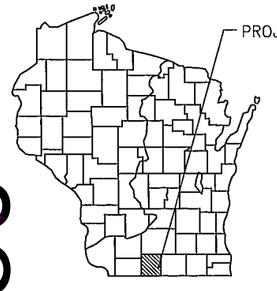
SWL JULY 2014

PROJECT ID: 5911-00-70
WITH: N/A

COUNTY: GREEN

ORDER OF SHEETS

TOTAL SHEETS = 94



08

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

CITY OF BRODHEAD, WEST 17TH STREET

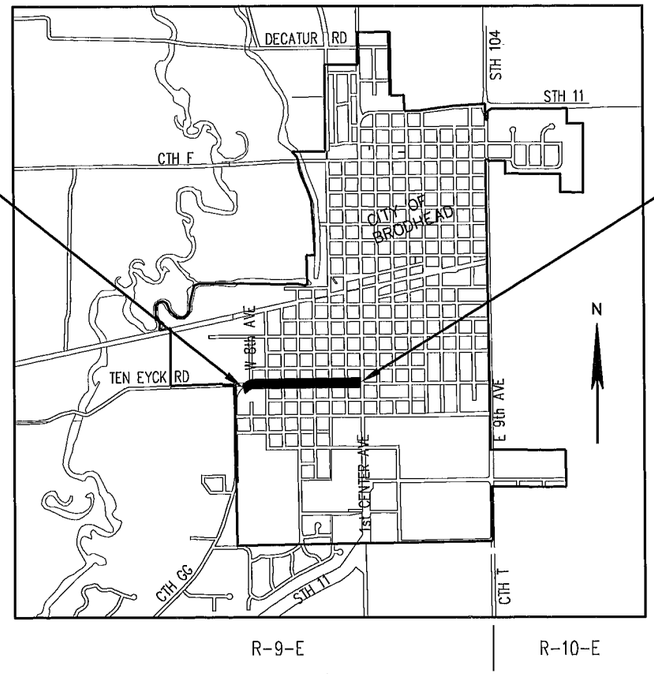
(WEST 8TH AVENUE TO 1ST CENTER AVENUE)

LOCAL STREET

GREEN COUNTY

STATE PROJECT NUMBER
5911-00-70

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5911-00-70	WISC 2014261	1



BEGIN PROJECT
STA 14+00
Y - 141941.376
X - 680001.389

END PROJECT
STA 37+52
Y - 141971.844
X - 682352.448

LAYOUT
SCALE 0 1 MI

TOTAL NET LENGTH OF CENTERLINE = 0.445 MI

COORDINATES ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), GREEN COUNTY.

DESIGN DESIGNATION

AADT (2014)	=	1,450
AADT (2034)	=	1,700
DHV (2014)	=	221
D (%)	=	50/50
T (% OF ADT)	=	5.0%
DESIGN SPEED	=	30 MPH
ESALS	=	204,400

CONVENTIONAL SYMBOLS

PLAN
- SECTION LINE
- QUARTER LINE
- PROPERTY LINE
- LIMITED EASEMENT
- EXISTING R/W LINE
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- HIGH VOLTAGE (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA
- RIGHT-OF-WAY MARKERS

PROFILE
- GRADE LINE
- ORIGINAL GROUND
- CULVERT (Profile View)

UTILITIES
- ELECTRIC — E —
- OVERHEAD LINES — OH —
- FIBER OPTIC — FO —
- GAS — G —
- SANITARY SEWER — SAN —
- STORM SEWER — SS —
- TELEPHONE — T —
- WATER — W —
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

ACCEPTED FOR CITY OF BRODHEAD
1-24-2014 DATE — MAYOR

ORIGINAL PLANS PREPARED BY
Cedar corporation
MENOMONIE - MADISON - GREEN BAY
www.cedarcorp.com
800-472-7372

WISCONSIN
DAVID K. SAUER
E-18399
MADISON, WI
PROFESSIONAL ENGINEER
1-24-2014 DATE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor: CEDAR CORPORATION
Designer: CEDAR CORPORATION
Management Consultant: Kjohnson Engineers, Inc.

APPROVED FOR THE DEPARTMENT
1/29/14 DATE — Kirby A. Johnson, Management Consultant Signature

E

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\010101_TI_48930008.DWG PLOT DATE : 1/22/2014 6:26 AM PLOT BY : BRYON MOTSZKO

LIST OF STANDARD ABBREVIATIONS

AC	ACRE
AE	APRON ENDWALL
AH	AHEAD
AGG	AGGREGATE
ALUM MON	ALUMINUM MONUMENT
AP	ACCESS POINT
ASPH	ASPHALT
B/C	BACK OF CURB
BK	BACK
B/L	BASE LINE
BM	BENCH MARK
BR	BRIDGE
C/L	CENTER LINE
CC	CENTER TO CENTER
Δ	CENTRAL ANGLE OR DELTA
CH	CHORD LENGTH
CONC MON	CONCRETE MONUMENT
CTH	COUNTY TRUNK HIGHWAY
CMP	CORRUGATED METAL PIPE
CONST	CONSTRUCTION
CP	CULVERT PIPE
CSM	CERTIFIED SURVEY MAP
D	DEGREE OF CURVE
DWY	DRIVEWAY
EA	EACH
E	EAST OR EAST PROJECT COORDINATE
ERLY	EASTERLY
X	EAST GRID COORDINATE
ETAL	AND OTHERS
EX, EXIST	EXISTING
F/C	FACE OF CURB
FE	FIELD ENTERANCE
FT	FEET
GAR	GARAGE
G	GAS
GN	GRID NORTH
GOV'T	GOVERNMENT
H	HOUSE
IP	IRON PIPE OR PIN
INL	INLET
LT	LEFT
L	LENGTH (OF CURVE)
LIN FT	LINEAR FOOT
LC	LONG CHORD OF CURVE
LCB	LONG CHORD BEARING
MAX	MAXIMUM
MIN	MINIMUM
MH	MANHOLE
MI	MILE
N	NORTH OR NORTH PROJECT COORDINATE
Y	NORTH GRID COORDINATE
NO	NUMBER
OL	OUTLOT
OD	OUTSIDE DIAMETER
P	PAGE
PE	PRIVATE ENTRANCE
PLE	PERMANENT LIMITED EASEMENT
PERM	PERMANENT
P/P	POWER POLE
PT	POINT
PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
PRC	POINT OF REVERSE CURVATURE
PT	POINT OF TANGENCY
POC	POINT ON CURVE
POT	POINT ON TANGENT
PE	PRIVATE ENTRANCE
PROJ	PROJECT
R	RADIUS
RCP	REINFORCED CONRETE PIPE
RP	RADIUS POINT
RR	RAILROAD
R	RANGE
R/L	REFERENCE LINE
RP	REFERENCE POINT
REM	REMAINING
REQD	REQUIRED
RES	RESIDENCE OR RESIDENTIAL
RT	RIGHT
R/W	RIGHT-OF-WAY
RD	ROAD
SEC	SECTION
SHLDR	SHOULDER
SD	STORM DRAINAGE
SF OR SQ FT	SQUARE FEET
SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
STH	STATE TRUNK HIGHWAY
STA	STATION
SUBD	SUBDIVISION
T	TOWNSHIP
TEMP	TEMPORARY
TLE	TEMPORARY LIMITED EASEMENT
TL OR T/L	TRANSIT LINE
TYP	TYPICAL
USH	U.S. HIGHWAY
VILL	VILLAGE
VOL	VOLUME
W	WEST
WRLY	WESTERLY
WISC	WISCONSIN

SECTION 2 SHEETS

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT PRIOR APPROVAL OF THE ENGINEER.

SILT FENCE TO BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER. SILT FENCE TO BE PLACED PRIOR TO CONSTRUCTION.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL EROSION CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER. ADDITIONAL EROSION CONTROL DEVICES MAY BE REQUIRED.

THE EXACT LOCATION OF PRIVATE AND COMMERCIAL ENTRANCES SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER. A VERTICAL SAWCUT SHALL BE MADE THROUGH EXISTING DRIVEWAYS AT REMOVAL LIMITS AS DIRECTED BY ENGINEER. ALL SAWCUTS SHALL BE A NEAT AND STRAIGHT LINE.

EXCAVATION BELOW SUBGRADE (EBS) IS NOT USED TO BALANCE YARDAGE, HOWEVER IS MEASURED AND PAID FOR AS COMMON EXCAVATION. EBS LOCATIONS TO BE VERIFIED IN THE FIELD BY THE ENGINEER.

BEARINGS SHOWN ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (GREEN COUNTY).

WHEN THE QUANTITY OF ITEMS OF SUBBASE OR SURFACE COURSE IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLAN IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

ALL RADIUS DIMENSIONS ON THE PLAN FOR CURB & GUTTER ARE TO FLAG LINE UNLESS OTHERWISE NOTED.

CURVE DATA IS BASED ON THE ARC DEFINITION.

CONTRACTOR SHALL NOT OPERATE ANY VALVE OR HYDRANTS.

SHRINKAGE IS ESTIMATED AT 25%.

HMA PAVEMENT SHALL CONSIST OF COURSES AS FOLLOWS UNLESS OTHERWISE NOTED ON THE PLANS:

4 1/2" TOTAL DEPTH	TYPE	NOMINAL MAX SIZE GRADATION	PERFORMANCE GRADE
2 1/2"-INCH LOWER	E-1	19 MM	PG 64-22
2"-INCH UPPER	E-1	12.5 MM	PG 64-28

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT												
ASPHALT	.70-.95											
CONCRETE	.80-.95											
BRICK	.70-.80											
DRIVES, WALKS	.75-.85											
ROOFS	.75-.95											
GRAVEL ROADS, SHOULDERS	.40-.60											

TOTAL PROJECT AREA = 3.6 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITES = 3.5 ACRES

PAVEMENT CORE LOG

WEST 17th ST (CTH GG)

BORE NO.	STATION/OFFSET	HMA	CONCRETE	SUBBASE
1	17+00, 3' RT	5.0"	7.0"	18.0"
2	22+50, 3' RT	5.5"	6.5"	24.0"
3	27+25, 0' RT	6.3"	6.8"	28.9"
4	33+75, 0' RT	6.5"	5.5"	12.0"

DNR LIAISON

DEPARTMENT OF NATURAL RESOURCES
ATTN: ERIC HEGGELUND
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
PHONE: 608-275-3301
eric.heggelund@wisconsin.gov

DESIGN CONSULTANT

CEDAR CORPORATION
ATTN: DAVE SAUER
2820 WALTON COMMONS WEST, SUITE 142
MADISON, WI 53718
PHONE: 608-354-0037
dave.sauer@cedarcorp.com

DIGGERS HOTLINE
Dial 811 or (800) 242-8511
www.DiggersHotline.com

** DENOTES UTILITIES THAT ARE NOT DIGGERS HOTLINE MEMBERS

UTILITIES

CITY OF BRODHEAD
PUBLIC WORKS
ATTN: RICH VOGEL
1500 11th STREET
BRODHEAD, WI 53520
PHONE: 608-897-4384
CELL: 608-931-3512
publicworks@cityofbrodheadwi.us

AMERICAN TRANSMISSION CO.
ENGINEERING
ATTN: JIM OLSON
5302 FENOAK DRIVE
MADISON, WI 53718
PHONE: 608-877-3622
jrolson@atcllc.com

WE ENERGIES
ATTN: DAN SANDE
DISTRICT SUPERVISOR
333 W EVERITT ST (A299)
MILWAUKEE, WI 53203
PHONE: 414-221-4578
dan.sande@we-energies.com

ALLIANT ENERGY
ATTN: STEVE LARSEN
1915 STH 69S
MONROE, WI 53566
PHONE: 608-328-5339
CELL: 608-751-7654
SteveLarsen@alliantenergy.com

BRODHEAD WATER & LIGHT
WATER
ATTN: PAT SULLIVAN
507 19th STREET
PO BOX 227
BRODHEAD, WI 53520
PHONE: 608-558-0598
psullivan@brodheadwl.com

BRODHEAD WATER & LIGHT
ELECTRIC
ATTN: TOM NIPPLE
507 19th STREET
PO BOX 227
BRODHEAD, WI 53520
PHONE: 608-558-9405
tnipple@brodheadwl.com

FRONTIER COMMUNICATIONS
ATTN: MICHAEL FABER
ENGINEER/CONSTRUCTION DETAILER
222 KENOSHA ST
WALWORTH, WI 53184
PHONE: 262-275-2113
CELL: 262-441-3529
michaeal.j.faber@ftr.com

CHARTER COMMUNICATIONS
ATTN: RANDY STEURER
CONSTRUCTION COORDINATOR
1348 PLAINFIELD AVE
JANESVILLE, WI 53545
PHONE: 608-373-7544
CELL: 608-209-3194
randy.steurer@chartercom.com

W 17th STREET

DESCRIPTION	STATION	NORTHING	EASTING
BEGIN ALIGNMENT	10+00.00	141656.46	679754.98
END ALIGNMENT	38+00.00	141971.74	682390.56

CURVE 1

PI STA 12+97.36
Y 141953.44
X 679769.95
DELTA= 86-41-11
T= 297.363
L= 476.701
R= 315.078
PC STA 10+00.00
Y 141656.46
X 679754.98
PT STA 14+76.70
Y 141955.56
X 680067.31
SE = NC

△ CP 510

BENCHMARKS

PT	LOCATION	NORTHING	EASTING	ELEV
1000	–	142015.00	682424.34	792.51
4226	–	141953.15	679832.84	794.05
10089	31+69.29, 30.55' RT	141937.74	681760.09	795.75

N

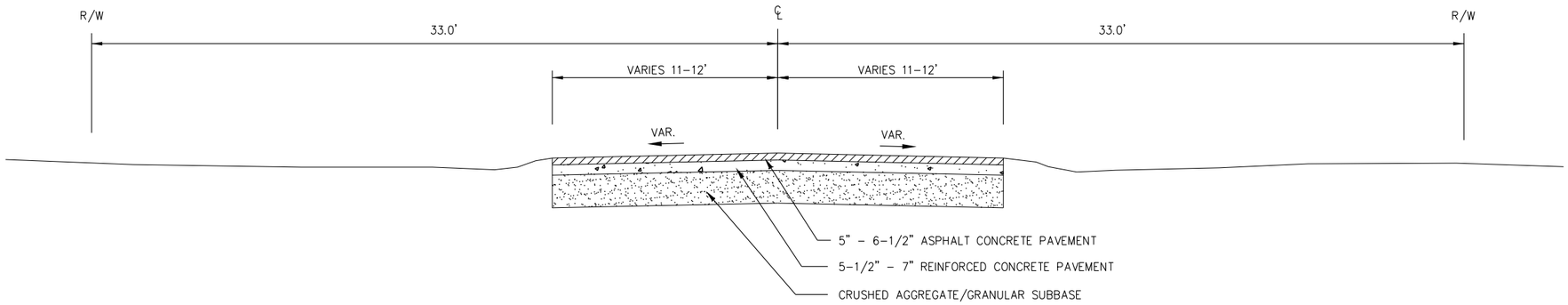
CONTROL

PT	LOCATION	NORTHING	EASTING	ELEV
100	34+97.27, 24.16' LT	141994.54	682097.58	790.45
101	31+66.91, 31.84' LT	142000.07	681757.22	792.44
102	28+76.05, 45.54' RT	141920.54	681466.96	792.48
510	–	141225.46	679785.89	794.31

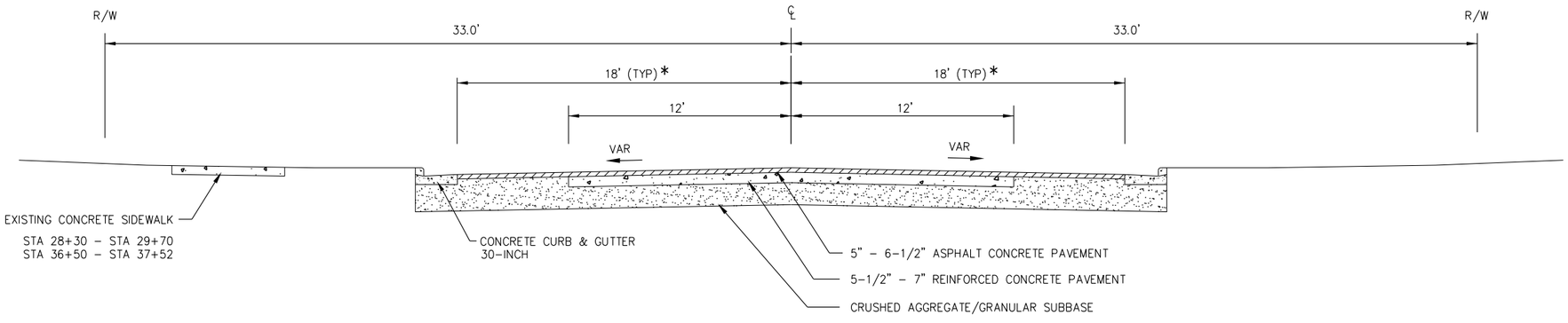
IRONS

PT	LOCATION	NORTHING	EASTING	ELEV
4412	21+86.94, 32.61' LT	141993.55	680777.29	792.62
9989	35+07.11, 32.88' RT	141937.78	682097.80	791.19
10027	34+41.07, 33.39' RT	141936.91	682031.89	791.50
10304	24+46.96, 28.27' RT	141934.63	681037.76	791.63
10342	21+83.13, 32.42' RT	141928.51	680773.98	791.47
10380	18+58.93, 33.01' RT	141925.50	680449.76	792.73
10383	17+86.97, 31.16' LT	141989.13	680377.35	792.59

