28 (EAST JEFFERSON STREET)=
STA. 100+41.49 (ALBANY STREET)

PAMELA MEIER-HAMACHEK

EXISTING ℄ SOUTH ALBANY STREET

FINISHED ℄ SOUTH ALBANY STREET

PECKHAM & NEIDER LLP

EXISTING ℄ NORTH ALBANY STREET

SLOPE INTERCEPTS

PAMELA E CHEHADE

ALLIANT ENERGY

CAUTION

SUSAN M MCCLINTOCK

VILLAGE OF SPRING GREEN (TYP.)

MATCHLINE STA. 16+00

N89°16'51"E

90°36'15"

BM 1

BM 2

33' 33' R/W

18.2' 18.9'

15.7' 18.1'

725.39 725.23 725.14 725.06 724.97 724.86 724.79 724.91 724.96 725.00 724.63 724.80 724.45 724.62 725.19 725.28 725.30 725.01 725.30 725.03 724.79 724.52 724.01 724.07 724.25 724.12 724.21 724.15 725.79 725.93 724.81 725.17 724.80 724.68 724.78 724.18 723.74 723.56 723.32

12+00 13+00 14+00 15+00 16+

5 5

730 728 726 724 722 720 718 716 714

PI STA = 11+05.00
EL = 725.68

MATCH EXISTING
STA. 11+05
EL. = 725.68

-0.78%

VPC STA = 11+80.00
EL = 725.10

K = 40.83'
L = 90.00'

PI STA 12+25.00
EL 724.75

VPT STA = 12+70.00
EL = 725.39

+1.43%

VPC STA = 12+71.00
EL = 725.41

K = 45.04'
L = 90.00'

PI STA 13+16.00
EL 726.05

VPT STA = 13+61.00
EL = 725.79

FINISHED ℄ PROFILE EAST JEFFERSON STREET

EXISTING ℄ PROFILE EAST JEFFERSON STREET

-0.57%

VPC STA = 14+29.00
EL = 725.41

PI STA 14+74.00
EL 725.15

K = 102.16'
L = 90.00'

VPT STA = 15+19.00
EL = 724.50

-1.45%

VPC STA = 15+20.00
EL = 724.48

PI STA 15+65.00
EL 723.83

K = 89.27'
L = 90.00'

STA. 11+05

EXISTING ℄ ELEVATION
FINISHED ℄ ELEVATION

725.89 725.94 725.85 725.78 725.73 725.70 725.53 725.56 725.31 725.41 725.44 725.25 725.44 725.10 725.45 724.99 725.44 724.98 725.43 725.07 725.37 725.26 725.28 725.53 725.09 725.73 724.96 725.84 724.83 725.86 724.74 725.80 724.72 725.69 724.68 725.57 724.60 725.46 724.56 725.34 724.56 725.18 724.47 724.99 724.35 724.76 724.29 724.48 724.22 724.22 724.18 723.99 724.15 723.81 724.11 723.68

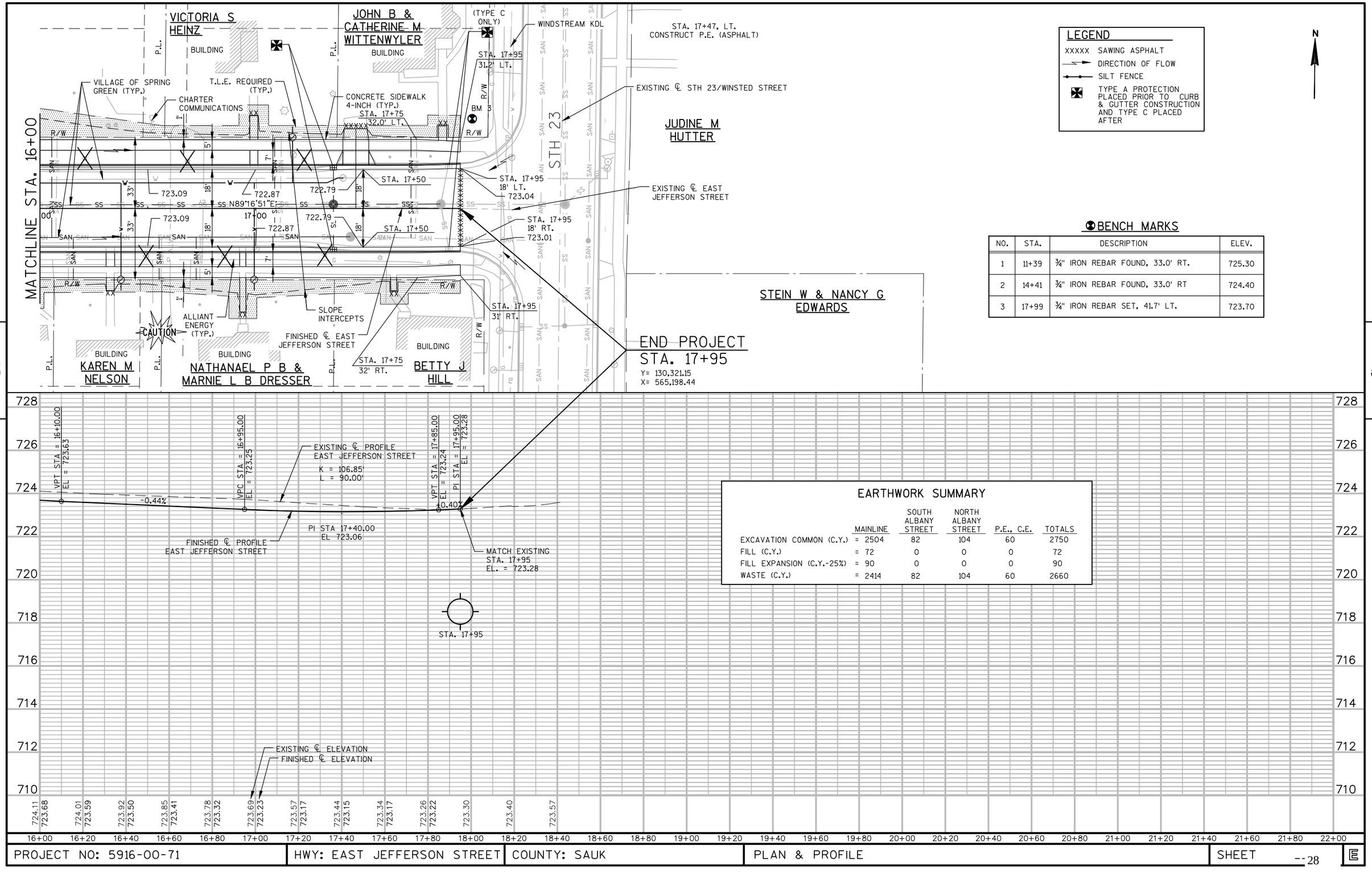
10+00 10+20 10+40 10+60 10+80 11+00 11+20 11+40 11+60 11+80 12+00 12+20 12+40 12+60 12+80 13+00 13+20 13+40 13+60 13+80 14+00 14+20 14+40 14+60 14+80 15+00 15+20 15+40 15+60 15+80 16+00

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
1	11+39	¾" IRON REBAR FOUND, 33.0' RT.	725.30
2	14+41	¾" IRON REBAR FOUND, 33.0' RT	724.40
3	17+99	¾" IRON REBAR SET, 41.7' LT.	723.70

EARTHWORK SUMMARY

	MAINLINE	SOUTH ALBANY STREET	NORTH ALBANY STREET	P.E., C.E.	TOTALS
EXCAVATION COMMON (C.Y.)	= 2504	82	104	60	2750
FILL (C.Y.)	= 72	0	0	0	72
FILL EXPANSION (C.Y.-25%)	= 90	0	0	0	90
WASTE (C.Y.)	= 2414	82	104	60	2660



EARTHWORK SUMMARY

	MAINLINE	SOUTH ALBANY STREET	NORTH ALBANY STREET	P.E., C.E.	TOTALS
EXCAVATION COMMON (C.Y.)	= 2504	82	104	60	2750
FILL (C.Y.)	= 72	0	0	0	72
FILL EXPANSION (C.Y.-25%)	= 90	0	0	0	90
WASTE (C.Y.)	= 2414	82	104	60	2660

PROJECT NO: 5916-00-71 | HWY: EAST JEFFERSON STREET | COUNTY: SAUK | PLAN & PROFILE | SHEET 28

LEGEND

XXXXX SAWING ASPHALT

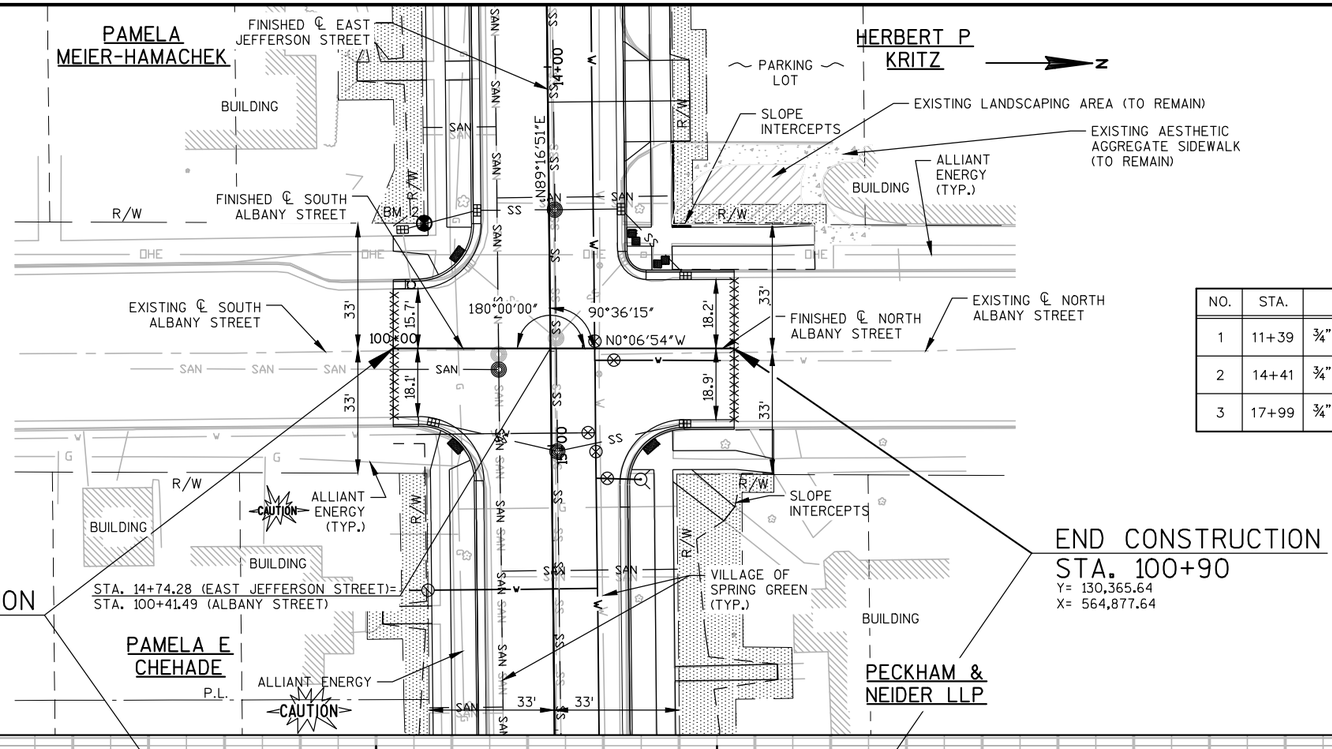
CURB RAMP DETECTABLE WARNING FIELD – TYPE 1

CURB RAMP DETECTABLE WARNING FIELD – TYPE 2

DIRECTION OF FLOW

SILT FENCE

TYPE A PROTECTION PLACED PRIOR TO CURB & GUTTER CONSTRUCTION AND TYPE C PLACED AFTER



BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
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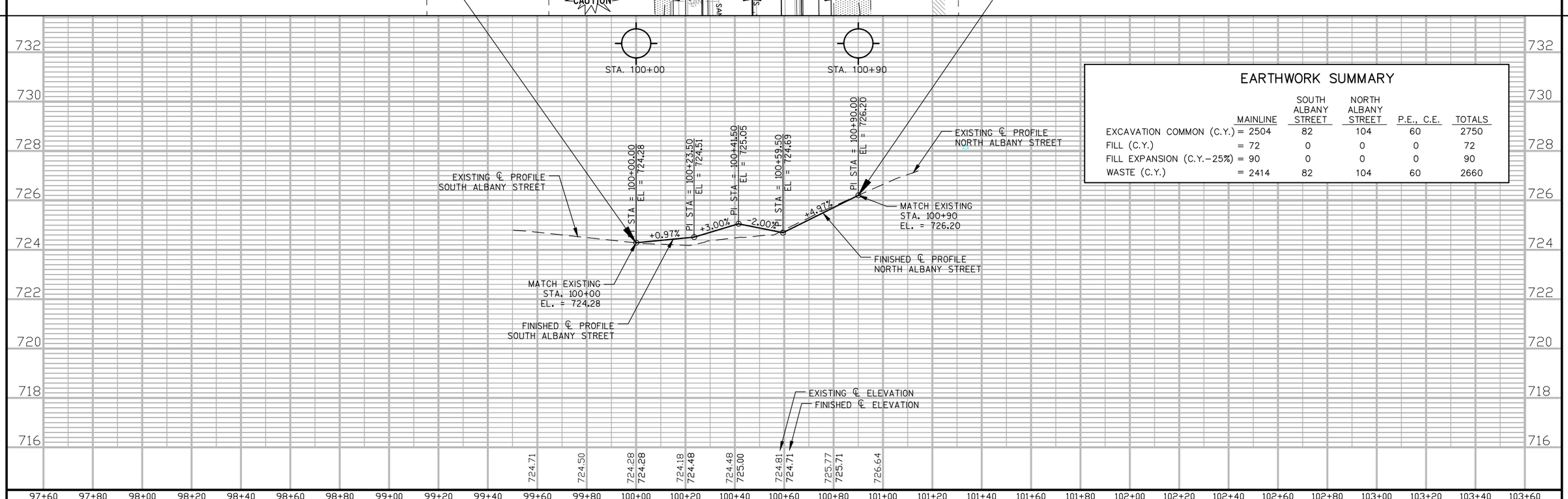
2	14+41	¾" IRON REBAR FOUND, 33.0' RT	724.40
3	17+99	¾" IRON REBAR SET, 41.7' LT.	723.70

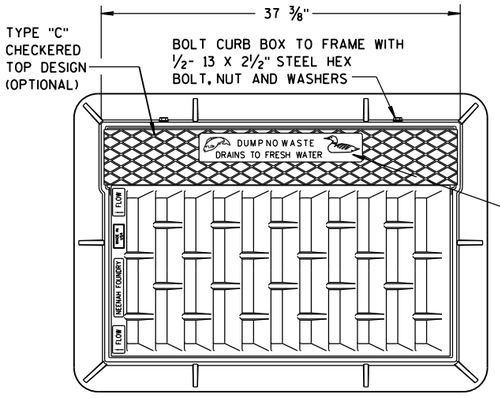


EARTHWORK SUMMARY

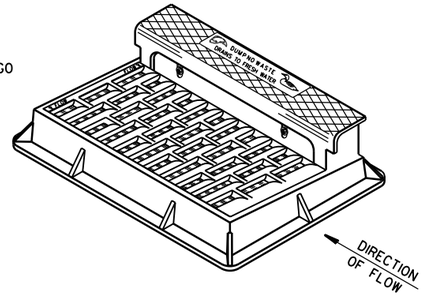
	MAINLINE	SOUTH ALBANY STREET	NORTH ALBANY STREET	P.E., C.E.	TOTALS
EXCAVATION COMMON (C.Y.)	= 2504	82	104	60	2750
FILL (C.Y.)	= 72	0	0	0	72
FILL EXPANSION (C.Y.–25%)	= 90	0	0	0	90
WASTE (C.Y.)	= 2414	82	104	60	2660

Standard Detail Drawing List

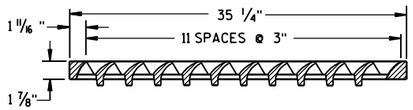
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-19B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08A05-19D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08B09-01	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER
08C07-01	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08C08-01	INLETS MEDIAN 1 AND 2 GRATE
08D01-17	CONCRETE CURB, CONCRETE CURB AND GUTTER AND TIES
08D05-15A	CURB RAMPS TYPES 1 AND 1-A
08D05-15B	CURB RAMPS TYPES 2 AND 3
08D05-15C	CURB RAMPS TYPES 4A AND 4A1
08D05-15D	CURB RAMPS TYPE 4B AND 4B1
08D05-15E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
14A02-01	TREE PLANTING DETAIL
15C02-05A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-05B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C03-02	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C33-01	STOP LINE AND CROSSWALK PAVEMENT MARKING

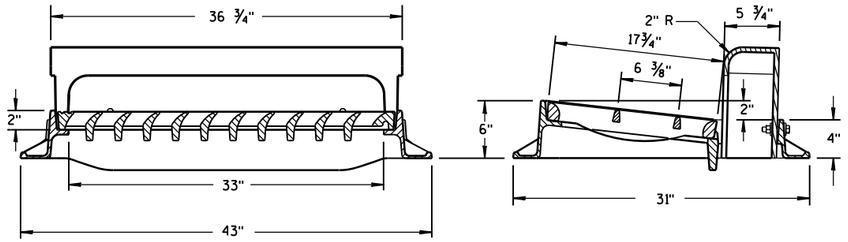


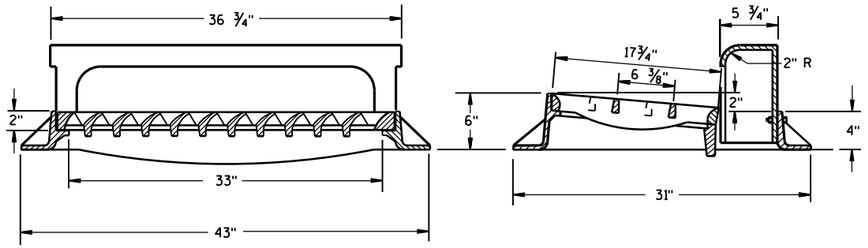
NOTE:
GRATE IS REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

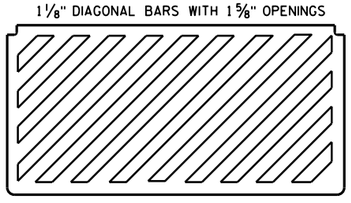






TYPE "H"

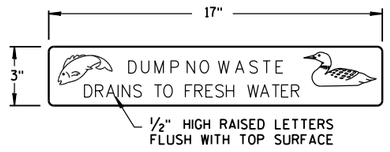
NOTE: EITHER CASTING IS ACCEPTABLE



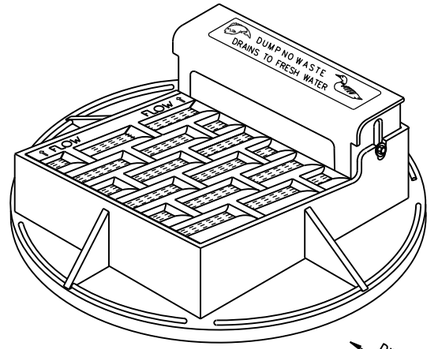
SPECIAL GRATE FOR TYPE "H" COVER

(MEASURES 35 1/4" X 17 3/4" X 2")

(NOTED AS TYPE H-S ON DRAINAGE TABLE)



LOGO DETAIL



NOTE: CURB BOX ADJUSTABLE 4" TO 9"

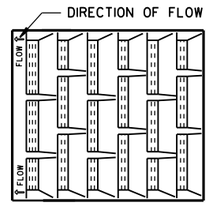
NOTE:
GRATE IS REVERSIBLE.

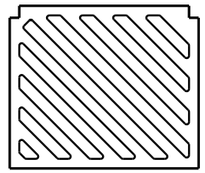
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

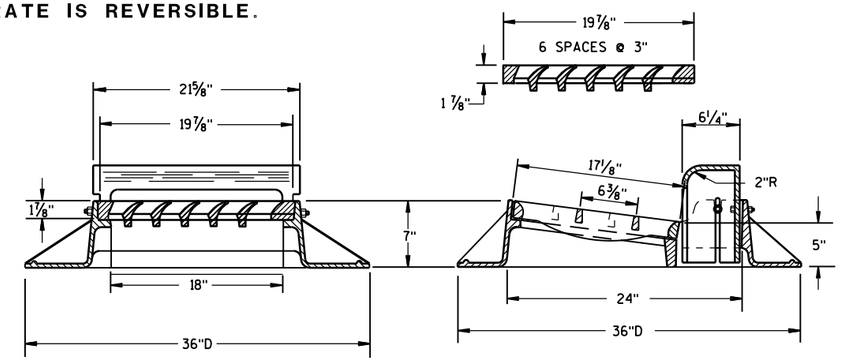




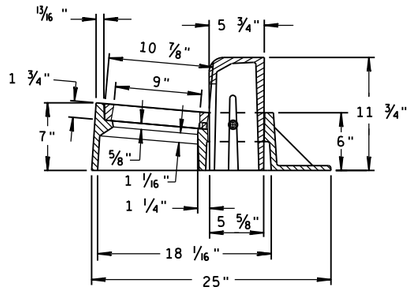
SPECIAL GRATE FOR TYPE "A" COVER

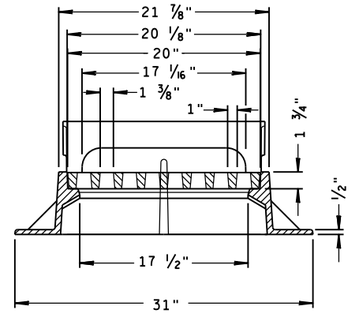
(MEASURES 19 3/4" X 17" X 1 7/8")

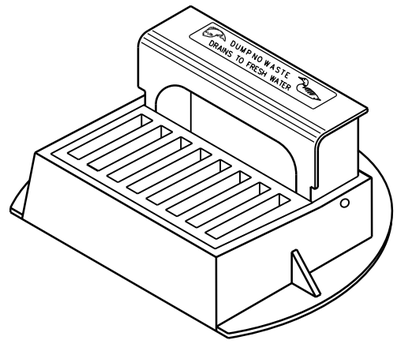
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"







TYPE "Z"

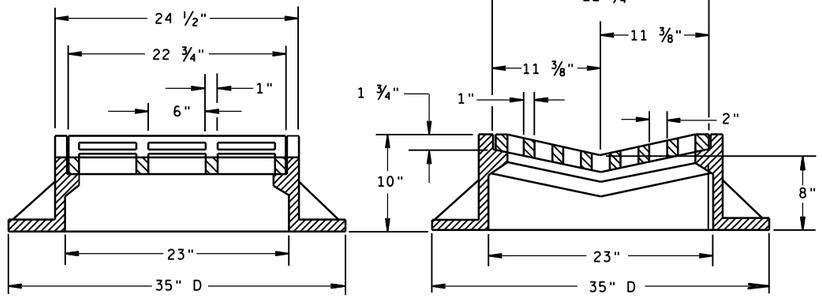
INLET COVERS
TYPE A, H, A-S, H-S & Z

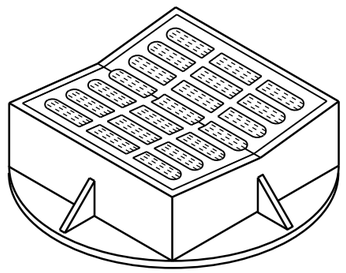
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11-27-13 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

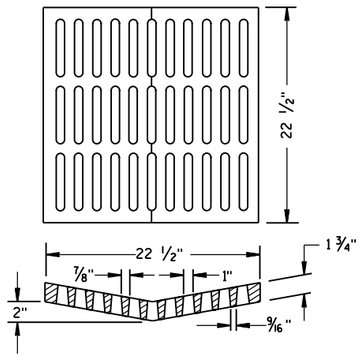
6

S.D.D. 8 A 5-19a



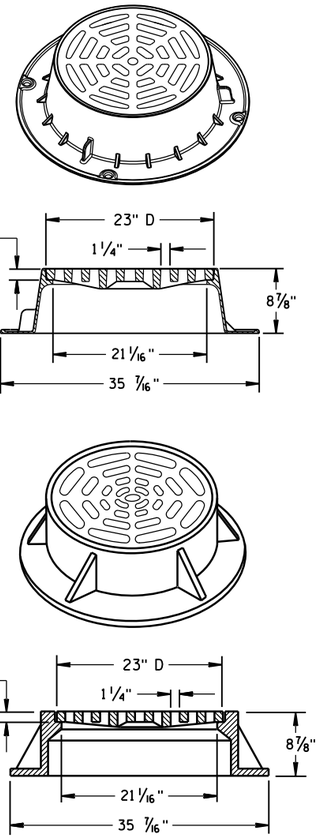


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

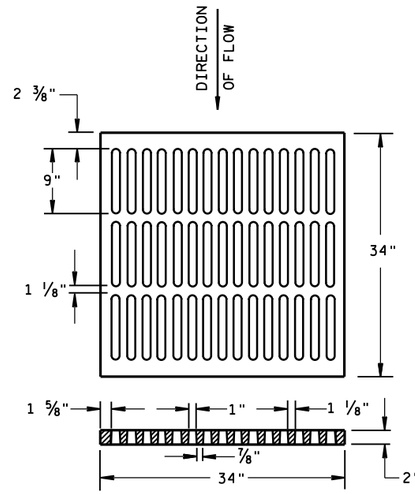
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

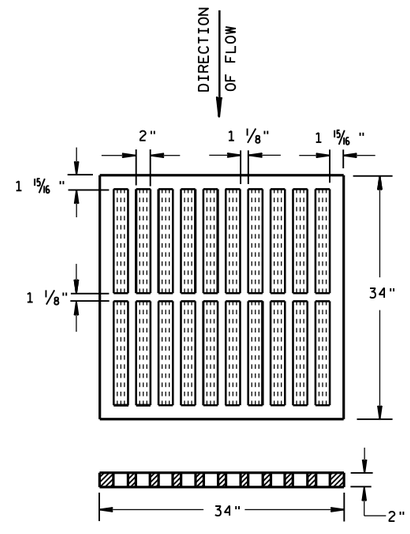
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



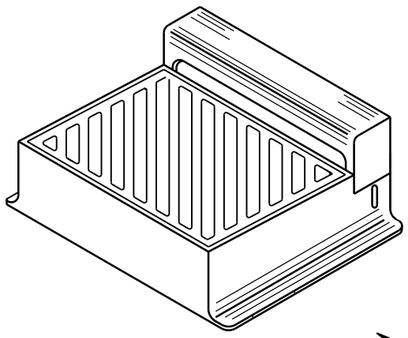
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE

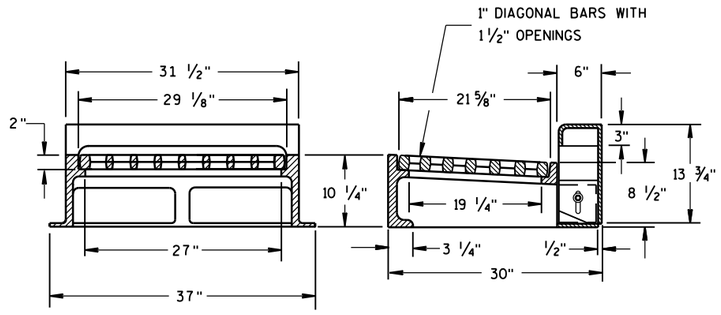


TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM

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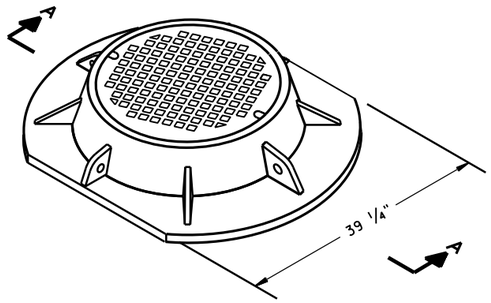
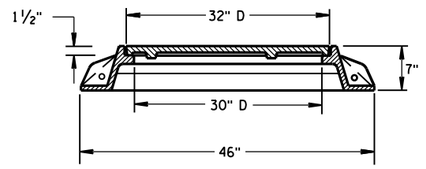
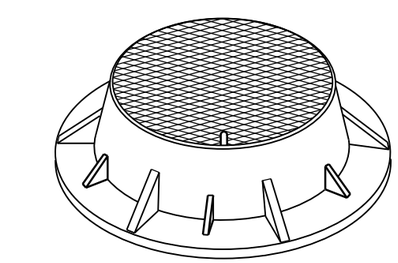
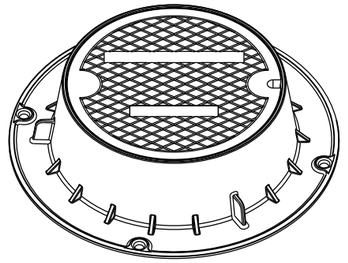
S.D.D. 8 A 5-19b

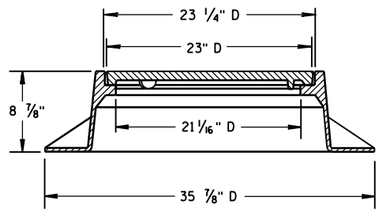


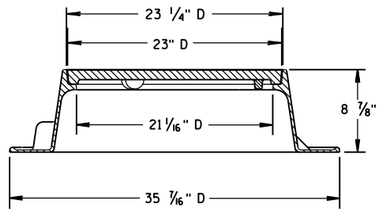


SECTION A-A
TYPE "K"









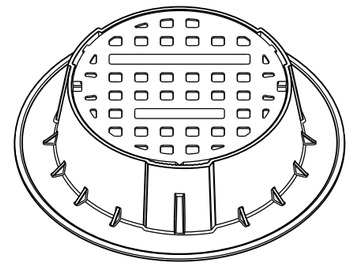
TYPE "J"
NOTE: EITHER CASTING IS ACCEPTABLE

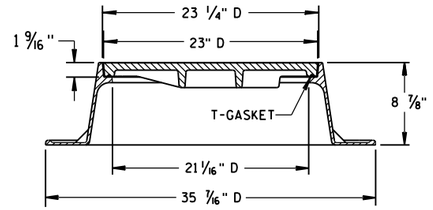
GENERAL NOTES

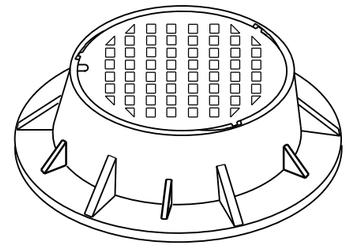
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

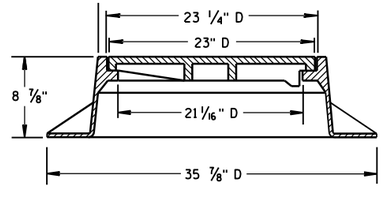
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

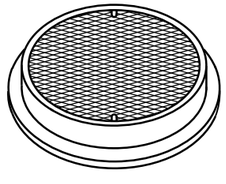


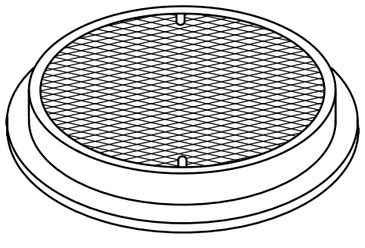


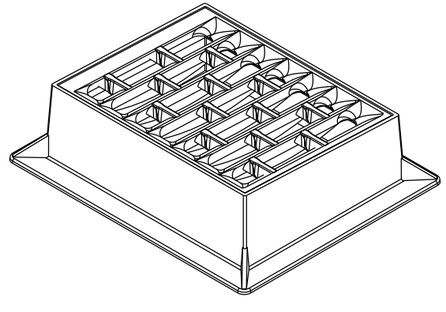


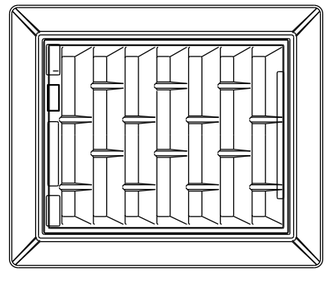


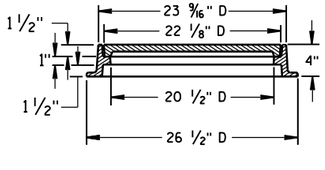
TYPE "J" SPECIAL
TYPE "B" NON-ROCKING SELF-SEAL LID
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)
NOTE: EITHER CASTING IS ACCEPTABLE



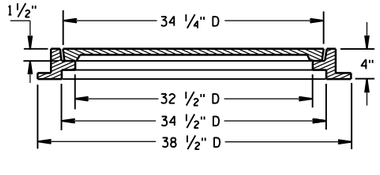




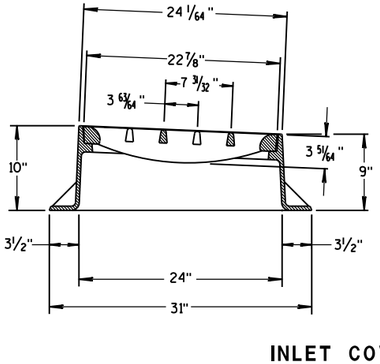


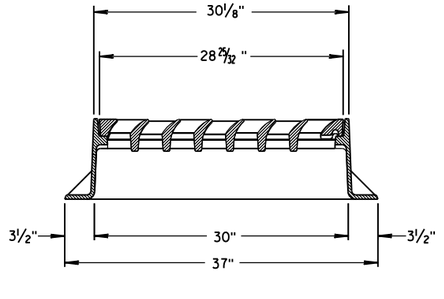


TYPE "L"



TYPE "M"





INLET COVER TYPE "BW"

INLET COVER TYPE BW
MANHOLE COVERS, TYPE K, J, J-S, L & M

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11/27/2013
DATE
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ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

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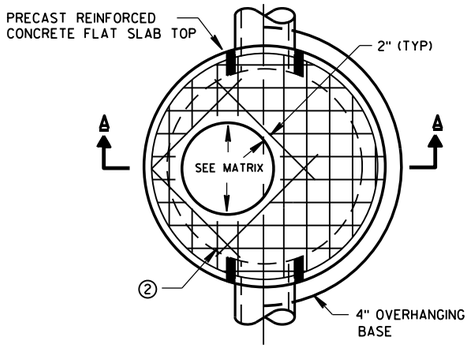
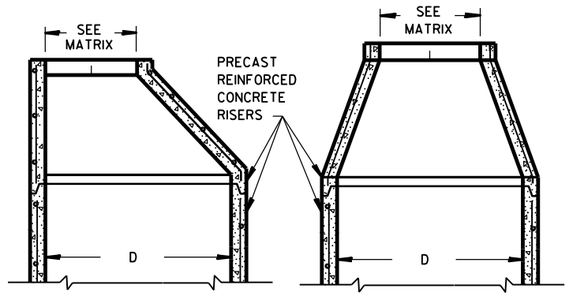
S.D.D. 8 A 5-19d

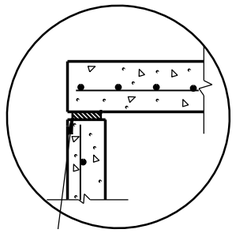


PLAN VIEW CIRCULAR OPENING

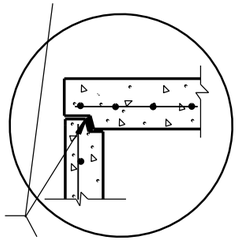


OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP

OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP

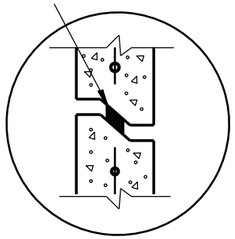


TOP WITH PLAIN END JOINT



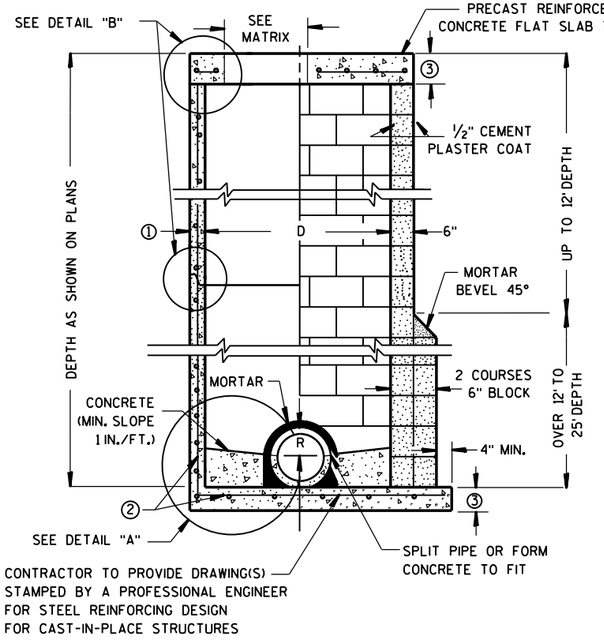
JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C990 (TYP)

TOP WITH TONGUE AND GROOVE JOINT



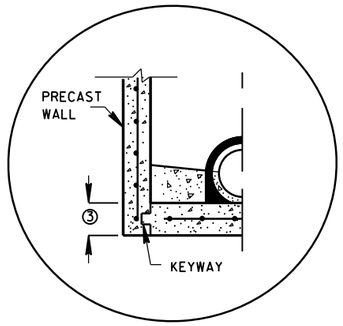
RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

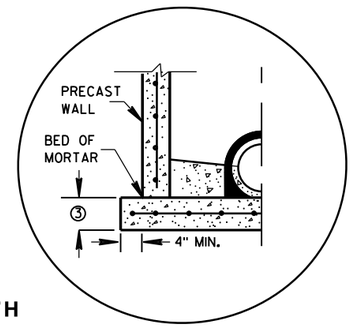


PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②

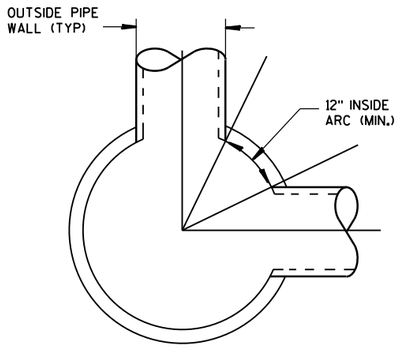


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

DETAIL "A"



DETAIL "C"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES. THE CONE TOPS SHALL BE INSTALLED ON A BED OF MORTAR.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES, AND CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF ½" AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

CONCRETE BLOCK WILL NOT BE PERMITED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M 199.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT, 5 INCHES FOR 4-FT, 6 INCHES FOR 5-FT, 7 INCHES FOR 6-FT, 8 INCHES FOR 7-FT AND 9 INCHES FOR 8-FT DIAMETER PRECAST MANHOLES.

② FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

③ PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS OF 8".

MANHOLE COVER OPENING MATRIX

MANHOLE COVER TYPE	C	ALL J'S	K	L	M
OPENING SIZE (FT)					
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

MANHOLE SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	36
7-FT	48	36
8-FT	60	42

MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER

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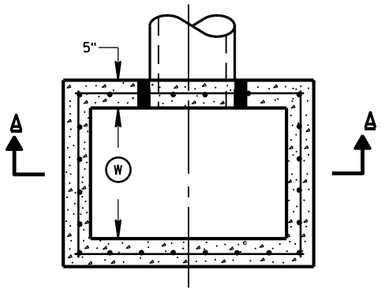
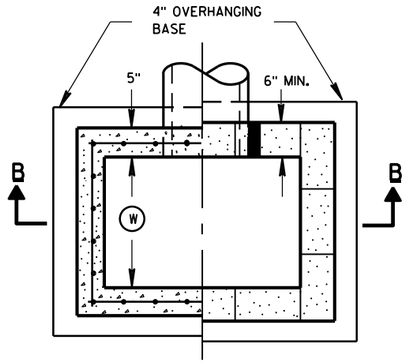
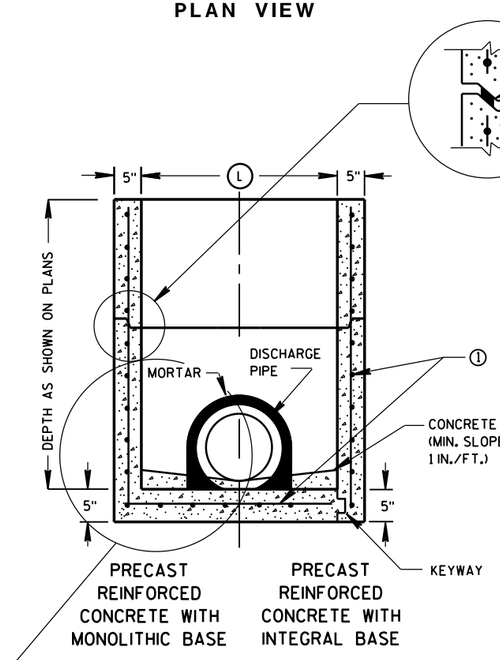


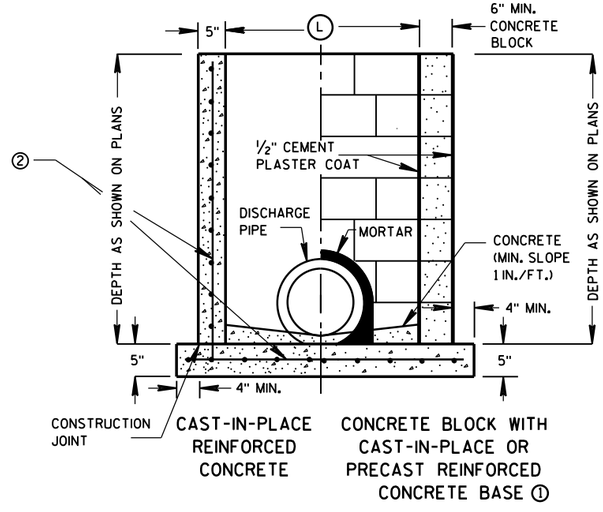
PLAN VIEW



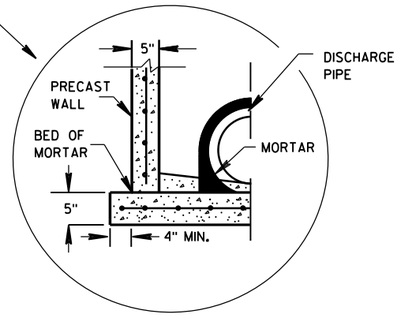
PLAN VIEW



SECTION A-A



SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS.
4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED.
OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

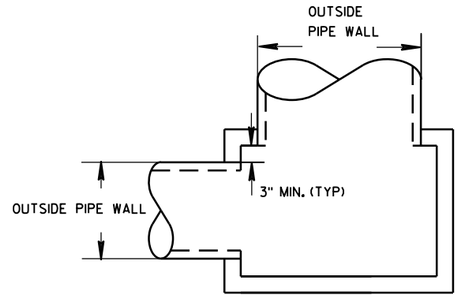
② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

INLET SIZE		INLET COVER TYPE	ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM
	WIDTH (W) (FT)	LENGTH (L) (FT)									
2X2-FT	2	2	X	X				X		X	
2X2.5-FT	2	2.5			X			X	X	X	X
2X3-FT	2	3					X				
2.5X3-FT	2.5	3				X					

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

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6/5/2012
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ROADWAY STANDARDS DE
ENGINEER

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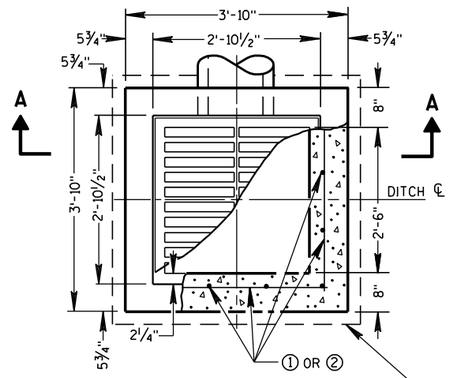
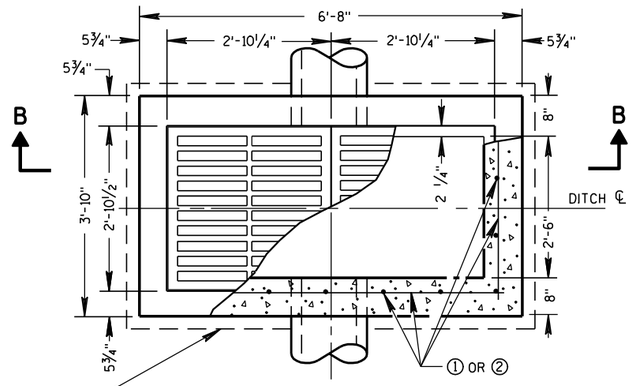
S.D.D. 8 C 7-1

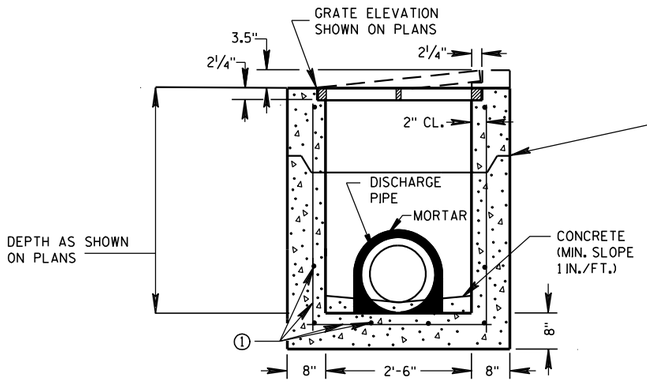


PLAN VIEW

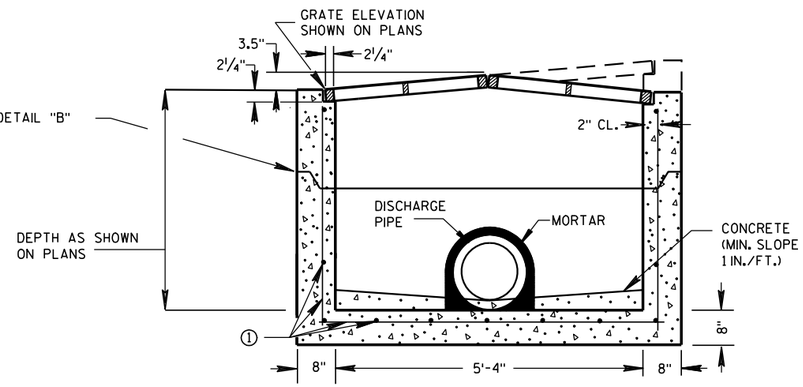


PLAN VIEW

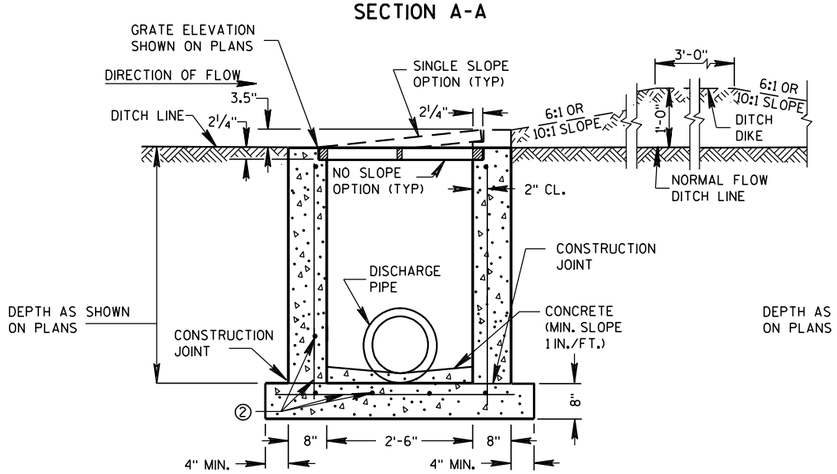
4" OVERHANGING BASE ON REINFORCED CAST-IN-PLACE CONCRETE INLETS



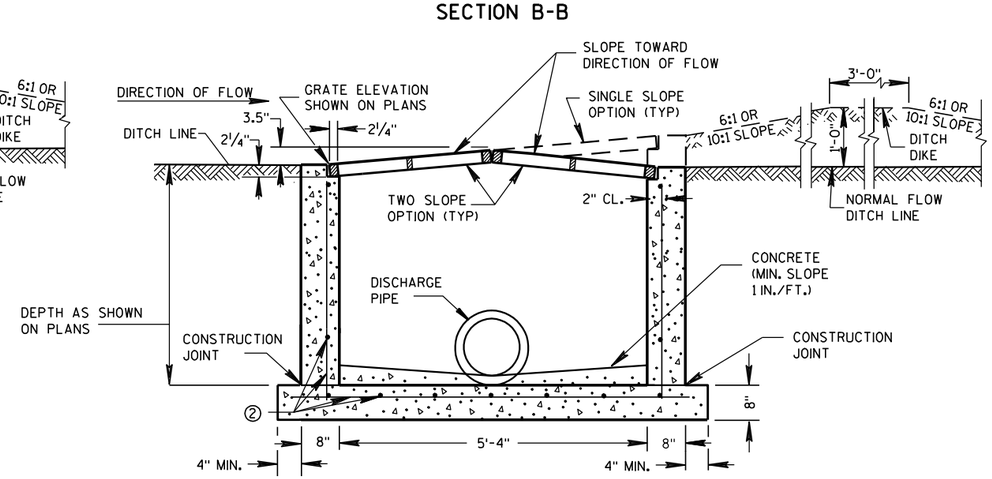
PRECAST REINFORCED CONCRETE SECTION A-A



PRECAST REINFORCED CONCRETE SECTION B-B



REINFORCED CAST-IN-PLACE CONCRETE SECTION A-A



REINFORCED CAST-IN-PLACE CONCRETE SECTION B-B

INLETS MEDIAN 1 GRATE

INLETS MEDIAN 2 GRATE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLETS WHICH MAY INCLUDE PRECAST REINFORCED CONCRETE INLETS, SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL MEDIAN INLETS ARE DESIGNATED ON THE PLANS AS "INLETS, 1G-MS", ETC. THE FIRST NUMBER AND LETTER DESIGNATE THE TYPE OF STRUCTURE, AND THE FOLLOWING LETTERS DESIGNATE THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

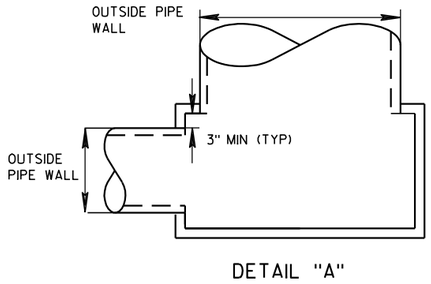
MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

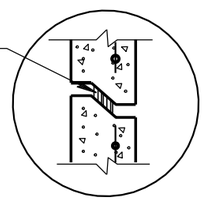
PIPE MATRIX

	MAXIMUM INSIDE PIPE DIAMETER	
INLET SIZE	WIDTH (IN)	LENGTH (IN)
1 GRATE	18	18
2 GRATE	18	42



DETAIL "A"

JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



DETAIL "B"

INLETS MEDIAN 1 AND 2 GRATE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

6/5/2012
DATE

/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

6

S.D.D. 8 C 8-1

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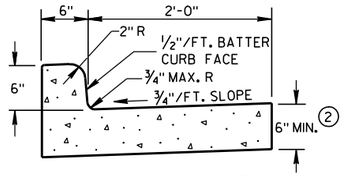
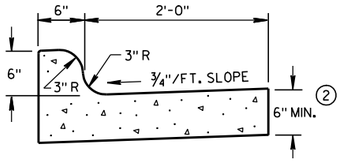
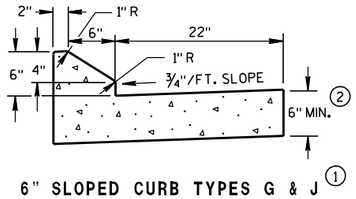
S.D.D. 8 C 8-1



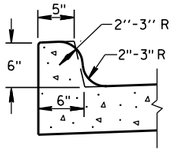
TYPES A & D (1)



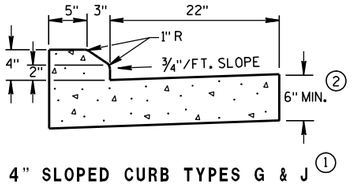
TYPES K & L (1)



6" SLOPED CURB TYPES G & J (1)

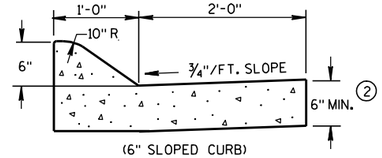


OPTIONAL CURB SHAPE FOR TYPES K & L (1)

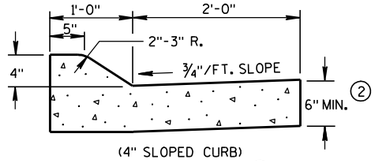


4" SLOPED CURB TYPES G & J (1)

CONCRETE CURB & GUTTER 30"

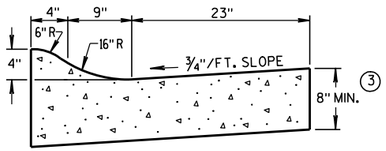


(6" SLOPED CURB)



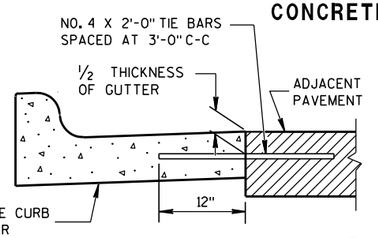
(4" SLOPED CURB)

TYPES A & D (1)

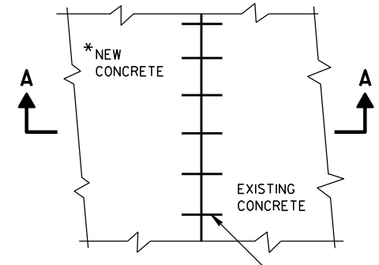


4" SLOPED CURB TYPES R & T (1) (4)

CONCRETE CURB & GUTTER 36"

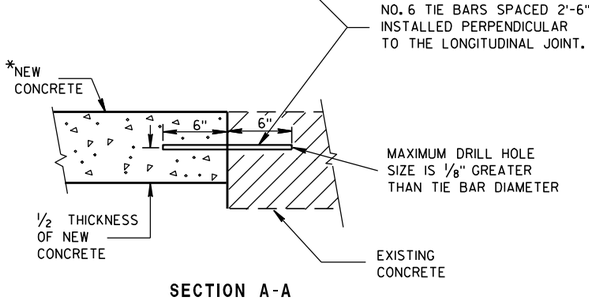


TYPICAL TIE BAR LOCATION (1)



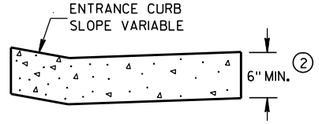
*NEW CURB & GUTTER, SURFACE DRAINS, CONCRETE PAVEMENT OR OTHER NEW CONCRETE.

PLAN VIEW

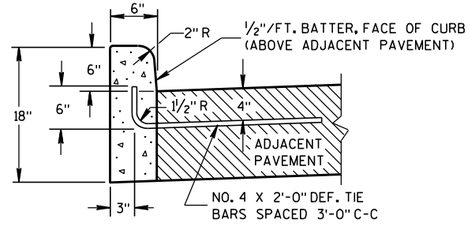


SECTION A-A

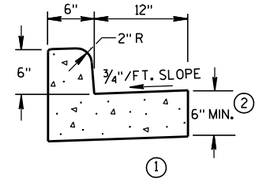
TIE BARS DRILLED INTO EXISTING PAVEMENT



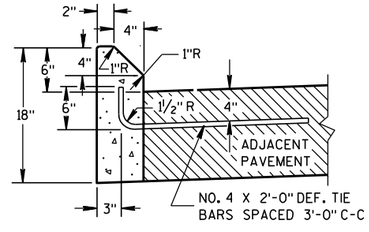
DRIVEWAY ENTRANCE CURB
(WHEN DIRECTED BY THE ENGINEER)



(1)
TYPES A & D

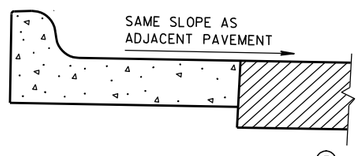


(1)
TYPES A & D
CONCRETE CURB & GUTTER 18"



(1)
TYPES G & J

CONCRETE CURB



REVERSE SLOPE GUTTER (5)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE. A LONGITUDINAL CONSTRUCTION JOINT IS NOT REQUIRED WITH INTEGRAL CURB AND GUTTER.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

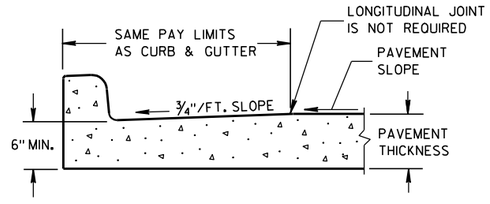
(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K AND R.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

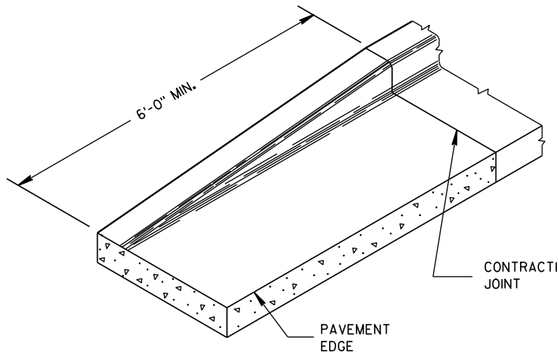
(3) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(4) THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

(5) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB & GUTTER



END SECTION CURB & GUTTER

CONCRETE CURB, CONCRETE CURB & GUTTER AND TIES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

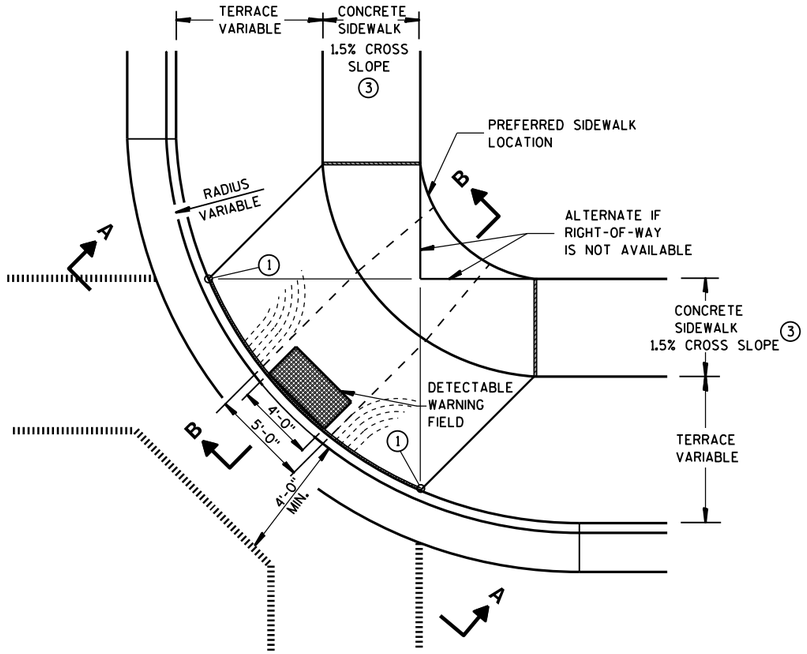
APPROVED
9/4/08 DATE
/S/ Jerry H. [illegible]
ROADWAY STANDARDS ENGINEER
FHWA

6

S.D.D. 8 D 1-17

6

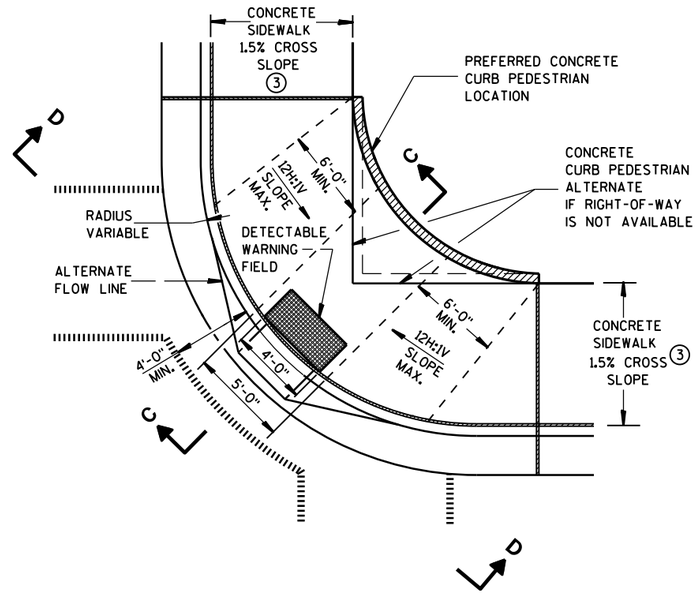
S.D.D. 8 D 1-17



PLAN VIEW

TYPE 1 RAMP

(CENTER OF CORNER RADIUS)



PLAN VIEW

TYPE 1-A RAMP

(NO TERRACE)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

RAMPS SHALL BE BUILT AT 12H:1V OR FLATTER. WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAL FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD".

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

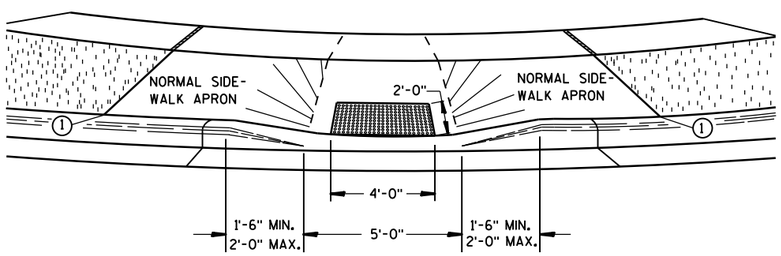
① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB.

② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE.

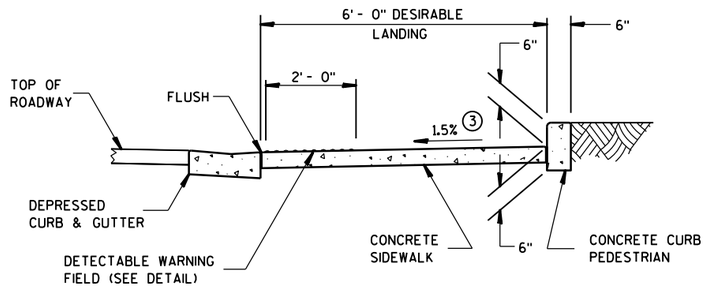
③ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

LEGEND

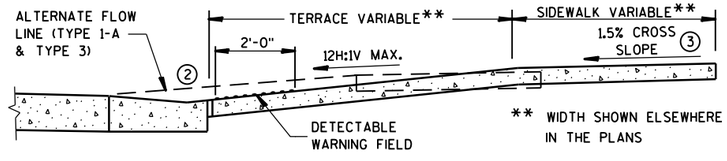
- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



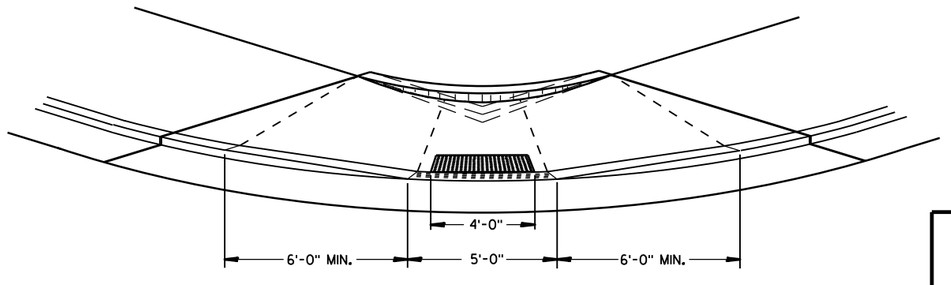
VIEW A-A



SECTION C-C



SECTION B-B



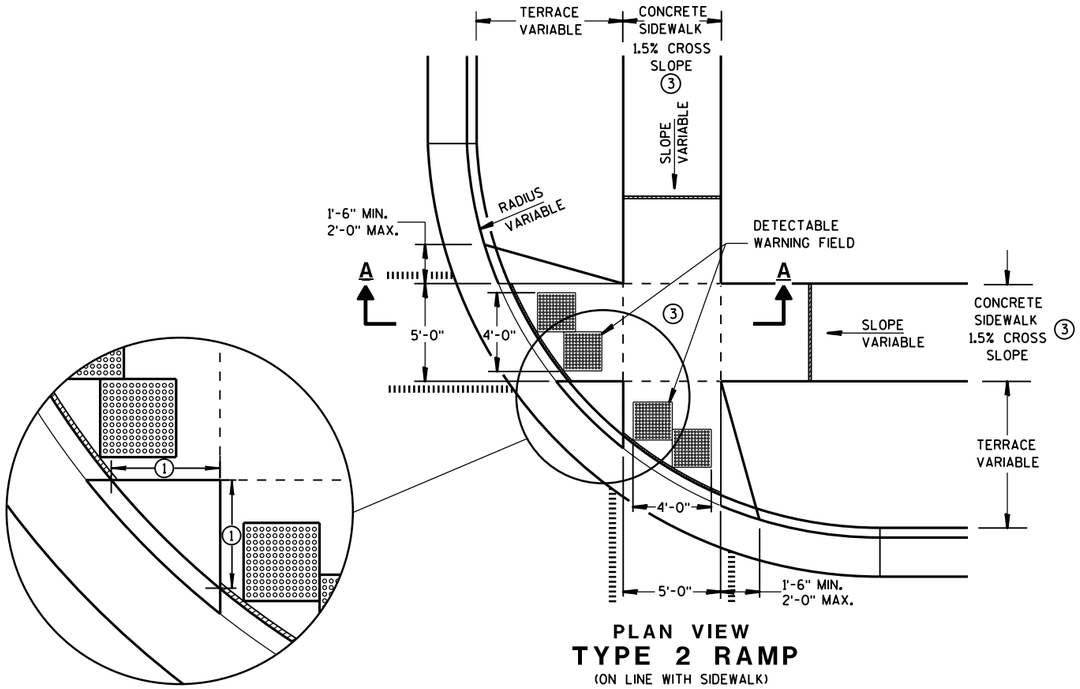
VIEW D-D

CURB RAMPS TYPES 1 AND 1-A

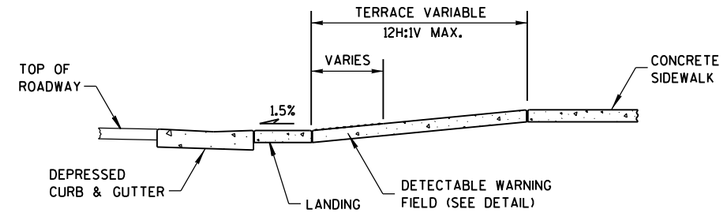
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

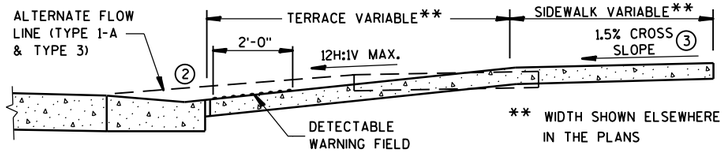
S.D.D. 8 D 5-15a



PLAN VIEW
TYPE 2 RAMP
(ON LINE WITH SIDEWALK)



SECTION A-A



SECTION B-B

GENERAL NOTES

USE THE TYPE 3 RAMP ONLY WHEN A TYPE 1 OR TYPE 2 CANNOT BE ACHIEVED BECAUSE OF FIELD CONDITIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

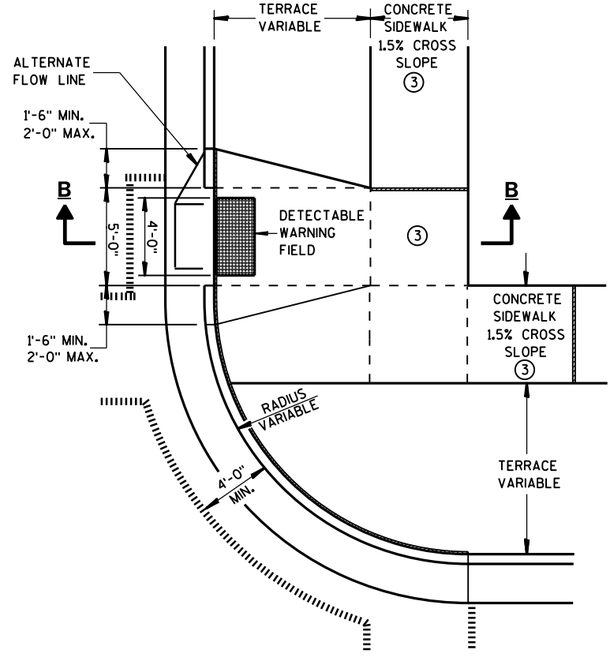
① WHEN THIS DISTANCE IS LESS THAN 6'-0" IT MAY BE DIFFICULT TO ACHIEVE A 12H:1V SLOPE, OR FLATTER, ON THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 12H:1V SLOPE, OR FLATTER, ON RAMP. 2" MINIMUM CURB HEIGHT.

② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE.

③ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



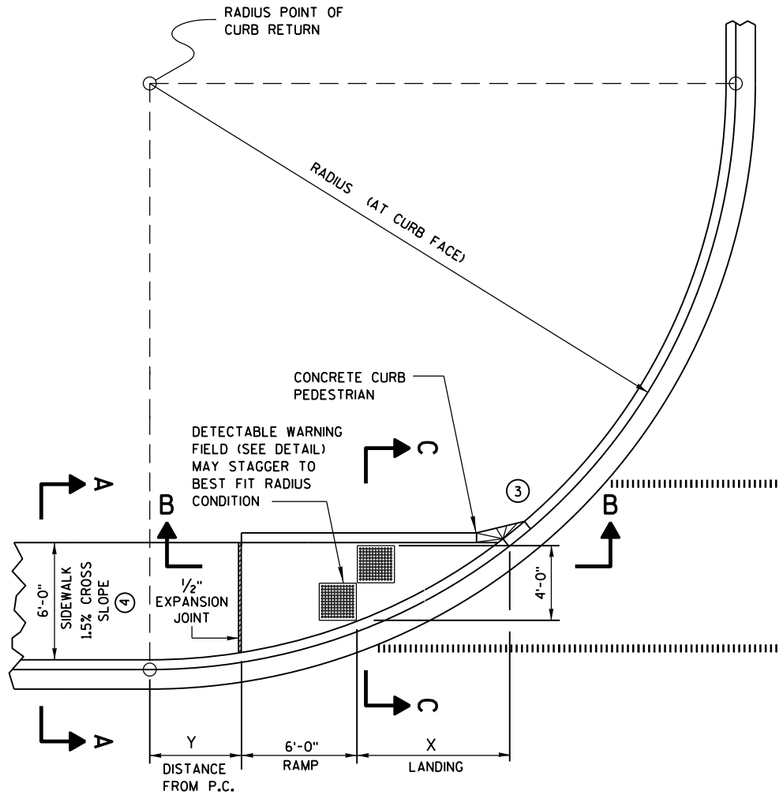
PLAN VIEW
TYPE 3 RAMP
(OUTSIDE OF CROSSWALK AREA)

CURB RAMPS TYPES 2 AND 3

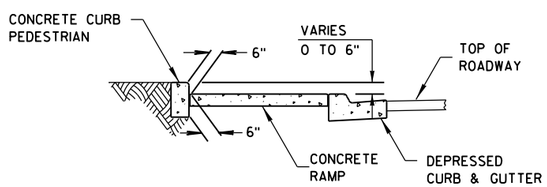
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

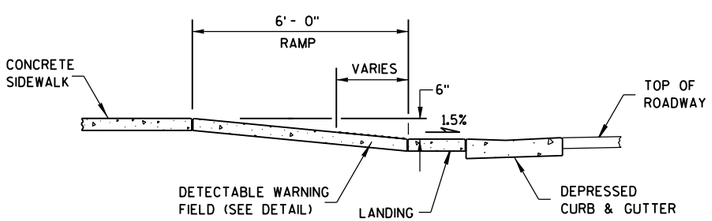
S.D.D. 8 D 5-15b



CURB RAMP TYPE 4A
PLAN VIEW



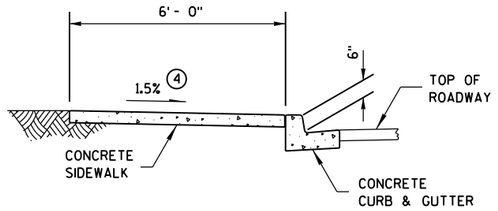
SECTION C-C FOR TYPE 4A



SECTION B-B FOR TYPE 4A

RADIUS (AT CURB FACE)	X	Y
20 FEET	6'-1¾"	2'-7¼"
30 FEET	7'-11¾"	4'-8¼"
40 FEET	9'-5¼"	6'-5"
50 FEET	10'-8¾"	7'-11¼"
60 FEET	11'-10¼"	9'-3½"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A-A FOR TYPE 4A

GENERAL NOTES

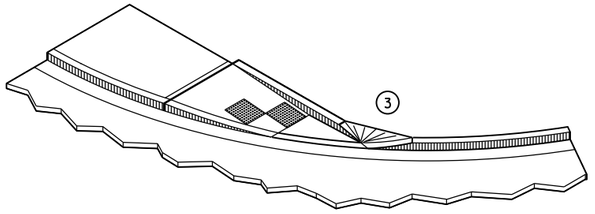
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

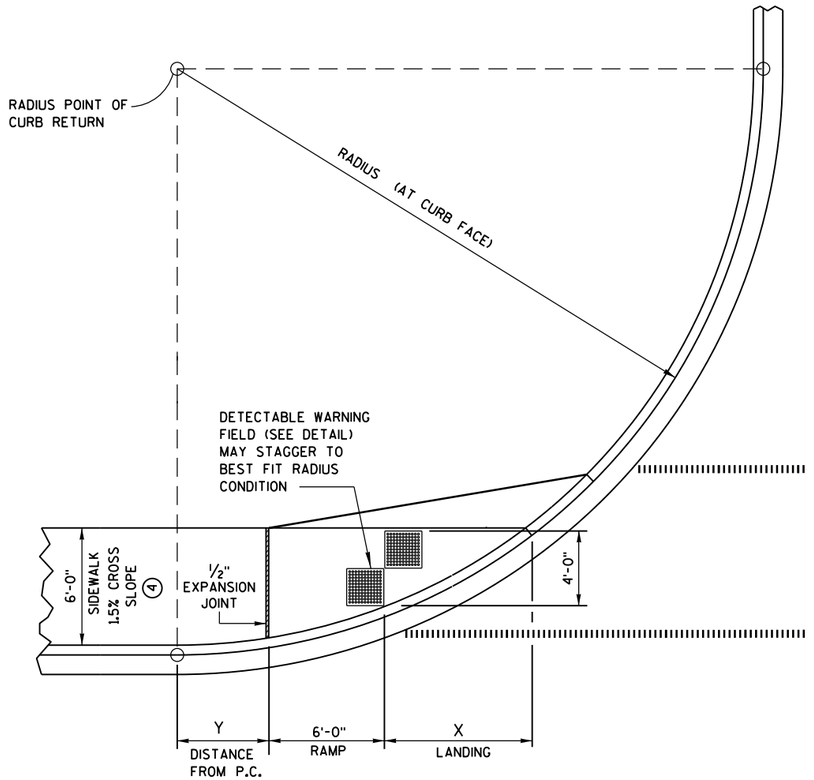
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

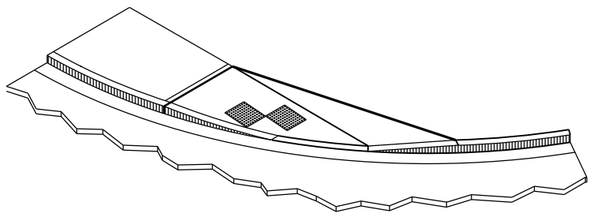
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



ISOMETRIC VIEW FOR TYPE 4A



CURB RAMP TYPE 4A1
PLAN VIEW



ISOMETRIC VIEW FOR TYPE 4A1

LEGEND

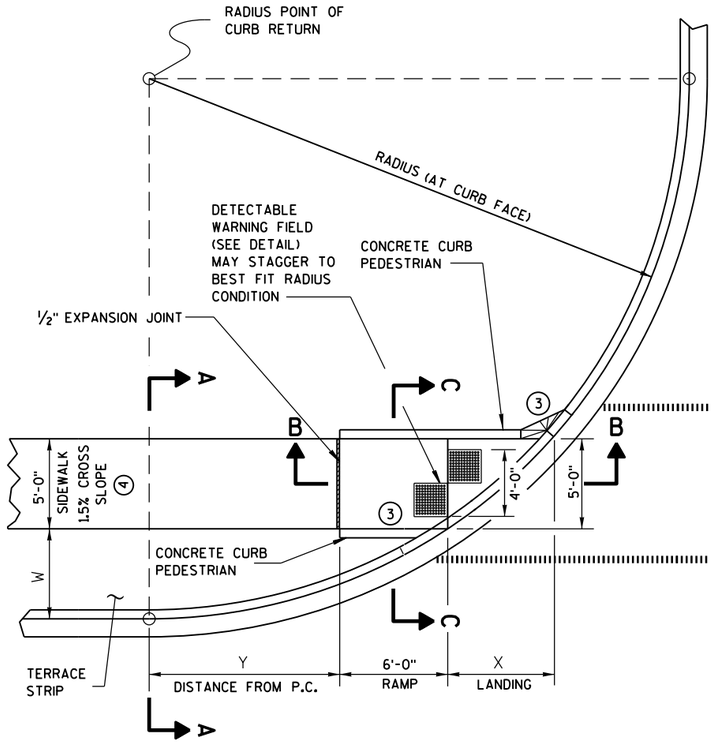
- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPES 4A AND 4A1

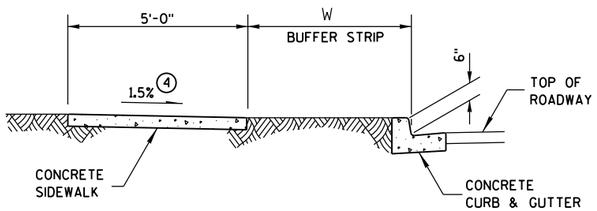
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

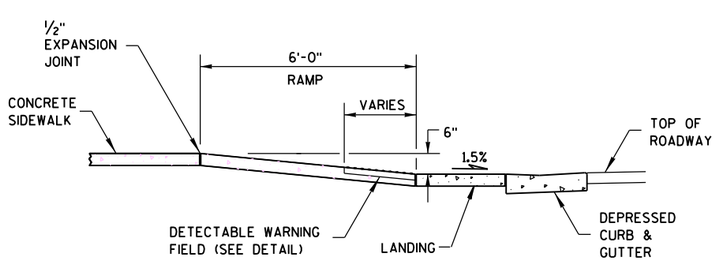
S.D.D. 8 D 5-15c



CURB RAMP TYPE 4B
PLAN VIEW



SECTION A-A FOR TYPE 4B



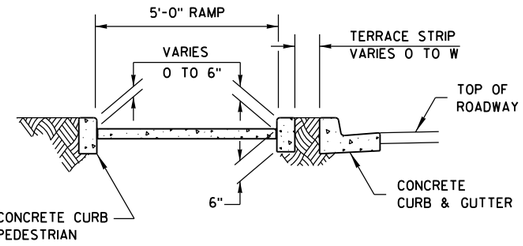
SECTION B-B FOR TYPE 4B

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

RADIUS (AT CURB FACE)	W = 3'-0"		W = 4'-0"		W = 5'-0"		W = 6'-0"		W = 7'-0"	
	X	Y	X	Y	X	Y	X	Y	X	Y
20 FEET	5'-5½"	4'-6½"	4'-8½"	6'-0"	4'-1"	7'-2¾"	3'-7"	8'-3½"	3'-1½"	9'-2½"
30 FEET	7'-3¾"	7'-1"	6'-5½"	8'-11½"	5'-9¼"	10'-7"	5'-2½"	12'-0"	4'-8¾"	13'-3¼"
40 FEET	8'-9½"	9'-2½"	7'-10"	11'-5¼"	7'-1"	13'-4½"	6'-5¾"	15'-¾"	5'-11½"	16'-7¼"
50 FEET	10'-¾"	11'-¾"	9'-¼"	13'-7¼"	8'-2½"	15'-9½"	7'-6½"	17'-9"	6'-11¾"	19'-6¼"
60 FEET	11'-2½"	12'-8¾"	10'-¾"	15'-6½"	9'-2¼"	17'-11¾"	8'-5¾"	20'-1¾"	7'-10½"	22'-1½"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION C-C FOR TYPE 4B

GENERAL NOTES

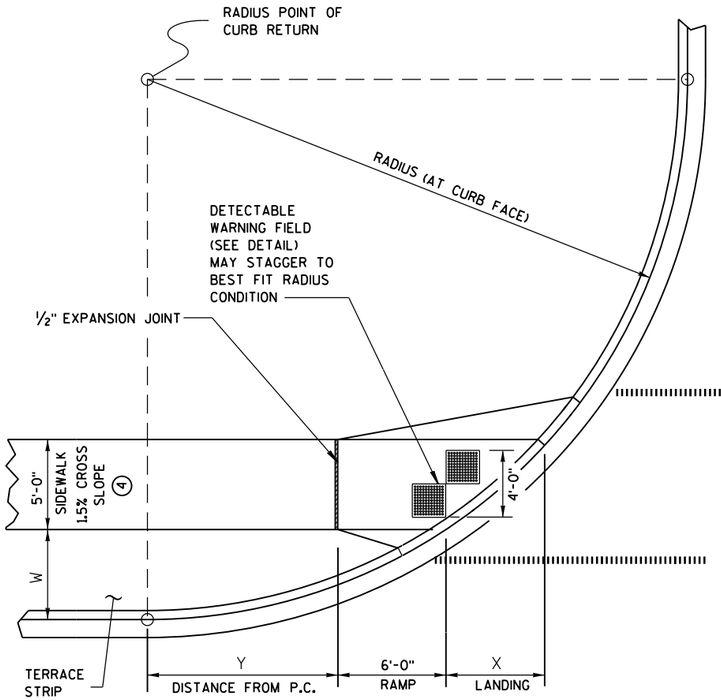
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

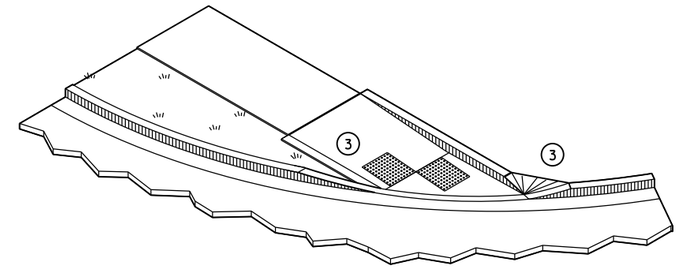
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

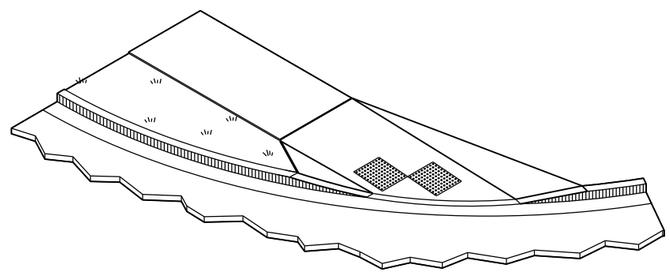
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



CURB RAMP TYPE 4B1
PLAN VIEW



ISOMETRIC VIEW FOR TYPE 4B



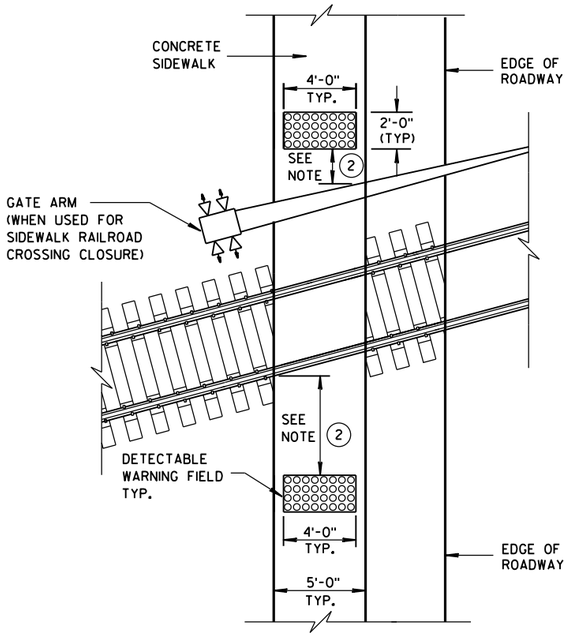
ISOMETRIC VIEW FOR TYPE 4B1

CURB RAMPS
TYPE 4B AND 4B1

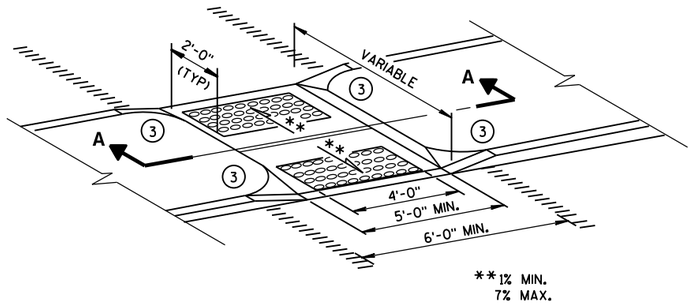
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 8 D 5-15d



TYPE 8
DETECTABLE WARNINGS AT RAILROAD CROSSING



MEDIAN ISLAND
NON-ELEVATED CROSSING
TYPE 5

GENERAL NOTES

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

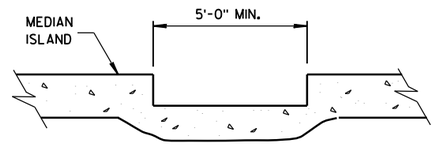
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(1) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

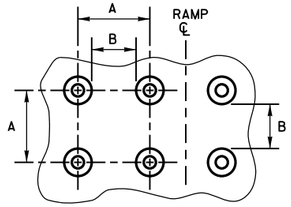
(2) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ± 0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



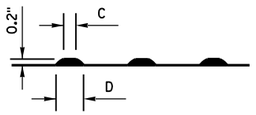
SECTION A-A



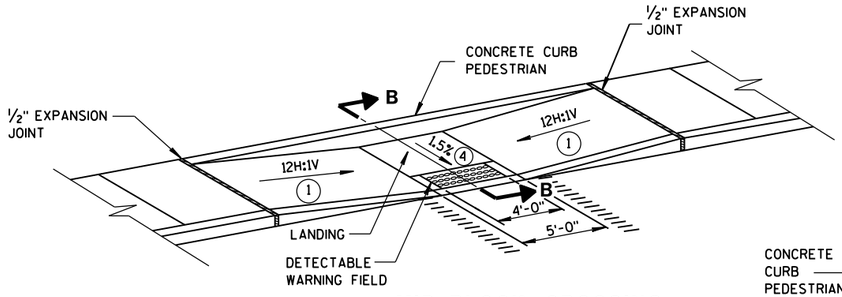
PLAN VIEW

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

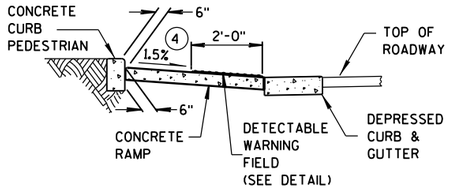
* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.