2



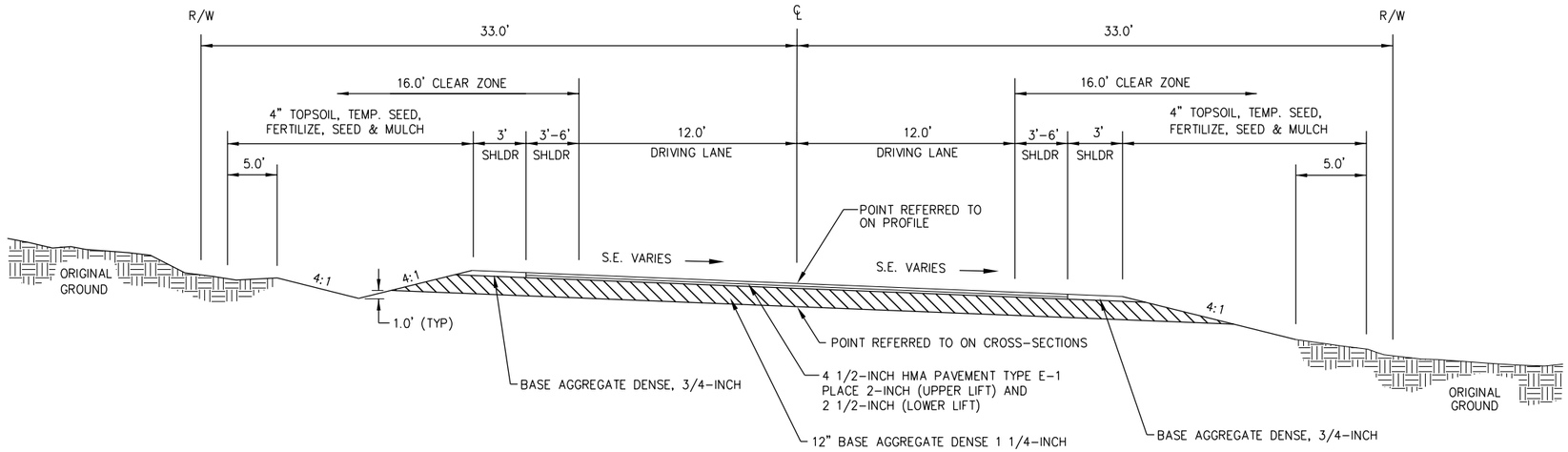
W 17th STREET
TYPICAL EXISTING SECTION
STA 14+00 – STA 25+00



*STA 28+00 – STA 31+50, 20' LANES

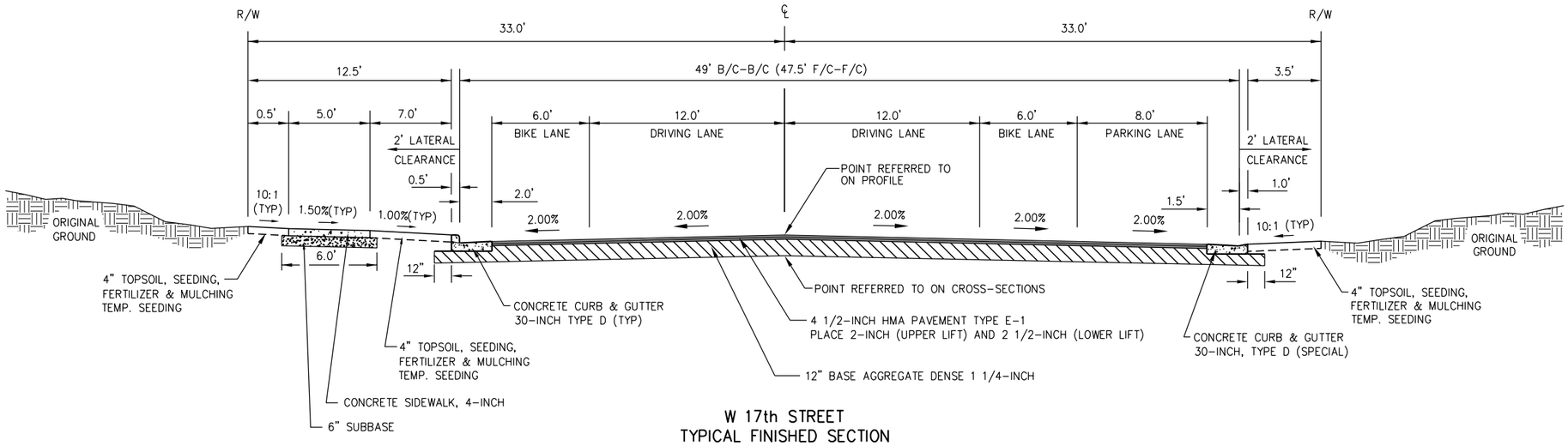
W 17th STREET
TYPICAL EXISTING SECTION
STA 25+00 – STA 37+52

PROJECT NO: 5911–00–70	HWY: LOCAL STREET	COUNTY: GREEN	TYPICAL SECTIONS	SHEET 4 E



W 17th STREET
TYPICAL FINISHED SECTION

STA 14+00 – STA 14+62

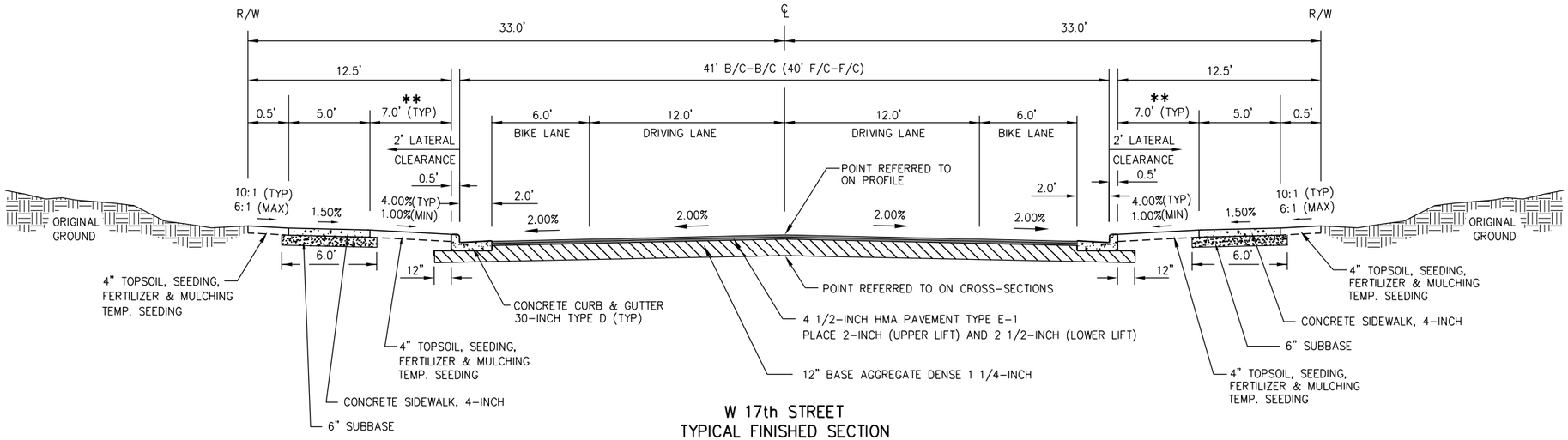


W 17th STREET
TYPICAL FINISHED SECTION

STA 14+62 – STA 18+00

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	TYPICAL SECTIONS	SHEET 5 E

2



W 17th STREET
TYPICAL FINISHED SECTION

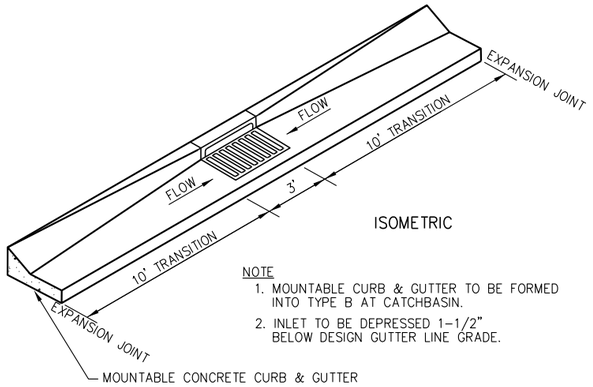
STA 18+00 – STA 37+52

** STA 18+50 – STA 21+50, LT
STA 25+00 – STA 26+00, RT
STA 26+10 – STA 28+00, LT
STA 31+50 – STA 33+00, RT
— 6.0' BOULEVARD

STA 36+55 – STA 37+52 RT — 2.5' BOULEVARD

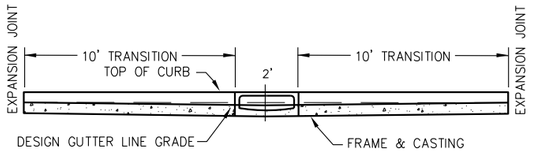
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	TYPICAL SECTIONS	SHEET 6 E

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\020301_TS_48930008.DWG PLOT DATE : 4/14/2014 9:39 AM PLOT BY : BRYON MOTSZKO PLOT NAME : PLOT SCALE : **********



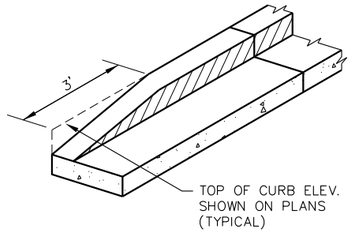
ISOMETRIC

NOTE
1. MOUNTABLE CURB & GUTTER TO BE FORMED INTO TYPE B AT CATCHBASIN.
2. INLET TO BE DEPRESSED 1–1/2" BELOW DESIGN GUTTER LINE GRADE.

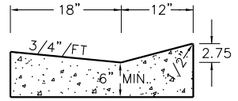


SECTION A–A

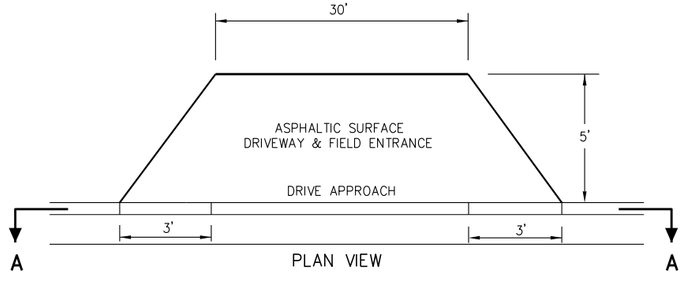
MOUNTABLE CURB & GUTTER CONSTRUCTION AT CATCH BASIN
NO SCALE



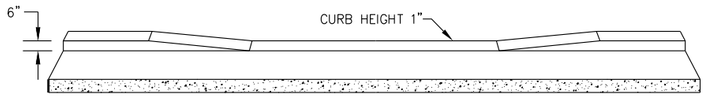
CURB & GUTTER END SECTION TAPER
NO SCALE



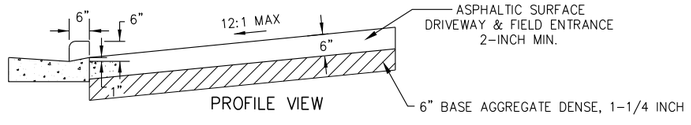
CONCRETE CURB & GUTTER 30–INCH, TYPE D (SPECIAL)
NO SCALE



PLAN VIEW

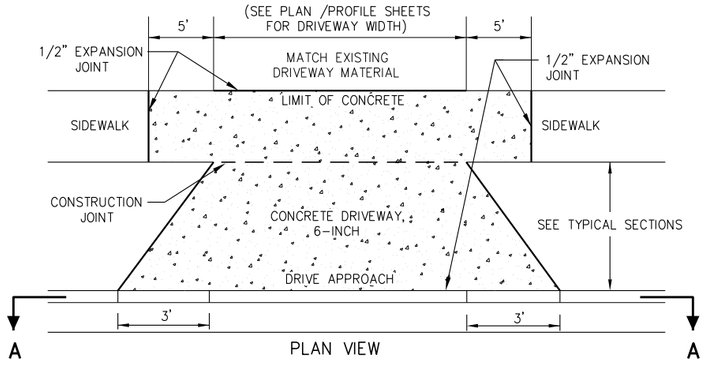


SECTION A–A

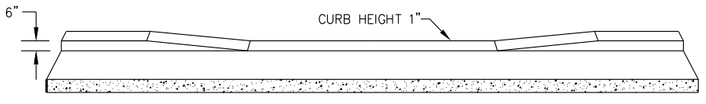


PROFILE VIEW

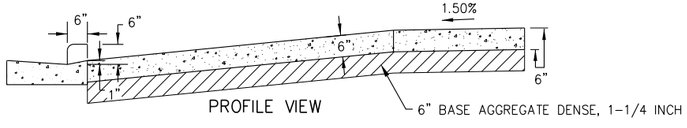
ASPHALTIC DRIVEWAY STA 14+88 RT
NO SCALE



PLAN VIEW



SECTION A–A

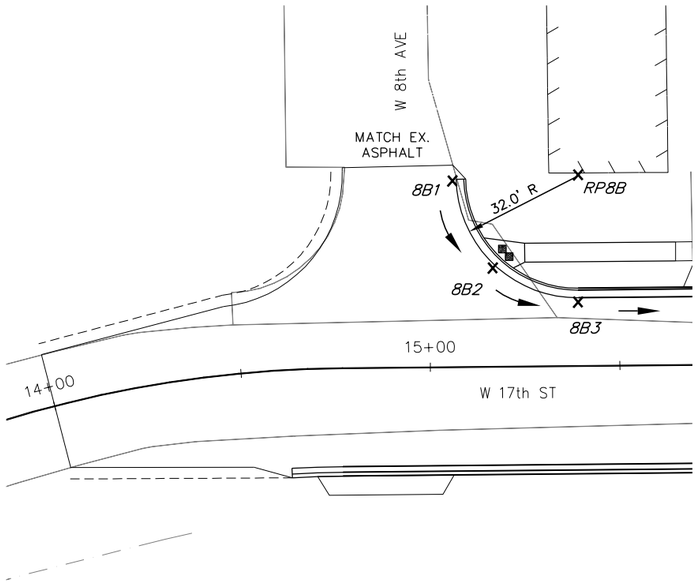


PROFILE VIEW

CONCRETE DRIVEWAY W/ SIDEWALK
NO SCALE

PROJECT NO: 5911–00–70	HWY: LOCAL STREET	COUNTY: GREEN	CONSTRUCTION DETAILS	SHEET 7 E

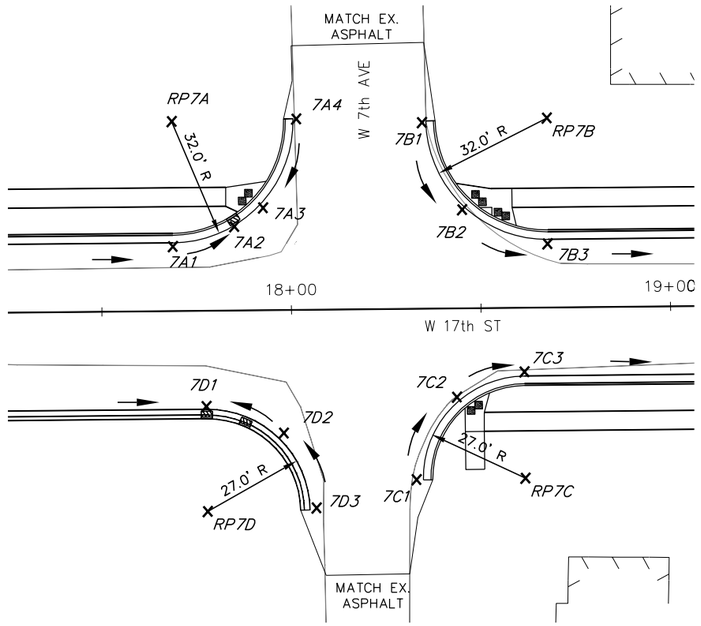
NOTE:
ALL CURB RAMPS ARE TYPE 2 UNLESS OTHERWISE NOTED.



W 8th AVE

NOTE:
ALL STATIONS, DISTANCES, COORD.S AND ELEV.S ARE TO FLAG OF CURB

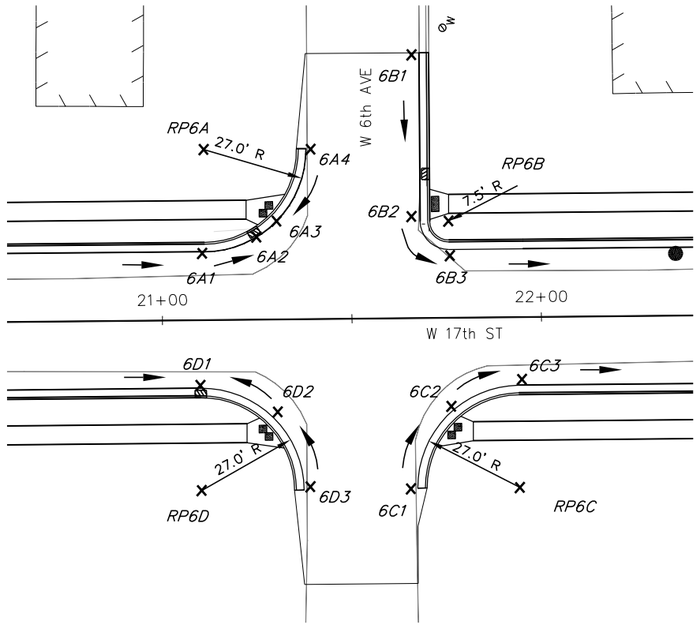
PT	STA	DIST.	Y	X	FLAG ELEV
RP8B	15+39.26	50.00' LT	142006.12	680129.49	(NA)
8B1	15+07.26	49.55' LT	142005.43	680097.50	794.65
8B2	15+16.79	27.21' LT	141983.17	680107.20	794.51
8B3	15+39.26	18.00' LT	141974.12	680129.73	794.37



W 7th AVE

NOTE:
ALL STATIONS, DISTANCES, COORD.S AND ELEV.S ARE TO FLAG OF CURB

PT	STA	DIST.	Y	X	FLAG ELEV
RP7A	17+68.84	50.00' LT	142007.83	680359.07	(NA)
7A1	17+68.84	18.00' LT	141975.83	680359.31	793.25
7A2	17+85.64	22.76' LT	141980.71	680376.05	793.15
7A3	17+91.63	27.53' LT	141985.54	680382.02	793.20
7A4	18+00.84	50.45' LT	142008.52	680391.06	793.30
RP7B	18+35.86	49.55' LT	142008.57	680458.08	(NA)
7B1	18+35.96	49.41' LT	142007.88	680426.09	793.28
7B2	18+45.39	27.21' LT	141985.62	680435.78	793.06
7B3	18+67.85	18.00' LT	141976.57	680458.32	792.85
RP7C	18+61.35	45.00' RT	141913.52	680452.29	(NA)
7C1	18+34.35	45.18' RT	141913.15	680425.29	793.45
7C2	18+42.20	25.97' RT	141932.41	680432.99	793.16
7C3	18+61.35	18.00' RT	141940.52	680452.09	792.88
RP7D	17+77.40	53.00' RT	141904.90	680368.40	(NA)
7D1	17+77.40	26.00' RT	141931.90	680368.20	793.05
7D2	17+96.43	33.85' RT	141924.20	680387.29	793.32
7D3	18+04.40	52.82' RT	141905.28	680395.40	793.60

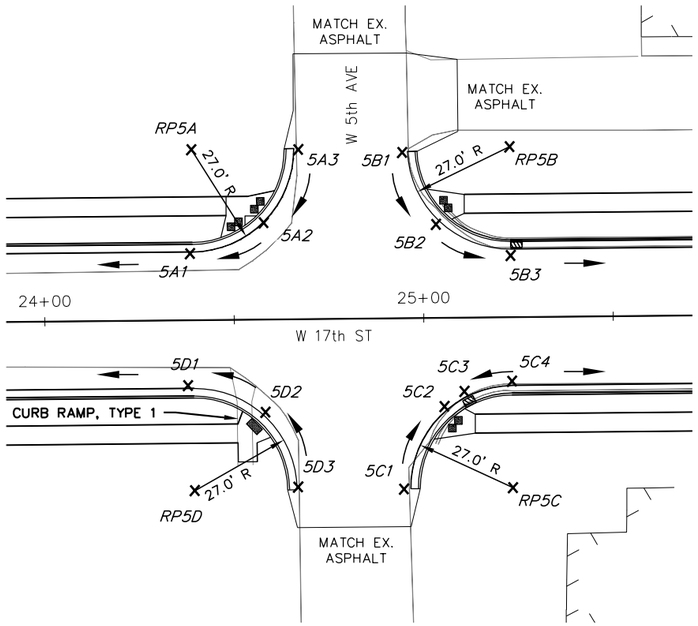


W 6th AVE

NOTE:
ALL STATIONS, DISTANCES, COORD.S AND ELEV.S ARE TO FLAG OF CURB

PT	STA	DIST.	Y	X	FLAG ELEV
RP6A	21+11.27	45.00' LT	142005.38	680701.52	(NA)
6A1	21+11.27	18.00' LT	141978.38	680701.72	791.89
6A2	21+25.01	21.76' LT	141982.24	680715.43	791.83
6A3	21+30.34	25.90' LT	141986.42	680720.74	792.00
6A4	21+38.27	44.96' LT	142005.55	680728.52	792.25
RP6B	21+75.74	25.50' LT	141986.36	680766.14	(NA)
6B1	21+68.30	70.00' LT	142030.81	680758.37	792.21
6B2	21+68.24	25.51' LT	141986.32	680758.64	791.72
6B3	21+75.74	18.00' LT	141978.86	680766.19	791.63
RP6C	21+93.93	45.00' RT	141916.00	680784.86	(NA)
6C1	21+66.93	44.88' RT	141915.92	680757.86	792.16
6C2	21+74.88	25.87' RT	141934.99	680765.66	791.86
6C3	21+93.93	18.00' RT	141943.00	680784.65	791.56
RP6D	21+09.93	45.00' RT	141915.38	680700.86	(NA)
6D1	21+09.93	18.00' RT	141942.37	680700.66	791.89
6D2	21+29.06	25.95' RT	141934.58	680719.85	792.02
6D3	21+36.93	45.12' RT	141915.46	680727.86	792.16

NOTE:
ALL CURB RAMPS ARE TYPE 2 UNLESS OTHERWISE NOTED.



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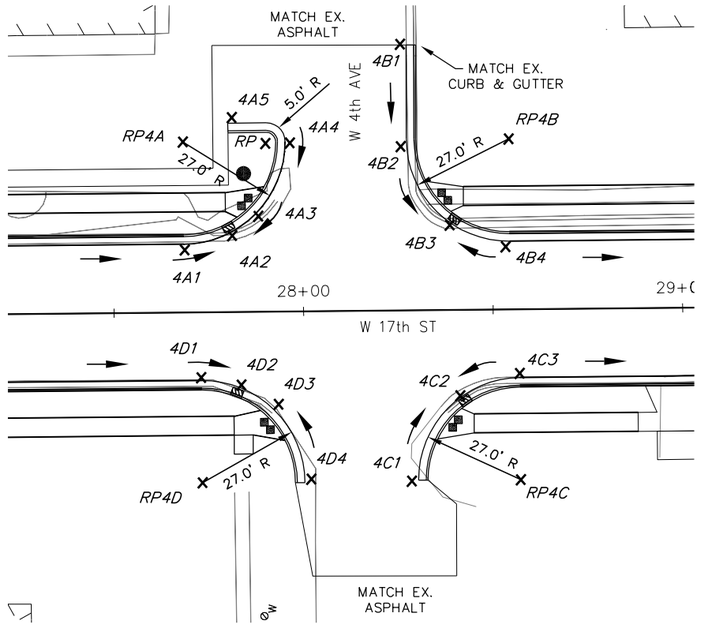

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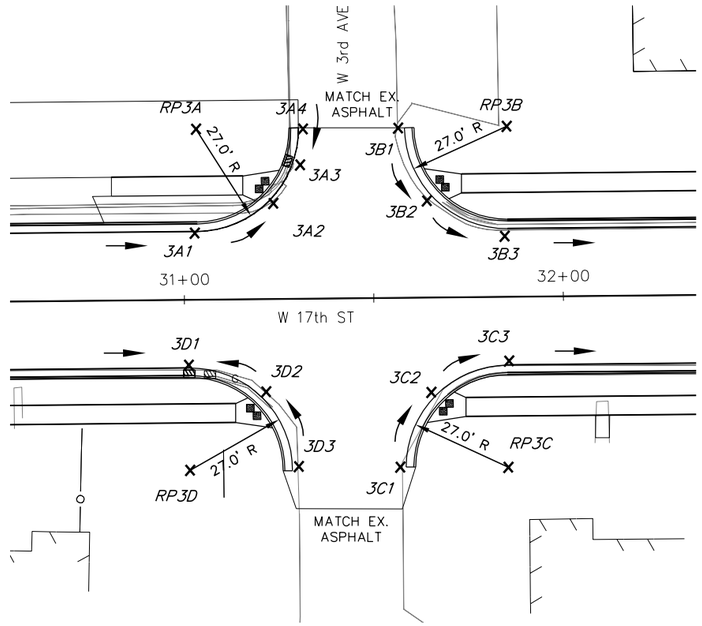


W 5th AVE

NOTE:
ALL STATIONS, DISTANCES, COORD.S AND ELEV.S ARE TO FLAG OF CURB

PT	STA	DIST.	Y	X	FLAG ELEV
RP5A	24+39.09	45.00' LT	142007.82	681029.33	(NA)
5A1	24+39.09	18.00' LT	141980.82	681029.53	792.12
5A2	24+58.19	25.92' LT	141988.89	681048.58	792.21
5A3	24+66.09	45.04' LT	142008.06	681056.33	792.30
RP5B	25+23.09	45.00' LT	142008.45	681113.33	(NA)
5B1	24+96.10	44.00' LT	142007.25	681086.36	792.25
5B2	25+04.35	25.56' LT	141988.87	681094.74	792.18
5B3	25+23.09	18.00' LT	141981.45	681113.53	792.10
RP5C	25+23.18	45.00' RT	141918.45	681114.09	(NA)
5C1	24+96.18	45.02' RT	141918.24	681087.09	792.12
5C2	25+04.08	25.91' RT	141937.40	681094.85	792.04
5C3	25+11.20	20.81' RT	141942.57	681101.96	792.00
5C4	25+23.18	18.00' RT	141945.45	681113.89	792.10
RP5D	24+39.18	45.00' RT	141917.83	681030.10	(NA)
5D1	24+39.18	18.00' RT	141944.83	681029.89	792.12
5D2	24+58.27	25.90' RT	141937.07	681049.04	792.20
5D3	24+66.18	44.98' RT	141918.04	681057.09	792.28

W 4th AVE

NOTE:
ALL STATIONS, DISTANCES, COORD.S AND ELEV.S ARE TO FLAG OF CURB

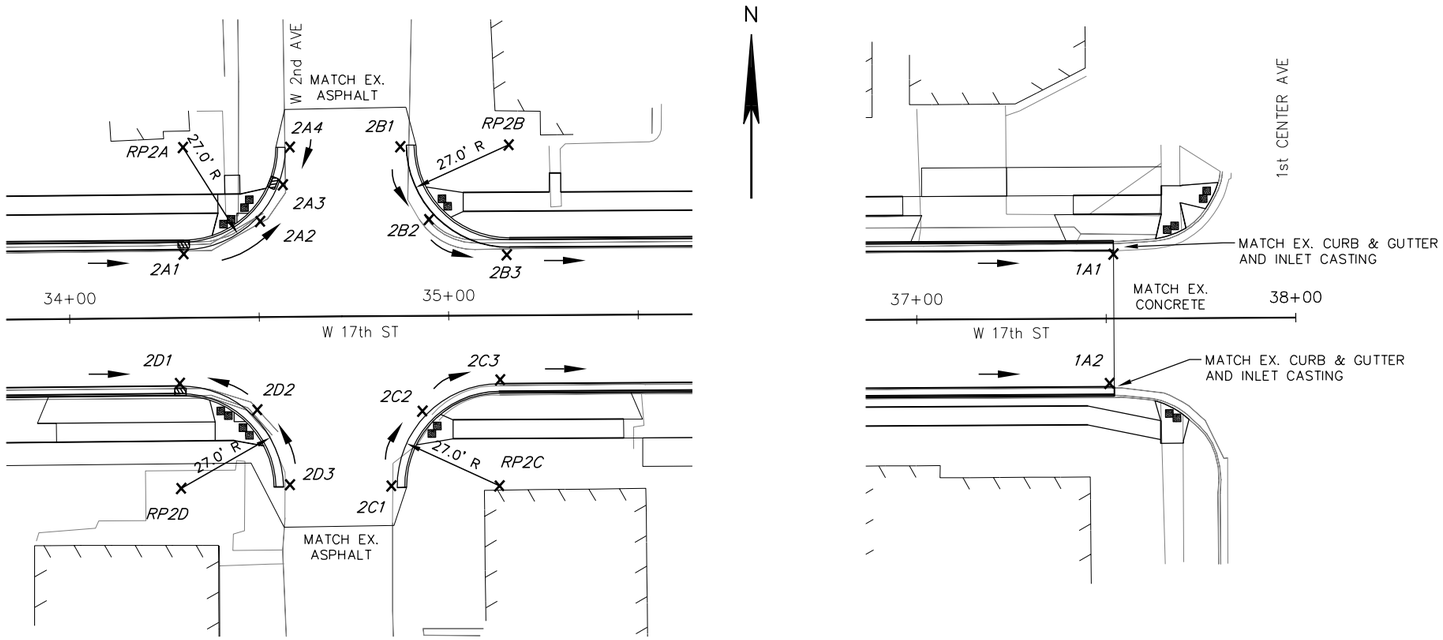
PT	STA	DIST.	Y	X	FLAG ELEV
RP4A	27+68.53	45.00' LT	142010.28	681358.77	(NA)
4A1	27+68.53	18.00' LT	141983.28	681358.97	790.99
4A2	27+81.09	21.10' LT	141986.49	681371.53	790.95
4A3	27+87.56	25.84' LT	141991.26	681377.94	791.09
4A4	27+95.53	44.81' LT	142010.29	681385.77	791.49
RP	27+90.53	44.85' LT	142010.29	681380.77	(NA)
4A5	27+80.46	49.82' LT	142015.19	681370.66	791.80
RP4B	28+54.53	45.00' LT	142010.92	681444.77	(NA)
4B1	28+27.70	70.00' LT	142035.72	681417.75	791.60
4B2	28+27.53	45.19' LT	142010.90	681417.77	791.31
4B3	28+39.04	22.88' LT	141991.75	681425.75	791.17
4B4	28+54.53	18.00' LT	141983.92	681444.97	791.24
RP4C	28+56.91	45.00' RT	141920.94	681447.82	(NA)
4C1	28+29.91	44.81' RT	141920.92	681420.82	791.53
4C2	28+41.93	22.54' RT	141943.11	681432.41	791.20
4C3	28+56.91	18.00' RT	141947.94	681447.62	791.25
RP5D	27+72.91	45.00' RT	141920.31	681363.82	(NA)
4D1	27+72.91	18.00' RT	141947.31	681363.62	790.97
4D2	27+82.79	19.87' RT	141945.50	681373.54	790.92
4D3	27+92.07	25.97' RT	141939.48	681382.83	791.12
4D4	27+99.91	45.19' RT	141920.33	681390.82	791.53

W 3rd AVE

NOTE:
ALL STATIONS, DISTANCES, COORD.S AND ELEV.S ARE TO FLAG OF CURB

PT	STA	DIST.	Y	X	FLAG ELEV
RP3A	31+03.50	45.00' LT	142012.77	681693.73	(NA)
3A1	31+03.50	18.00' LT	141985.77	681693.93	791.85
3A2	31+22.63	25.95' LT	141993.86	681713.00	791.58
3A3	31+29.02	35.89' LT	142003.88	681719.22	791.45
3A4	31+30.50	45.11' LT	142013.09	681720.72	791.50
RP3B	31+85.50	45.00' LT	142013.38	681775.72	(NA)
3B1	31+58.50	44.89' LT	142013.07	681748.73	791.75
3B2	31+66.45	25.87' LT	141994.11	681756.82	791.64
3B3	31+85.50	18.00' LT	141986.38	681775.93	791.52
RP3C	31+85.11	45.00' RT	141923.38	681776.01	(NA)
3C1	31+58.11	44.65' RT	141923.53	681749.01	792.00
3C2	31+66.14	25.79' RT	141942.45	681756.90	791.76
3C3	31+85.11	18.00' RT	141950.38	681775.81	791.52
RP3D	31+01.10	45.00' RT	141922.76	681692.00	(NA)
3D1	31+01.10	18.00' RT	141949.75	681691.80	791.86
3D2	31+20.32	26.03' RT	141941.87	681711.07	791.98
3D3	31+28.10	45.35' RT	141922.61	681719.00	792.10

NOTE:
ALL CURB RAMPS ARE TYPE 2 UNLESS OTHERWISE NOTED.