ELEVATION VIEW

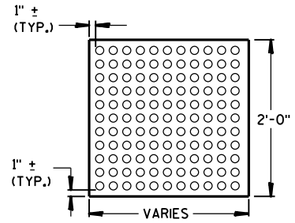


MID-BLOCK CROSSING
TYPE 7A



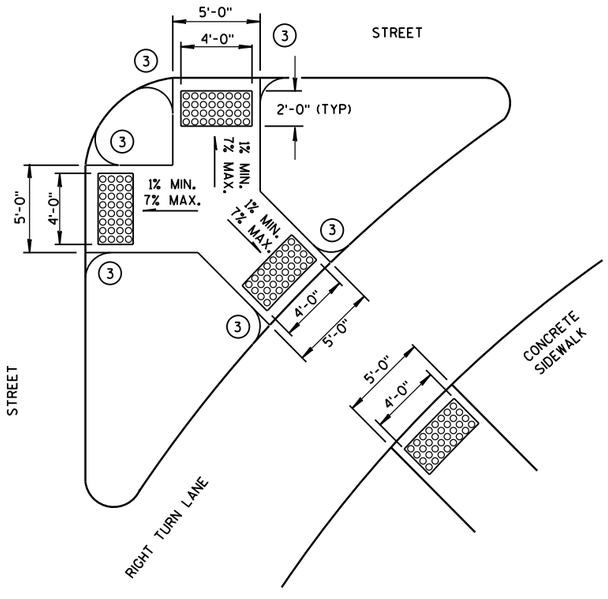
SECTION B-B

TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL

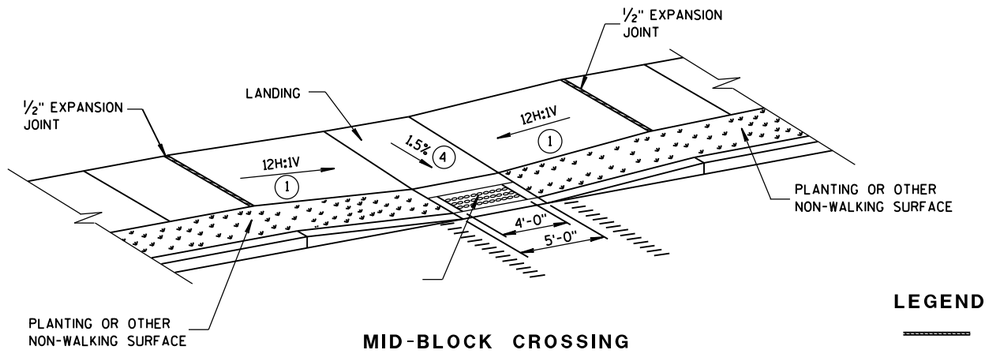


PLAN VIEW

DETECTABLE WARNING FIELD (TYPICAL)



TYPE 6
DETECTABLE WARNING AT ISLANDS



MID-BLOCK CROSSING
TYPE 7B

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

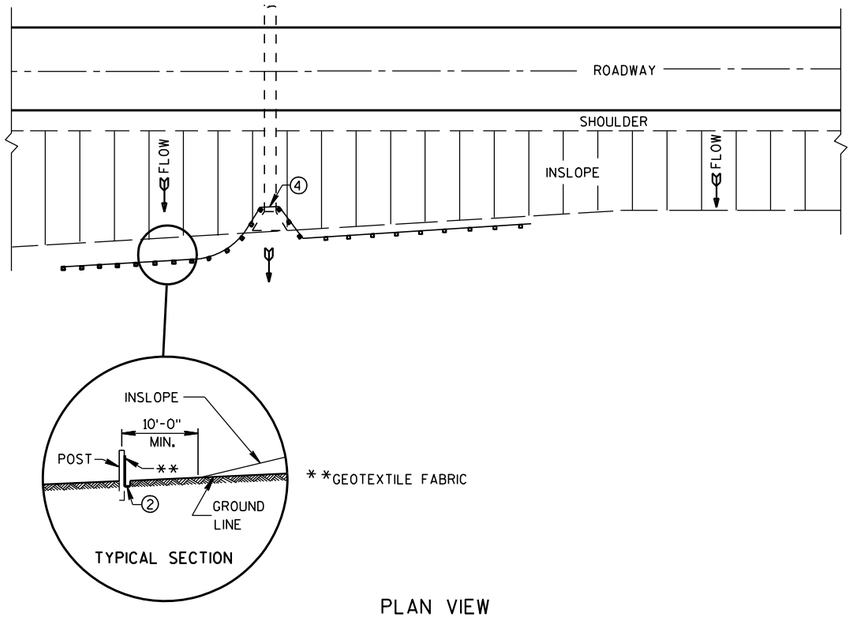
CURB RAMPS
TYPES 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
2-6-2013 DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS [illegible] ENGINEER
FHWA

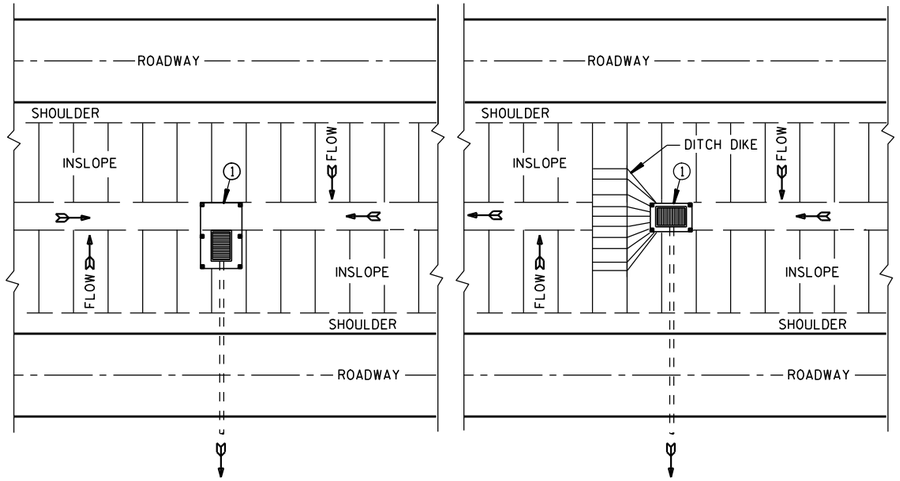
6

S.D.D. 8 D 5-15e



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

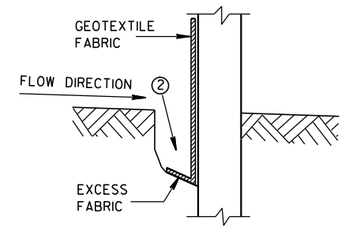
① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.

② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.

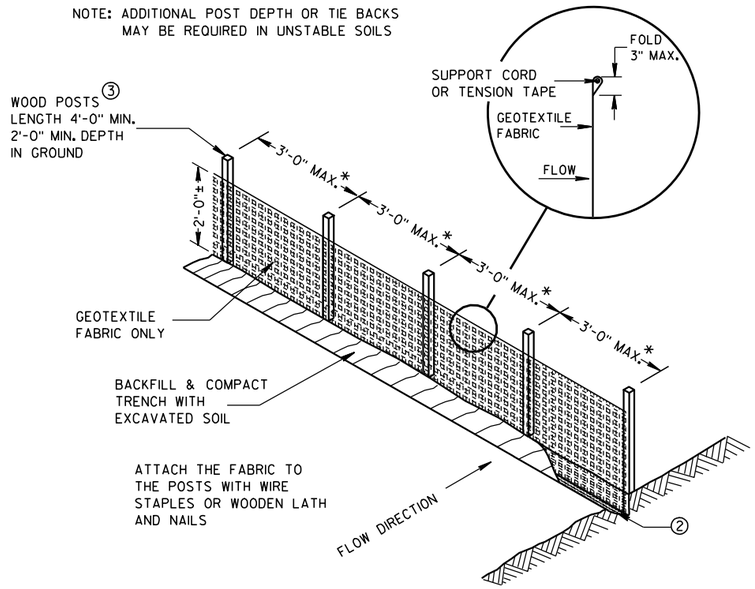
③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.

④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.

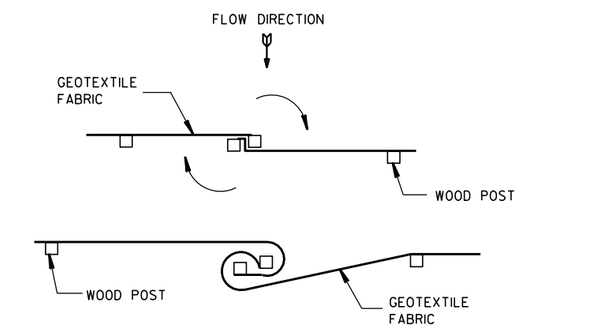
⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



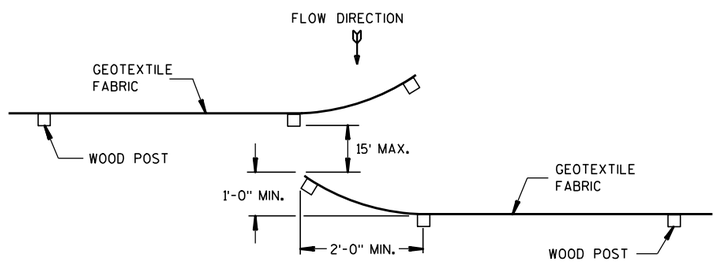
TRENCH DETAIL



SILT FENCE

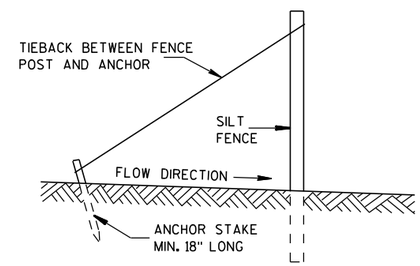


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

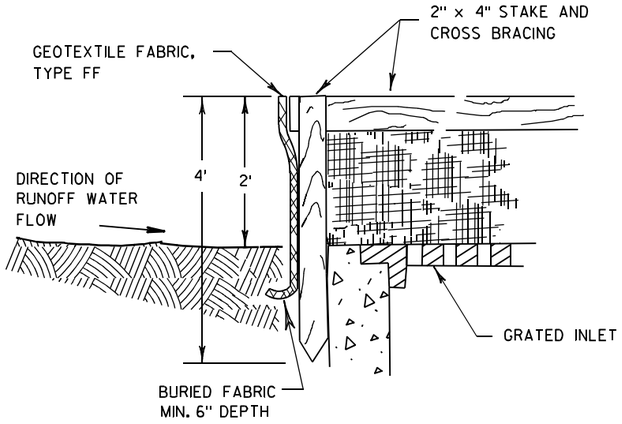
APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

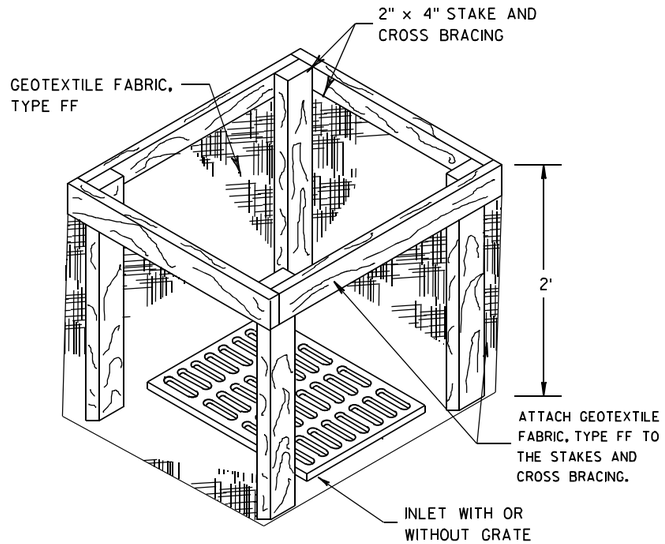
6

S.D.D. 8 E 9-6

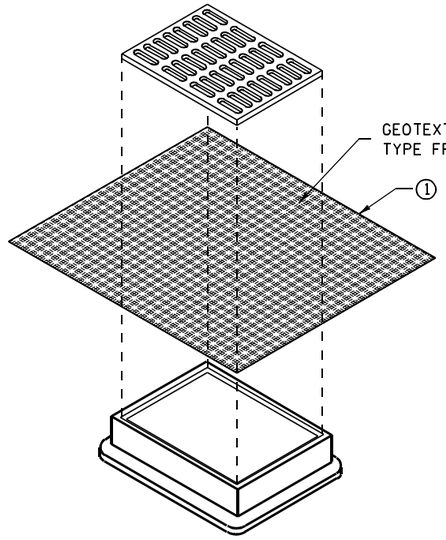
6

S.D.D. 8 E 9-6



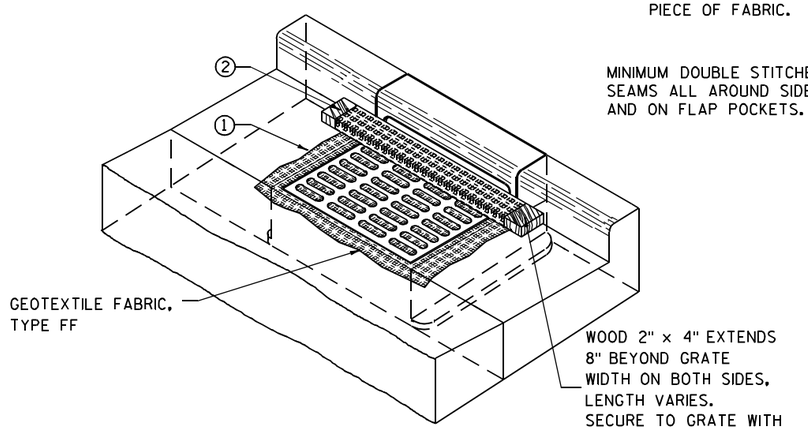


INLET PROTECTION, TYPE A

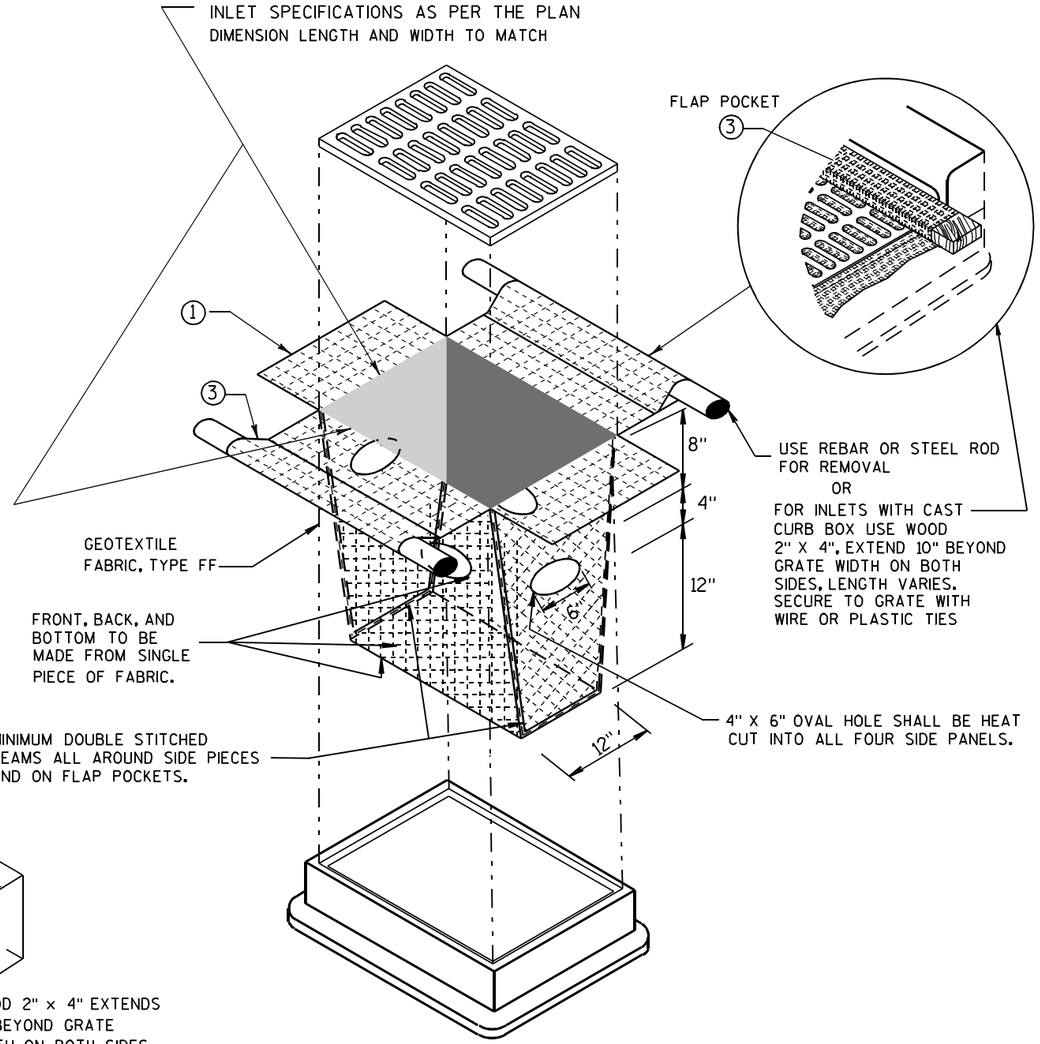


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

10/16/02
DATE

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

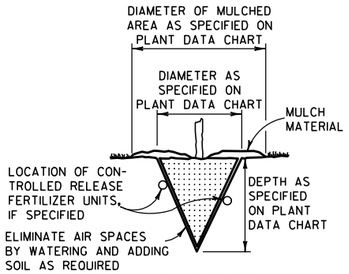
FHWA

6

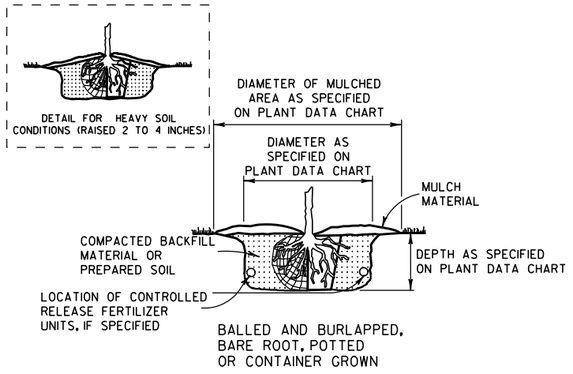
S.D.D. 8 E 10-2

6

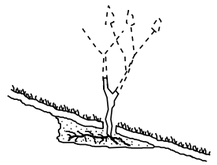
S.D.D. 8 E 10-2



MACHINE MOVED

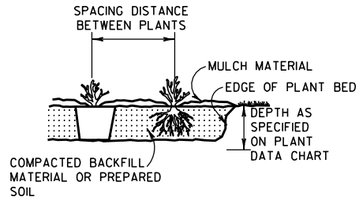


BALLED AND BURLAPPED, BARE ROOT, POTTED OR CONTAINER GROWN



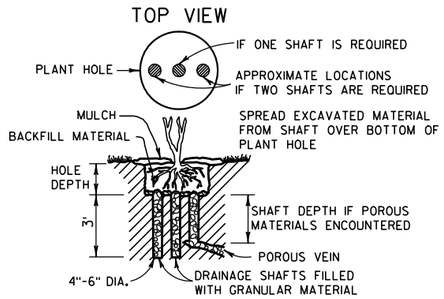
ACCOMMODATE ROOTS (SMOOTH AND STAGHORN SUMAC)

NOTE:
1) ENGINEER SHALL REQUIRE 3 SLITS IN POT TO SPEED DETERIORATION
2) METAL, PLASTIC OR OTHER NONDEGRADABLE POTS SHALL BE REMOVED PRIOR TO PLANTING



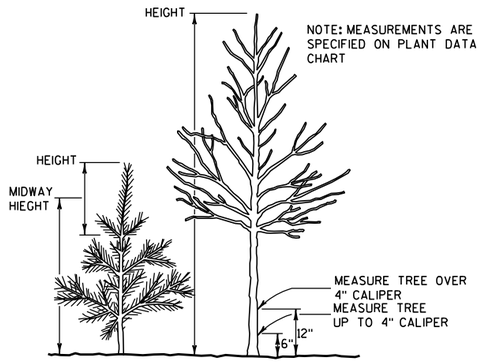
PREPARED BED (BARE ROOT AND POTTED)

PLANTING

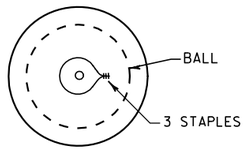


NOTE:
DRAINAGE SHAFT AS SPECIFIED ON PLANT DATA CHART

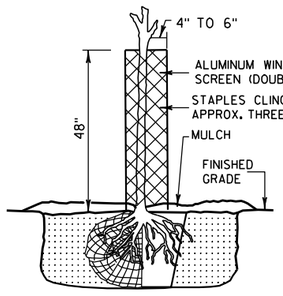
DRAINING



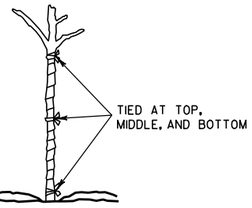
MEASURING



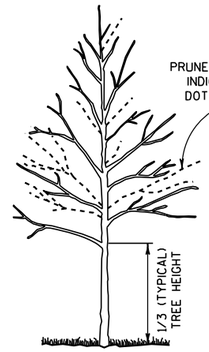
TOP VIEW



RODENT PROTECTION



WRAPPING



TALL DECIDUOUS TREES (ASH, MAPLE, LINDEN, OAK)

SINGLE STEMMED LOW DECIDUOUS TREES (CRABS AND OTHER SINGLE STEM TYPE)

MULTI-STEMMED LOW DECIDUOUS TREES OR LARGE SHRUBS (HAWTHORN, SERVICEBERRY)

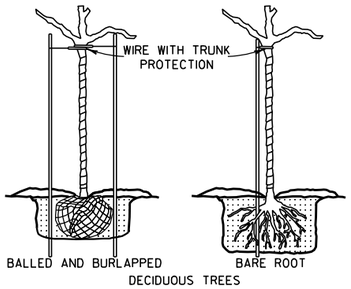
NOTE: WHEN PRUNING, PRESERVE CHARACTER AND SHAPE OF TREE. AVOID LEAVING STUBS - REMOVE BRANCH OR TWIG BACK TO THE NEAREST CROTCH
1) PRUNE TO REMOVE DEAD AND BROKEN BRANCHES
2) PRUNE TO REMOVE BRANCHES THAT TOUCH OR ARE TOO CLOSE TO OTHER BRANCHES

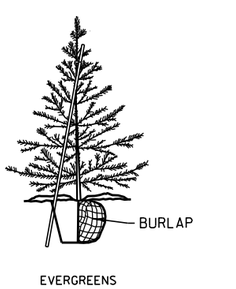
SHRUBS (GRAY DOGWOOD ARROWWOOD VIBURNUM)

SUMAC

TREE TYPE EVERGREENS (PINE, SPRUCE, FIR) EVERGREENS USUALLY ARE NOT PRUNED

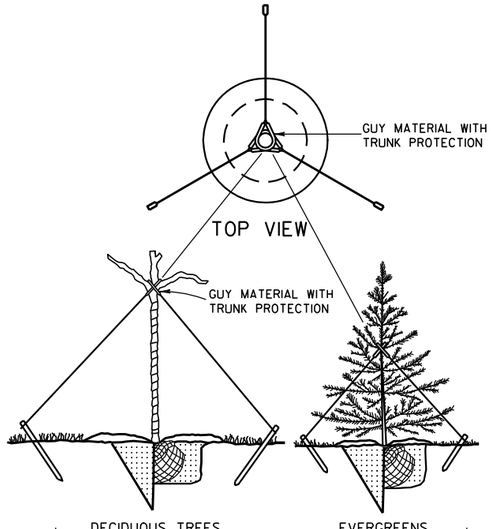
PRUNING



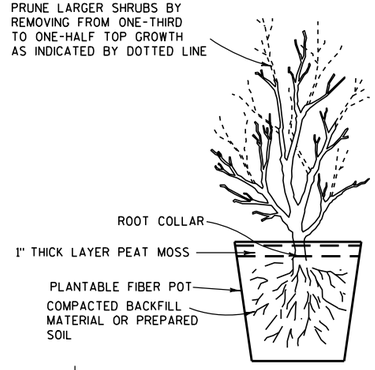


NOTE: BRACING STAKE
1) SHALL BE DRIVEN INTO THE GROUND AS CLOSE TO THE TREE AS POSSIBLE WITHOUT DAMAGING THE BRANCHES.
2) MAY BE DRIVEN AT SUCH AN ANGLE THAT IT DOES NOT PENETRATE THE BALL OR POT.
3) SHALL NOT PROTRUDE ABOVE THE TOP OF THE TREE; AND
4) SHALL HAVE A HOLE NEAR THE TOP TO HOLD THE WIRE IN PLACE.

BRACING



GUYING



POTTING

NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

BRACING, WRAPPING, GUYING, RODENT PROTECTION, FERTILIZER AND MULCH SHALL BE USED ONLY WHEN SPECIFIED ON THE PLANT DATA CHART (PART OF PLAN) OR SPECIAL PROVISIONS.

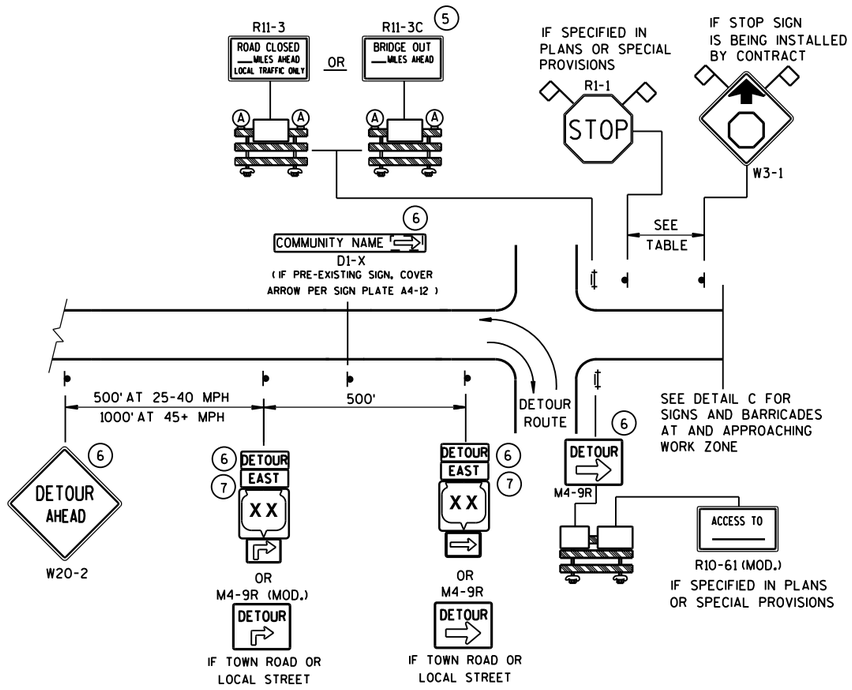
TREE PLANTING DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4/11/94
DATE
/S/ Rory L. Rhinesmith
CHIEF METHODS DEVELOPMENT ENGINEER
FHWA

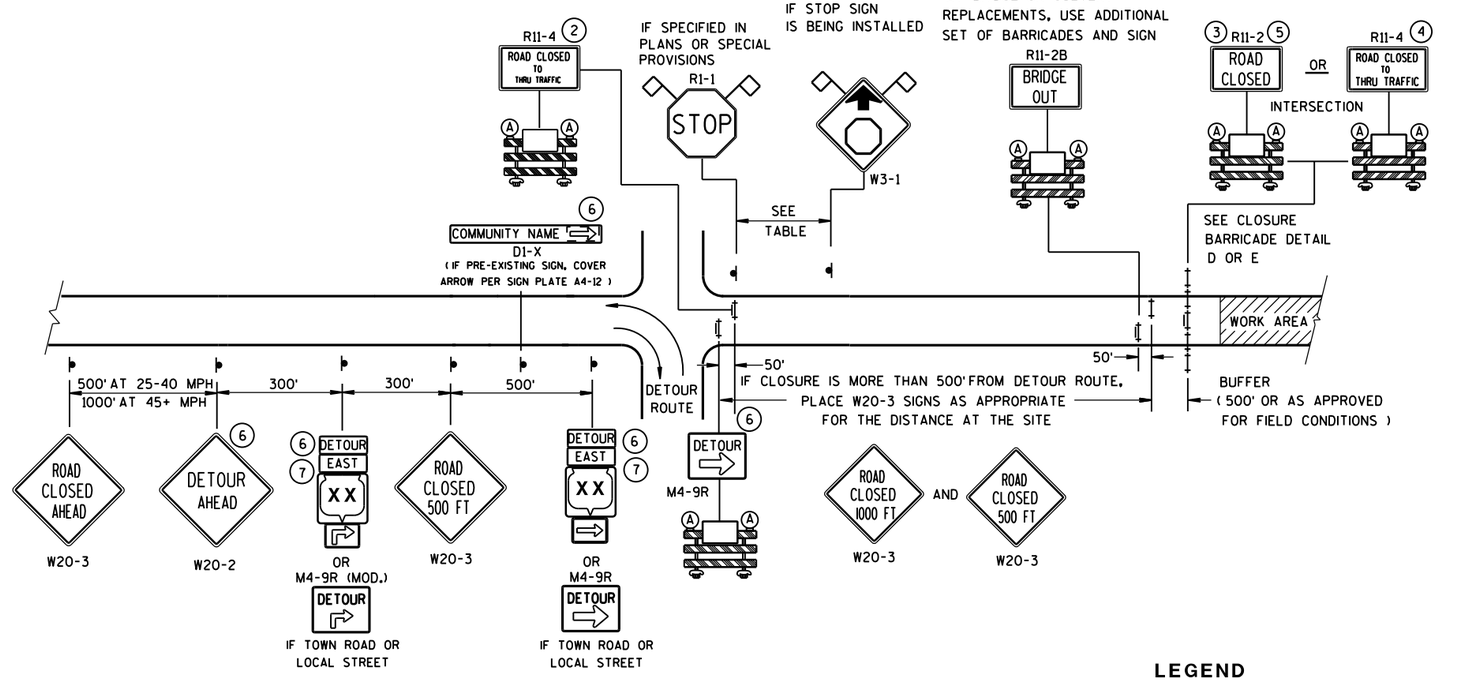
6

S.D.D. 14 A 2-1



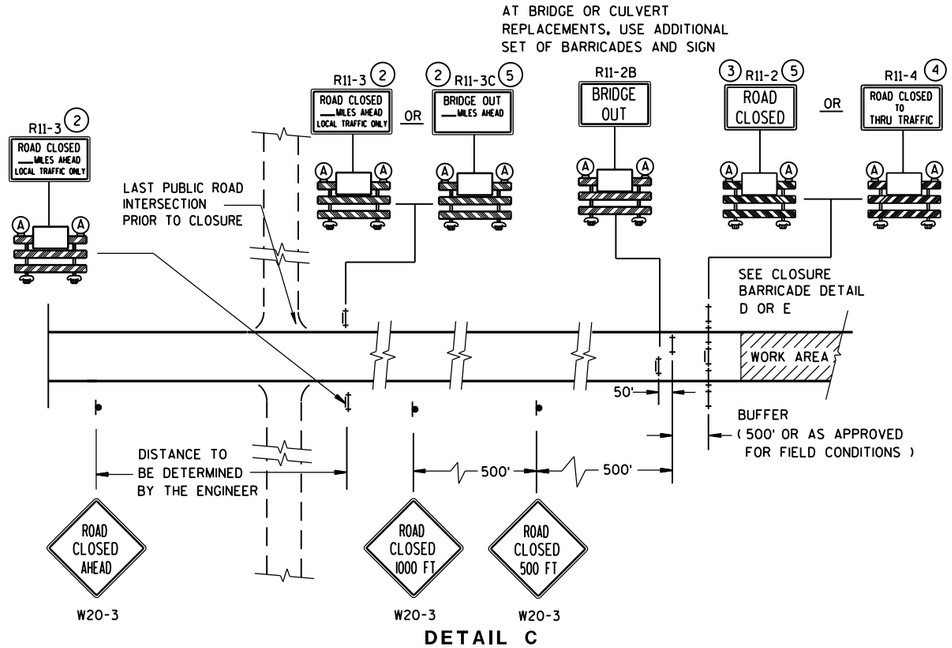
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "**b**"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- DETOUR / EAST — M4-8 / M3-X
- XX — M1-4; COUNTY XX — M1-5A; XX — M1-6
- M05-1 OR M06-1
- FLAGS, 16" X 16" MIN., (ORANGE)

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

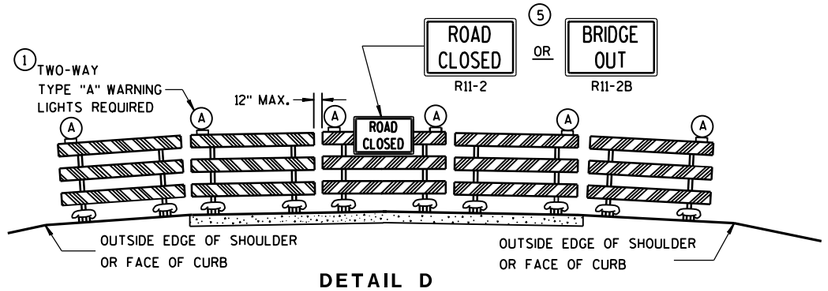
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

8/2013 DATE — /S/ Travis Feltes STATE TRAFFIC ENGINEER — DESIGN

FHWA

6

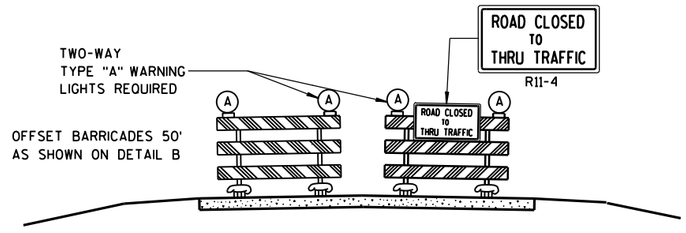
S.D.D. 15 C 2-5a



DETAIL D

ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW



DETAIL E

LANE CLOSURE BARRICADE DETAIL

APPROACH VIEW

SEE SDD 15C2-SHEET "**a**" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

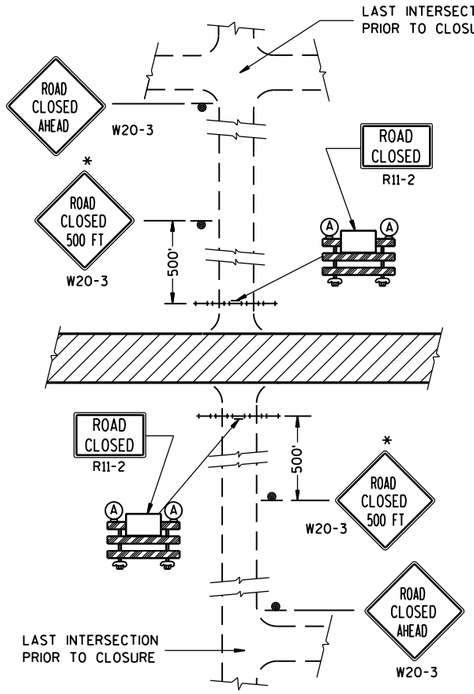
8/2013 DATE

/S/ Travis Feltes
STATE TRAFFIC ENGINEER

FHWA

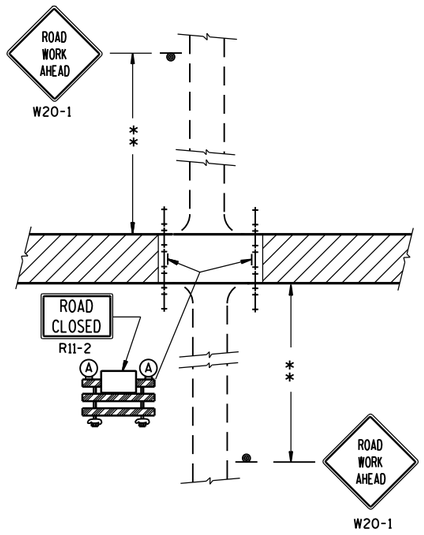
6

S.D.D. 15 C 2-5b



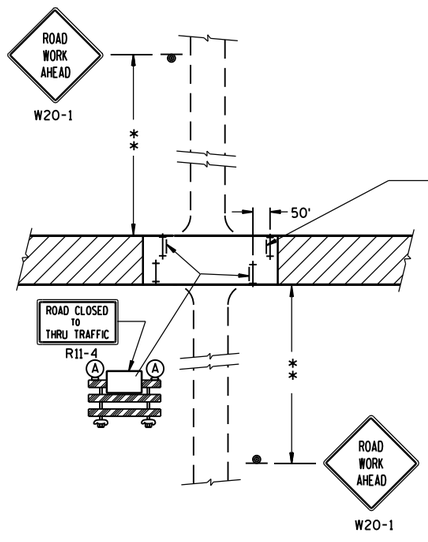
DETAIL 1

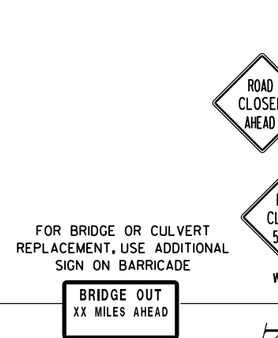
(NO ACCESS TO PROJECT)



DETAIL 2

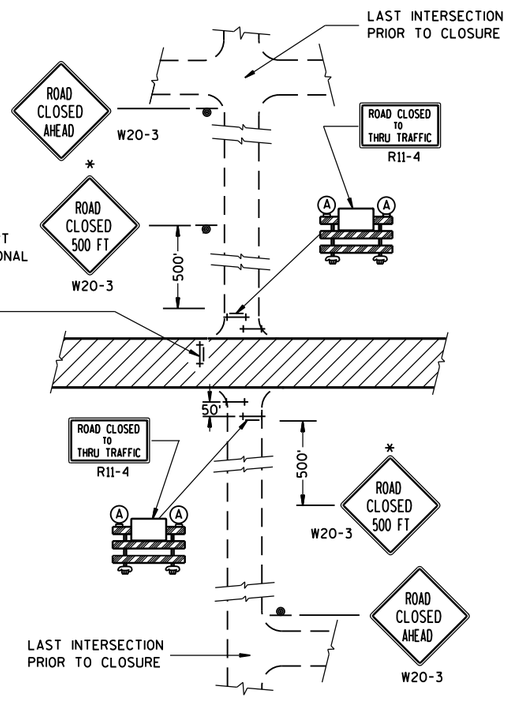
(PUBLIC CROSS-TRAFFIC MAINTAINED. NO ACCESS TO PROJECT).





DETAIL 3

(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).



DETAIL 4

(CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

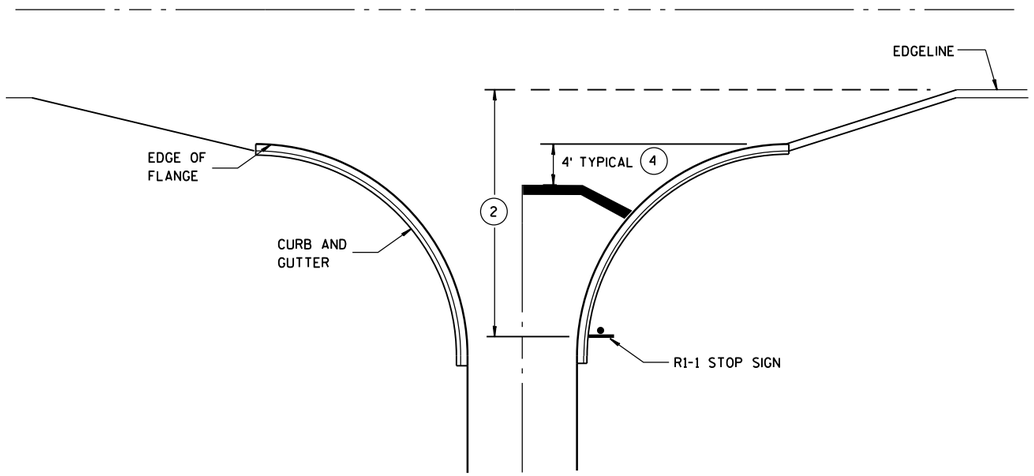
APPROVED
8/2013 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

6

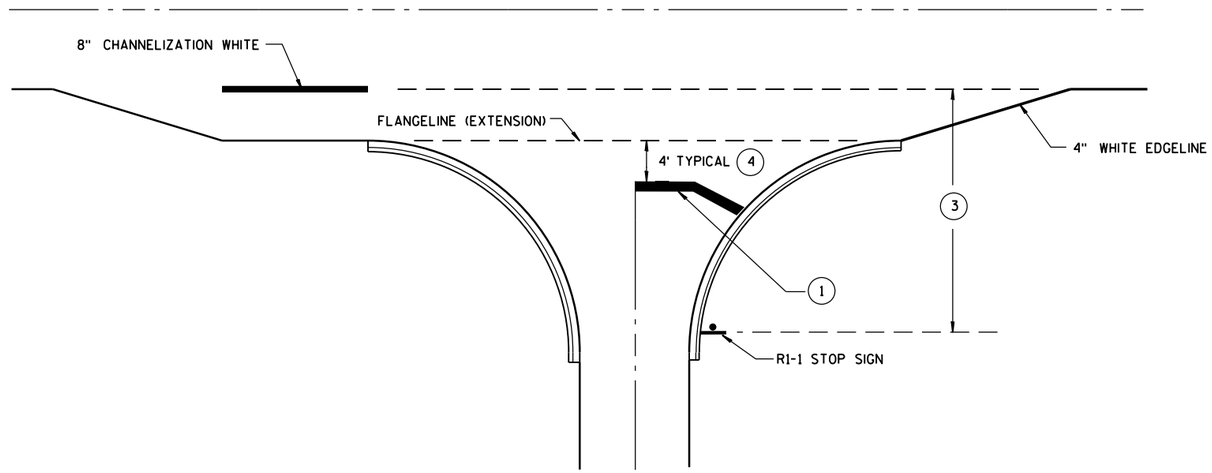
S.D.D. 15 C 3-2

6

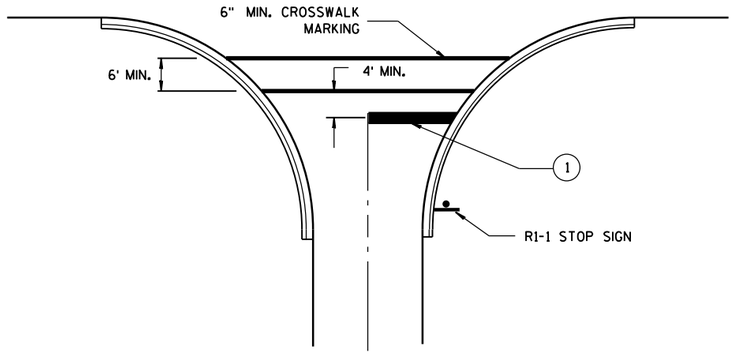
S.D.D. 15 C 3-2



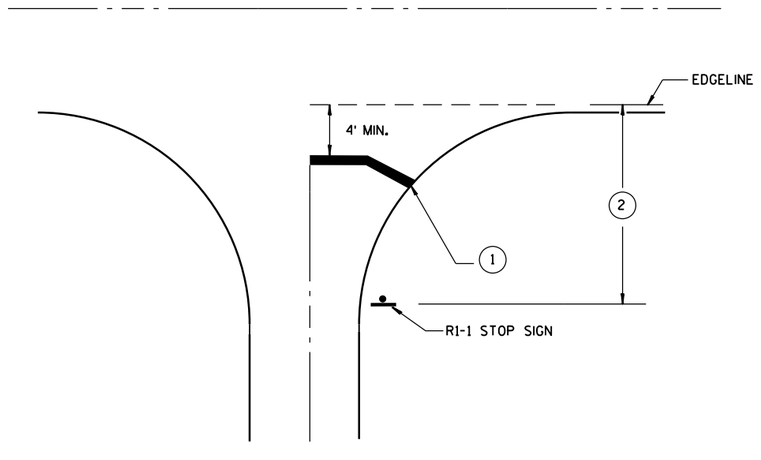
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
3. IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
4. MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES.

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

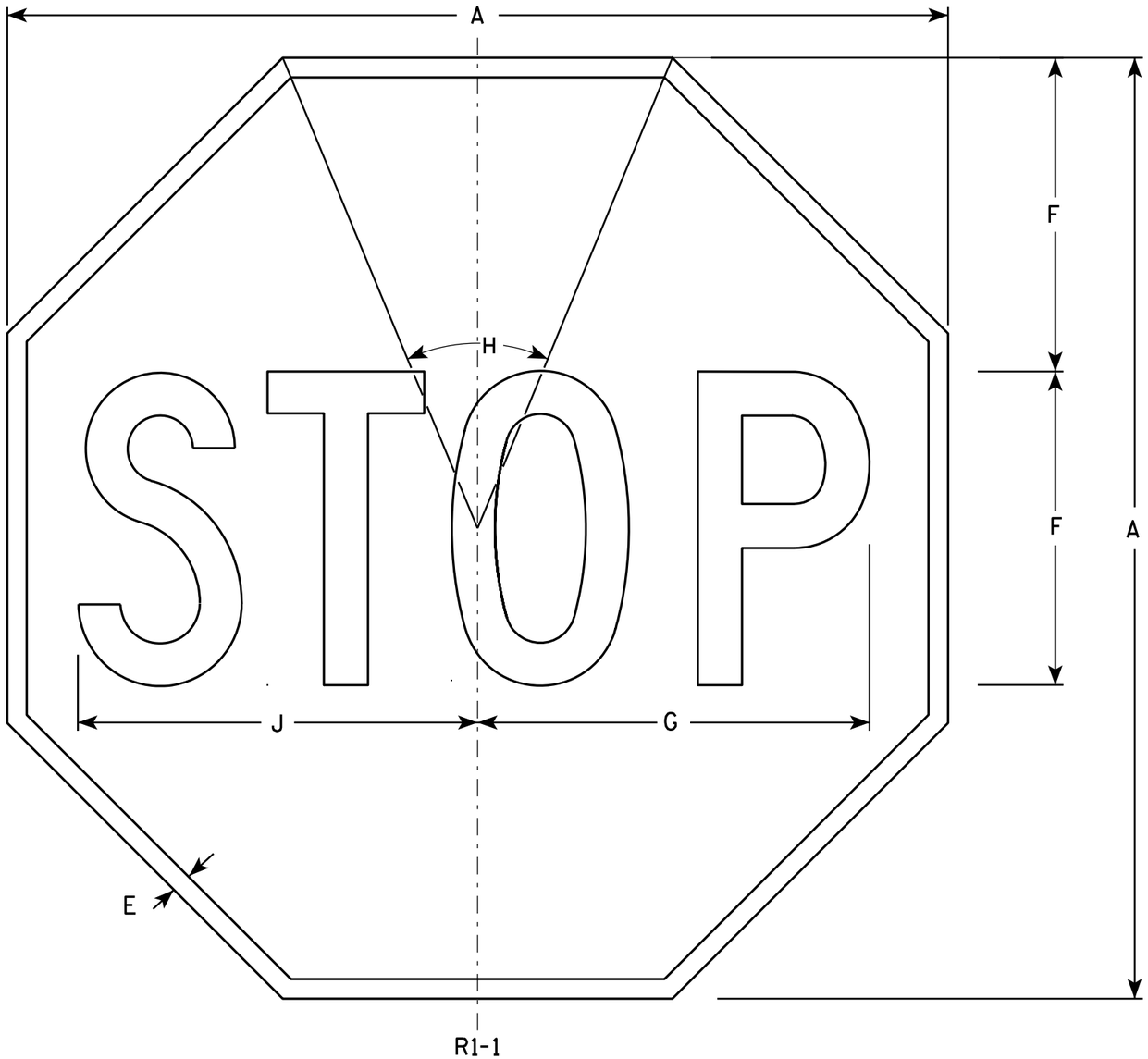
4/30/2013
DATE

/S/ Travis Feltes
STATE TRAFFIC E

FHWA

6

S.D.D. 15 C 33-1



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24				3/8	8	10	45°		10 1/4																	3.31
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

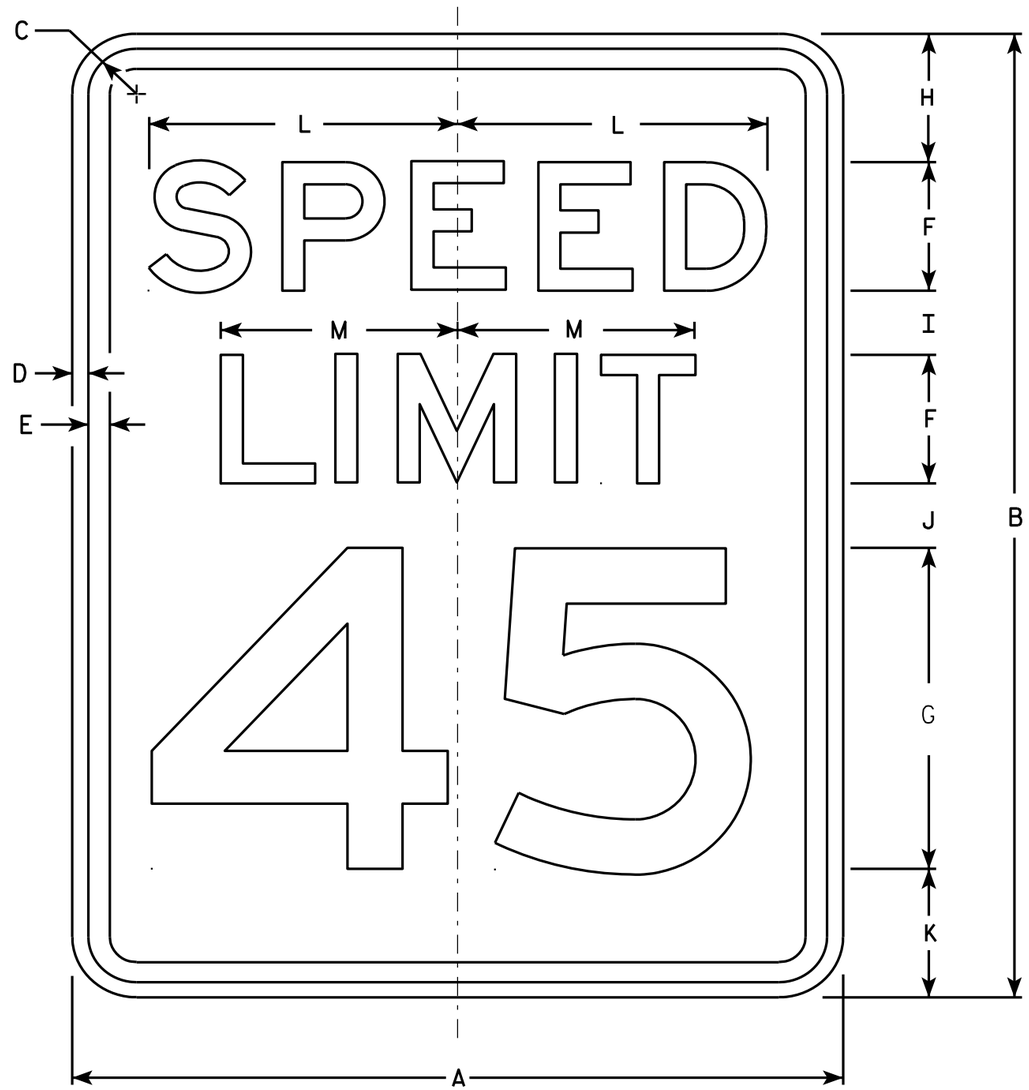
STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1.12

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 50	E



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN
R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

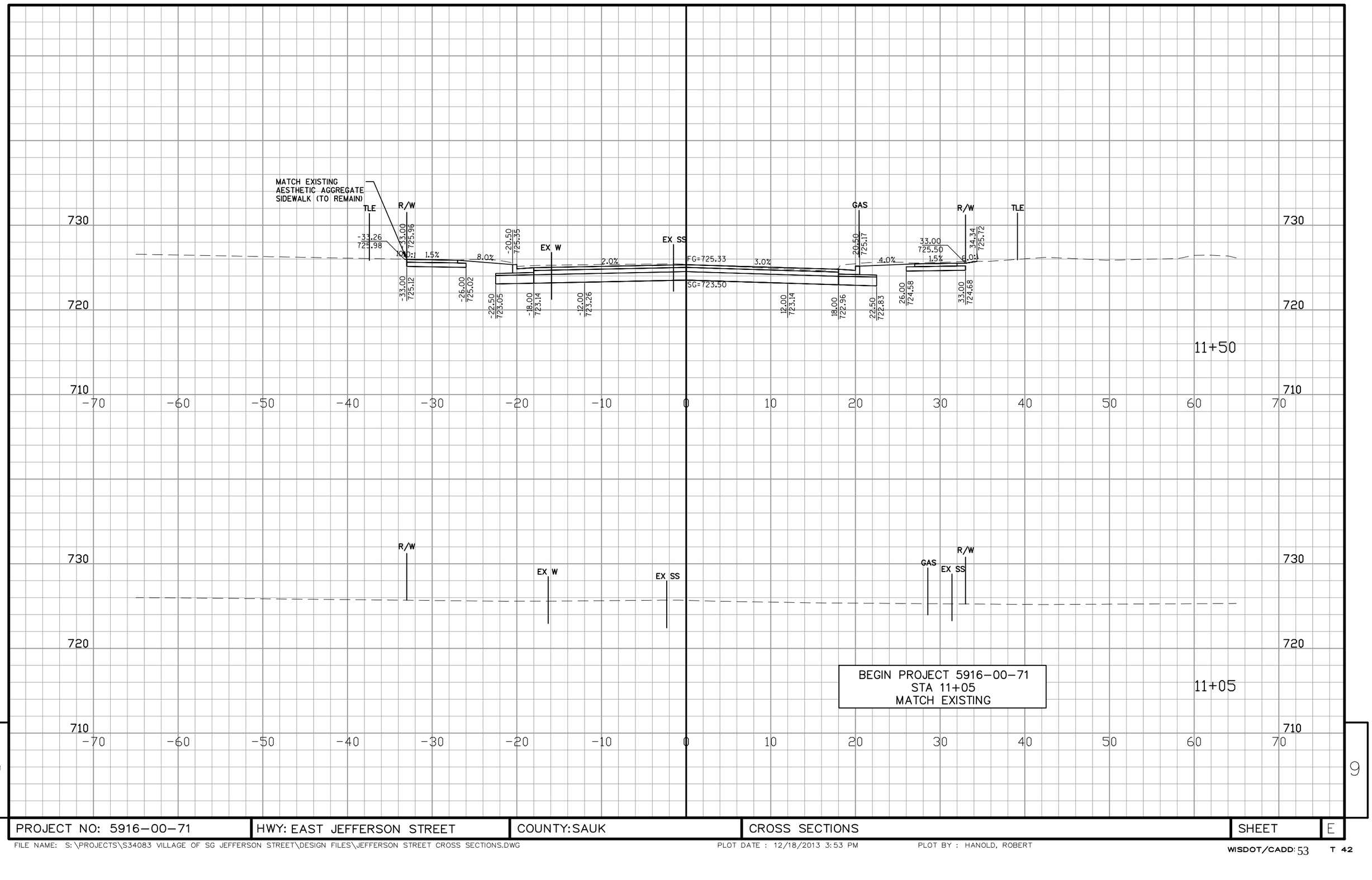
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 51 | E

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R21.DGN PLOT DATE : 28-MAY-2010 08:32 PLOT BY : ditjph PLOT NAME : PLOT SCALE : 4.717577:1.000000
WISDOT/CADDS SHEET 42

EARTHWORK-MAINLINE

	AREA (SF)					INCREMENTAL VOL (CY)								CUMMULATIVE VOLUME (CY)							
STATION	CUT	SALVAGED/ UNUSABLE PAV'T MATERIAL	FILL	MARSH EX	EBS	CUT NOTE 1	SALVAGED/ UNUSABLE PAV'T MATERIAL NOTE 2	FILL NOTE 3	MARSH EX	REDUCED MARSH IN FILL (0.6) NOTE 4	FILL (25%)	SELECT CRUSHED MATERIAL (1.5)	EBS	CUT 1.00 NOTE 1	FILL	MARSH EX	REDUCED MARSH IN FILL (0.6) NOTE 4	FILL (25%) NOTE 5	SELECT CRUSHED MATERIAL (1.5)	EBS	MASS ORDINATE NOTE 6
11+05	165	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11+25	165	0	0	0	0	122	0	0	0	0	0	0	0	122	0	0	0	0	0	0	122
11+25	99	0	0	0	0	0	0	0	0	0	0	0	0	122	0	0	0	0	0	0	122
11+50	99	0	0	0	0	92	0	0	0	0	0	0	0	214	0	0	0	0	0	0	214
12+00	124	0	0	0	0	201	0	0	0	0	0	0	0	415	0	0	0	0	0	0	415
12+50	107	0	1	0	0	210	0	1	0	0	1	0	0	625	1	0	0	1	0	0	624
13+00	62	0	9	0	0	153	0	9	0	0	11	0	0	778	10	0	0	13	0	0	766
13+50	54	0	10	0	0	105	0	18	0	0	23	0	0	883	28	0	0	35	0	0	848
14+00	57	0	13	0	0	101	0	21	0	0	26	0	0	984	49	0	0	61	0	0	923
14+49	57	0	13	0	0	104	0	23	0	0	29	0	0	1088	72	0	0	90	0	0	998
14+49	40	0	0	0	0	0	0	0	0	0	0	0	0	1088	72	0	0	90	0	0	998
14+50	40	0	0	0	0	2	0	0	0	0	0	0	0	1090	72	0	0	90	0	0	1000
15+00	55	0	0	0	0	87	0	0	0	0	0	0	0	1177	72	0	0	90	0	0	1087
15+11	55	0	0	0	0	22	0	0	0	0	0	0	0	1199	72	0	0	90	0	0	1109
15+11	121	0	0	0	0	0	0	0	0	0	0	0	0	1199	72	0	0	90	0	0	1109
15+50	121	0	0	0	0	172	0	0	0	0	0	0	0	1371	72	0	0	90	0	0	1281
16+00	137	0	0	0	0	238	0	0	0	0	0	0	0	1609	72	0	0	90	0	0	1519
16+50	129	0	0	0	0	247	0	0	0	0	0	0	0	1856	72	0	0	90	0	0	1766
17+00	130	0	0	0	0	240	0	0	0	0	0	0	0	2096	72	0	0	90	0	0	2006
17+50	111	0	0	0	0	223	0	0	0	0	0	0	0	2319	72	0	0	90	0	0	2229
17+95	111	0	0	0	0	185	0	0	0	0	0	0	0	2504	72	0	0	90	0	0	2414
			COLUMN SUBTOTALS =			2504	0	72	0	0	90	0	0	2504	72	0	0	90	0	0	2414
			SOUTH ALBANY STREET =			82	0	0	0	0	0	0	0	2586	72	0	0	90	0	0	2496
			NORTH ALBANY STREET =			104	0	0	0	0	0	0	0	2690	72	0	0	90	0	0	2600
			P.E., C.E. =			60	0	0	0	0	0	0	0	2750	72	0	0	90	0	0	2660
			COLUMN TOTALS =			2750	0	72	0	0	90	0	0								

NOTES:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
4 - REDUCED MARSH IN FILL	REDUCED MARSH THAT CAN BE USED IN FILL
5 - FILL (25%)	FILL 25%: (FILL -REDUCED MARSH IN FILL)*1.25
6 - MASS ORDINATE	(CUT - FILL (25%))



PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E

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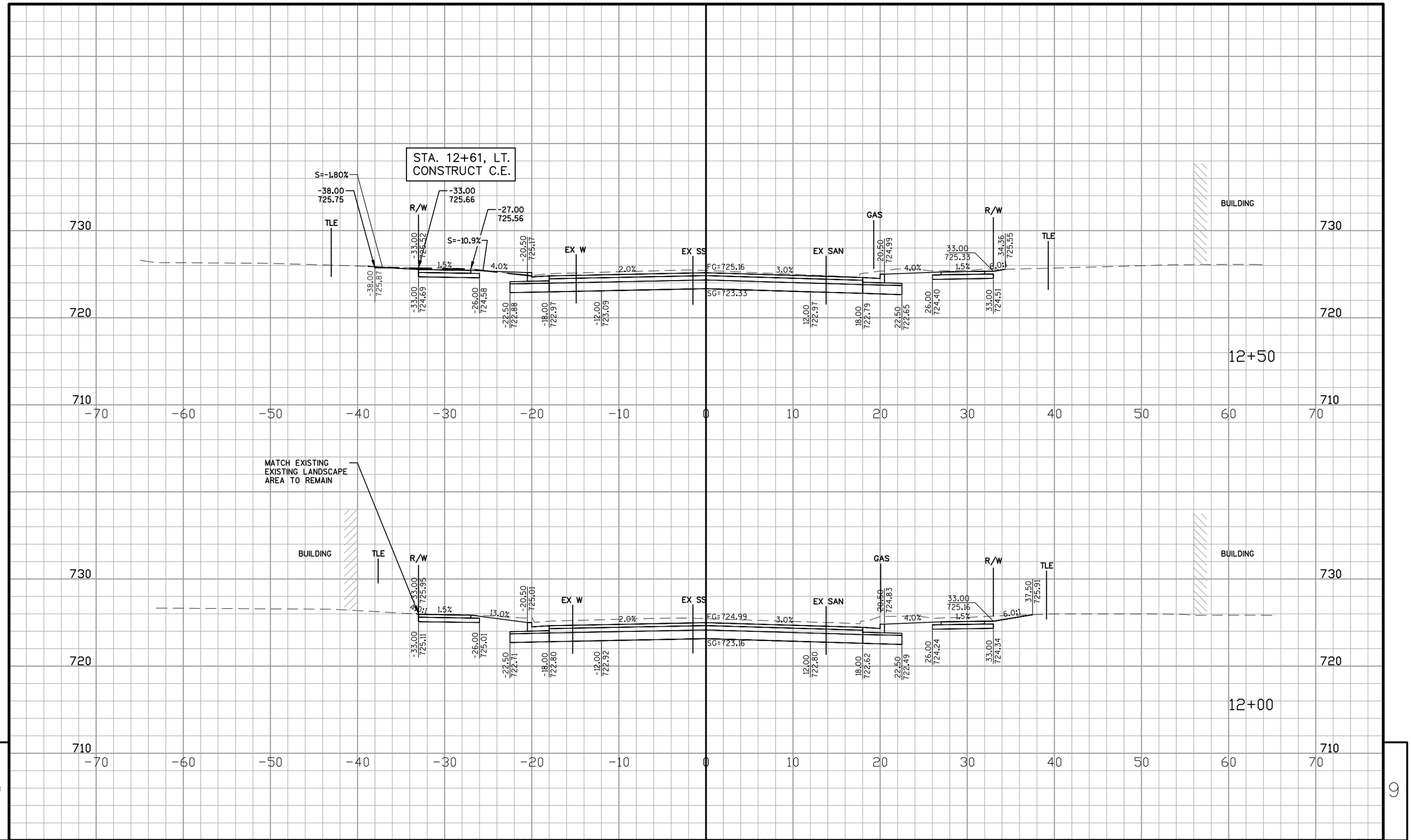
WISDOT/CADD: 53 T 42



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FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\JEFFERSON STREET CROSS SECTIONS.DWG

PLOT DATE : 12/18/2013 3:53 PM

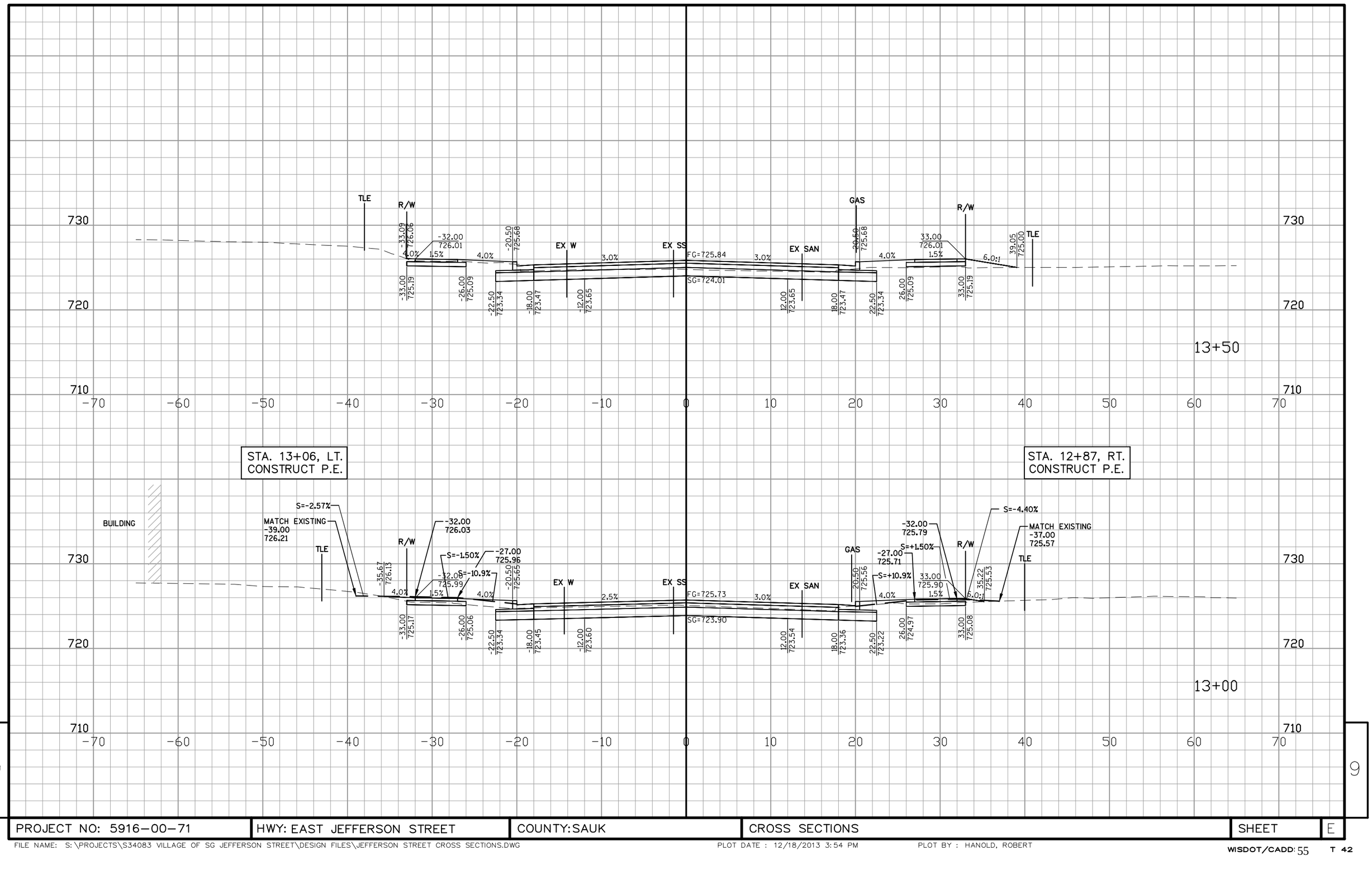
PLOT BY : HANOLD, ROBERT

WISDOT/CADD: 54

T 42

9

9



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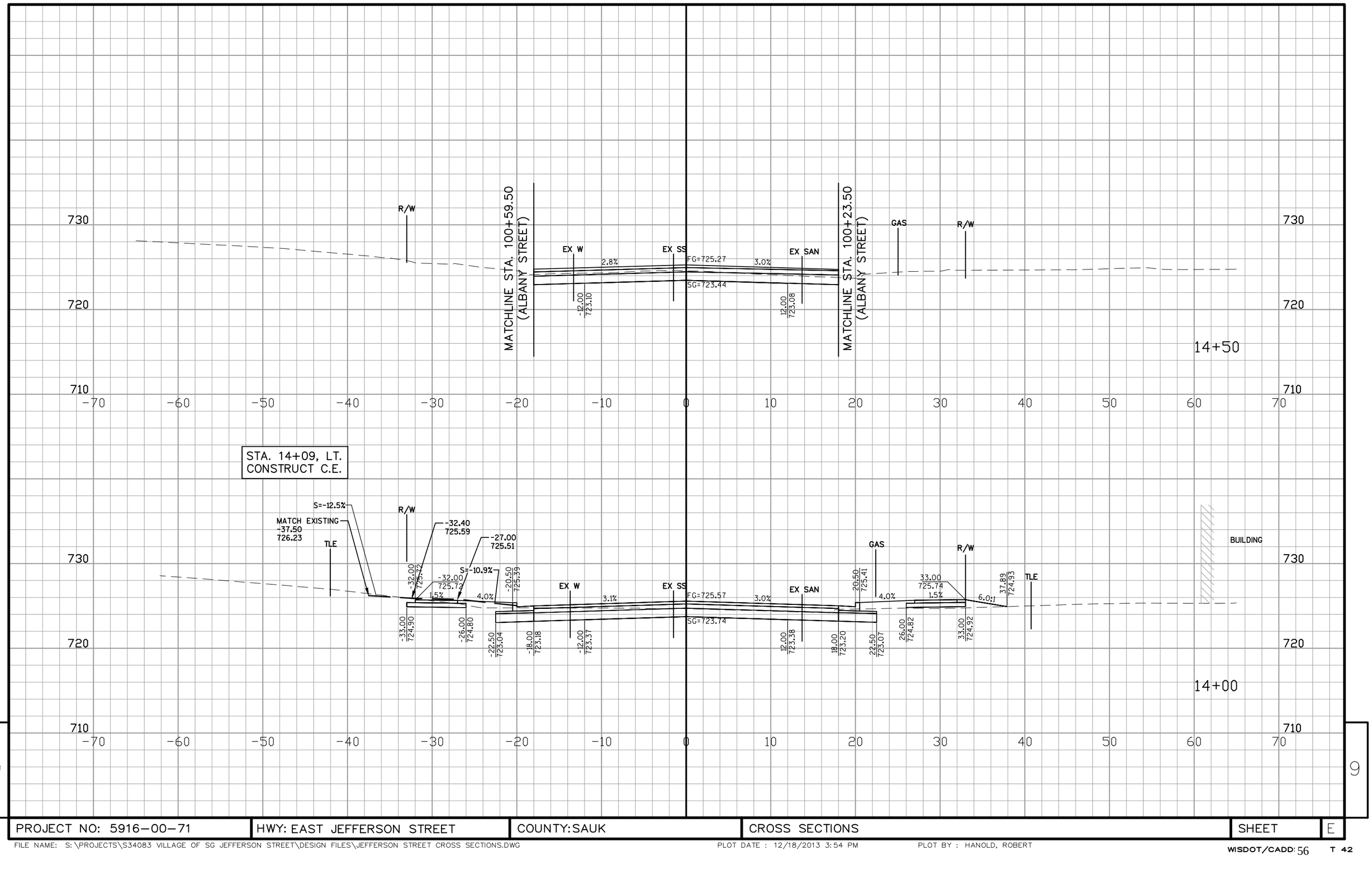
FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\JEFFERSON STREET CROSS SECTIONS.DWG PLOT DATE : 12/18/2013 3:54 PM PLOT BY : HANOLD, ROBERT

WISDOT/CADD: 55 T 42



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PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E

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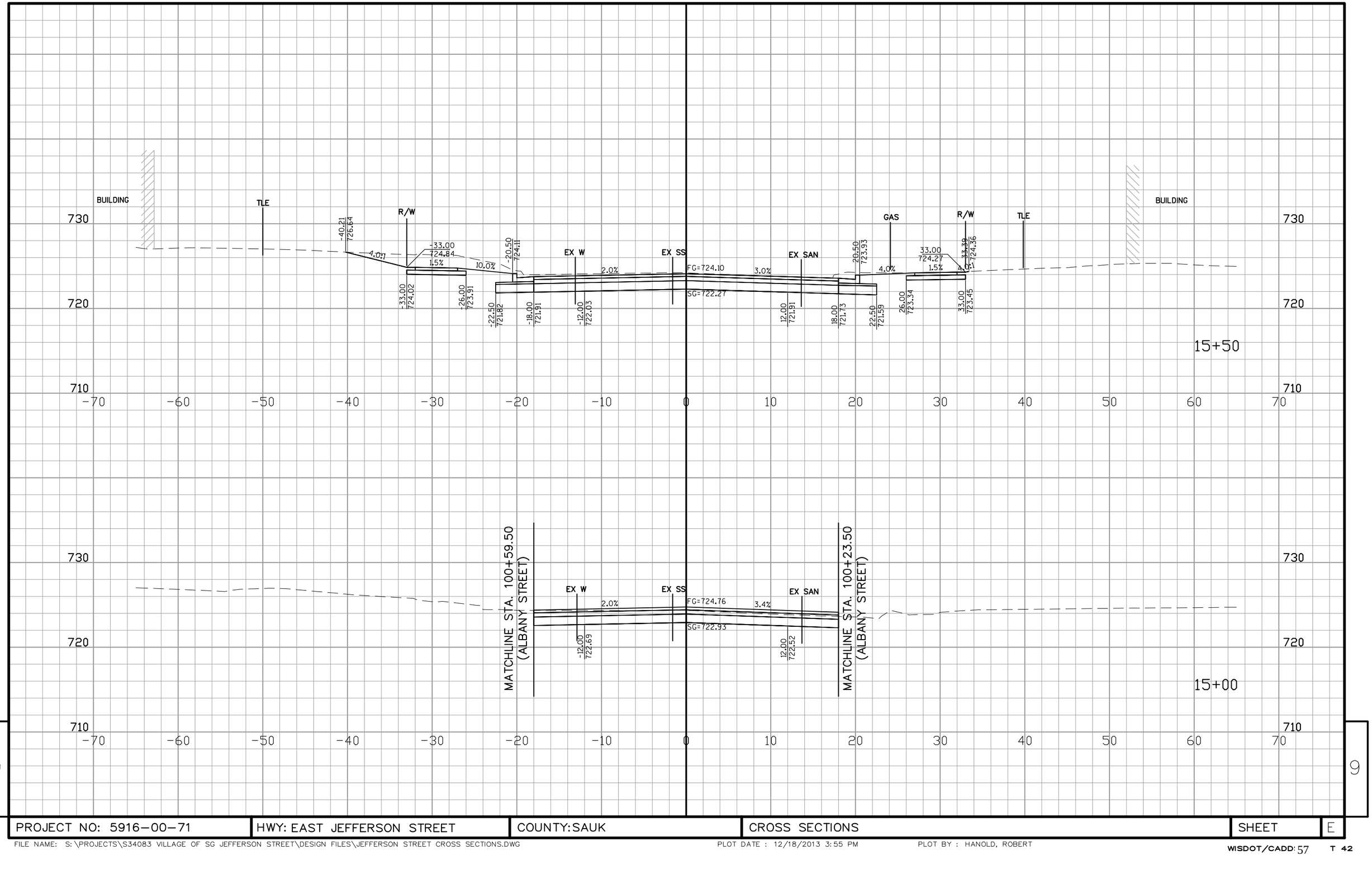

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FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\JEFFERSON STREET CROSS SECTIONS.DWG

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PLOT BY : HANOLD, ROBERT

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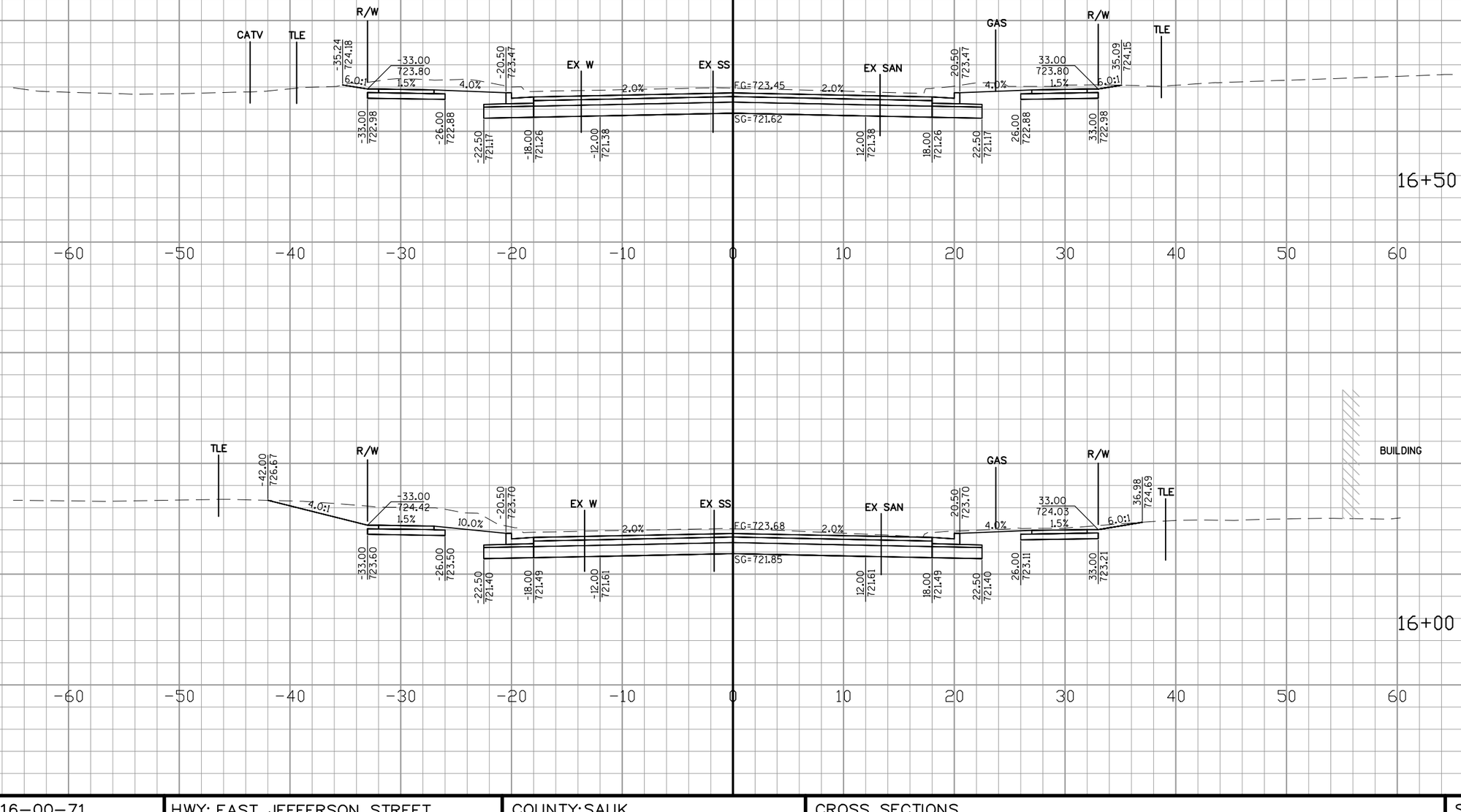

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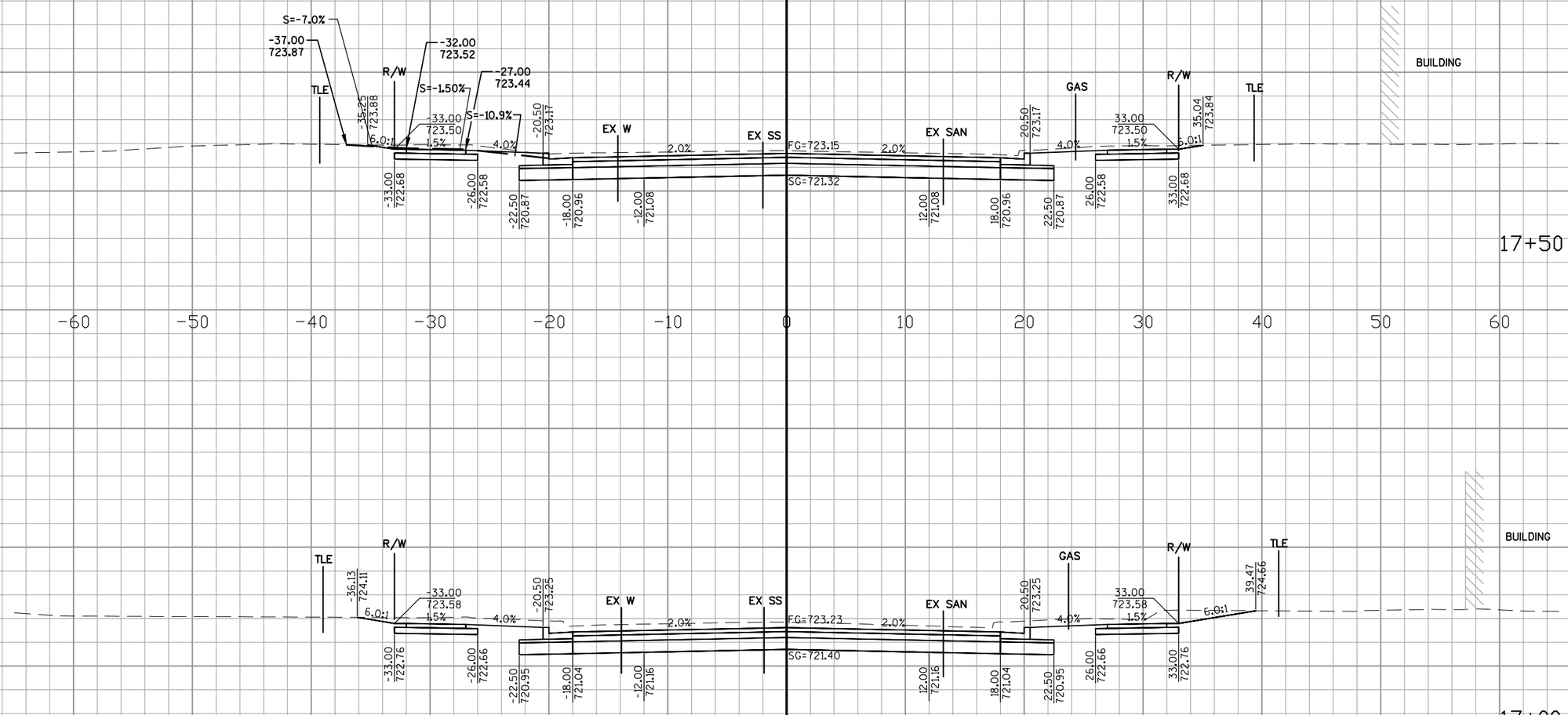
WISDOT/CADD: 58 T 42

STA. 17+47, LT.
CONSTRUCT C.E.