W 2nd AVE

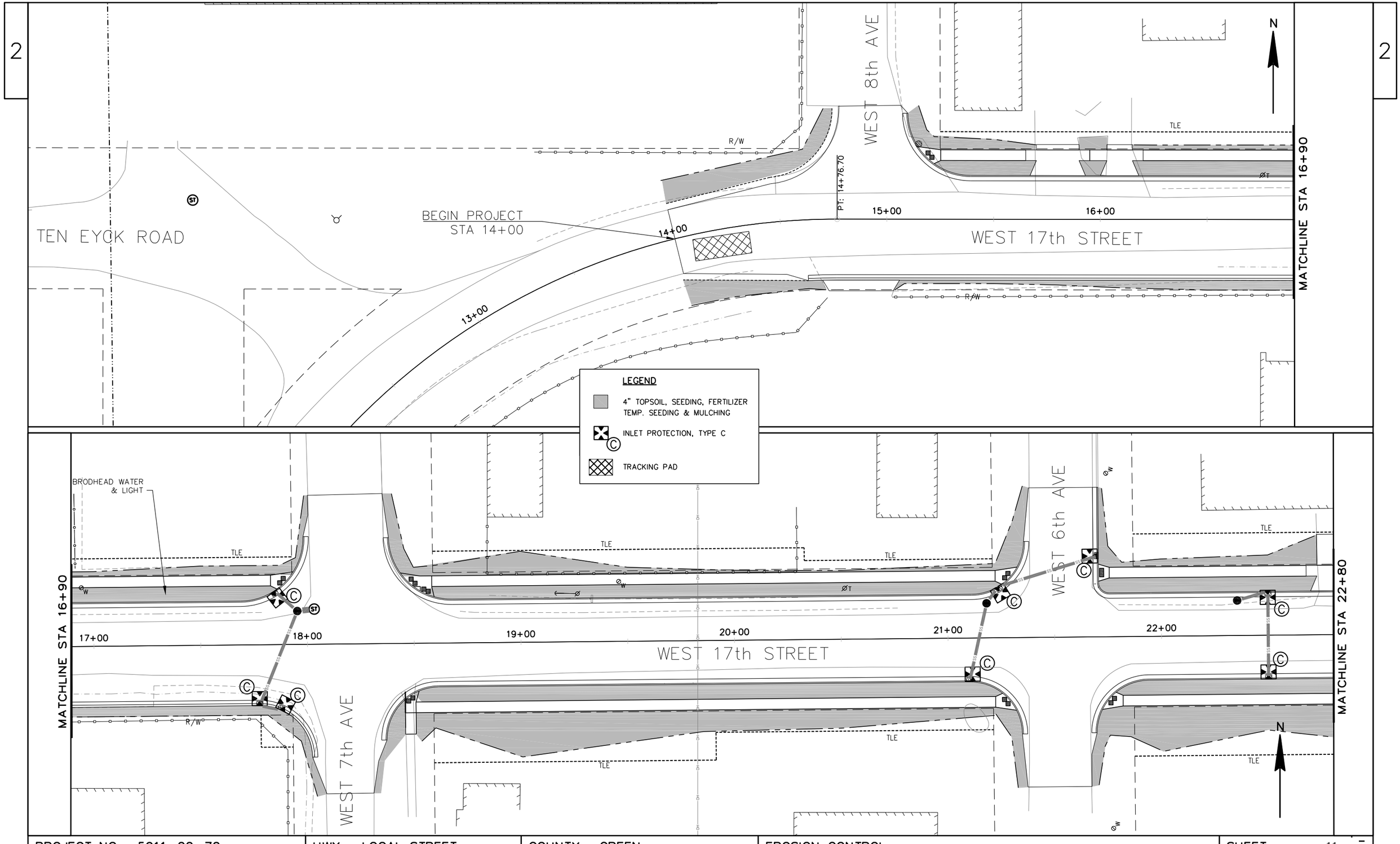
NOTE:
ALL STATIONS, DISTANCES, COORD.S AND ELEV.S ARE TO FLAG OF CURB

PT	STA	DIST.	Y	X	FLAG ELEV
RP2A	34+30.29	45.00' LT	142015.20	682020.51	(NA)
2A1	34+30.29	18.00' LT	141988.20	682020.71	790.47
2A2	34+49.30	25.83' LT	141996.18	682039.66	790.12
2A3	34+55.32	34.84' LT	142005.33	682045.64	789.94
2A4	34+57.29	44.78' LT	142015.18	682047.51	790.00
RP2B	35+16.12	45.00' LT	142015.68	682106.51	(NA)
2B1	34+89.12	45.12' LT	142015.70	682079.51	790.25
2B2	34+96.99	25.95' LT	141996.56	682087.44	790.17
2B2	35+16.12	18.00' LT	141988.68	682106.61	790.09
RP2C	35+13.16	45.00' RT	141925.67	682103.88	(NA)
2C1	34+86.16	45.25' RT	141925.32	682076.89	790.35
2C2	34+93.98	26.00' RT	141944.60	682084.63	790.23
2C3	35+13.16	18.00' RT	141952.67	682103.78	790.10
RP2D	34+28.99	45.00' RT	141925.20	682019.88	(NA)
2D1	34+28.99	18.00' RT	141952.20	682019.68	790.44
2D2	34+48.03	25.86' RT	141944.48	682038.78	790.52
2D3	34+55.99	44.86' RT	141925.54	682046.88	790.60

1st AVE

NOTE:
ALL STATIONS, DISTANCES, COORD.S AND ELEV.S ARE TO FLAG OF CURB

PT	STA	DIST.	Y	X	FLAG ELEV
1A1	37+52.00	18.00' LT	141989.56	682342.48	789.05
1A2	37+52.00	18.00' RT	141953.56	682342.62	789.05



PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	EROSION CONTROL	SHEET 11

2

MATCHLINE STA 22+80

WEST 5th AVE

WEST 4th AVE

N

23+00 24+00 25+00 26+00 27+00 28+00

WEST 17th STREET

TLE

MATCHLINE STA 28+70

LEGEND

4" TOPSOIL, SEEDING, FERTILIZER TEMP. SEEDING & MULCHING

INLET PROTECTION, TYPE C

MATCHLINE STA 28+70

WEST 3rd AVE

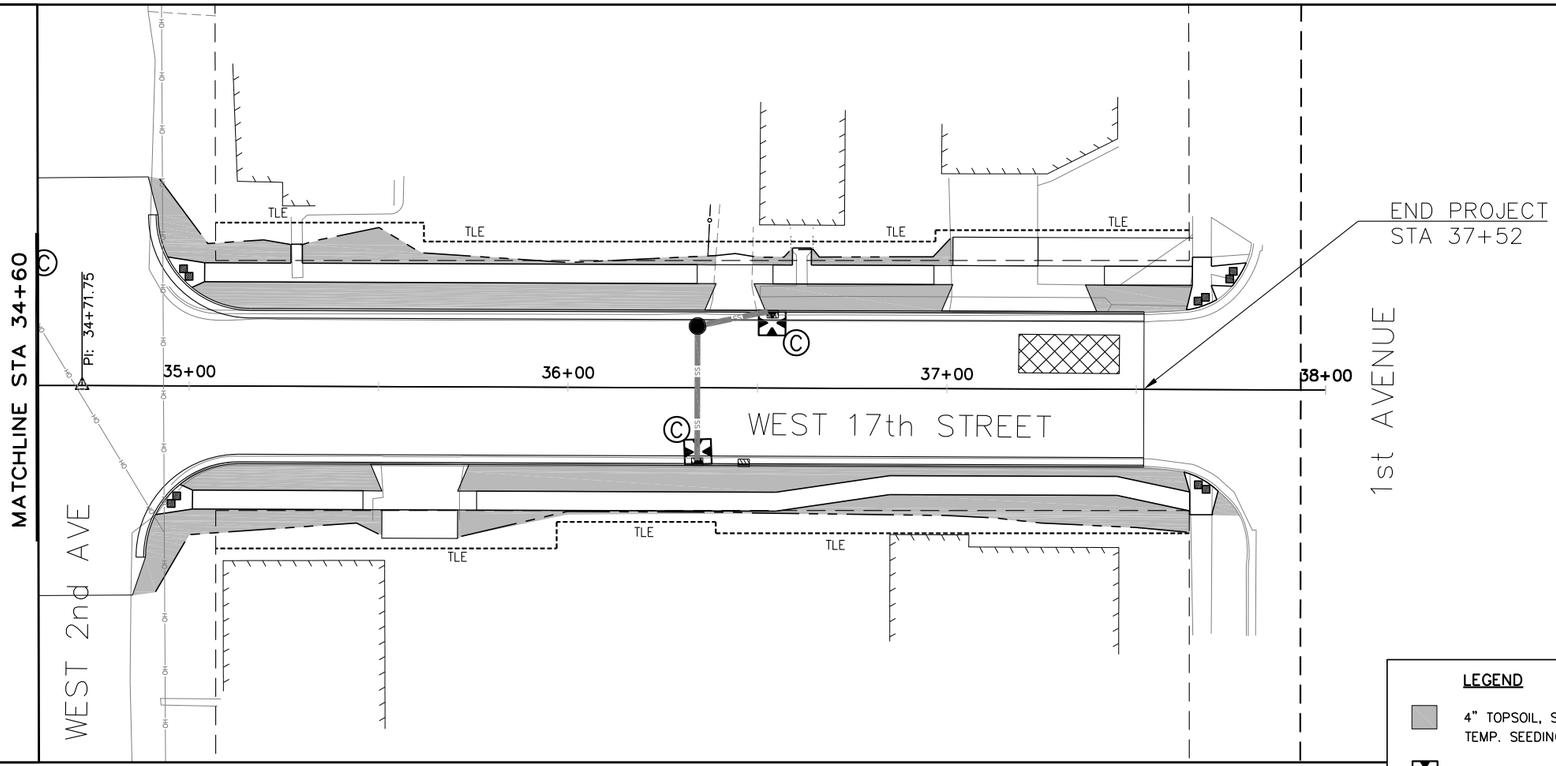
N

29+00 30+00 31+00 32+00 33+00 34+00

WEST 17th STREET

MATCHLINE STA 34+60

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	EROSION CONTROL	SHEET 12



LEGEND

- 4" TOPSOIL, SEEDING, FERTILIZER TEMP. SEEDING & MULCHING
- INLET PROTECTION, TYPE C
- TRACKING PAD

2

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	EROSION CONTROL	SHEET 13

N2929 CTH GG

BEGIN CONSTRUCTION
STA 14+00.0

STA 14+89, 24' LT
ADJUST UTILITY MANHOLE COVER
EXIST. SANITARY MH
EX RIM EL = 795.02
PROP RIM EL = 794.70

W 8th AVE

STA 15+15, 35' LT
REMOVE INLET &
ABANDON CULVERT PIPE

1507 17th ST

1505 17th ST

1504 17th ST

EX STM MH 1

TEN EYCK RD

W 17th ST

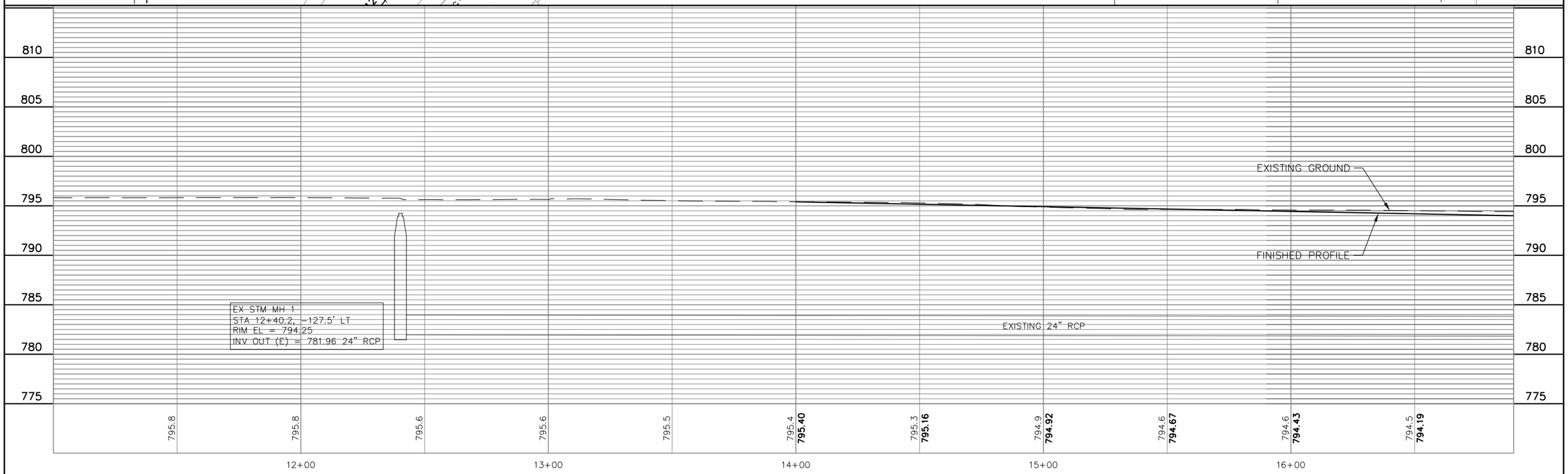
Pt: 14+76.70

EXISTING 24" RCP

CAUTION

MATCH LINE – STA 16+90

N



PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	STORM SEWER PLAN / UTILITY PLAN:	SHEET 14

2

1607 W 7th AVE | 1606 W 7th AVE | 1607 W 6th AVE | 1307 17th ST

W 7th AVE | W 6th AVE | W 17th ST

MATCH LINE – STA 16+90

MATCH LINE – STA 22+80

STA 17+85 LT
REMOVE STORM SEWER
(12-INCH)

EX 15" CMP

STM INL 2A

STM MH 2A

EX STM MH 2B

EXISTING 24" RCP

STA 18+26, 25' LT
ADJUST UTILITY MANHOLE COVER
EXIST. SANITARY MH
EX RIM EL = 793.92
PROP RIM EL = 793.20

EXISTING 21" RCP

EX 8" RCP

STM INL 3B

STM INL 3A

STM MH 3A

REMOVE STORM SEWER (12-INCH)

STA 21+70, 32' LT
REMOVE INLET

STM INL 3C

STM INL 3D

STM MH 3B

STM INL 3E

17+00 | 18+00 | 19+00 | 20+00 | 21+00 | 22+00

STM INL 2B

STM INL 2C

CAUTION

STA 21+50, 34' RT
ADJUST UTILITY MANHOLE COVER
EXIST. SANITARY MH
EX RIM EL = 793.92
PROP RIM EL = 793.20
1 – MANHOLE COVER, TYPE J SPECIAL SANITARY

1504 17th ST | 1702 W 7th AVE | 1402 17th ST | 1708 W 6th AVE

N

NOTE:
ALL STATION / OFFSETS TO INLETS ARE TO BACK OF CURB.

STM INL 2A
STA 17+84.3, –24.9' LT
RIM EL = 793.02
INV SE = 789.02 12" RCP

STM MH 2A
STA 17+95.4, –15.6' LT
RIM EL = 793.31
INV W = 781.80 24" RCP
INV E = 781.80 24" RCP
INV S = 785.09 12" RCP
INV NW = 785.09 12" RCP

STM INL 2B
STA 17+77.4, 28.5' RT
RIM EL = 792.93
INV E = 789.03 12" RCP
INV N = 788.93 12" RCP

STM INL 2C
STA 17+87.2, 30.5' RT
RIM EL = 793.07
INV W = 789.23 12" RCP

EX STM MH 2B
STA 18+03.3, –16.5' LT
PROP RIM EL = 793.26
EX RIM EL = 793.53
INV IN (N) = 786.79 15" CMP
RECONSTRUCT MANHOLE
INV IN (E) = 781.90 21" RCP
INV OUT (W) = 781.90 24" RCP

STM INL 3A
STA 21+09.9, 20.5' RT
RIM EL = 791.77
INV N = 787.77 12" RCP

STM INL 3B
STA 21+23.7, –23.9' LT
RIM EL = 791.71
INV E = 787.25 12" RCP
INV SW = 787.15 12" RCP

STM INL 3C
STA 21+70.8, –38.0' LT
RIM EL = 791.74
INV W = 787.74 12" RCP

STM MH 3A
STA 21+18.2, –16.2' LT
RIM EL = 791.90
INV N = 784.38 8" RCP
INV E = 782.77 21" RCP
INV S = 782.97 12" RCP
INV NE = 786.50 12" RCP
INV W = 782.59 21" RCP

STM MH 3B
STA 22+35.6, –16.4' LT
RIM EL = 791.43
INV E = 787.84 12" RCP
INV E = 782.81 21" RCP
INV W = 782.81 21" RCP

STM INL 3D
STA 22+50.0, –20.5' LT
RIM EL = 791.22
INV S = 788.02 12" RCP
INV W = 787.92 12" RCP

STM INL 3E
STA 22+50.0, 20.5' RT
RIM EL = 791.22
INV N = 788.22 12" RCP

EXISTING GROUND

FINISHED PROFILE

10' – 12" RCP @ 2.00%

12' – 12" RCP @ 31.52%

8' – 24" RCP @ 1.25%

48' – 12" RCP @ 8.06%

38' – 12" RCP @ 12.76%

49' – 12" RCP @ 1.00%

9' – 12" RCP @ 6.85%

15' – 12" RCP @ 0.50%

41' – 12" RCP @ 0.50%

EXISTING 24" RCP

EXISTING 24" RCP

EXISTING 21" RCP

EX. 21" RCP

805 | 800 | 795 | 790 | 785 | 780 | 775 | 770

794.4	794.4	794.2	794.1	794.0	793.9	793.7	793.5	793.3	793.3
793.95	**793.71**	**793.48**	**793.28**	**793.09**	**792.89**	**792.69**	**792.49**	**792.29**	**792.10**	**791.90**	**791.70**

17+00 | 18+00 | 19+00 | 20+00 | 21+00 | 22+00

PROJECT NO: 5911–00–70	HWY: LOCAL STREET	COUNTY: GREEN	STORM SEWER PLAN / UTILITY PLAN:	SHEET 15

2

MATCH LINE – STA 22+80

MATCH LINE – STA 28+70

W 5th AVE

W 4th AVE

W 17th ST

1208 16th ST

1201 17th ST

1708 W 6th AVE

1208 17th ST

1202 17th ST

TLE

R/W

SS

G

FO

OE

SA

SAN

W

OH

EX 12" RCP

EX 18" RCP

EXISTING 21" RCP

STM MH 4A

EX STM MH 4B

STM MH 5

STM INL 4

STM INL 5A

STM INL 5B

STM INL 5C

STM INL 5D

STM INL 5E

REMOVE STORM SEWER (12-INCH)

REMOVE STORM SEWER (18-INCH)

STA 25+24, 20' RT
REMOVE INLET

STA 27+88, 24' RT
STA 28+30, 30' LT
REMOVE INLET

STA 27+99, 26' RT
REMOVE MANHOLE

STA 28+12, 30' LT
ADJUST UTILITY MANHOLE COVER
EXIST. SANITARY MH
EX RIM EL = 791.82
PROP RIM EL = 791.35

23+00 24+00 25+00 26+00 27+00 28+00

N

NOTE:
ALL STATION / OFFSETS TO INLETS ARE TO BACK OF CURB.