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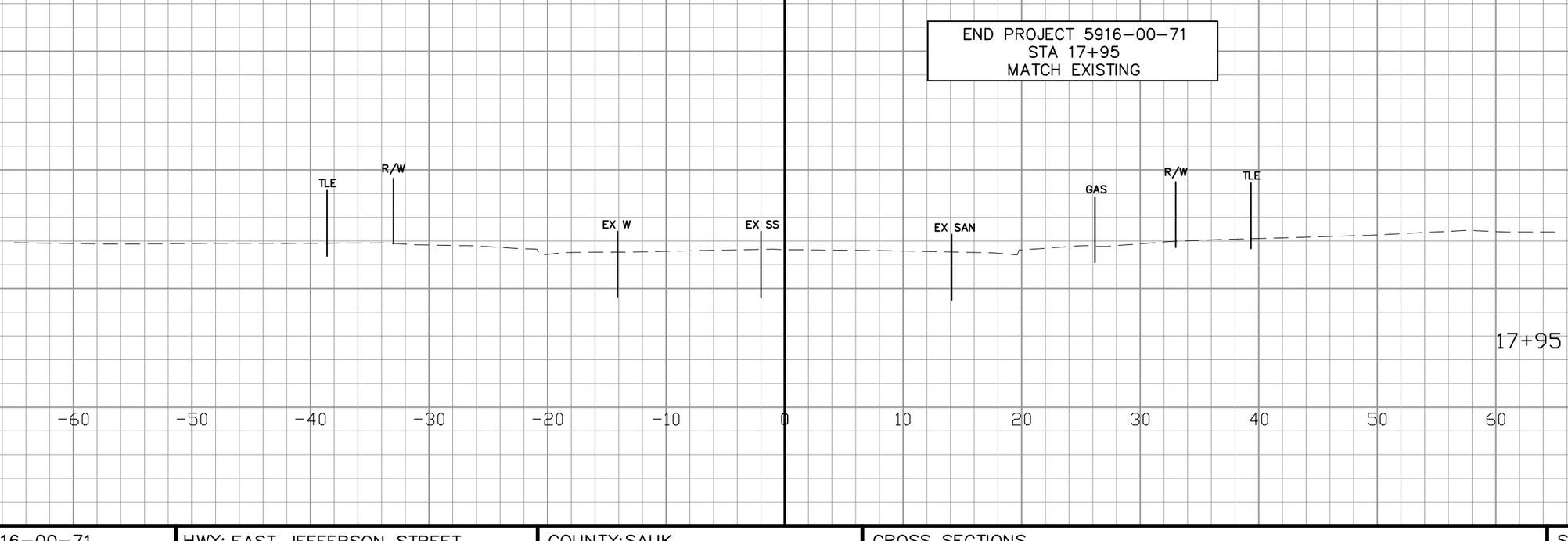
PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E

END PROJECT 5916-00-71
STA 17+95
MATCH EXISTING



9

9

PROJECT NO: 5916-00-71	HWY: EAST JEFFERSON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E

Notes

Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

http://www.dot.wisconsin.gov

SWL JULY 2014

PROJECT ID: 5916-00-72
WITH: 5916-00-68, 5916-00-69, 5916-00-71

COUNTY: SAUK

ORDER OF SHEETS

TOTAL SHEETS = 92

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5916-00-72	WISC 2014263	1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

VILLAGE OF SPRING GREEN, WEST MADISON STREET

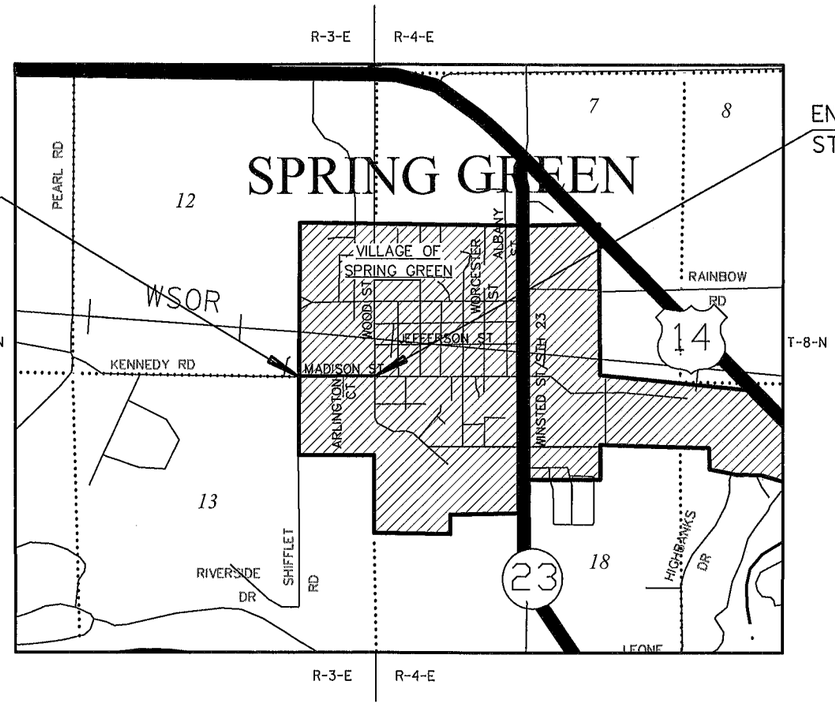
(SHIFFLET ROAD TO WOOD STREET)

LOCAL STREET

SAUK COUNTY

STATE PROJECT NUMBER
5916-00-72

N



DESIGN DESIGNATION

A.A.D.T. 2014	=	2730
A.A.D.T. 2034	=	3330
D.H.V. 2034	=	240
D.D.	=	62/38
T.	=	5.8%
DESIGN SPEED	=	30 MPH
ESALS	=	401,500

CONVENTIONAL SYMBOLS

PLAN
- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS/ ELECTRIC (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE
- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES
- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

LAYOUT
SCALE 0 1/2 MI.

TOTAL NET LENGTH OF CENTERLINE = 0.287 MI.

Coordinates on this plan are referenced to the Wisconsin County Coordinate System (WCCS), Sauk County.

ACCEPTED FOR
TOWN of SPRING GREEN
1-29-14 (Date) (TOWN CHAIRMAN)

ACCEPTED FOR
VILLAGE of SPRING GREEN
1-14-14 (Date) (Village President)

ORIGINAL PLANS PREPARED BY
JEWELL associates engineers, inc.
Engineers - Planners - Surveyors

WISCONSIN
ELLERY A. SCHAFFER
E-41742-6
SPRING GREEN, WI
PROFESSIONAL ENGINEER

1/24/2014 (Date) (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor JEWELL ASSOCIATES ENGINEERS, INC.
Designer JEWELL ASSOCIATES ENGINEERS, INC.
Management Consultant KJOHNSON ENGINEERS, INC.

APPROVED FOR THE DEPARTMENT
DATE: 1/31/14
Kirby A. Johnson
Management Consultant Signature

E

FILE NAME: S34084 - Title Sheet

2

LIST OF STANDARD ABBREVIATIONS

ABUT	Abutment	INV	Invert	RDWY	Roadway
AC	Acre	IP	Iron Pipe or Pin	SALV	Salvaged
AGG	Aggregate	IRS	Iron Rod Set	SAN S	Sanitary Sewer
AH	Ahead	JT	Joint	SEC	Section
<	Angle	JCT	Junction	SHLDR	Shoulder
ASPH	Asphaltic	LHF	Left–Hand Forward	SHR	Shrinkage
AVG	Average	L	Length of Curve	SW	Sidewalk
ADT	Average Daily Traffic	LIN FT or LF	Linear Foot	S	South
BAD	Base Aggregate Dense	LC	Long Chord of Curve	SQ	Square
BK	Back	MH	Manhole	SF or SQ FT	Square Feet
BF	Back Face	MB	Mailbox	SY or SQ YD	Square Yard
BM	Bench Mark	ML or M/L	Match Line	STD	Standard
BR	Bridge	N	North	SDD	Standard Detail Drawings
C or C/L	Center Line	Y	North Grid Coordinate	STH	State Trunk Highways
CC	Center to Center	OD	Outside Diameter	STA	Station
CTH	County Trunk Highway	PLE	Permanent Limited Easement	SS	Storm Sewer
CR	Creek	PT	Point	SG	Subgrade
CR	Crushed	PC	Point of Curvature	SE	Superelevation
CY or CU YD	Cubic Yard	PI	Point of Intersection	SL or S/L	Survey Line
CP	Culvert Pipe	PRC	Point of Reverse Curvature	SV	Septic Vent
C & G	Curb and Gutter	PT	Point of Tangency	T	Tangent
D	Degree of Curve	POC	Point On Curve	TEL	Telephone
DHV	Design Hour Volume	POT	Point on Tangent	TEMP	Temporary
DIA	Diameter	PVC	Polyvinyl Chloride	TI	Temporary Interest
E	East	PCC	Portland Cement Concrete	TLE	Temporary Limited Easement
X	East Grid Coordinate	LB	Pound	t	Ton
ELEC	Electric (al)	PSI	Pounds Per Square Inch	T or TN	Town
EL or ELEV	Elevation	PE	Private Entrance	TRANS	Transition
ESALS	Equivalent Single Axle Loads	R	Radius	TL or T/L	Transit Line
EBS	Excavation Below Subgrade	RR	Railroad	T	Trucks (percent of)
FF	Face to Face	R	Range	TYP	Typical
FE	Field Entrance	RL or R/L	Reference Line	UNCL	Unclassified
F	Fill	RP	Reference Point	UG	Underground Cable
FG	Finished Grade	RCCP	Reinforced Concrete Culvert Pipe	USH	United States Highway
FL or F/L	Flow Line	REQD	Required	VAR	Variable
FT	Foot	RES	Residence or Residential	V	Velocity or Design Speed
FTG	Footing	RW	Retaining Wall	VERT	Vertical
GN	Grid North	RT	Right	VC	Vertical Curve
HT	Height	RHF	Right–Hand Forward	VOL	Volume
CWT	Hundredweight	R/W	Right–of–Way	WM	Water Main
HYD	Hydrant	R	River	WV	Water Valve
INL	Inlet	RD	Road	W	West
ID	Inside Diameter			WB	Westbound
				YD	Yard

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE	0–2	2–6	6 & OVER	0–2	2–6	6 & OVER	0–2	2–6	6 & OVER	0–2	2–6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP–TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE–TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT												
ASPHALT	.70 – .95											
CONCRETE	.80 – .95											
BRICK	.70 – .80											
DRIVES, WALKS	.75 – .85											
ROOFS	.75 – .95											
GRAVEL ROADS, SHOULDERS	.40 – .60											

TOTAL PROJECT AREA= 2.82 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 2.55 ACRES

UTILITIES

ELECTRIC
ALLIANT ENERGY
142 S. CINCINNATI STREET
P.O. BOX 99
SPRING GREEN, WI 53588
CELL: (608) 214–4441
OFFICE: (608) 588–9702
ATTN: CHRIS WILHELM
EMAIL: chriswilhelm@alliantenergy.com

TELEPHONE
FRONTIER COMMUNICATIONS
100 COMMUNICATIONS DRIVE
SUN PRAIRIE, WI 53590
PH: (608) 837–1605
ATTN: DANA GILLETT
EMAIL: dana.gillett@frontier.com

CABLE TV
CHARTER COMMUNICATIONS
315 KING STREET
DODGEVILLE, WI 53533
PH: (608) 576–2613
ATTN: STEVE HEGGE
EMAIL: steve.hegge@chartercom.com

WATER/SANITARY SEWER/STORM SEWER
VILLAGE OF SPRING GREEN
112 W. MONROE STREET
SPRING GREEN, WI 53588
CELL: (608) 588–4983
OFFICE: (608) 588–2335
ATTN: GREG WIPPERFURTH

GAS
ALLIANT ENERGY
142 S. CINCINNATI STREET
P.O. BOX 99
SPRING GREEN, WI 53588
CELL: (608) 214–4441
OFFICE: (608) 588–9702
ATTN: CHRIS WILHELM
EMAIL: chriswilhelm@alliantenergy.com

DIGGERS HOTLINE
Dial 811 or (800) 242-8511
www.DiggersHotline.com

* DENOTES UTILITY IS NOT A MEMBER OF DIGGERS HOTLINE

CONTACTS

DESIGN CONSULTANT:
JEWELL ASSOCIATES ENGINEERS, INC.
560 SUNRISE DRIVE
SPRING GREEN, WI 53588
ATTN: ED LILLA, P.E.
PH: (608) 588–7484
FAX: (608) 588–9322
E MAIL: ed.lilla@jewellassoc.com

WDNR LIAISON:
STATE OF WISCONSIN
DEPT. OF NATURAL RESOURCES
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
ATTN: CATHY BLESER
PH: (608) 275–3308
E MAIL: catherine.bleser@wisconsin.gov

VILLAGE OF SPRING GREEN:
EUGENE HAUSNER, VILLAGE PRESIDENT
520 WORCESTER STREET
SPRING GREEN, WI 53588
PH: (608) 588–7780

TOWN OF SPRING GREEN:
DENNIS POLIVKA, TOWN CHAIRPERSON
E3681 CTH JJ
SPRING GREEN, WI 53588
PH: (608) 335–3291

ORDER OF SECTION 2 SHEETS:

GENERAL NOTES

COORDINATES AND BEARINGS ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), SAUK COUNTY.

ELEVATIONS SHOWN ON THE PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88)

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

EXCAVATION BELOW SUBGRADE (EBS) IS NOT USED TO BALANCE YARDAGE, AND IS NOT SHOWN ON THE CROSS SECTIONS BUT IS MEASURED AND PAID FOR AS COMMON EXCAVATION. EXACT LOCATIONS OF EBS WILL BE DETERMINED BY THE ENGINEER.

DISTURBED AREAS SHOWN WITHIN THE RIGHT–OF–WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS ARE TO BE FERTILIZED (TYPE B), SEEDED (USE SEED MIX NO. 40) AND MULCHED AS DIRECTED BY THE ENGINEER.

WHEN THE QUANTITY OF THE ITEM OF BASE AGGREGATE DENSE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER IN THE FIELD.

EROSION CONTROL ITEMS SHALL BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER IN THE FIELD. SILT FENCE SHALL BE PLACED PRIOR TO CONSTRUCTION.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

FILL EXPANSION IS VARIABLE AND IS ESTIMATED AT 25%.

THE EXACT LOCATION OF PRIVATE, FIELD, AND COMMERCIAL ENTRANCES TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

REMOVAL OF ASPHALTIC SURFACES WHERE AN ABUTTING ASPHALTIC SURFACE IS TO REMAIN IN PLACE SHALL REQUIRE A SAWCUT MEETING THE APPROVAL OF THE ENGINEER IN THE FIELD.

ACCURACY OF INLET AND DISCHARGE ELEVATIONS FOR DRAINAGE STRUCTURES SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

CURB AND GUTTER ELEVATIONS ARE GIVEN ON THE FLANGE LINE, UNLESS OTHERWISE NOTED.

ALL RADII DIMENSIONS ON THE PLAN FOR CURB AND GUTTER ARE TO THE FLANGE OF THE CURB AND GUTTER.

EXISTING DRIVEWAYS SHALL BE RESTORED IN KIND AND THEIR LOCATION VERIFIED BY THE ENGINEER IN THE FIELD.

HMA PAVEMENT TYPE E–1 QUANTITIES WERE CALCULATED USING 115 LB/SY/IN. 4–INCHES OF HMA PAVEMENT TYPE E–1 SHALL BE CONSTRUCTED WITH A 1 3/4–INCH UPPER LAYER AND A 2 1/4–INCH LOWER LAYER. ASPHALTIC MATERIAL PG 64–28 SHALL BE USED ON THE UPPER LAYER AND ASPHALTIC MATERIAL PG 58–28 SHALL BE USED ON THE LOWER LAYER.

EXPANSION JOINTS SHALL BE CONSTRUCTED AT ALL RADII POINTS IN THE CURB & GUTTER.

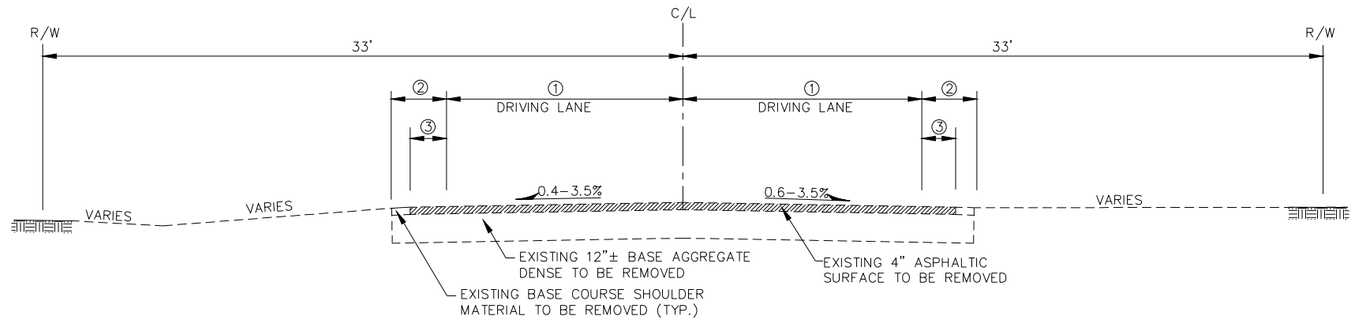
THE COST OF CONNECTING CULVERT PIPE OR STORM SEWER TO EXISTING DRAINAGE STRUCTURES SHALL BE INCIDENTAL TO THE COST OF INSTALLING THE CULVERT OR STORM SEWER.

STORM SEWER ELEVATION ELEVATIONS, LENGTHS, AND LOCATIONS AS SHOWN ON THE PLANS AND CROSS SECTIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.

MISCELLANEOUS REMOVAL ITEMS REQUIRING RESTORATIONS OF CONCRETE OR ASPHALT DRIVEWAYS, SIDEWALKS, OR SIDE STREETS SHALL BE REMOVED TO AN EXISTING JOINT OR SAWED AS DETERMINED BY THE ENGINEER IN THE FIELD OR AS SHOWN ON THE PLANS.

TRANSVERSE JOINTS IN CONCRETE SIDEWALK SHALL BE CONSTRUCTED AT INTERVALS EQUAL TO THE WIDTH OF THE CONCRETE SIDEWALK, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

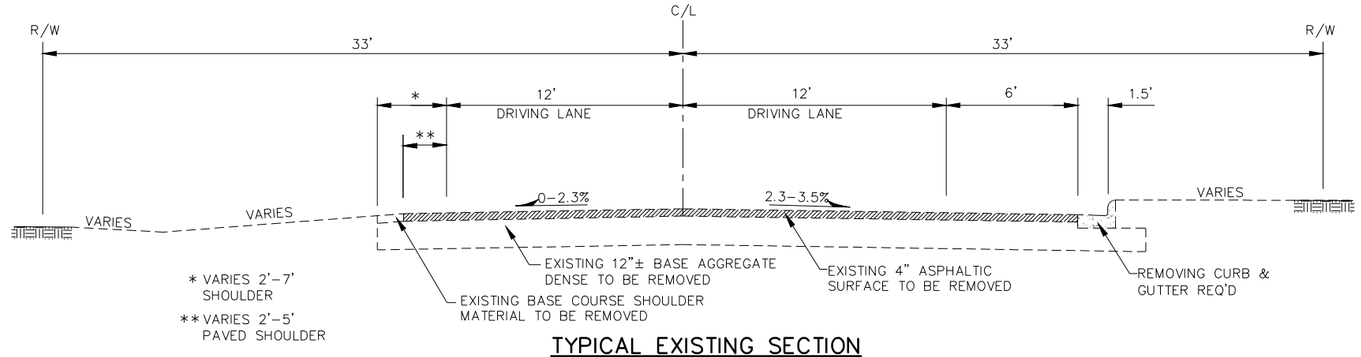
NO STORM SEWER SHALL BE REMOVED UNTIL DIRECTED BY THE ENGINEER.



TYPICAL EXISTING SECTION

(WEST MADISON STREET)

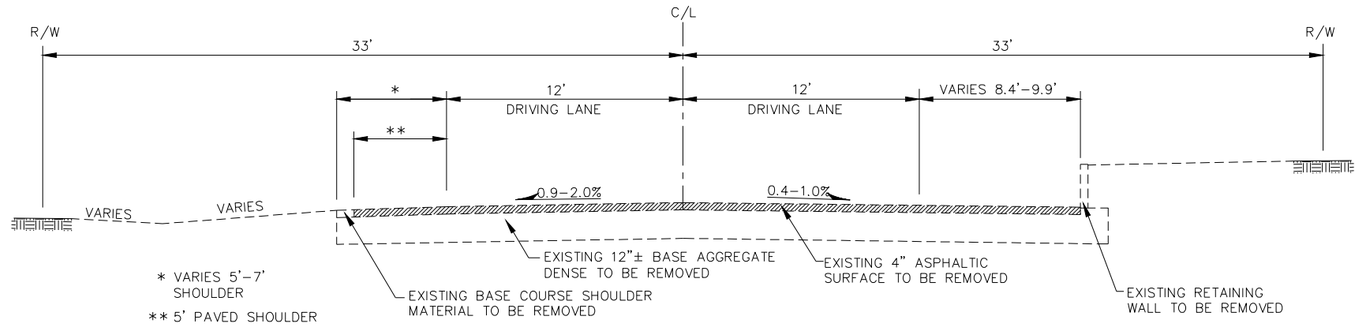
STATION – STATION	① (FT.)	② SHOULDER LT. (FT.)	② SHOULDER RT. (FT.)	③ PAVED SHOULDER LT. (FT.)	③ PAVED SHOULDER RT. (FT.)
50+00 – 50+50	11'	0'–2'	0'–2'	0'–2'	–
50+50 – 51+55	11'	0'–2'	0'–2'	–	–
53+45 – 63+60	12'	2'–6'	1'–11'	2'–4'	1'–9'



TYPICAL EXISTING SECTION

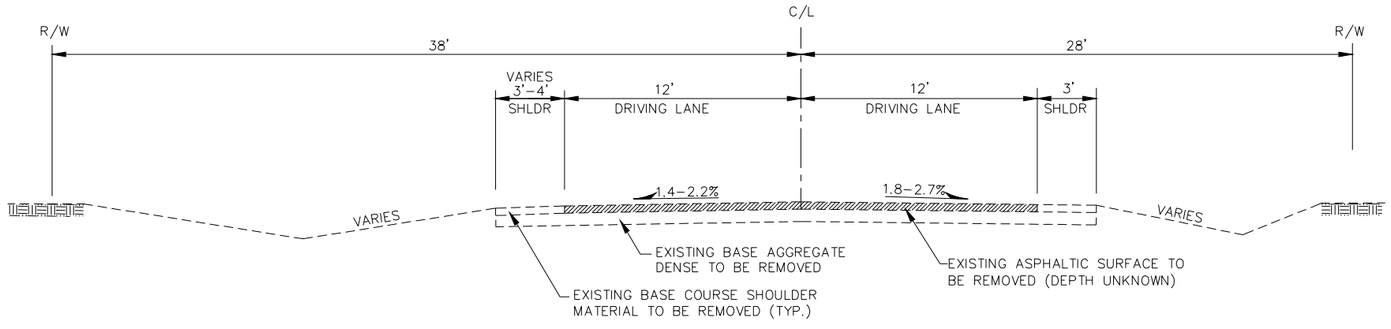
(WEST MADISON STREET)
STA. 51+55 – STA. 53+45

PROJECT NO: 5916–00–72	HWY: WEST MADISON STREET	COUNTY: SAUK	TYPICAL EXISTING SECTIONS	SHEET 3	E



TYPICAL EXISTING SECTION

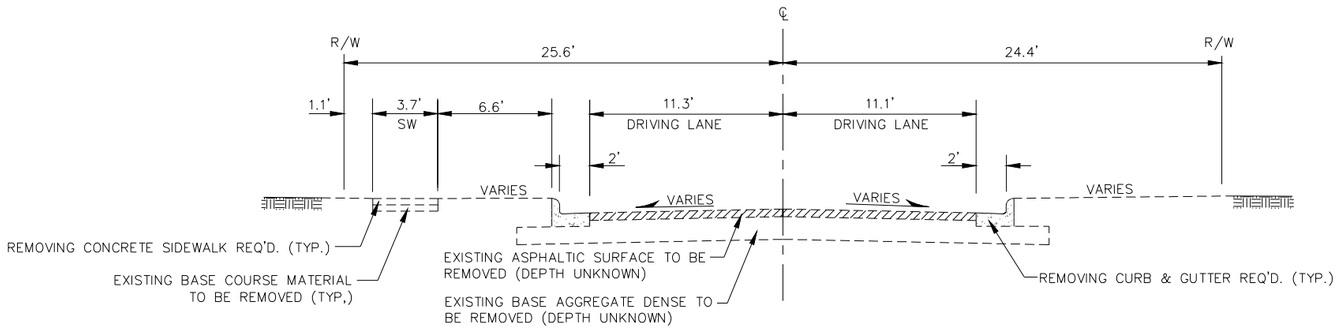
(WEST MADISON STREET)
STA. 63+60 – STA. 65+15



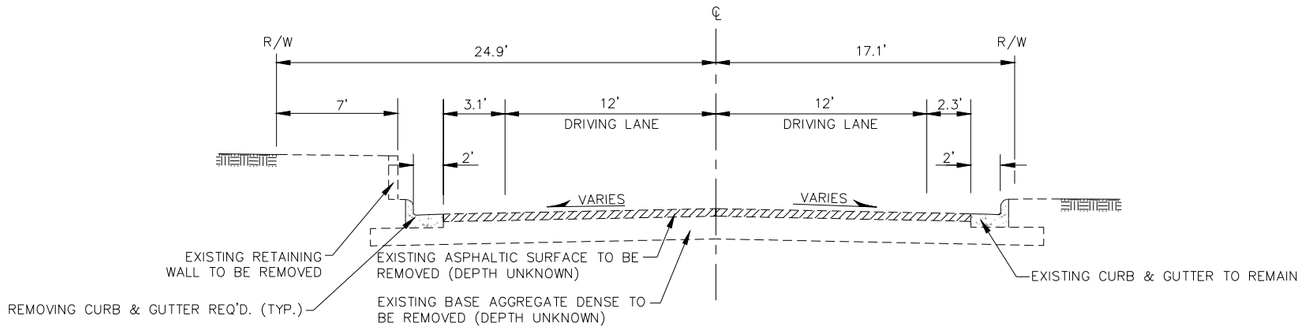
TYPICAL EXISTING SECTION

(SHIFFLET ROAD)

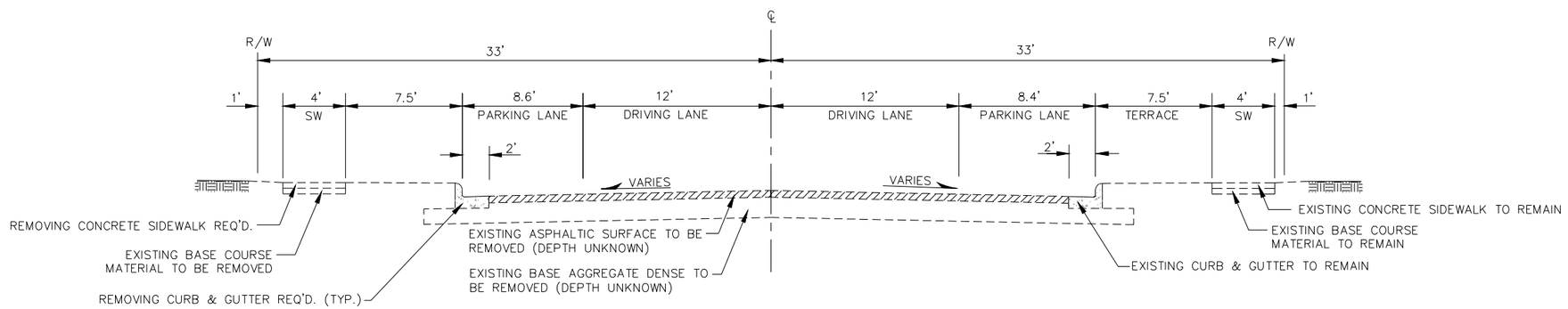
PROJECT NO: 5916–00–72	HWY: WEST MADISON STREET	COUNTY: SAUK	TYPICAL EXISTING SECTIONS	SHEET 4	E



TYPICAL EXISTING SECTION
(ARLINGTON COURT)

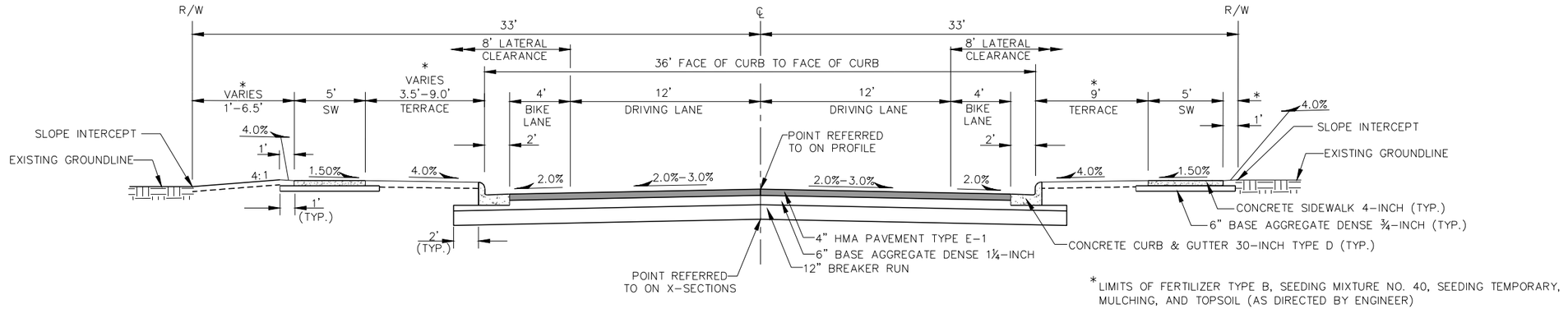


TYPICAL EXISTING SECTION
(SOUTH WOOD STREET)



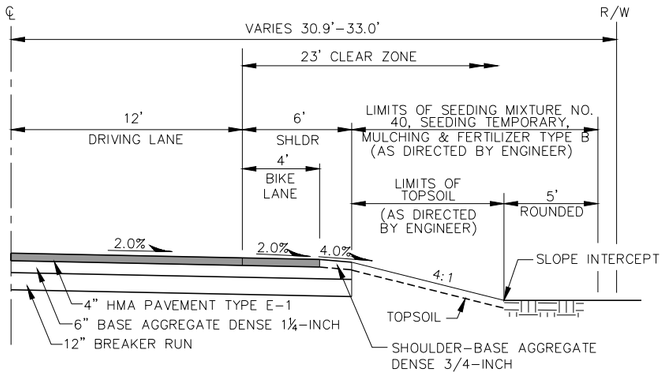
TYPICAL EXISTING SECTION
(NORTH WOOD STREET)

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY:SAUK	TYPICAL EXISTING SECTIONS	SHEET 5	E



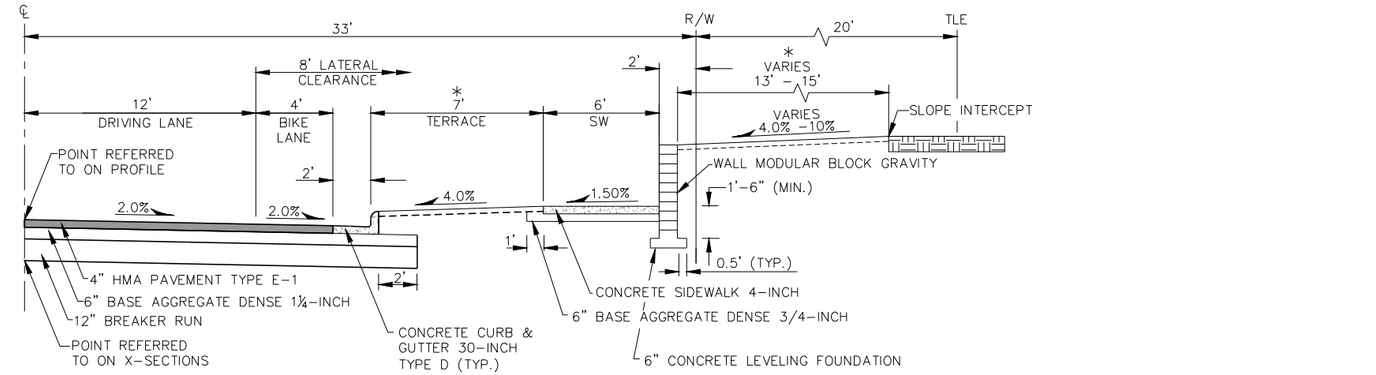
TYPICAL FINISHED SECTION

(WEST MADISON STREET)
STA. 51+41 – STA. 63+60, RT.
STA. 51+74 – STA. 65+15, LT.



PARTIAL RURAL TYPICAL FINISHED SECTION

(WEST MADISON STREET)
STA. 50+00 – STA. 51+41, RT.

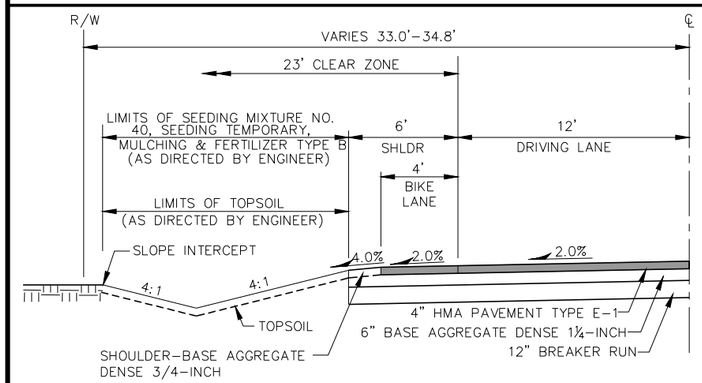


PARTIAL URBAN TYPICAL FINISHED SECTION WITH RETAINING WALL

STA. 63+60 – STA. 64+64, RT.

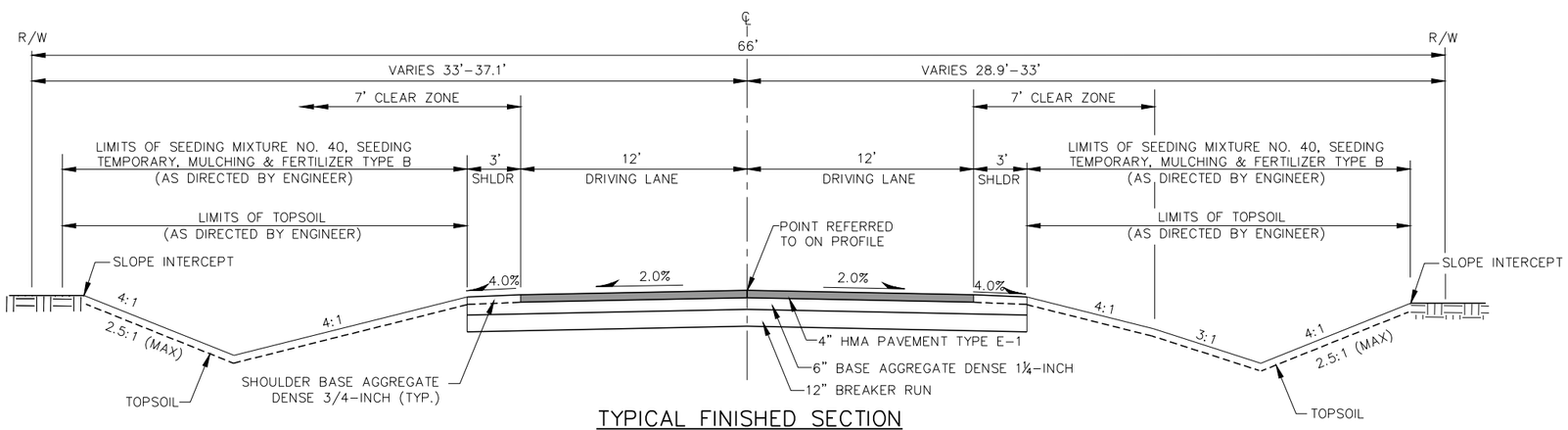
* LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, SEEDING TEMPORARY, MULCHING, AND TOPSOIL (AS DIRECTED BY ENGINEER)

NOTE: SEE WALL MODULAR BLOCK GRAVITY DETAILS FOR ADDITIONAL INFORMATION



PARTIAL RURAL TYPICAL FINISHED SECTION

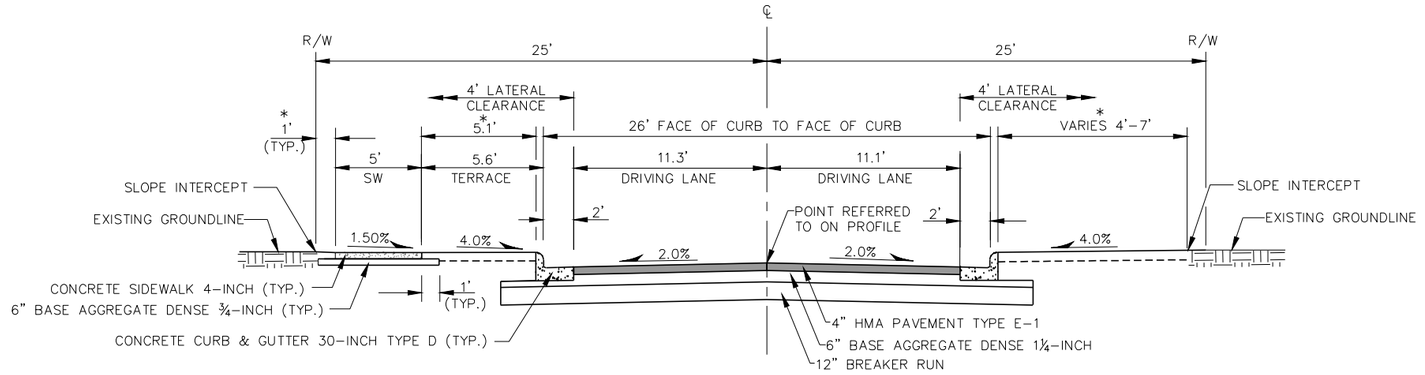
(WEST MADISON STREET)
STA. 50+00 – STA. 51+74, LT.



TYPICAL FINISHED SECTION

(SHIFFLET ROAD)

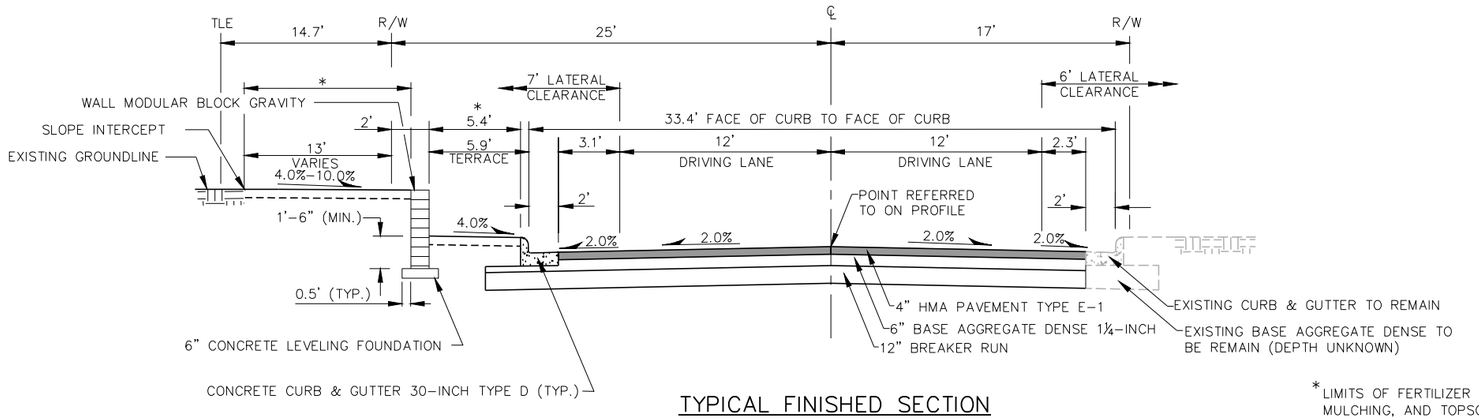
PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	TYPICAL FINISHED SECTION	SHEET 6	E



TYPICAL FINISHED SECTION

(ARLINGTON COURT)

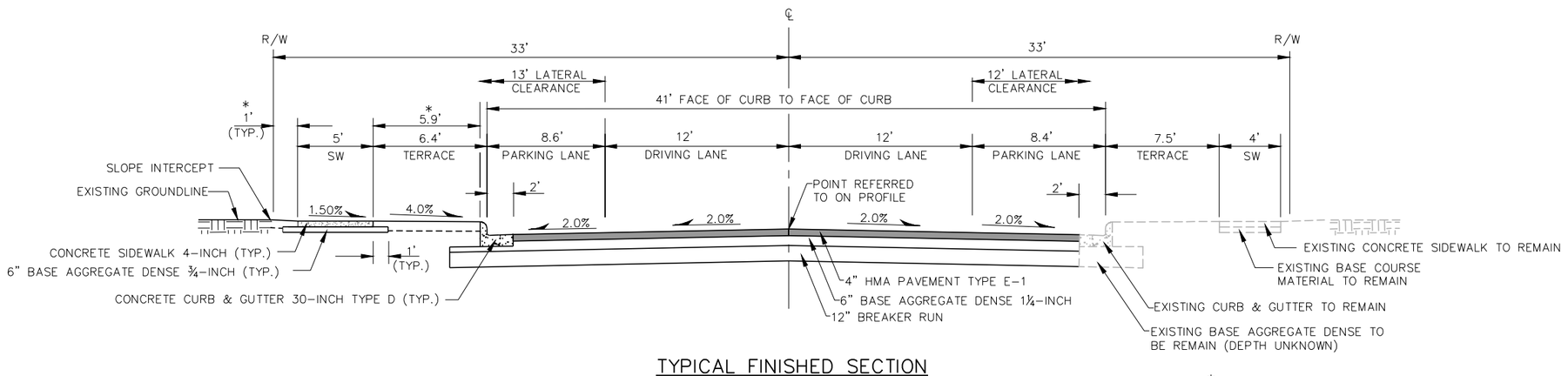
*LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, SEEDING TEMPORARY, MULCHING, AND TOPSOIL (AS DIRECTED BY ENGINEER)



TYPICAL FINISHED SECTION

(SOUTH WOOD STREET)

*LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, SEEDING TEMPORARY, MULCHING, AND TOPSOIL (AS DIRECTED BY ENGINEER)

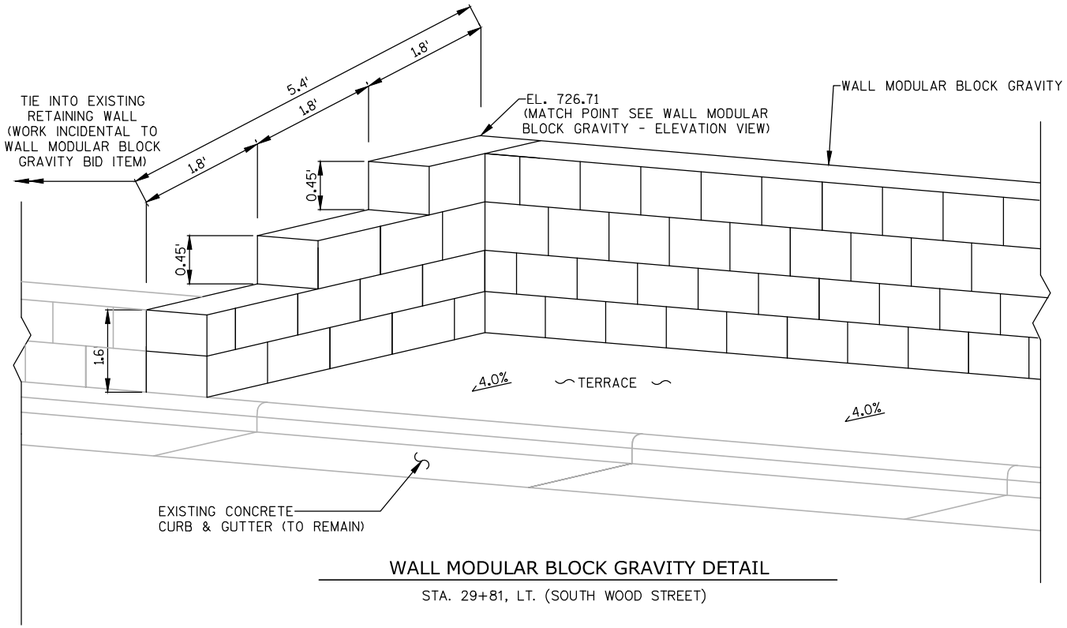


TYPICAL FINISHED SECTION

(NORTH WOOD STREET)

*LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, SEEDING TEMPORARY, MULCHING, AND TOPSOIL (AS DIRECTED BY ENGINEER)

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY:SAUK	TYPICAL FINISHED SECTION	SHEET 7	E



WALL MODULAR BLOCK GRAVITY DETAIL

STA. 29+81, LT. (SOUTH WOOD STREET)

SOIL PARAMETERS

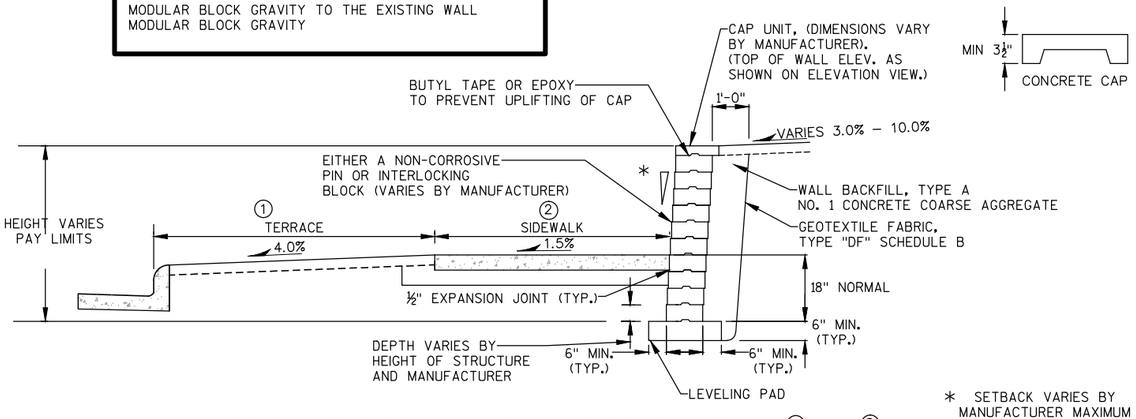
SOIL DESCRIPTION	UNIT WEIGHT (pcf)	FRICTION ANGLE (degrees)	COHESION (psf)
TOPSOIL (EL. 722.71 - EL. 721.71)	100	20	250
SILT (N/A)	110	20	500
SAND, MEDIUM DENSE (EL. 721.71 - BELOW)	115	34	0
RETAINED SOIL (EL. 726.71 - EL. 722.71)	120	34	0

ELEVATIONS AND LENGTHS ARE APPROXIMATE.
CONSTRUCT TO FIT TERRAIN.

DRAWING NOT TO SCALE.

RETAINING WALL BLOCK DEPTH MIN. 8" - MAX. 12"

COLOR - MATCH THE COLOR OF THE PROPOSED WALL MODULAR BLOCK GRAVITY TO THE EXISTING WALL MODULAR BLOCK GRAVITY

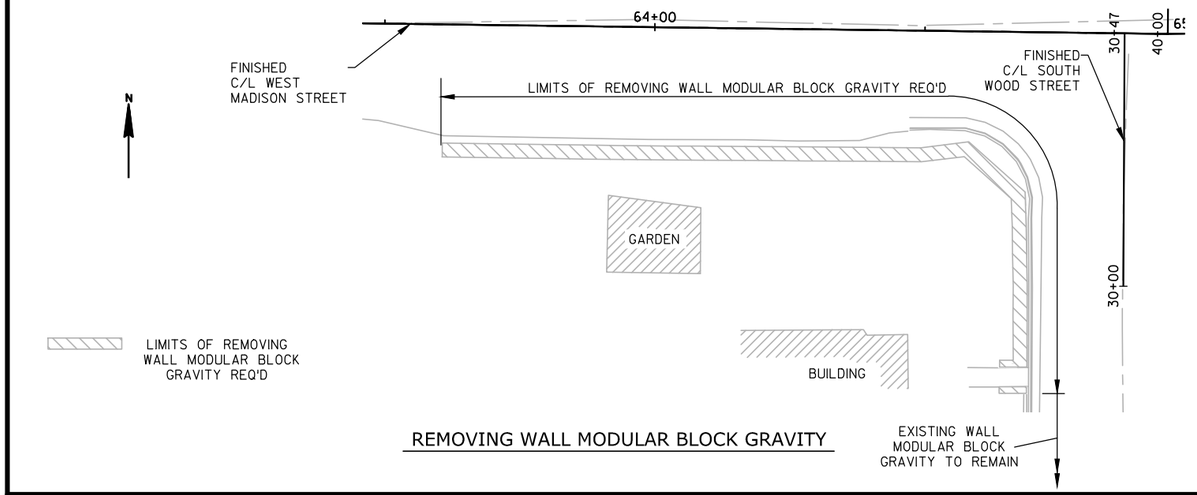


TYPICAL DETAIL FOR WALL MODULAR BLOCK GRAVITY

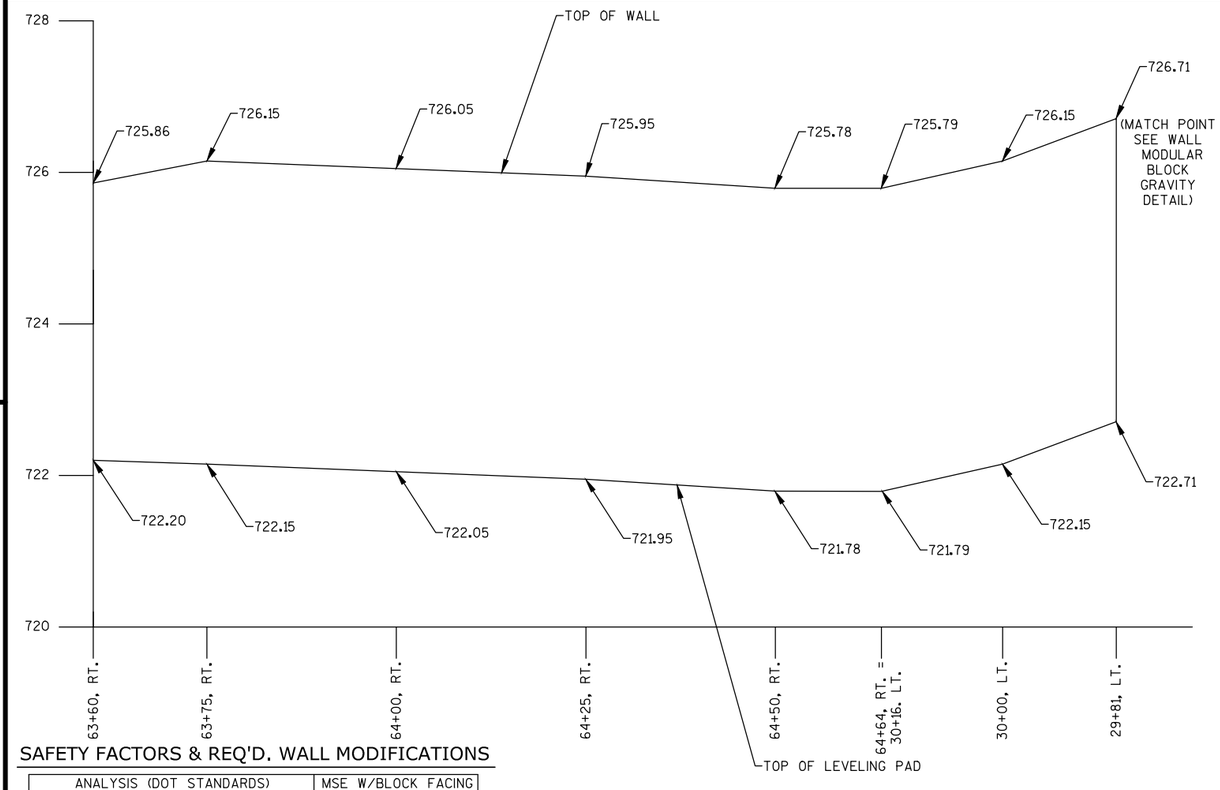
STA. 63+60 – STA. 64+64, RT. (WEST MADISON STREET)
STA. 29+81 – STA. 30+16, LT. (SOUTH WOOD STREET)

① (FT)	② (FT)
7'	6'
5.4'	0'

* SETBACK VARIES BY MANUFACTURER MAXIMUM FRONT FACE SLOPE FROM VERTICAL IS 1 HORIZONTAL TO 8 VERTICAL.



REMOVING WALL MODULAR BLOCK GRAVITY



WALL MODULAR BLOCK GRAVITY - ELEVATION VIEW

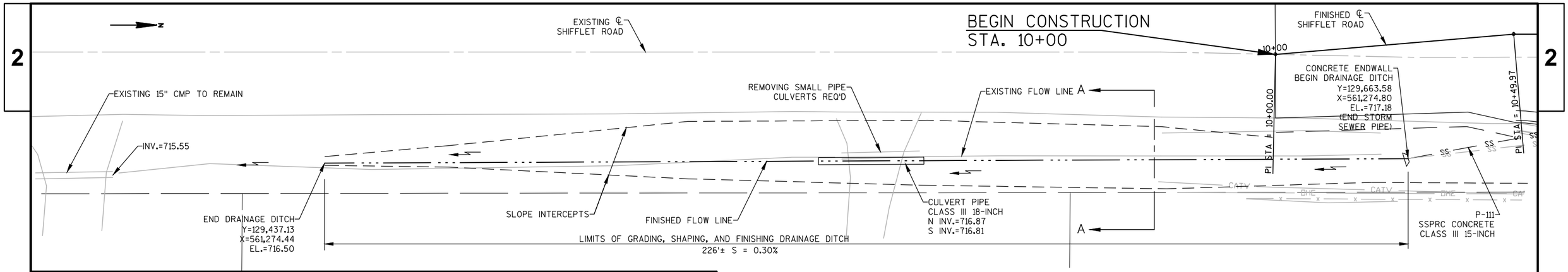
STA. 63+60 – STA. 64+64, RT. (WEST MADISON STREET)
STA. 29+81 – STA. 30+16, LT. (SOUTH WOOD STREET)

SAFETY FACTORS & REQ'D. WALL MODIFICATIONS

ANALYSIS (DOT STANDARDS)	MSE W/BLOCK FACING
SLIDING (FS>1.5)	1.9
OVERTURNING (FS>2.0)	2.3
GLOBAL STABILITY (FS>1.3)	2.3
ALLOWABLE BEARING CAPACITY. PSF BASED ON WALL WIDTHS & IMBEDMENT DEPTHS SHOWN IN TABLE	3000 PSF
MINIMUM WALL WIDTH*	6'
MINIMUM WALL IMBEDMENT	1'-6"

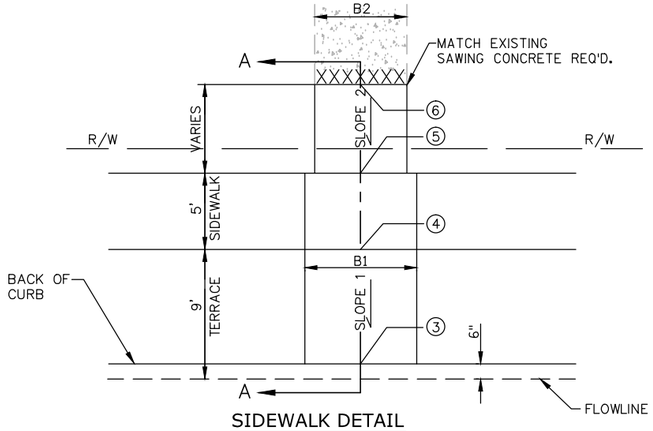
*THE LENGTH OF MSE REINFORCEMENT IS MEASURED FROM THE BACK OF THE WALL FACING TO THE BACK OF THE REINFORCED SOIL MASS.

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CONSTRUCTION DETAILS	SHEET 8	E



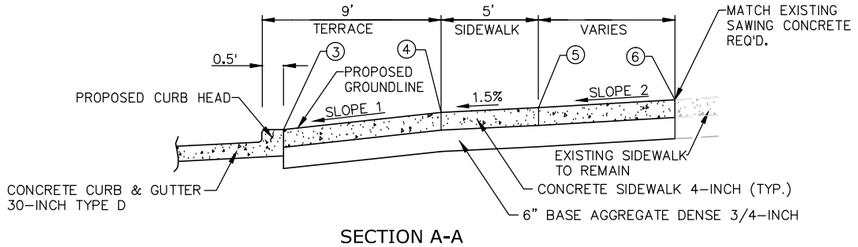
ENTRANCE DATA TABLE

STATION	LOCATION	WIDTH "B1" (FT.)	WIDTH "B2" (FT.)	SAWCUT OFFSET FROM ℄ (FT.)	③	④	⑤	⑥	SLOPE 1	SLOPE 2
62+53	RT.	4.0'	2.8'	38.0'	723.17	723.51	723.58	723.99	+4.00%	+6.83%



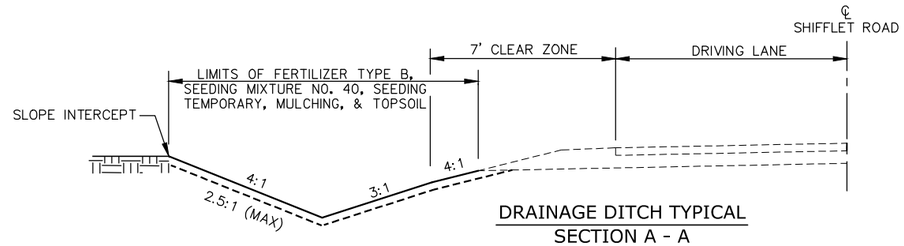
SIDEWALK DETAIL

NOTE: ELEVATIONS ARE TO ℄ OF SIDEWALK

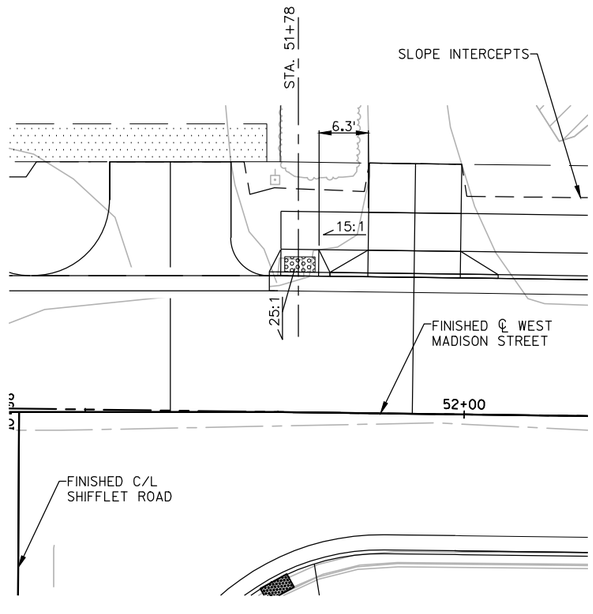


SECTION A-A

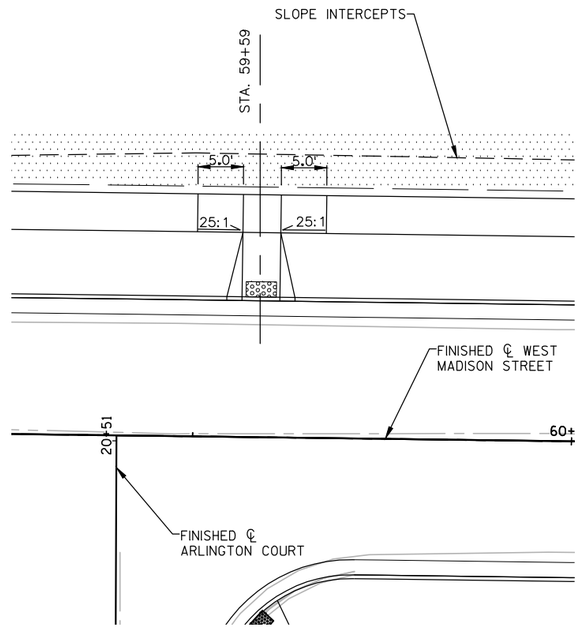
DRAINAGE DITCH DETAIL



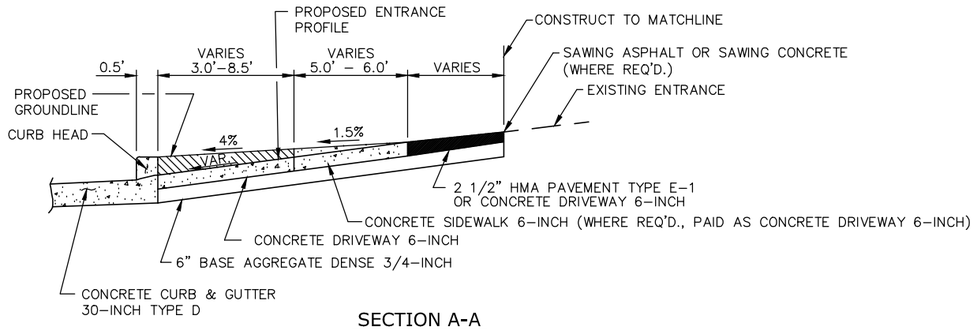
DRAINAGE DITCH TYPICAL SECTION A - A



SIDEWALK DETAIL
STA. 51+78, LT.

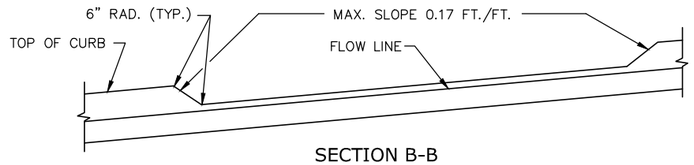


SIDEWALK DETAIL
STA. 59+59, LT.

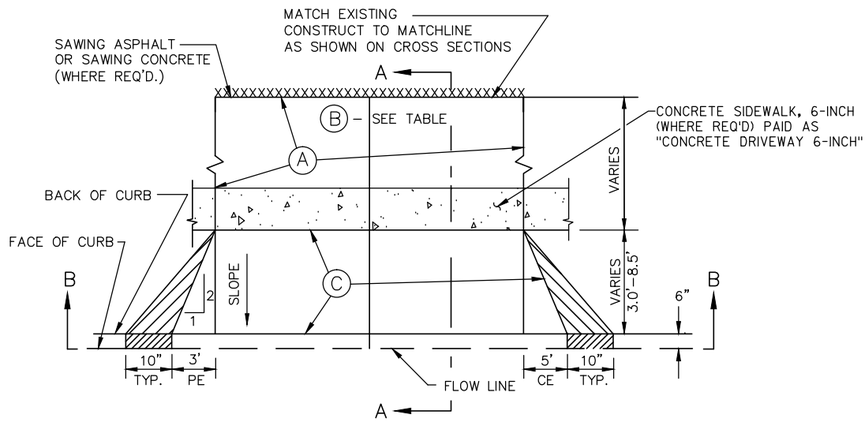


SECTION A-A

(AT CONCRETE SIDEWALK)



SECTION B-B



DRIVEWAY DETAIL (URBAN)

(WHERE REQ'D.)

ENTRANCE DATA TABLE

STATION	LOCATION	TYPE	"A" PAVEMENT STRUCTURE	"B" WIDTH	MATCH EXISTING AT
51+61	LT.	P.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	16.0'	16.0'
51+93	LT.	C.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	12.2'	33.0'
54+73	LT.	C.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	VARIES 10.5'-12.0'	33.0'
55+84	LT.	C.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	12.0'	33.0'
56+61	LT.	C.E.	NONE - PROVIDED FOR FUTURE EXPANSION OF CEMETARY	12.0'	33.0'
56+95	RT.	P.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	13.1'	40.0'
57+18	RT.	P.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	VARIES 8.6'-12.0'	40.0'
57+72	LT.	C.E.	NONE - PROVIDED FOR FUTURE EXPANSION OF CEMETARY	12.0'	33.0'
58+31	RT.	P.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	VARIES 11.7'-12.0'	37.0'
61+56	RT.	P.E.	CONCRETE DRIVEWAY 6-INCH OVER 6" BAD 3/4-INCH	VARIES 13.0'-13.2'	42.0'
61+69	LT.	C.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	VARIES 15.8'-16.0'	37.0'
61+81	RT.	P.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	VARIES 14.0'-14.2'	42.0'

Ⓐ

EXISTING	PROPOSED
BASE AGGREGATE DENSE 3/4-INCH	2 1/2" HMA PAVEMENT E-1 OVER 6" BASE AGGREGATE DENSE 3/4-INCH
ASPHALT	2 1/2" HMA PAVEMENT TYPE E-1 OVER 6" BASE AGGREGATE DENSE 3/4-INCH
CONCRETE	CONCRETE DRIVEWAY 6-INCH OVER 6" BASE AGGREGATE DENSE 3/4-INCH

Ⓑ WIDTH

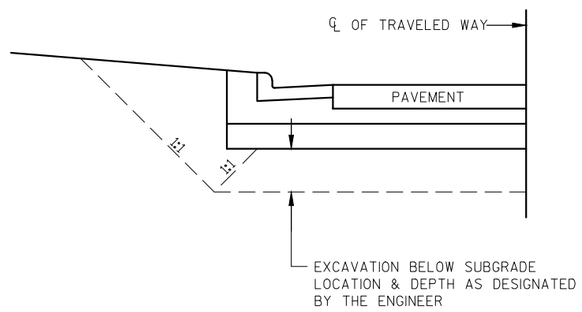
P.E.'S – MATCH EXISTING OR 12' MIN. – 24' MAX.

C.E.'S – MATCH EXISTING OR 35' MAX.

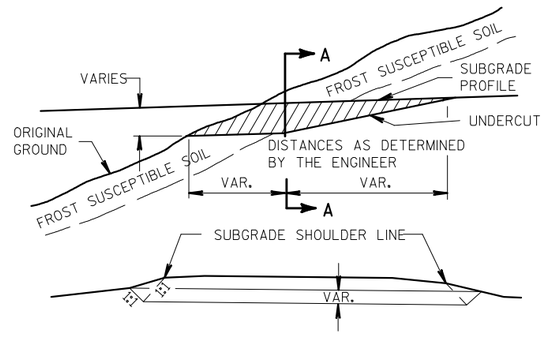
Ⓒ

EXISTING	PROPOSED
ALL TYPES	CONCRETE DRIVEWAY 6-INCH OVER 6" BASE AGGREGATE DENSE 3/4-INCH

NOTE: APRON WIDTH SHALL BE 3.0' AT STA. 51+61 LT.



DETAIL FOR EXCAVATION BELOW SUBGRADE



SECTION A-A

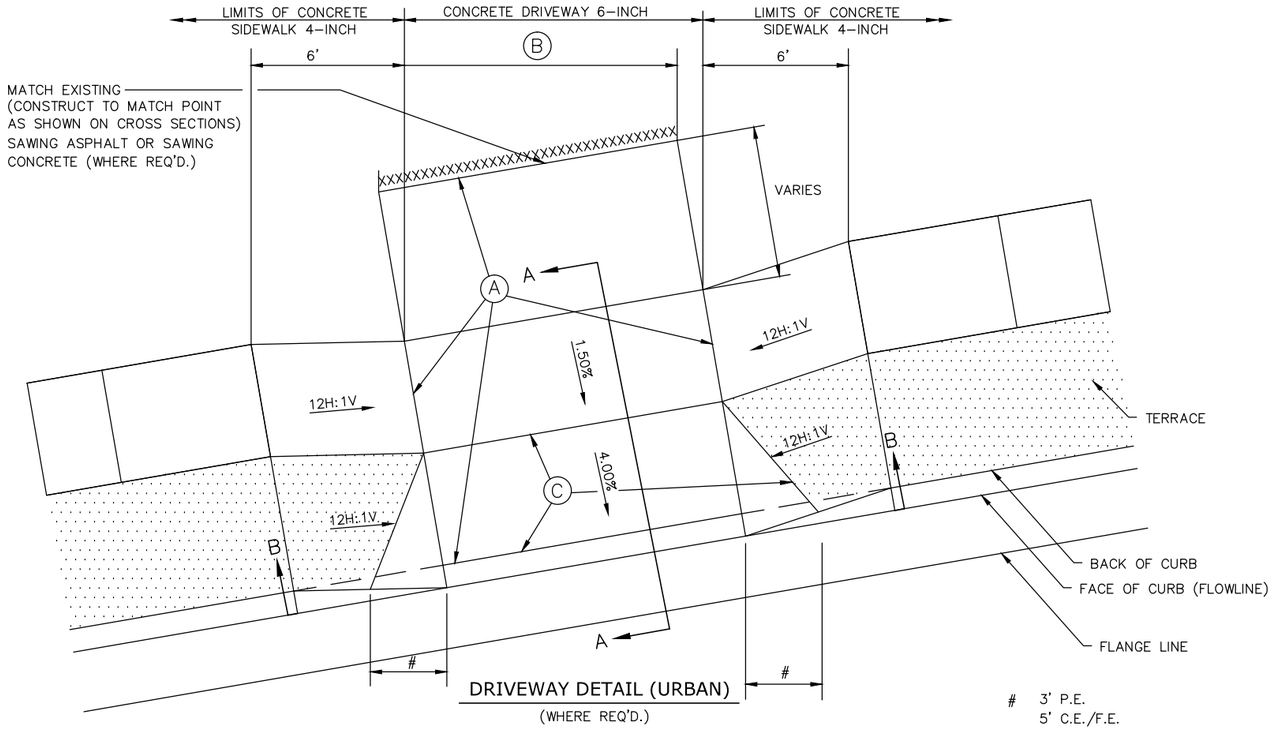
CROSS SECTION SHOWING UNDERCUT

DETAIL FOR EXCAVATION BELOW SUBGRADE AT CUTS

NOTE: EXACT LOCATIONS AND EXTENT OF E.B.S. SECTIONS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

E.B.S. AREA TO BE BACKFILLED WITH BREAKER RUN.

THE FILL SECTION WITHIN 100' OF THE MOUTH OF THE CUT MUST BE KEPT 2' BELOW SUBGRADE UNTIL E.B.S. IS COMPLETED.



DRIVEWAY DETAIL (URBAN)
(WHERE REQ'D.)

\# 3' P.E.
5' C.E./F.E.

ENTRANCE DATA TABLE

STATION	LOCATION	TYPE	(A) PAVEMENT STRUCTURE	(B) WIDTH	MATCH EXISTING AT
52+67	RT.	P.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	12.0'	53.0'
53+92	RT.	P.E.	CONCRETE DRIVEWAY 6-INCH OVER 6" BAD 3/4-INCH	VARIES 12.0'-21.9'	39.0'
55+06	RT.	P.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	19.3'	37.0'
55+37	RT.	P.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	VARIES 10.8'-12.0'	37.0'
60+26	RT.	P.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	VARIES 10.5'-12.0'	37.0'
61+05	LT.	F.E.	2 ½" HMA PAVEMENT TYPE E-1 OVER 6" BAD 3/4-INCH	35.0'	37.0'

(A)

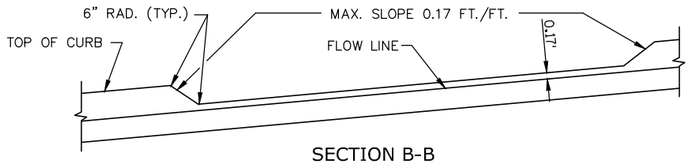
EXISTING	PROPOSED
BASE AGGREGATE DENSE 3/4-INCH	2 1/2" HMA PAVEMENT E-1 OVER 6" BASE AGGREGATE DENSE 3/4-INCH
ASPHALT	2 1/2" HMA PAVEMENT TYPE E-1 OVER 6" BASE AGGREGATE DENSE 3/4-INCH
CONCRETE	CONCRETE DRIVEWAY 6-INCH OVER 6" BASE AGGREGATE DENSE 3/4-INCH

(B) WIDTH

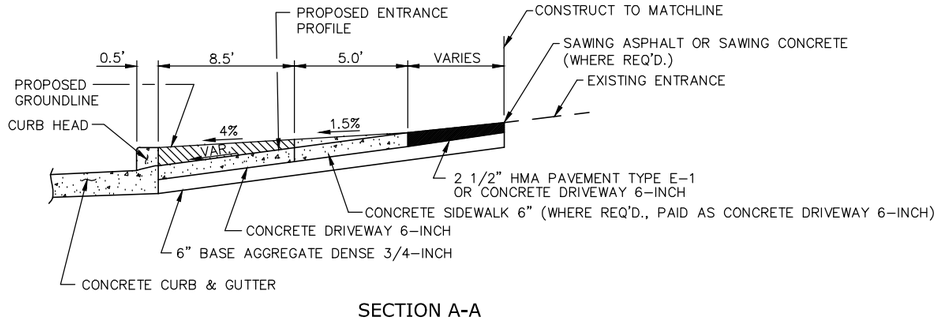
P.E.'S – MATCH EXISTING OR 12' MIN. – 24' MAX.
C.E.'S – MATCH EXISTING OR 35' MAX.
F.E.'S – MATCH EXISTING OR 35' MAX.

(C)

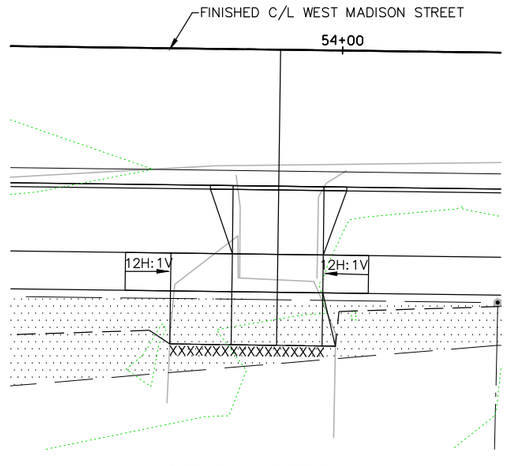
EXISTING	PROPOSED
ALL TYPES	CONCRETE DRIVEWAY 6-INCH OVER 6" BASE AGGREGATE DENSE 3/4-INCH



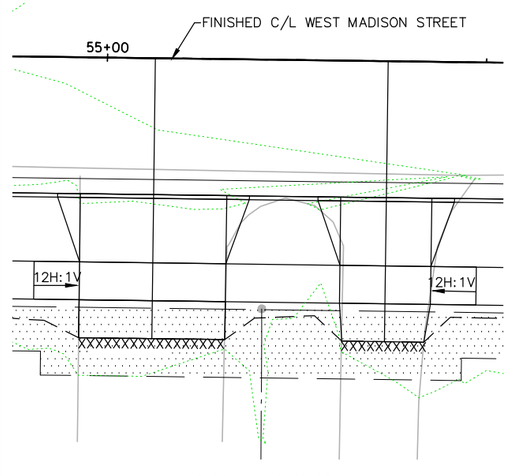
SECTION B-B



SECTION A-A
(AT CONCRETE SIDEWALK)

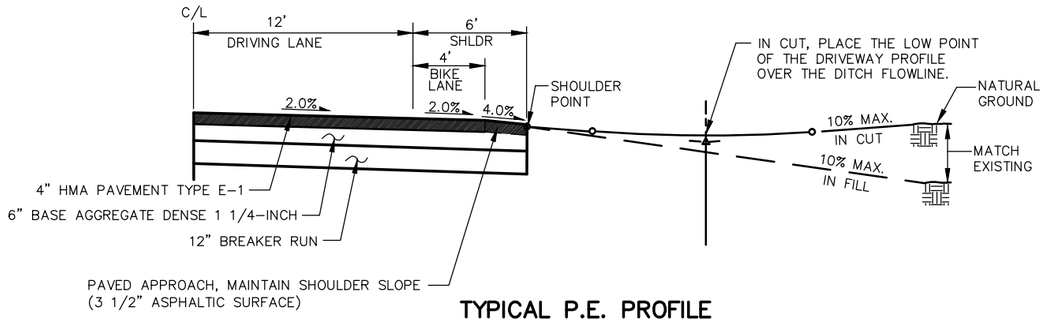


DRIVEWAY DETAIL
STATION 53+92, RT.

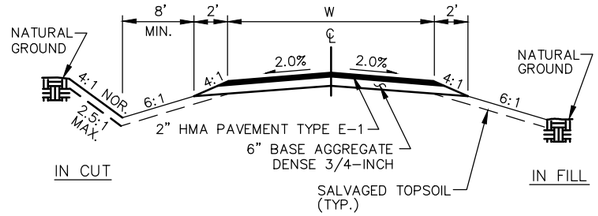


DRIVEWAY DETAIL
STATION 55+06, RT.
STATION 55+37, RT.

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CONSTRUCTION DETAILS	SHEET 11	E

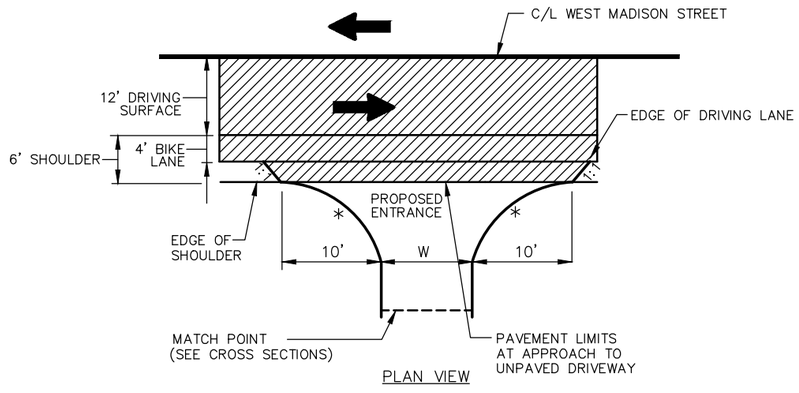


TYPICAL P.E. PROFILE



TYPICAL CROSS-SECTION FOR P.E.

STATION	W (FT)
51+19, LT.	15
51+61, LT.	16



APPROACH AT P.E.

TYPICAL PRIVATE ENTERANCE (P.E.) DETAILS

LIMITS OF ASPHALTIC SURFACE

* RADIUS = 10' (UNLESS OTHERWISE NOTED)

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CONSTRUCTION DETAILS	SHEET --- 12	E

FILE NAME : S:±Projects±S34083 Village of SG Jefferson Street±CADD Files±S34084 Madison CADD±S34084_Rural Driveway Detail.dwg

2

LEGEND

XXXXX SAWING ASPHALT OR SAWING CONCRETE

CURB RAMP DETECTABLE WARNING FIELD – TYPE 7B

CURB RAMP DETECTABLE WARNING FIELD – TYPE 1

FLOW ARROW

TRAFFIC DIRECTION

N

STA. 51+19, LT.
CONSTRUCT P.E. (ASPHALT)

STA. 51+61, LT.
CONSTRUCT P.E. (ASPHALT)

STA. 51+93, LT.
CONSTRUCT C.E. (ASPHALT)

LIMITS OF CEMETERY

SLOPE INTERCEPTS

STA. 51+41.24 (WEST MADISON STREET)=
STA. 10+98.39 (SHIFFLET ROAD)
TYPE D (MODIFIED) INTERSECTION REQ'D

CONCRETE SIDEWALK
4-INCH (TYP.)

BEGIN PROJECT
STA. 50+00

STA. 50+00
18.2' LT.

FINISHED ℄
WEST MADISON
STREET

STA. 50+93
16.0' LT.

R=5'

720.76

720.88

721.41

WEST MADISON ST.

PI STA = 50+93.03

50+00

N88°51'47"E

51+00

S89°59'52"E

52+00

S89°15'03"E

178°51'39"

90°15'21"

10.98

N0°15'29"E

PI STA = 51+75.43

720.86

720.88

721.41

720.47

20:1

721.06

720.59

STA. 50+00
11.3' RT.

93'

STA. 50+50, RT.
20.2' RT.

STA. 50+50, RT.
BEGIN SPECIAL DITCH
EL. = 719.92

R = 32.0'

A

B

720.11
16'

720.11
16'

STA. 10+50.17
48.0' LT.

Y = 129,686.21
X = 561,200.90

STA. 50+92.07
48.0' RT.

PI STA = 10+49.97

STA. 10+50.42
48.0' RT.

Y = 129,686.02
X = 561,296.90

STA. 51+89.66
48.0' RT.

CONCRETE CURB & GUTTER
30-INCH TYPE D (TYP.)

SPECIAL DITCH
(GRADE TO DRAIN)

FINISHED ℄ SHIFFLET ROAD

SHIFFLET ROAD
N4°31'26"W

PI STA = 10+00.00

STA. 10+00
13.3' RT.

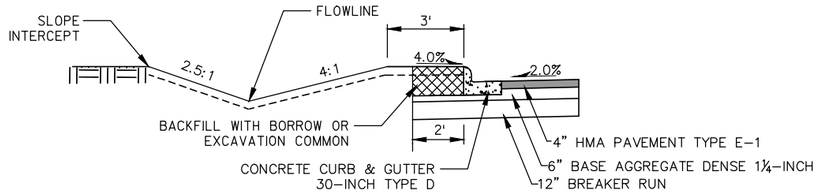
STA. 10+00
12.0' LT.