STM INL 4
STA 25+12.3, 23.0' RT
RIM EL = 791.88
INV N = 787.88 12" RCP

STM MH 4A
STA 24+60.7, −16.8' LT
RIM EL = 792.23
INV E = 783.11 21" RCP
INV W = 782.88 21" RCP
INV N = 784.53 12" RCP
INV S = 783.21 18" RCP

EX STM MH 4B
STA 25+24.7, −20.3' LT
RIM EL = 791.98
EX RIM EL = 791.80
PROP RIM EL = 791.98
INV E = 783.20 21" RCP
INV S = 784.27 12" RCP
INV W = 783.19 21" RCP

STM INL 5E
STA 27+84.5, −36.5' LT
RIM EL = 791.00
INV S = 788.00 12" RCP

STM INL 5A
STA 27+79.9, −23.3' LT
RIM EL = 790.82
INV E = 787.80 12" RCP
INV N = 787.90 12" RCP

STM INL 5B
STA 27+81.9, 22.2' RT
RIM EL = 790.80
INV N = 786.80 12" RCP

STM INL 5C
STA 28+43.0, 24.8' RT
RIM EL = 791.08
INV N = 787.08 12" RCP

STM INL 5D
STA 28+40.7, −24.8' LT
RIM EL = 791.04
INV W = 786.73 12" RCP
INV S = 786.83 12" RCP

STM MH 5
STA 27+92.2, −18.7' LT
RIM EL = 790.98
INV E = 783.31 21" RCP
INV W = 787.50 12" RCP
INV S = 786.38 12" RCP
INV W = 783.44 21" RCP
INV E = 786.48 12" RCP

EXISTING GROUND

FINISHED PROFILE

64' – 21" RCP @ 0.13%

45' – 12" RCP @ 8.00%

268' – 21" RCP @ 0.09%

13' – 12" RCP @ 2.29%

14' – 12" RCP @ −0.70%

49' – 12" RCP @ 0.50%

50' – 12" RCP @ −0.50%

42' – 12" RCP @ 1.00%

805 800 795 790 785 780 775 770

793.2 **791.91** | 793.2 **792.11** | 793.0 **792.32** | 792.9 **792.52** | 792.9 **792.56** | 792.8 **792.34** | 792.7 **792.11** | 792.6 **791.89** | 792.5 **791.66** | 792.4 **791.44** | 792.6 **791.38** | 792.7 **791.58**

23+00 24+00 25+00 26+00 27+00 28+00

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	STORM SEWER PLAN / UTILITY PLAN:	SHEET 16

2

1107 17th ST

1607 W 3rd AVE

1605 17th ST

1607 W 2nd AVE

N

W 3rd AVE

W 2nd AVE

EX 12" RCP

STA 31+21, 27' LT
REMOVE INLET

REMOVE STORM SEWER (12-INCH)

STM INL 6B

STM MH 6

EXISTING 21" RCP

STA 31+21, 20' LT
REMOVE INLET

STM INL 7B

STM INL 7C

STM MH 7

EXISTING 21" RCP

MATCH LINE – STA 28+70

MATCH LINE – STA 34+60

29+00

30+00

31+00

32+00

33+00

34+00

W 17th ST

W 17th ST

REMOVE STORM SEWER (12-INCH)

STM INL 6A

REMOVE STORM SEWER (12-INCH)

STM INL 7A

STA 31+07, 20' RT
REMOVE INLET

HYDRANT TO BE
RELOCATED BY OTHERS

STA 31+43, 21' RT
ADJUST UTILITY MANHOLE COVER
EXIST. SANITARY MH
EX RIM EL = 792.22
PROP RIM EL = 791.80

STA 34+36, 20' RT
REMOVE INLET

1104 17th ST

1102 17th ST

1008 17th ST

1002 W 2nd AVE

NOTE:
ALL STATION / OFFSETS TO INLETS ARE TO BACK OF CURB.

STM INL 6A
STA 31+01.1, 20.5' RT
RIM EL = 791.74
INV NE = 787.74 12" RCP

STM INL 6B
STA 31+26.6, -36.8' LT
RIM EL = 791.32
INV S = 788.52 12" RCP

STM INL 7B
STA 34+53.0, -35.8' LT
RIM EL = 789.52
INV S = 786.82 12" RCP

STM INL 7C
STA 34+30.3, -20.5' LT
RIM EL = 790.34
INV E = 787.34 12" RCP

EXISTING GROUND

FINISHED PROFILE

329' – 21" RCP @ 0.27%

44' – 12" RCP @ 5.00%

19' – 12" RCP @ 2.00%

329' – 21" RCP @ 0.06%

21' – 12" RCP @ 1.64%

45' – 12" RCP @ 1.93%

18' – 12" RCP @ 4.13%

STM MH 6
STA 31+21.6, -18.4' LT
RIM EL = 791.76
INV N = 784.81 12" RCP
INV E = 784.26 21" RCP
INV N = 788.14 12" RCP
INV SW = 785.54 12" RCP
INV W = 784.20 21" RCP

STM INL 7A
STA 34+29.0, 20.5' RT
RIM EL = 790.30
INV NE = 787.30 12" RCP

STM MH 7
STA 34+50.9, -18.3' LT
RIM EL = 791.08
INV E = 784.30 21" RCP
INV SW = 786.44 12" RCP
INV N = 786.09 12" RCP
INV W = 787.00 12" RCP
INV W = 784.46 21" RCP

805 800 795 790 785 780 775 770

792.8 **791.78** | 792.7 **791.98** | 792.8 **792.18** | 792.7 **792.38** | 792.6 **792.22** | 792.5 **792.02** | 792.4 **791.82** | 792.2 **791.62** | 791.9 **791.40** | 791.5 **791.18** | 791.1 **790.96** | 790.9 **790.74**

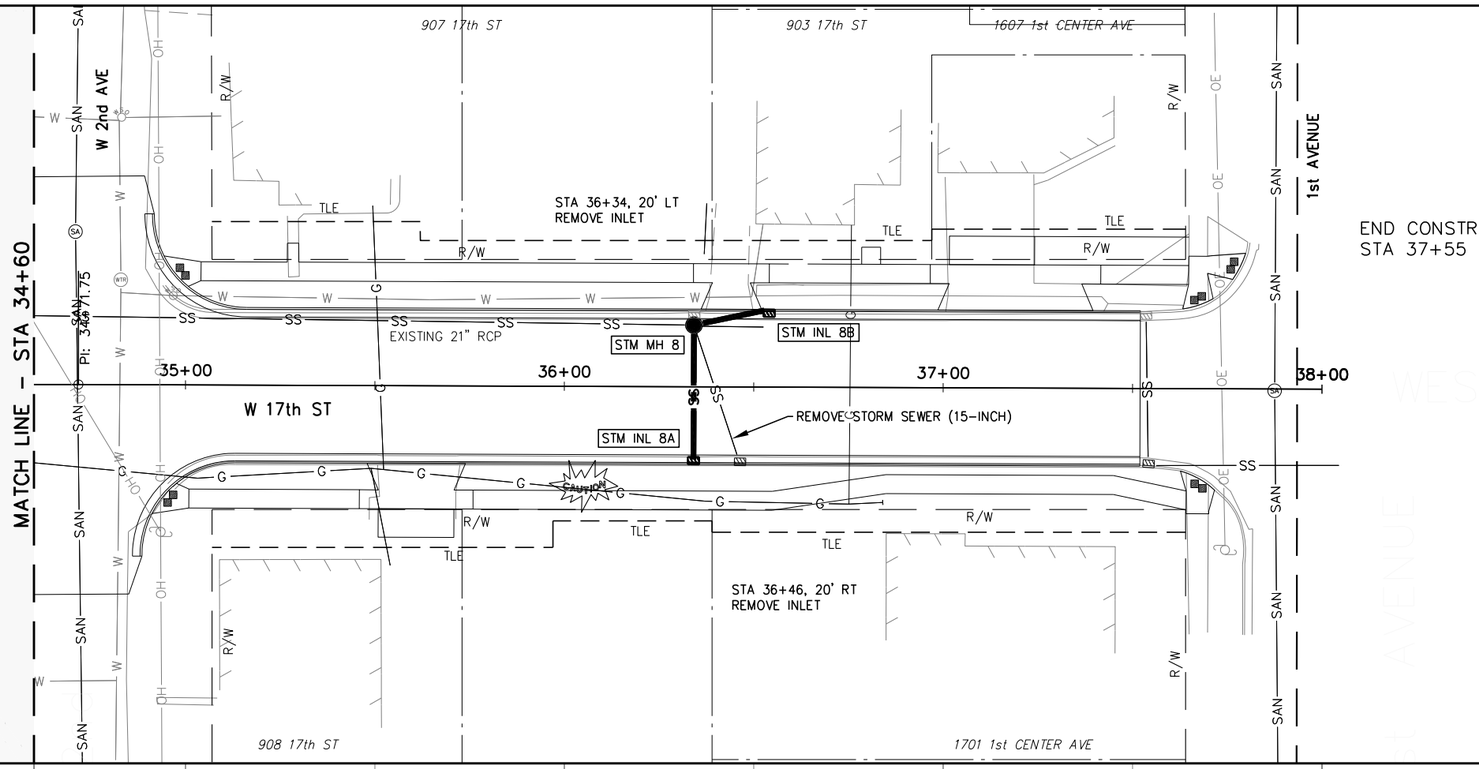
29+00 30+00 31+00 32+00 33+00 34+00

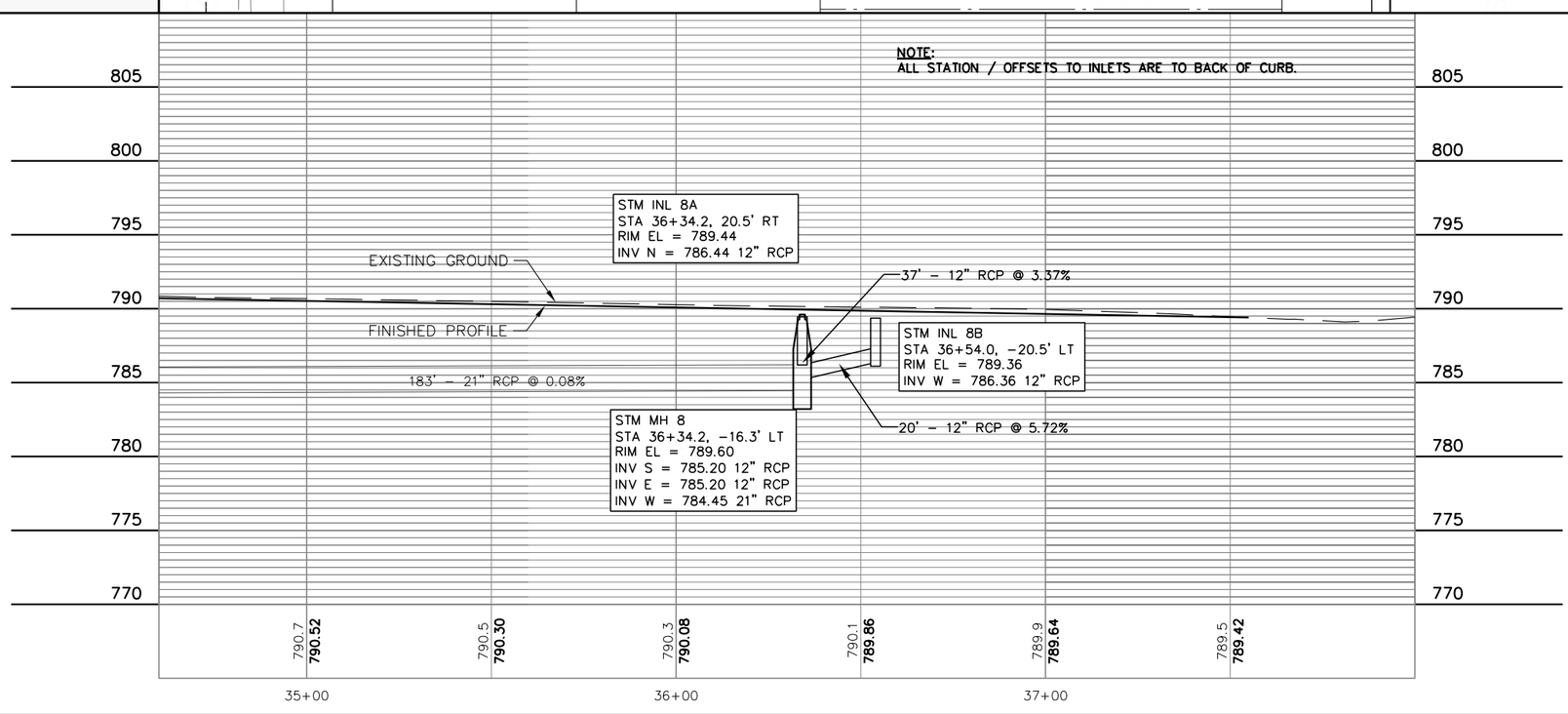
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	STORM SEWER PLAN / UTILITY PLAN:	SHEET 17

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\022501_SS_48930008.DWG PLOT DATE : 1/27/2014 9:13 AM PLOT BY : KATHRYN BLASER **PLOT NAME :** PLOT SCALE : 1 IN:20 FT

STA 34+71, 40' LT
ADJUST UTILITY MANHOLE COVER
EXIST. SANITARY MH
EX RIM EL = 790.61
PROP RIM EL = 790.40

STA 34+83, 28' LT
ADJUST UTILITY MANHOLE COVER
EXIST. WATER MAIN MH
EX RIM EL = 790.30
PROP RIM EL = 790.25





PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	STORM SEWER PLAN / UTILITY PLAN:	SHEET 18

2

N

WEST 6th AVE

17+00 18+00 19+00 20+00 21+00 22+00

W 17th STREET

WEST 7th AVE

MATCHLINE STA 22+80

RM 1

CS 1

AB 1

SH 1

RO 1

PLANT LIST

KEY	COMMON NAME
NH	ELM, NEW HORIZON
SH	HONEY LOCUST, SKYLINE
AB	MAPLE, AUTUMN BLAZE
RM	MAPLE, RED
RO	OAK, RED
SS	CRAB, SPRING SNOW
JT	LILAC, JAPENESE TREE
CS	PEAR CLEVELAND SELECT