MATCH EXISTING AND SAWING ASPHALT REQ'D.
STA. 10+00

STA. 10+00, LT.
END SPECIAL DITCH
EL. = 718.83

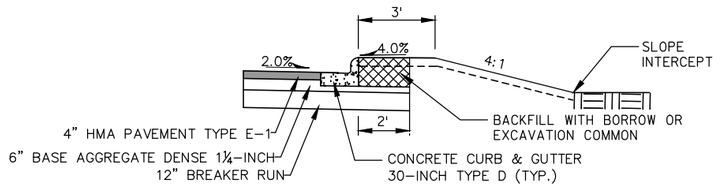
BUILDING

WEST MADISON STREET & SHIFFLET ROAD



SECTION A-A

(BERM DETAIL BEHIND CONCRETE CURB & GUTTER WITH DITCH)

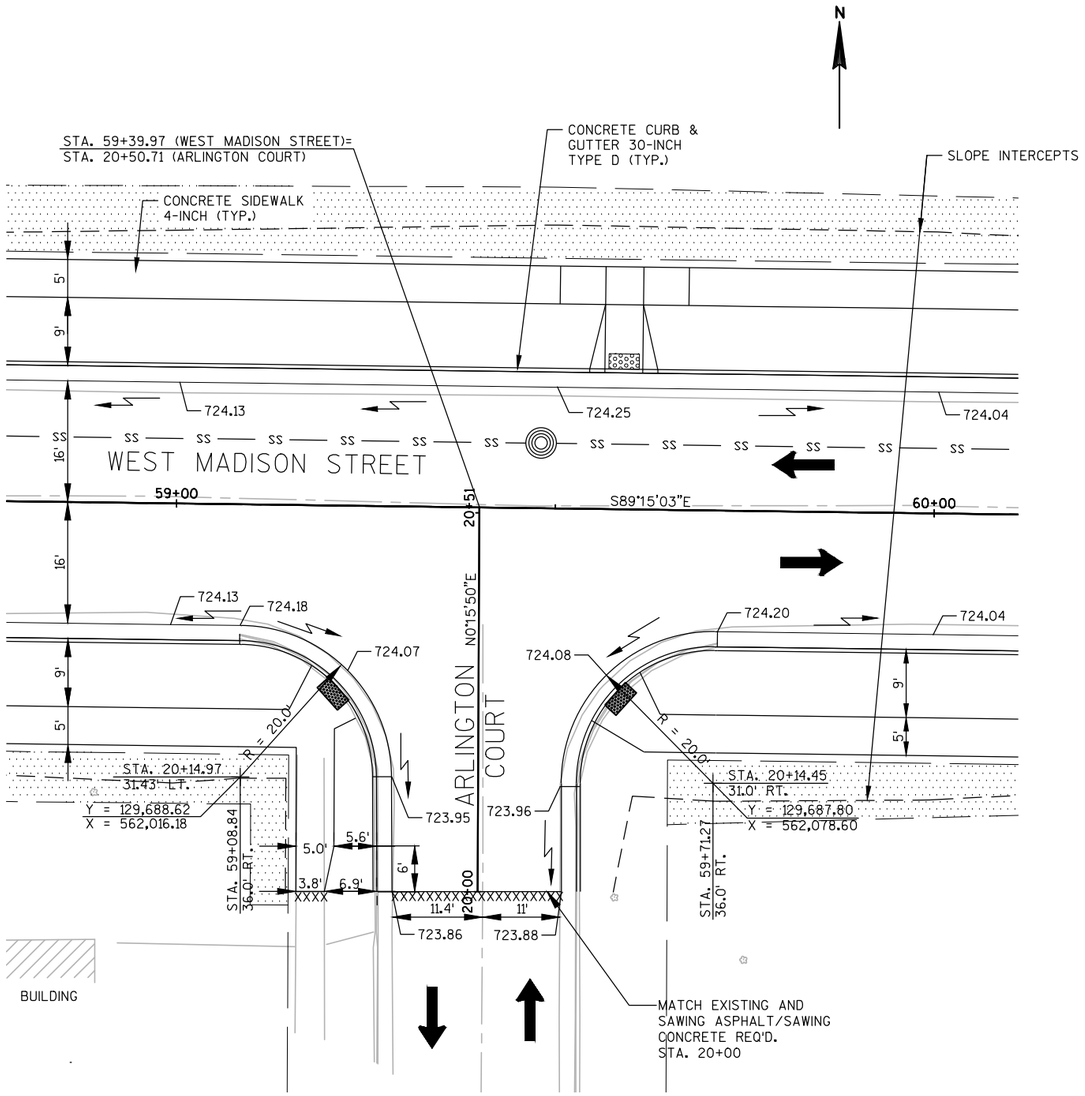


SECTION B-B

(BERM DETAIL BEHIND CONCRETE CURB & GUTTER WITHOUT DITCH)

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	INTERSECTION DETAILS	SHEET -- 13	E

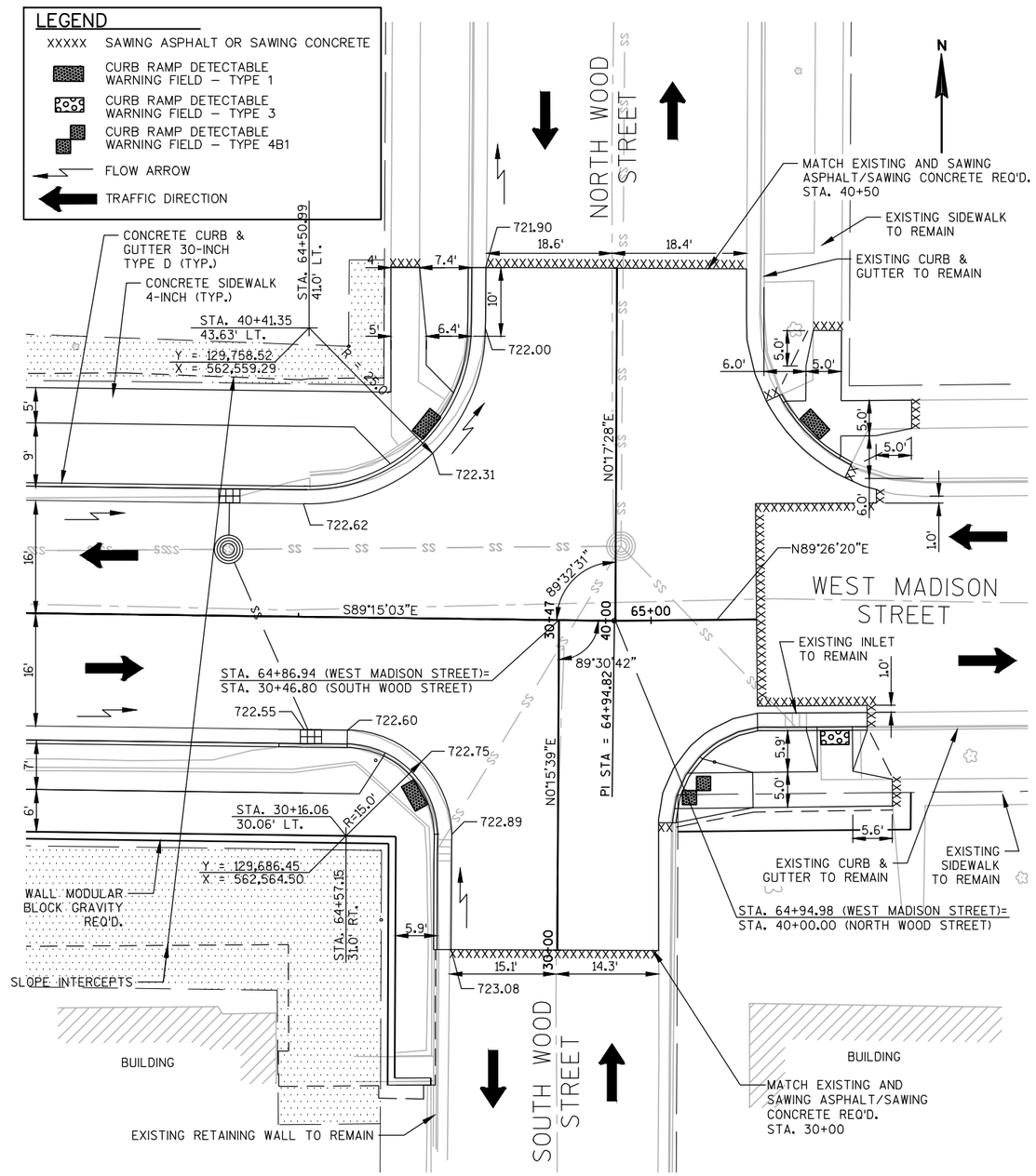
S:\Projects\S34083 Village of SG Jefferson Street\CADD Files\S34084 Madison CADD\Intersection Details\Madison Street Intersection Details.dwg



WEST MADISON STREET & ARLINGTON COURT

LEGEND

XXXXX	SAWING ASPHALT OR SAWING CONCRETE
	CURB RAMP DETECTABLE WARNING FIELD – TYPE 7B
	CURB RAMP DETECTABLE WARNING FIELD – TYPE 1
	FLOW ARROW
	TRAFFIC DIRECTION



WEST MADISON STREET & WOOD STREET

2

NOTE: CONSTRUCT DRIVEWAY AT STA. 51+19, LT. SO THAT STORMWATER IN DITCH FLOWS OVER DRIVEWAY TO I-111.

NOTE: P-111, P-112, AND P-113, SHALL BE CLASS IV RCP

ROSE M - JEFFERY N ANDERSON & HUTCHINSON

ST JOHNS CATHOLIC CONG

LIMITS OF CEMETERY

N

REMOVE EXISTING INLET

I-111
STA:51+41.00 OFF:23.00LT
RIM:719.60

I-110
STA:51+83.50 OFF:16.00LT
RIM:720.79

FRONTIER COMMUNICATIONS (TYP.)

SLOPE INTERCEPTS

FINISHED ℄ WEST MADISON STREET

R/W

P-113

P-112

PI STA = 51+75.43

50+00

N88°51'47"E

51+00

S89°59'52"E

52+00

53+00

54+00

55+00

S89°15'03"E

56+00

MATCHLINE STA. 56+00

REMOVE EXISTING STORM SEWER

I-112
STA:51+86.53 OFF:16.16RT
RIM:720.85

I-113
STA:52+99.72 OFF:16.00RT
RIM:722.15

P-116

P-117

PI STA = 50+93.03

N01°15'29"E

P-115

I-108
STA:51+59.74 OFF:35.40RT
RIM:720.30

I-114
STA:53+00.33 OFF:52.92RT
RIM:719.80

PHILWALD PARTNERS LLC

P-111

PI STA = 10+49.97

REMOVE EXISTING INLET AND STORM SEWER

CHARTER COMMUNICATIONS (TYP.)

ALLIANT ENERGY (TYP.)

VILLAGE OF SPRING GREEN (TYP.)

ALLIANT ENERGY (TYP.)

CAUTION

BEGIN PROJECT
STA. 50+00

CONCRETE ENDWALL
STA. 51+67.76
OFF. 70.6' RT.

N04°31'26"W

SHIFFLET ROAD

WALTER E & HANNELORE ALLEN

H JAMES & MURIEL A KLOTH

JAMES M BRICKL

GORDON E & MARY J NEEFE

NOTE: P-111, P-112, AND P-113, SHALL BE CLASS IV RCP

I-111
P-113: 717.98

I-110
P-113: 717.81
P-112: 717.71

I-112
P-116: 717.58
P-112: 717.58
P-115: 717.48

P-113

42' of 12" RCP @ 0.40%

P-112

33' of 15" RCP @ 0.38%

CONCRETE ENDWALL

INV. = 717.18

P-111

35' of 15" RCP @ 0.27%

I-108
P-115: 717.38
P-111: 717.28

P-115

32' of 15" RCP @ 0.30%

I-112
P-116: 717.58
P-112: 717.58
P-115: 717.48

P-116

113' of 12" RCP @ 0.30%

I-113
P-117: 718.02
P-116: 717.92

I-114
P-117: 718.13

P-117

36' of 12" RCP @ 0.30%

726
724
722
720
718
716
714
712
710

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	STORM SEWER PLAN	SHEET --15

COUNCIL OF SPRING GREEN
ST JOHNS CATHOLIC CHURCH

CARDINAL ST

N

MATCHLINE STA. 56+00

MATCHLINE STA. 62+00

I-104
STA:56+58.06 OFF:16.00LT
RIM:722.61

I-105
STA:56+68.05 OFF:16.00LT
RIM:722.61

P-108

STMH-103
STA:56+68.05 OFF:-8.66LT
RIM:722.81

P-107

P-110

P-109

I-107
STA:56+58.05 OFF:16.00RT
RIM:722.61

I-106
STA:56+68.05 OFF:16.00RT
RIM:722.60

FINISHED ℄ WEST MADISON STREET

FRONTIER COMMUNICATIONS (TYP.)

ALLIANT ENERGY (TYP.)

CAUTION

P-102

STMH-102
STA:59+48.02 OFF:-8.58LT
RIM:724.40

VILLAGE OF SPRING GREEN

P-101

N0°15'50"E

S89°15'03"E

20.51

200.00

R/W

VILLAGE OF SPRING GREEN (TYP.)

SLOPE INTERCEPTS ALLIANT ENERGY (TYP.)

CHARTER COMMUNICATIONS (TYP.)

WILLARD NEEFE

JANET GEIGER

CAROL OLSON

CHADD A MAXWELL

JOHN R & SUZANNE R SHARP

DAVID J & DONNA DRUCKREY

SAUK COUNTY HOUSING AUTH

EXISTING ℄ PROFILE
WEST MADISON STREET

FINISHED ℄ PROFILE
WEST MADISON STREET

P-102 280' of 24" RCP @ 0.50%

P-101 280' of 24" RCP @ 0.50%

STMH-103
P-108: 718.50
P-109: 718.50
P-102: 717.50

STMH-102
P-102: 716.10
P-101: 716.00

EXISTING ℄ ELEVATION
FINISHED ℄ ELEVATION

726 724 722 720 718 716 714 712 710

56+00	56+20	56+40	56+60	56+80	57+00	57+20	57+40	57+60	57+80	58+00	58+20	58+40	58+60	58+80	59+00	59+20	59+40	59+60	59+80	60+00	60+20	60+40	60+60	60+80	61+00	61+20	61+40	61+60	61+80	62+00
723.51	723.63	723.75	723.86	723.97	724.07	724.13	724.20	724.26	724.32	724.39	724.48	724.57	724.60	724.61	724.62	724.61	724.61	724.55	724.46	724.36	724.31	724.25	724.18	724.10	724.04	724.00	723.93	723.84	723.77	723.71
723.24	723.13	723.04	722.99	722.99	723.05	723.16	723.30	723.45	723.59	723.74	723.88	724.03	724.17	724.32	724.45	724.54	724.58	724.56	724.48	724.36	724.23	724.10	723.96	723.83	723.70	723.57	723.44	723.31	723.19	723.11

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	STORM SEWER PLAN	SHEET 16

COUNCIL OF SPRING GREEN
ST JOHNS CATHOLIC CHURCH

CARDINAL ST

N

MATCHLINE STA. 56+00

MATCHLINE STA. 62+00

I-104
STA:56+58.06 OFF:16.00LT
RIM:722.61

I-105
STA:56+68.05 OFF:16.00LT
RIM:722.61

P-108

STMH-103
STA:56+68.05 OFF:-8.66LT
RIM:722.81

P-107

P-109

P-110

I-107
STA:56+58.05 OFF:16.00RT
RIM:722.61

I-106
STA:56+68.05 OFF:16.00RT
RIM:722.60

FINISHED ℄ WEST
MADISON STREET

FRONTIER
COMMUNICATIONS
(TYP.)

ALLIANT
ENERGY
(TYP.)

CAUTION

STMH-102
STA:59+48.02 OFF:-8.58LT
RIM:724.40

P-102

P-101

57+00

58+00

59+00

60+00

61+00

S89°15'03"E

N0°15'50"E

20.51

200.00

R/W

CHARTER
COMMUNICATIONS
(TYP.)

VILLAGE OF SPRING
GREEN (TYP.)

ALLIANT ENERGY (TYP.)

CAUTION

WILLARD
NEEFE

JANET
GEIGER

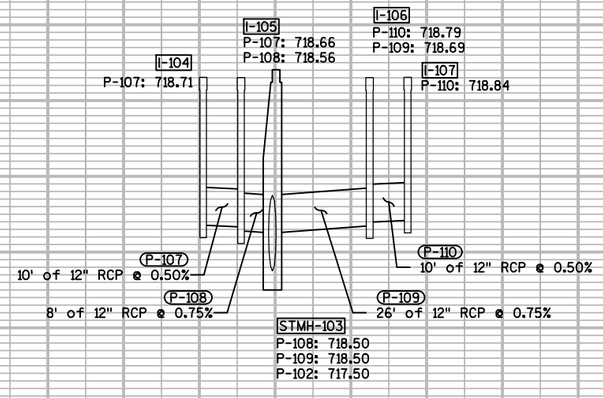
CAROL
OLSON

CHADD A
MAXWELL

JOHN R & SUZANNE R
SHARP

DAVID J & DONNA
DRUCKREY

SAUK COUNTY
HOUSING AUTH



726 724 722 720 718 716 714 712 710

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	STORM SEWER PLAN	SHEET 17

2

CARDINAL ST

CAUTION

ALLIANT ENERGY (TYP.)

FRONTIER COMMUNICATIONS (TYP.)

VILLAGE OF SPRING GREEN (TYP.)

STA. 63+49, 7.6' LT.
ADJUST MANHOLE COVER
RIM = 723.16
(EXISTING = 723.37)

EX STMH-100
ADJUSTING MANHOLE COVERS
STA:63+48.89 OFF:-7.63LT
RIM:723.16

CONNECT TO EXISTING MANHOLE INCIDENTAL TO REINFORCED CONCRETE PIPE STORM SEWER

I-100
STA:62+18.12 OFF:16.00LT
RIM:722.70

I-101
STA:62+28.00 OFF:16.00LT
RIM:722.69

I-102
STA:62+28.20 OFF:16.00RT
RIM:722.69

I-103
STA:62+18.19 OFF:16.00RT
RIM:722.68

STMH-101
STA:62+28.00 OFF:-8.50LT
RIM:722.90

I-116
STA:64+40.00 OFF:16.00LT
RIM:722.60

I-115
STA:64+52.00 OFF:16.00RT
RIM:722.55

STMH-104
STA:64+40.00 OFF:-9.40LT
RIM:722.79

P-100 P-101 P-103 P-104 P-105 P-106 P-118 P-119 EX P-101 EX P-102 EX-P103

MATCHLINE STA. 62+00

63+00 64+00 65+00

S89°15'03"E

N89°26'20"E

N01°17'28"E

N01°15'39"E 30.47

PI STA = 64+94.82

30.00 40.00

ASAP PROPERTIES LLC

STA. 64+96, 10.6' LT.
ADJUST MANHOLE COVER
RIM = 722.54
(EXISTING = 722.86)

ADJUSTING MANHOLE COVERS

ADJUST EXISTING INLET
RIM = 722.95
(EXISTING = 722.75)

SAUK COUNTY HOUSING AUTH

HUGH C & PHYLLIS A STEELE

RICHARD A & PERRY L KNEEDLER

CHARTER COMMUNICATIONS (TYP.)

SLOPE INTERCEPTS

FINISHED ℄ WEST MADISON STREET

R/W

END PROJECT
STA. 65+15

NOTE: CONTRACTOR MUST VERIFY SIZES AND INVERTS ON EX P-101 & EX P-102

N

EXISTING ℄ PROFILE
WEST MADISON STREET

FINISHED ℄ PROFILE
WEST MADISON STREET

P-100 121' of 24" RCP @ 0.65%

STMH-101
P-104: 717.50
P-105: 717.50
P-101: 714.60
P-100: 714.50

EX STMH-100
P-100: 713.71
EX P-101: 710.27
EX P-102: 710.27

EX P-102

EX-P103

STMH-104
P-119: 717.50
P-118: 717.50
EX P-102: 710.58
EX-P103: 710.60

EXISTING 48" STORM SEWER PIPE TO REMAIN

EXISTING ℄ ELEVATION

FINISHED ℄ ELEVATION

I-116
P-119: 717.54

I-115
P-118: 717.64

P-119 8' of 12" RCP @ 0.50%

P-118 29' of 12" RCP @ 0.50%

STMH-104
P-119: 717.50
P-118: 717.50
EX P-102: 710.58
EX-P103: 710.60

I-100
P-103: 717.69

I-101
P-103: 717.64
P-104: 717.54

I-102
P-106: 717.73
P-105: 717.63

I-103
P-106: 717.78

P-103 10' of 12" RCP @ 0.50%

P-104 9' of 15" RCP @ 0.50%

P-105 26' of 12" RCP @ 0.50%

P-106 10' of 12" RCP @ 0.50%

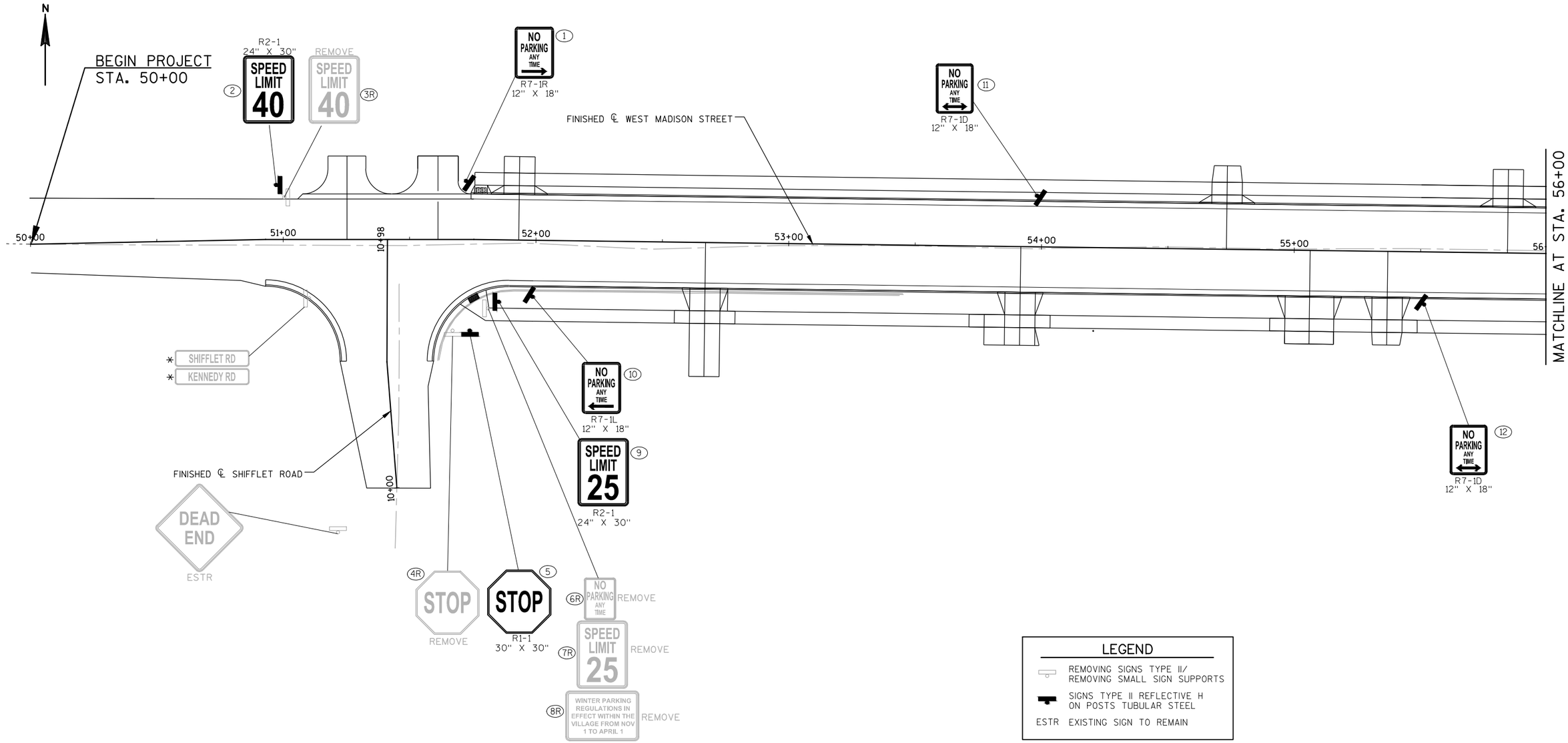
STMH-101
P-104: 717.50
P-105: 717.50
P-101: 714.60
P-100: 714.50

722 720 718 716 714 712 710 708 706

723.71 723.11 | 723.69 723.08 | 723.66 723.08 | 723.61 723.13 | 723.54 723.22 | 723.47 723.29 | 723.39 723.33 | 723.30 723.33 | 723.24 723.29 | 723.19 723.22 | 723.08 723.14 | 723.06 723.06 | 723.05 722.98 | 722.97 722.90 | 722.84 722.82 | 722.77 722.74 | 722.66 | 722.63 | 722.53 | 722.40 | 722.27 | 722.14 | 721.97 | 722.06

62+00 62+20 62+40 62+60 62+80 63+00 63+20 63+40 63+60 63+80 64+00 64+20 64+40 64+60 64+80 65+00 65+20 65+40 65+60 65+80 66+00 66+20 66+40 66+60 66+80 67+00 67+20 67+40 67+60 67+80 68+00

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	STORM SEWER PLAN	SHEET -- 18



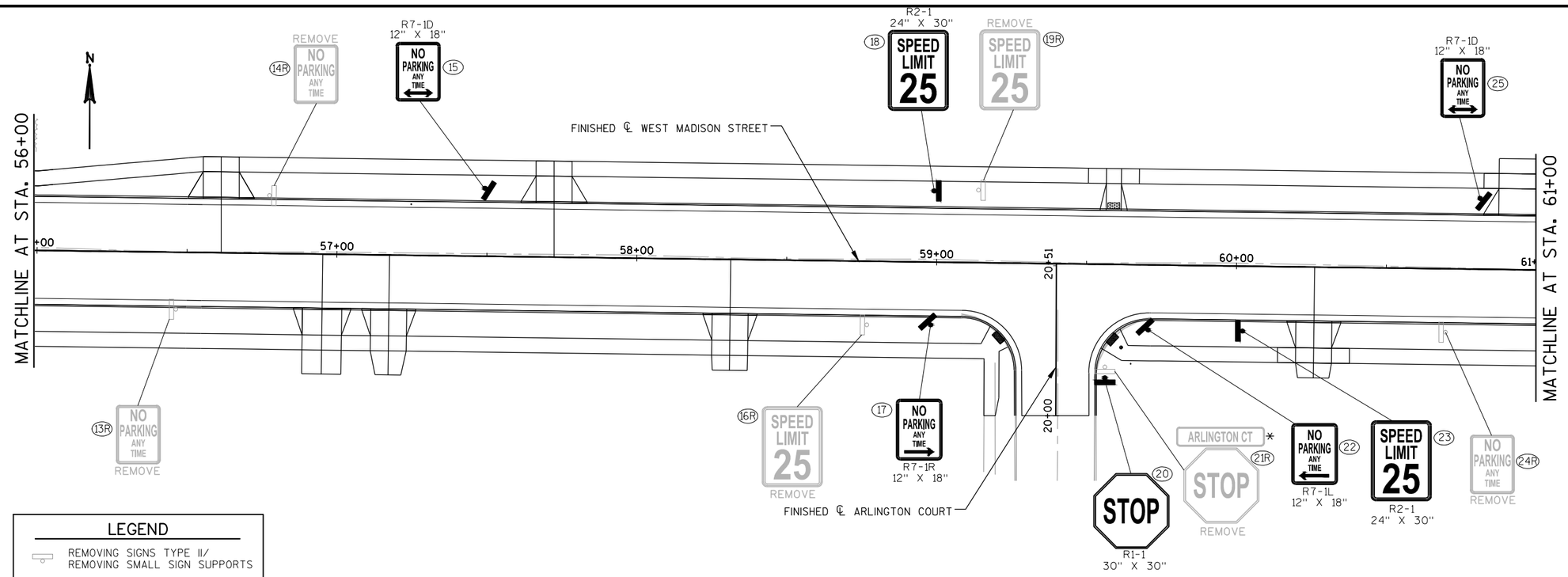
LEGEND

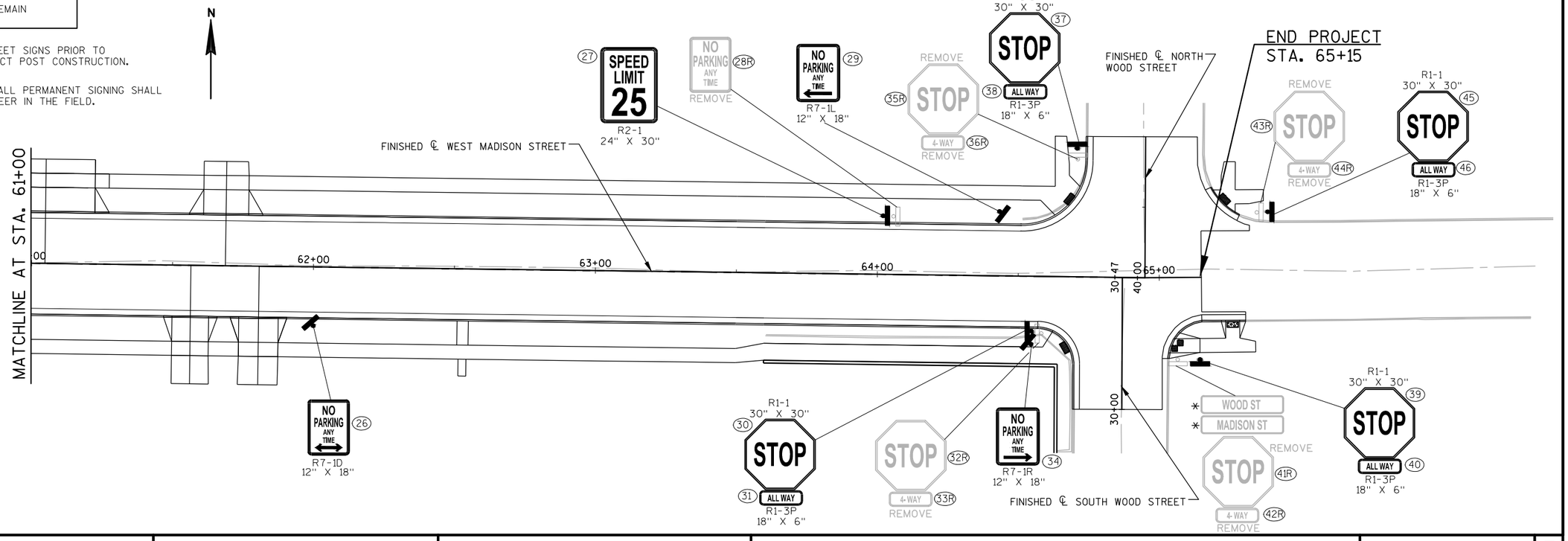
- REMOVING SIGNS TYPE II/ REMOVING SMALL SIGN SUPPORTS
- SIGNS TYPE II REFLECTIVE H ON POSTS TUBULAR STEEL
- ESTR EXISTING SIGN TO REMAIN

* VILLAGE WILL REMOVE STREET SIGNS PRIOR TO CONSTRUCTION AND RE-ERECT POST CONSTRUCTION.

NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

2





LEGEND

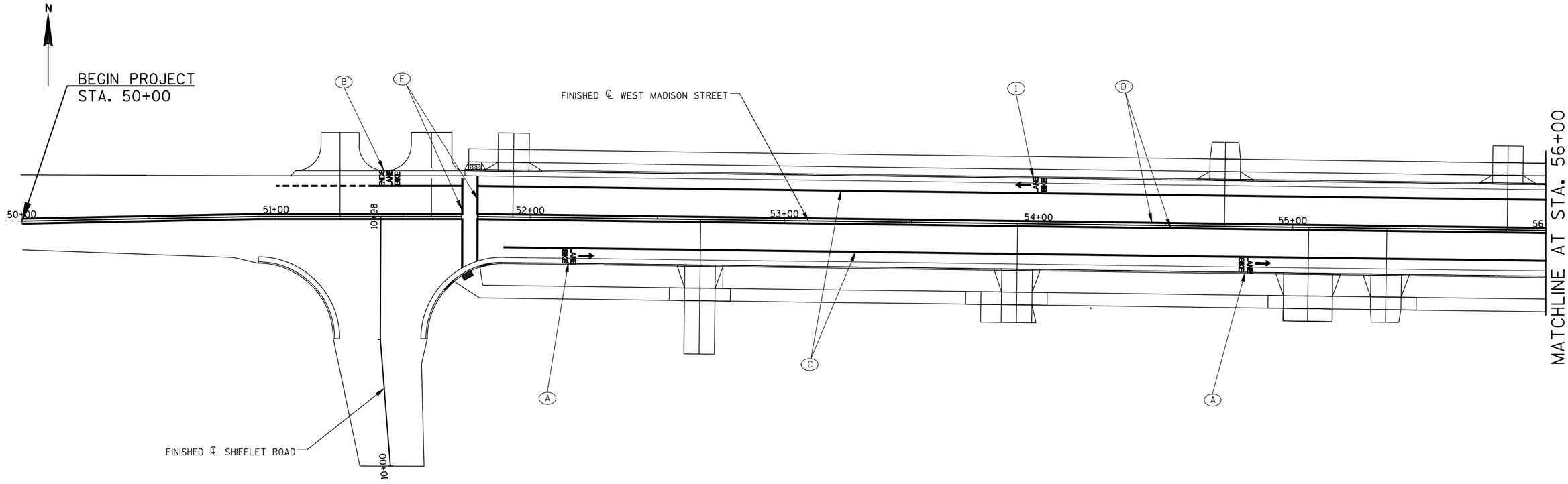
- REMOVING SIGNS TYPE II/ REMOVING SMALL SIGN SUPPORTS
- SIGNS TYPE II REFLECTIVE H ON POSTS TUBULAR STEEL
- ESTR EXISTING SIGN TO REMAIN

* VILLAGE WILL REMOVE STREET SIGNS PRIOR TO CONSTRUCTION AND RE-ERECT POST CONSTRUCTION.

NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	PERMANENT SIGNING	SHEET -- 20	E

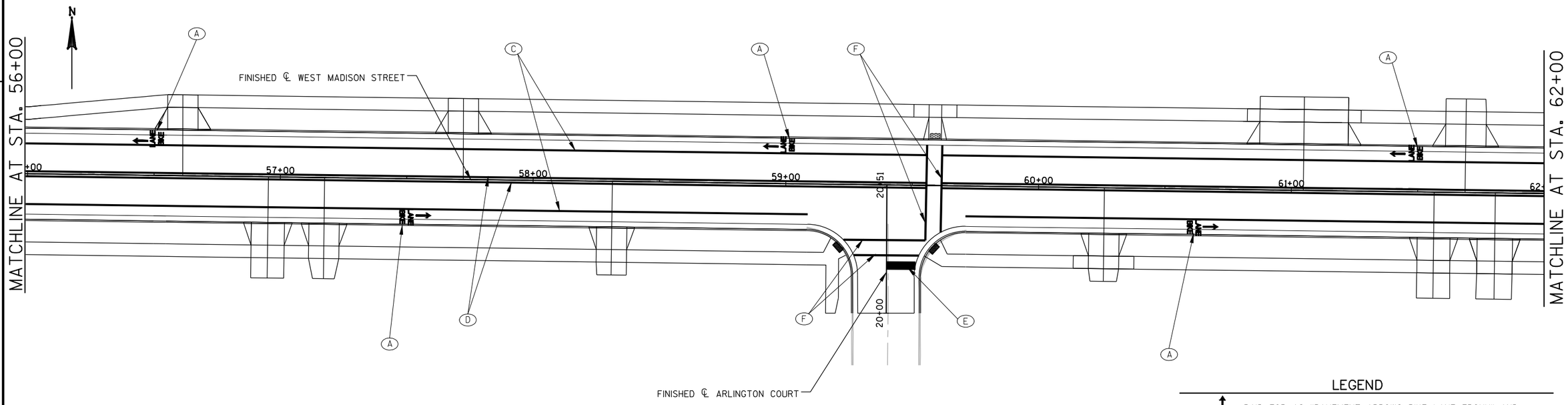
S:\Projects\S34083 Village of SQ Jefferson Street\CADD Files\S34084 Madison CADD\Madison Street Permanent Signing.dwg



LEGEND

Ⓐ	LANE BIKE	PAID FOR AS "PAVEMENT ARROWS BIKE LANE EPOXY" AND "PAVEMENT MARKING WORDS BIKE LANE" (SEE MISCELLANEOUS QUANTITIES)
Ⓑ	ENDS LANE BIKE	PAID FOR AS "PAVEMENT MARKING WORDS BIKE LANE"
Ⓒ		PAVEMENT MARKING 4-INCH EPOXY (WHITE EDGELINE)
Ⓓ		PAVEMENT MARKING 4-INCH EPOXY (DOUBLE YELLOW)
Ⓔ		PAVEMENT MARKING STOP LINE EPOXY 18-INCH
Ⓕ		PAVEMENT MARKING CROSSWALK EPOXY 6-INCH

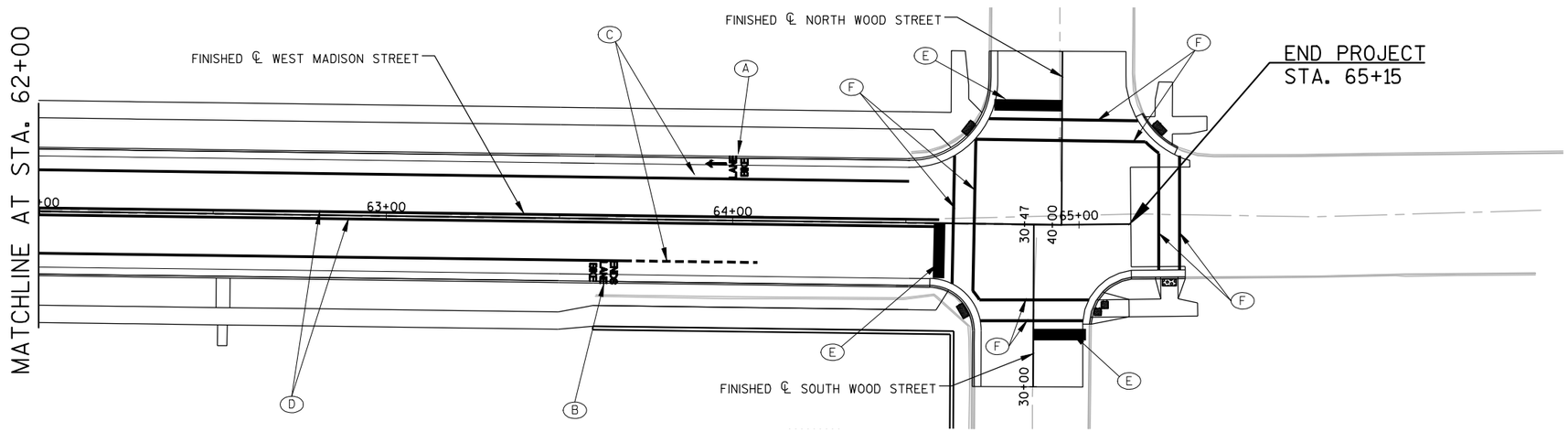
NOTE: THE FINAL LOCATION OF PAVEMENT MARKING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.



LEGEND

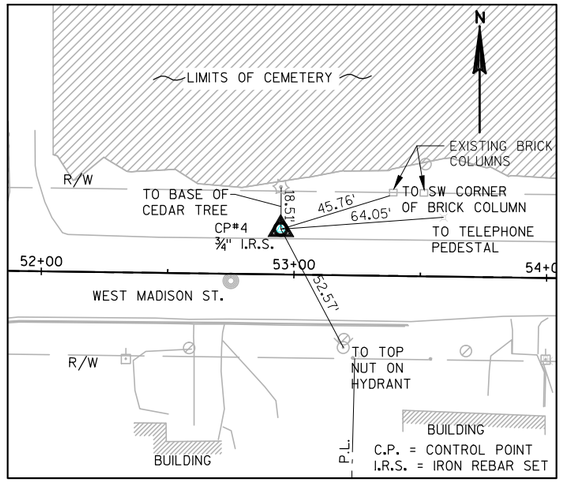
- (A) LANE BIKE — PAID FOR AS "PAVEMENT ARROWS BIKE LANE EPOXY" AND "PAVEMENT MARKING WORDS BIKE LANE" (SEE MISCELLANEOUS QUANTITIES)
- (B) ENDS LANE BIKE — PAID FOR AS "PAVEMENT MARKING WORDS BIKE LANE"
- (C) PAVEMENT MARKING 4-INCH EPOXY (WHITE EDGELINE)
- (D) PAVEMENT MARKING 4-INCH EPOXY (DOUBLE YELLOW)
- (E) PAVEMENT MARKING STOP LINE EPOXY 18-INCH
- (F) PAVEMENT MARKING CROSSWALK EPOXY 6-INCH

NOTE: THE FINAL LOCATION OF PAVEMENT MARKING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

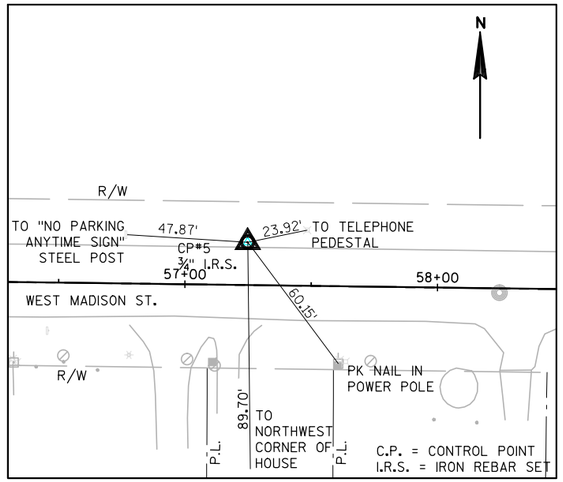


PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	PAVEMENT MARKING	SHEET -- 22	E

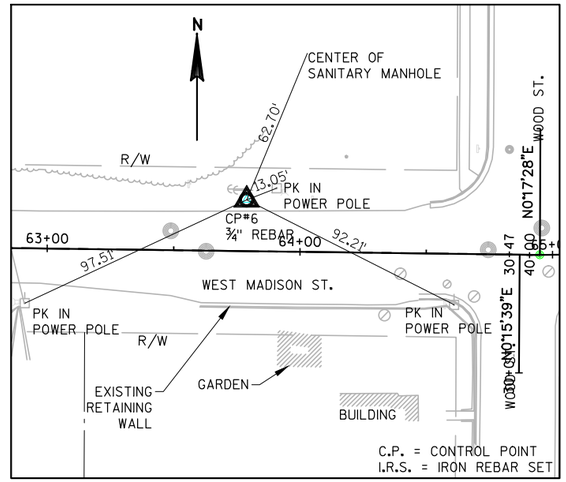
S:\Projects\S34083 Village of SG Jefferson Street\CADD Files\S34084 Madison CADD\Madison Street Pavement Marking.dwg



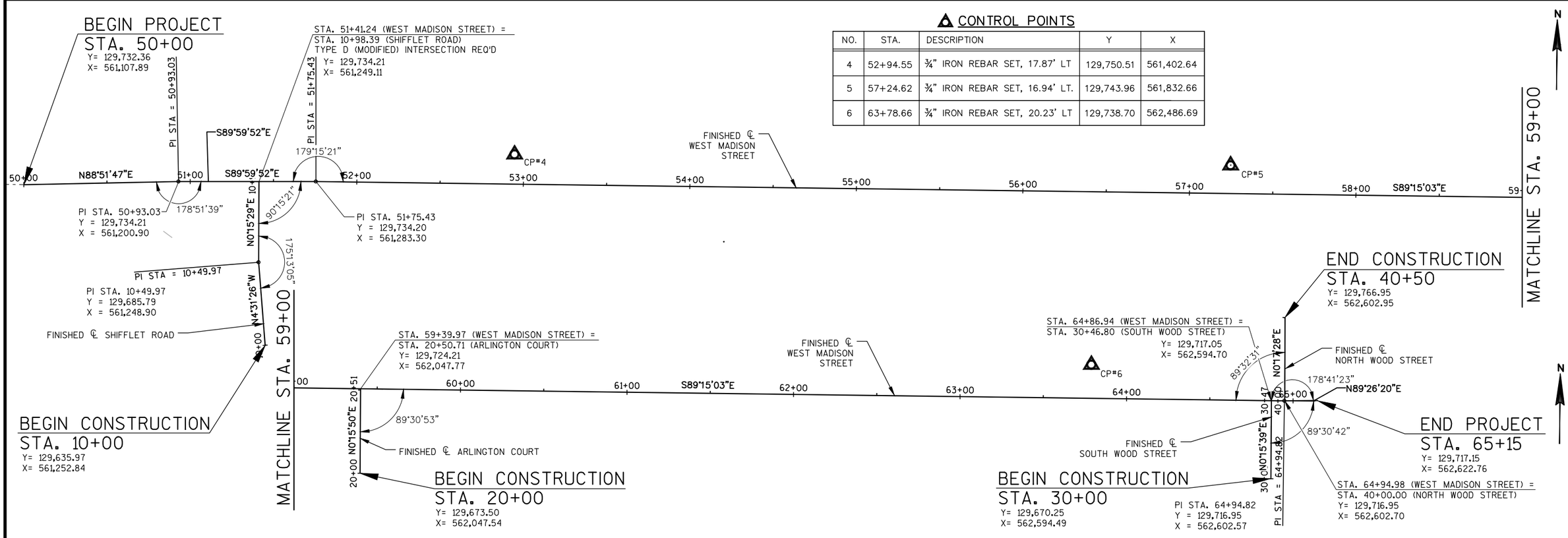
TIES TO CP#4
STA. 52+94.55, 17.87' LT
Y=129,750.51
X=561,402.64



TIES TO CP#5
STA. 57+24.62, 16.94' LT
Y=129,743.96
X=561,832.66



TIES TO CP#6
STA. 63+78.66, 20.23' LT
Y=129,738.70
X=562,486.69



▲ CONTROL POINTS

NO.	STA.	DESCRIPTION	Y	X
4	52+94.55	¾" IRON REBAR SET, 17.87' LT	129,750.51	561,402.64
5	57+24.62	¾" IRON REBAR SET, 16.94' LT.	129,743.96	561,832.66
6	63+78.66	¾" IRON REBAR SET, 20.23' LT	129,738.70	562,486.69

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	TIES & ALIGNMENT PLAN	SHEET 23	E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\CADD FILES\S34084 MADISON CADD\MADISON ALIGNMENT AND TIE DIAGRAMS.DWG PLOT DATE : 12/19/2013 8:40 AM PLOT BY : BALLWEG,THOMAS

WEST MADISON STREET STATION LAYOUT

Station	Y	X	Remarks
50+00	129,732.36'	561,107.89'	Begin Project
50+50	129,733.35'	561,157.88'	-
51+00	129,734.21'	561,207.87'	-
51+50	129,734.21'	561,257.87'	-
52+00	129,733.88'	561,307.87'	-
52+50	129,733.23'	561,357.86'	-
53+00	129,732.58'	561,407.86'	-
53+50	129,731.92'	561,457.85'	-
54+00	129,731.27'	561,507.85'	-
54+50	129,730.61'	561,557.85'	-
55+00	129,729.96'	561,607.84'	-
55+50	129,729.31'	561,657.84'	-
56+00	129,728.65'	561,707.83'	-
56+50	129,728.00'	561,757.83'	-
57+00	129,727.35'	561,807.82'	-
57+50	129,726.69'	561,857.82'	-
58+00	129,726.04'	561,907.82'	-
58+50	129,725.38'	561,957.81'	-
59+00	129,724.73'	562,007.81'	-
59+50	129,724.08'	562,057.80'	-
60+00	129,723.42'	562,107.80'	-
60+50	129,722.77'	562,157.79'	-
61+00	129,722.11'	562,207.79'	-
61+50	129,721.46'	562,257.79'	-
62+00	129,720.81'	562,307.78'	-
62+50	129,720.15'	562,357.78'	-
63+00	129,719.50'	562,407.77'	-
63+50	129,718.85'	562,457.77'	-
64+00	129,718.19'	562,507.76'	-
64+50	129,717.54'	562,557.76'	-
65+00	129,717.00'	562,607.76'	-
65+15	129,717.15'	562,622.76'	End Project

SHIFFLET ROAD STATION LAYOUT

Station	Y	X	Remarks
10+00	129,635.97	561,252.84	Begin Construction
10+50	129,685.82	561,248.90	-
10+82.39	129,718.21	561,249.04	End Construction

ARLINGTON COURT STATION LAYOUT

Station	Y	X	Remarks
20+00	129,673.50	562,047.54	Begin Construction
20+34.71	129,708.21	562,047.70	End Construction

SOUTH WOOD STREET STATION LAYOUT

Station	Y	X	Remarks
30+00	129,670.25	562,594.49	Begin Construction
30+30.80	129,701.05	562,594.63	End Construction

NORTH WOOD STREET STATION LAYOUT

Station	Y	X	Remarks
40+16	129,732.95	562,602.78	Begin Construction
40+50	129,766.95	562,602.95	End Construction

2

DATE 28APR14 E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	5916-00-72 QUANTITY
0010	201.0105	CLEARING	STA	2.000	2.000
0030	201.0205	GRUBBING	STA	3.000	3.000
0050	203.0100	REMOVING SMALL PIPE CULVERTS	EACH	1.000	1.000
0060	204.0100	REMOVING PAVEMENT	SY	40.000	40.000
0070	204.0150	REMOVING CURB & GUTTER	LF	430.000	430.000
0080	204.0155	REMOVING CONCRETE SIDEWALK	SY	65.000	65.000
0100	204.0220	REMOVING INLETS	EACH	2.000	2.000
0110	204.0245	REMOVING STORM SEWER (SIZE) 01. 12-INCH	LF	92.000	92.000
0140	204.9090.S	REMOVING (ITEM DESCRIPTION) 01. WALL MODULAR BLOCK GRAVITY	LF	155.000	155.000
0150	205.0100	EXCAVATION COMMON	CY	5,900.000	5,900.000
0170	213.0100	FINISHING ROADWAY (PROJECT) 02. 5916-00-72	EACH	1.000	1.000
0180	305.0110	BASE AGGREGATE DENSE 3/4-INCH	TON	960.000	960.000
0190	305.0120	BASE AGGREGATE DENSE 1 1/4-INCH	TON	2,650.000	2,650.000
0200	311.0110	BREAKER RUN	TON	5,600.000	5,600.000
0210	416.0160	CONCRETE DRIVEWAY 6-INCH	SY	395.000	395.000
0220	440.4410.S	INCENTIVE IRI RIDE	DOL	1,148.000	1,148.000
0230	455.0105	ASPHALTIC MATERIAL PG58-28	TON	49.000	49.000
0240	455.0120	ASPHALTIC MATERIAL PG64-28	TON	41.000	41.000
0250	455.0605	TACK COAT	GAL	160.000	160.000
0270	460.1101	HMA PAVEMENT TYPE E-1	TON	1,500.000	1,500.000
0280	460.2000	INCENTIVE DENSITY HMA PAVEMENT	DOL	960.000	960.000
0290	520.0118	CULVERT PIPE CLASS III 18-INCH	LF	22.000	22.000
0300	522.1015	APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 15-INCH	EACH	1.000	1.000
0310	532.0200.S	WALL MODULAR BLOCK GRAVITY	SF	575.000	575.000
0320	601.0411	CONCRETE CURB & GUTTER 30-INCH TYPE D	LF	2,800.000	2,800.000
0340	602.0405	CONCRETE SIDEWALK 4-INCH	SF	12,630.000	12,630.000
0350	602.0505	CURB RAMP DETECTABLE WARNING FIELD YELLOW	SF	80.000	80.000
0370	608.0312	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 12-INCH	LF	286.000	286.000
0380	608.0315	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 15-INCH	LF	41.000	41.000
0390	608.0324	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 24-INCH	LF	681.000	681.000
0410	608.0412	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 12-INCH	LF	42.000	42.000
0420	608.0415	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 15-INCH	LF	68.000	68.000
0430	611.0535	MANHOLE COVERS TYPE J-SPECIAL	EACH	4.000	4.000
0440	611.0639	INLET COVERS TYPE H-S	EACH	14.000	14.000
0450	611.0645	INLET COVERS TYPE MS-A	EACH	2.000	2.000
0460	611.2004	MANHOLES 4-FT DIAMETER	EACH	4.000	4.000
0490	611.2007	MANHOLES 7-FT DIAMETER	EACH	1.000	1.000
0500	611.3230	INLETS 2X3-FT	EACH	13.000	13.000
0510	611.3901	INLETS MEDIAN 1 GRATE	EACH	2.000	2.000
0520	611.8110	ADJUSTING MANHOLE COVERS	EACH	2.000	2.000
0530	611.8115	ADJUSTING INLET COVERS	EACH	1.000	1.000
0540	619.1000	MOBILIZATION	EACH	0.420	0.420
0550	624.0100	WATER	MGAL	67.000	67.000
0560	625.0100	TOPSOIL	SY	5,200.000	5,200.000
0570	627.0200	MULCHING	SY	5,200.000	5,200.000
0580	628.1504	SILT FENCE	LF	1,250.000	1,250.000

3

DATE 28APR14 E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	5916-00-72 QUANTITY
0590	628.1520	SILT FENCE MAINTENANCE	LF	2,500.000	2,500.000
0600	628.1905	MOBILIZATIONS EROSION CONTROL	EACH	5.000	5.000
0610	628.1910	MOBILIZATIONS EMERGENCY EROSION CONTROL	EACH	3.000	3.000
0620	628.7005	INLET PROTECTION TYPE A	EACH	16.000	16.000
0630	628.7015	INLET PROTECTION TYPE C	EACH	16.000	16.000
0640	628.7504	TEMPORARY DITCH CHECKS	LF	25.000	25.000
0650	629.0210	FERTILIZER TYPE B	CWT	3.000	3.000
0660	630.0140	SEEDING MIXTURE NO. 40	LB	100.000	100.000
0670	630.0200	SEEDING TEMPORARY	LB	50.000	50.000
0710	633.5200	MARKERS CULVERT END	EACH	1.000	1.000
0720	634.0814	POSTS TUBULAR STEEL 2X2-INCH X 14-FT	EACH	18.000	18.000
0730	634.0816	POSTS TUBULAR STEEL 2X2-INCH X 16-FT	EACH	4.000	4.000
0740	637.2210	SIGNS TYPE II REFLECTIVE H	SF	75.580	75.580
0750	638.2602	REMOVING SIGNS TYPE II	EACH	20.000	20.000
0760	638.3000	REMOVING SMALL SIGN SUPPORTS	EACH	14.000	14.000
0770	642.5001	FIELD OFFICE TYPE B	EACH	0.500	0.500
0790	643.0100	TRAFFIC CONTROL (PROJECT) 02. 5916-00-72	EACH	1.000	1.000
0810	646.0106	PAVEMENT MARKING EPOXY 4-INCH	LF	5,340.000	5,340.000
0830	647.0206	PAVEMENT MARKING ARROWS BIKE LANE EPOXY	EACH	9.000	9.000
0840	647.0406	PAVEMENT MARKING WORDS BIKE LANE EPOXY	EACH	24.000	24.000
0860	647.0566	PAVEMENT MARKING STOP LINE EPOXY 18-INCH	LF	62.000	62.000
0870	647.0766	PAVEMENT MARKING CROSSWALK EPOXY 6-INCH	LF	515.000	515.000
0900	650.4000	CONSTRUCTION STAKING STORM SEWER	EACH	21.000	21.000
0910	650.4500	CONSTRUCTION STAKING SUBGRADE	LF	1,700.000	1,700.000
0920	650.5000	CONSTRUCTION STAKING BASE	LF	1,700.000	1,700.000
0930	650.5500	CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER	LF	2,800.000	2,800.000
0940	650.6000	CONSTRUCTION STAKING PIPE CULVERTS	EACH	1.000	1.000
0960	650.9910	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) 02. 5916-00-72	LS	1.000	1.000
0970	650.9920	CONSTRUCTION STAKING SLOPE STAKES	LF	1,700.000	1,700.000
0980	690.0150	SAWING ASPHALT	LF	280.000	280.000
0990	690.0250	SAWING CONCRETE	LF	85.000	85.000
1090	SPV.0060	SPECIAL 10. REESSTABLISH SECTION CORNER MONUMENT	EACH	1.000	1.000

3

3

CLEARING & GRUBBING

STATION - STATION	LOCATION	201.0105 CLEARING (STA)	201.0205 GRUBBING (STA)
* 62+00 - 64+00	MAINLINE, LT.	2	2
** 10+00 - 10+05	SHIFFLET RD, RT.	-	1
	TOTALS =	2	3

* INCLUDES STUMP REMOVAL AT STA. 63+60, RT. AND STA. 64+42, RT.
INCLUDES CONIFEROUS TREE REMOVAL AT STA. 63+75, RT.
** INCLUDES BRUSH/TREE REMOVAL STA. 56+43 - STA. 56+75, RT.

REMOVING PAVEMENT

STATION - STATION	LOCATION	204.0100 (SY)
53+77 - 54+00	MAINLINE	22
61+50 - 61+63	MAINLINE	18
	TOTAL =	40

REMOVING CURB & GUTTER

STATION - STATION	LOCATION	204.0150 (LF)
51+60 - 53+46	MAINLINE	203
20+00 - 20+35	ARLINGTON COURT, RT.	44
20+00 - 20+35	ARLINGTON COURT, LT.	44
30+00 - 30+31	SOUTH WOOD STREET, LT.	46
40+36 - 40+50	NORTH WOOD STREET, LT.	43
65+01 - 65+15	MAINLINE, RT.	35
65+17 - 65+28	MAINLINE, LT.	15
	TOTAL =	430

ALL BID ITEMS ARE CATEGORY 010 UNLESS OTHERWISE NOTED

REMOVING CONCRETE SIDEWALK

STATION - STATION	LOCATION	204.0155 (SY)
20+00 - 20+18	ARLINGTON COURT, LT.	8
62+53	MAINLINE	8
64+65	MAINLINE	14
* 29+83	SOUTH WOOD STREET, LT.	10
65+17 - 65+28	MAINLINE	15
65+24 - 65+34	MAINLINE	10
	TOTAL =	65

3

REMOVING SMALL PIPE CULVERTS

LOCATION	203.0100 (EACH)	REMARKS
P.E. - SHIFFLET ROAD, RT.	1	15" CMP, 17 LF
TOTAL	1	

EARTHWORK SUMMARY

CATEGORY	FROM/TO STA	LOCATION	(1) 205.0100 COMMON EXCAVATION CUT (2) (CY)	EBS (3) (CY)	SALVAGED/ UNUSABLE PAVEMENT MATERIAL (CY) (4)	AVAILABLE MATERIAL (CY) (5)	205.0400 MARSH EXCAVATION (CY) (6)	205.0200 ROCK EXCAVATION (CY) (7)	REDUCED MARSH IN FILL (CY) FACTOR 0.6 (8)	REDUCED EBS IN FILL (CY) FACTOR 0.8 (9)	EXPANDED MARSH BACKFILL (CY) FACTOR 1.5 (10)	EXPANDED EBS BACKFILL (CY) FACTOR 1.5 (11)	EXPANDED ROCK (CY) FACTOR 1.1 (12)	UNEXPANDED FILL (CY)	EXPANDED FILL (CY) FACTOR 1.25 (13)	MASS ORDINATE +/- (CY) (14)	WASTE (CY)	208.0100 BORROW (CY)	COMMENT:
010	50+00 - 65+15	MAINLINE	5010	-	-	5010	-	-	-	-	-	-	-	255	319	4691	4691	-	
	10+00 - 10+82.39	SHIFFLET ROAD	385	-	-	385	-	-	-	-	-	-	-	10	13	372	372	-	
	20+00 - 20+34.71	ARLINGTON COURT	100	-	-	100	-	-	-	-	-	-	-	0	0	100	100	-	
	30+00 - 30+30.80	SOUTH WOOD STREET	80	-	-	80	-	-	-	-	-	-	-	0	0	80	80	-	
	40+16 - 40+50	NORTH WOOD STREET	115	-	-	115	-	-	-	-	-	-	-	0	0	115	115	-	
	-	P.E., F.E., C.E.	210			210								6	8	202	202		
	TOTALS =		5900			5900								271	340	5560	5560		

REMOVING INLETS

STATION	LOCATION	204.0220 REMOVING INLETS (EACH)
51+63	36.6' RT	1
51+77	18.3' LT	1
TOTAL		2

NOTES:
1.) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100
2.) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT
3.) EBS EXCAVATION TO BE BACKFILLED WITH SELECT CRUSHED MATERIAL.
4.) SALVAGED/UNUSABLE PAVEMENT MATERIAL
5.) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
6.) MARSH EXCAVATION - TO BE BACKFILLED WITH SELECT CRUSHED MATERIAL. ITEM 205.0400
7.) ROCK EXCAVATION. ITEM NUMBER 205.0200
8.) REDUCED MARSH IN FILL - EXCAVATED MARSH MATERIAL IS USABLE IN FILLS OUTISDE THE 1:1 SLOPE. MARSH IN FILL REDUCTION FACTOR = 0.6
9.) REDUCED EBS IN FILL - EXCAVATED EBS MATERIAL IS USEABLE IN FILLS OUTISDE 1:1 SLOPE. EBS IN FILL REDUCTION FACTOR = 0.8
10) EXPANDED MARSH BACKFILL - THIS IS TO BE FILLED WITH SELECT CRUSHED MATERIAL. MARSH BACKFILL FACTOR = 1.5. ITEM NUMBER 312.0115
11.)EXPANDED EBS BACKFILL - THIS IS TO BE FILLED WITH SELECT CRUSHED MATERIAL. EBS BACKFILL FACTOR = 1.3. ITEM NUMBER 312.0115
12.) EXPANDED ROCK FACTOR = 1.1
13.) EXPANDED FILL FACTOR 1.25: EXPANDED FILL = (UNEXPANDED FILL - REDUCED MARSH IN FILL)*1.25
14.) THE MASS ORDINATE+ OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE CATEGORY. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE CATEGORY.

REMOVING STORM SEWER

FROM STATION	FROM LOCATION	TO STATION	TO LOCATION	REMOVING STORM SEWER 204.0245 12-INCH (L.F.)
51+63	36.6' RT	51+67	70.6' RT	35
51+63	36.6' RT	51+77	18.3' LT	57
			TOTAL	92

REMOVING WALL MODULAR BLOCK GRAVITY

STATION - STATION	LOCATION	204.9090.S (LF)
63+60 - 64+64/	MAINLINE, RT./	155
29+81 - 30+16	SOUTHWOOD STREET, LT.	
	TOTAL =	155

BASE AGGREGATE DENSE

STATION - STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH (TON)	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH (TON)	311.0110 BREAKER RUN (TON)
* 50+00 - 65+15	MAINLINE	-	2337	4992
50+00 - 50+93	MAINLINE, RT. (SHOULDER)	13	-	-
50+00 - 51+50	MAINLINE, LT. (SHOULDER)	8	-	-
51+69 - 64+69	MAINLINE, RT. (SIDEWALK)	346	-	-
51+72 + 64+72	MAINLINE, LT. (SIDEWALK)	361	-	-
65+03 - 65+07	MAINLINE, RT.	12	-	-
65+19 - 65+27	MAINLINE, LT.	8	-	-
10+00 - 10+50	SHIFFLET ROAD (SHOULDER)	8	-	-
10+00 - 10+82.39	SHIFFLET ROAD	-	128	256
20+00 - 20+19	ARLINGTON COURT (SIDEWALK)	6	-	-
20+00 - 20+34.71	ARLINGTON COURT	-	41	82
-	P.E., F.E., C.E.	144	-	-
-	UNDISTRIBUTED	54	144	270
	TOTALS =	960	2650	5600

*SOUTH WOOD STREET AND NORTH WOOD STREET QUANTITIES ARE INCLUDED IN THIS QUANTITY.

CONCRETE DRIVEWAY 6-INCH

STATION	LOCATION	416.0160 (SY)
51+93	C.E. - MAINLINE, LT.	13
52+67	P.E. - MAINLINE, RT.	21
53+92	P.E. - MINLINE, RT.	37
54+73	C.E. - MAINLINE, LT.	13
55+06	P.E. - MAINLINE, RT.	32
55+37	P.E. MAINLINE, RT.	21
55+84	C.E. MAINLINE, LT.	13
56+61	C.E. MAINLINE, LT.	16
56+95	P.E. - MAINLINE, RT.	23
57+18	P.E. - MAINLINE, RT.	21
57+72	C.E. MAINLINE, LT.	16
58+31	P.E. - MAINLINE, RT.	21
60+26	P.E. - MAINLINE, RT.	21
61+05	F.E. - MAINLINE, LT.	36
61+56	P.E. - MAINLINE, RT.	37
61+69	C.E. - MAINLINE, LT.	30
61+81	P.E. - MAINLINE, RT.	24
	TOTAL =	395

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\CADD FILES\S34084 MADISON CADD\MQ.DWG PLOT DATE: 2/3/2014 8:26 AM PLOT BY: BALLWEG,THOMAS

ALL BID ITEMS ARE CATEGORY 010 UNLESS OTHERWISE NOTED

HMA PAVEMENT

STATION - STATION	LOCATION	455.0105 ASPHALTIC MATERIAL PG 58-28 (TON)	455.0120 ASPHALTIC MATERIAL PG 64-28 (TON)	455.0605 TACK COAT (GAL)	460.1101 HMA PAVEMENT TYPE E-1 (TON)
* 50+00 - 65+15	MAINLINE	44	34	141	1,297
10+00 - 10+82.39	SHIFFLET ROAD	3	2	8	77
20+00 - 20+34.71	ARLINGTON COURT	1	1	3	25
-	P.E., F.E., C.E.	-	3	-	37
-	UNDISTRIBUTED	1	1	8	64
	TOTALS =	49	41	160	1,500

*SOUTH WOOD STREET AND NORTH WOOD STREET QUANTITIES ARE INCLUDED IN THIS QUANTITY.

CONCRETE SIDEWALK 4-INCH

STATION - STATION	LOCATION	602.0405 (SF)	REMARKS
51+72 - 64+72	MAINLINE, LT.	6315	
51+69 - 64+69	MAINLINE, RT.	5842	
62+53	MAINLINE, RT.	51	PRIVATE SIDEWALK
65+03 - 65+07	MAINLINE, RT.	200	
65+19 - 65+27	MAINLINE, LT.	130	
20+00 - 20+19	ARLINGTON COURT, LT.	92	
	TOTAL =	12,630	

NOTE: EXCLUDES AREAS OF CONCRETE DRIVEWAY 6-INCH

CULVERT PIPE CLASS III 18-INCH

LOCATION	520.0118 CULVERT PIPE CLASS III 18-INCH (LF)	650.6000 CONSTRUCTION STAKING PIPE CULVERTS (EACH)
P.E. - SHIFFLET ROAD, RT.	22	1
TOTAL	22	1

NOTE:
STEEL THICKNESS = 0.064 INCHES
ALUMINUM THICKNESS = 0.060 INCHES

WALL MODULAR BLOCK GRAVITY

STATION - STATION	LOCATION	532.0200.S (SF)
63+60 - 64+64	MAINLINE, RT.	414
29+81 - 30+16	SOUTH WOOD STREET, LT.	161
	TOTAL =	575

CURB RAMP DETECTABLE WARNING FIELD YELLOW

STATION	LOCATION	602.0505 (SF)	REMARKS
51+78	MAINLINE, LT.	8	TYPE 7B
51+75	MAINLINE, RT.	8	TYPE 1
59+22	MAINLINE, RT.	8	TYPE 1
59+58	MAINLINE, RT.	8	TYPE 1
59+59	MAINLINE, LT.	8	TYPE 7B
64+66	MAINLINE, RT.	8	TYPE 1
64+69	MAINLINE, LT.	8	TYPE 1
65+06	MAINLINE, RT.	8	TYPE 4B1
65+23	MAINLINE, LT.	8	TYPE 1
65+26	MAINLINE, RT.	8	TYPE 3
	TOTAL =	80	

STORM SEWER PIPE

PIPE NUMBER	FROM STRUCTURE	TO STRUCTURE	UPSTREAM ELEVATION	DISCHARGE ELEVATION	% SLOPE	522.1015 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 15-INCH (EACH)	REINFORCED CONCRETE PIPE CLASS III STORM SEWER 608.0312 12-INCH L.F.	608.0315 15-INCH L.F.	608.0324 24-INCH L.F.	REINFORCED CONCRETE PIPE CLASS IV STORM SEWER 608.0412 12-INCH L.F.	608.0415 15-INCH L.F.	INFOMATIONAL PURPOSES ONLY JOINT TIES (EACH)
EX P-101	EX ST MH-100	-	710.27	-	0.52	-	-	-	-	-	-	-
EX P-102	STMH-104	EX ST MH-100	710.58	710.27	0.35	-	-	-	-	-	-	-
EX P-103	-	STMH-104	710.79	710.60	0.35	-	-	-	-	-	-	-
P-100	STMH-101	EX ST MH-100	714.50	713.71	0.65	-	-	-	121	-	-	-
P-101	STMH-102	STMH-101	716.00	714.60	0.50	-	-	-	280	-	-	-
P-102	STMH-103	STMH-102	717.50	716.10	0.50	-	-	-	280	-	-	-
P-103	I-100	I-101	717.69	717.64	0.50	-	10	-	-	-	-	-
P-104	I-101	STMH-101	717.54	717.50	0.50	-	-	9	-	-	-	-
P-105	I-102	STMH-101	717.63	717.50	0.50	-	26	-	-	-	-	-
P-106	I-103	I-102	717.78	717.73	0.50	-	10	-	-	-	-	-
P-107	I-104	I-105	718.71	718.66	0.50	-	10	-	-	-	-	-
P-108	I-105	STMH-103	718.56	718.50	0.75	-	8	-	-	-	-	-
P-109	I-106	STMH-103	718.69	718.50	0.75	-	26	-	-	-	-	-
P-110	I-107	I-106	718.84	718.79	0.50	-	10	-	-	-	-	-
P-111	I-108	EW-1	717.28	717.18	0.27	1	-	-	-	-	35	6
P-112	I-110	I-112	717.71	717.58	0.38	-	-	-	-	-	33	-
P-113	I-111	I-110	717.98	717.81	0.40	-	-	-	-	42	-	-
P-115	I-112	I-108	717.48	717.38	0.30	-	-	32	-	-	-	-
P-116	I-113	I-112	717.92	717.58	0.30	-	113	-	-	-	-	-
P-117	I-114	I-113	718.13	718.02	0.30	-	36	-	-	-	-	-
P-118	I-115	STMH-104	717.65	717.50	0.50	-	29	-	-	-	-	-
P-119	I-116	STMH-104	717.54	717.50	0.50	-	8	-	-	-	-	-
	PROJECT TOTALS					1	286	41	681	42	68	6

NOTES:
PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURES.
ALL NEW CONCRETE PIPE SHALL HAVE JOINT TIES INSTALLED ON THE LAST TWO PIPE-TO-ENDWALL JOINT AT BOTH THE INLET AND DISCHARGE ENDS.
THE NUMBER OF TIES REQUIRED IS LISTED IN THE MISCELLANEOUS QUANTITIES. JOINT TIES ARE A NON-BID ITEM: THE COST SHALL BE INCLUDED IN THE LINEAR FOOT OF PIPE.

STORM SEWER STRUCTURES

STRUCTURE NUMBER	STATION	LOCATION	RIM ELEVATION (FT)	611.0535 MANHOLE COVERS TYPE J-S (EACH)	611.0639 INLET COVERS TYPE H-S (EACH)	611.0645 INLET COVER TYPE MS-A (EACH)	611.2004 MANHOLES 4-FT DIAMETER (EACH)	611.2007 MANHOLES 7-FT DIAMETER (EACH)	611.3230 INLETS 2X3-FT (EACH)	611.3901 INLETS MEDIAN 1 GRATE (EACH)	STRUCTURE DEPTH (FT)	PIPE INVERT ELEVATION	DISCHARGE ELEVATION	650.4000 CONSTRUCTION STAKING STORM SEWER EACH	628.7005 INLET PROTECTION TYPE A (EACH)	628.7015 INLET PROTECTION TYPE C (EACH)
I-100	62+18.12	16.0 LT	722.70	-	1	-	-	-	1	-	4.01	-	-	1	1	1
I-101	62+28.00	16.0 RT	722.69	-	1	-	-	-	1	-	4.15	-	-	1	1	1
I-102	62+28.20	16.0 RT	722.69	-	1	-	-	-	1	-	4.06	-	-	1	1	1
I-103	62+18.19	16.0 RT	722.68	-	1	-	-	-	1	-	3.90	-	-	1	1	1
I-104	56+58.06	16.0 LT	722.61	-	1	-	-	-	1	-	2.90	-	-	1	1	1
I-105	56+68.05	16.0 LT	722.61	-	1	-	-	-	1	-	3.05	-	-	1	1	1
I-106	56+68.05	16.0 RT	722.60	-	1	-	-	-	1	-	2.91	-	-	1	1	1
I-107	56+58.05	16.0 RT	722.61	-	1	-	-	-	1	-	2.77	-	-	1	1	1
I-108	51+60.64	16.0 RT	720.30	-	1	-	1	-	-	-	1.77	-	-	1	1	1
I-110	52+08.30	16.0 LT	720.79	-	1	-	-	-	1	-	1.98	-	-	1	1	1
I-111	51+36.49	16.0 LT	719.60	-	-	1	-	-	-	1	1.62	-	-	1	1	-
I-112	51+86.53	16.0 RT	720.85	-	1	-	-	-	1	-	2.37	-	-	1	1	1
I-113	52+99.72	16.0 RT	722.15	-	1	-	-	-	1	-	3.23	-	-	1	1	1
I-114	53+00.33	52.92 RT	719.80	-	-	1	-	-	-	1	1.67	-	-	1	1	-
I-115	64+52	16.0' RT	722.55	-	1	-	-	-	1	-	3.91	-	-	1	1	1
I-116	64+40	16.0' LT	722.60	-	1	-	-	-	1	-	4.06	-	-	1	1	1
STMH-101	62+28.00	8.50 LT	722.90	1	-	-	1	-	-	-	7.15	-	-	1	-	-
STMH-102	59+48.02	8.58 LT	724.40	1	-	-	1	-	-	-	7.15	-	-	1	-	-
STMH-103	56+68.05	8.66 LT	722.81	1	-	-	1	-	-	-	4.06	-	-	1	-	-
STMH-104	64+40	9.4' LT	722.79	1	-	-	-	1	-	-	10.96	-	-	1	-	-
EW-1	51+66.93	70.62 RT	-	-	-	-	-	-	-	-	-	717.17	717.12	1	-	-
-	65+22	RT	-	-	-	-	-	-	-	-	-	-	-	-	-	1
-	30+13	LT	-	-	-	-	-	-	-	-	-	-	-	-	-	1
	PROJECT TOTALS			4	14	2	4	1	13	2				21	16	16

NOTES:
STATION AND OFFSET OF MANHOLE AND AREA INLET STRUCTURES ARE MEASURED FROM CENTER OF STRUCTURE
STATION AND OFFSET OF CURB INLET STRUCTURES ARE MEASURED TO FLANGE OF GUTTER
ALL RIM ELEVATIONS ARE MEASURED TO THE FLANGE OF THE INLET.
STRUCTURE DEPTH (INLET) = RIM ELEVATION - INVERT LOWEST PIPE- 6 INCHES (RINGS) - 6 INCHES (CASTING HEIGHT)
STRUCTURE DEPTH (MANHOLE) = RIM ELEVATION - INVERT LOWEST PIPE- 6 INCHES (RINGS) - 9 INCHES (CASTING HEIGHT)

ALL BID ITEMS ARE CATEGORY 010 UNLESS OTHERWISE NOTED

3

ADJUSTING MANHOLE COVERS

STATION	LOCATION	611.8110 (EACH)
63+49	7.6' LT	1
64+96	10.6' LT	1
	*TOTAL	2

CONCRETE CURB & GUTTER 30-INCH TYPE D

STATION - STATION	LOCATION	601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D (LF)	650.5500 CONSTRUCTION STAKING CURB & GUTTER (LF)
51+82 - 64+76	MAINLINE, LT.	1,323	1,323
50+93 - 51+25	MAINLINE, RT.	48	48
51+57 - 59+29	MAINLINE, RT.	813	813
59+51 - 64+72	MAINLINE, RT.	566	566
65+01 - 65+15	MAINLINE, RT.	35	35
65+17 - 65+28	MAINLINE, LT.	15	15
	TOTALS =	2,800	2,800

ADJUSTING INLET COVERS

STATION	LOCATION	611.8115 (EACH)
64+70	32.9' RT	1
	TOTAL	1

WATER

LOCATION	624.0100 (MGAL)
PROJECT	67
TOTAL =	67

FINISHING ITEMS

STATION - STATION	LOCATION	625.0100 TOPSOIL (SY)	627.0200 MULCHING (SY)	629.0210 FERTILIZER TYPE B (CWT)	630.0140 SEEDING MIXTURE NO. 40 (LB)	630.0200 SEEDING TEMPORARY (LB)
50+00 - 65+15	MAINLINE	3,650	3,650	2.3	66	33
10+00 - 10+82.39	SHIFFLET ROAD	385	385	0.2	9	5
20+00 - 20+34.71	ARLINGTON COURT	55	55	0.1	1	1
30+00 - 30+30.80	SOUTH WOOD STREET	120	120	0.1	3	1
40+16 - 40+50	NORTH WOOD STREET	15	15	0.1	1	1
-	UNDISTRIBUTED	975	975	0.2	20	9
	TOTALS =	5,200	5,200	3.0	100	50

MOBILIZATION EROSION CONTROL

PROJECT	628.1905 MOBILIZATIONS EROSION CONTROL (EACH)	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL (EACH)
5916-00-72	5	3
TOTALS =	5	3

PERMANENT SIGNING

SIGN NUMBER	APPROX. STATION	LOCATION	POSITION	SIGN CODE	SIGN DESCRIPTION	ORDER LINES	SIGN SIZE IN X IN	637.2210 SIGNS TYPE II REFLECTIVE H (SF)	634.0814 POSTS TUBULAR STEEL 2X2-INCH X 14-FT (EACH)	634.0816 POSTS TUBULAR STEEL 2X2-INCH X 16-FT (EACH)	638.2602 REMOVING SIGNS TYPE II (EACH)	638.3000 REMOVING SMALL SIGN SUPPORTS (EACH)	SIGN MOUNTED ON SAME
1	51+72	Mainline	Left	R7-1R	No Parking Any Time (arrows)	Right Arrow	12X18	1.50	1	---	---	---	
2	51+00	Mainline	Left	R2-1	Speed Limit _MPH	40	24X30	5.00	1	---	---	---	
3R	51+00	Mainline	Left	R2-1	Speed Limit _MPH	40	24X30	---	---	---	1	1	
4R	10+60	Shifflet Road	Right	R1-1	Stop		30X30	---	---	---	1	1	
5	10+60	Shifflet Road	Right	R1-1	Stop		30X30	5.18	1	---	---	---	
6R	51+79	Mainline	Right	R7-1	No Parking Any Time		12X18	---	---	---	1	1	
7R	51+79	Mainline	Right	R2-1	Speed Limit _MPH	25	24X30	---	---	---	1	---	6R
8R	51+79	Mainline	Right	R7-66	Village Parking Regulations	November 1 to April 1	24X30	---	---	---	1	---	6R
9	51+83	Mainline	Right	R2-1	Speed Limit _MPH	25	24X30	5.00	1	---	---	---	
10	51+98	Mainline	Right	R7-1L	No Parking Any Time (arrows)	Left Arrow	12x18	1.50	1	---	---	---	
11	54+00	Mainline	Left	R7-1D	No Parking Any Time (arrows)	Double Arrow	12x18	1.50	1	---	---	---	
12	55+50	Mainline	Right	R7-1D	No Parking Any Time (arrows)	Double Arrow	12x18	1.50	1	---	---	---	
13R	56+47	Mainline	Right	R7-1	No Parking Any Time		12X18	---	---	---	1	1	
14R	56+76	Mainline	Left	R7-1	No Parking Any Time		12X18	---	---	---	1	1	
15	57+50	Mainline	Left	R7-1D	No Parking Any Time (arrows)	Double Arrow	12X18	1.50	1	---	---	---	
16R	58+76	Mainline	Right	R2-1	Speed Limit _MPH	25	24X30	---	---	---	1	1	
17	59+00	Mainline	Right	R7-1R	No Parking Any Time (arrows)	Right Arrow	12X18	1.50	1	---	---	---	
18	59+00	Mainline	Left	R2-1	Speed Limit _MPH	25	24X30	5.00	1	---	---	---	
19R	59+13	Mainline	Left	R2-1	Speed Limit _MPH	25	24X30	---	---	---	1	1	
20	20+12	Arlington Court	Right	R1-1	Stop		30X30	5.18	1	---	---	---	
21R	20+15	Arlington Court	Right	R1-1	Stop		30X30	---	---	---	1	1	
22	59+70	Mainline	Right	R7-1L	No Parking Any Time (arrows)	Left Arrow	12X18	1.50	1	---	---	---	
23	60+00	Mainline	Right	R2-1	Speed Limit _MPH	25	24X30	5.00	1	---	---	---	
24R	60+68	Mainline	Right	R7-1	No Parking Any Time		12X18	---	---	---	1	1	
25	60+83	Mainline	Left	R7-1D	No Parking Any Time (arrows)	Double Arrow	12X18	1.50	1	---	---	---	
26	62+00	Mainline	Right	R7-1D	No Parking Any Time (arrows)	Double Arrow	12X18	1.50	1	---	---	---	
27	64+00	Mainline	Left	R2-1	Speed Limit _MPH	25	24X30	5.00	1	---	---	---	
28R	64+06	Mainline	Left	R7-1	No Parking Any Time		12X18	---	---	---	1	1	
29	64+43	Mainline	Left	R7-1L	No Parking Any Time (arrows)	Left Arrow	12X18	1.50	1	---	---	---	
30	64+53	Mainline	Right	R1-1	Stop		30X30	5.18	---	1	---	---	
31	64+53	Mainline	Right	R1-3P	All Way		18X6	0.75	---	---	---	---	30
32R	64+57	Mainline	Right	R1-1	Stop		30X30	---	---	---	1	1	
33R	64+57	Mainline	Right	R1-3P	4-Way		18X6	---	---	---	1	---	32R
34	64+53	Mainline	Right	R7-1R	No Parking Any Time (arrows)	Right Arrow	12X18	1.50	1	---	---	---	
35R	40+40	North Wood Street	Left	R1-1	Stop		30X30	---	---	---	1	1	
36R	40+40	North Wood Street	Left	R1-3P	4-Way		18X6	---	---	---	1	---	35R
37	40+45	North Wood Street	Left	R1-1	Stop		30X30	5.18	---	1	---	---	
38	40+45	North Wood Street	Left	R1-3P	All Way		18X6	0.75	---	---	---	---	37
39	30+17	South Wood Street	Right	R1-1	Stop		30X30	5.18	---	1	---	---	
40	30+17	South Wood Street	Right	R1-3P	All Way		18X6	0.75	---	---	---	---	39
41R	30+17	South Wood Street	Right	R1-1	Stop		30X30	---	---	---	1	1	
42R	30+17	South Wood Street	Right	R1-3P	4-Way		18X6	---	---	---	1	---	41R
43R	65+34	Mainline	Left	R1-1	Stop		30X30	---	---	---	1	1	
44R	65+34	Mainline	Left	R1-3P	4-Way		18X6	---	---	---	1	---	43R
45	65+38	Mainline	Left	R1-1	Stop		30X30	5.18	---	1	---	---	
46	65+38	Mainline	Left	R1-3P	All Way		18X6	0.75	---	---	---	---	
						SHEET TOTALS		75.58	18	4	20	14	

TEMPORARY DITCH CHECKS

STATION	LOCATION	628.7504 (LF)
10+00	SHIFFLET ROAD, LT.	8
10+00	SHIFFLET ROAD, RT.	8
-	UNDISTRIBUTED	9
	TOTAL =	25

MARKERS CULVERT END

STATION	LOCATION	633.5200 (EACH)
10+27	SHIFFLET ROAD, RT.	1
	TOTAL =	1

ALL BID ITEMS ARE CATEGORY 010 UNLESS OTHERWISE NOTED

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SILT FENCE

STATION - STATION	LOCATION	628.1504 SILT FENCE (LF)	628.1520 SILT FENCE MAINTENANCE (LF)
50+00 - 50+83	MAINLINE, RT.	83	166
51+73 - 52+58	MAINLINE, RT.	106	212
52+73 - 53+73	MAINLINE, RT.	106	212
53+99 - 54+21	MAINLINE, RT.	22	44
54+73 - 54+90	MAINLINE, RT.	17	34
55+19 - 55+28	MAINLINE, RT.	9	18
55+43 - 56+75	MAINLINE, RT.	132	264
58+28 - 60+93	MAINLINE, LT.	265	530
58+39 - 59+11	MAINLINE, RT.	85	170
59+64 - 60+16	MAINLINE, RT.	62	124
60+30 - 60+70	MAINLINE, RT.	40	80
63+73 - 64+60	MAINLINE, LT.	101	202
-	UNDISTRIBUTED	222	444
	TOTALS =	1,250	2,500

CONSTRUCTION STAKING

		CONSTRUCTION STAKING			
STATION - STATION	LOCATION	650.4500 SUBGRADE (LF)	650.5000 BASE (LF)	650.9910 SUPPLEMENTAL CONTROL 02.5916-00-72 (LS)	650.9920 SLOPE STAKES (LF)
50+00 - 65+15	MAINLINE	1,517	1,517	-	1517
10+00 - 10+82 39	SHIFFLET ROAD	83	83	-	83
20+00 - 20+34.71	ARLINGTON COURT	35	35	-	35
30+00 - 30+30.80	SOUTH WOOD STREET	31	31	-	31
40+16 - 40+50	NORTH WOOD STREET	34	34	-	34
-	PROJECT	-	-	1	-
	TOTALS =	1,700	1,700	1	1,700

SAWING ASPHALT/SAWING CONCRETE

STATION	LOCATION	690.0150 SAWING ASPHALT (LF)	690.0250 SAWING CONCRETE (LF)
50+00	MAINLINE	30	-
51+93	MAINLINE, LT.	12	-
52+67	MAINLINE, RT.	12	-
53+92	MAINLINE, RT.	-	23
54+73	MAINLINE, LT.	10	-
55+06	MAINLINE, RT.	19	-
55+37	MAINLINE, RT.	11	-
55+84	MAINLINE, LT.	12	-
61+56	MAINLINE, RT.	-	14
61+81	MAINLINE, RT.	14	-
65+15	MAINLINE	47	-
10+00	SHIFFLET ROAD	25	-
20+00	ARLINGTON COURT	22	8
30+00	SOUTH WOOD STREET	29	6
40+50	NORTH WOOD STREET	37	9
-	NE QUADRANT OF WEST MADISON STREET & WOOD STREET INTERSECTION	-	15
-	SE QUADRANT OF WEST MADISON STREET & WOOD STREET INTERSECTION	-	10
	TOTALS =	280	85

PAVEMENT MARKING

			PAVEMENT MARKING				
STATION - STATION	LOCATION	TYPE	646.0106 EPOXY 4-INCH (LF)	647.0206 ARROWS BIKE LANE EPOXY (EACH)	647.0406 WORDS BIKE LANE EPOXY (EACH)	647.0566 STOP LINE EPOXY 18-INCH (LF)	647.0766 CROSSWALK EPOXY 6-INCH (LF)
50+00 - 64+59	MAINLINE	SOLID DOUBLE YELLOW	2,898	-	-	-	-
51+00 - 64+50	MAINLINE, LT.	WHITE EDGELINE	1,311	-	-	-	-
51+48	MAINLINE, LT.	-	-	-	3	-	-
51+76	MAINLINE	-	-	-	-	-	71
51+89 - 59+08	MAINLINE, RT.	WHITE EDGELINE	719	-	-	-	-
52+16	MAINLINE, RT.	-	-	1	2	-	-
54+00	MAINLINE, LT.	-	-	1	2	-	-
54+82	MAINLINE, RT.	-	-	1	2	-	-
56+50	MAINLINE, LT.	-	-	1	2	-	-
57+50	MAINLINE RT.	-	-	1	2	-	-
59+00	MAINLINE, LT.	-	-	1	2	-	-
59+58	MAINLINE	-	-	-	-	-	73
59+71 - 64+08	MAINLINE, RT.	WHITE EDGELINE	412	-	-	-	-
60+63	MAINLINE, RT.	-	-	1	2	-	-
61+47	MAINLINE, LT.	-	-	1	2	-	-
63+52	MAINLINE, RT.	-	-	-	3	-	-
64+00	MAINLINE, LT.	-	-	1	2	-	-
64+58	MAINLINE	-	-	-	-	16	-
64+67	MAINLINE	-	-	-	-	-	82
64+71 - 65+03	MAINLINE, RT.	-	-	-	-	-	62
65+22 - 65+28	MAINLINE	-	-	-	-	-	68
20+19	ARLINGTON COURT	-	-	-	-	12	-
20+26	ARLINGTON COURT	-	-	-	-	-	57
30+23	SOUTH WOOD STREET	-	-	-	-	15	-
40+27	NORTH WOOD STREET	-	-	-	-	-	102
40+34	NORTH WOOD STREET	-	-	-	-	19	-
		TOTALS =	5,340	9	24	62	515

REESTABLISH SECTION CORNER MONUMENT

STATION	LOCATION	SPV.0060.10 (EACH)
64+94.82	INTERSECTION OF WEST MADISON ST. & WOOD ST.	1
	TOTAL =	1

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	MISCELLANEOUS QUANTITIES	SHEET E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\CADD FILES\S34084 MADISON CADD\MQ.DWG
PLOT DATE: 4/17/2014 1:24 PM
PLOT BY: STRINE, THERESA
WISDOT/CADDS SHEET 42

CONVENTIONAL ABBREVIATIONS

ACCESS POINT/ DRIVEWAY CONNECTION	AP
ACCESS RIGHTS	AR
ACRES	AC.
AND OTHERS	ET.AL.
BARN	B.
CENTERLINE	C/L
CERTIFIED SURVEY MAP	CSM
CORNER	COR.
CONVEYANCE OF RIGHTS	CR
DOCUMENT	DOC.
EASEMENT	EASE.
GARAGE	G.
HIGHWAY EASEMENT	H.E.
HOUSE	H.
HOUSE TRAILER	H.T.
LAND CONTRACT	LC
MONUMENT	MON.
PAGE	P.
PERMANENT LIMITED EASEMENT	PLE
PROPERTY LINE	PL
RECORDED AS	(100')
REFERENCE LINE	R/L
RELEASE OF RIGHTS	ROR
REMAINING	REM.
RIGHT-OF-WAY	R/W
SECTION	SEC.
SHED	S.
STATION	STA.
TEMPORARY LIMITED EASEMENT	TLE
VOLUME	V.

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE OR DELTA	DELTA
LENGTH OF CURVE	L
TANGENT	TAN

CONVENTIONAL SYMBOLS

FOUND SURVEY MONUMENT (WITH POINT NUMBER) o 1040

R/W MONUMENT ○ ● (SET)

R/W STANDARD △ ▲ (SET)

SIGN ISIGN

SECTION CORNER MONUMENT

SECTION CORNER SYMBOL

FEE (HATCH VARIES)

TEMPORARY LIMITED EASEMENT

PERMANENT LIMITED EASEMENT

R/W BOUNDARY POINT RWB20

PARCEL NUMBER (8)

UTILITY PARCEL NUMBER (92)

SIGN NUMBER (OFF PREMISE) 21

BUILDING

PROPOSED R/W LINE

EXISTING H.E. LINE

PROPERTY LINE

LOT & TIE LINES

SLOPE INTERCEPTS

CORPORATE LIMITS

NO ACCESS (BY PREVIOUS ACQUISITION/CONTROL)

NO ACCESS (BY ACQUISTION)

NO ACCESS (BY STATUTORY AUTHORITY)

SECTION LINE

QUARTER LINE

SIXTEENTH LINE

EXISTING CENTERLINE

PROPOSED REFERENCE LINE

PARALLEL OFFSET

ENCROACHMENT E-1/TYPE

CONVENTIONAL UTILITY SYMBOLS

WATER	W	SANITARY SEWER	SAN
GAS	G	STORM SEWER	SS
TELEPHONE	T		
OVERHEAD TRANSMISSION LINES	OH		
ELECTRIC	E		
CABLE TELEVISION	TV		
FIBER OPTIC	FO		

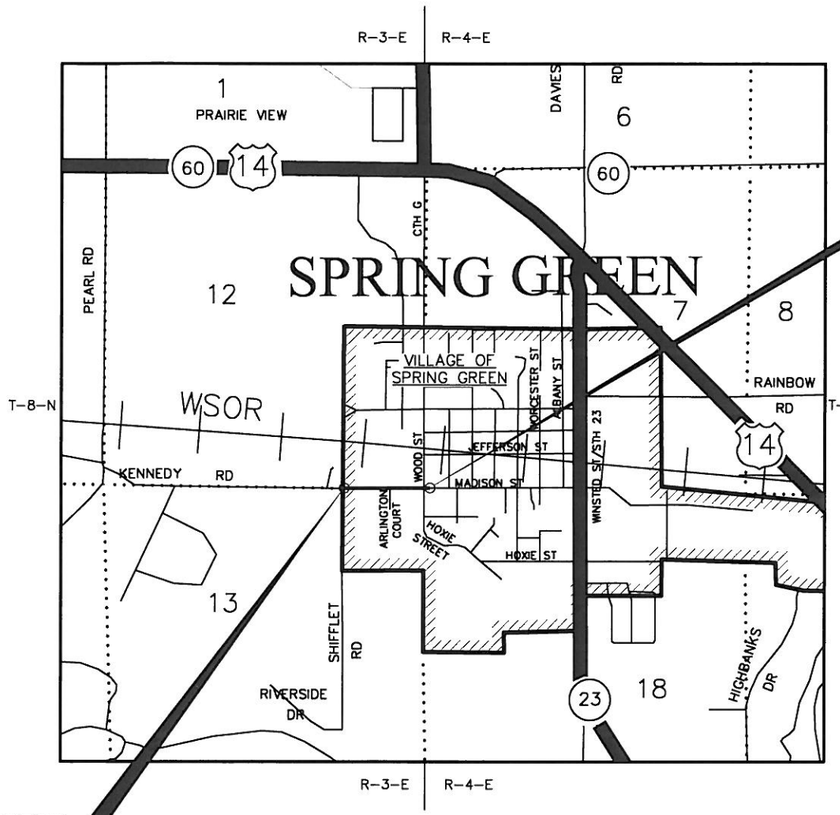
	NON COMPENSABLE	COMPENSABLE
POWER POLE		
TELEPHONE POLE		
TELEPHONE PEDESTAL		
ELECTRIC TOWER	⊠	

NOTES

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, (SAUK) COUNTY, NAD 83 (2007) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

RIGHT-OF-WAY MONUMENTS ARE TYPE 2 MONUMENTS (TYPICALLY ¾" X 24" REBAR) AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER SURVEYS OF PUBLIC RECORD.



END RELOCATION ORDER
STA. 65+36.51
0.41' NORTH AND 41.57' EAST OF THE NORTHWEST CORNER OF SECTION 18, T.8N., R.4E., VILLAGE OF SPRING GREEN, SAUK COUNTY, WI
Y = 129717.36
X = 562644.27

BEGIN RELOCATION ORDER
STA. 50+00
19.13' SOUTH AND 1146.28' EAST OF THE N¼ CORNER OF SECTION 13, T.8N., R.3E., VILLAGE OF SPRING GREEN, SAUK COUNTY, WI
Y = 129,732.36
X = 561,107.89

N

LAYOUT
SCALE 0 1/2 MI.

TOTAL NET LENGTH OF CENTERLINE = 0.291 MI.

R/W PROJECT NUMBER	SHEET NUMBER	TOTAL SHEETS
5916-00-02	4.01	4
FEDERAL PROJECT NUMBER		

PLAT OF RIGHT-OF-WAY REQUIRED FOR
VILLAGE OF SPRING GREEN, WEST MADISON STREET
(SHIFFLET ROAD TO WOOD STREET)

LOCAL STREET — SAUK COUNTY

CONSTRUCTION PROJECT NUMBER
5916-00-68/72

JEWELL
associates engineers, inc.
Engineers - Surveyors - Architects

560 SUNRISE DRIVE
SPRING GREEN, WI 53588
PHONE : 608.588.7484
FAX : 608.588.9322

I HEREBY CERTIFY THAT THIS PLAT WAS MADE FOR THE VILLAGE OF SPRING GREEN AND IS CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

WISCONSIN
FREDERICK G. GRUBER
S-2282
SPRING GREEN WIS.
LAND SURVEYOR

2/21/14

REVISION DATE
2/21/14

APPROVED FOR THE VILLAGE OF SPRING GREEN

DATE: _____ (NAME/TITLE) _____

FILE NAME : S:\Projects\S34083 Village of SG Jefferson Street\Right-of-Way\S34084 RW PLAT.DWG PLOT DATE : 2/21/2014 2:49 PM PLOT BY : Kraemer, Wes PLOT NAME : PLOT SCALE : 1:2

WISDOT/CADDS SHEET 50

SCHEDULE OF LANDS & INTERESTS REQUIRED

SHEET NUMBER	PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	TOTAL ACRES	R/W ACRES REQUIRED			TOTAL ACRES REM.	T.L.E. ACRES	P.L.E. ACRES
					NEW	EXISTING	TOTAL			
4.03	2	WALTER E & HANNELORE ALLEN	TLE & PLE	0.38	--	--	--	0.38	0.05	0.008
4.03	3	HARRY J & MURIEL A KLOTH	TLE	0.31	--	--	--	0.31	0.02	--
4.03	4	JAMES M BRICKL	TLE	1.70	--	--	--	1.70	0.015	--
4.03	5	GORDON E & MARY J NEEFE	TLE	0.65	--	--	--	0.65	0.015	--
4.03	6	WILLARD NEEFE	TLE	0.71	--	--	--	0.71	0.02	--
4.03 & 4.04	7	JANET GEIGER	TLE	0.36	--	--	--	0.36	0.01	--
4.03 & 4.04	9	COUNCIL OF SPRING GREEN ST JOHNS CATHOLIC CHURCH	TLE	7.64		--	--	7.64	0.10	--
4.04	10	CAROL OLSON	TLE	0.62	--	--	--	0.62	0.01	--
4.04	11	CHADD A MAXWELL	TLE	0.23	--	--	--	0.23	0.01	--
4.04	12	JOHN R & SUZANNE R SHARP	TLE	0.37	--	--	--	0.37	0.01	--
4.04	13	DAVID J & DONNA DRUCKREY	TLE	0.62	--	--	--	0.62	0.02	--
4.04	14	SAUK COUNTY HOUSING AUTHORITY	TLE	0.74	--	--	--	0.74	0.03	--
4.04	15	HUGH C & PHYLLIS A STEELE	TLE	0.73	--	--	--	0.73	0.07	--
4.04	16	CARDINAL SOLAR TECHNOLOGIES CO	TLE	3.77	--	--	--	3.77	0.06	--
4.05	17	RICHARD A. & PERRY L. KNEEDLER	FEE	0.33	0.004	0.025	0.029	0.301	--	--
4.03 & 4.04	201	CHARTER COMMUNICATIONS, INC.	TEMPORARY RELEASE OF RIGHTS							
4.03	202	ALLIANT ENERGY CORPORATION	TEMPORARY RELEASE OF RIGHTS							

NOTE: AREAS SHOWN IN THE TOTAL ACRES COLUMN MAY BE APPROXIMATE AND ARE DERIVED FROM THE TAX ROLLS OR OTHER AVAILABLE SOURCES AND MAY NOT INCLUDE LANDS OF THE OWNER WHICH ARE NOT CONTIGUOUS TO THE AREA TO BE ACQUIRED. OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE VILLAGE OF SPRING GREEN.

4

4

REVISION DATE 2/21/14	DATE: 12/4/13	SCALE, FEET 0 50	HWY: WEST MADISON STREET	STATE R/W PROJECT NUMBER: 5916-00-02	PLAT SHEET 4.02	
	GRID FACTOR N/A		COUNTY: SAUK	CONSTRUCTION PROJECT NUMBER: 5916-00-68/72	PS&E SHEET 32	E

BEGIN RELOCATION ORDER
STA. 50+00

19.13' SOUTH AND 1146.28' EAST OF THE N¼ CORNER OF SECTION 13, T.8N., R.3E., VILLAGE OF SPRING GREEN, SAUK COUNTY, WI
Y = 129,732.36
X = 561,107.89

TOWN OF SPRING GREEN

COORDINATE TABLE – TLE POINTS				
PT.#	STATION	OFFSET	Y	X
24	55+92.02	33.00 L	129761.75	561700.29
25	55+92.02	35.65 L	129764.40	561700.33
26	56+49.76	39.55 L	129767.54	561758.11
27	57+50.00	40.50 L	129767.19	561858.35
46	57+50.00	40.00 R	129686.69	561857.30
47	57+26.83	40.07 R	129686.93	561834.13
48	57+26.83	45.00 R	129681.99	561834.06
49	56+83.74	45.00 R	129682.55	561790.98
50	56+83.74	39.51 R	129688.04	561791.05
51	55+47.20	39.04 R	129690.29	561654.53
52	55+47.20	42.00 R	129687.33	561654.49
53	54+91.66	42.00 R	129688.06	561598.95
54	54+91.66	38.85 R	129691.21	561598.99
55	54+20.86	38.61 R	129692.37	561528.20
56	53+24.27	47.94 R	129684.31	561431.50
57	52+78.10	47.23 R	129685.63	561385.35
58	52+78.10	58.00 R	129674.86	561385.21
59	52+56.10	58.00 R	129675.14	561363.22
60	52+56.10	46.89 R	129686.25	561363.36
61	52+00.00	48.50 R	129685.37	561307.24
62	51+74.04	45.61 R	129688.58	561281.91

COORDINATE TABLE – PLE POINTS				
PT.#	STATION	OFFSET	Y	X
63	53+06.74	47.67 R	129684.81	561413.98
64	52+93.76	47.47 R	129685.18	561401.01
65	53+06.55	33.00 R	129699.48	561413.98
66	53+06.89	58.79 R	129673.70	561413.97
67	52+93.91	58.96 R	129673.70	561401.00
68	52+93.57	33.00 R	129699.65	561401.01

S¼ CORNER SEC. 12
FOUND SLIMLINE HARRISON MONUMENT
Y = 129751.49
X = 559961.61

S.E. CORNER SEC. 12
FOUND 1¼"ø IRON REBAR
Y = 129716.95
X = 562602.70

T.8 N. R.3 E. (12, 12, 13, 13)
T.8 N. R.3 E. R.4 E. (12, 7, 13, 18)

SECTION LINE
S¼ CORNER OF SEC. 12 TO S.E. CORNER OF SEC. 12
S89°15'03"E, 2641.32'

S.E. CORNER OF SEC. 12 TO PT. #65
S89°09'31"W, 1188.86'

S.E. CORNER OF SEC. 12 TO PT. #27
N86°08'19"W, 746.05'

ST JOHNS CATHOLIC CONGREGATION

VILLAGE OF SPRING GREEN
SAUK COUNTY

SE ¼ - SE ¼
SEC. 12, T8N, R3E

COUNCIL OF SPRING GREEN
ST JOHNS CATHOLIC CHURCH

(E-4) CEMETERY SIGN, FENCE, BRICK COLUMNS, AND CONCRETE FOOTINGS

LIMITS OF CEMETERY

ALLIANT ENERGY CORPORATION (UNDERGROUND GAS)

FRONTIER COMMUNICATIONS (UNDERGROUND TELEPHONE) (TYP.)

S89°15'03"E, 1286.49', PT. #2 TO #3
416.83', PT. #2 TO #24

S89°15'03"E, 1319.39', PROPOSED C/L WEST MADISON STREET, COLLINEAR WITH THE SOUTH LINE OF THE SE¼ OF SECTION 12 FROM STA. 51+75.43 TO STA. 64+94.82

WILLIAM D CARVER

ROSE M ANDERSON & JEFFREY N HUTCHINSON

QUARTER LINE
EAST LINE OF THE SW1/4 OF THE SE1/4

SW¼ - SE ¼
SEC. 12, T8N, R3E

VILLAGE LIMITS

STA 51+41.24
(WEST MADISON ST.)
=STA. 10+98.39
(SHIFFLET ROAD)
Y=129,734.21
X=561,249.11

S01°08'13"E 66.01'

S89°59'52"E 176.30', PT. #1 TO #2

(202) ALLIANT ENERGY CORPORATION (OVERHEAD ELECTRIC)

(201) CHARTER COMMUNICATIONS, INC. (UNDERGROUND CABLE TV)

(E-3) METAL POLE

PI STA=51+75.43
Y=129,734.20
X=561,283.30

SECTION LINE, VILLAGE LIMITS

181°8'21"

180°44'49"

N88°51'47"E 93.03'

PI STA=50+93.03
Y=129,734.21
X=561,200.90

S89°59'52"E 82.40'

90°15'21"

N00°15'29"E 10.98'

48.42'

184°46'55"

WEST MADISON STREET

66'

R/W 12 N89°59'52"W 82.46'

PROPOSED C/L WEST MADISON STREET

(E-1) /WOODEN SIGN POSTS AND METAL FENCE POST

(E-2) /SIGN

PHILWALD PARTNERS LLC
LOT 5
PHILWALD SUBDIVISION

N44°52'11"W 35.28'

N00°15'29"E 39.24'

SHIFFLET ROAD

N04°31'26"W 48.42'

PI STA=10+49.97
Y=129,685.79
X=561,248.90

10+00

9+76

186°03'41"

PI STA=10+00.00
Y=129,635.97
X=561,252.84

N88°27'45"W 66.02'

66'

NW ¼ - NE ¼
SEC. 13, T8N, R3E

S00°15'29"W 66.02' PT. #9 TO #10
12.62' PT. #9 TO #62
53.40' PT. #62 TO #10

QUARTER LINE
EAST LINE OF THE NW1/4 OF THE NE1/4

(2) WALTER E & HANNELORE ALLEN
DOC. #1003122

(3) HARRY J & MURIEL A KLOTH
LOT 1
CSM 1057

S89°06'06"W 56.13'

N82°47'01"W 25.53'

N00°44'57"E 11.11'

N89°15'03"W 22.00'

S00°44'57"W 10.77'

PLE, DRAINAGE

S85°13'59"W 97.04'

N89°03'23"W 70.80'

N89°15'03"W, 740.72'
EX. R/W PT. #74 TO #9

(4) JAMES M BRICKL

N00°44'57"E 3.15'

N89°15'03"W 55.54'

(5) GORDON E & MARY J NEEFE

S00°44'57"W 2.96'

N89°03'23"W 136.54'

N00°44'57"E 5.49'

TLE, SLOPES & DRIVEWAYS

(6) WILLARD NEEFE

N89°15'03"W 43.09'

NE ¼ - NE ¼
SEC. 13, T8N, R3E

(7) JANET GEIGER

S00°44'57"W 4.94'

N89°24'55"W 23.17'

S00°44'57"W MATCH LINE STA. 57+50

80.50'

67'

TLE, SLOPES

SLOPE INTERCEPTS

N86°53'15"E 57.87'

N00°44'57"E 2.65'

S89°47'43"E 100.24'

VILLAGE OF SPRING GREEN
(UNDERGROUND SANITARY & WATER)

(E-5) /SIGN

(E-6) /SIGN

(E-7) /LIGHT POLE

R/W

PL

H.

G.

LINE TABLE		
PT.# TO PT.#	BEARING	DISTANCE
56–63	N88°21'49"W	17.53'
63–65	N00°00'00"E	14.67'
65–68	N89°15'03"W	12.97'
68–64	S00°00'00"E	14.47'
64–57	N88°21'49"W	15.66'
65–66	S00°00'00"E	25.79'
66–67	N90°00'00"W	12.97'
67–68	N00°00'00"E	25.96'

COORDINATE TABLE – NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
1	50+00.00	34.86 L	129767.21	561107.20
2	51+75.43	33.00 L	129767.20	561283.50
9	51+74.10	32.99 R	129701.22	561281.97
10	51+73.80	99.01 R	129635.18	561281.67
11	51+07.81	97.24 R	129636.95	561215.67
12	50+00.00	31.16 R	129701.21	561108.51

COORDINATE TABLE – FOUND SURVEY MONUMENTS					
PT.#	STATION	OFFSET	Y	X	DESCRIPTION
75	55+20.78	32.88 R	129696.81	561628.19	¾"ø REBAR
76	54+20.91	33.00 R	129698.00	561528.33	¾"ø REBAR
77	51+07.98	58.00 R	129676.19	561215.85	1¼"ø REBAR
78	50+82.44	32.80 R	129701.20	561190.96	1¼"ø REBAR

ROAD NAME	BASIS OF EXISTING R/W
WEST MADISON STREET & SHIFFLET ROAD	SECTION LINE, COUNTY RECORDS, AND WIS. STATUTE 82.31(2) & PHILWALD SUBDIVISION PLAT

ENCROACHMENT TABLE

NUMBER	OWNER	LOCATION	ENCROACHMENT TYPE
E–1	PHILWALD PARTNERS LLC	STA. 50+84 – STA. 50+89, 29.4'–33.4' RT.	WOODEN SIGN POST AND METAL FENCE POST
E–2	WISCONSIN RIVERSIDE RESORT	STA. 51+10, 39.4' RT.	SIGN
E–3	ST JOHNS CATHOLIC CONGREGATION	STA. 51+75, 32.9' LT.	METAL POLE
E–4	ST JOHNS CATHOLIC CONGREGATION	STA. 53+37 – STA. 53+53, 31.6'–33.0' LT.	CEMETERY SIGN, FENCE, BRICK COLUMNS, AND CONCRETE FOOTINGS
E–5	GORDON E. & MARY J. NEEFE	STA. 55+57, 26.7' RT.	SIGN
E–6	GORDON E. & MARY J. NEEFE	STA. 55+91, 30.2' RT.	SIGN
E–7	WILLARD NEEFE	STA. 56+78, 28.2' RT.	LIGHT POLE

REVISION DATE 2/21/14	DATE: 12/4/13	SCALE, FEET 0 50	HWY: WEST MADISON STREET	STATE R/W PROJECT NUMBER: 5916–00–02	PLAT SHEET 4.03
	GRID FACTOR N/A		COUNTY: SAUK	CONSTRUCTION PROJECT NUMBER: 5916–00–68/72	PS&E SHEET 33 E

FILE NAME : S:\Projects\S34083 Village of SG Jefferson Street\Right–of–Way\S34084 RW PLAT.DWG PLOT DATE : 4/17/2014 1:40 PM PLOT BY : Strine, Theresa PLOT NAME : PLOT SCALE : 1:2

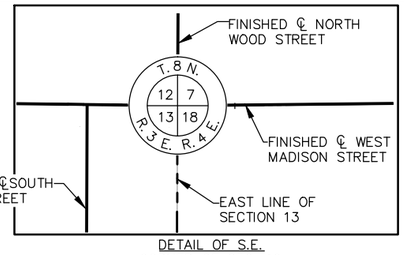
WISDOT/CADDS SHEET 75

S¼ CORNER SEC. 12
FOUND SLIMLINE HARRISON MONUMENT
Y = 129751.49
X = 559961.61

S.E. CORNER SEC. 12
FOUND 1¼"ø IRON REBAR
Y = 129716.95
X = 562602.70

SECTION LINE
S¼ CORNER OF SEC. 12 TO
S.E. CORNER OF SEC. 12
S89°15'03"E, 2641.32'

S.E. CORNER OF
SEC. 12 TO PT. #27
N86°08'19"W, 746.05'



DETAIL OF S.E. CORNER OF SEC. 12
(STA. 64+94.94, 0' RT.)

VILLAGE OF SPRING GREEN
SAUK COUNTY

SE ¼ - SE ¼
SEC. 12, T8N, R3E

NE ¼ - NE ¼
SEC. 13, T8N, R3E

COORDINATE TABLE – TLE POINTS				
PT.#	STATION	OFFSET	Y	X
27	57+50.00	40.50 L	129767.17	561858.35
28	59+49.96	42.41 L	129766.46	562058.32
29	60+89.72	42.00 L	129764.23	562198.05
30	62+49.90	43.46 L	129763.59	562358.24
31	64+56.64	38.43 L	129755.86	562564.89
32	64+56.54	50.68 L	129768.11	562564.96
33	64+48.11	72.09 R	129645.49	562554.93
34	64+47.67	53.01 R	129664.57	562554.74
35	63+60.00	53.00 R	129665.72	562467.08
36	63+15.29	39.77 R	129679.54	562422.55

COORDINATE TABLE – TLE POINTS				
PT.#	STATION	OFFSET	Y	X
37	61+92.74	38.78 R	129682.12	562300.01
38	61+92.74	47.00 R	129673.90	562299.91
39	61+45.05	47.00 R	129674.53	562252.22
40	61+45.05	38.57 R	129682.96	562252.33
41	60+40.27	38.57 R	129684.32	562147.56
42	59+65.32	41.53 R	129682.34	562072.58
43	59+10.41	52.84 R	129671.74	562017.53
44	59+10.30	39.54 R	129685.04	562017.59
45	58+43.77	39.73 R	129685.72	561951.06
46	57+50.00	40.00 R	129686.68	561857.30

(9) COUNCIL OF SPRING GREEN
ST JOHNS CATHOLIC CHURCH

(16) CARDINAL SOLAR
TECHNOLOGIES CO

FRONTIER COMMUNICATIONS
(UNDERGROUND TELEPHONE)
(TYP.)

E-9/METAL POST IN PVC

ALLIANT ENERGY CORPORATION
(UNDERGROUND GAS)

P.O.T STA. 59+39.97 (WEST MADISON ST.)
STA. 20+50.71 (ARLINGTON COURT)

S89°47'43"E 199.97'
S89°05'05"E 139.75'
S89°46'24"E 160.19'
S87°51'25"E 206.80'

TLE, SLOPES

S89°15'03"E, 1286.49'
EX. R/W PT. #2 TO #3

SLOPE INTERCEPTS

S89°15'03"E, 1319.39', PROPOSED C/L WEST MADISON STREET, COLLINEAR WITH THE SOUTH LINE OF THE SE¼ OF SECTION 12 FROM STA. 51+75.43 TO STA. 64+94.82

VILLAGE OF SPRING GREEN
(UNDERGROUND SANITARY SEWER, WATER, STORM SEWER)

N89°48'59"E 5.00'
N00°17'28"E 12.25'
N00°17'28"E 17.76'
N00°17'28"E 95.45'

66' R/W WIDTH
NORTH WOOD STREET
40+95

89°08'53"
SEE DETAIL

P.O.T. STA. 64+86.94 (WEST MADISON ST.)
=STA. 30+46.80 (SOUTH WOOD ST.)
64+00 MATCH LINE

S00°16'34"W 66.00'
PI STA=64+94.82
Y=129,716.95
X=562,602.57
496.98'

90°29'18"
N00°15'39"E 30+00
S00°15'39"W 29+72
74.67'
38.76'

N72°45'39"W 46.63'
N89°14'47"W 87.67'
N00°32'47"W 19.08'
S89°27'13"W 14.45'

STA 64+94.94
(WEST MADISON ST.)
=STA. 40+00.00
(NORTH WOOD ST.)

SOUTH WOOD ST.

N89°26'20"E, 2018', PROPOSED C/L WEST MADISON STREET, STA. 64+94.82 TO STA. 65+15.00

EAST LINE OF SECTION 13
33' 9'

MATCH LINE STA. 57+50
N00°44'57"E 80.50'

58+00 59+00 60+00 61+00 62+00 63+00 64+00 65+00

WEST MADISON STREET

E-8/LANDSCAPE AREA

N89°30'53" 20.51'
N00°15'50"E 19+76
20+00

N89°24'55"W 93.77'
N89°24'45"W 66.53'
N89°15'03"W 740.72'
EX. R/W PT. #9 TO #74
N00°15'50"E 13.30'
N89°44'10"W 5.00'
N00°15'50"E 19.80'
N89°44'10"W 50.00'

N89°15'03"W
N89°15'15"W 104.78'
S88°29'16"W 75.01'
S00°15'50"W 19.38' PT. #73 TO #7
8.53' PT. #73 TO #42
10.85' PT. #42 TO #7

N00°44'57"E 8.43'
N89°15'03"W 47.69'
N88°47'31"W 122.56'
S00°44'57"W 8.22'

E-10/LANDSCAPE AREA

E-11/LANDSCAPING BLOCK WALL

50'
ARLINGTON COURT

(7) JANET GEIGER

(10) CAROL OLSON

(11) CHADD A MAXWELL
LOT 1
ARLINGTON HEIGHTS

(12) JOHN R & SUZANNE R SHARP
LOT 1 CSM 435

(13) DAVID J & DONNA DRUCKREY

(201) CHARTER COMMUNICATIONS, INC.
(UNDERGROUND CABLE TV)

(14) SAUK COUNTY HOUSING AUTHORITY

ALLIANT ENERGY CORPORATION
(OVERHEAD ELECTRIC)

(15) HUGH C & PHYLLIS A STEELE

PL R/W H.

ENCROACHMENT TABLE

E-8	CAROL OLSON	STA. 58+05 – STA. 58+14, 31.8'–33.0' RT.	LANDSCAPE AREA
E-9	COUNCIL OF SPRING GREEN ST JOHNS CATHOLIC CHURCH	STA. 61+50, 32.8' RT.	SIGN
E-10	HUGH C & PHYLLIS A STEELE	STA. 63+92 – STA. 64+06, 31.2'–33.0' RT.	LANDSCAPE AREA
E-11	CHADD A MAXWELL	STA. 20+00 – STA. 20+04, (ARLINGTON COURT)31.2'–33.0' RT. 14.2'–17.2' LT.	LANDSCAPING BLOCK WALL

COORDINATE TABLE – FOUND SURVEY MONUMENTS					
PT.#	STATION	OFFSET	Y	X	DESCRIPTION
70	61+48.05	33.00 L	129754.49	562256.26	¾"ø REBAR
71	61+65.24	32.93 R	129688.33	562272.59	¾"ø REBAR
72	60+40.20	33.01 R	129689.89	562147.57	1¼"ø REBAR
73	59+65.24	33.00 R	129690.88	562072.61	¾"ø REBAR
74	59+15.24	33.00 R	129691.53	562022.62	1¼"ø REBAR

ROAD NAME	BASIS OF EXISTING R/W
WEST MADISON STREET, NORTH WOOD STREET, & SOUTH WOOD STREET	SECTION LINE, COUNTY RECORDS, AND WIS. STATUTE 82.31(2) & ARLINGTON HEIGHTS SUBDIVISION PLAT
ARLINGTON COURT	COUNTY RECORDS & ARLINGTON HEIGHTS SUBDIVISION PLAT

COORDINATE TABLE – NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
3	64+61.68	33.00 L	129750.38	562569.87
4	64+61.54	50.76 L	129768.14	562569.96
5	64+62.22	33.00 R	129684.38	562569.55
6	64+62.55	71.76 R	129645.62	562569.38
7	59+65.41	52.38 R	129671.49	562072.53
8	59+15.41	52.80 R	129671.72	562022.53

REVISION DATE
2/21/14 (N.C.)

DATE: 12/4/13
GRID FACTOR N/A

SCALE, FEET
0 50

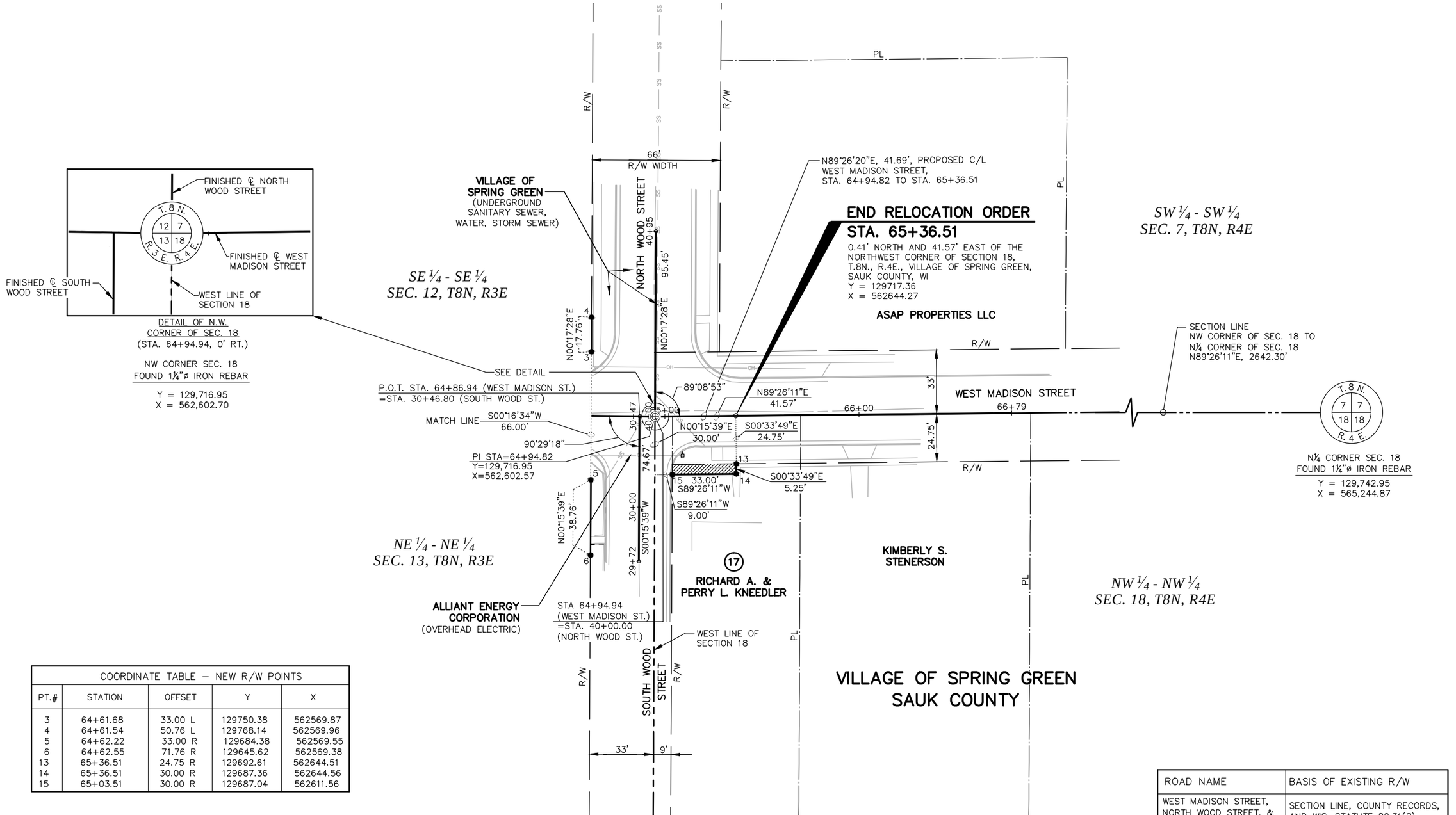
HWY: WEST MADISON STREET
COUNTY: SAUK

STATE R/W PROJECT NUMBER: 5916-00-02
CONSTRUCTION PROJECT NUMBER: 5916-00-68/72

PLAT SHEET 4.04
PS&E SHEET 34 E

FILE NAME : S:\Projects\S34083 Village of SG Jefferson Street\Right-of-Way\S34084 RW PLAT.DWG
PLOT DATE : 4/18/2014 3:58 PM PLOT BY : Strine, Theresa PLOT NAME : PLOT SCALE : 1:2

WISDOT/CADDS SHEET 75



COORDINATE TABLE – NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
3	64+61.68	33.00 L	129750.38	562569.87
4	64+61.54	50.76 L	129768.14	562569.96
5	64+62.22	33.00 R	129684.38	562569.55
6	64+62.55	71.76 R	129645.62	562569.38
13	65+36.51	24.75 R	129692.61	562644.51
14	65+36.51	30.00 R	129687.36	562644.56
15	65+03.51	30.00 R	129687.04	562611.56

VILLAGE OF SPRING GREEN
SAUK COUNTY

ROAD NAME	BASIS OF EXISTING R/W
WEST MADISON STREET, NORTH WOOD STREET, & SOUTH WOOD STREET	SECTION LINE, COUNTY RECORDS, AND WIS. STATUTE 82.31(2)

REVISION DATE 2/21/14	DATE: 12/4/13	SCALE, FEET 0 50	HWY: WEST MADISON STREET	STATE R/W PROJECT NUMBER: 5916-00-02	PLAT SHEET 4.05
	GRID FACTOR N/A		COUNTY: SAUK	CONSTRUCTION PROJECT NUMBER: 5916-00-68/72	PS&E SHEET 35 E

FILE NAME : S:\Projects\S34083 Village of SG Jefferson Street\Right-of-Way\S34084 RW PLAT.DWG PLOT DATE : 4/17/2014 1:54 PM PLOT BY : Strine, Theresa PLOT NAME : PLOT SCALE : 1:2

WISDOT/CADDS SHEET 75

PI STA. = 50+93.03
Y = 129,734.21
X = 561,200.90

PI STA. = 51+75.43
Y = 129,734.20
X = 561,283.30

ROSE M ANDERSON &
JEFFERY N HUTCHINSON

STA. 51+93, LT.
CONSTRUCT C.E. (ASPHALT)

ST JOHNS CATHOLIC
CONGREGATION

LIMITS OF CEMETERY

STA. 54+73, LT.
CONSTRUCT C.E. (ASPHALT)

STA. 51+19, LT.
CONSTRUCT P.E. (ASPHALT)

STA. 51+61, LT.
CONSTRUCT P.E. (ASPHALT)

STA. 55+84, LT.
CONSTRUCT P.E. (ASPHALT)

FRONTIER
COMMUNICATIONS
(TYP.)

FINISHED ℄ WEST
MADISON STREET

EXISTING BRICK COLUMNS AND
METAL GATE TO REMAIN

ALLIANT
ENERGY
(TYP.)

CAUTION

SLOPE
INTERCEPTS

STA. 50+00
18.2' LT.

STA. 50+93
16.0' LT.

(TYPE A
ONLY)

P.I. STA. = 51+75.43

BM#4

720.76 720.88 721.41 722.22 722.86 723.27 723.46 723.43 723.20

50+00 N88°51'47"E 51+00 S89°59'52"E 52+00 53+00 54+00 55+00 S89°15'03"E

178°51'39" 90°15'21" 179°15'11"

721.06 720.59 720.88 720.86 720.47 721.41 722.22 722.70 723.11 723.46 723.43 723.20

P.I. STA. = 50+93.03

N0°15'29"E 100.98

STA. 50+93
16.0' RT.

CAUTION

EXISTING ℄ WEST
MADISON STREET

STA. 50+00
11.3' RT.

16' 16'

P.I. STA. = 10+49.97

175°13'05"

720.11 720.11

STA. 51+41.24 (WEST MADISON STREET)=
STA. 10+98.39 (SHIFFLET ROAD)
TYPE D (MODIFIED) INTERSECTION REQ'D

FINISHED ℄
SHIFFLET ROAD

CHARTER
COMMUNICATIONS
(TYP.)

BUILDING

WALTER E &
HANNELORE ALLEN

(TYPE A ONLY)

P.L.E. REQUIRED

T.L.E. REQUIRED
(TYP.)

BUILDING

HARRY J. &
MURIEL A. KLOTH

BUILDING

CONCRETE SIDEWALK
4-INCH (TYP.)

ALLIANT
ENERGY
(TYP.)

CAUTION

ALLIANT
ENERGY
(TYP.)

VILLAGE OF SPRING
GREEN (TYP.)

JAMES M BRICKL

GORDON E & MARY J
NEEFE

BEGIN PROJECT
STA. 50+00
Y= 129,732.36
X= 561,107.89

PHILWALD
PARTNERS LLC

SPECIAL DITCH
(GRADE TO DRAIN)

CAUTION

N4°31'26"W 100.00

37' 28'

EXISTING FENCE
TO REMAIN

EXISTING ℄
SHIFFLET ROAD

STA. 52+67, RT.
CONSTRUCT P.E. (ASPHALT)

STA. 53+92, RT.
CONSTRUCT P.E. (CONCRETE)

STA. 55+06, RT.
CONSTRUCT P.E. (ASPHALT)

STA. 55+37, RT.
CONSTRUCT P.E. (ASPHALT)

MATCHLINE STA. 56+00

N

5 5

LEGEND

- XXXXX SAWING ASPHALT OR SAWING CONCRETE
- CURB RAMP DETECTABLE WARNING FIELD - TYPE 7B
- CURB RAMP DETECTABLE WARNING FIELD - TYPE 1
- FLOW ARROW
- SILT FENCE
- TEMPORARY DITCH CHECK
- TYPE A INLET PROTECTION PLACED PRIOR TO CURB & GUTTER CONSTRUCTION AND TYPE C PLACED AFTER

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
4	52+95	¾" IRON REBAR SET, 17.9' LT.	721.78
5	57+25	¾" IRON REBAR SET, 16.9' LT.	723.69
6	63+79	¾" IRON REBAR SET, 20.2' LT.	723.28

730 728 726 724 722 720 718 716 714

STA. 50+00

VPC STA = 50+70.00
EL = 721.65

VPC STA = 52+70.00
EL = 722.05

VPT STA = 55+30.00
EL = 723.63

PI STA 54+00.00
EL 724.34

FINISHED ℄ PROFILE
WEST MADISON STREET

-0.55%

-1.36%

K = 63.96'
L = 200.00'

K = 112.43'
L = 260.00'

EXISTING ℄ PROFILE
WEST MADISON STREET

STA. 50+00
EL = 722.61

MATCH EXISTING
STA. 50+00
EL. = 722.61

S=-0.93%

STA. 50+50, RT.
BEGIN SPECIAL DITCH
EL. = 719.92

PI STA 51+70.00
EL 720.29

MATCHPOINT
STA. 51+00, RT. (WEST MADISON STREET)
STA. 10+67, LT. (SHIFFLET ROAD)
EL. = 719.44

EXISTING ℄ ELEVATION
FINISHED ℄ ELEVATION

722.61 722.61 | 722.29 722.33 | 721.98 722.06 | 721.67 721.79 | 721.38 721.52 | 721.21 721.31 | 721.16 721.17 | 721.11 721.08 | 721.06 721.06 | 721.11 721.10 | 721.19 721.20 | 721.26 721.36 | 721.32 721.59 | 721.51 721.88 | 721.70 722.22 | 721.86 722.54 | 722.00 722.82 | 722.14 723.07 | 722.28 723.28 | 722.42 723.45 | 722.56 723.59 | 722.69 723.69 | 722.80 723.76 | 722.90 723.79 | 723.00 723.79 | 723.10 723.75 | 723.23 723.68 | 723.35 723.57 | 723.40 723.46 | 723.45 723.35 | 723.51 723.24

50+00 50+20 50+40 50+60 50+80 51+00 51+20 51+40 51+60 51+80 52+00 52+20 52+40 52+60 52+80 53+00 53+20 53+40 53+60 53+80 54+00 54+20 54+40 54+60 54+80 55+00 55+20 55+40 55+60 55+80 56+00

EARTHWORK SUMMARY

	MAINLINE	SHIFFLET ROAD	ARLINGTON COURT	SOUTH WOOD STREET	NORTH WOOD STREET	P.E., F.E., C.E.	TOTALS
EXCAVATION COMMON (C.Y.)	5,010	385	100	80	115	210	5,900
FILL (C.Y.)	255	10	--	--	--	6	271
FILL EXPANSION (C.Y.-25%)	319	13	--	--	--	8	340
WASTE (C.Y.)	4,691	372	100	80	115	202	5,560

PROJECT NO: 5916-00-72 | HWY: WEST MADISON STREET | COUNTY: SAUK | PLAN & PROFILE | SHEET 36

COUNCIL OF SPRING GREEN
ST JOHNS CATHOLIC CHURCH

STA. 59+39.97 (WEST MADISON STREET)=
STA. 20+50.71 (ARLINGTON COURT)

STA. 61+05, LT.
CONSTRUCT F.E. (ASPHALT)

STA. 61+69, LT.
CONSTRUCT C.E. (ASPHALT)

CARDINAL SOLAR
TECHNOLOGIES CO.

N

MATCHLINE STA. 56+00

MATCHLINE STA. 62+00

STA. 56+55
27' LT.

STA. 56+00
21.5' LT.

CONCRETE SIDEWALK
4-INCH (TYP.)

T.L.E. REQUIRED (TYP.)

FINISHED ℄ WEST
MADISON STREET

FRONTIER
COMMUNICATIONS
(TYP.)

EXISTING ℄ WEST
MADISON STREET

ALLIANT
ENERGY
(TYP.)

CAUTION

SLOPE
INTERCEPTS

BM#5

722.93 722.69 722.73 723.05 723.42 723.78 724.13 724.25 724.04 723.71 723.22 723.05

722.93 722.69 722.73 723.05 723.34 723.70 724.13 724.18 724.20 724.04 723.71 723.30 722.98

724.07 724.08 723.96 723.95 723.86 723.88

N0°15'50"E 89°30'53" S89°15'03"E

20.51

11.3' 11.1' 20.00'

25.6' 24.4'

R/W

P.L.

STA. 56+95, RT.
CONSTRUCT P.E. (ASPHALT)

WILLARD
NEEFE

STA. 57+18, RT.
CONSTRUCT P.E.
(ASPHALT)

JANET
GEIGER

CHARTER
COMMUNICATIONS
(TYP.)

STA. 58+31, RT.
CONSTRUCT P.E. (ASPHALT)

CAROL
OLSON

BUILDING

CHADD A
MAXWELL

CAUTION

FINISHED ℄
ARLINGTON COURT

EXISTING ℄
ARLINGTON COURT

JOHN R & SUZANNE R
SHARP

STA. 60+26, RT.
CONSTRUCT P.E. (ASPHALT)

VILLAGE OF SPRING
GREEN (TYP.)

T.L.E. REQUIRED (TYP.)

DAVID J & DONNA
DRUCKREY

STA. 61+56, RT.
CONSTRUCT P.E. (CONCRETE)

ALLIANT
ENERGY
(TYP.)

SAUK COUNTY
HOUSING AUTH

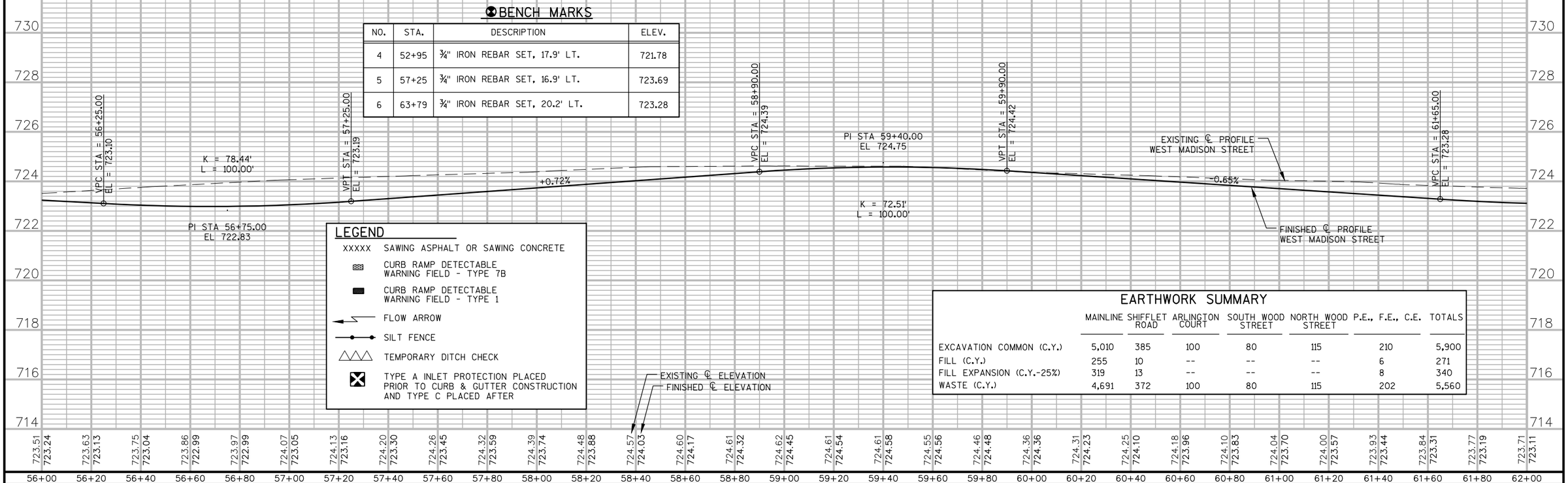
STA. 61+81, RT.
CONSTRUCT P.E.
(ASPHALT)

5

5

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
4	52+95	¾" IRON REBAR SET, 17.9' LT.	721.78
5	57+25	¾" IRON REBAR SET, 16.9' LT.	723.69
6	63+79	¾" IRON REBAR SET, 20.2' LT.	723.28



LEGEND

- XXXXX SAWING ASPHALT OR SAWING CONCRETE
- CURB RAMP DETECTABLE WARNING FIELD - TYPE 7B
- CURB RAMP DETECTABLE WARNING FIELD - TYPE 1
- FLOW ARROW
- SILT FENCE
- TEMPORARY DITCH CHECK
- TYPE A INLET PROTECTION PLACED PRIOR TO CURB & GUTTER CONSTRUCTION AND TYPE C PLACED AFTER

EARTHWORK SUMMARY

	MAINLINE	SHIFFLET ROAD	ARLINGTON COURT	SOUTH WOOD STREET	NORTH WOOD STREET	P.E., F.E., C.E.	TOTALS
EXCAVATION COMMON (C.Y.)	5,010	385	100	80	115	210	5,900
FILL (C.Y.)	255	10	--	--	--	6	271
FILL EXPANSION (C.Y.-25%)	319	13	--	--	--	8	340
WASTE (C.Y.)	4,691	372	100	80	115	202	5,560

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	PLAN & PROFILE	SHEET 37

CARDINAL ST

ASAP PROPERTIES LLC

STA. 64+86.94 (WEST MADISON STREET)=
STA. 30+46.80 (SOUTH WOOD STREET)

EXISTING ℄ NORTH WOOD STREET

FINISHED ℄ NORTH WOOD STREET

STA. 64+94.98 (WEST MADISON STREET)=
STA. 40+00.00 (NORTH WOOD STREET)

FINISHED ℄ WEST MADISON STREET

T.L.E. REQUIRED (TYP.)

ALLIANT ENERGY (TYP.)

FRONTIER COMMUNICATIONS (TYP.)

VILLAGE OF SPRING GREEN (TYP.)

BM*6

SLOPE INTERCEPTS

MATCHLINE STA. 62+00

EXISTING ℄ WEST MADISON STREET

STA. 63+60 - STA. 64+64, RT. (WEST MADISON STREET/
STA. 29+81 - STA. 30+16, LT. (SOUTH WOOD STREET)
REMOVING WALL MODULAR BLOCK GRAVITY
STA. 63+60 - STA. 64+64, RT. (WEST MADISON STREET)/
STA. 29+81 - STA. 30+16, LT. (SOUTH WOOD STREET)
WALL MODULAR BLOCK GRAVITY REQ'D.

LEGEND

- XXXXX SAWING ASPHALT OR SAWING CONCRETE
- CURB RAMP DETECTABLE WARNING FIELD - TYPE 7B
- CURB RAMP DETECTABLE WARNING FIELD - TYPE 1
- CURB RAMP DETECTABLE WARNING FIELD - TYPE 3
- CURB RAMP DETECTABLE WARNING FIELD - TYPE 4B1
- FLOW ARROW
- SILT FENCE
- TEMPORARY DITCH CHECK
- TYPE A INLET PROTECTION PLACED PRIOR TO CURB & GUTTER CONSTRUCTION AND TYPE C PLACED AFTER

722.79 722.78 722.97 722.99 722.82 722.62 722.71 722.78 722.97 722.99 722.82 722.55 722.60 722.75 722.89 723.08

63+00 64+00 STA. 63+50 27' LT. STA. 63+60 25' LT.

721.90 722.00 722.31 722.82

89°32'31" N01°17'28"E 178°41'23" N89°26'20"E 89°30'42" S89°15'03"E N01°15'39"E

18.6' 18.4' 32.3' 33.7' 15.1' 14.3' 24.9' 17.1' 30.47' 40.00' 65.00' 30.00' 64+94.82 PI STA. 64+94.82

33' 24.75' 16' 9' 5' 1' 6' 7'

R/W P.L. SAN SS W UGT OHE G

CAUTION

CHARTER COMMUNICATIONS (TYP.)

ALLIANT ENERGY (TYP.)

CONCRETE SIDEWALK 4-INCH (TYP.)

T.L.E. REQUIRED (TYP.)

(REMOVING WALL MODULAR BLOCK GRAVITY REQ'D)

WALL MODULAR BLOCK GRAVITY REQ'D

EXISTING RETAINING WALL TO REMAIN

GARDEN

BUILDING

(TYPE C ONLY)

FINISHED ℄ SOUTH WOOD STREET

EXISTING ℄ SOUTH WOOD STREET

PI STA. = 64+94.82
Y = 129,716.95
X = 562,602.57

SAUK COUNTY HOUSING AUTH

HUGH C & PHYLLIS A STEELE

RICHARD A & PERRY L KNEEDLER

END PROJECT
STA. 65+15
Y= 129,717.15
X= 562,622.76

STA. 65+15

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
4	52+95	¾" IRON REBAR SET, 17.9' LT.	721.78
5	57+25	¾" IRON REBAR SET, 16.9' LT.	723.69
6	63+79	¾" IRON REBAR SET, 20.2' LT.	723.28

730 728 726 724 722 720 718 716 714

K = 95.45'
L = 110.00'

VPC STA = 62+75.00
EL = 723.19

PI STA 62+20.00
EL 722.92

PI STA 63+25.00
EL 723.44

K = 111.35'
L = 100.00'

VPT STA = 63+75.00
EL = 723.24

-0.40%

PI STA = 65+15.00
EL = 722.68

FINISHED ℄ PROFILE WEST MADISON STREET

MATCH EXISTING STA. 65+15 EL. = 722.68

EXISTING ℄ PROFILE WEST MADISON STREET

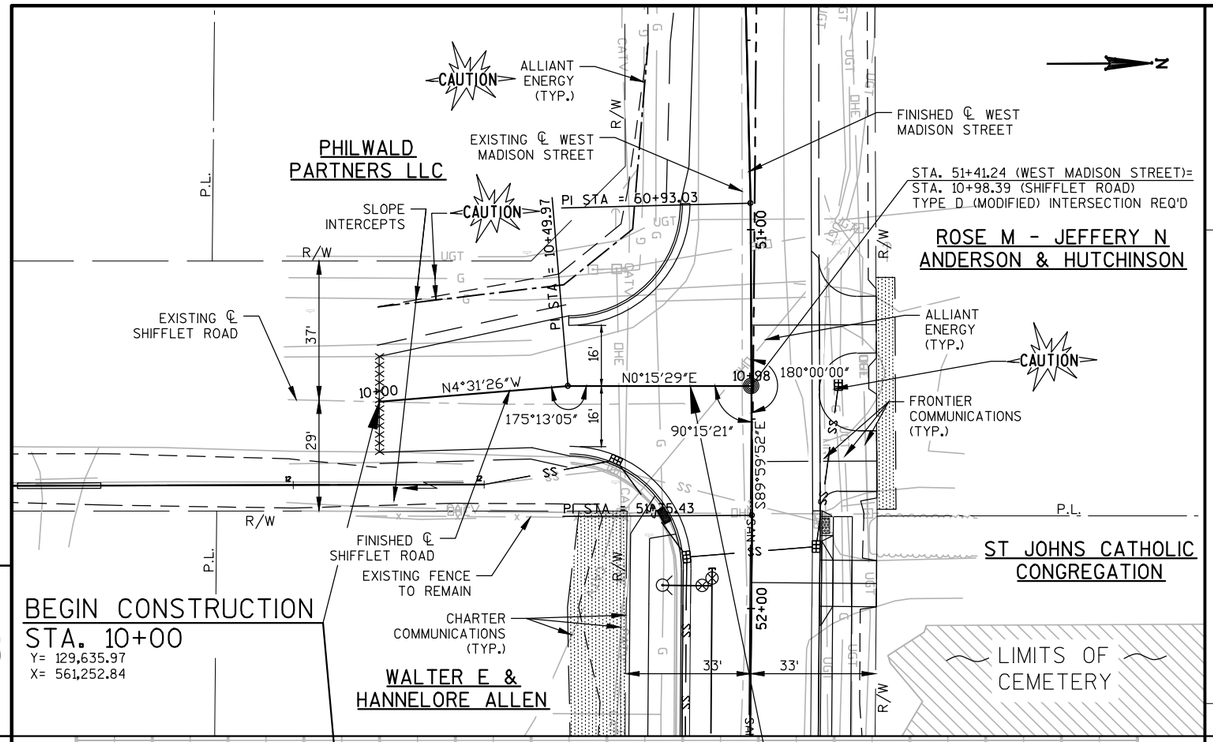
EXISTING ℄ ELEVATION
FINISHED ℄ ELEVATION

723.71 723.11 | 723.69 723.08 | 723.66 723.08 | 723.61 723.13 | 723.54 723.22 | 723.47 723.29 | 723.39 723.33 | 723.30 723.33 | 723.24 723.29 | 723.19 723.22 | 723.08 723.14 | 723.06 723.06 | 723.05 722.98 | 722.97 722.90 | 722.84 722.82 | 722.77 722.74 | 722.66 | 722.63 | 722.53 | 722.40 | 722.27 | 722.14 | 721.97 | 722.06

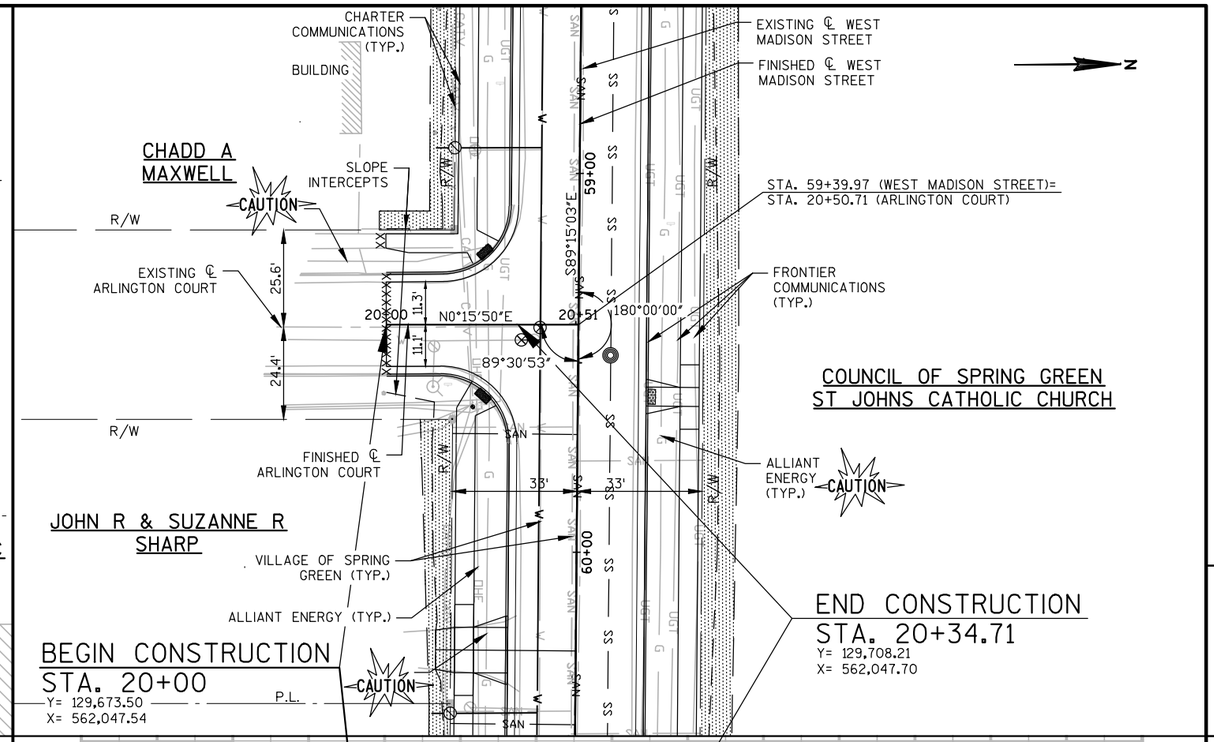
62+00 62+20 62+40 62+60 62+80 63+00 63+20 63+40 63+60 63+80 64+00 64+20 64+40 64+60 64+80 65+00 65+20 65+40 65+60 65+80 66+00 66+20 66+40 66+60 66+80 67+00 67+20 67+40 67+60 67+80 68+00

EARTHWORK SUMMARY

	MAINLINE	SHIFFLET ROAD	ARLINGTON COURT	SOUTH WOOD STREET	NORTH WOOD STREET	P.E., F.E., C.E.	TOTALS
EXCAVATION COMMON (C.Y.)	5,010	385	100	80	115	210	5,900
FILL (C.Y.)	255	10	--	--	--	6	271
FILL EXPANSION (C.Y.-25%)	319	13	--	--	--	8	340
WASTE (C.Y.)	4,691	372	100	80	115	202	5,560



NO.	STA.	DESCRIPTION	ELEV.
4	52+95	¾" IRON REBAR SET, 17.9' LT.	721.78
5	57+25	¾" IRON REBAR SET, 16.9' LT.	723.69
6	63+79	¾" IRON REBAR SET, 20.2' LT.	723.28

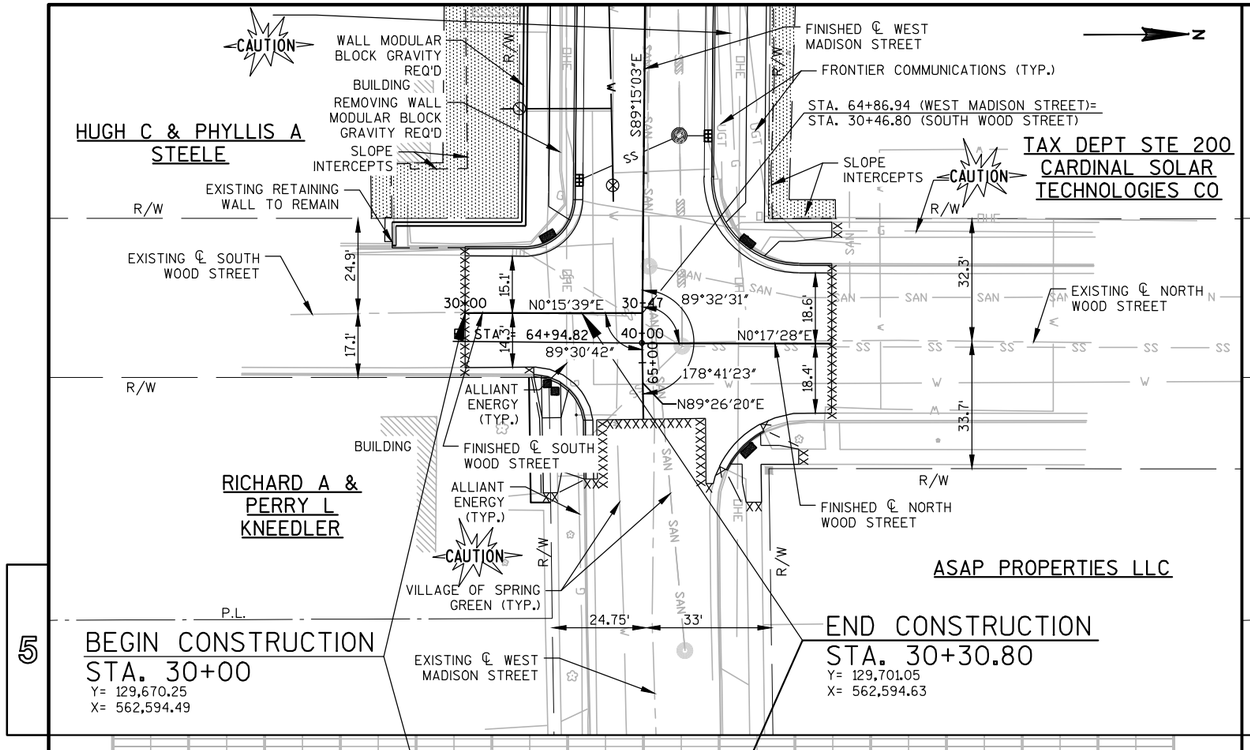


LEGEND

- XXXXX SAWING ASPHALT OR SAWING CONCRETE
- CURB RAMP DETECTABLE WARNING FIELD – TYPE 7B
- CURB RAMP DETECTABLE WARNING FIELD – TYPE 1
- FLOW DIRECTION

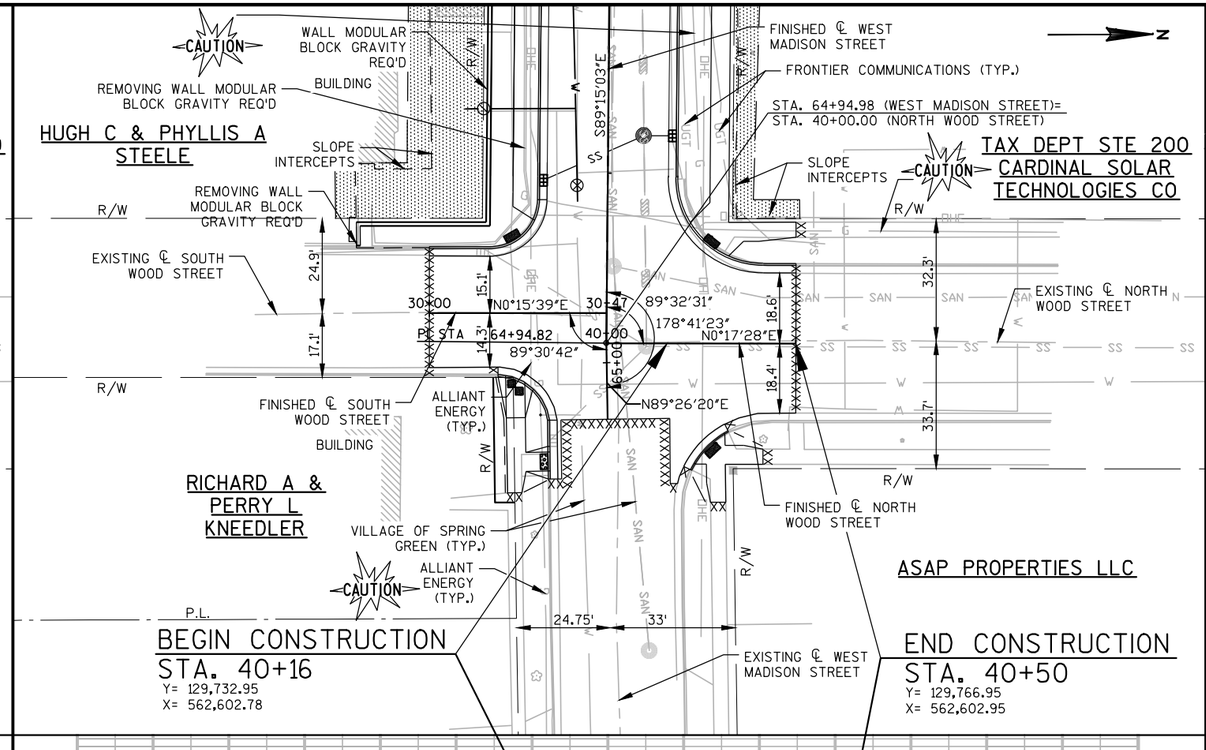
EARTHWORK SUMMARY

	MAINLINE	SHIFFLET ROAD	ARLINGTON COURT	SOUTH WOOD STREET	NORTH WOOD STREET	P.E., F.E., C.E.	TOTALS
EXCAVATION COMMON (C.Y.)	5,010	385	100	80	115	210	5,900
FILL (C.Y.)	255	10	--	--	--	6	271
FILL EXPANSION (C.Y.–25%)	319	13	--	--	--	8	340
WASTE (C.Y.)	4,691	372	100	80	115	202	5,560



BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
4	52+95	¾" IRON REBAR SET, 17.9' LT.	721.78
5	57+25	¾" IRON REBAR SET, 16.9' LT.	723.69
6	63+79	¾" IRON REBAR SET, 20.2' LT.	723.28



LEGEND

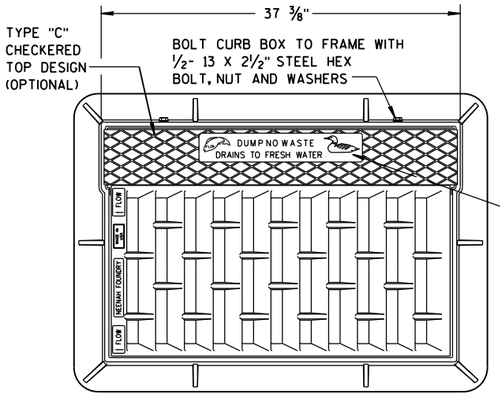
XXXXX	SAWING ASPHALT OR SAWING CONCRETE
	CURB RAMP DETECTABLE WARNING FIELD – TYPE 7B
	CURB RAMP DETECTABLE WARNING FIELD – TYPE 1
	CURB RAMP DETECTABLE WARNING FIELD – TYPE 3
	CURB RAMP DETECTABLE WARNING FIELD – TYPE 4B1

EARTHWORK SUMMARY

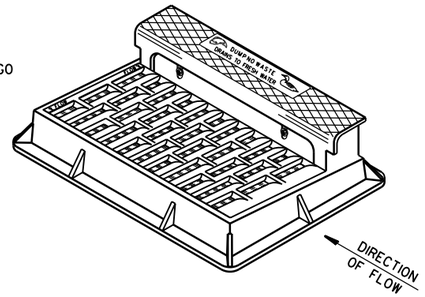
	MAINLINE	SHIFFLET ROAD	ARLINGTON COURT	SOUTH WOOD STREET	NORTH WOOD STREET	P.E., F.E., C.E.	TOTALS
EXCAVATION COMMON (C.Y.)	5,010	385	100	80	115	210	5,900
FILL (C.Y.)	255	10	--	--	--	6	271
FILL EXPANSION (C.Y.-25%)	319	13	--	--	--	8	340
WASTE (C.Y.)	4,691	372	100	80	115	202	5,560

Standard Detail Drawing List

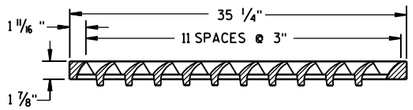
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-19B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08A05-19D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08B09-01	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER
08C07-01	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08C08-01	INLETS MEDIAN 1 AND 2 GRATE
08D01-17	CONCRETE CURB, CONCRETE CURB AND GUTTER AND TIES
08D05-15A	CURB RAMPS TYPES 1 AND 1-A
08D05-15B	CURB RAMPS TYPES 2 AND 3
08D05-15C	CURB RAMPS TYPES 4A AND 4A1
08D05-15D	CURB RAMPS TYPE 4B AND 4B1
08D05-15E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F04-07	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
09A01-13A	AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND D AND TEE INTERSECTION BYPASS LANE
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-05A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-05B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C03-02	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C08-16B	PAVEMENT MARKING (INTERSECTIONS)
15C29-03A	BICYCLE LANE MARKING
15C29-03D	URBAN BICYCLE LANE MARKING
15C29-03E	PAVEMENT MARKING FOR BIKE LANES
15C33-01	STOP LINE AND CROSSWALK PAVEMENT MARKING
16A01-06	LANDMARK REFERENCE MONUMENTS AND COVERS

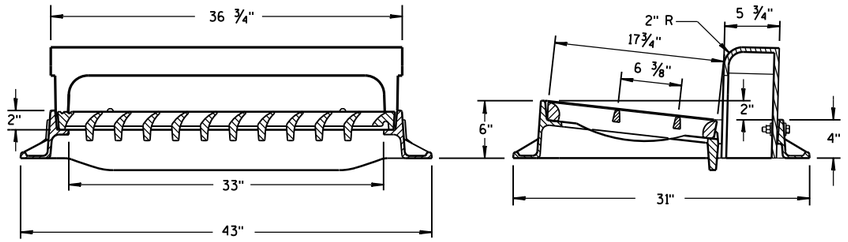


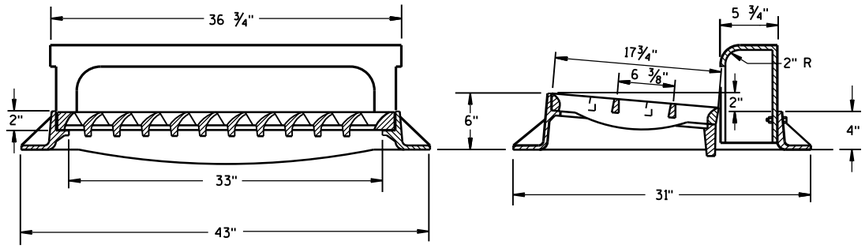
NOTE:
GRATE IS REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

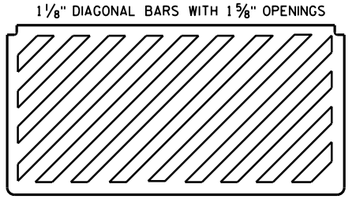






TYPE "H"

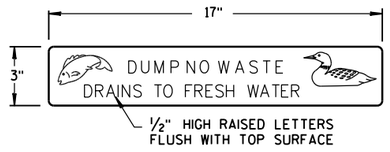
NOTE: EITHER CASTING IS ACCEPTABLE



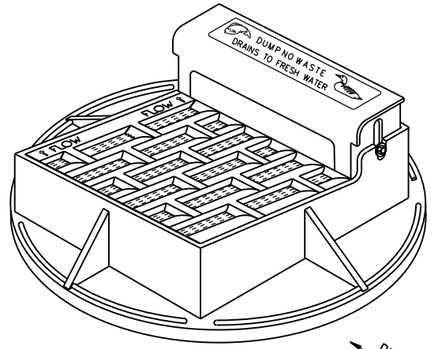
SPECIAL GRATE FOR TYPE "H" COVER

(MEASURES 35 1/4" X 17 3/4" X 2")

(NOTED AS TYPE H-S ON DRAINAGE TABLE)



LOGO DETAIL



NOTE: CURB BOX ADJUSTABLE 4" TO 9"

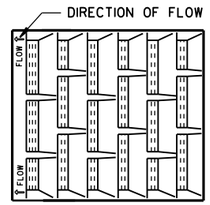
NOTE:
GRATE IS REVERSIBLE.