LEGEND

DECIDUOUS TREE LOCATION

RO 1 — PLANT KEY / QUANTITY

N

WEST 5th AVE

WEST 4th AVE

23+00 24+00 25+00 26+00 27+00 28+00

W 17th STREET

MATCHLINE STA 22+80

MATCHLINE STA 28+70

JT 1

AB 1

SH 1

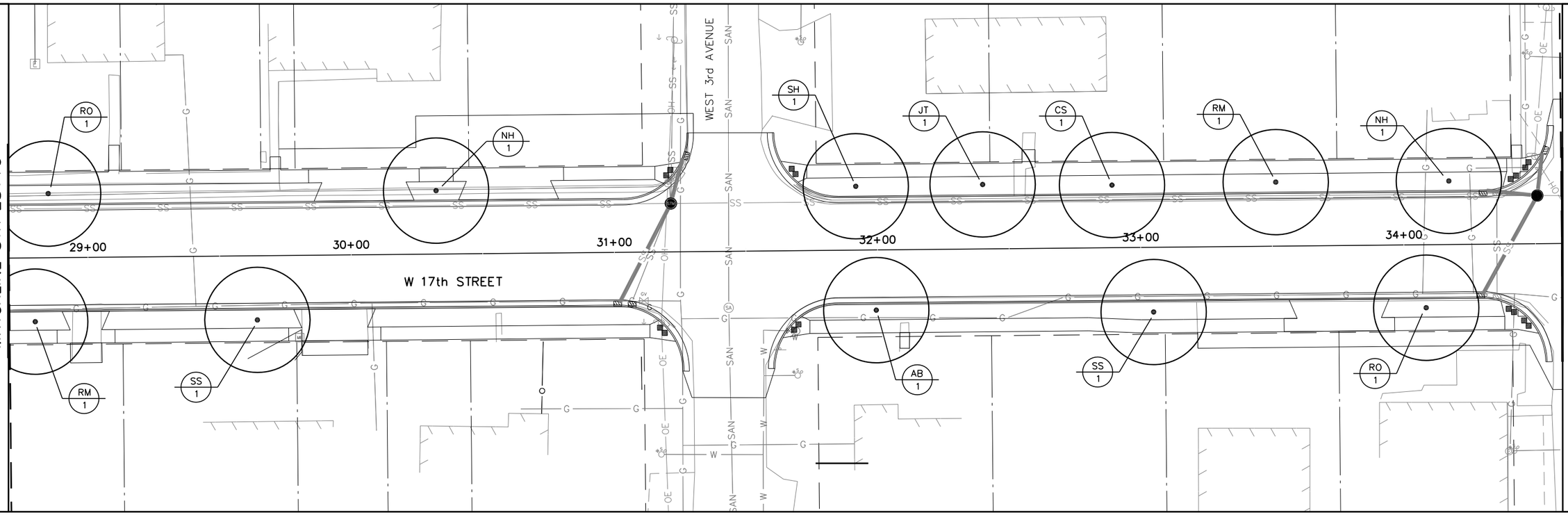
NH 1

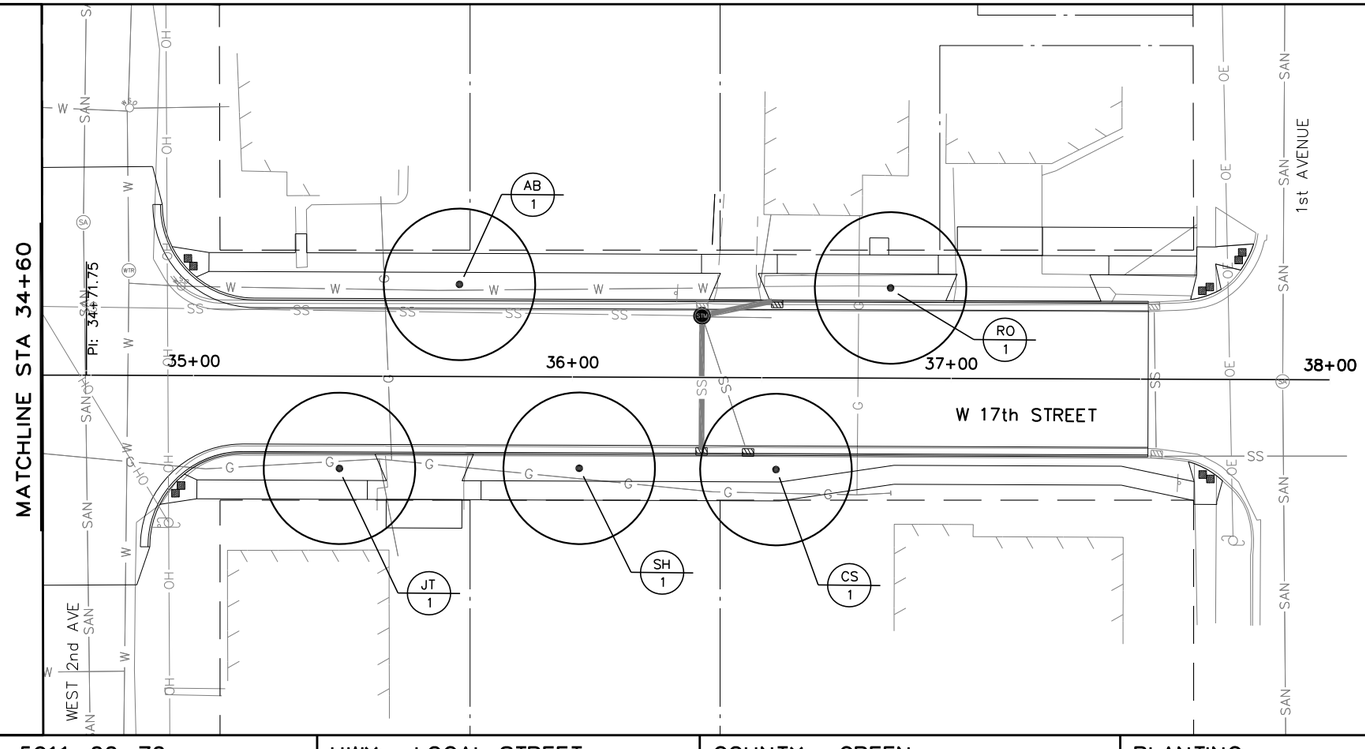
CS 1

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	PLANTING	SHEET 19

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\023501_LS_48930008.DWG PLOT DATE : 1/28/2014 2:06 PM PLOT BY : KATHRYN BLASER PLOT NAME : PLOT SCALE : ########

WISDOT/CADDS SHEET 42





N

PLANT LIST

KEY	COMMON NAME
NH	ELM, NEW HORIZON
SH	HONEY LOCUST, SKYLINE
AB	MAPLE, AUTUMN BLAZE
RM	MAPLE, RED
RO	OAK, RED
SS	CRAB, SPRING SNOW
JT	LILAC, JAPENESE TREE
CS	PEAR CLEVELAND SELECT

LEGEND

DECIDUOUS TREE LOCATION

RO 1 — PLANT KEY / QUANTITY

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	PLANTING	SHEET 20

PLANT DATA CHART

SYM.	COMMON NAME	SCIENTIFIC NAME	TYPE	AVE. MATURE HEIGHT	SIZE (CAL. OR HT. WHEN PLANTED	ROOT ZONE MODE	MINIMUM SIZE BALL/POT DIAM.	BALL/POT DEPTH	ROOT SPREAD	PLANT HOLE DIAM.	PLANT HOLE DEPTH	BRACE OR GUY	FERT. UNITS REQ,D.	RODENT PROTCT. REQ,D.	MULCH RING REQ,D.	PLT BED DLT
	LARGE DECIDUOUS TREES															
NH	ELM, NEW HORIZON	ULMUS CARPINIFOLIA 'NEW HORIZON'	2	50'	2"	B&B	24"	16"	–	42"	16"	NO	4	NO	48"	–
SH	HONEYLOCUST, SKYLINE	GLEDITSIA TRICANTHOS 'SKYCOLE'	1	50'	2"	B&B	24"	16"	–	42"	16"	NO	4	NO	48"	–
AB	MAPLE, AUTUMN BLAZE	ACER x FREEMANII 'JEFFERSRED'	1	60'	2"	B&B	24"	16"	–	42"	16"	NO	4	NO	48"	–
RM	MAPLE, RED	ACER RUBRUM	1	60'	2"	B&B	24"	16"	–	42"	16"	NO	4	NO	48"	–
RO	OAK, RED	QUERCUS RUBRUM	1	75'	2"	B&B	24"	16"	–	42"	16"	NO	4	NO	48"	–
	MEDUIM DECIDUOUS TREES															
SS	CRAB, SPRING SNOW	MALUS 'SPRING SNOW'	3	30'	1-1/2"	B&B	20"	14"	–	36"	14"	NO	3	NO	42"	–
JT	LILAC, JAPANESE TREE	SYRINGA RETICULATA	3	25'	1-1/2"	B&B	20"	14"	–	36"	14"	NO	3	NO	42"	–
CS	PEAR, CLEVELAND SELECT	PYRUS CALLERYANA 'CLEVELAND SELECT'	2	35'	1-1/2"	B&B	20"	14"	–	36"	14"	NO	3	NO	42"	–

EXISTING SIGN ON WOOD POST
PROPOSED SIGN ON WOOD POST
(X) DESIGNATES SIGN ITEM NUMBER

STOP R1-1 REMOVE
STOP R1-1
1

WEST 8th AVE

18" STOP LINE, WHITE

PI: 14+76.70

4" CENTERLINE, DOUBLE YELLOW

TEN EYCK ROAD

14+00
15+00
16+00
13+00

4" EDGELINE, WHITE

W 17th STREET

MATCHLINE STA 16+90

N

2

MATCHLINE STA 16+90

NO PARKING ANY TIME R7-1
6

STOP R1-1 REMOVE
STOP R1-1
1

18" STOP LINE, WHITE

S2-1 REMOVE
S1-1
2

W1-1L
SPEED LIMIT 25
R2-1 REMOVE
3
4

W1-1L
SPEED LIMIT 25
R2-1

NO PARKING ANY TIME R7-1
6

18" STOP LINE, WHITE

STOP R1-1 REMOVE
STOP R1-1
1

WEST 6th AVE

NO PARKING ANY TIME R7-1
6

N

17+00
18+00
19+00
20+00
21+00
22+00

4" EDGELINE, WHITE

4" CENTERLINE, DOUBLE YELLOW

CROSSWALK EPOXY 6-INCH, WHITE (TYP)

W 17th STREET

CROSSWALK EPOXY 6-INCH, WHITE (TYP)

18" STOP LINE, WHITE

STA 17+75, LT & RT
END EDGELINE MARKING

S1-1
2

WEST 7th AVE

STOP R1-1
STOP R1-1 REMOVE
1

NO PARKING ANY TIME R7-1
6

NO PARKING ANY TIME R7-1
6

18" STOP LINE, WHITE

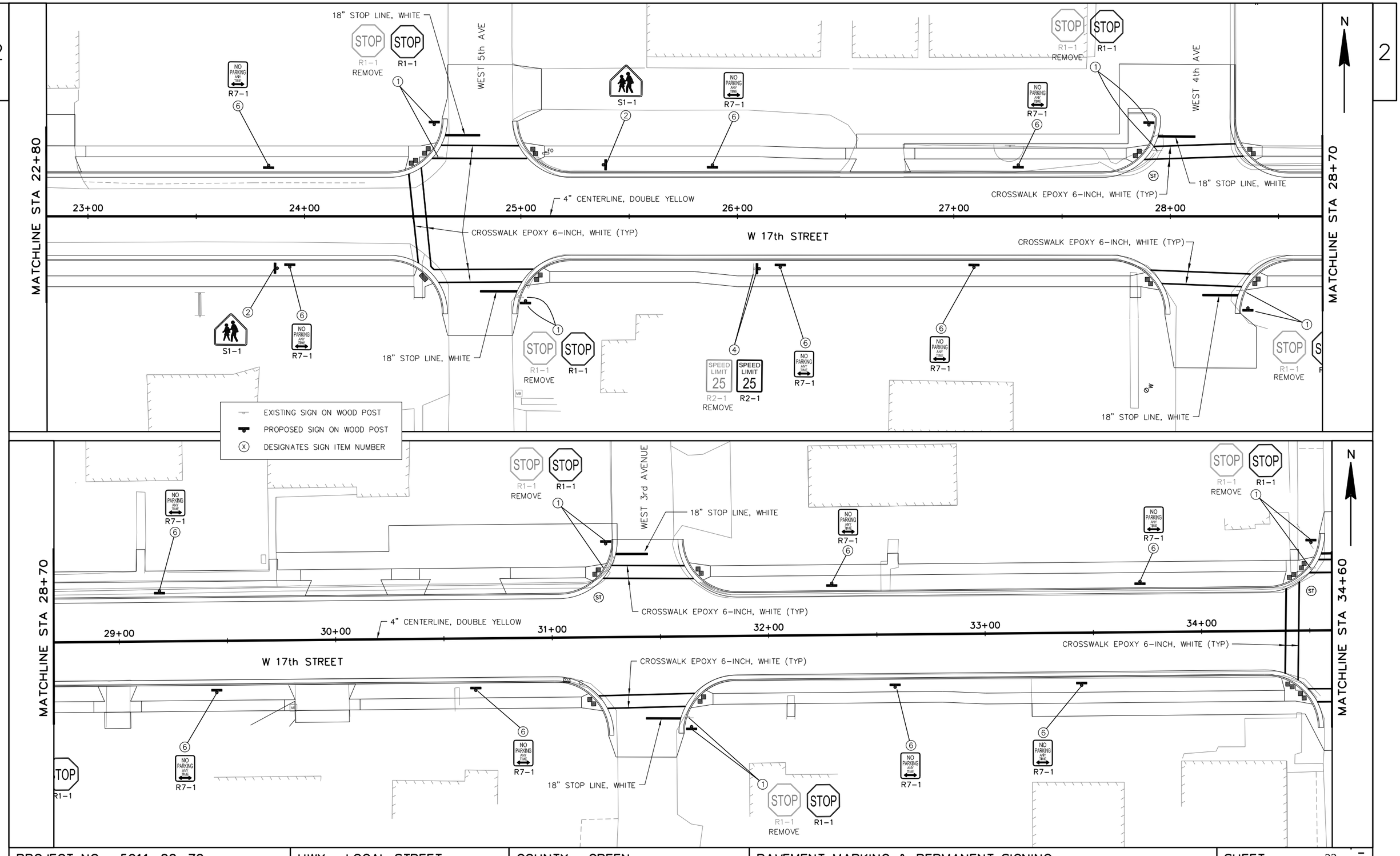
STOP R1-1
1

NO PARKING ANY TIME R7-1
6

MATCHLINE STA 22+80

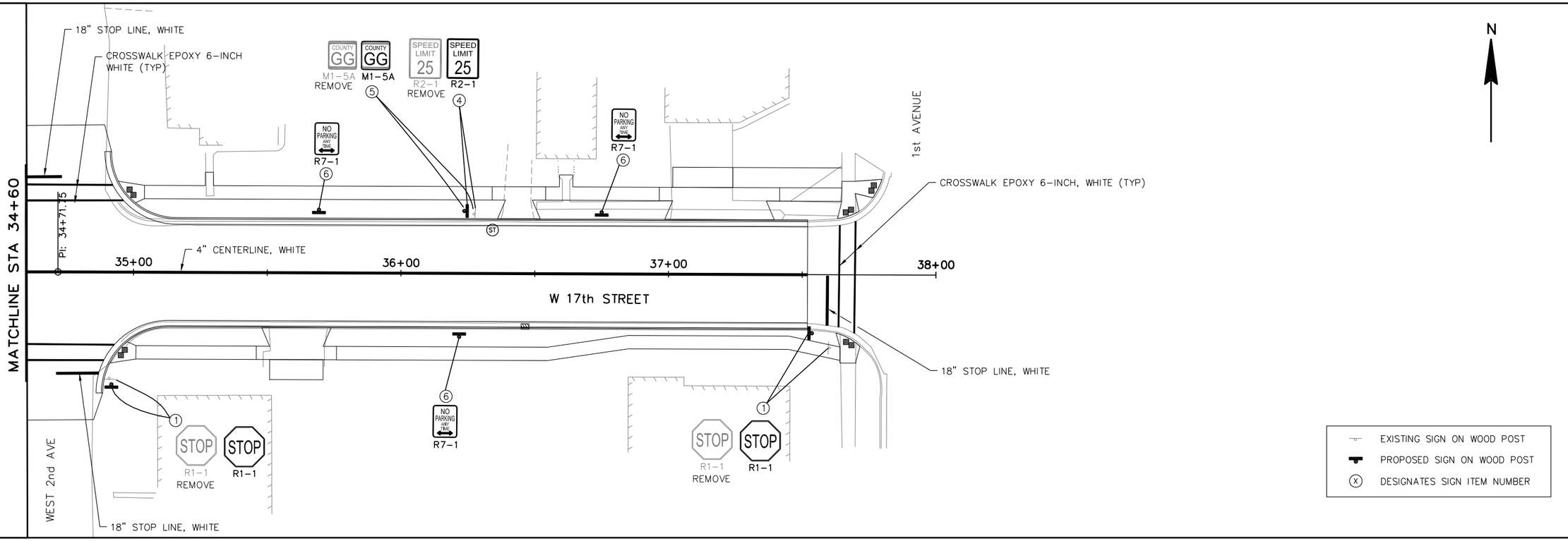
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	PAVEMENT MARKING & PERMANENT SIGNING	SHEET 22

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\024501_PM_48930008.DWG PLOT DATE : 1/23/2014 3:41 PM PLOT BY : KATHRYN BLASER PLOT NAME : PLOT SCALE : 1 IN: 20 FT
WISDOT/CADDS SHEET 42