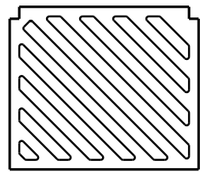
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

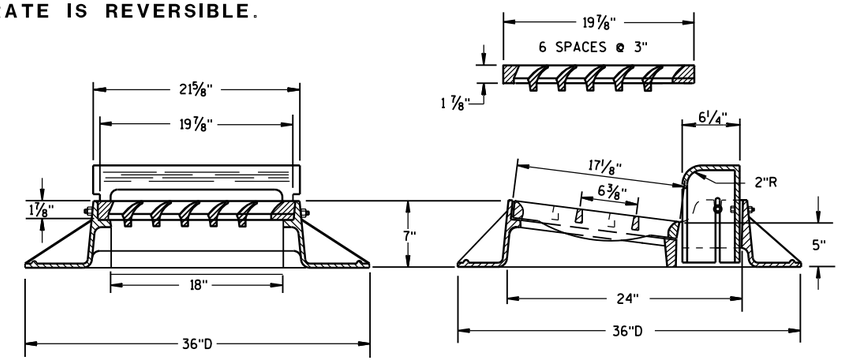




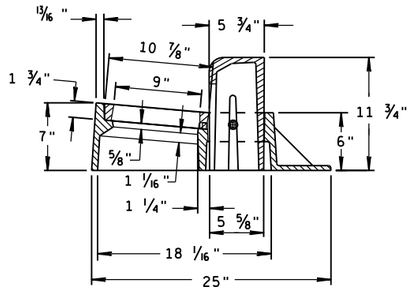
SPECIAL GRATE FOR TYPE "A" COVER

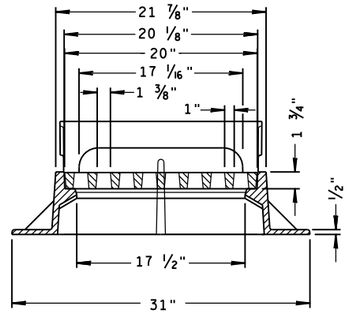
(MEASURES 19 3/4" X 17" X 1 7/8")

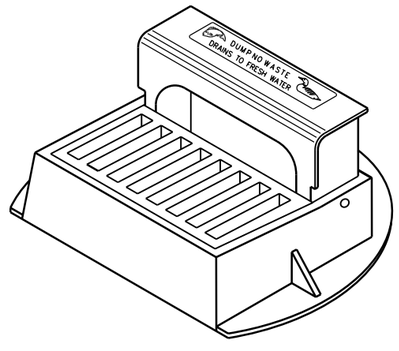
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"







TYPE "Z"

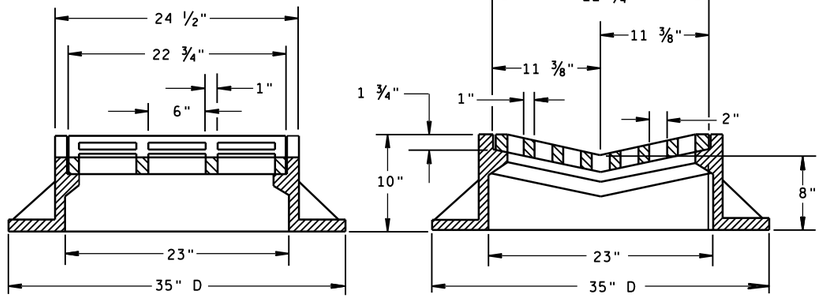
INLET COVERS
TYPE A, H, A-S, H-S & Z

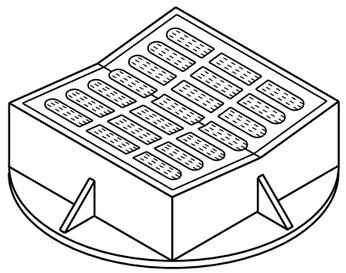
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11-27-13 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

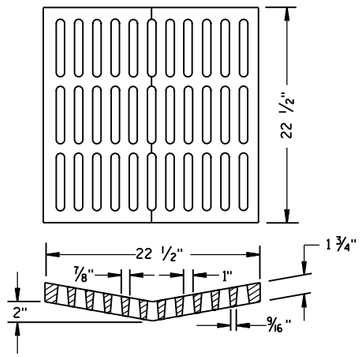
6

S.D.D. 8 A 5-19a



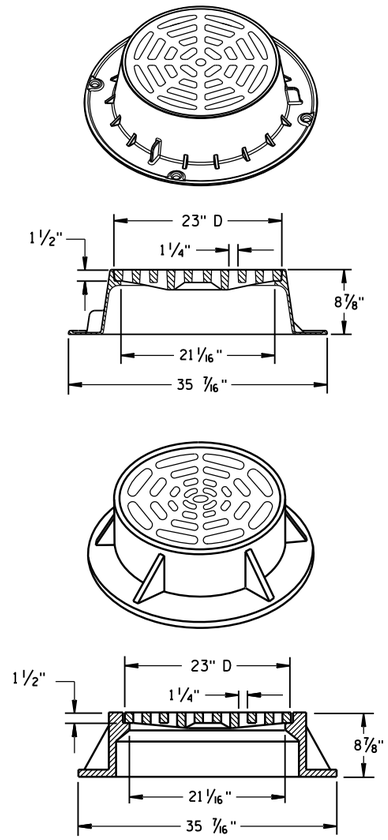


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

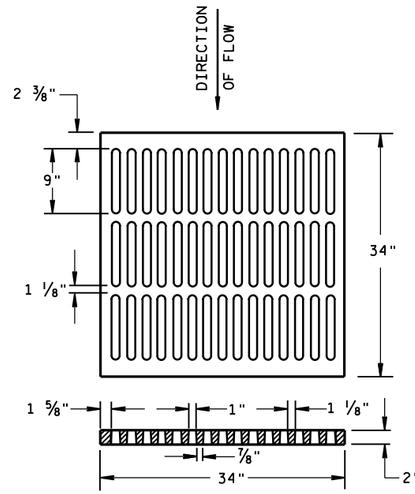
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

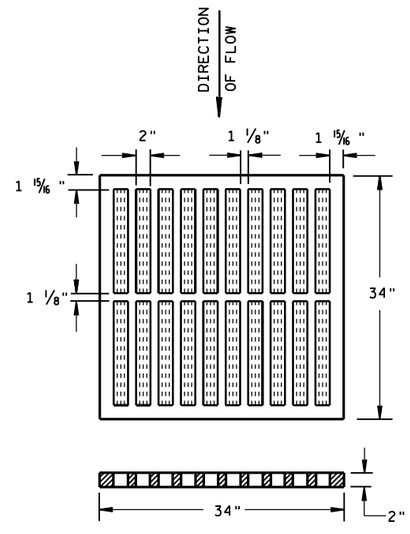
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



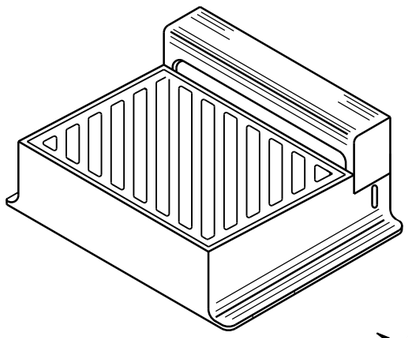
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE

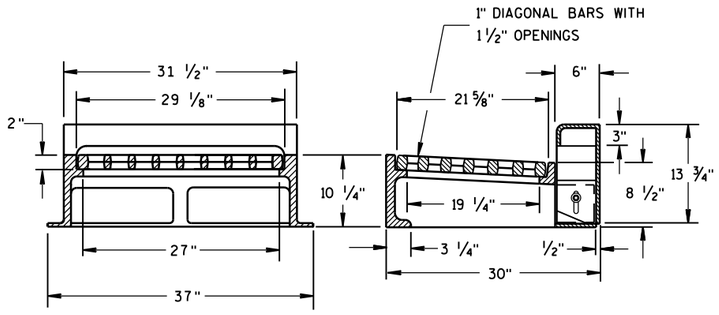


TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

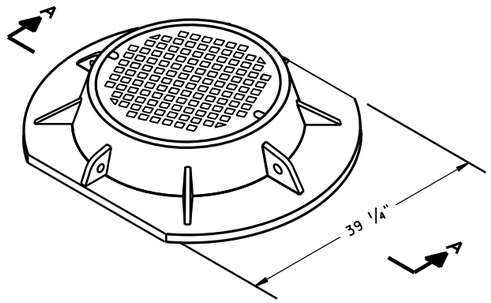
INLET COVERS
TYPE B, B-A, C,
MS, MS-A, & WM

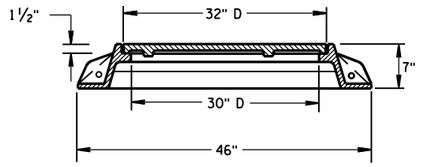
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/27/2013 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

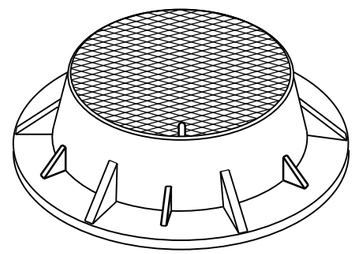
6

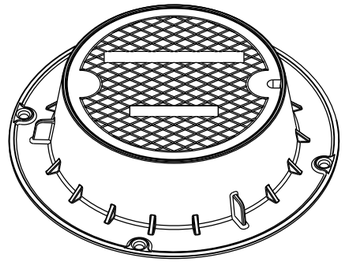
S.D.D. 8 A 5-19b

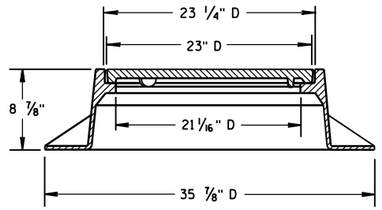


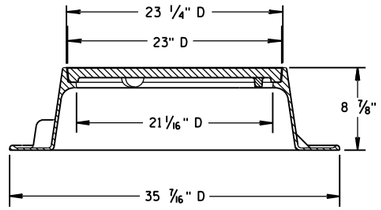


SECTION A-A
TYPE "K"









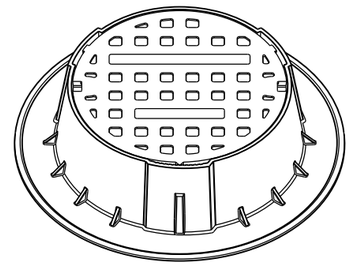
TYPE "J"
NOTE: EITHER CASTING IS ACCEPTABLE

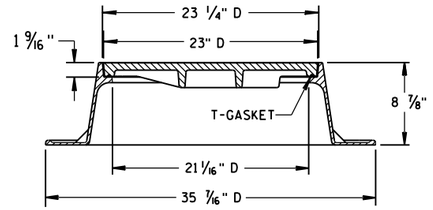
GENERAL NOTES

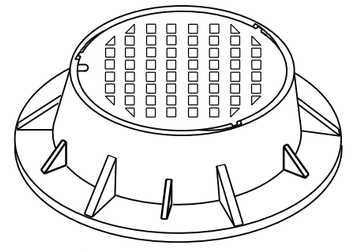
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

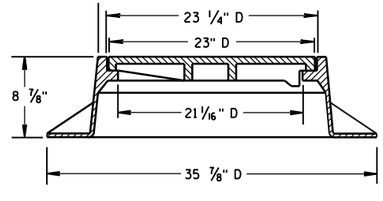
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

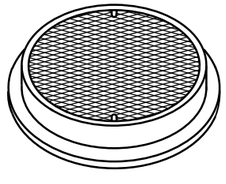


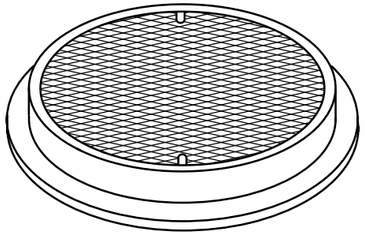


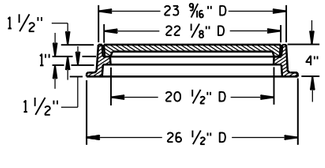




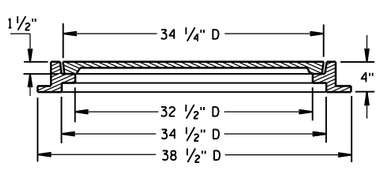
TYPE "J" SPECIAL
TYPE "B" NON-ROCKING SELF-SEAL LID
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)
NOTE: EITHER CASTING IS ACCEPTABLE



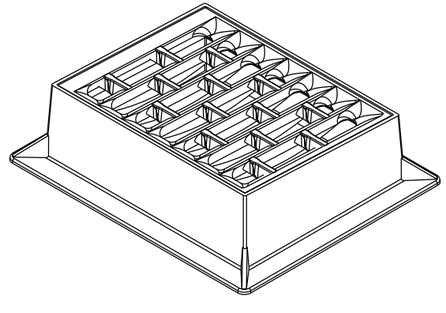


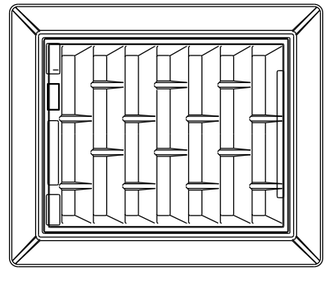


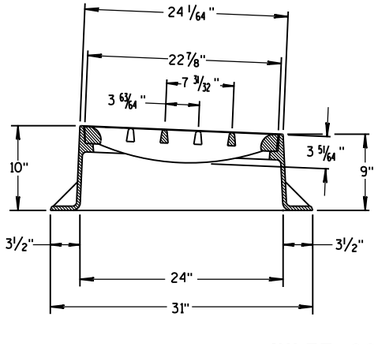
TYPE "L"

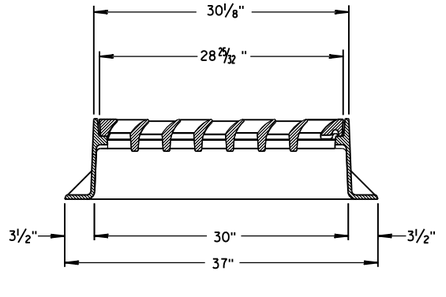


TYPE "M"









INLET COVER TYPE "BW"

INLET COVER TYPE BW
MANHOLE COVERS, TYPE K, J, J-S, L & M

STATE OF WISCONSIN
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11/27/2013
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/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

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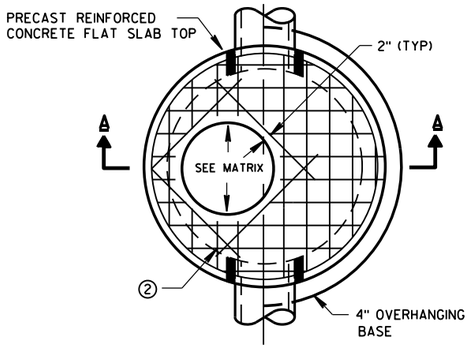
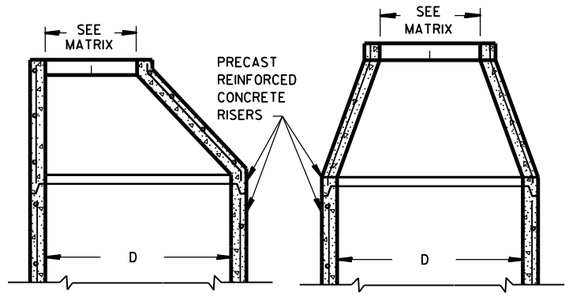
S.D.D. 8 A 5-19d

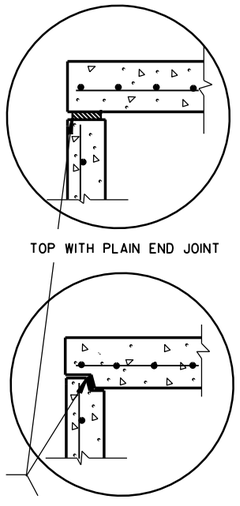


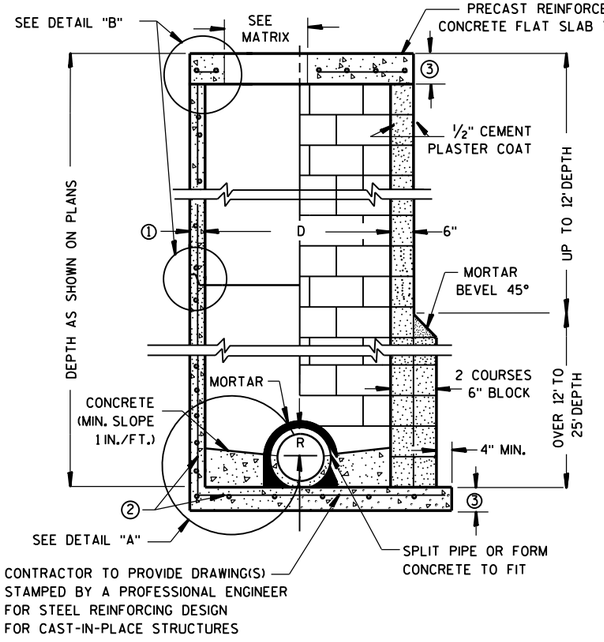
PLAN VIEW CIRCULAR OPENING



OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP

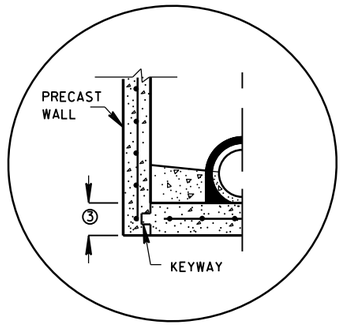
OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP



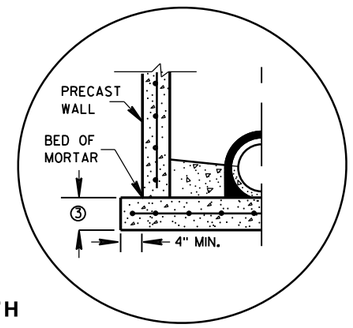


PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②

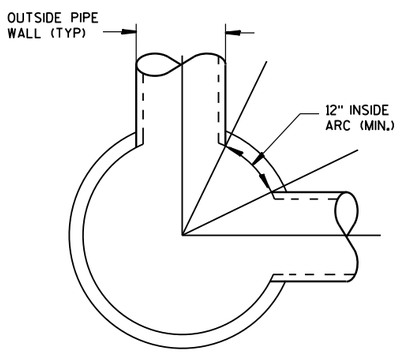


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

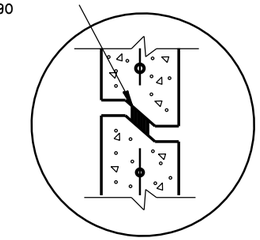


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

DETAIL "A"



DETAIL "C"



DETAIL "B"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES. THE CONE TOPS SHALL BE INSTALLED ON A BED OF MORTAR.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES, AND CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF ½" AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

CONCRETE BLOCK WILL NOT BE PERMITED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M 199.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT, 5 INCHES FOR 4-FT, 6 INCHES FOR 5-FT, 7 INCHES FOR 6-FT, 8 INCHES FOR 7-FT AND 9 INCHES FOR 8-FT DIAMETER PRECAST MANHOLES.

② FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

③ PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS OF 8".

MANHOLE COVER OPENING MATRIX

MANHOLE COVER TYPE	C	ALL J'S	K	L	M
OPENING SIZE (FT)					
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

MANHOLE SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	36
7-FT	48	36
8-FT	60	42

MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER

STATE OF WISCONSIN
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FHWA

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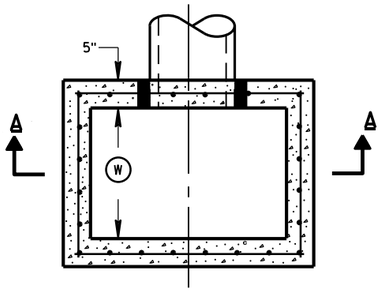
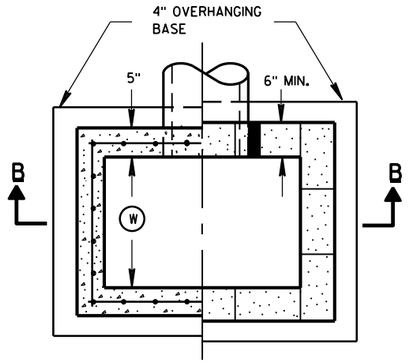
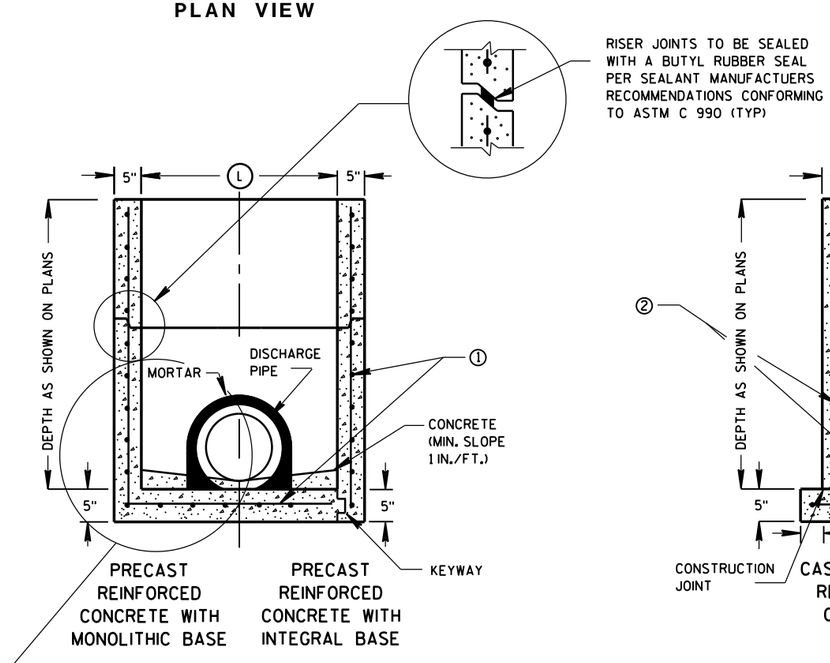


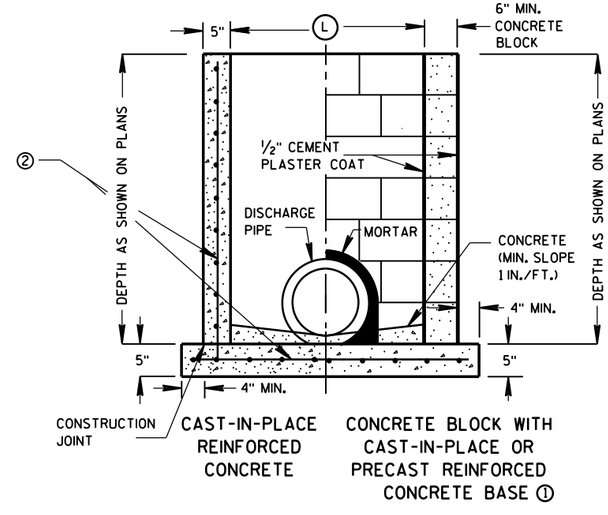
PLAN VIEW



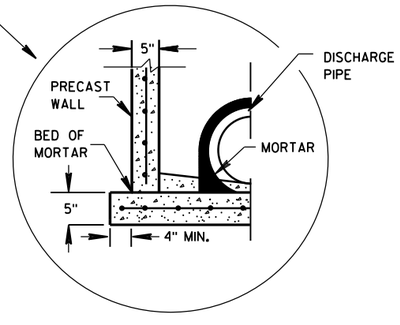
PLAN VIEW



SECTION A-A



SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS.
4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED.
OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

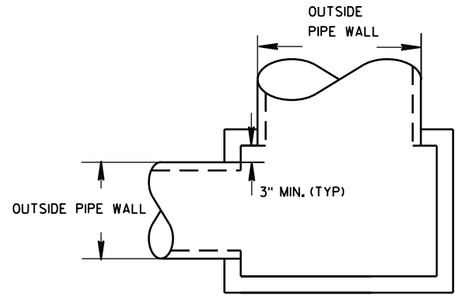
② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

INLET SIZE		INLET COVER TYPE	ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM
	WIDTH (W) (FT)	LENGTH (L) (FT)									
2X2-FT	2	2	X	X				X		X	
2X2.5-FT	2	2.5			X			X	X	X	X
2X3-FT	2	3					X				
2.5X3-FT	2.5	3				X					

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

STATE OF WISCONSIN
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ROADWAY STANDARDS DE
ENGINEER

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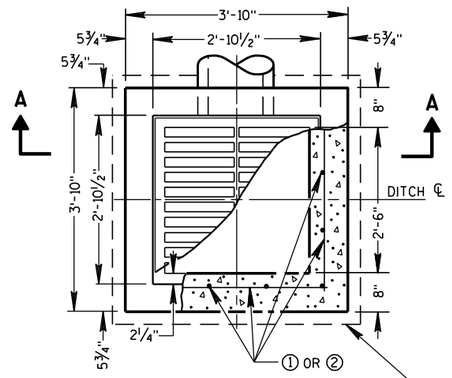
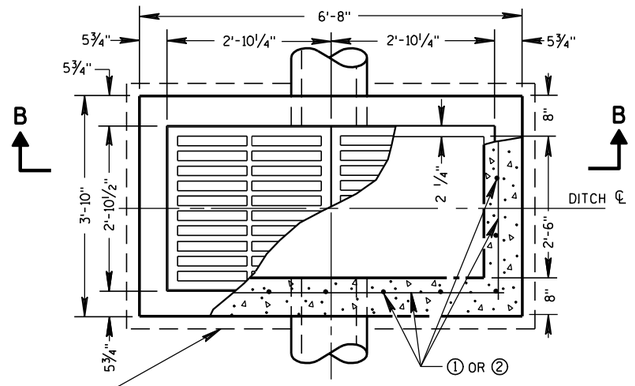
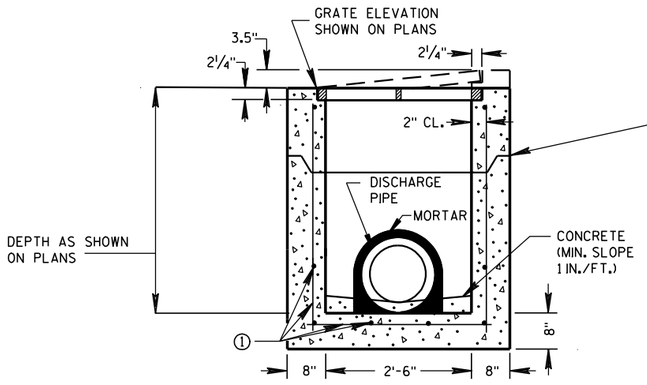


PLAN VIEW

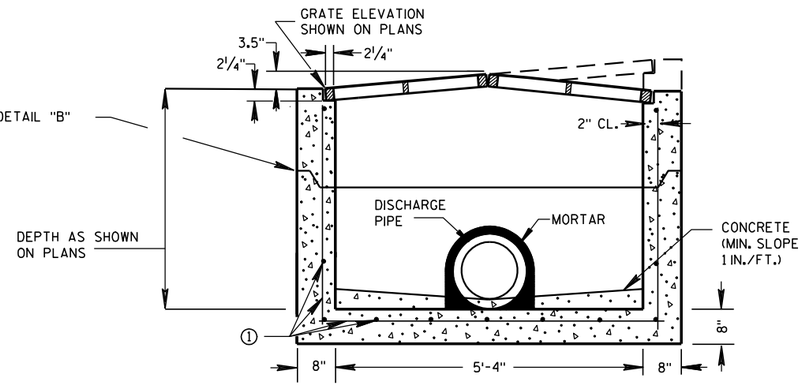


PLAN VIEW

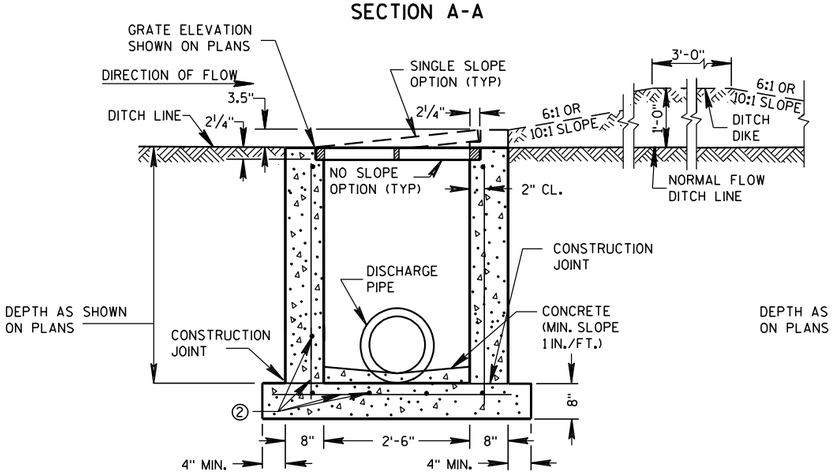
4" OVERHANGING BASE ON REINFORCED CAST-IN-PLACE CONCRETE INLETS



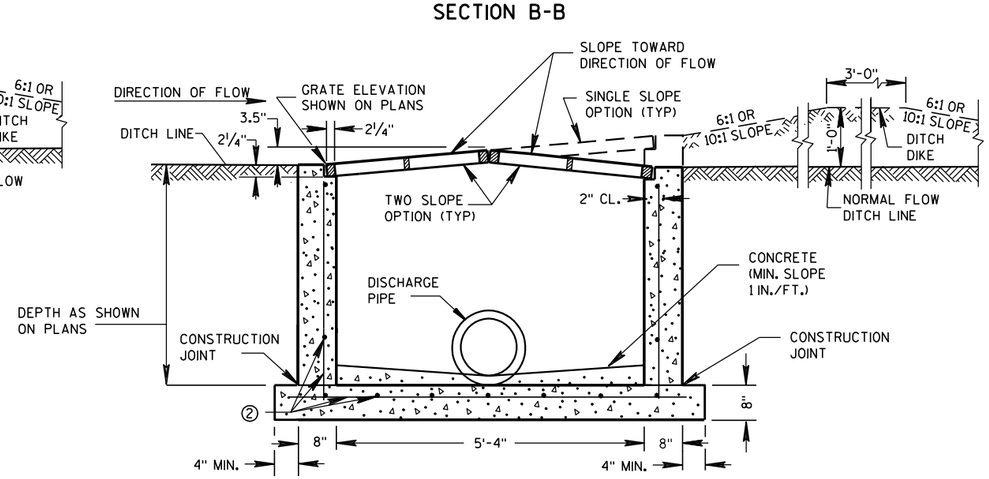
PRECAST REINFORCED CONCRETE SECTION A-A



PRECAST REINFORCED CONCRETE SECTION B-B



REINFORCED CAST-IN-PLACE CONCRETE SECTION A-A



REINFORCED CAST-IN-PLACE CONCRETE SECTION B-B

INLETS MEDIAN 1 GRATE

INLETS MEDIAN 2 GRATE

GENERAL NOTES

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UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLETS WHICH MAY INCLUDE PRECAST REINFORCED CONCRETE INLETS, SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL MEDIAN INLETS ARE DESIGNATED ON THE PLANS AS "INLETS, 1G-MS", ETC. THE FIRST NUMBER AND LETTER DESIGNATE THE TYPE OF STRUCTURE, AND THE FOLLOWING LETTERS DESIGNATE THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

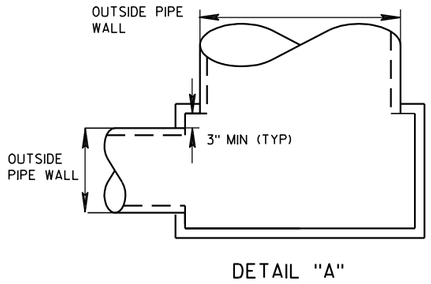
MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

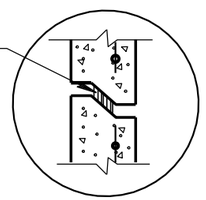
PIPE MATRIX

	MAXIMUM INSIDE PIPE DIAMETER	
INLET SIZE	WIDTH (IN)	LENGTH (IN)
1 GRATE	18	18
2 GRATE	18	42



DETAIL "A"

JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



DETAIL "B"

INLETS MEDIAN 1 AND 2 GRATE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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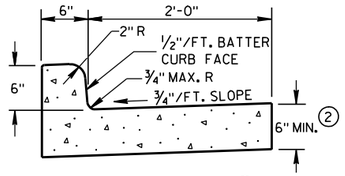
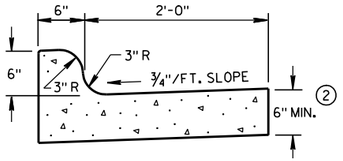
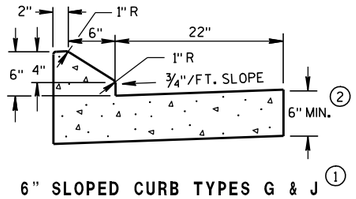


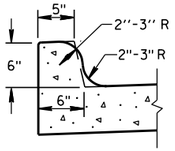
TYPES A & D (1)



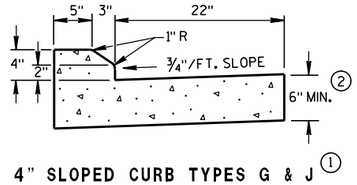
TYPES K & L (1)



6" SLOPED CURB TYPES G & J (1)

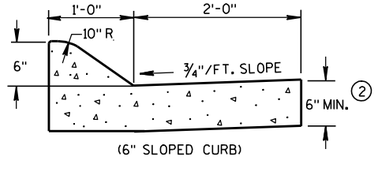


OPTIONAL CURB SHAPE FOR TYPES K & L (1)

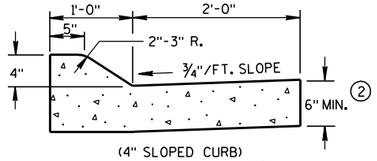


4" SLOPED CURB TYPES G & J (1)

CONCRETE CURB & GUTTER 30"

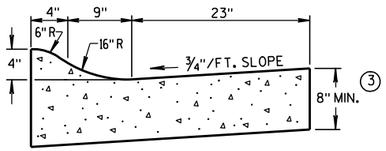


(6" SLOPED CURB)



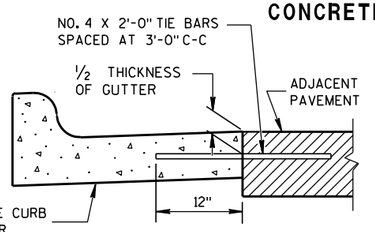
(4" SLOPED CURB)

TYPES A & D (1)

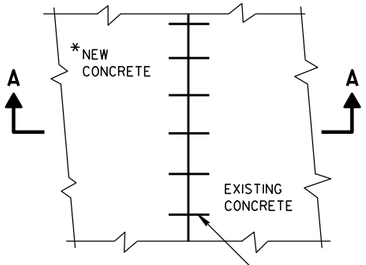


4" SLOPED CURB TYPES R & T (1) (4)

CONCRETE CURB & GUTTER 36"

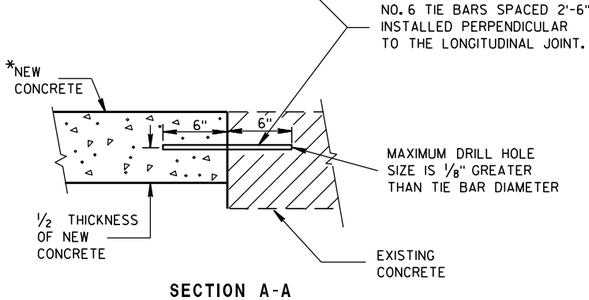


TYPICAL TIE BAR LOCATION (1)



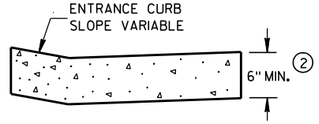
*NEW CURB & GUTTER, SURFACE DRAINS, CONCRETE PAVEMENT OR OTHER NEW CONCRETE.

PLAN VIEW



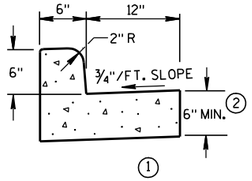
SECTION A-A

TIE BARS DRILLED INTO EXISTING PAVEMENT



DRIVEWAY ENTRANCE CURB

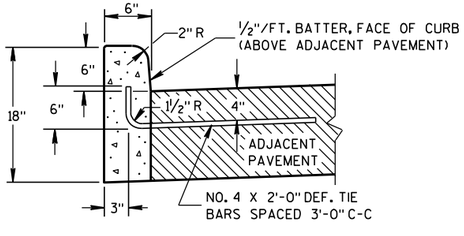
(WHEN DIRECTED BY THE ENGINEER)



(1)

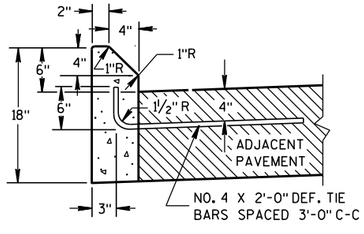
TYPES A & D

CONCRETE CURB & GUTTER 18"



(1)

TYPES A & D



(1)

TYPES G & J

CONCRETE CURB

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE. A LONGITUDINAL CONSTRUCTION JOINT IS NOT REQUIRED WITH INTEGRAL CURB AND GUTTER.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

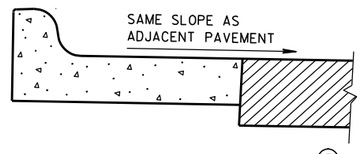
(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K AND R.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(3) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

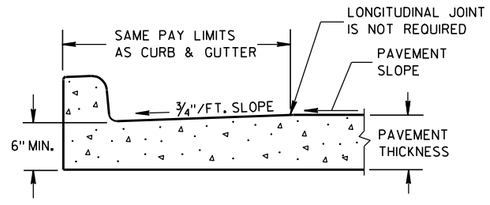
(4) THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

(5) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.

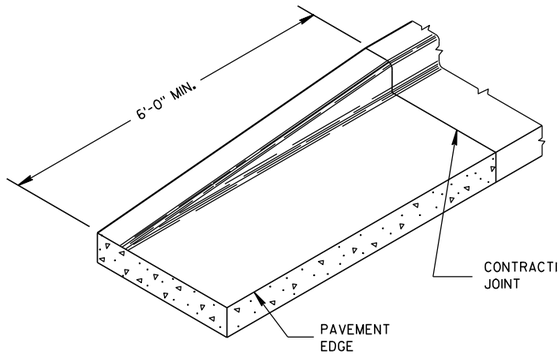


REVERSE SLOPE GUTTER (5)

(TYPICAL FOR ALL CURB & GUTTER TYPES)



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB & GUTTER



END SECTION CURB & GUTTER

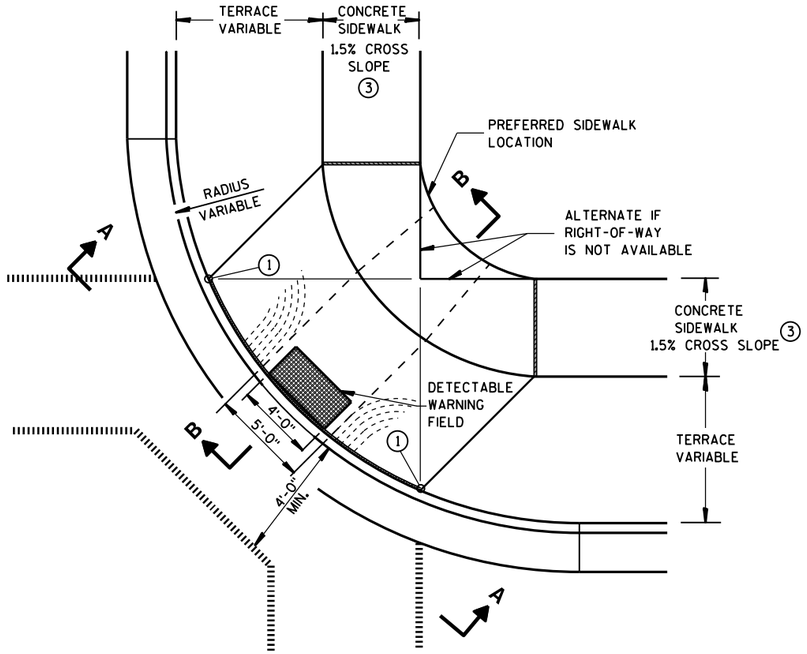
CONCRETE CURB, CONCRETE CURB & GUTTER AND TIES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
9/4/08 DATE
/S/ Jerry H. [illegible]
ROADWAY STANDARDS ENGINEER
FHWA

6

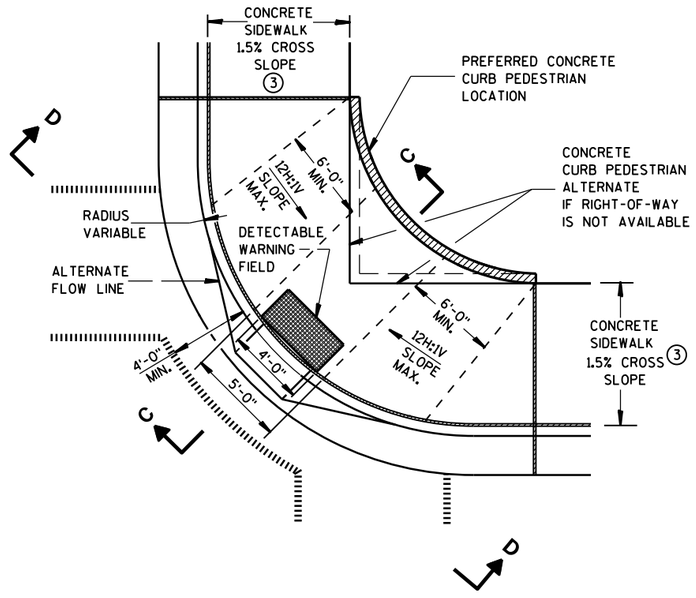
S.D.D. 8 D 1-17



PLAN VIEW

TYPE 1 RAMP

(CENTER OF CORNER RADIUS)



PLAN VIEW

TYPE 1-A RAMP

(NO TERRACE)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

RAMPS SHALL BE BUILT AT 12H:1V OR FLATTER. WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAL FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD".

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

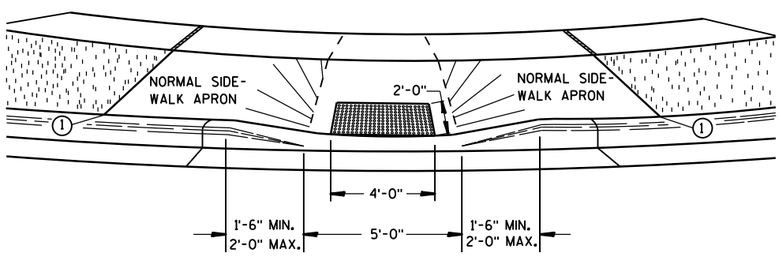
① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB.

② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE.

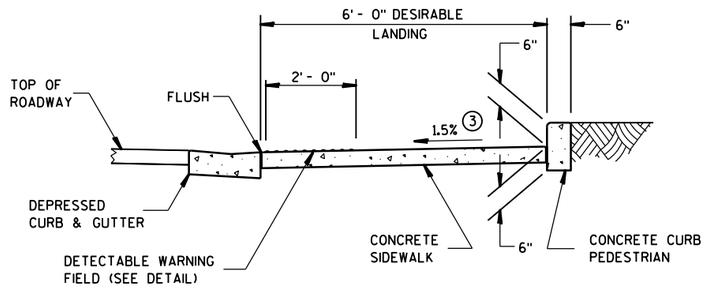
③ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

LEGEND

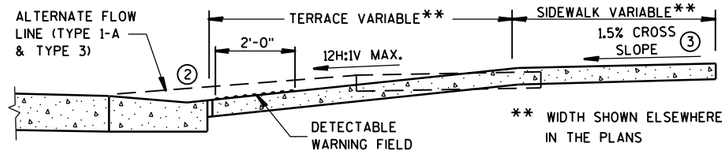
- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



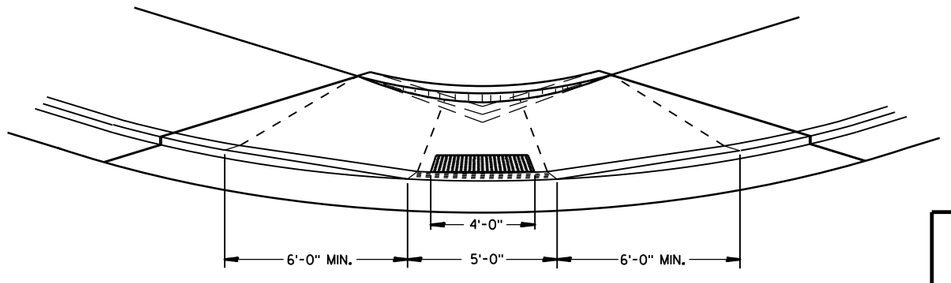
VIEW A-A



SECTION C-C



SECTION B-B



VIEW D-D

CURB RAMPS TYPES 1 AND 1-A

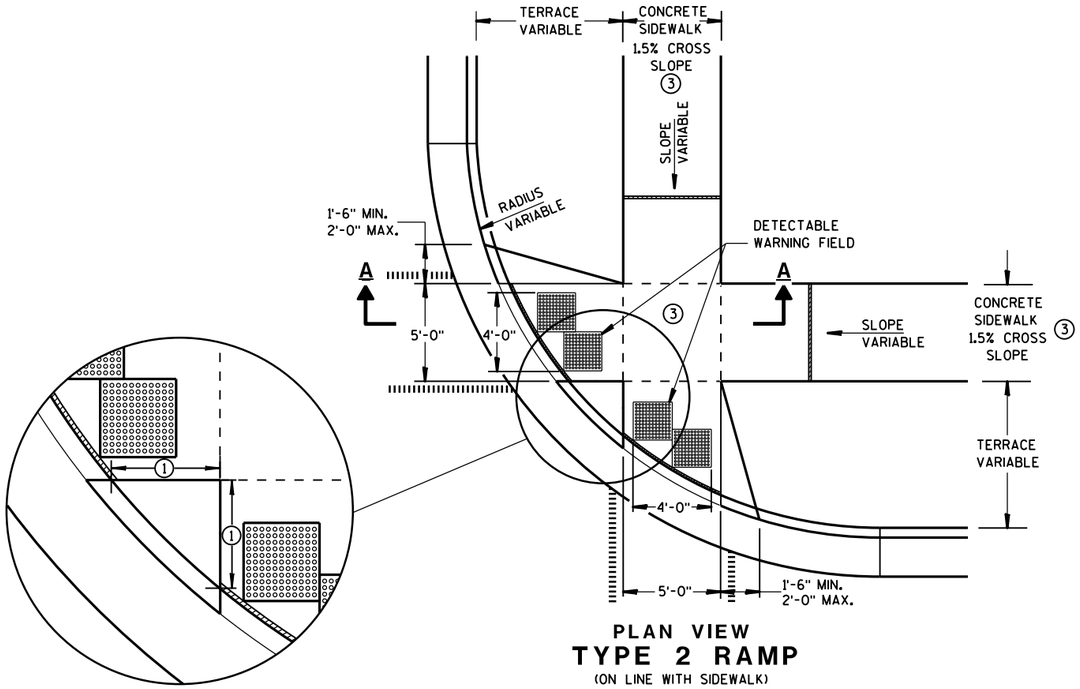
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 8 D 5-15a

6

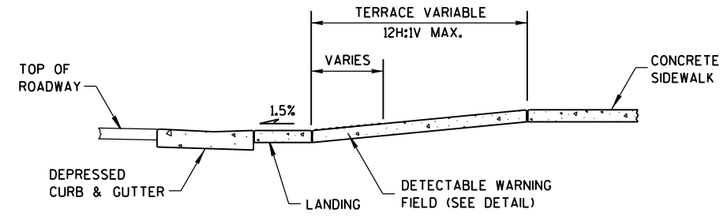
S.D.D. 8 D 5-15a



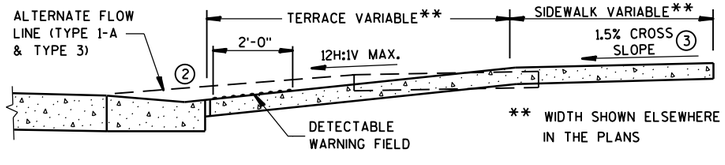
PLAN VIEW

TYPE 2 RAMP

(ON LINE WITH SIDEWALK)



SECTION A-A



SECTION B-B

GENERAL NOTES

USE THE TYPE 3 RAMP ONLY WHEN A TYPE 1 OR TYPE 2 CANNOT BE ACHIEVED BECAUSE OF FIELD CONDITIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

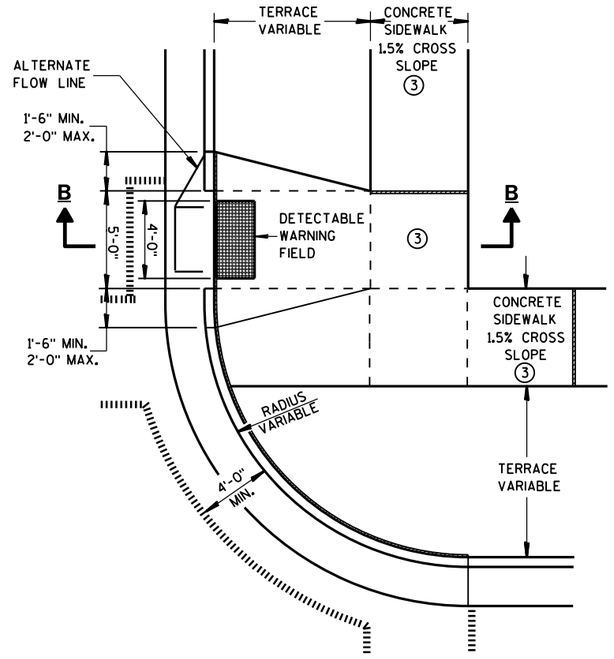
① WHEN THIS DISTANCE IS LESS THAN 6'-0" IT MAY BE DIFFICULT TO ACHIEVE A 12H:1V SLOPE, OR FLATTER, ON THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 12H:1V SLOPE, OR FLATTER, ON RAMP. 2" MINIMUM CURB HEIGHT.

② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE.

③ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



PLAN VIEW

TYPE 3 RAMP

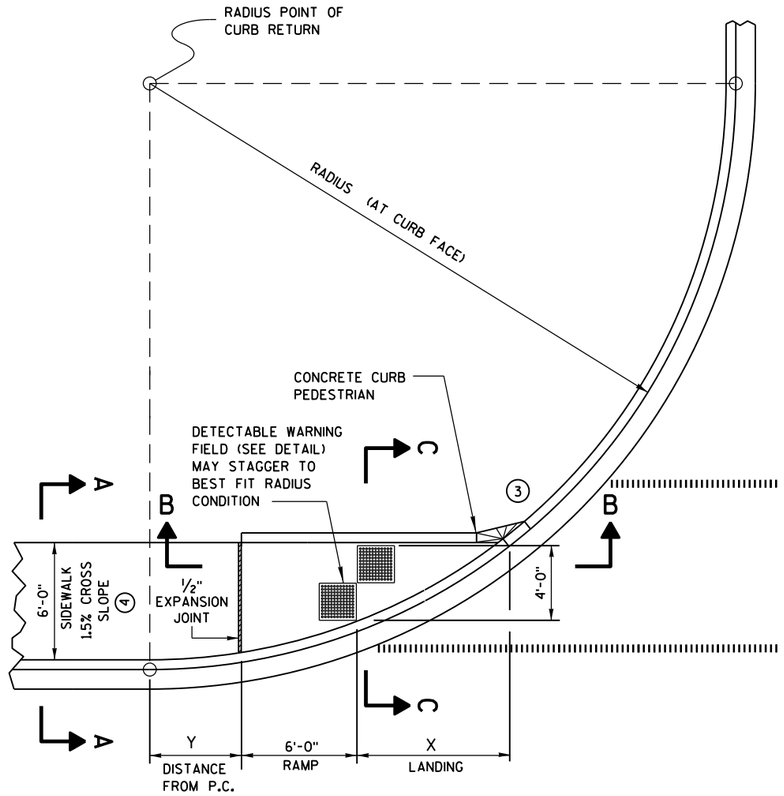
(OUTSIDE OF CROSSWALK AREA)

CURB RAMPS TYPES 2 AND 3

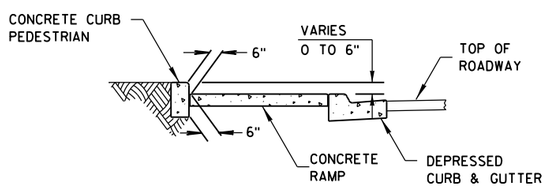
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

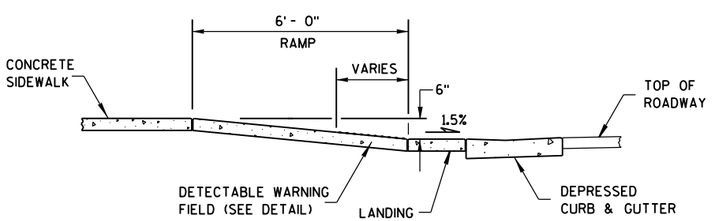
S.D.D. 8 D 5-15b



CURB RAMP TYPE 4A
PLAN VIEW



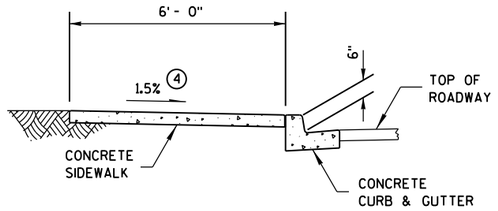
SECTION C-C FOR TYPE 4A



SECTION B-B FOR TYPE 4A

RADIUS (AT CURB FACE)	X	Y
20 FEET	6'-1¾"	2'-7¼"
30 FEET	7'-11¾"	4'-8¼"
40 FEET	9'-5¼"	6'-5"
50 FEET	10'-8¾"	7'-11¼"
60 FEET	11'-10¼"	9'-3½"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A-A FOR TYPE 4A

GENERAL NOTES

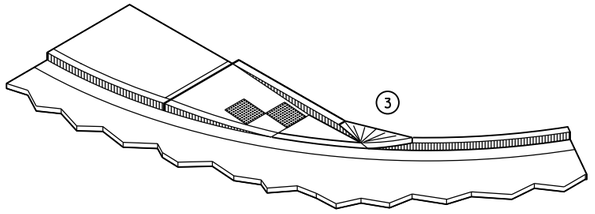
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

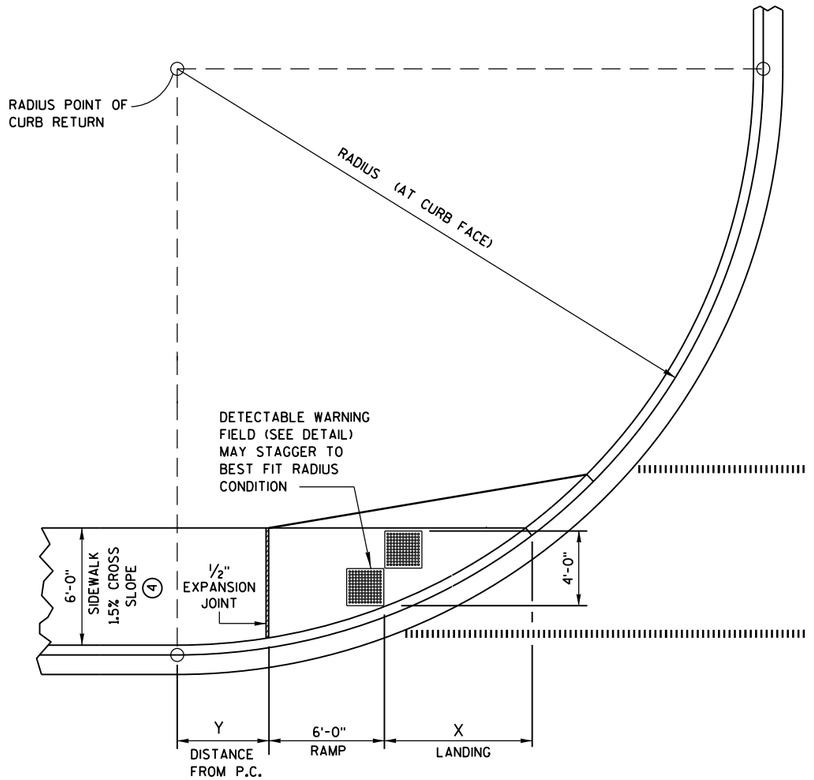
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

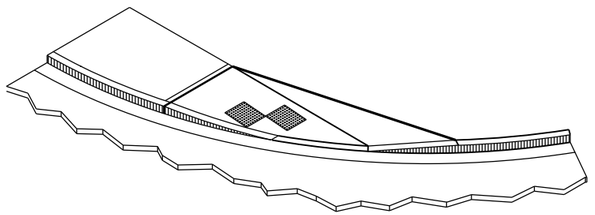
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



ISOMETRIC VIEW FOR TYPE 4A



CURB RAMP TYPE 4A1
PLAN VIEW



ISOMETRIC VIEW FOR TYPE 4A1

LEGEND

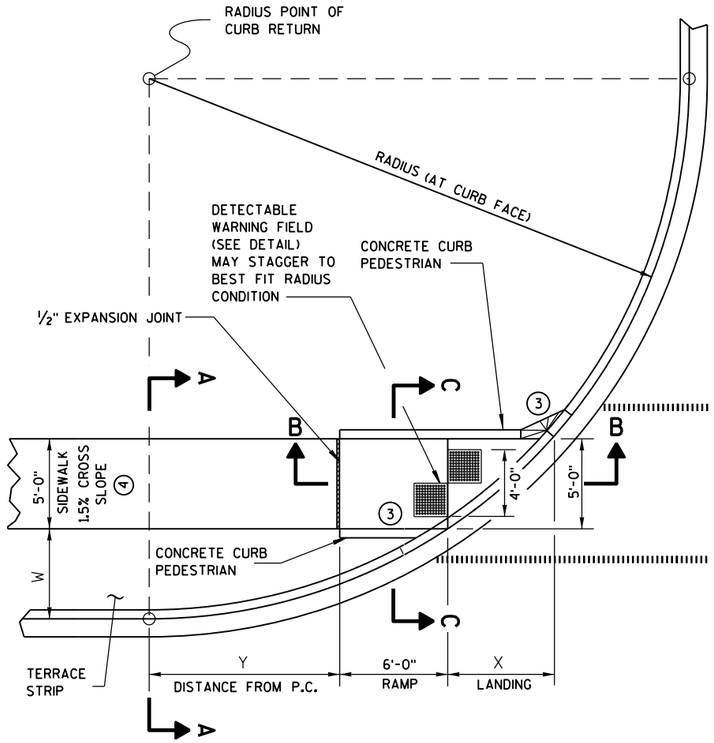
- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPES 4A AND 4A1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 8 D 5-15c



CURB RAMP TYPE 4B
PLAN VIEW

LEGEND

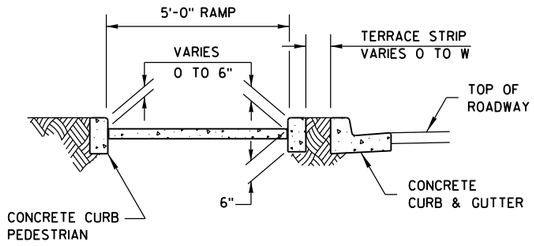
½" EXPANSION JOINT-SIDEWALK

CONTRACTION JOINT FIELD LOCATED

PAVEMENT MARKING CROSSWALK (WHITE)

RADIUS (AT CURB FACE)	W = 3'-0"		W = 4'-0"		W = 5'-0"		W = 6'-0"		W = 7'-0"	
	X	Y	X	Y	X	Y	X	Y	X	Y
20 FEET	5'-5½"	4'-6½"	4'-8½"	6'-0"	4'-1"	7'-2¾"	3'-7"	8'-3½"	3'-1½"	9'-2½"
30 FEET	7'-3¾"	7'-1"	6'-5½"	8'-11½"	5'-9¼"	10'-7"	5'-2½"	12'-0"	4'-8¾"	13'-3¼"
40 FEET	8'-9½"	9'-2½"	7'-10"	11'-5¼"	7'-1"	13'-4½"	6'-5¾"	15'-¾"	5'-11½"	16'-7¼"
50 FEET	10'-¾"	11'-¾"	9'-¼"	13'-7¼"	8'-2½"	15'-9½"	7'-6½"	17'-9"	6'-11¾"	19'-6¼"
60 FEET	11'-2½"	12'-8¾"	10'-¾"	15'-6½"	9'-2¼"	17'-11¾"	8'-5¾"	20'-1¾"	7'-10½"	22'-1½"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION C-C FOR TYPE 4B

GENERAL NOTES

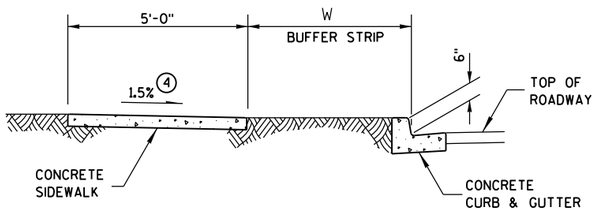
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

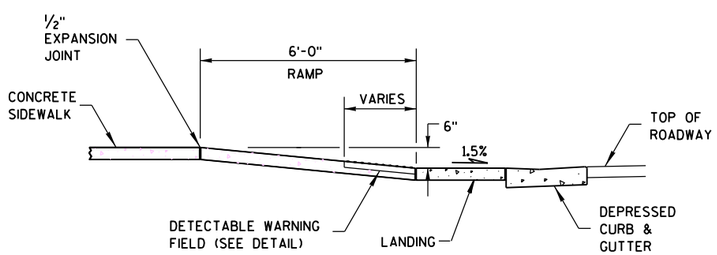
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

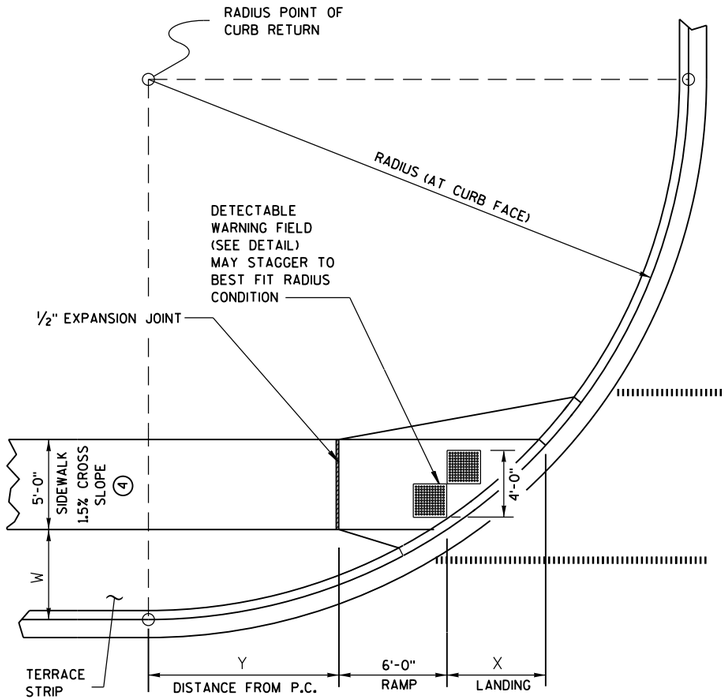
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



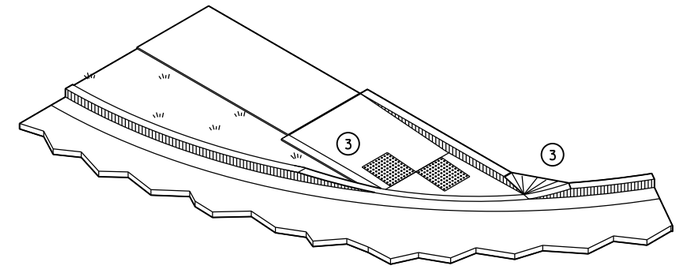
SECTION A-A FOR TYPE 4B



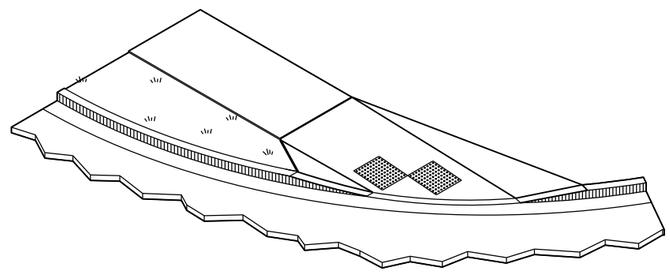
SECTION B-B FOR TYPE 4B



CURB RAMP TYPE 4B1
PLAN VIEW



ISOMETRIC VIEW FOR TYPE 4B



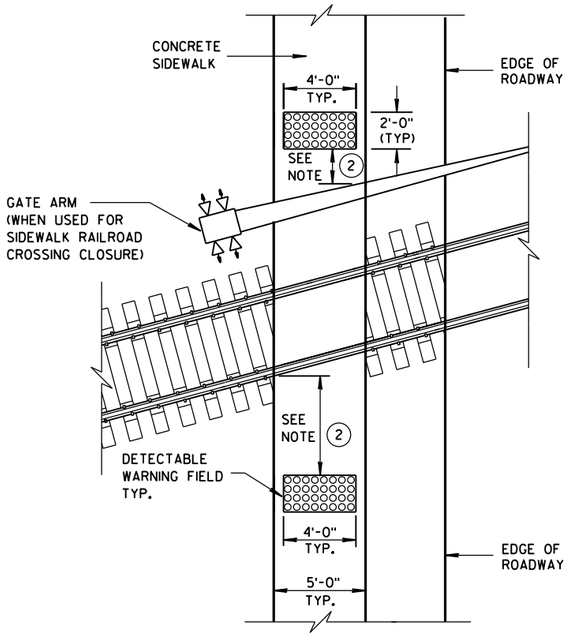
ISOMETRIC VIEW FOR TYPE 4B1

CURB RAMPS
TYPE 4B AND 4B1

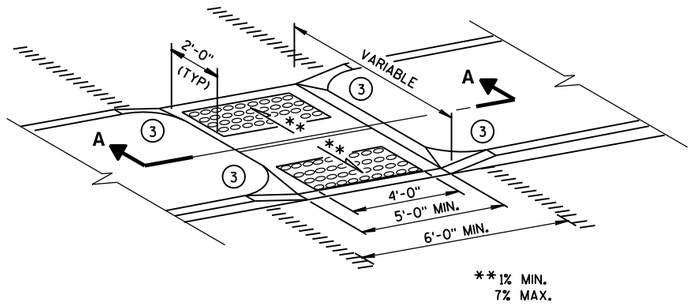
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 8 D 5-15d



TYPE 8
DETECTABLE WARNINGS AT RAILROAD CROSSING



MEDIAN ISLAND
NON-ELEVATED CROSSING
TYPE 5

GENERAL NOTES

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

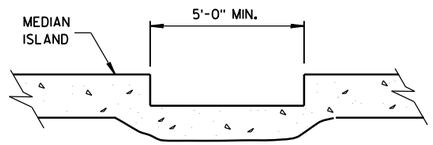
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(1) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

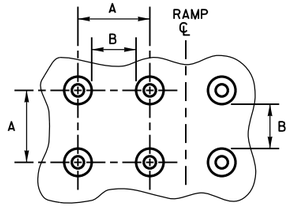
(2) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ± 0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



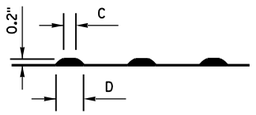
SECTION A-A



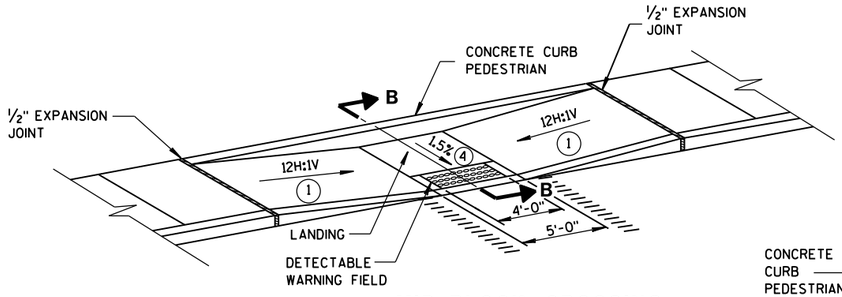
PLAN VIEW

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

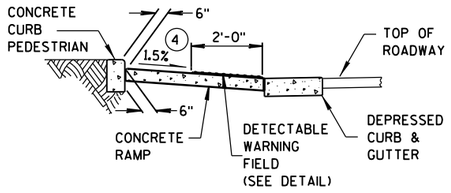
* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.



ELEVATION VIEW

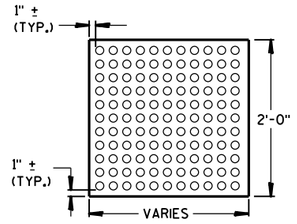


MID-BLOCK CROSSING
TYPE 7A



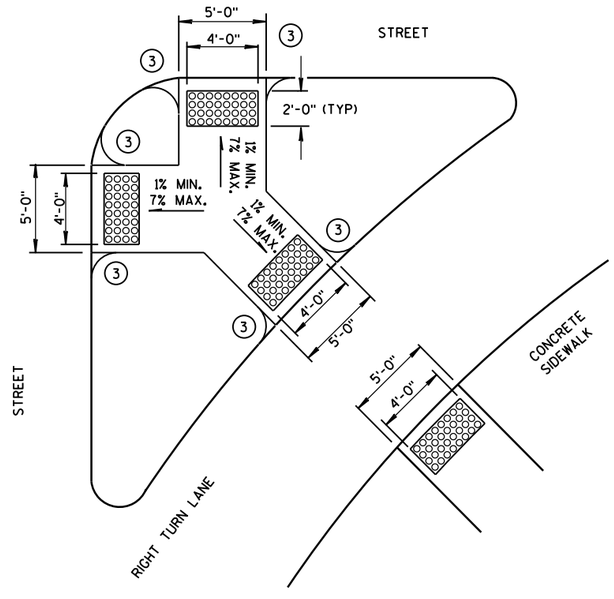
SECTION B-B

TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL

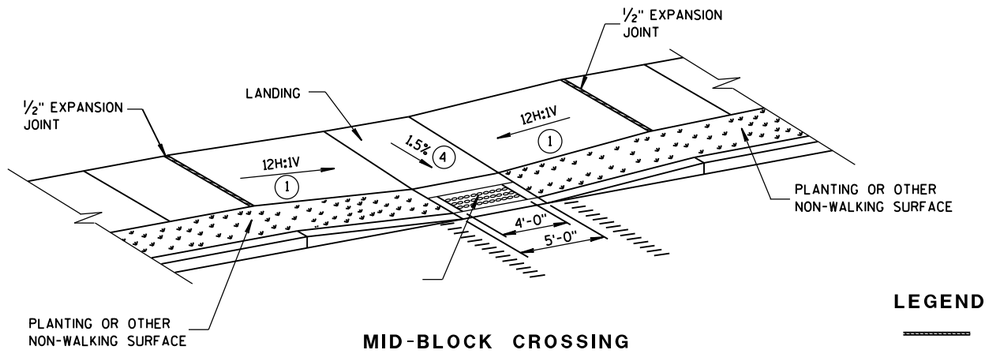


PLAN VIEW

DETECTABLE WARNING FIELD (TYPICAL)



TYPE 6
DETECTABLE WARNING AT ISLANDS



MID-BLOCK CROSSING
TYPE 7B

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPES 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

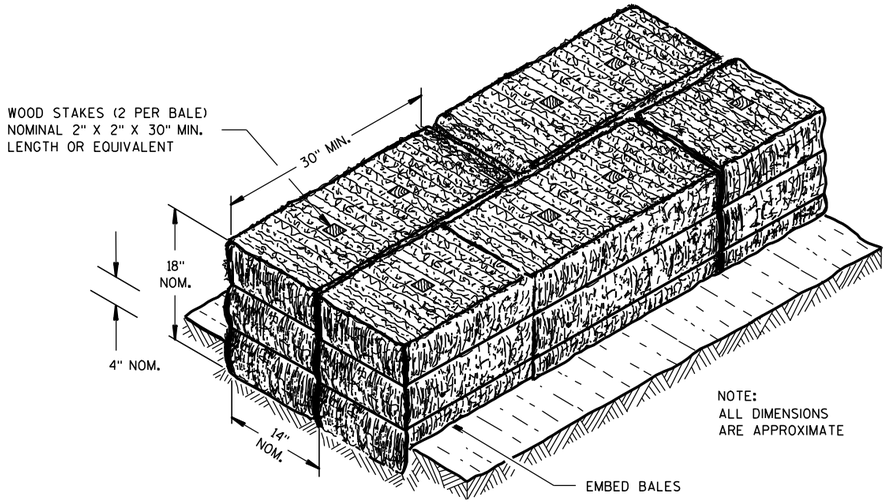
APPROVED
2-6-2013 DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS [illegible] ENGINEER
FHWA

6

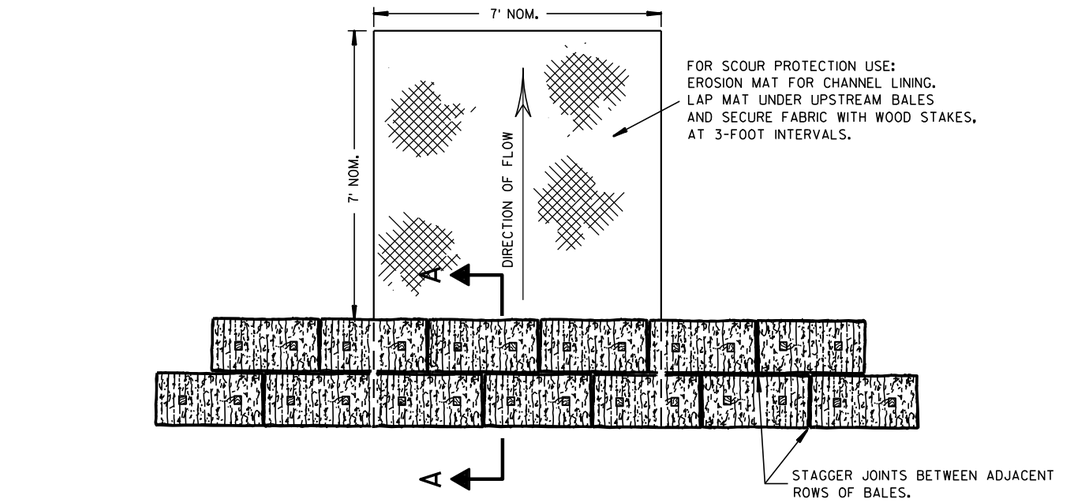
S.D.D. 8 D 5-15e

6

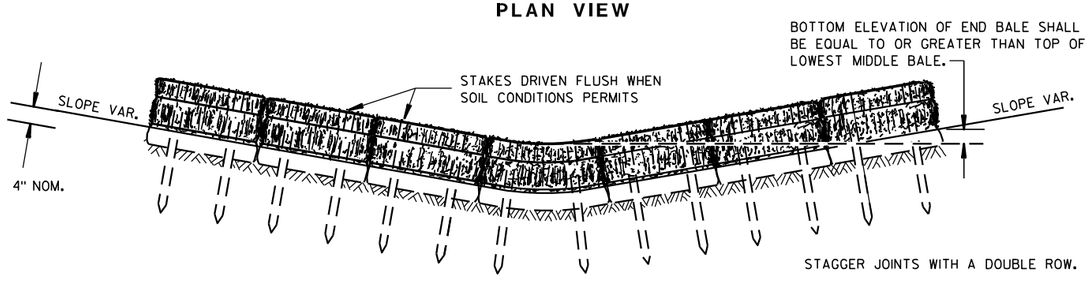
S.D.D. 8 D 5-15e



SECTION A-A



PLAN VIEW



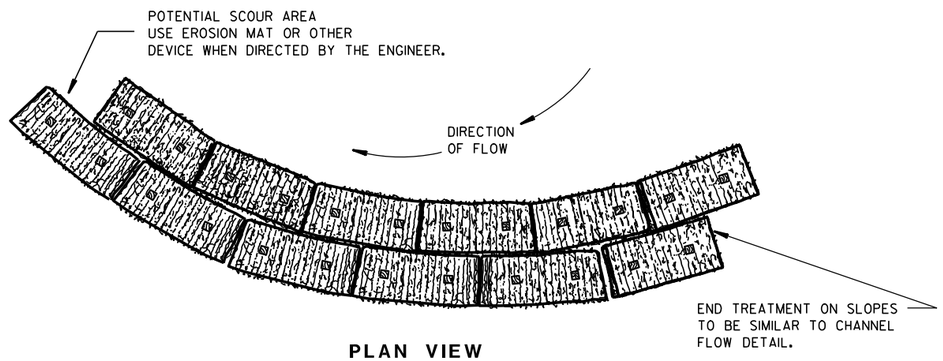
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES ①

GENERAL NOTES

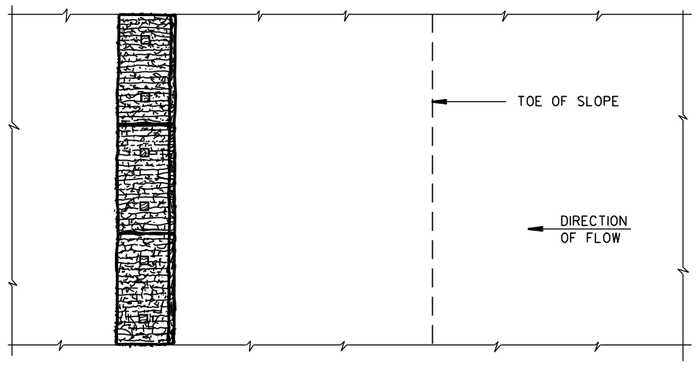
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

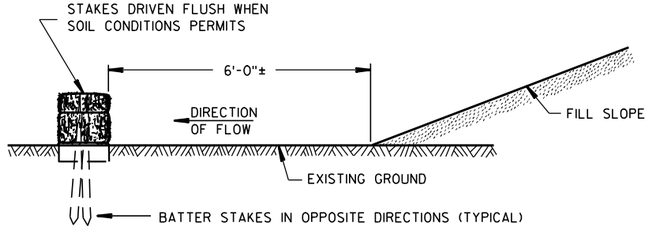


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

6/04/02 DATE — /S/ Beth Cannestra — CHIEF ROADWAY DEVELOPMENT ENGINEER

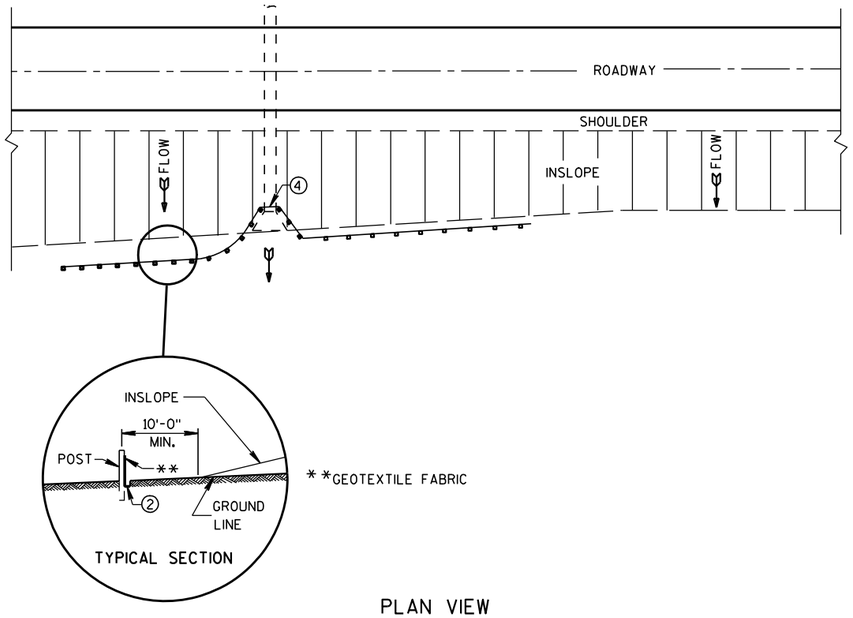
FHWA

6

S.D.D. 8 E 8-3

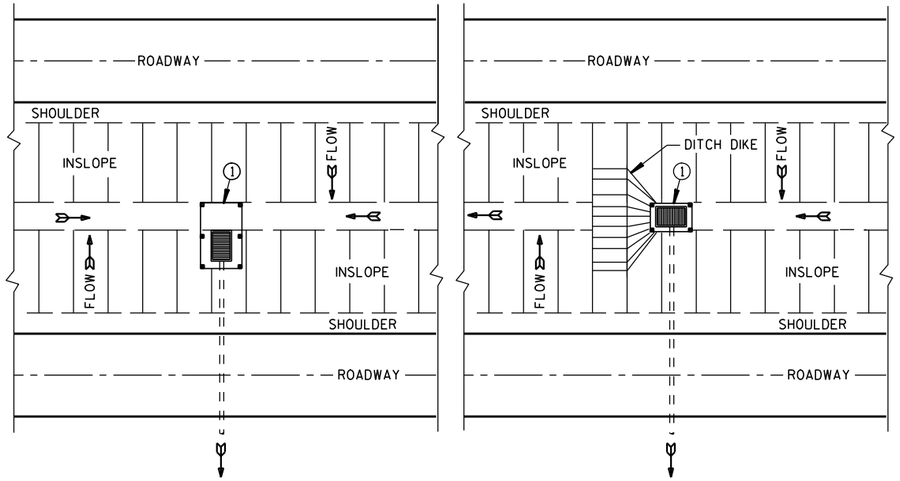
6

S.D.D. 8 E 8-3



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

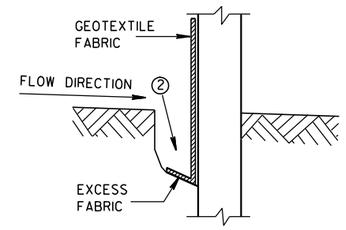
① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.

② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.

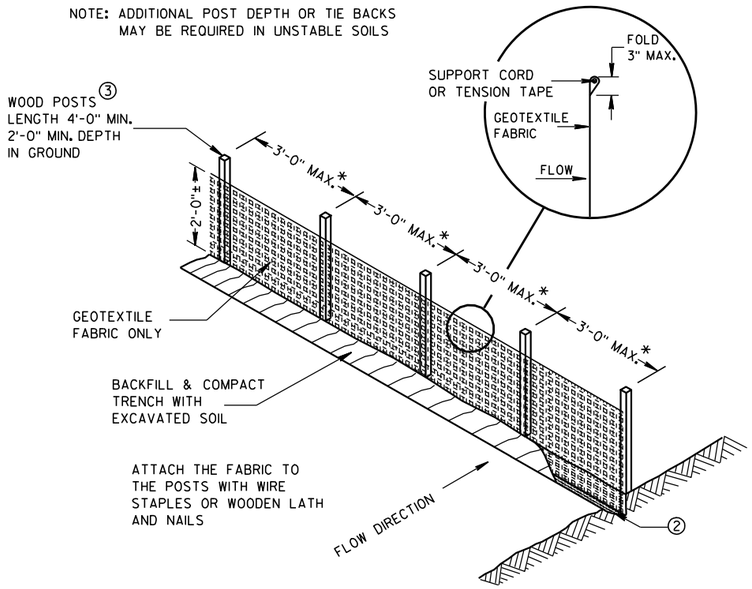
③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.

④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.

⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

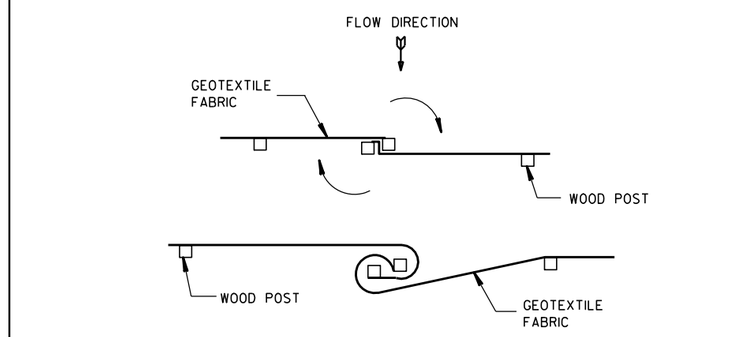


TRENCH DETAIL

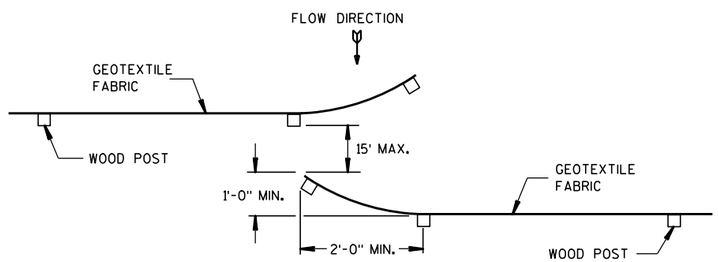


*NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

SILT FENCE

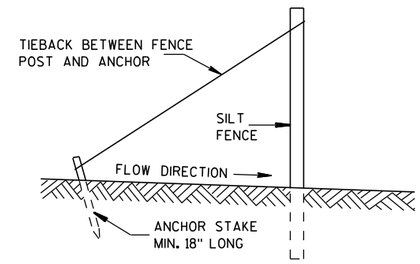


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

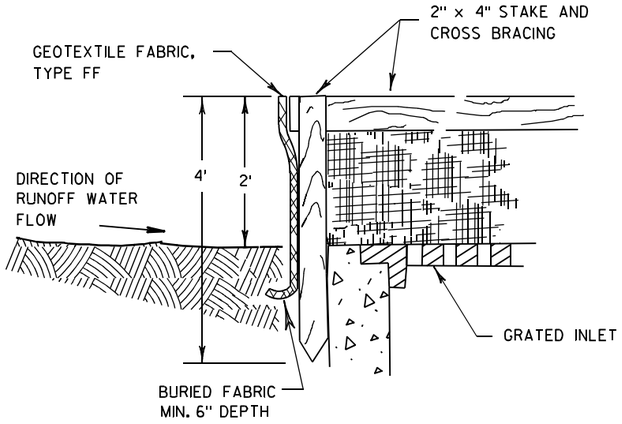
APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

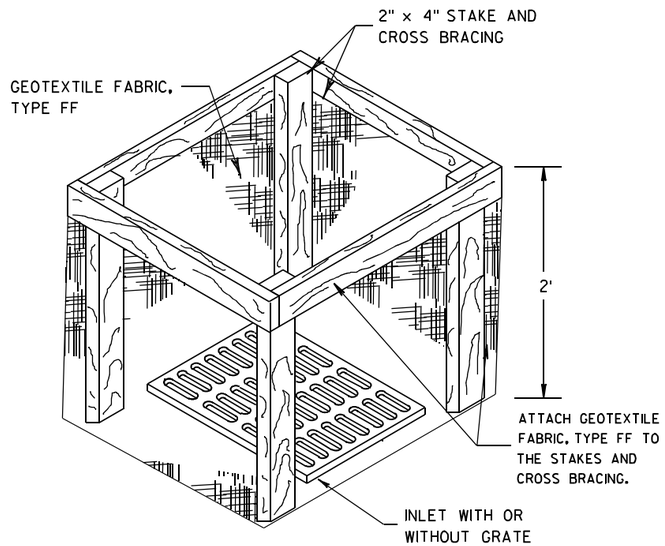
6

S.D.D. 8 E 9-6

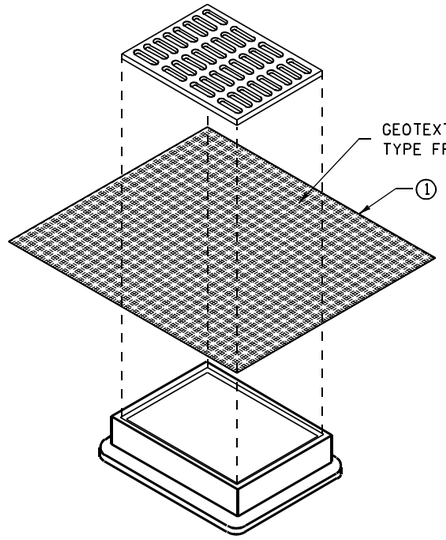
6

S.D.D. 8 E 9-6



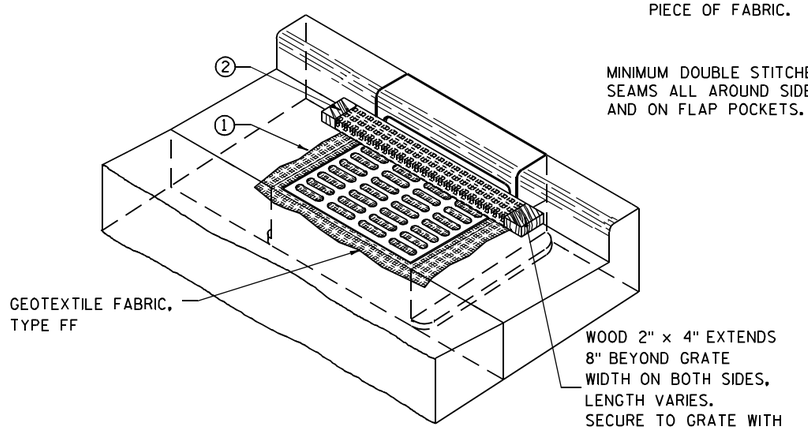


INLET PROTECTION, TYPE A

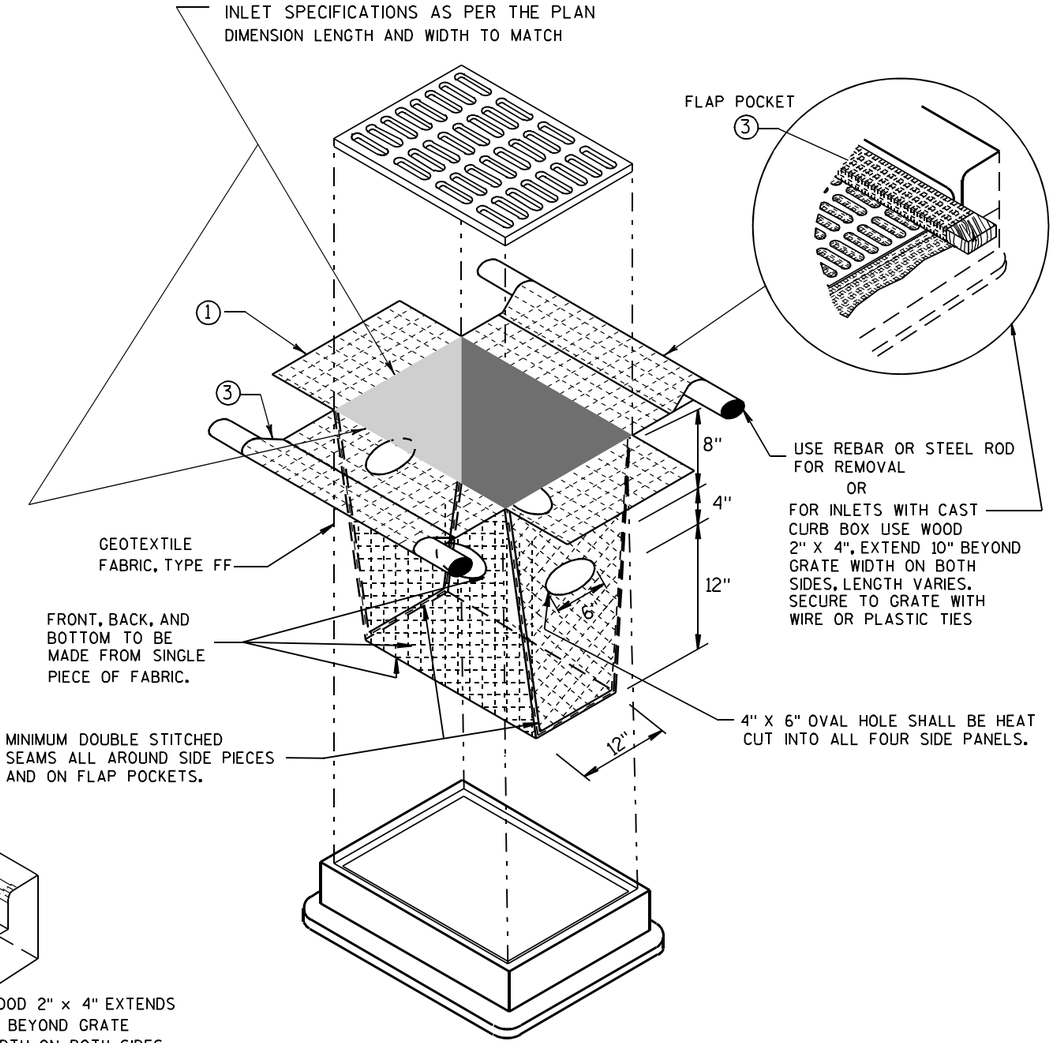


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6
S.D.D. 8 E 10-2

METAL APRON ENDWALLS

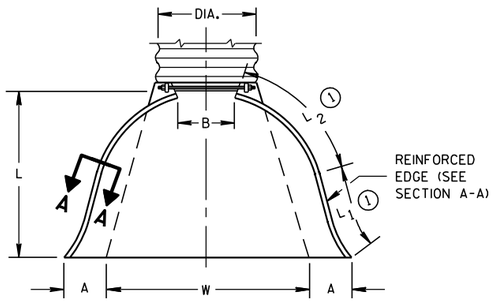
PIPE DIA. (IN.)	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109×	.105×	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109×	.105×	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109×	.105×	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109×	.105×	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109×	.105×	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109×	.105×	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109×	.105×	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

× EXCEPT CENTER PANEL SEE GENERAL NOTES

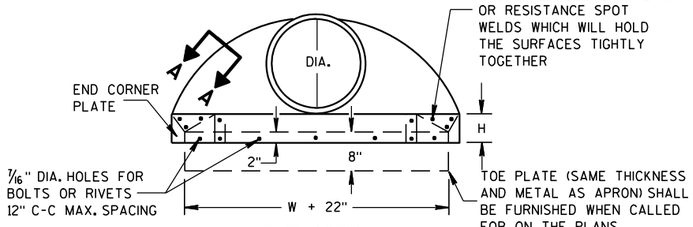
REINFORCED CONCRETE APRON ENDWALLS

PIPE DIA. (IN.)	DIMENSIONS (Inches) T	A	B	C	D	E	G	APPROX. SLOPE
12	2	4	24	48 7/8	72 1/8	24	2	3 to 1
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1
48	5	24	72	26	98	84	5	3 to 1
54	5 1/2	27	65	* 33 1/4 - ** 35	* 98 1/4 - ** 100	90	5 1/2	2 2/5 to 1
60	6	* 30 - ** 35	60	39	99	96	5	2 to 1
66	6 1/2	* 24 - ** 30	* 72 - ** 78	* 21 - ** 27	99	102	5 1/2	2 to 1
72	7	* 24 - ** 36	78	21	99	108	6	2 to 1
78	7 1/2	* 24 - ** 36	78	21	99	114	6 1/2	2 to 1
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1

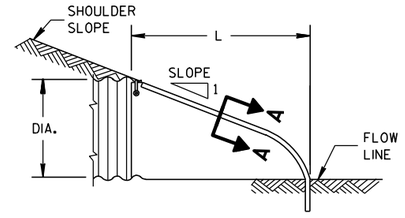
*MINIMUM
**MAXIMUM



PLAN VIEW

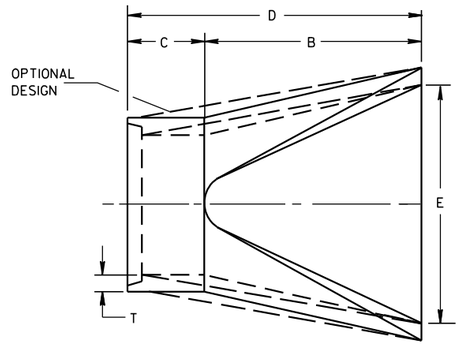


END VIEW

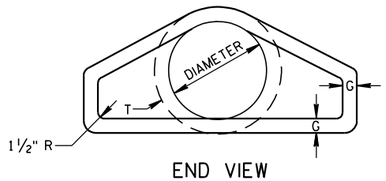


SIDE ELEVATION

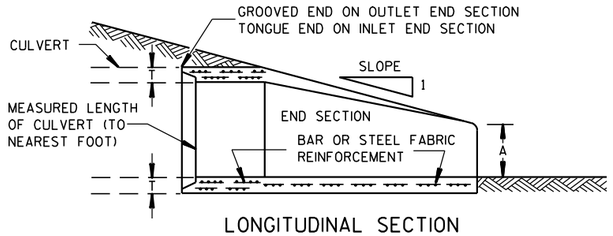
METAL ENDWALLS



PLAN

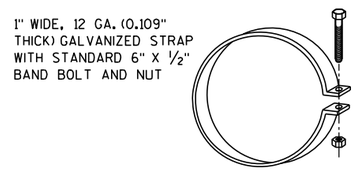


END VIEW



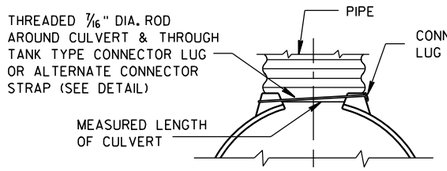
LONGITUDINAL SECTION

CONCRETE ENDWALLS

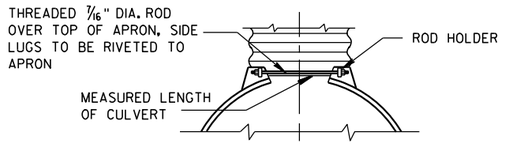


ALTERNATE FOR TYPE 1 CONNECTION

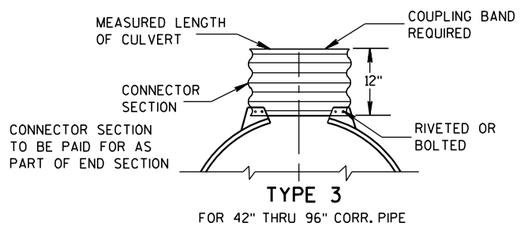
END SECTION CONNECTOR STRAP



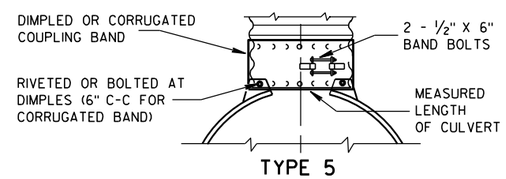
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



TYPE 5
ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

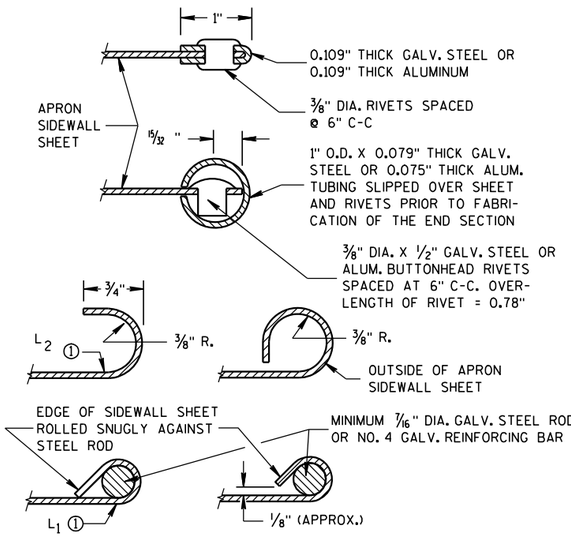
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL. AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

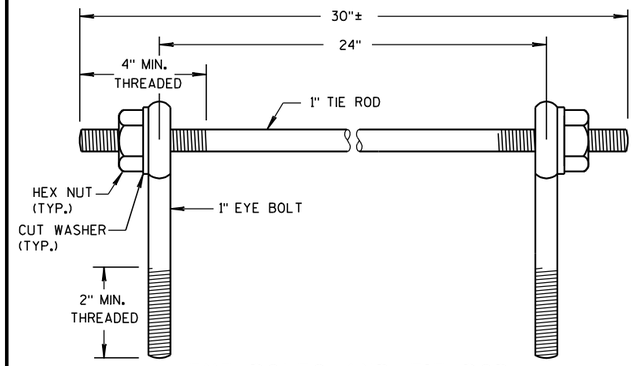
APRON ENDWALLS FOR CULVERT PIPE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

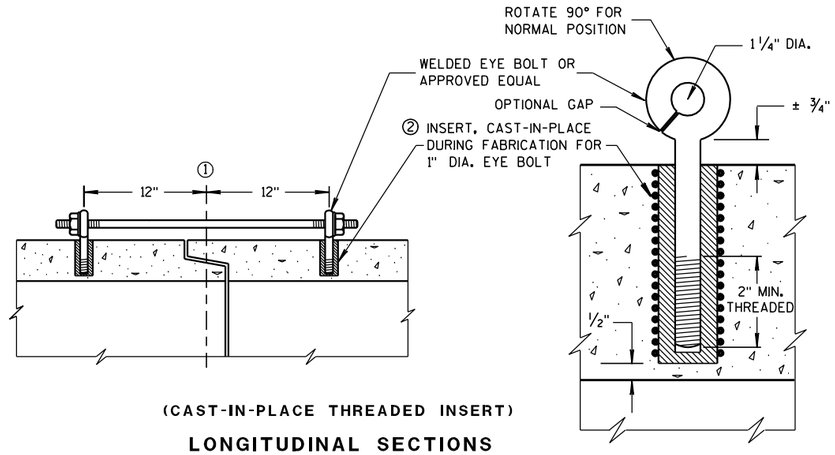
APPROVED
11/30/94 DATE
/S/ Rory L. Rhinesmith
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

S.D.D. 8 F 1-11



EYE BOLTS AND TIE ROD



(CAST-IN-PLACE THREADED INSERT)

LONGITUDINAL SECTIONS

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

① ℄ OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.

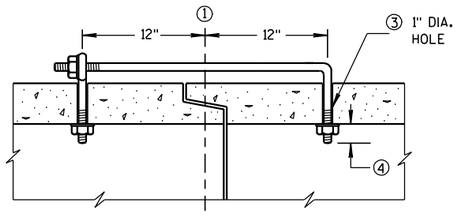
② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.

③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED 12 INCHES FROM ℄ OF TONGUE AND GROOVE.

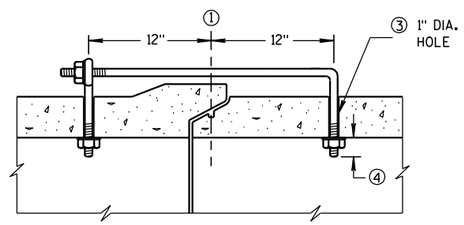
④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.

⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.

⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.



(TONGUE & GROOVE PIPE)

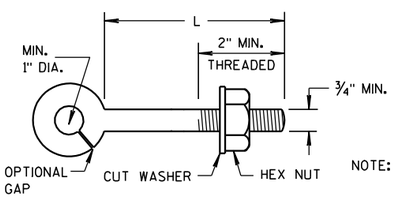


(MODIFIED BELL PIPE)

LONGITUDINAL SECTION

EYE BOLT DIMENSION TABLE

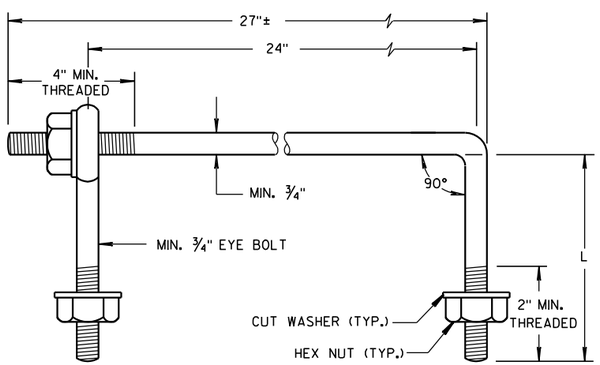
PIPE SIZE	L = LENGTH	
	TONGUE & GROOVE PIPE	MODIFIED BELL PIPE
18" TO 24"	4 1/2"	6 1/4"
30"	5"	7"
36"	5 1/2"	7"
42"	6"	
48"	6 1/2"	
60"	7 1/2"	
66"	8"	



NOTE:

EYE BOLT

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



EYE BOLT AND TIE ROD

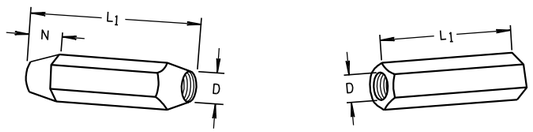
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L_1	N
12-60	5/8	5/8	5	1/2
66-84	3/4	3/4	5	1/2
90-108	1	1	7	1 1/16

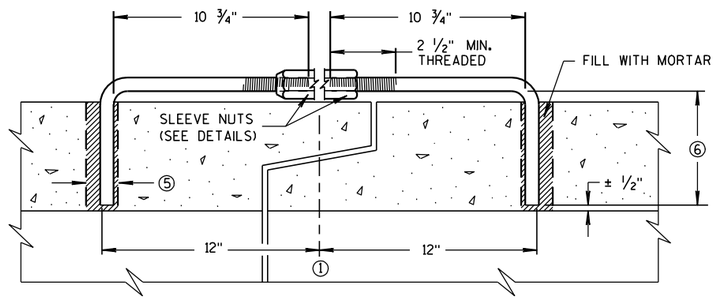
DIMENSIONS SHOWN ARE IN INCHES



TAPERED PLAIN

RIGHT AND LEFT THREADS

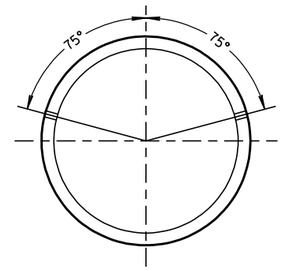
SLEEVE NUTS



LONGITUDINAL SECTION

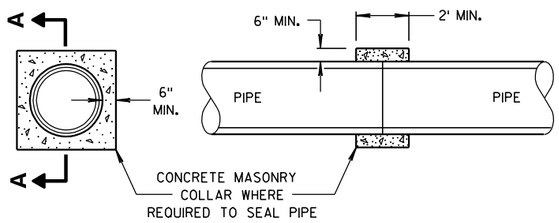
(JOINT TIES FOR 12" TO 108" DIA. CONCRETE PIPE)

ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A-A

CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

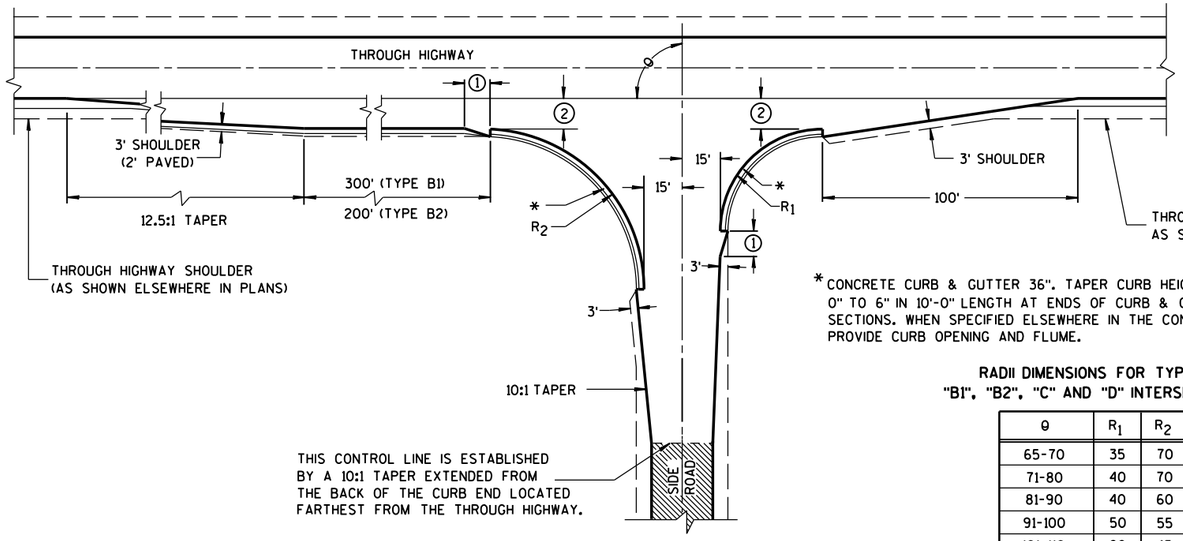
APPROVED
6/5/2012 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

6

S.D.D. 8 F 4-7

6

S.D.D. 8 F 4-7

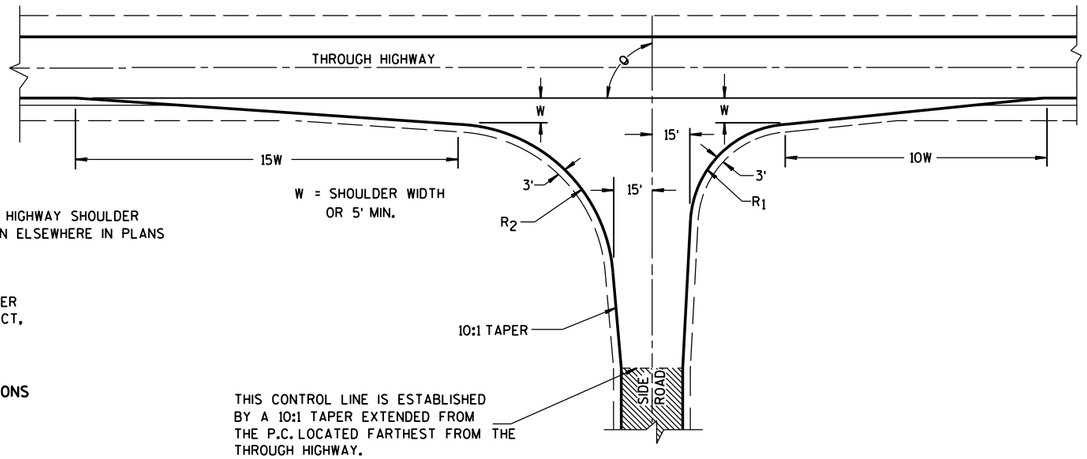


TYPE "B1" AND "B2"

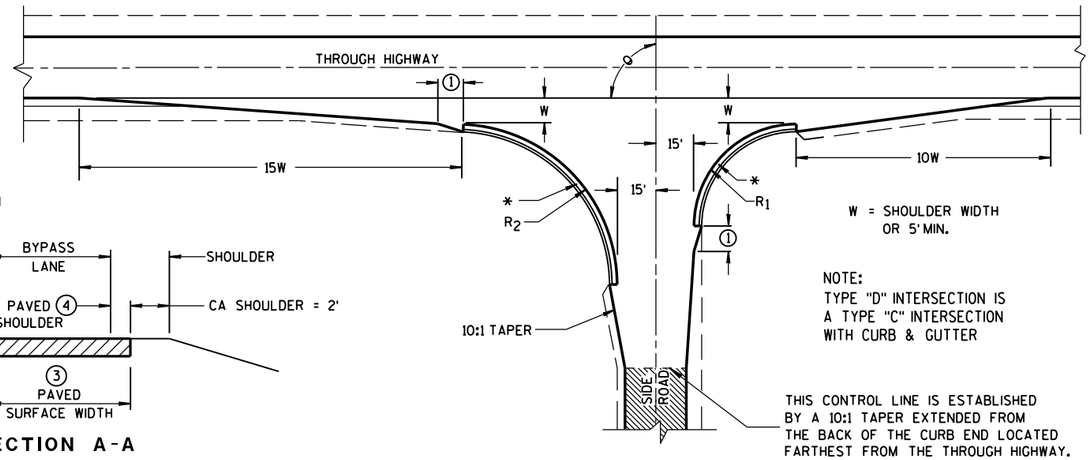
*CONCRETE CURB & GUTTER 36". TAPER CURB HEIGHT 0" TO 6" IN 10'-0" LENGTH AT ENDS OF CURB & GUTTER SECTIONS. WHEN SPECIFIED ELSEWHERE IN THE CONTRACT, PROVIDE CURB OPENING AND FLUME.

RADII DIMENSIONS FOR TYPES "B1", "B2", "C" AND "D" INTERSECTIONS

θ	R_1	R_2
65-70	35	70
71-80	40	70
81-90	40	60
91-100	50	55
101-110	60	45



TYPE "C"



TYPE "D"

GENERAL NOTES

DESIGNS MAY BE USED INTERCHANGEABLY IN COMBINATION OR SEPARATELY FOR ANY ONE COMPLETE INTERSECTION DEPENDING UPON INTERSECTION ANGLE AND SURFACING OF EACH APPROACH ROADWAY.

SIDE ROAD SURFACING NOTE

WHEN THE SIDE ROAD IS NOT PRESENTLY PAVED, PAVEMENT SHALL BE PLACED TO THE LIMITS SHOWN UNLESS OTHERWISE PROVIDED IN THE CONTRACT. WHERE THE CONSTRUCTION LIMITS ARE BEYOND THE PAVING LIMITS, CRUSHED AGGREGATE SURFACING SHALL BE PLACED BETWEEN THE PAVING LIMITS AND CONSTRUCTION LIMITS.

WHEN THE SIDE ROAD IS PRESENTLY PAVED, NEW PAVEMENT SHALL BE PLACED TO THE LIMITS OF DESIGN AS SHOWN AND BEYOND, IF NECESSARY, TO MEET EXISTING PAVEMENT.

WHEN THE SIDE ROAD IS THE CONSTRUCTION PROJECT, THE INTERSECTION SURFACING SHALL BE THE SAME AS FOR THE PROJECT.

EXISTING PAVED SURFACE

BYPASS LANE

(1) 10-FT TYPICAL.

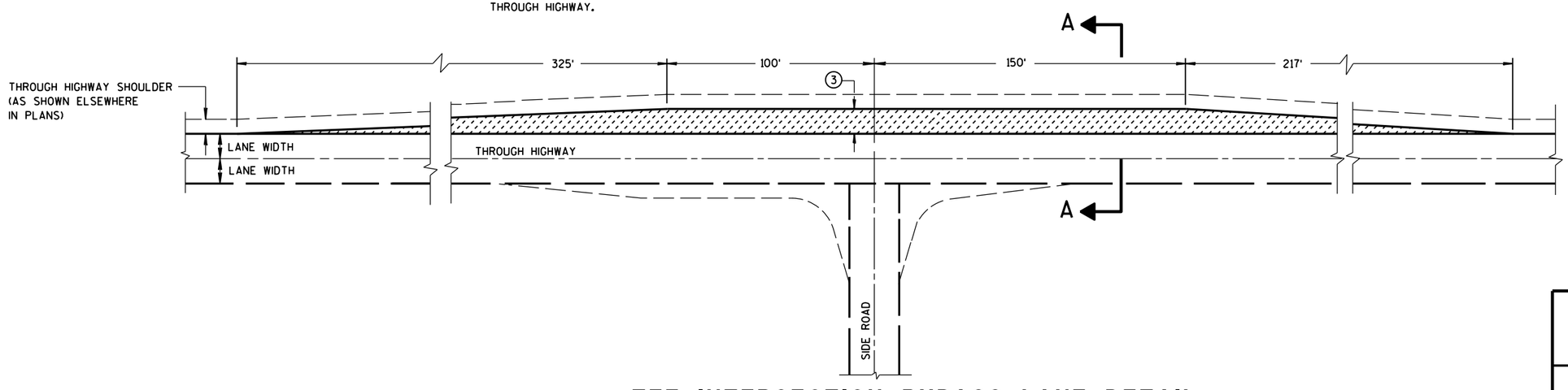
(2) 12-FT** PLUS ADDITIONAL WIDTH FOR BIKE LANE IF SHOWN ELSEWHERE IN THE PLAN.

**10-FT MAY BE USED ON TYPE B2 ON RESURFACING PROJECTS IF SPECIFIED IN THE CONTRACT.

(3) BYPASS LANE PAVED SURFACE WIDTH OUTSIDE OF TRAVEL LANE
-ASPHALT = 12-FT PLUS PAVED SHOULDER WIDTH.
-PC CPNCRETE = 13-FT PLUS PAVED SHOULDER WIDTH.

(4) BYPASS LANE PAVED SHOULDER WIDTH = THE GREATER OF 1-FT OR THE PAVED SHOULDER WIDTH OF THE THROUGH HIGHWAY.

SECTION A-A
(SHOWING BYPASS LANE AND SHOULDER)



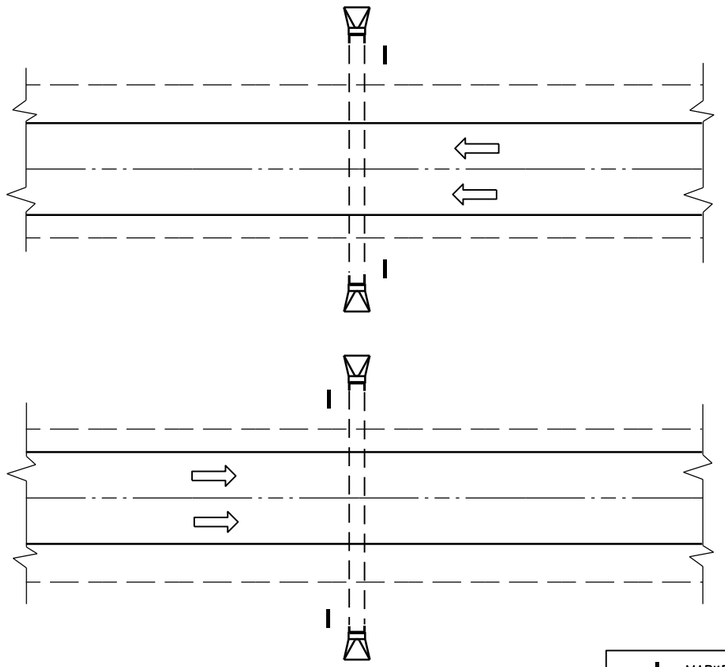
TEE INTERSECTION BYPASS LANE DETAIL

AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND "D" AND TEE INTERSECTION BYPASS LANE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

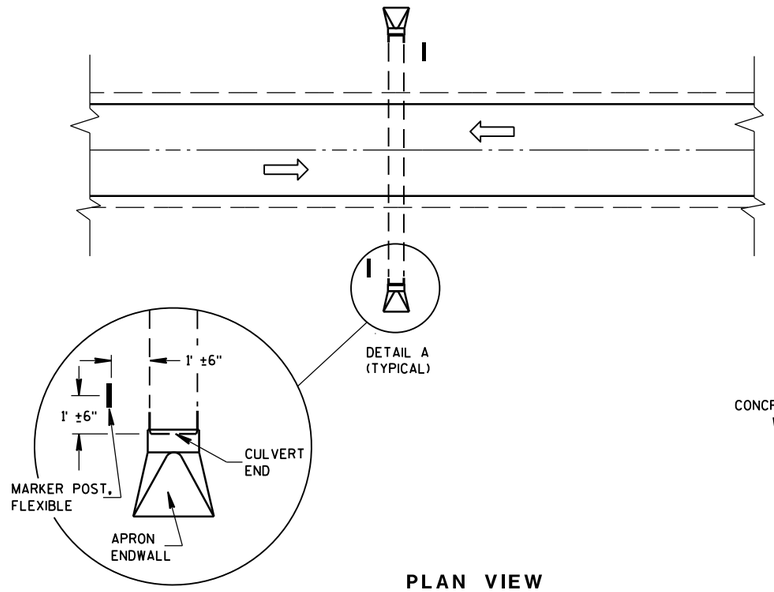
6

S.D.D. 9 A 1-13a



PLAN VIEW

DIVIDED HIGHWAY



DETAIL A
(TYPICAL)

PLAN VIEW

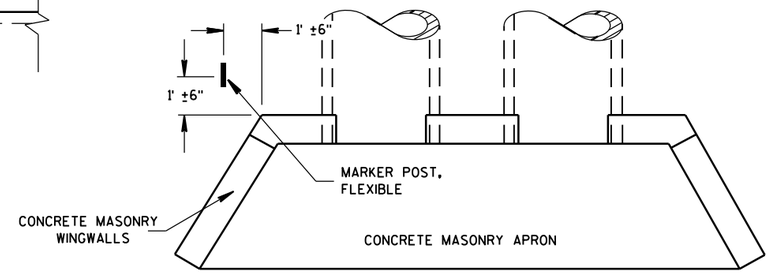
UNDIVIDED HIGHWAY

MARKER POST, FLEXIBLE

DIRECTION OF TRAFFIC FLOW

GENERAL NOTES

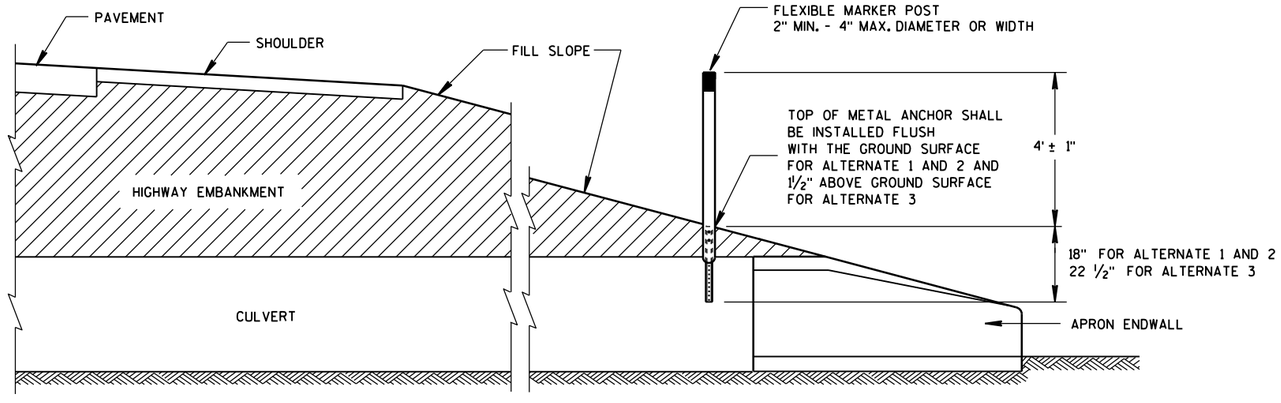
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW

CONCRETE MASONRY ENDWALLS FOR CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION

FLEXIBLE MARKER POST

FLEXIBLE MARKER POST FOR CULVERT END

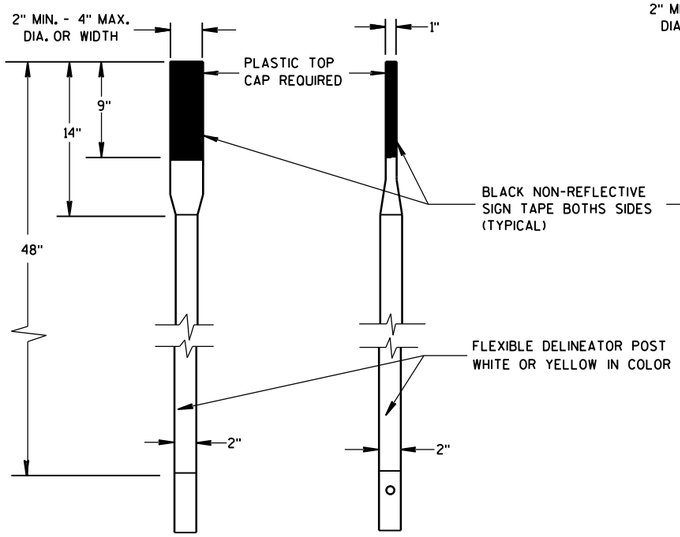
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

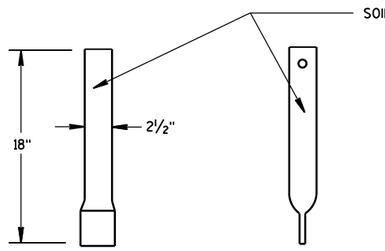
6

S.D.D. 15 A 3-2a

6

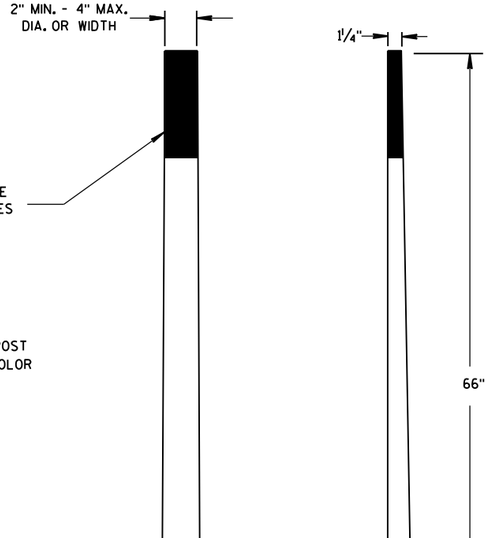
S.D.D. 15 A 3-2a





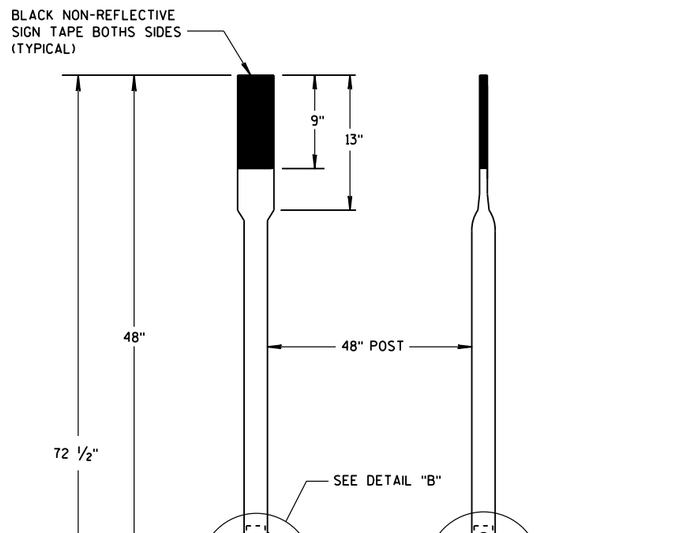
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

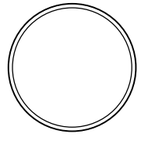
ALTERNATE 2



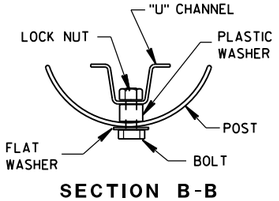
FRONT VIEW SIDE VIEW

ALTERNATE 3

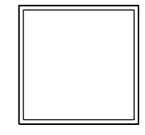
FLEXIBLE MARKER POSTS



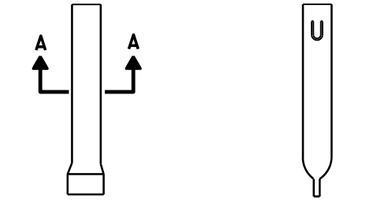
SECTION A-A



SECTION B-B

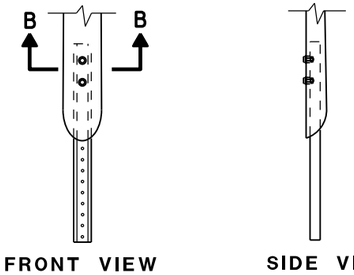


SECTION C-C



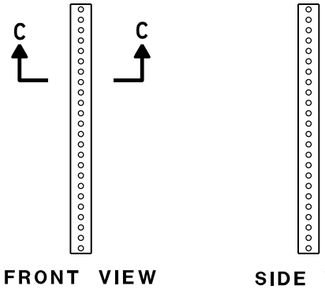
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

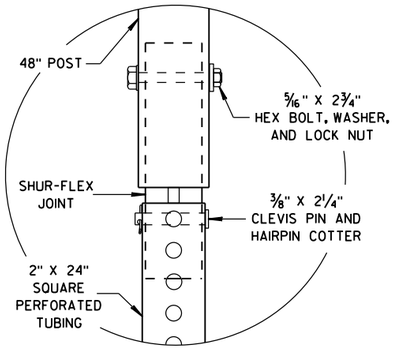
ALTERNATE 2



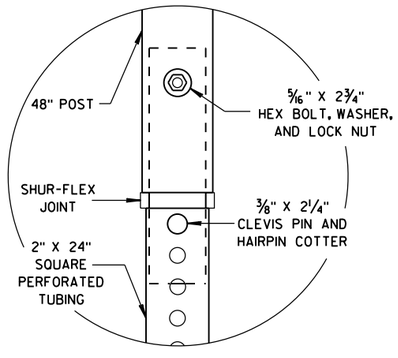
FRONT VIEW SIDE VIEW

ALTERNATE 3

FLEXIBLE MARKER POST ANCHORS



DETAIL B



DETAIL C

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

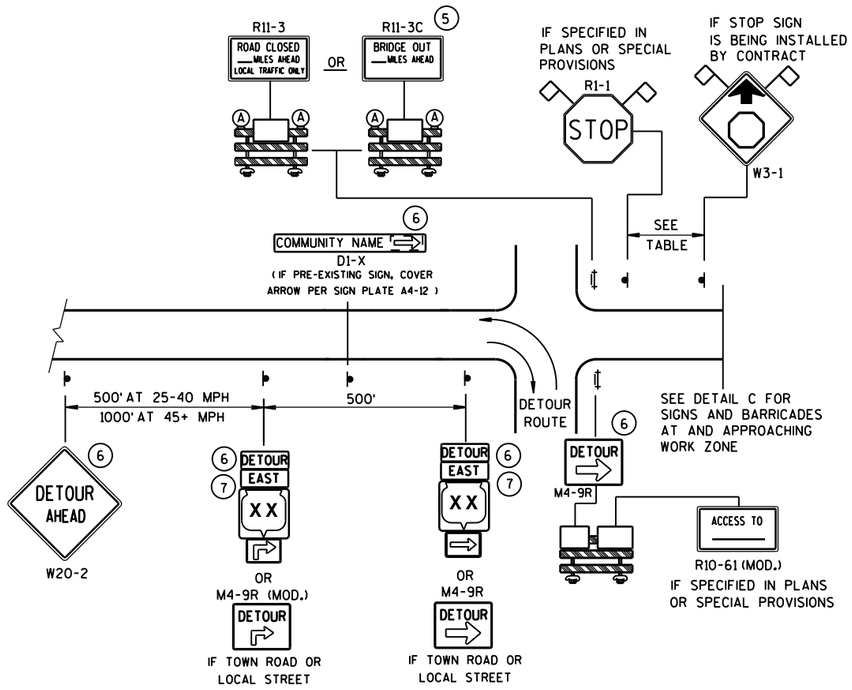
APPROVED
10/1/2012 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

6

S.D.D. 15 A 3-2b

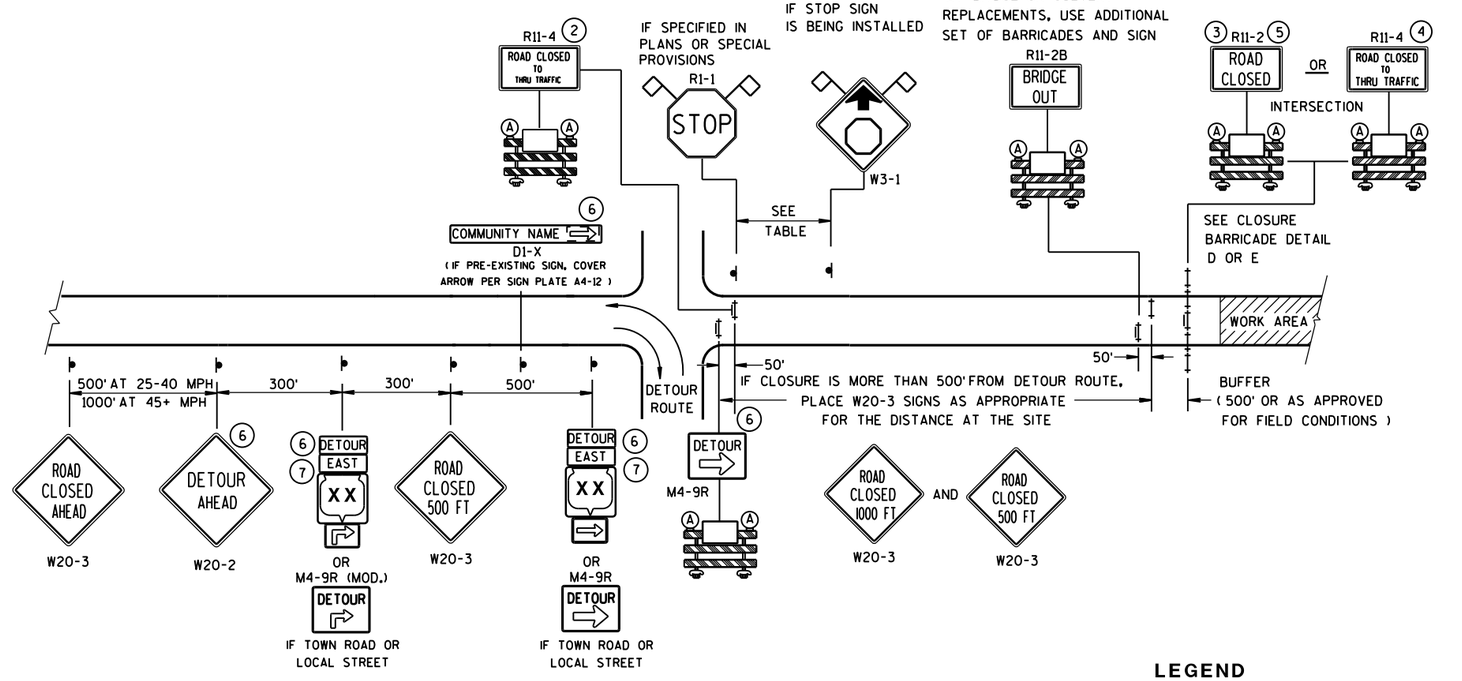
6

S.D.D. 15 A 3-2b



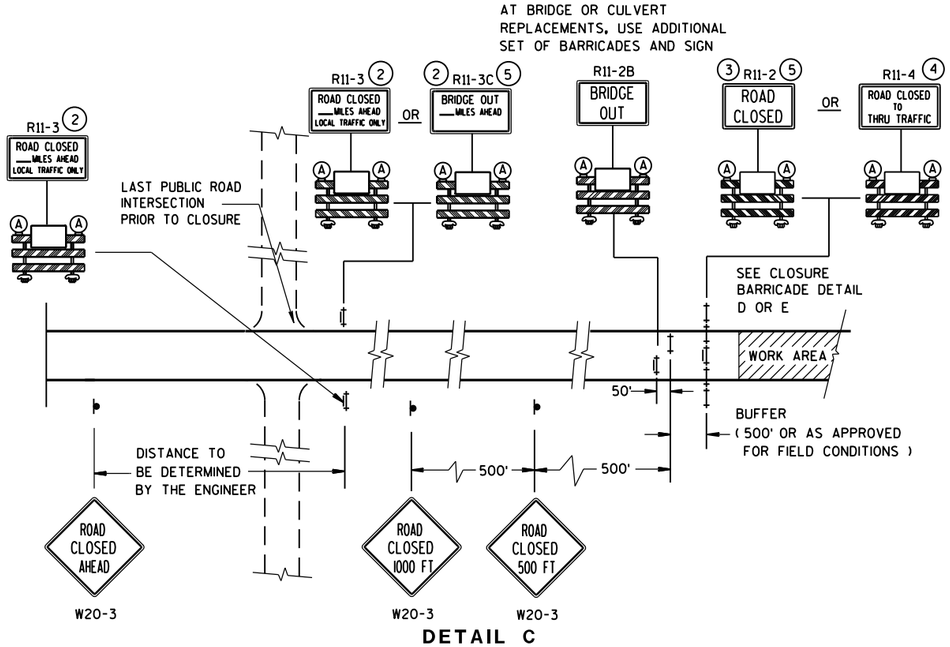
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "**b**" FOR GENERAL NOTES AND FOOTNOTES (1) THROUGH (7)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- DETOUR / EAST — M4-8 / M3-X
- XX — M1-4; COUNTY XX — M1-5A; XX — M1-6
- M05-1 OR M06-1
- FLAGS, 16" X 16" MIN., (ORANGE)

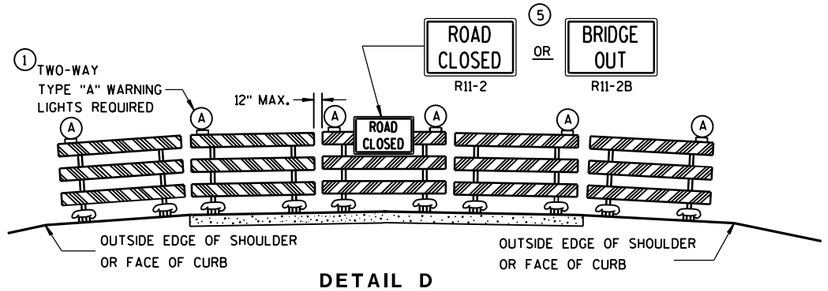
BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

8/2013 DATE — /S/ Travis Feltes STATE TRAFFIC ENGINEER — DESIGN

FHWA

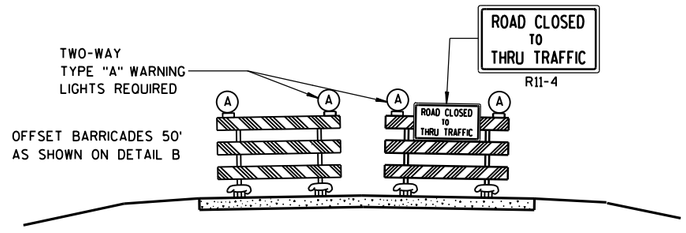
6
S.D.D. 15 C 2-5a



DETAIL D

ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW



DETAIL E

LANE CLOSURE BARRICADE DETAIL

APPROACH VIEW

SEE SDD 15C2-SHEET "**a**" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

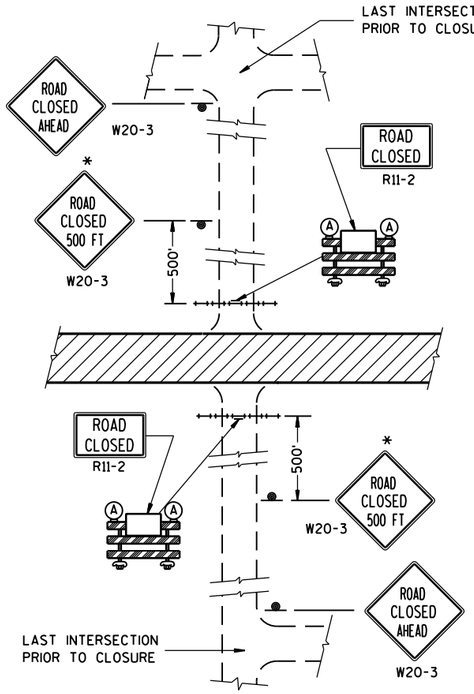
BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

8/2013 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

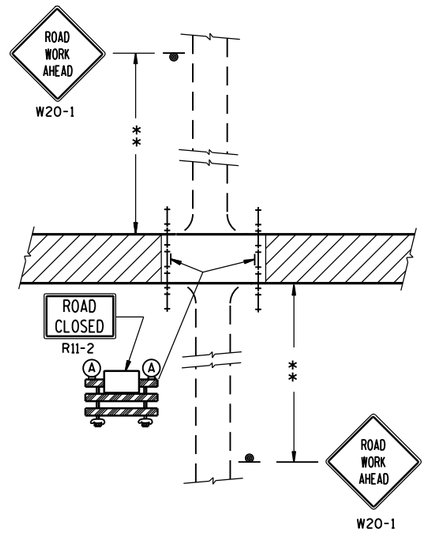
6

S.D.D. 15 C 2-5b



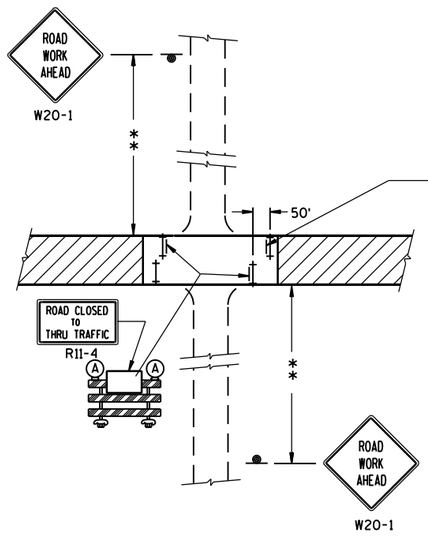
DETAIL 1

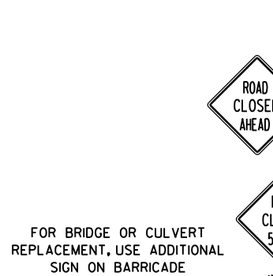
(NO ACCESS TO PROJECT)



DETAIL 2

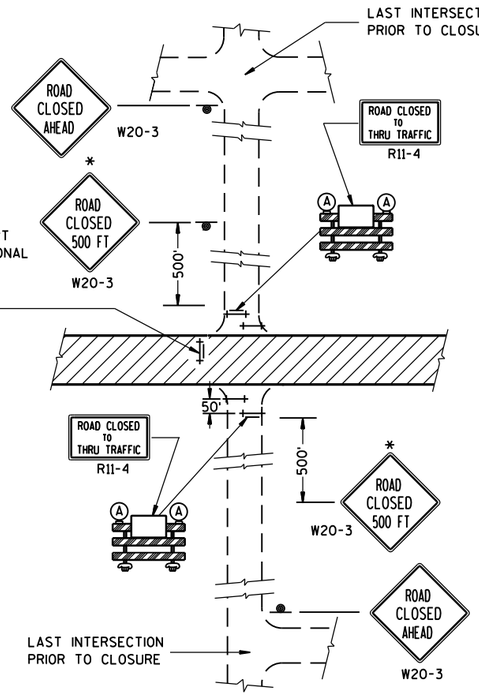
(PUBLIC CROSS-TRAFFIC MAINTAINED. NO ACCESS TO PROJECT).





DETAIL 3

(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).



DETAIL 4

(CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

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S.D.D. 15 C 3-2

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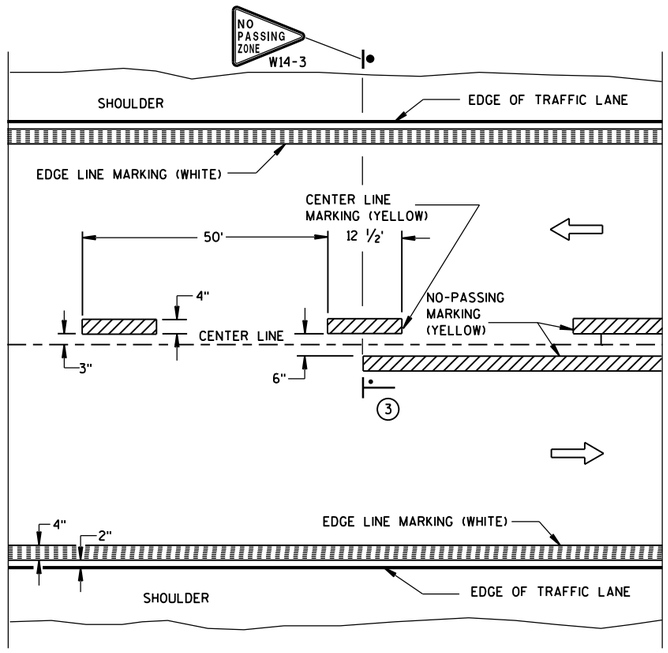
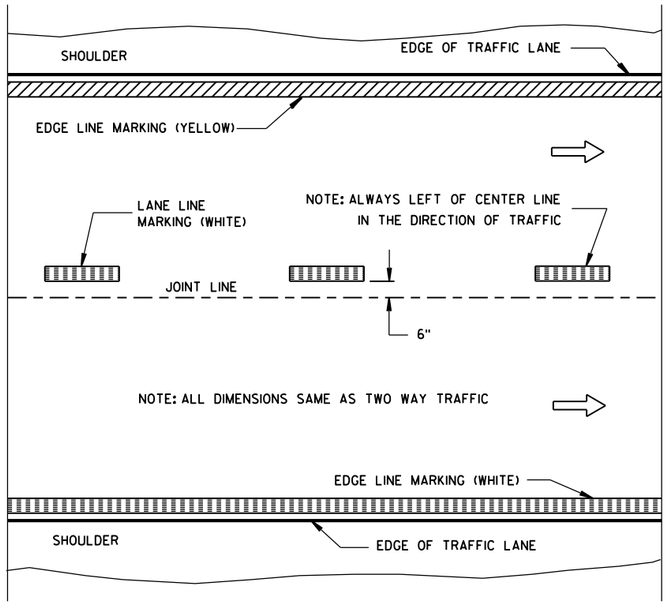
S.D.D. 15 C 3-2

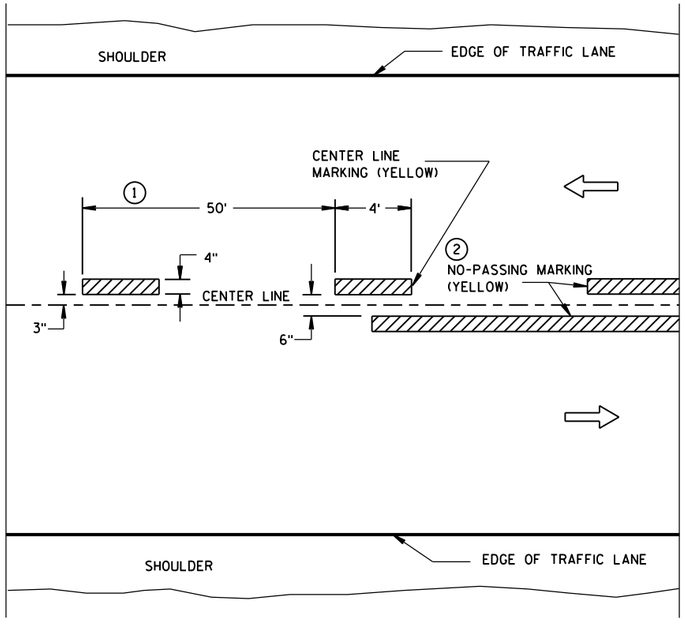


TWO WAY TRAFFIC

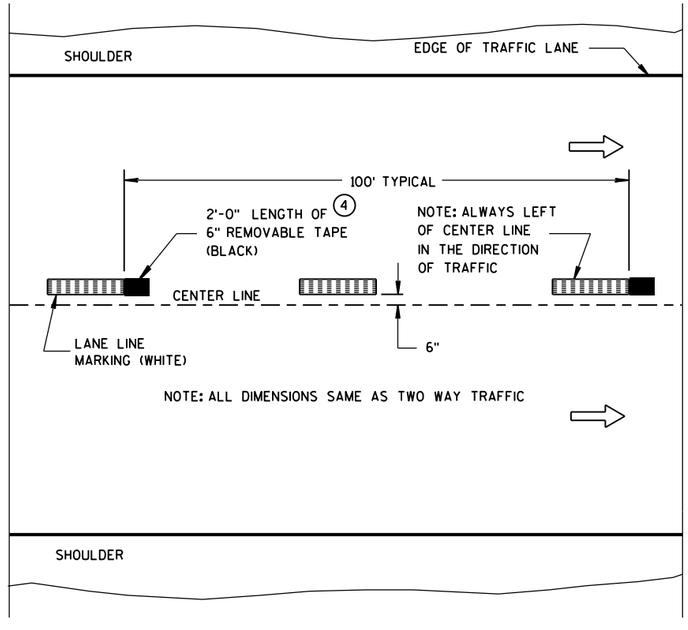


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING

(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

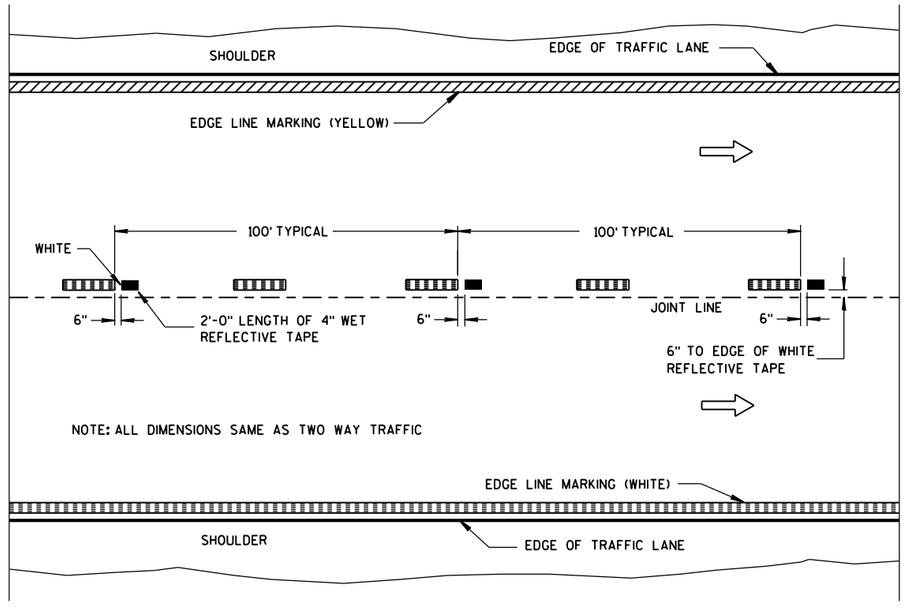
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

1. HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
2. NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
3. NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
4. CONCRETE ONLY.

NOTE

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL



WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE

LEGEND

"T" MARKING

POST MOUNTED SIGN

PAVEMENT MARKING (MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5-13-2013 DATE
/S/ Travis Feltes STATE TRAFFIC E
FHWA

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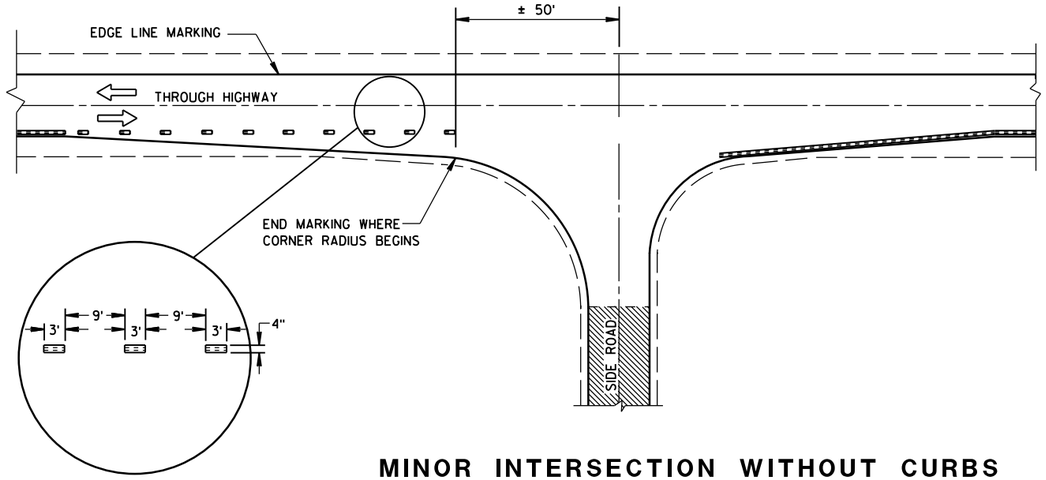
S.D.D. 15 C 8-16a

6

S.D.D. 15 C 8-16a



MINOR INTERSECTION WITHOUT CURBS

GENERAL NOTES

EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.

(1) WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.

(2) WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.

(3) ALTERNATIVE MARKING SHALL BE PROVIDED WHEN SPECIFIED IN THE CONTRACT. TYPICAL SITUATIONS WHERE THIS MARKING MAY BE REQUIRED ARE WHERE THE INTERSECTION IS ON A SHARP HORIZONTAL CURVE OR CREST VERTICAL CURVE IN AN UNLIGHTED AREA SUCH THAT THE EDGE LINE MAY BE MISLEADING TO THE MOTORIST OR DISAPPEAR FROM SIGHT.

(4) THE EDGE LINE IN THE TAPER AREAS OF THE BYPASS LANE AND THE BYPASS LANE SHALL BE LOCATED 1-FOOT FROM EDGE OF PAVEMENT TO THE OUTSIDE EDGE OF EDGE LINE.

(5) BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.

(6) BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

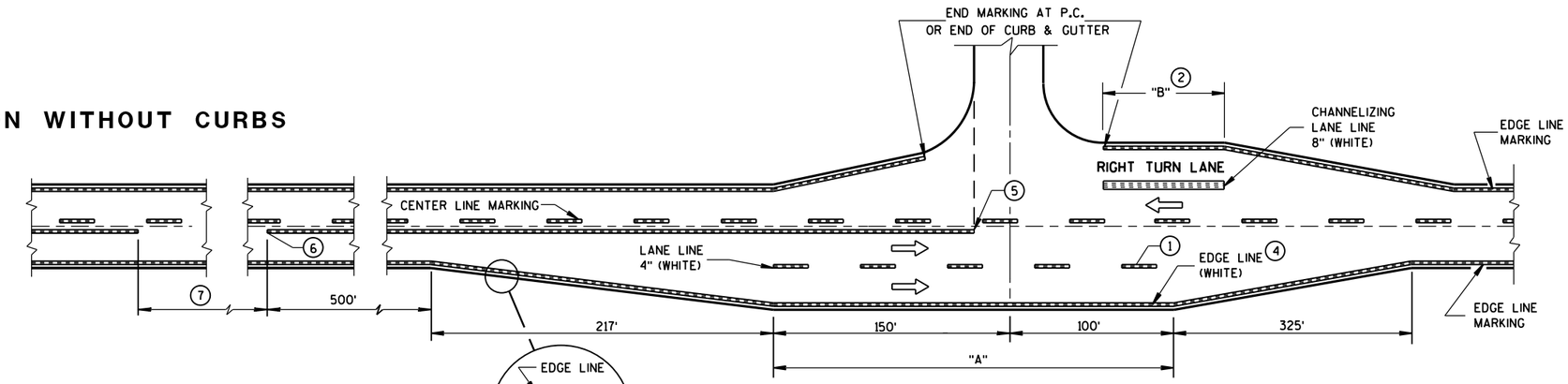
(7) IF THE DISTANCE BETWEEN 2 SUCCESSIVE NO-PASSING ZONES IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES, CONNECT THE 2 ZONES.

(8) 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL

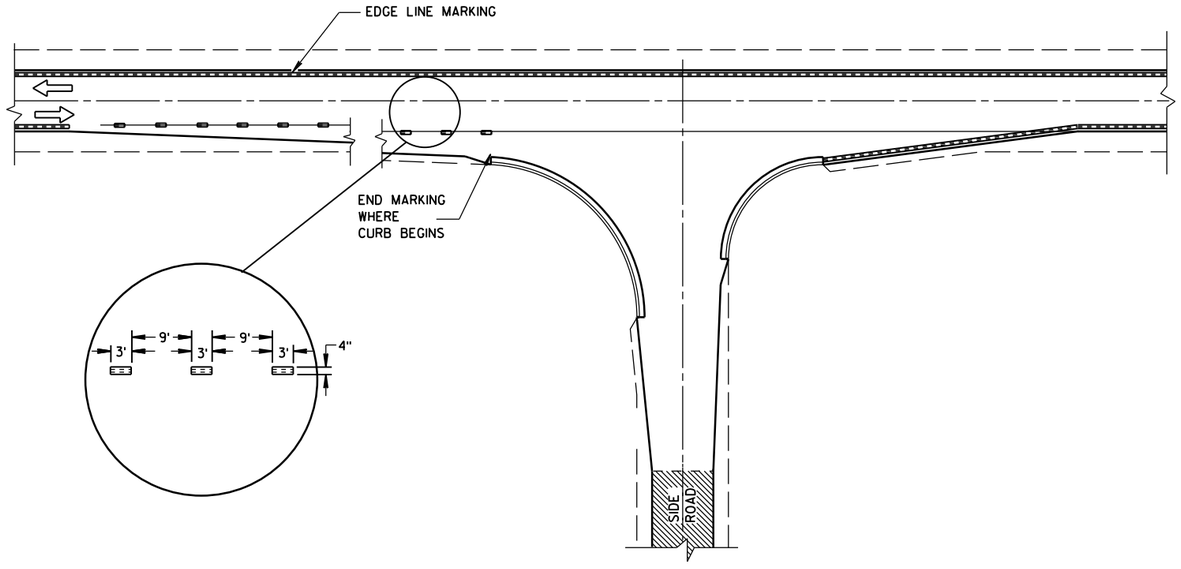
(7)

POSTED SPEED (MPH)	MINIMUM DISTANCE BETWEEN ZONES (FEET)
25 - 30	528
35 - 40	528
45 - 50	686
55	792



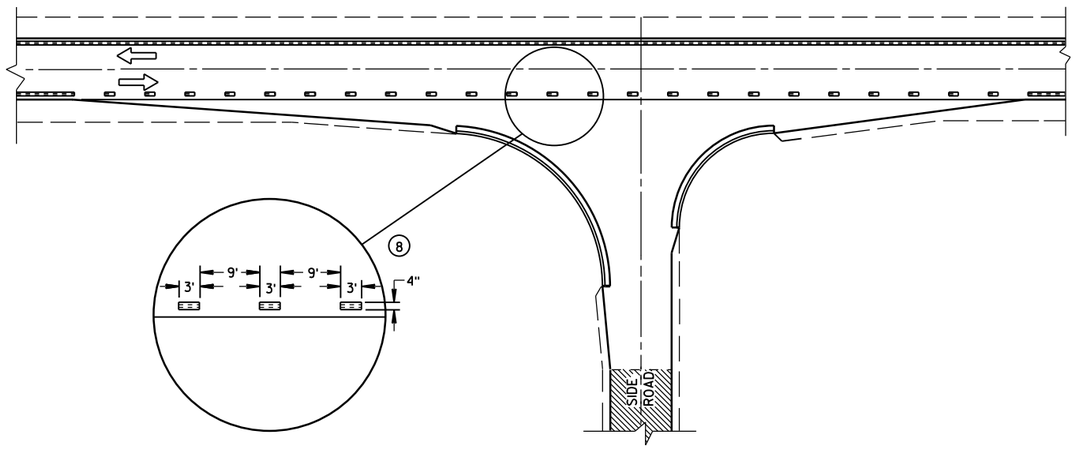
MAJOR INTERSECTIONS

(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)



MINOR INTERSECTION WITH CURBS

(TYPICAL MARKING)



MINOR INTERSECTION WITH CURBS

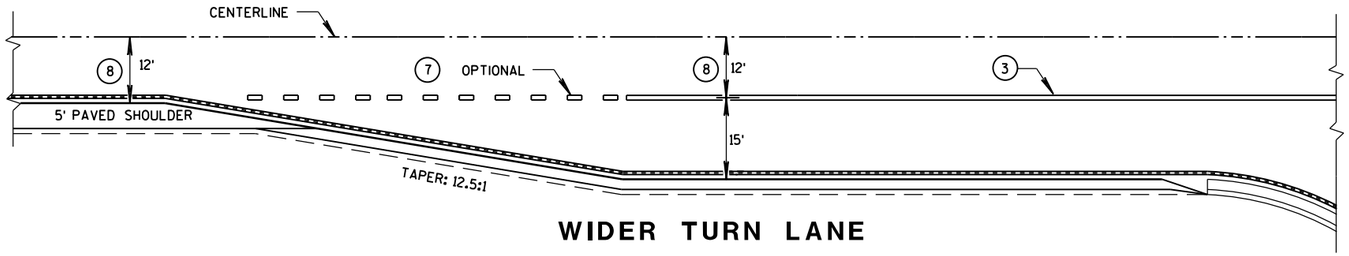
(3) (FOR SPECIAL CONDITIONS AS SPECIFIED)

PAVEMENT MARKING
(INTERSECTIONS)

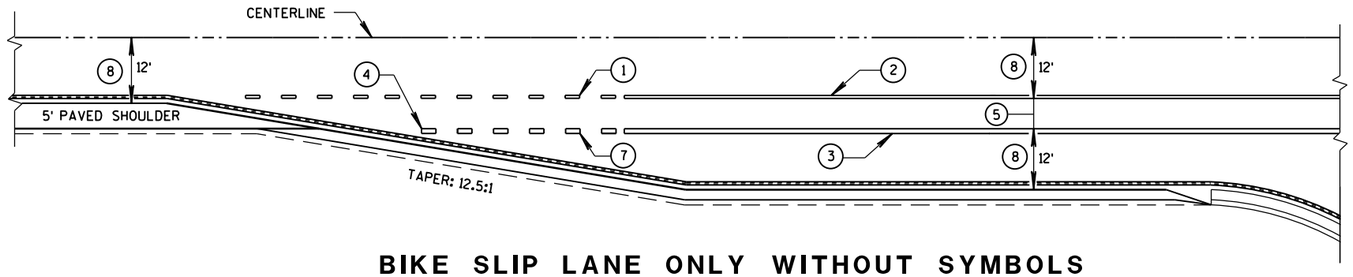
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

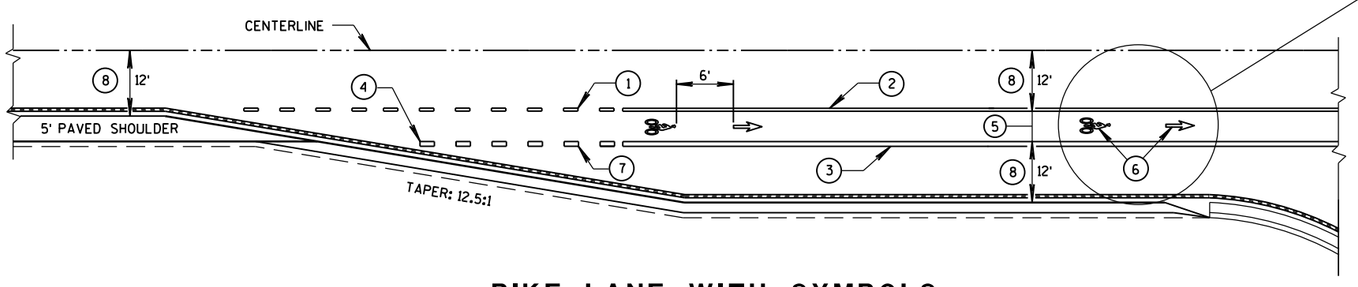
S.D.D. 15 C 8-16b



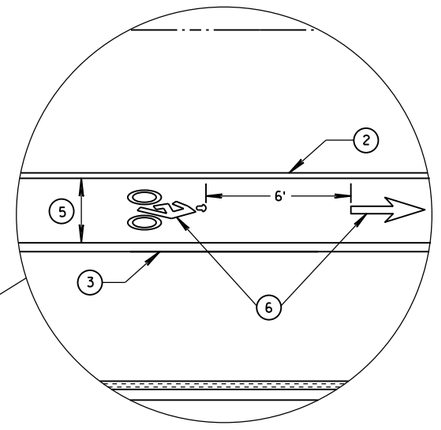
WIDER TURN LANE



BIKE SLIP LANE ONLY WITHOUT SYMBOLS



BIKE LANE WITH SYMBOLS



GENERAL NOTES

1. 3' LINE, 9' GAP - 4-INCH WIDE, WHITE.
2. 4-INCH, WHITE.
3. 8-INCH, WHITE.
4. IF SIGNED AND/OR MARKED AS A BICYCLE FACILITY INCLUDE SECOND LINE OF LINE-SPACE MARKING, OTHERWISE DO NOT.
5. BIKE ACCOMMODATION FOR CONCRETE PAVEMENT IS 5' WIDE. BIKE ACCOMMODATION FOR ASPHALT PAVEMENT IS A MINIMUM OF 4', 5' AT ≥ 45 MPH.
6. REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.
7. 3' LINE, 9' GAP - 8-INCH WIDE, WHITE.
8. REFER TO CONTRACT PLANS.

6

S.D.D. 15 C 29-3a

BICYCLE LANE MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

4/30/2013
DATE

/S/ Travis Feltes
STATE TRAFFIC E

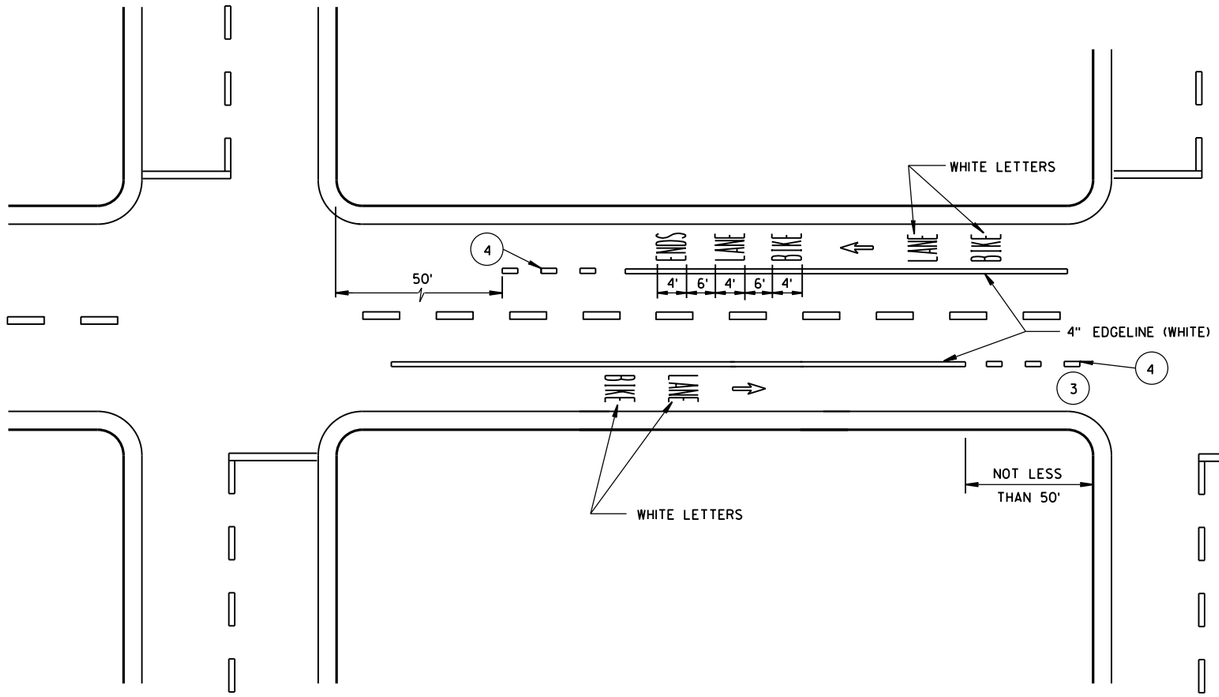
FHWA

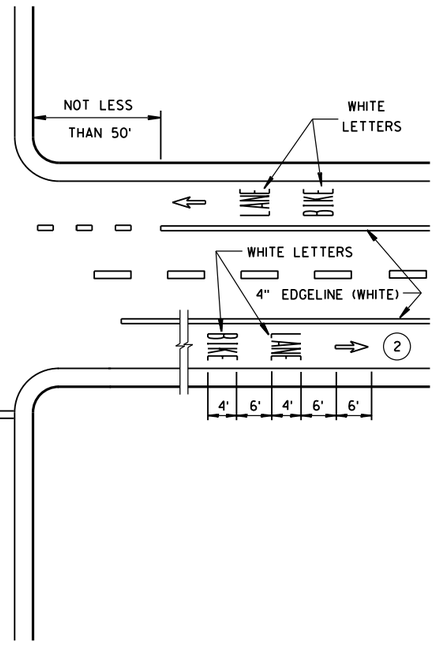
6

S.D.D. 15 C 29-3a

GENERAL NOTES

1. DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
2. THE SERIES OF PAVEMENT MARKING SYMBOLS SHALL BE REPEATED AFTER INTERSECTIONS AND SPACED A MAXIMUM OF 250'. NO PAVEMENT MARKING WILL TAKE PLACE IN THE CROSSWALK.
3. DOTTED LINES SHOULD BE USED 50' TO 200' IN ADVANCE OF AN INTERSECTION WHERE THERE IS NO RIGHT TURN ONLY LANE AND THERE IS HEAVY RIGHT TURN TRAFFIC OR THERE IS A NEAR-SIDE BUS STOP. AT OTHER INTERSECTIONS WHERE RIGHT TURN TRAFFIC IS LIGHT TO MODERATE, A SOLID LINE CAN BE USED UP TO THE INTERSECTION.
4. 3' LINE, 9' GAP - 4" WIDE, WHITE.





DESIGNATED BICYCLE LANE
NO PARKING

URBAN BICYCLE LANE MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	
4/30/2013 DATE	/S/ Travis Feltes STATE TRAFFIC EN
FHWA	

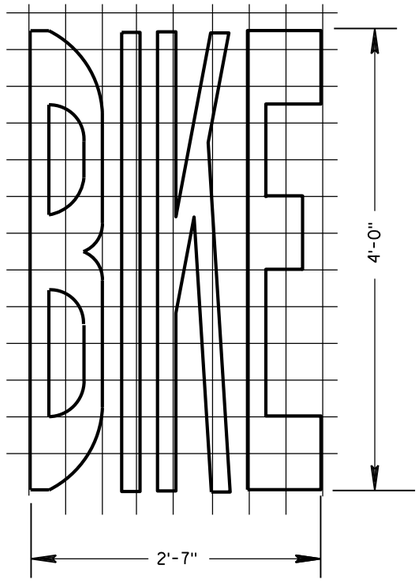
6

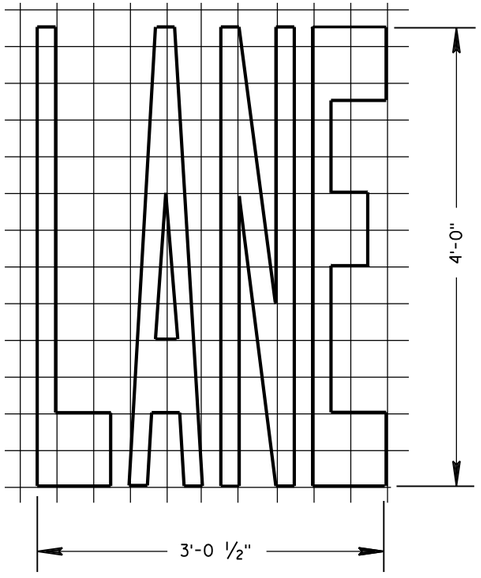
S.D.D. 15 C 29-3d

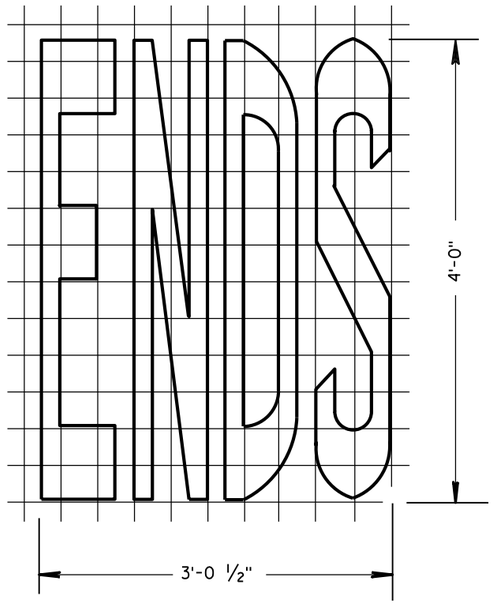
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

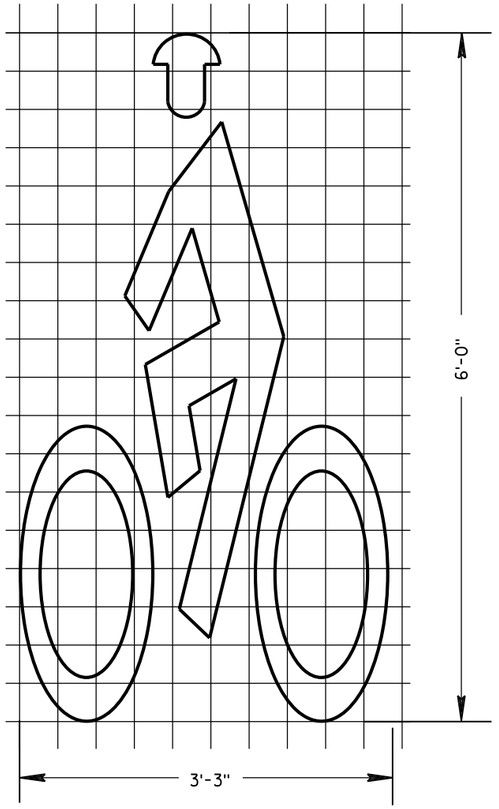
ALL LETTERS, ARROWS AND SYMBOLS SHALL BE IN CONFORMANCE WITH REQUIREMENTS INCLUDED IN "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKING" BOOK BY THE FEDERAL HIGHWAY ADMINISTRATION. ALL LETTERS, ARROWS AND SYMBOLS SHALL BE WHITE AND REFLECTORIZED. SMALL DIFFERENCES IN DIMENSIONS WITHIN THE TOLERANCES OF THAT BOOK ARE ACCEPTABLE.