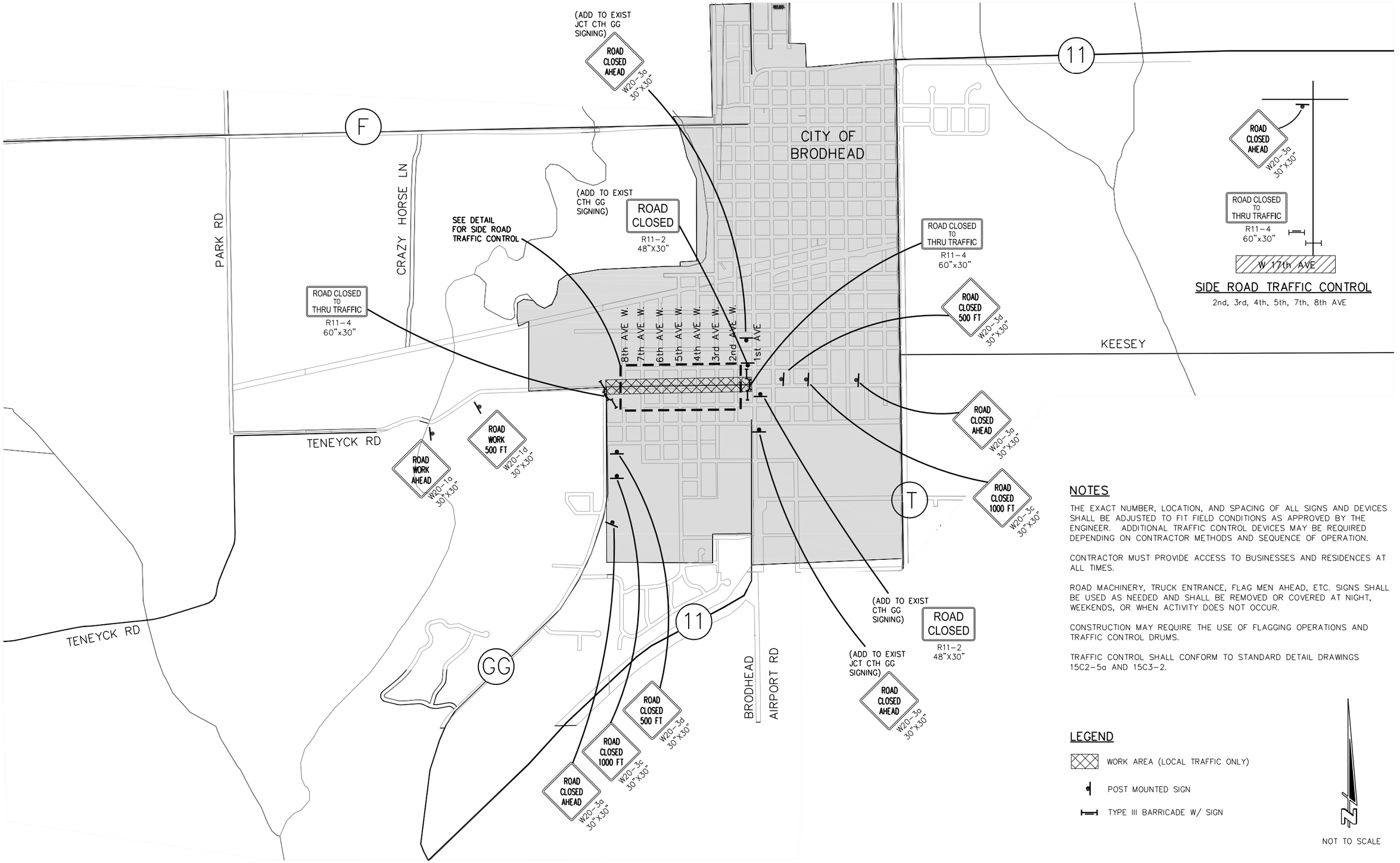
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	PAVEMENT MARKING & PERMANENT SIGNING	SHEET 23

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\024501_PM_48930008.DWG PLOT DATE : 1/23/2014 3:42 PM PLOT BY : KATHRYN BLASER PLOT NAME : PLOT SCALE : 1 IN:20 FT



PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	PAVEMENT MARKING & PERMANENT SIGNING	SHEET 24

2



SIDE ROAD TRAFFIC CONTROL

2nd, 3rd, 4th, 5th, 7th, 8th AVE

NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER. ADDITIONAL TRAFFIC CONTROL DEVICES MAY BE REQUIRED DEPENDING ON CONTRACTOR METHODS AND SEQUENCE OF OPERATION.

CONTRACTOR MUST PROVIDE ACCESS TO BUSINESSES AND RESIDENCES AT ALL TIMES.

ROAD MACHINERY, TRUCK ENTRANCE, FLAG MEN AHEAD, ETC. SIGNS SHALL BE USED AS NEEDED AND SHALL BE REMOVED OR COVERED AT NIGHT, WEEKENDS, OR WHEN ACTIVITY DOES NOT OCCUR.

CONSTRUCTION MAY REQUIRE THE USE OF FLAGGING OPERATIONS AND TRAFFIC CONTROL DRUMS.

TRAFFIC CONTROL SHALL CONFORM TO STANDARD DETAIL DRAWINGS 15C2-5a AND 15C3-2.

LEGEND

- WORK AREA (LOCAL TRAFFIC ONLY)
- POST MOUNTED SIGN
- TYPE III BARRICADE W/ SIGN

NOT TO SCALE

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	TRAFFIC CONTROL	SHEET 25 E

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\025001_TC_48930008.DWG PLOT DATE : 1/21/2014 10:11 PM PLOT BY : KATHRYN BLASER PLOT NAME : PLOT SCALE : ##########

DATE 29APR14 — E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	5911-00-70 QUANTITY
0010	201.0105	CLEARING	STA	13.000	13.000
0020	201.0205	GRUBBING	STA	13.000	13.000
0030	204.0100	REMOVING PAVEMENT	SY	6,300.000	6,300.000
0040	204.0150	REMOVING CURB & GUTTER	LF	2,365.000	2,365.000
0050	204.0155	REMOVING CONCRETE SIDEWALK	SY	150.000	150.000
0060	204.0210	REMOVING MANHOLES	EACH	1.000	1.000
0070	204.0220	REMOVING INLETS	EACH	11.000	11.000
0080	204.0245	REMOVING STORM SEWER (SIZE) 01. 12"	LF	282.000	282.000
0090	204.0245	REMOVING STORM SEWER (SIZE) 02. 15"	LF	34.000	34.000
0100	204.0245	REMOVING STORM SEWER (SIZE) 03. 18"	LF	41.000	41.000
0110	204.0270	ABANDONING CULVERT PIPES	EACH	1.000	1.000
0120	205.0100	EXCAVATION COMMON	CY	9,150.000	9,150.000
0130	213.0100	FINISHING ROADWAY (PROJECT) 01. 5911-00-70	EACH	1.000	1.000
0140	305.0110	BASE AGGREGATE DENSE 3/4-INCH	TON	10.000	10.000
0150	305.0120	BASE AGGREGATE DENSE 1 1/4-INCH	TON	9,050.000	9,050.000
0160	350.0102	SUBBASE	CY	385.000	385.000
0170	416.0160	CONCRETE DRIVEWAY 6-INCH	SY	530.000	530.000
0180	455.0115	ASPHALTIC MATERIAL PG64-22	TON	96.000	96.000
0190	455.0120	ASPHALTIC MATERIAL PG64-28	TON	77.000	77.000
0200	455.0605	TACK COAT	GAL	300.000	300.000
0210	460.1101	HMA PAVEMENT TYPE E-1	TON	3,080.000	3,080.000
0220	460.2000	INCENTIVE DENSITY HMA PAVEMENT	DOL	1,980.000	1,980.000
0230	465.0120	ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES	TON	45.000	45.000
0240	601.0411	CONCRETE CURB & GUTTER 30-INCH TYPE D	LF	4,280.000	4,280.000
0250	602.0405	CONCRETE SIDEWALK 4-INCH	SF	17,100.000	17,100.000
0260	602.0505	CURB RAMP DETECTABLE WARNING FIELD YELLOW	SF	208.000	208.000
0270	608.0312	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 12-INCH	LF	639.000	639.000
0280	608.0324	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 24-INCH	LF	8.000	8.000
0290	611.0420	RECONSTRUCTING MANHOLES	EACH	2.000	2.000
0300	611.0530	MANHOLE COVERS TYPE J	EACH	9.000	9.000
0310	611.0612	INLET COVERS TYPE C	EACH	1.000	1.000
0320	611.0624	INLET COVERS TYPE H	EACH	21.000	21.000
0330	611.2004	MANHOLES 4-FT DIAMETER	EACH	8.000	8.000
0340	611.3003	INLETS 3-FT DIAMETER	EACH	1.000	1.000
0350	611.3230	INLETS 2X3-FT	EACH	20.000	20.000
0360	611.8120.S	COVER PLATES TEMPORARY	EACH	7.000	7.000
0370	619.1000	MOBILIZATION	EACH	1.000	1.000
0380	625.0100	TOPSOIL	SY	4,650.000	4,650.000
0390	627.0200	MULCHING	SY	4,650.000	4,650.000
0400	628.1104	EROSION BALES	EACH	5.000	5.000
0410	628.1504	SILT FENCE	LF	500.000	500.000
0420	628.1520	SILT FENCE MAINTENANCE	LF	500.000	500.000
0430	628.1905	MOBILIZATIONS EROSION CONTROL	EACH	5.000	5.000
0440	628.1910	MOBILIZATIONS EMERGENCY EROSION CONTROL	EACH	3.000	3.000
0450	628.2004	EROSION MAT CLASS I TYPE B	SY	100.000	100.000
0460	628.7015	INLET PROTECTION TYPE C	EACH	20.000	20.000
0470	628.7560	TRACKING PADS	EACH	2.000	2.000
0480	629.0205	FERTILIZER TYPE A	CWT	2.930	2.930
0490	630.0140	SEEDING MIXTURE NO. 40	LB	83.700	83.700
0500	630.0200	SEEDING TEMPORARY	LB	125.600	125.600

3

DATE 29APR14 E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	5911-00-70 QUANTITY
0510	632.0101	TREES (SPECIES, ROOT, SIZE) 01. ELM, NEW HORIZON B&B 2" CAL	EACH	3.000	3.000
0520	632.0101	TREES (SPECIES, ROOT, SIZE) 02. HONEYLOCUST, SKYLINE B&B 2" CAL	EACH	4.000	4.000
0530	632.0101	TREES (SPECIES, ROOT, SIZE) 03. MAPLE AUTUMN BLAZE B&B 2" CAL	EACH	4.000	4.000
0540	632.0101	TREES (SPECIES, ROOT, SIZE) 04. MAPLE, RED B&B 2" CAL	EACH	3.000	3.000
0550	632.0101	TREES (SPECIES, ROOT, SIZE) 05. OAK, RED B&B 2" CAL	EACH	4.000	4.000
0560	632.0101	TREES (SPECIES, ROOT, SIZE) 06. CRAB, SPRING SNOW B&B 1-1/2" CAL	EACH	2.000	2.000
0570	632.0101	TREES (SPECIES, ROOT, SIZE) 07. LILAC, JAPANESE TREE B&B 1-1/2" CAL	EACH	3.000	3.000
0580	632.0101	TREES (SPECIES, ROOT, SIZE) 08. PEAR, CLEVELAND SELECT B&B 1-1/2" CAL	EACH	4.000	4.000
0590	632.9101	LANDSCAPE PLANTING SURVEILLANCE AND CARE CYCLES	EACH	20.000	20.000
0600	634.0410	POSTS WOOD 4X4-INCH X 10-FT	EACH	43.000	43.000
0610	637.2230	SIGNS TYPE II REFLECTIVE F	SF	181.750	181.750
0620	638.2602	REMOVING SIGNS TYPE II	EACH	18.000	18.000
0630	638.3000	REMOVING SMALL SIGN SUPPORTS	EACH	18.000	18.000
0640	642.5201	FIELD OFFICE TYPE C	EACH	1.000	1.000
0650	643.0100	TRAFFIC CONTROL (PROJECT) 01. 5911-00-70	EACH	1.000	1.000
0660	643.0300	TRAFFIC CONTROL DRUMS	DAY	1,000.000	1,000.000
0670	643.0420	TRAFFIC CONTROL BARRICADES TYPE III	DAY	3,200.000	3,200.000
0680	643.0705	TRAFFIC CONTROL WARNING LIGHTS TYPE A	DAY	6,400.000	6,400.000
0690	643.0900	TRAFFIC CONTROL SIGNS	DAY	4,000.000	4,000.000
0700	646.0106	PAVEMENT MARKING EPOXY 4-INCH	LF	5,450.000	5,450.000
0710	647.0566	PAVEMENT MARKING STOP LINE EPOXY 18-INCH	LF	237.000	237.000
0720	647.0766	PAVEMENT MARKING CROSSWALK EPOXY 6-INCH	LF	1,240.000	1,240.000
0730	649.0100	TEMPORARY PAVEMENT MARKING 4-INCH	LF	4,700.000	4,700.000
0740	650.4000	CONSTRUCTION STAKING STORM SEWER	EACH	21.000	21.000
0750	650.4500	CONSTRUCTION STAKING SUBGRADE	LF	2,350.000	2,350.000
0760	650.5000	CONSTRUCTION STAKING BASE	LF	2,350.000	2,350.000
0770	650.5500	CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER	LF	4,635.000	4,635.000
0780	650.9910	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) 01. 5911-00-70	LS	1.000	1.000
0790	650.9920	CONSTRUCTION STAKING SLOPE STAKES	LF	2,350.000	2,350.000
0800	690.0150	SAWING ASPHALT	LF	940.000	940.000
0810	690.0250	SAWING CONCRETE	LF	67.000	67.000
0820	ASP.1T0A	ON-THE-JOB TRAINING APPRENTICE AT $5.00/HR	HRS	300.000	300.000
0830	ASP.1T0G	ON-THE-JOB TRAINING GRADUATE AT $5.00/HR	HRS	150.000	150.000
0840	SPV.0060	SPECIAL 01. MANHOLE COVER TYPE J SPECIAL SANITARY	EACH	1.000	1.000
0850	SPV.0060	SPECIAL 02. ADJUSTING SANITARY MANHOLE COVERS	EACH	7.000	7.000
0860	SPV.0090	SPECIAL 01. CONCRETE CURB & GUTTER 30-INCH TYPE D SPECIAL	LF	355.000	355.000

3

REMOVING PAVEMENT

STATION - STATION	LOCATION	204.0100 SY	REMARKS
14+00 - 37+52	CTH GG	6300	24' WIDTH
TOTAL		6300	

REMOVING CURB & GUTTER

STATION - STATION	LOCATION	204.0150 LF
21+70	W. 6TH AVE	35
25+05 - 27+93	CTH GG, LT	270
	CTH GG, RT	300
28+29	W. 4TH AVE	40
28+29 - 31+28	CTH GG, LT	320
	CTH GG, RT	300
31+68 - 34+52	CTH GG, LT	290
	CTH GG, RT	290
34+94 - 37+52	CTH GG, LT	260
	CTH GG, RT	260
TOTAL		2365

REMOVING CONCRETE SIDEWALK

STATION - STATION	LOCATION	204.0155 SY
24+50	CTH GG, RT	10
27+84	CTH GG, RT	4
28+30 - 29+73	CTH GG, LT	60
34+43	CTH GG, LT	4
35+29	CTH GG, LT	3
36+50 - 37+70	CTH GG, LT	53
37+67	CTH GG, RT	6
37+67	CTH GG, LT	5
37+75	CTH GG, LT	5
TOTAL		150

ABANDONING CULVERT PIPES

STATION - STATION	LOCATION	204.0270 EACH
15+15	LT	1
TOTAL		1

REMOVING INLETS & MANHOLES

STATION - STATION	LOCATION	204.0220 REMOVING INLETS EACH	204.0210 REMOVING MANHOLES EACH
15+05	35' LT	1	--
21+70	32' LT	1	--
25+24	20' RT	1	--
27+99	26' RT	--	1
27+88	24' RT	1	--
28+30	30' LT	1	--
31+21	27' LT	1	--
31+07	20' RT	1	--
31+21	20' LT	1	--
34+36	20' RT	1	--
36+34	20' LT	1	--
36+46	20' RT	1	--
TOTAL		11	1

REMOVING STORM SEWER 12", 15", 18"

STATION - STATION	LOCATION	204.0245.01 12" LF	204.0245.02 15" LF	204.0245.03 18" LF
17+85	LT	16	--	--
21+20 - 21+69	16' LT	53	--	--
25+25	CTH GG	35	--	--
27+75 - 28+29	19' LT	51	--	--
27+75 - 28+30	25' RT	50	--	--
28+00	CTH GG	--	--	41
31+06 - 31+20	19' RT	37	--	--
31+21	24' LT	4	--	--
34+35	CTH GG	36	--	--
36+40	CTH GG	--	34	--
TOTAL		282	34	41

ALL ITEMS ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.