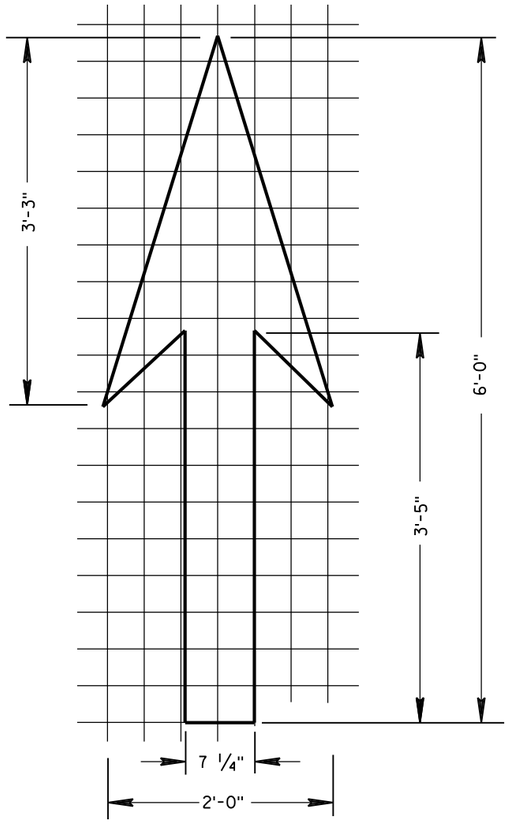




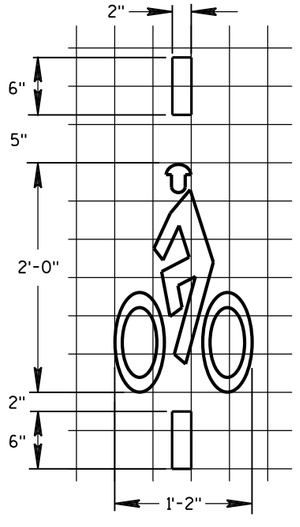
BIKE LANE WORDS



BIKE LANE SYMBOL



BIKE LANE ARROW



BICYCLE DETECTOR PAVEMENT MARKING

PAVEMENT MARKING FOR BIKE LANES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-30-2013
DATE

/S/ Travis Feltes
STATE TRAFFIC EN

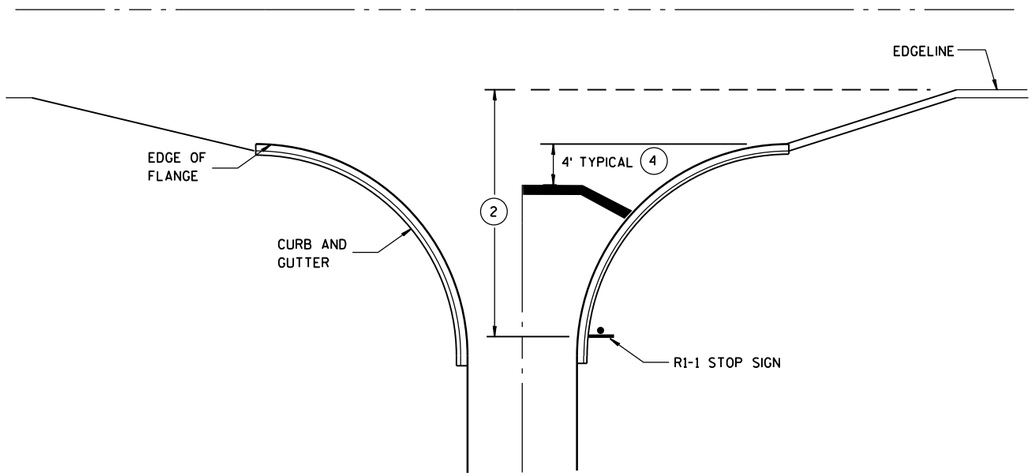
FHWA

6

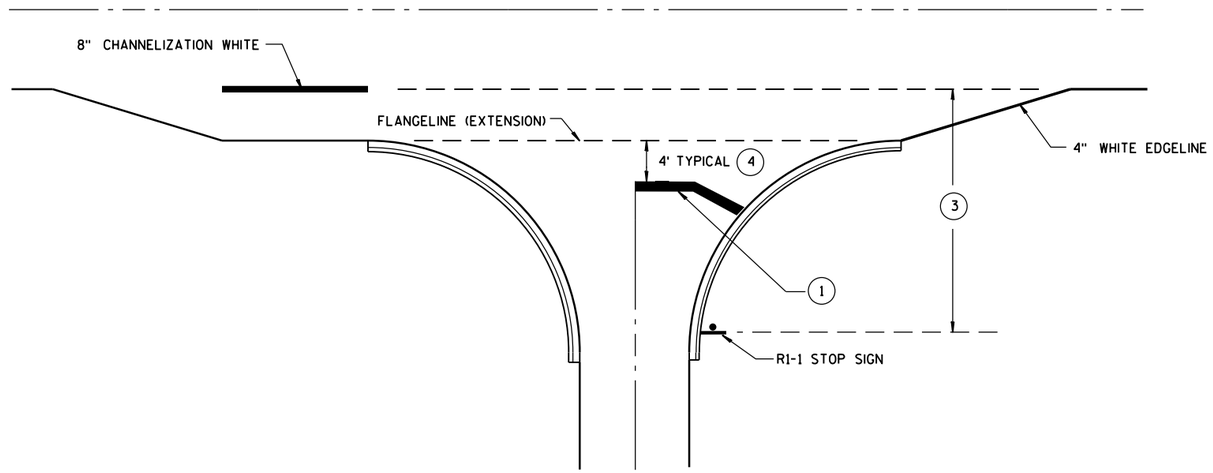
S.D.D. 15 C 29-3e

6

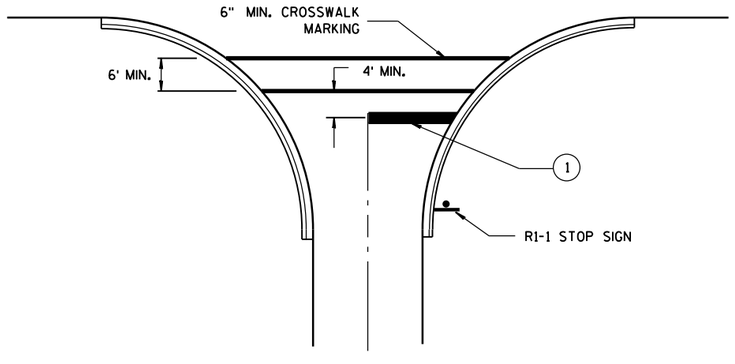
S.D.D. 15 C 29-3e



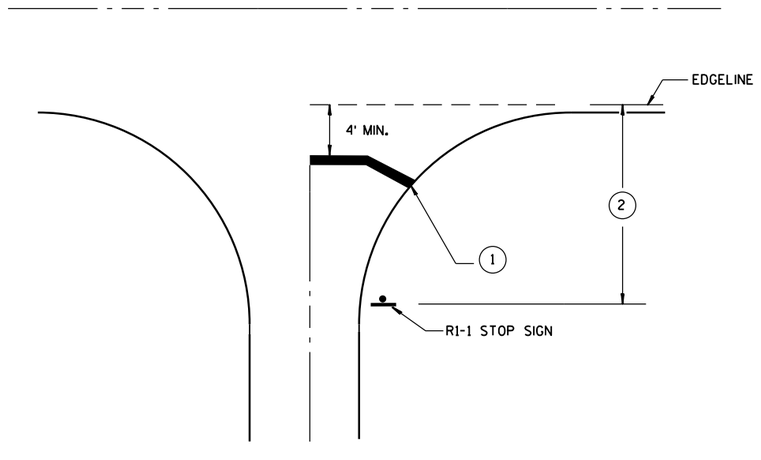
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
3. IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
4. MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES.

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

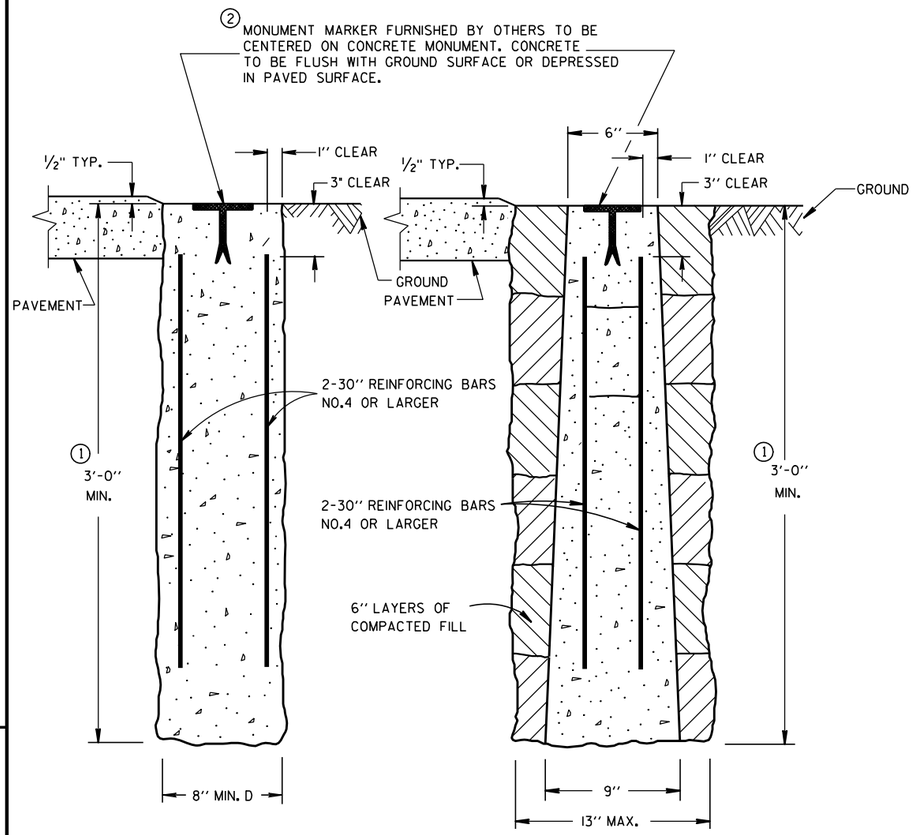
4/30/2013
DATE

/S/ Travis Feltes
STATE TRAFFIC E

FHWA

6

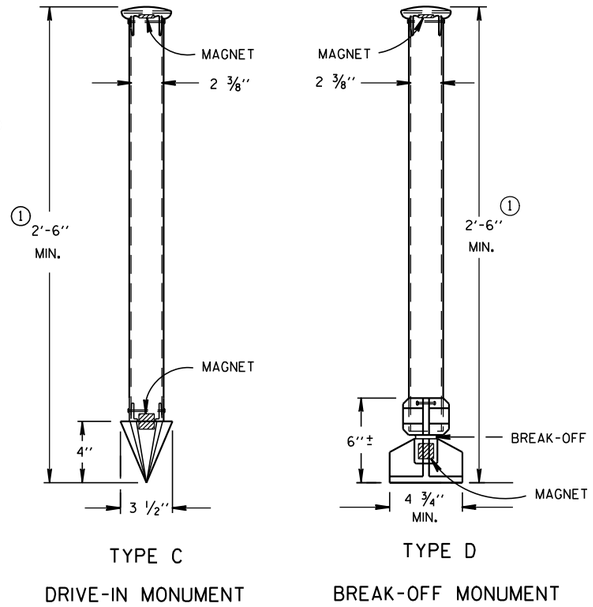
S.D.D. 15 C 33-1
S.D.D. 15 C 33-1



CAST-IN-PLACE PRECAST

CONCRETE MONUMENTS

TYPE A



TYPE C DRIVE-IN MONUMENT

TYPE D BREAK-OFF MONUMENT

ALUMINUM MONUMENTS

(INCLUDES MARKER)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAILED DRAWINGS OF PROPOSED ALTERNATE DESIGNS FOR METAL MONUMENTS OR MONUMENT COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

INSTALLED METAL MONUMENTS MUST BE EASILY DETECTED WITH A DIP NEEDLE. INSERT PERMANENT MAGNETS SHALL BE ATTACHED NEAR THE TOP AND BOTTOM OF THOSE MONUMENTS CONSTRUCTED OF A METAL ALLOY WHICH IS NOT ATTRACTIVE TO A DIP NEEDLE.

THE CAST IRON MONUMENT COVER SHALL BE A "NON-ROCKING" TYPE. ADJUSTMENT OF THE COVER TO GRADE MAY BE ACCOMPLISHED BY THE USE OF MORTAR AND BRICK, OR BY EITHER PRECAST OR CAST-IN-PLACE REINFORCED CONCRETE GRADE RINGS.

MONUMENTS SHALL BE LOCATED AND PLACED AT THE DIRECTION OF THE ENGINEER.

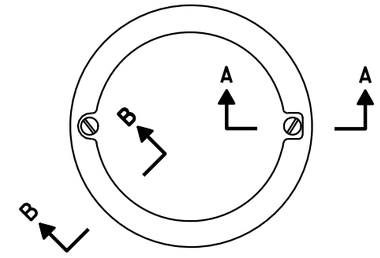
ALUMINUM MONUMENTS AND MONUMENT COVERS SHALL BE MADE FROM AN ALUMINUM AND MAGNESIUM ALLOY AS DETERMINED BY THE MANUFACTURER.

THE MONUMENT COVERS DETAILED ON THIS DRAWING ARE NOT EQUAL ALTERNATES. MONUMENT COVERS SHALL BE CAST IRON UNLESS ALUMINUM IS SPECIFIED ELSEWHERE IN THE CONTRACT.

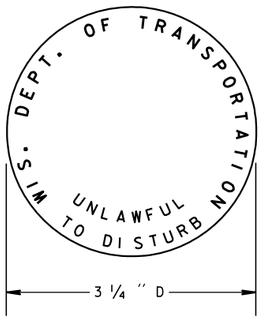
MONUMENT SHALL BE CAST-IN-PLACE CONCRETE UNLESS PRECAST CONCRETE OR ALUMINUM MONUMENTS ARE SPECIFIED IN THE CONTRACT OR PERMITTED BY THE ENGINEER.

① MINIMUM LENGTH SHALL BE 4'-0" FOR MONUMENTS INSTALLED IN PAVED AREAS.

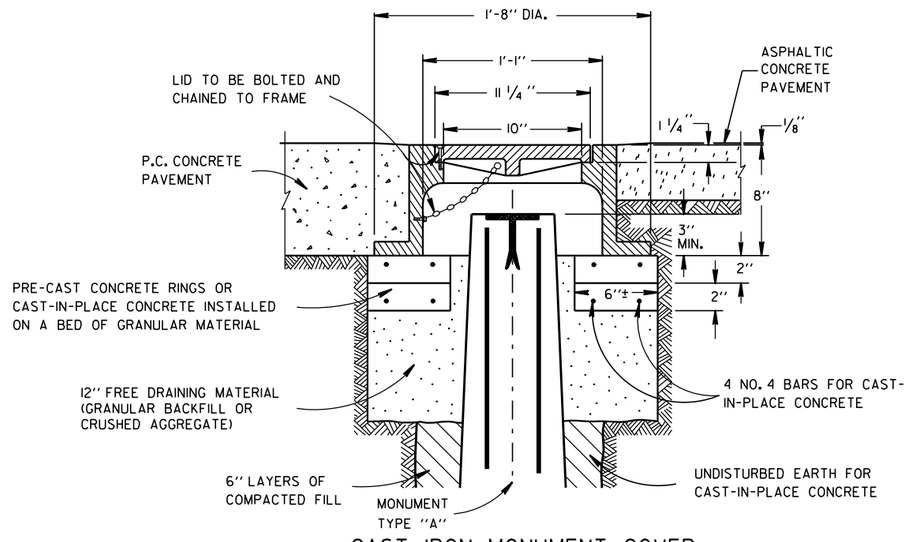
② AN OFFICIAL COUNTY MONUMENT MARKER SUPPLIED BY A COUNTY MAY BE REQUIRED FOR SOME SECTION CORNERS AND WITNESS MONUMENTS INSTEAD OF THIS WIS DOT MARKER.



TOP VIEW

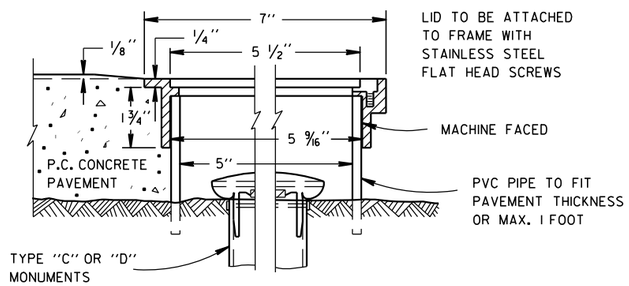


② WIS DOT MONUMENT MARKER LOGO FOR TYPES "A", "C" & "D"



CAST IRON MONUMENT COVER

(APPROXIMATE WEIGHT - 95 LBS.)



SECTION B-B SECTION A-A

ALUMINUM MONUMENT COVER

(APPROXIMATE WEIGHT 2 LBS)

(FOR CONCRETE PAVEMENT ONLY)

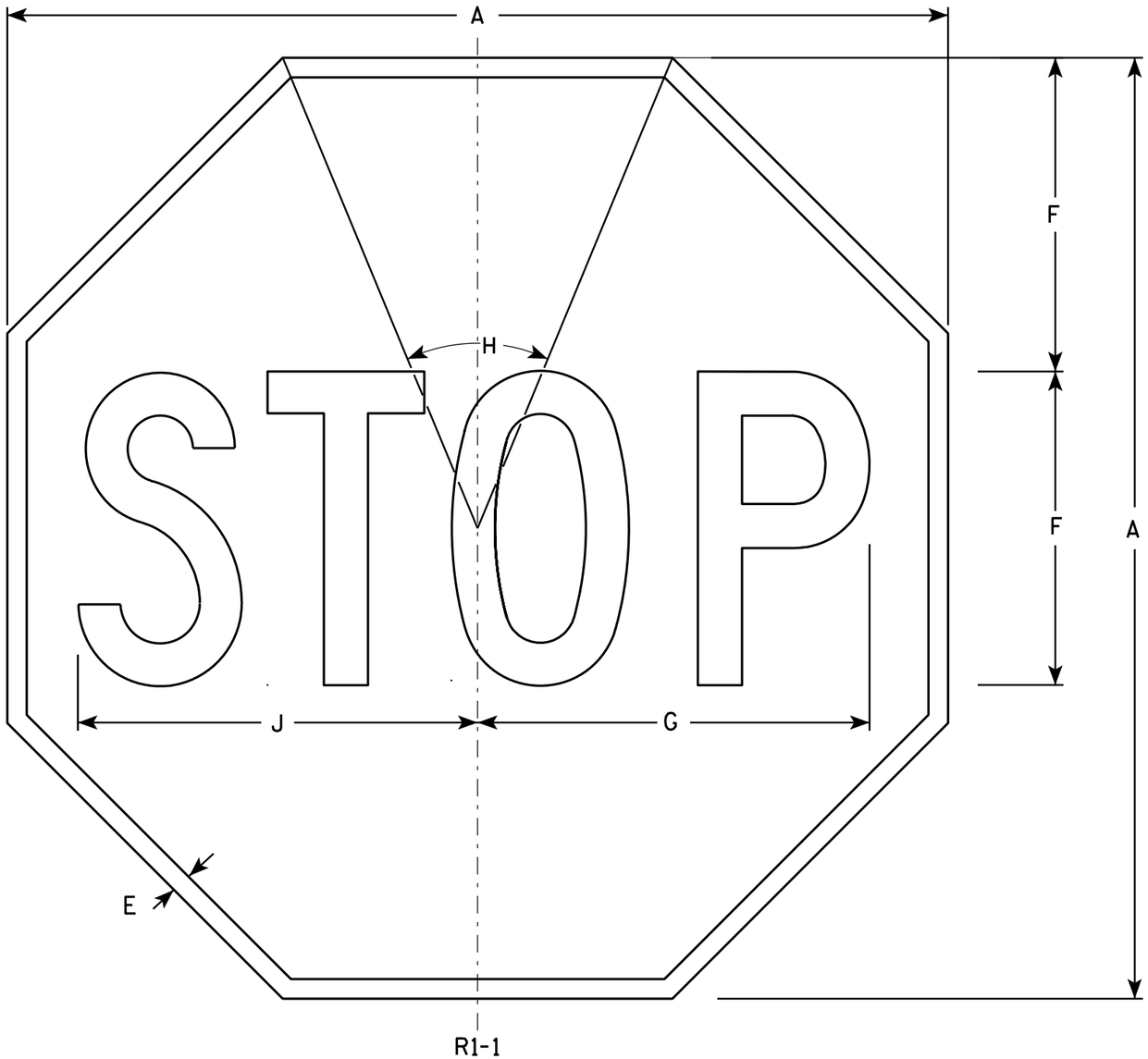
LANDMARK REFERENCE MOUNMENTS AND COVERS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
9/22/1999 DATE
/S/ Rory L. Rhinesmith
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

S.D.D. 16 A 1-6



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24				3/8	8	10	45°		10 1/4																	3.31
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

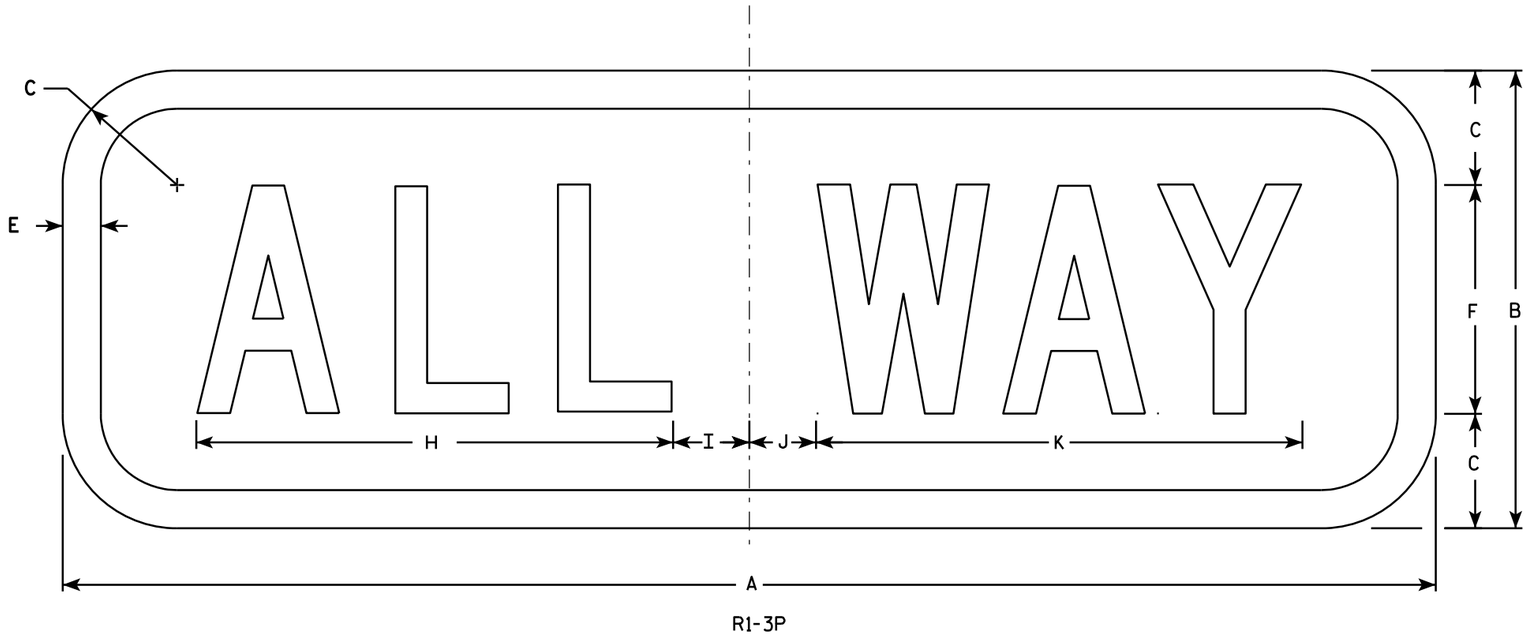
STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1.12

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 72	E



R1-3P

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																0.75
2S	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																1.5
2M	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
3	30	12	2 1/4		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5
4																											
5																											

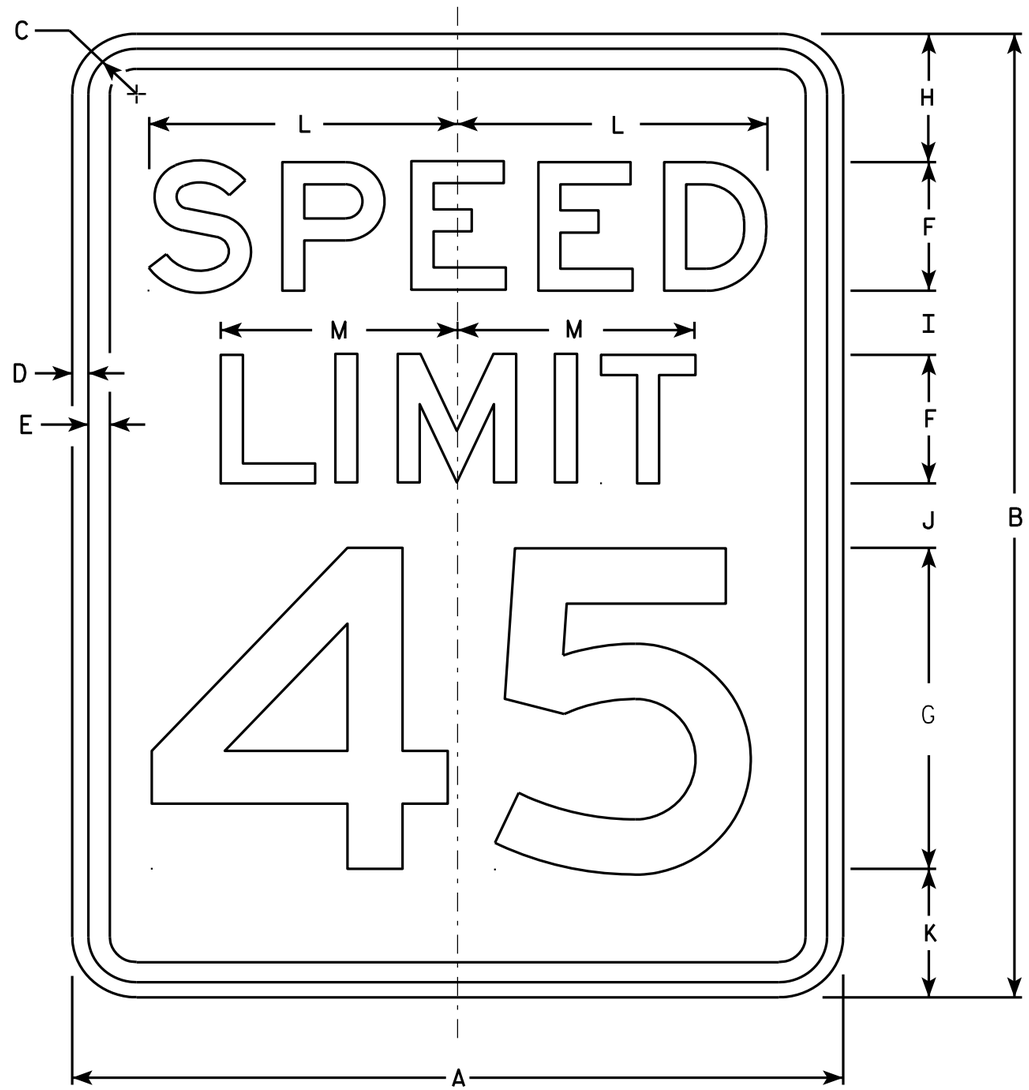
STANDARD SIGN
R1-3P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/02/10 PLATE NO. R1-3P.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 73	E



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN
R2-1

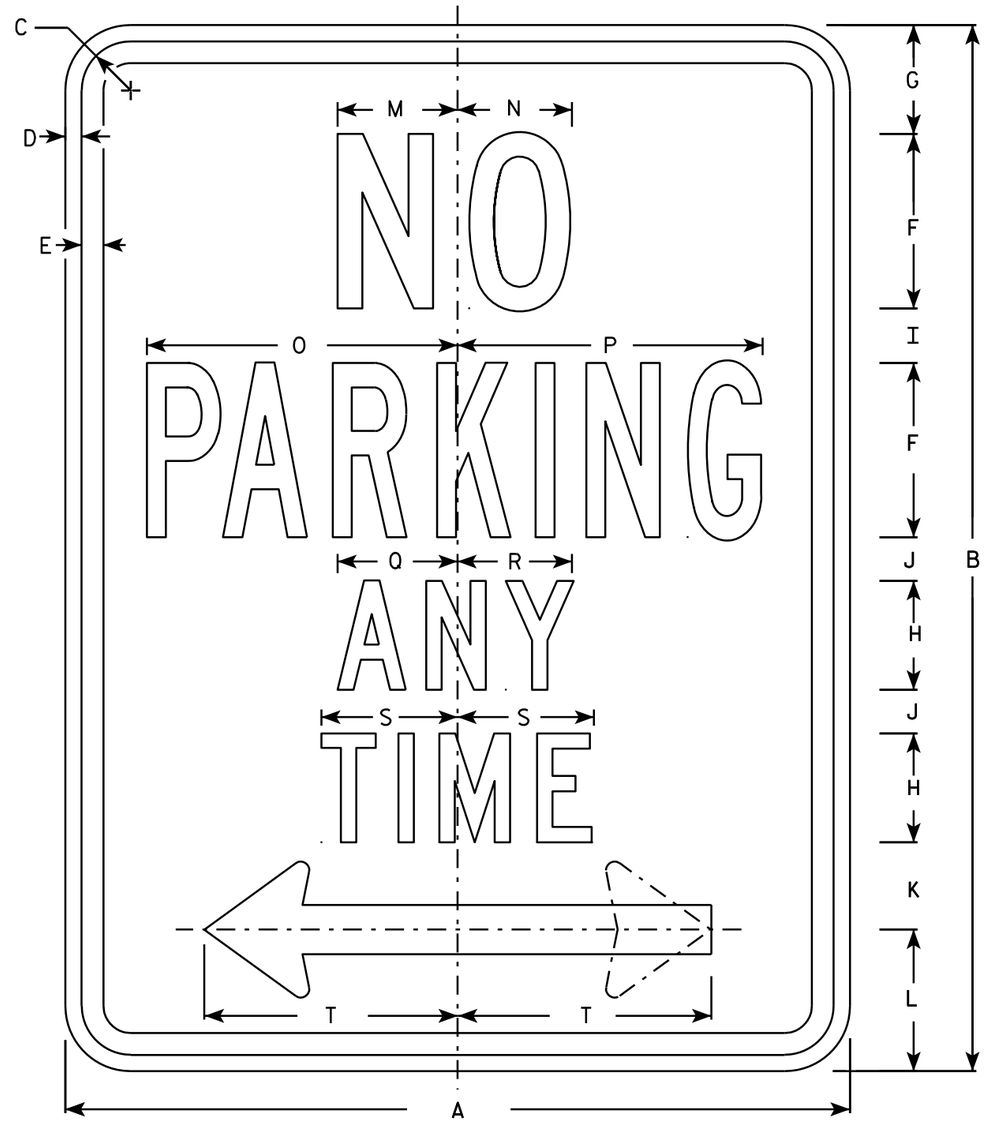
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 74 | E

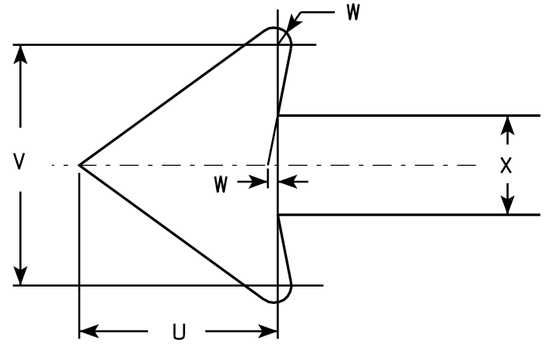
FILE NAME : C:\Users\PROJECTS\tr_stdplate\R21.DGN PLOT DATE : 28-MAY-2010 08:32 PLOT BY : ditjph PLOT NAME : PLOT SCALE : 4.717577:1.000000
WISDOT/CADDS SHEET 42



R7-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Red
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 3 and 4 are series C, line 2 is series B.
6. R7-1D (double arrow)
   R7-1L (left arrow)
   R7-1R (right arrow)



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 1/4	2 1/8	2 1/2	3 7/8	1 1/2	1 3/4	1/8	3/4			1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	2 3/4	2 5/8	3 1/8	5 7/8	2 1/4	2 5/8	1/4	1 1/8			3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
4																											
5																											

STANDARD SIGN
R7-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/31/2011 PLATE NO. R7-1.9

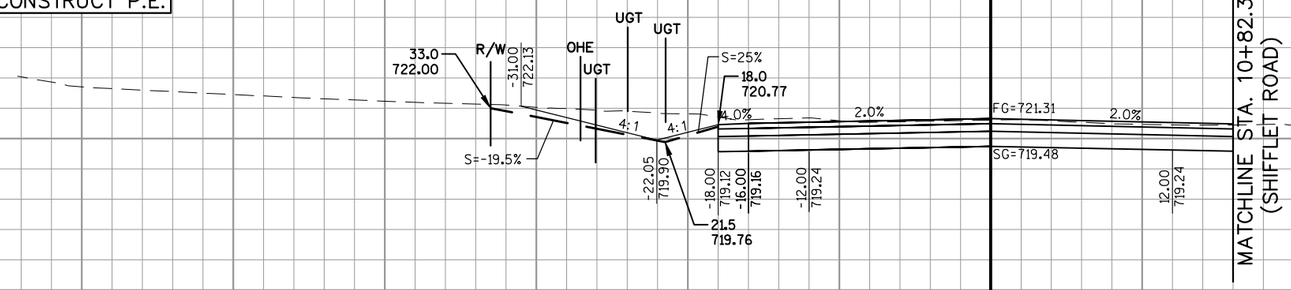
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 75	E

EARTHWORK-MAINLINE

	AREA (SF)					INCREMENTAL VOL (CY)								CUMMULATIVE VOLUME (CY)							
STATION	CUT	SALVAGED/ UNUSABLE PAV'T MATERIAL	FILL	MARSH EX	EBS	CUT NOTE 1	SALVAGED/ UNUSABLE PAV'T MATERIAL NOTE 2	FILL NOTE 3	MARSH EX	REDUCED MARSH IN FILL (0.6) NOTE 4	FILL (25%)	SELECT CRUSHED MATERIAL (1.5)	EBS	CUT 1.00 NOTE 1	FILL	MARSH EX	REDUCED MARSH IN FILL (0.6) NOTE 4	FILL (25%) NOTE 5	SELECT CRUSHED MATERIAL (1.5)	EBS	MASS ORDINATE NOTE 6
50+00	60	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
50+50	69	0	3	0	0	120	0	3	0	0	4	0	0	120	3	0	0	4	0	0	120
50+93	69	0	3	0	0	110	0	5	0	0	6	0	0	230	8	0	0	10	0	0	230
50+93	66	0	0	0	0	0	0	0	0	0	0	0	0	230	8	0	0	10	0	0	230
51+00	66	0	0	0	0	17	0	0	0	0	0	0	0	247	8	0	0	10	0	0	247
51+50	69	0	1	0	0	125	0	1	0	0	1	0	0	372	9	0	0	11	0	0	372
51+89	69	0	1	0	0	99	0	1	0	0	1	0	0	471	10	0	0	13	0	0	471
51+89	82	0	21	0	0	0	0	0	0	0	0	0	0	471	10	0	0	13	0	0	471
52+00	82	0	21	0	0	33	0	8	0	0	10	0	0	504	18	0	0	23	0	0	504
52+50	70	0	18	0	0	141	0	36	0	0	45	0	0	645	54	0	0	68	0	0	645
53+00	52	0	34	0	0	113	0	48	0	0	61	0	0	758	103	0	0	128	0	0	758
53+50	49	0	17	0	0	93	0	47	0	0	59	0	0	851	150	0	0	187	0	0	851
54+00	47	0	9	0	0	89	0	24	0	0	30	0	0	940	174	0	0	217	0	0	940
54+50	54	0	7	0	0	94	0	14	0	0	18	0	0	1034	188	0	0	235	0	0	1034
55+00	56	0	9	0	0	102	0	15	0	0	18	0	0	1136	202	0	0	253	0	0	1136
55+50	78	0	1	0	0	125	0	9	0	0	11	0	0	1261	211	0	0	264	0	0	1261
56+00	106	0	1	0	0	170	0	1	0	0	1	0	0	1431	212	0	0	266	0	0	1431
56+50	129	0	0	0	0	217	0	1	0	0	1	0	0	1648	213	0	0	267	0	0	1648
57+00	138	0	0	0	0	247	0	0	0	0	0	0	0	1895	213	0	0	267	0	0	1895
57+50	130	0	0	0	0	248	0	0	0	0	0	0	0	2143	213	0	0	267	0	0	2143
58+00	115	0	1	0	0	227	0	1	0	0	1	0	0	2370	214	0	0	268	0	0	2370
58+50	100	0	5	0	0	199	0	5	0	0	6	0	0	2569	219	0	0	274	0	0	2569
59+00	81	0	17	0	0	168	0	20	0	0	26	0	0	2737	240	0	0	300	0	0	2737
59+08	81	0	17	0	0	24	0	5	0	0	6	0	0	2761	245	0	0	307	0	0	2761
59+08	81	0	17	0	0	0	0	0	0	0	0	0	0	2761	245	0	0	307	0	0	2761
59+08	66	0	0	0	0	0	0	0	0	0	0	0	0	2761	245	0	0	307	0	0	2761
59+50	66	0	0	0	0	103	0	0	0	0	0	0	0	2864	245	0	0	307	0	0	2864
59+69	66	0	0	0	0	46	0	0	0	0	0	0	0	2910	245	0	0	307	0	0	2910
59+69	75	0	0	0	0	0	0	0	0	0	0	0	0	2910	245	0	0	307	0	0	2910
60+00	75	0	0	0	0	86	0	0	0	0	0	0	0	2996	245	0	0	307	0	0	2996
60+50	89	0	0	0	0	152	0	0	0	0	0	0	0	3148	245	0	0	307	0	0	3148
61+00	103	0	0	0	0	178	0	0	0	0	0	0	0	3326	245	0	0	307	0	0	3326
61+50	115	0	0	0	0	202	0	0	0	0	0	0	0	3528	245	0	0	307	0	0	3528
62+00	127	0	0	0	0	224	0	0	0	0	0	0	0	3752	245	0	0	307	0	0	3752
62+50	131	0	0	0	0	239	0	0	0	0	0	0	0	3991	245	0	0	307	0	0	3991
63+00	109	0	0	0	0	222	0	0	0	0	0	0	0	4213	245	0	0	307	0	0	4213
63+50	115	0	1	0	0	207	0	1	0	0	1	0	0	4420	246	0	0	308	0	0	4420
64+00	104	0	2	0	0	203	0	3	0	0	4	0	0	4623	249	0	0	312	0	0	4623
64+50	108	0	2	0	0	197	0	4	0	0	5	0	0	4820	253	0	0	317	0	0	4820
64+73	108	0	2	0	0	92	0	2	0	0	3	0	0	4912	255	0	0	319	0	0	4912
64+73	63	0	0	0	0	0	0	0	0	0	0	0	0	4912	255	0	0	319	0	0	4912
65+00	63	0	0	0	0	63	0	0	0	0	0	0	0	4975	255	0	0	319	0	0	4975
65+15	63	0	0	0	0	35	0	0	0	0	0	0	0	5010	255	0	0	319	0	0	5010
65+15	0	0	0	0	0	0	0	0	0	0	0	0	0	5010	255	0	0	319	0	0	5010
65+50	0	0	0	0	0	0	0	0	0	0	0	0	0	5010	255	0	0	319	0	0	5010
						COLUMN SUBTOTALS =	5010	0	255	0	0	319	0	0	5010	255	0	0	319	0	0	5010
						SHIFFLET ROAD =	385	0	10	0	0	13	0	0	5395	265	0	0	332	0	0	5395
						ARLINGTON COURT =	100	0	0	0	0	0	0	0	5495	265	0	0	332	0	0	5495
						SOUTH WOOD STREET =	80	0	0	0	0	0	0	0	5575	265	0	0	332	0	0	5575
						NORTH WOOD STREET =	115	0	0	0	0	0	0	0	5690	265	0	0	332	0	0	5690
						P.E., F.E.,,C.E. =	210	0	6	0	0	8	0	0	5900	271	0	0	340	0	0	5560
						COLUMN TOTALS =	5900	0	271	0	0	340	0	0								

NOTES:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
4 - REDUCED MARSH IN FILL	REDUCED MARSH THAT CAN BE USED IN FILL
5 - FILL (25%)	FILL 25%: (FILL -REDUCED MARSH IN FILL)*1.25
6 - MASS ORDINATE	(CUT - FILL (25%))

STA. 51+19, LT.
CONSTRUCT P.E.



MATCHLINE STA. 10+82.39
(SHIFFLET ROAD)

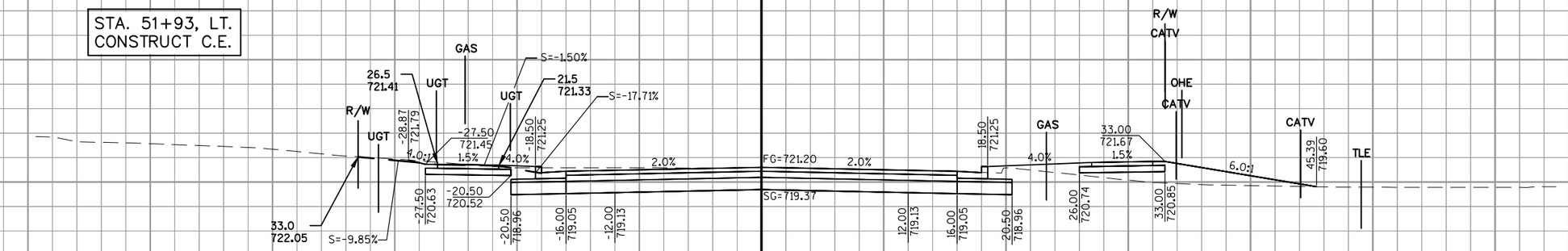
STA. 10+00, LT. (SHIFFLET ROAD)
END SPECIAL DITCH
EL.718.83

STA. 50+50, RT.
BEGIN SPECIAL DITCH
EL.719.92

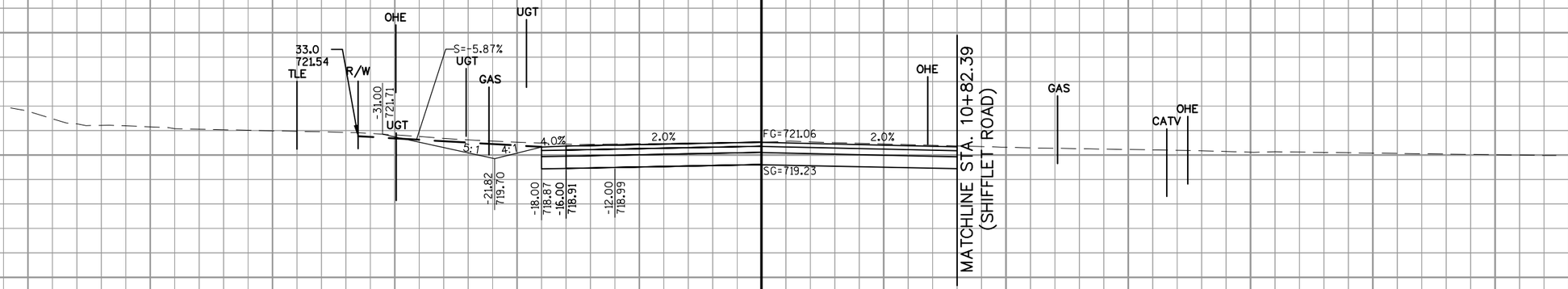
BEGIN PROJECT 5916-00-72
STA 50+00
MATCH EXISTING

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E

STA. 51+93, LT.
CONSTRUCT C.E.

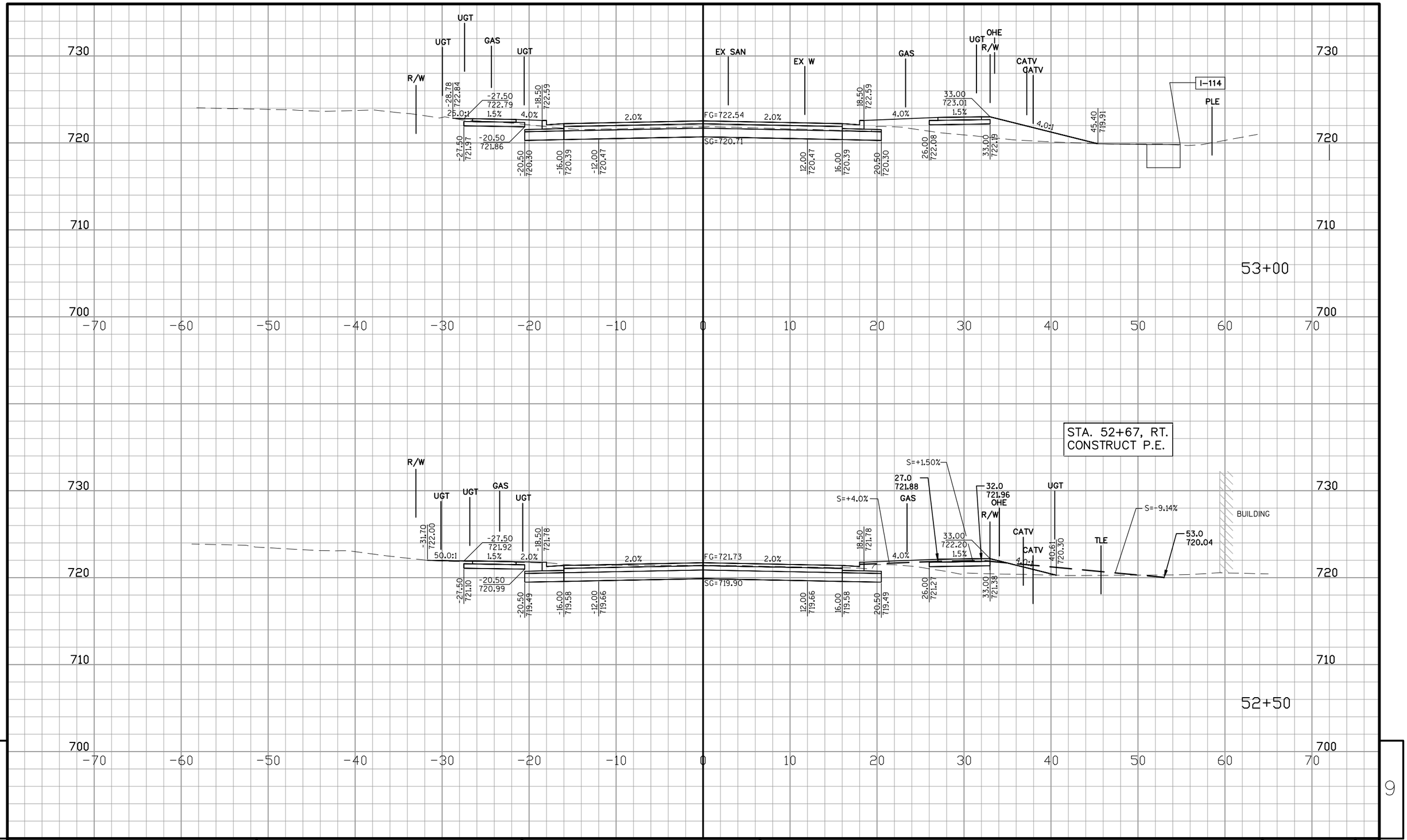


STA. 51+61, LT.
CONSTRUCT C.E.



PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E

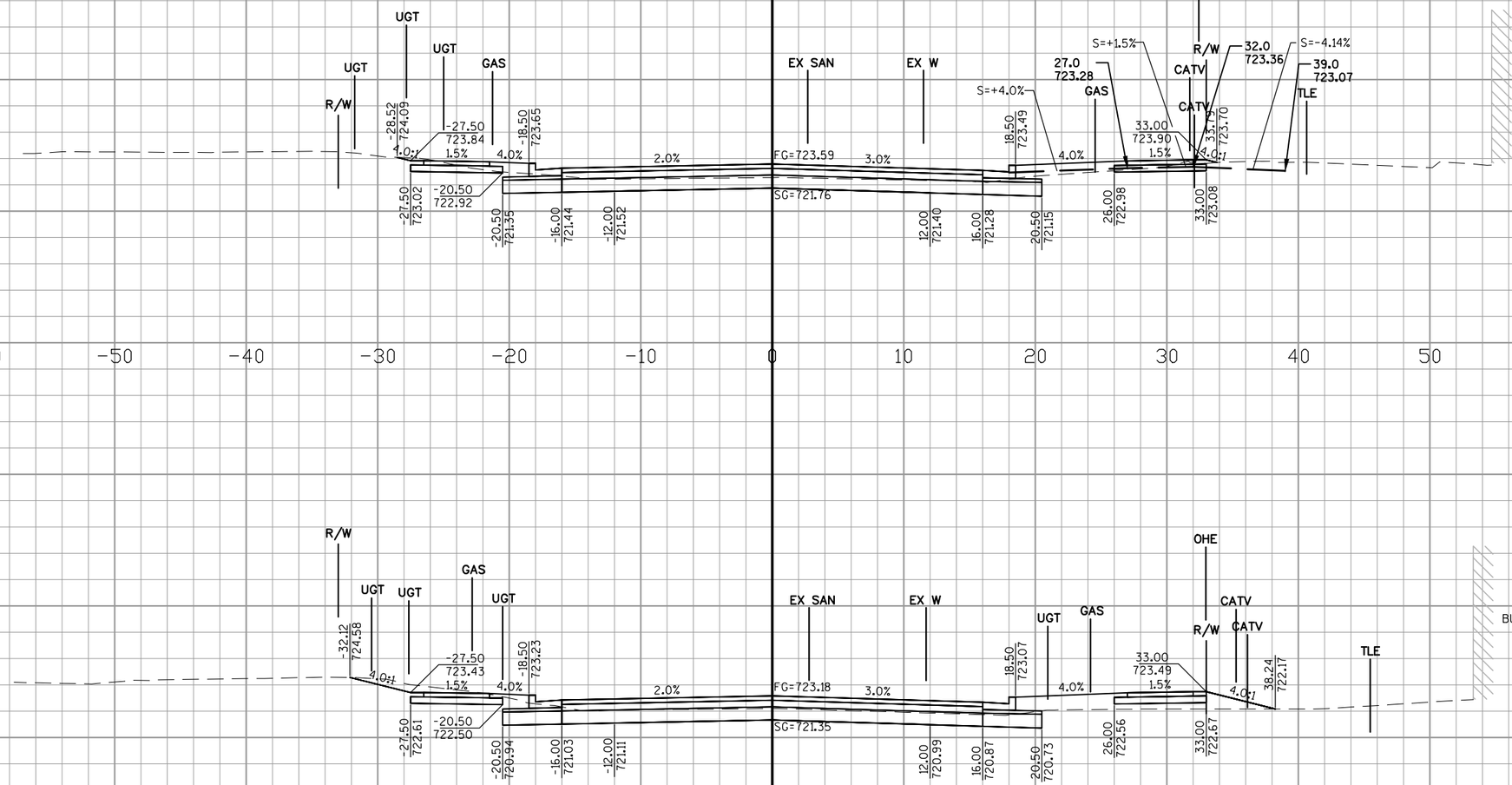
FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\MADISON STREET CROSS SECTIONS.DWG
PLOT DATE : 1/27/2014 10:49 AM
PLOT BY : BALLWEG,THOMAS
WISDOT/CADD: 78 T 42



PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY:SAUK	CROSS SECTIONS	SHEET	E

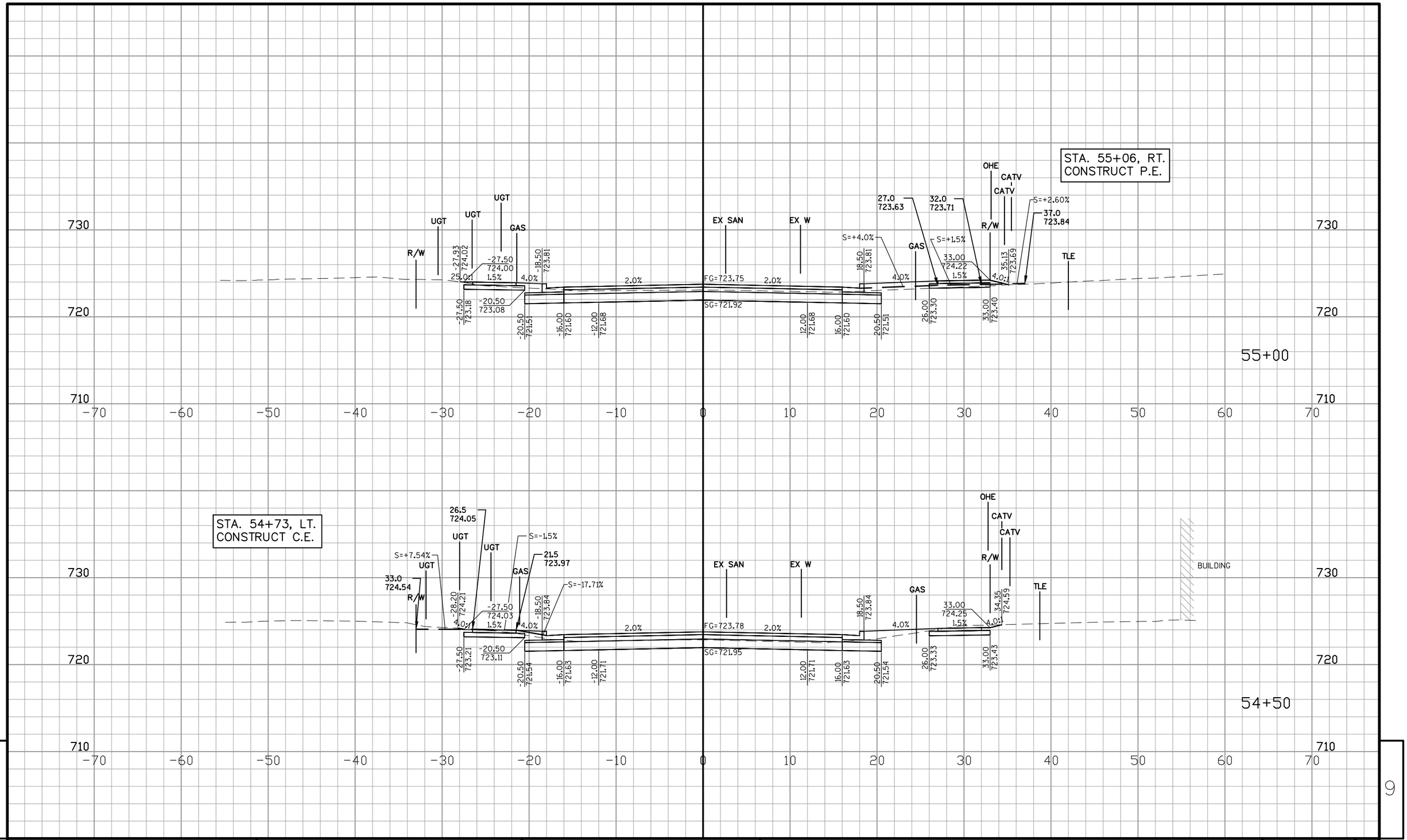
FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\MADISON STREET CROSS SECTIONS.DWG PLOT DATE : 12/16/2013 4:06 PM PLOT BY : HANOLD, ROBERT

STA. 53+92, RT.
CONSTRUCT P.E.



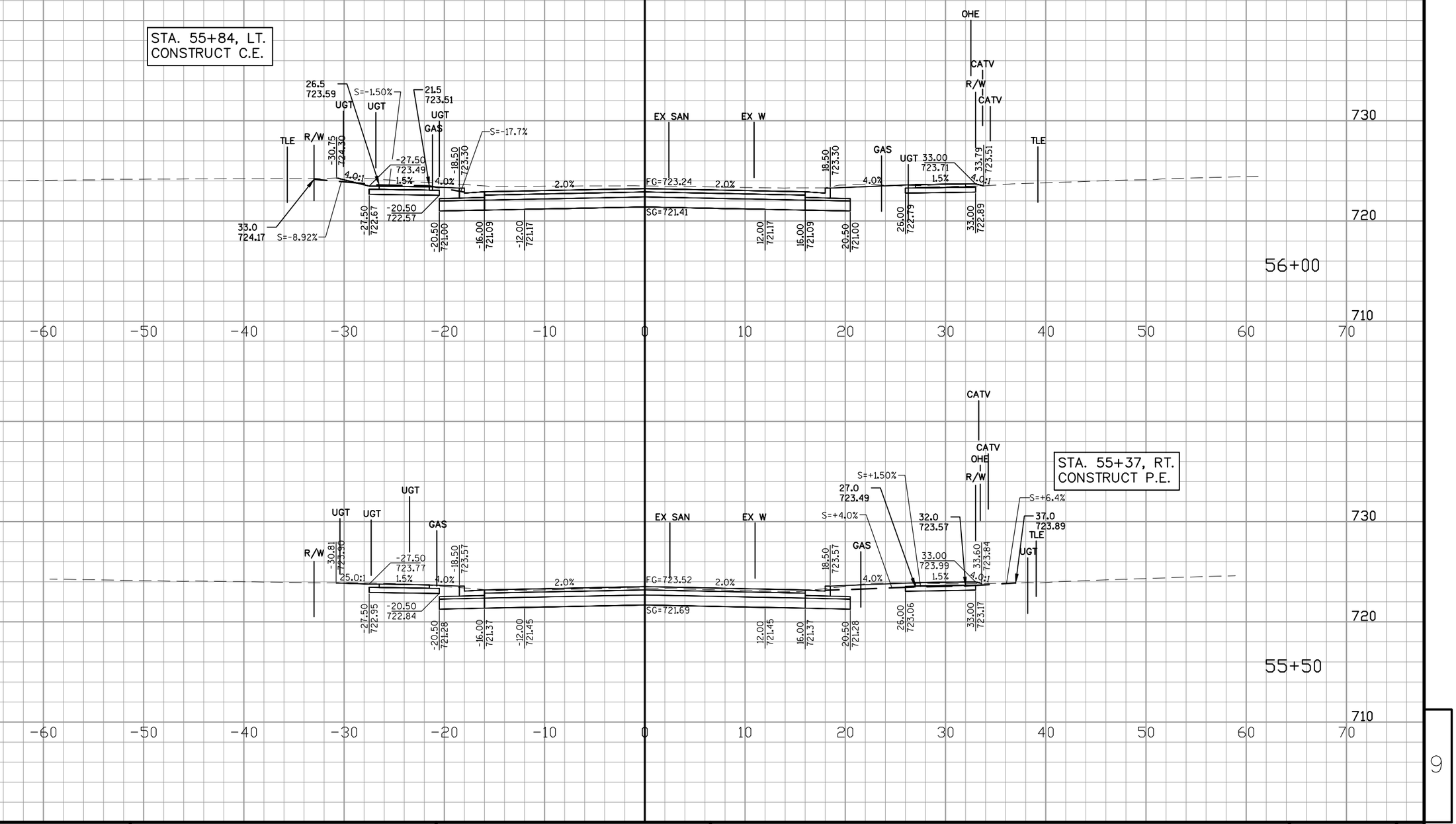
PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY:SAUK	CROSS SECTIONS	SHEET	E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\MADISON STREET CROSS SECTIONS.DWG
PLOT DATE : 12/16/2013 4:06 PM
PLOT BY : HANOLD, ROBERT
WISDOT/CADI 80 ET 42



PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\MADISON STREET CROSS SECTIONS.DWG PLOT DATE : 12/16/2013 4:07 PM PLOT BY : HANOLD, ROBERT

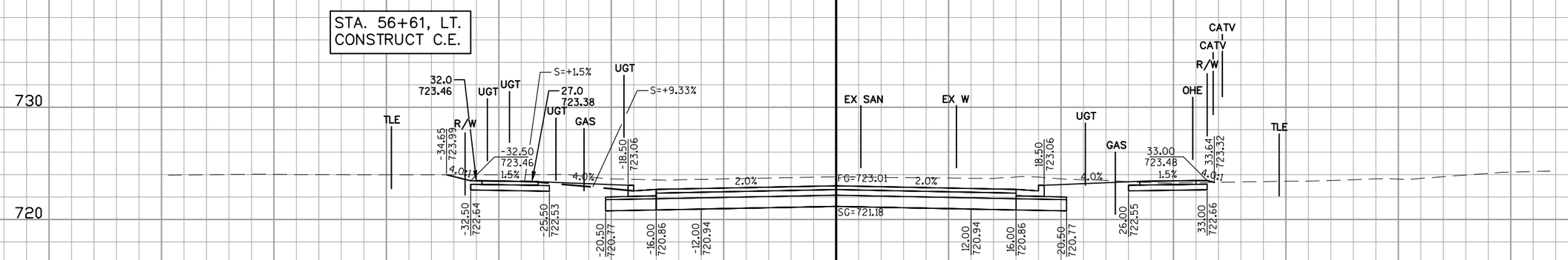


PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E

STA. 56+95, RT.
CONSTRUCT P.E.

57+00

STA. 56+61, LT.
CONSTRUCT C.E.

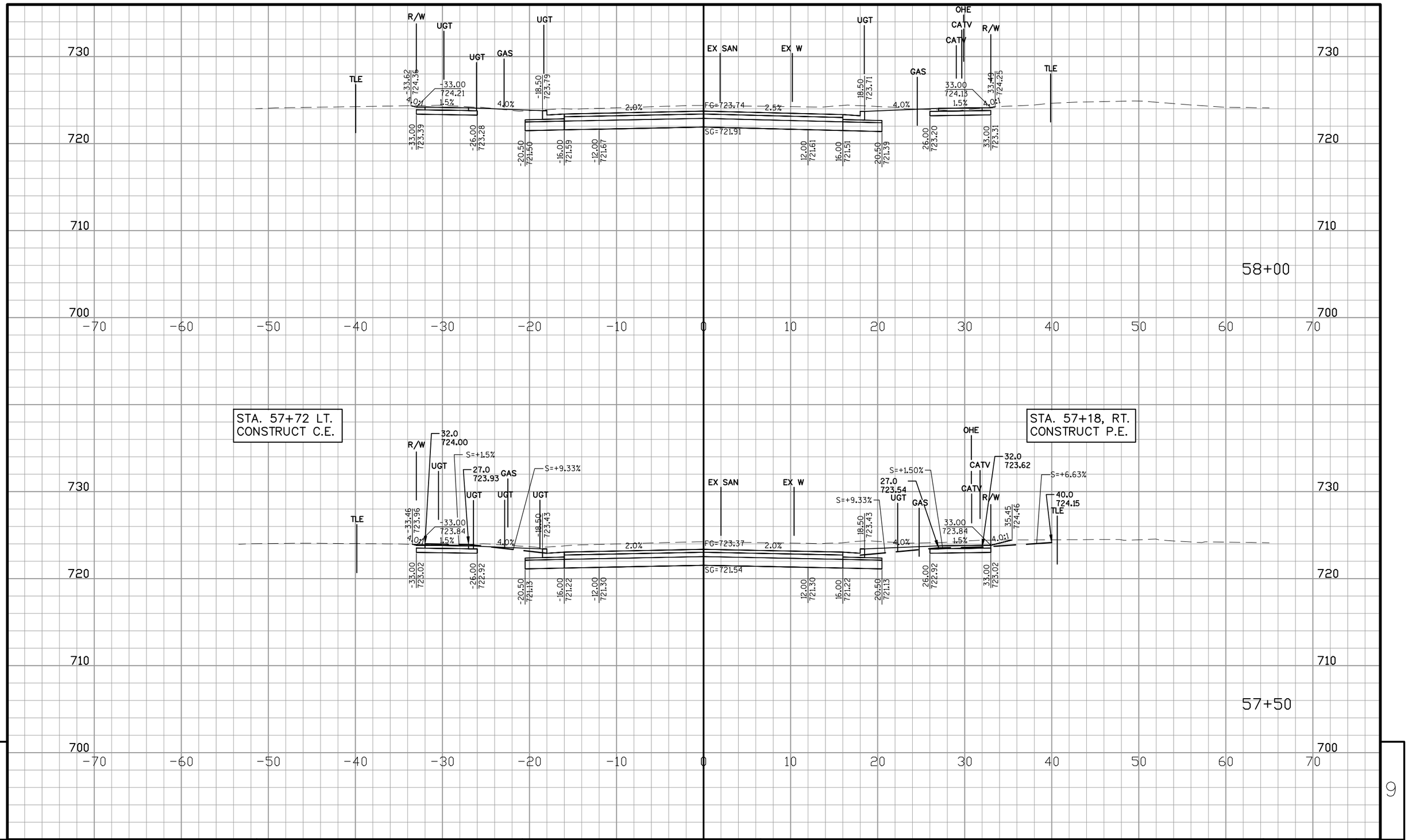


56+50

9

9

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E



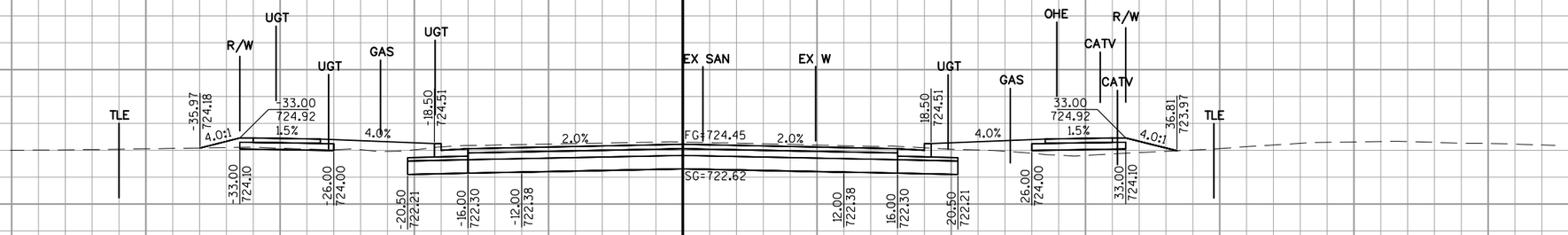
PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\MADISON STREET CROSS SECTIONS.DWG

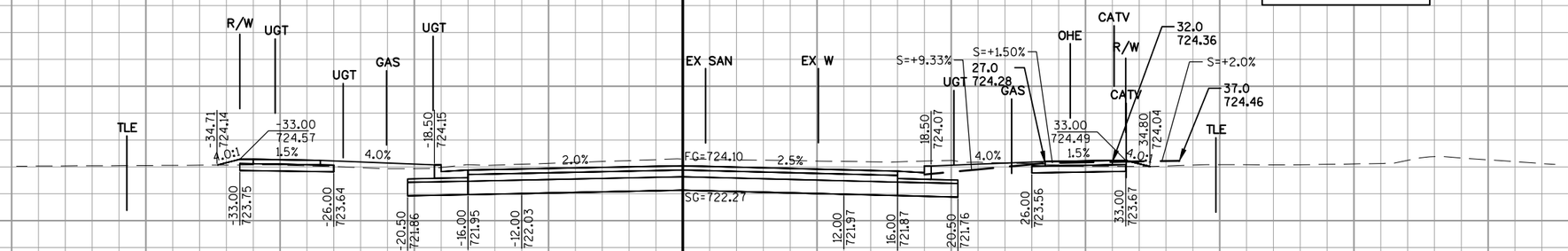
PLOT DATE : 1/24/2014 2:54 PM

PLOT BY : BALLWEG,THOMAS

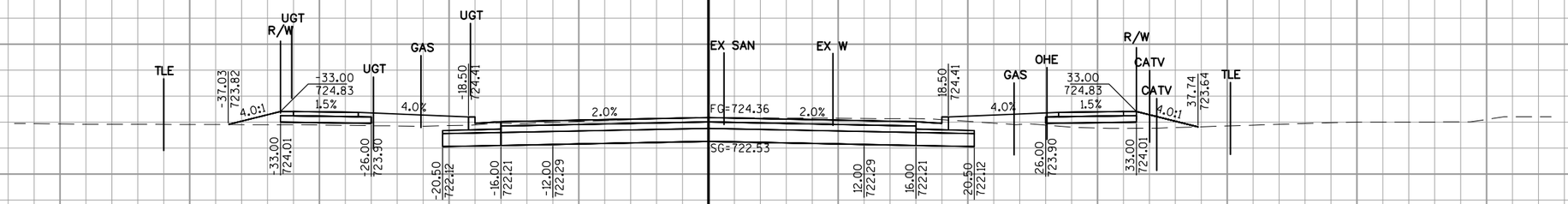
WISDOT/CADI 84 ET 42

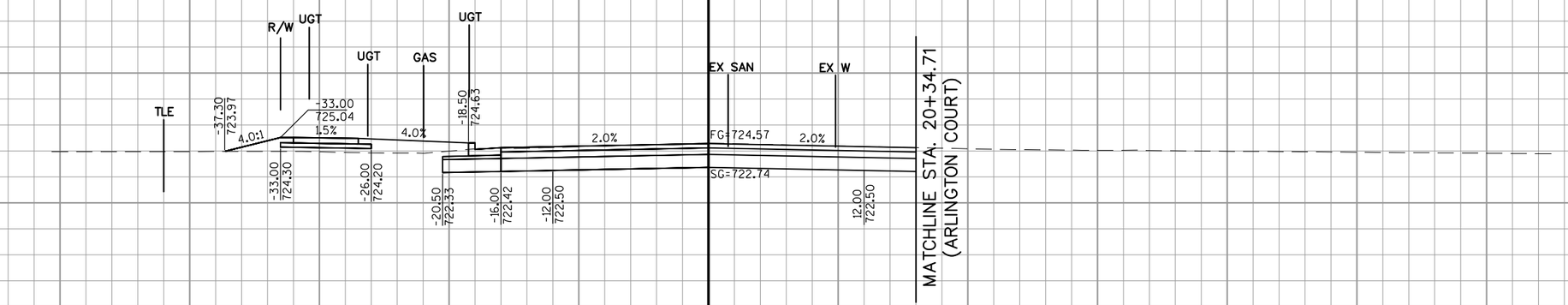


STA. 58+31, RT.
CONSTRUCT P.E.



PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY:SAUK	CROSS SECTIONS	SHEET	E

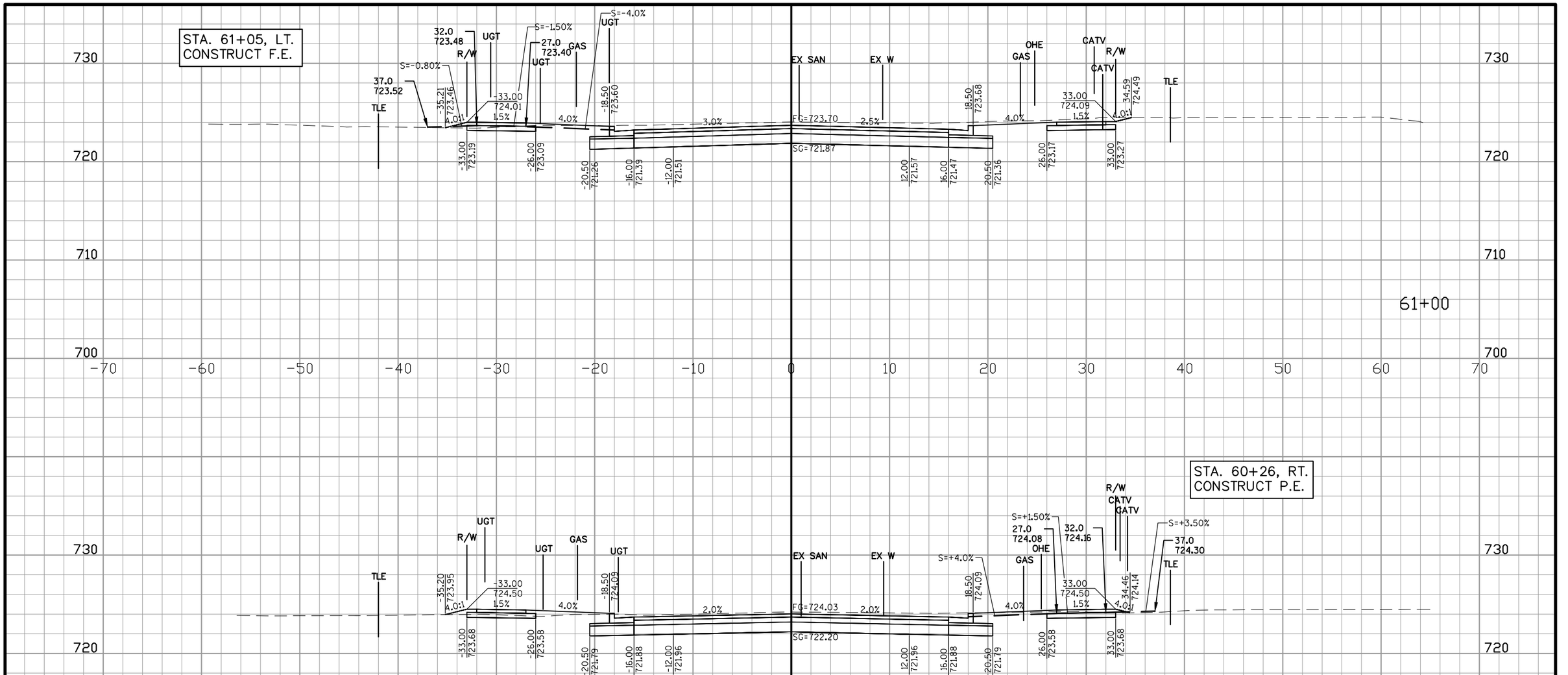




PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY:SAUK	CROSS SECTIONS	SHEET	E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\MADISON STREET CROSS SECTIONS.DWG PLOT DATE : 12/16/2013 4:10 PM PLOT BY : HANOLD, ROBERT

WISDOT/CADI 86 ET 42

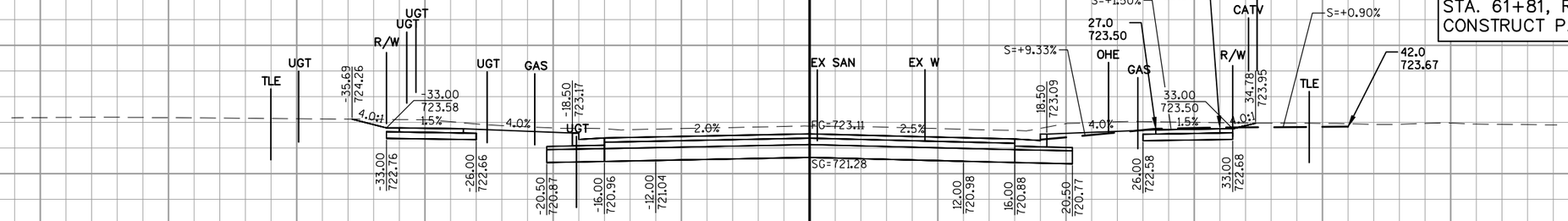


PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\MADISON STREET CROSS SECTIONS.DWG PLOT DATE : 12/16/2013 4:10 PM PLOT BY : HANOLD, ROBERT

WISDOT/CADI 87 ET 42

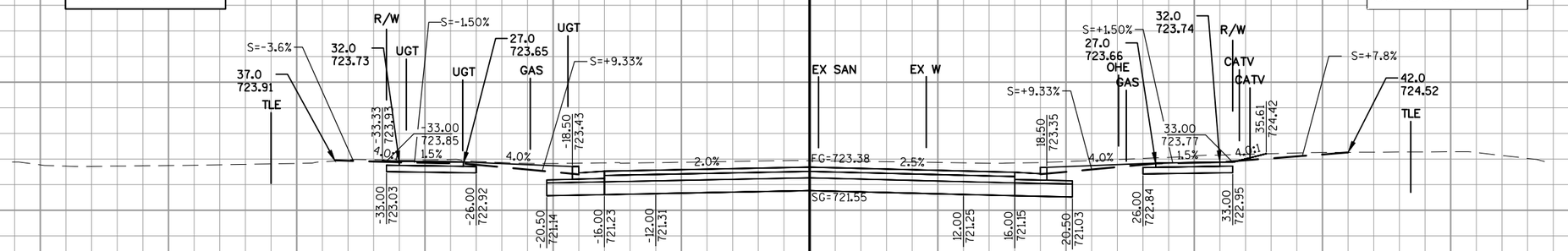
STA. 61+81, RT.
CONSTRUCT P.E.



62+00

STA. 61+69, LT.
CONSTRUCT P.E.

STA. 61+56, RT.
CONSTRUCT P.E.

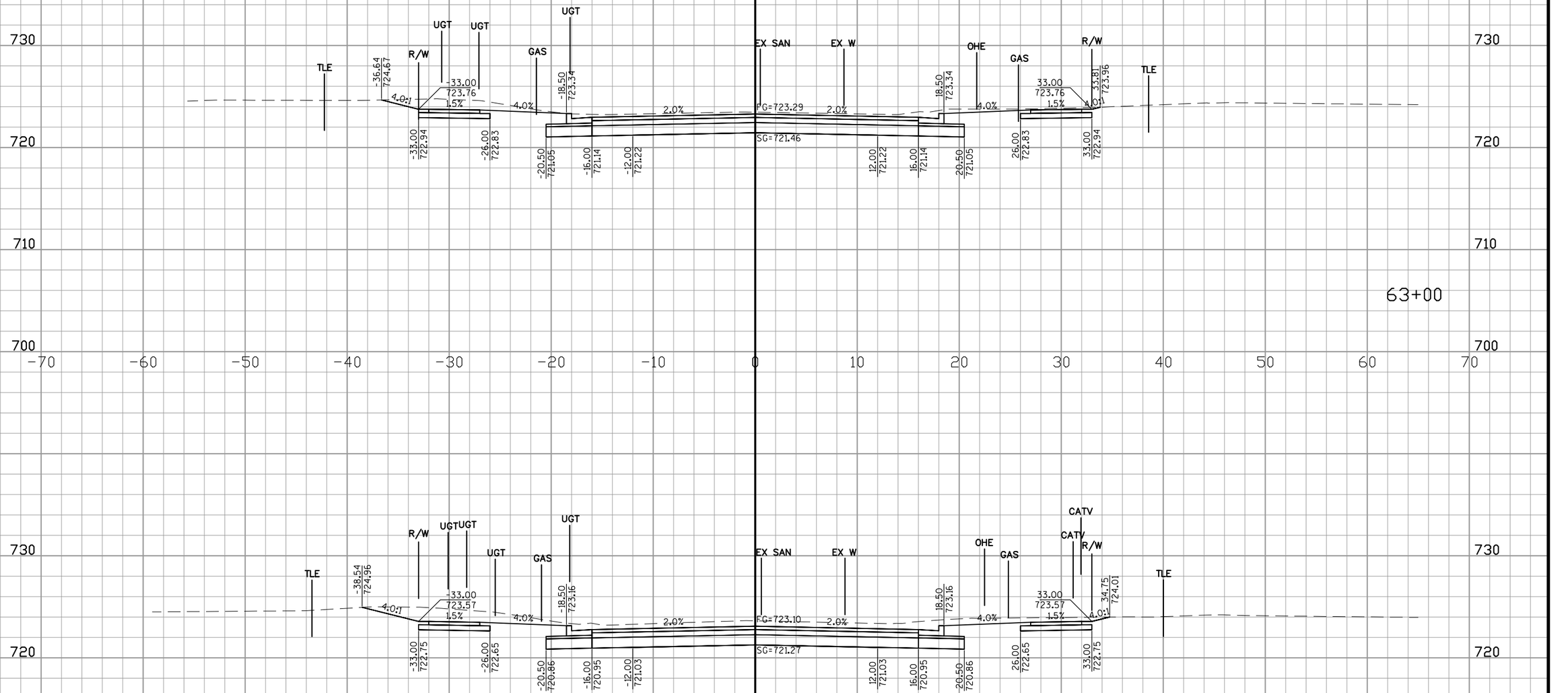


61+50

9

9

PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY: SAUK	CROSS SECTIONS	SHEET	E



PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY:SAUK	CROSS SECTIONS	SHEET	E

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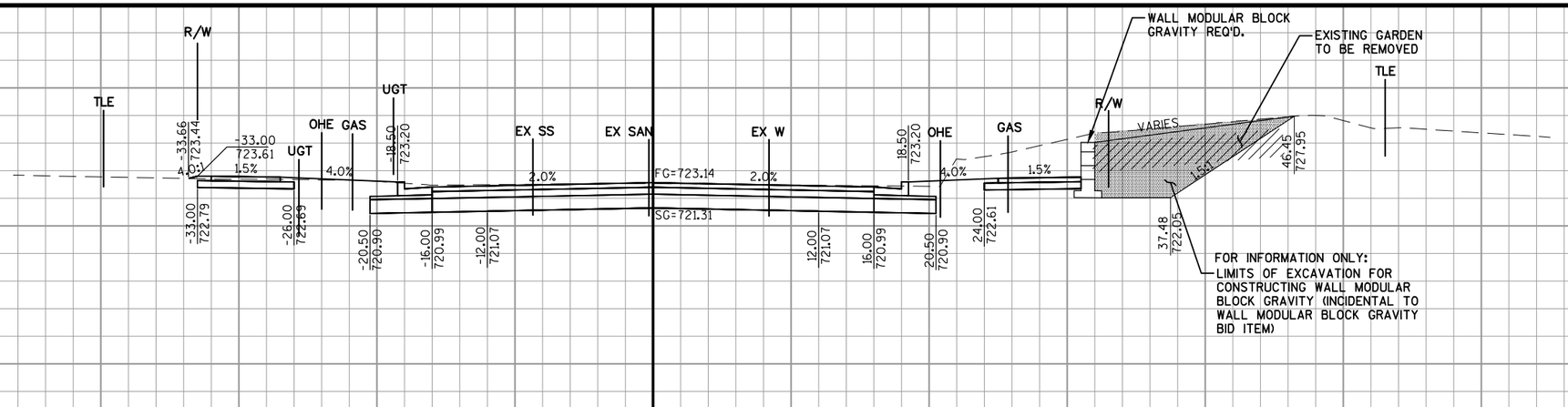
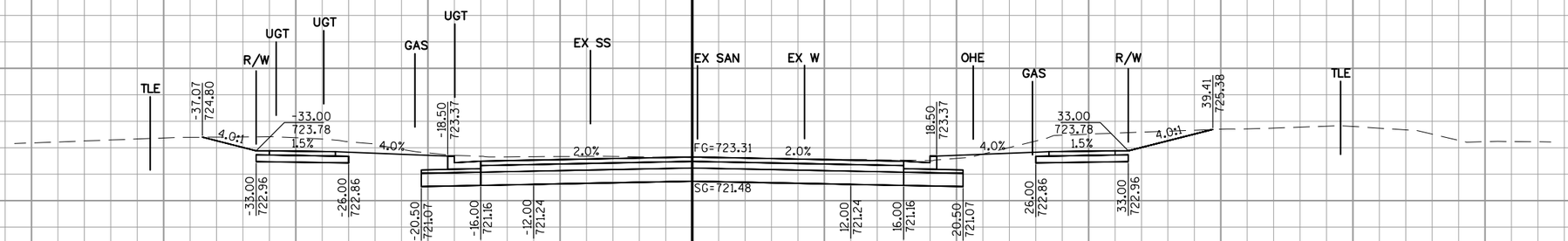
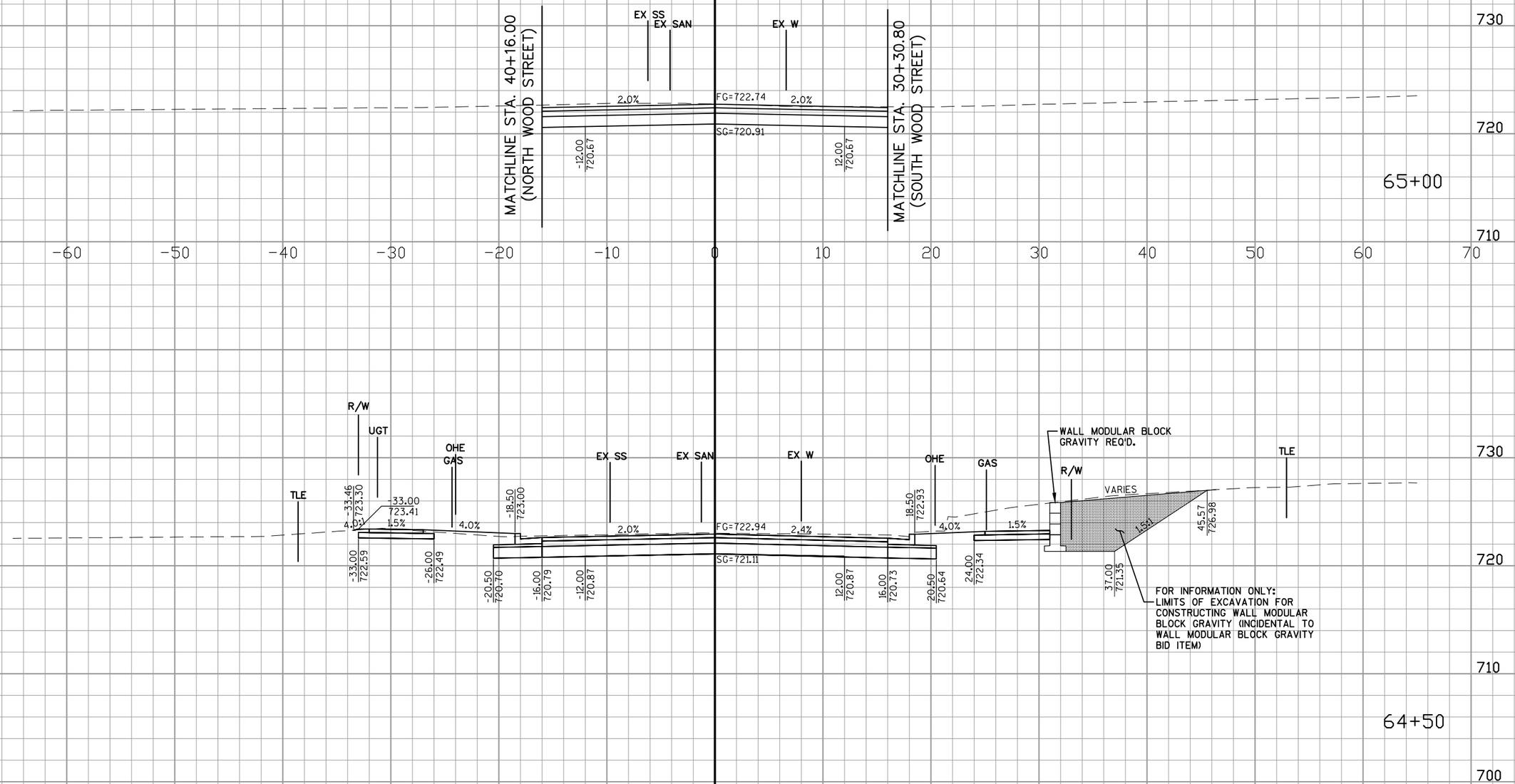




PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY:SAUK	CROSS SECTIONS	SHEET	E

END PROJECT 5916-00-72
STA 65+15
MATCH EXISTING



PROJECT NO: 5916-00-72	HWY: WEST MADISON STREET	COUNTY:SAUK	CROSS SECTIONS	SHEET	E

FILE NAME: S:\PROJECTS\S34083 VILLAGE OF SG JEFFERSON STREET\DESIGN FILES\MADISON STREET CROSS SECTIONS.DWG
PLOT DATE : 12/17/2013 3:55 PM
PLOT BY : HANOLD, ROBERT

Wisconsin Department of Transportation

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