TRAFFIC CONTROL

LOCATION	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0900 TRAFFIC CONTROL SIGNS DAY	643.0300 TRAFFIC CONTROL DRUMS DAY	643.0100 TRAFFIC CONTROL 5911-00-70 EACH
5911-00-70	--	--	--	--	1
TENEYCK ROAD	--	--	200	--	--
CTH GG	600	1200	1200	--	--
W. 8TH AVE	200	400	200	--	--
W. 7TH AVE	400	800	400	--	--
W. 6TH AVE	400	800	400	--	--
W. 5TH AVE	400	800	400	--	--
W. 4TH AVE	400	800	400	--	--
W. 3RD AVE	400	800	400	--	--
W. 2ND AVE	400	800	400	--	--
UNDISTRIBUTED	--	--	--	1000	--
TOTAL	3200	6400	4000	1000	1

CONCRETE DRIVEWAY 6-INCH

STATION - STATION	LOCATION	416.0160 SY
15+65 - 15+95	DRIVEWAY LT	35
15+95 - 16+20	DRIVEWAY LT	26
22+68 - 22+99	DRIVEWAY LT	70
26+50 - 26+82	DRIVEWAY LT	35
28+88 - 29+10	DRIVEWAY RT	24
29+76 - 30+11	DRIVEWAY RT	41
29+84 - 30+25	DRIVEWAY LT	51
30+39 - 30+81	DRIVEWAY LT	51
33+55 - 33+95	DRIVEWAY RT	50
35+46 - 35+76	DRIVEWAY RT	51
36+34 - 36+54	DRIVEWAY LT	22
36+96 - 37+41	DRIVEWAY LT	74
TOTAL		530

3

CLEARING & GRUBBING

STATION - STATION	LOCATION	201.0105 CLEARING STA.	201.0205 GRUBBING STA.
19+00 - 21+00	CTH GG	2	2
22+00 - 25+00	CTH GG	3	3
28+00 - 30+00	CTH GG	2	2
31+00 - 37+00	CTH GG	6	6
TOTAL		13	13

BASE AGGREGATE DENSE

STATION - STATION	LOCATION	305.0120 1 1/4 - INCH TON	305.0110 3/4 - INCH TON
14+00 - 37+52	CTH GG	9050	10
TOTAL		9050	10

HMA PAVEMENT

STATION - STATION	LOCATION	460.1101 HMA PAVEMENT TYPE E - 1 TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS & FIELD ENTERANCES TON
14+70 - 15+06	DRIVEWAY RT	--	2
15+70 - 16+15	DRIVEWAY LT	--	1
26+55 - 27+80	DRIVEWAY LT	--	10
28+93 - 29+05	DRIVEWAY RT	--	1
29+81 - 30+06	DRIVEWAY RT	--	2
29+73 - 31+28	DRIVEWAY LT	--	15
33+22 - 34+56	DRIVEWAY RT	--	10
37+24 - 37+65	DRIVEWAY LT	--	4
14+00 - 37+52	CTH GG, MAINLINE	3080	--
TOTAL		3080	45

ASPHALTIC MATERIAL

STATION - STATION	LOCATION	455.0115 PG 64-22 TON	455.0120 PG 64-28 TON	455.0605 TACK COAT GAL
14+00 - 37+52	CTH GG	96	77	300
TOTAL		96	77	300

3

RESTORATION ITEMS

STATION - STATION	LOCATION	625.0100 TOPSOIL SY	627.0200 MULCHING SY	629.0205 FERTILIZER TYPE A CWT	630.0140 SEEDING MIXTURE NO. 40 LB	630.0200 SEEDING TEMPORARY LB	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.1104 EROSION BALES EACH	628.2004 EROSION MAT CLASS I TYPE B SY	628.7560 TRACKING PADS EACH	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATION EMERGENCY EROSION CONTROL EACH
14+00 - 14+77	LT	55	55	0.03	1.0	1.5	--	--	--	--	--	--	--
	RT	45	45	0.03	0.8	1.2	--	--	--	--	--	--	--
15+00 - 18+00	LT	270	270	0.17	4.9	7.3	--	--	--	--	--	--	--
	RT	130	130	0.08	2.3	3.5	--	--	--	--	--	--	--
18+25 - 21+25	LT	280	280	0.18	5.0	7.6	--	--	--	--	--	--	--
	RT	440	440	0.28	7.9	11.9	--	--	--	--	--	--	--
21+75 - 24+70	LT	320	320	0.20	5.8	8.6	--	--	--	--	--	--	--
	RT	360	360	0.23	6.5	9.7	--	--	--	--	--	--	--
25+00 - 28+00	LT	250	250	0.16	4.5	6.8	--	--	--	--	--	--	--
	RT	370	370	0.23	6.7	10	--	--	--	--	--	--	--
28+25 - 31+25	LT	220	220	0.14	4.0	5.9	--	--	--	--	--	--	--
	RT	300	300	0.19	5.4	8.1	--	--	--	--	--	--	--
31+70 - 34+60	LT	300	300	0.19	5.4	8.1	--	--	--	--	--	--	--
	RT	300	300	0.19	5.4	8.1	--	--	--	--	--	--	--
34+85 - 37+75	LT	210	210	0.13	3.8	5.7	--	--	--	--	--	--	--
	RT	300	300	0.19	5.4	8.1	--	--	--	--	--	--	--
UNDISTRIBUTED	CTH GG	500	500	0.32	9.0	13.5	500	500	5	100	2	5	3
TOTAL		4650	4650	2.93	83.7	125.6	500	500	5	100	2	5	3

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PAVEMENT MARKING EPOXY, 4-INCH

		646.0106	
		EDGELINE WHITE	CENTERLINE DOUBLE-YELLOW
STATION - STATION	LOCATION	LF	LF
14+00 - 37+52	CTH GG	750	4700
SUBTOTAL		750	4700
TOTAL		5450	

PAVEMENT MARKING CROSSWALK EPOXY, 6-INCH

	647.0766
LOCATION	LF
W. 7TH AVE	195
W. 6TH AVE	145
W. 5TH AVE	260
W. 4TH AVE	150
W. 3RD AVE	150
W 2ND AVE	260
1ST CENTER AVE	80
TOTAL	1240

PAVEMENT MARKING STOP LINE EPOXY, 18-INCH

	647.0566
LOCATION	LF
W. 8TH AVE	17
W. 7TH AVE	39
W. 6TH AVE	31
W. 5TH AVE	33
W. 4TH AVE	33
W. 3RD AVE	31
W 2ND AVE	34
CTH GG	19
TOTAL	237

TEMPORARY PAVEMENT MARKING 4-INCH

		649.0100
STATION - STATION	LOCATION	LF
14+00 - 37+52	CTH GG, CENTER DBL YLW	4700
TOTAL		4700

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CURB RAMP DETECTABLE WARNING FIELD, YELLOW

		602.0505
STATION - STATION	LOCATION	SF
15+20	30' LT	8
17+88	30' LT	8
18+45	27' RT	8
18+51	26' LT	8
21+27	30' RT	8
21+27	29' LT	8
21+77	30' RT	8
21+71	30' LT	8
24+56	27' RT	8
24+56	28' LT	8
25+08	28' RT	8
25+06	30' LT	8
27+88	26' RT	8
27+85	27' LT	8
28+40	30' RT	8
28+37	30' LT	8
31+18	30' RT	8
31+21	30' LT	8
31+69	29' RT	8
31+68	30' LT	8
34+46	27' RT	8
34+46	26' LT	8
34+96	30' RT	8
34+99	30' LT	8
37+67	24' RT	8
37+67	24' LT	8
TOTAL		208

CONSTRUCTION STAKING

		650.4000	650.4500	650.5000	650.5500	650.9910	650.9920
		STORM SEWER	SUBGRADE	BASE	CURB GUTTER & CURB & GUTTER	SUPPLEMENTAL CONTROL	SLOPE STAKES
STATION - STATION	LOCATION	EACH	LF	LF	LF	LS	LF
12+40 - 37+52	CTH GG	21	2350	2350	4635	1	2350
TOTAL		21	2350	2350	4635	1	2350

SAWING ASPHALT

		690.0150
CTH GG STATION	LOCATION	LF
14+77 - 15+07	W. 8TH AVE	29
18+00 - 18+36	W. 7TH AVE	57
21+38 - 21+68	W. 6TH AVE	59
24+66 - 24+96	W. 5TH AVE	59
27+76 - 28+28	W. 4TH AVE	89
31+28 - 31+56	W. 3RD AVE	55
34+57 - 34+90	W. 2ND AVE	62
26+55 - 27+76	PRKG LOT, LT	161
29+73 - 31+28	PRKG LOT, LT	177
33+21 - 34+57	PRKG LOT, RT	151
37+24 - 37+65	PRKG LOT, LT	41
TOTAL		940

SAWING CONCRETE

		690.0250
STATION	LOCATION	LF
14+00	CTH GG	31
37+52	CTH GG	36
TOTAL		67

MOBILIZATION

		619.1000
STATION - STATION	LOCATION	EACH
14+00 - 37+52	CTH GG	1
TOTAL		1

INLET PROTECTION, TYPE C

		628.7015
STATION - STATION	LOCATION	EACH
17+77	26' RT	1
17+88	29' RT	1
17+97	37' LT	1
21+10	18' RT	1
21+25	22' LT	1
21+69	38' LT	1
22+50	18' RT	1
22+50	18' LT	1
25+11	21' RT	1
25+25	18' LT	1
27+82	20' RT	1
27+81	22' LT	1
28+42	23' RT	1
28+40	23' LT	1
31+01	18' RT	1
31+28	36' LT	1
34+41	21' RT	1
34+53	32' LT	1
36+34	18' RT	1
36+54	18' LT	1
TOTAL		20

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SUBBASE

STATION - STATION	LOCATION	350.0102 CY
15+14 - 17+93	CTH GG, LT	26
18+41 - 21+33	CTH GG, LT	32
	CTH GG, RT	34
21+70 - 24+60	CTH GG, LT	29
	CTH GG, RT	32
25+00 - 27+92	CTH GG, LT	29
	CTH GG, RT	34
28+32 - 31+25	CTH GG, LT	24
	CTH GG, RT	26
31+63 - 34+50	CTH GG, LT	34
	CTH GG, RT	32
34+91 - 37+70	CTH GG, LT	24
	CTH GG, RT	29
TOTAL		385

SIGNING QUANTITIES

ALL ITEMS ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.

SIGN NO.	SIGN CODE	SIGN MESSAGE	SIGN SIZE	637.2230 SIGNS TYPE II REFLECTIVE F SF	634.0410 POSTS WOOD 4X4 INCH X 10-FT EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	COMMENTS
1	R1-1	STOP	30 X 30	87.50	14	14	14	14 SIGNS
2	S1-1	PED CROSSING	36 X 36	36.00	4	1	1	4 SIGNS
3	W1-1L	LEFT ARROW	30 X 30	6.25	1	1	1	1 SIGN
4	R2-1	SPEED LIMIT 25 MPH	24 X 30	15.00	1	1	1	MOUNT 1 SIGN ON #3 POST AND 1 SIGN ON #5 POST (3 TOTAL)
5	M1-5A	COUNTY GG	24 X 24	4.00	1	1	1	1 SIGN
6	R7-1	NO PARKING ANY TIME	12 X 18	33.00	22	--	--	22 SIGNS
TOTAL				181.75	43	18	18	

FIELD OFFICE, TYPE C

STATION - STATION	LOCATION	642.5201 EACH
PROJECT	CTH GG	1
TOTAL		1

FINISHING ROADWAY

STATION - STATION	LOCATION	213.0100 EACH
PROJECT	CTH GG	1
TOTAL		1

CONCRETE SIDEWALK, 4-INCH

STATION - STATION	LOCATION	602.0405 SF
15+14 - 17+93	CTH GG, LT	1125
18+41 - 21+33	CTH GG, LT	1440
	CTH GG, RT	1530
21+70 - 24+60	CTH GG, LT	1305
	CTH GG, RT	1440
25+00 - 27+92	CTH GG, LT	1260
	CTH GG, RT	1485
28+32 - 31+25	CTH GG, LT	1080
	CTH GG, RT	1170
31+63 - 34+50	CTH GG, LT	1485
	CTH GG, RT	1440
34+91 - 37+70	CTH GG, LT	1080
	CTH GG, RT	1260
TOTAL		17100

CONCRETE CURB & GUTTER, 30-INCH, TYPE D

STATION - STATION	LOCATION	601.0411 LF	SPV.0090.01 LF
14+63 - 18+05	CTH GG, LT	330	--
	CTH GG, RT	--	355
18+35 - 21+38	CTH GG, LT	335	--
	CTH GG, RT	335	--
21+67 - 24+65	CTH GG, LT	360	--
	CTH GG, RT	330	--
24+96 - 28+00	CTH GG, LT	345	--
	CTH GG, RT	335	--
28+27 - 31+30	CTH GG, LT	360	--
	CTH GG, RT	330	--
31+58 - 34+57	CTH GG, LT	330	--
	CTH GG, RT	330	--
34+86 - 37+52	CTH GG, LT	280	--
	CTH GG, RT	280	--
TOTAL		4280	355

CATEGORY 0020 NON-PARTICIPATING ITEMS

CATEGORY	STATION	LOCATION	SPV.0060.02 ADJUSTING SANITARY MANHOLE COVERS EACH	611.8120.S COVER PLATES TEMPORARY EACH	SPV.0060.01 MANHOLE COVER TYPE J SPECIAL SANITARY EACH
0020	14+89	24' LT	1	1	--
0020	14+91	37' RT	1	1	--
0020	18+26	25' LT	1	1	--
0020	21+50	34' RT	1	1	1
0020	28+12	30' LT	1	1	--
0020	31+43	22' RT	1	1	--
0020	34+71	40' LT	1	1	--
TOTAL			7	7	1

Division	From/To Station	Location	205.0100 Common Excavation (CY)	Salvaged/Unusable Pavement Material	Available Material (CY) (1)	Unexpanded Fill	Expanded Fill	Mass Ordinate +/- (2)
			Cut				Factor 1.25	
1	14+00 - 37+50	CTH GG Mainline	9150	0	9150	40	49	9100
Division 1 Subtotal			9150	0	9150	40	49	9100
Grand Total			9150	0	9150	40	49	9100
		Total Common Ex	9150					

1) Available Material = Cut - Salvaged/Unusuable Pavement Material
2) The Mass Ordinate + or - Qty calculated for the Division. Plus quantity indicates an excess of material. Minus indicates a shortage of material.

PLANTING QUANTITIES

				SHEET NUMBERS		632.0101
		SIZE WHEN	ROOT ZONE	1	2	TOTAL
SYM.	COMMON NAME	PLANTED	MODE	EACH	EACH	EACH
NH	ELM, NEW HORIZON	2" CAL.	B&B	1	2	3
SH	HONEYLOCUST, SKYLINE	2" CAL.	B&B	2	2	4
AB	MAPLE, AUTUMN BLAZE	2" CAL.	B&B	2	2	4
RM	MAPLE, RED	2" CAL.	B&B	1	2	3
RO	OAK, RED	2" CAL.	B&B	1	3	4
SS	CRAB, SPRING SNOW	1-1/2" CAL.	B&B	--	2	2
JT	LILAC, JAPANESE TREE	1-1/2" CAL.	B&B	1	2	3
CS	PEAR, CLEVELAND SELECT	1-1/2" CAL.	B&B	2	2	4

LANDSCAPE PLANTING SURVEILLANCE AND CARE CYCLES

LOCATION	632.9101 EACH
W. 17TH ST.	20
TOTAL	20

ALL ITEMS ARE CATEGORY 0010
UNLESS OTHERWISE NOTED.

STORM SEWER

STRUCTURE NUMBER	LOCATION	CASTING FLAG ELEV.	REINF. CONC. PIPE CL. III 608.0312 12" LF	REINF. CONC. PIPE CL. III 608.0324 24" LF	% SLOPE	611.0530 MANHOLE COVERS TYPE J EACH	611.0612 INLET COVERS TYPE C EACH	611.0624 INLET COVERS TYPE H EACH	611.3230 INLETS 2X3-FT EACH	611.2004 MANHOLES 4-FT DIA EACH	611.3003 INLETS 3-FT DIA EACH	611.0420 RECONSTRUCTING MANHOLES EACH	DEPTH IN FEET	FROM STRUCT.	TO STRUCT.	INLET ELEV.	DISCH. ELEV.
EX MH 1	12+40, 127' LT	794.25	--	--	--	--	--	--	--	--	--	--	12.29	EX MH 1	OUTFALL	781.80	
MH 2A	17+95, 15.6' LT	793.31	--	--	0.03	1	--	--	--	1	--	--	11.51	MH 2A	EX MH 1	781.96	781.80
EX MH 2B	18+03, 16.5' LT	793.26	--	8	1.25	1	--	--	--	--	--	1	11.62	EX MH 2B	MH 2A	781.90	781.80
INL 2A	17+84, 24.9' LT	793.02	12	--	31.52	--	--	1	1	--	--	--	4.00	INL 2A	MH 2A	789.02	785.09
INL 2B	17+77, 28.5' RT	792.93	48	--	8.06	--	--	1	1	--	--	--	4.00	INL 2B	MH 2A	788.93	785.09
INL 2C	17+87, 30.5' RT	793.07	10	--	2.00	--	--	1	1	--	--	--	3.84	INL 2C	INL 2B	789.23	789.03
MH 3A	21+18, 16.2' LT	791.90	--	--	0.22	1	--	--	--	1	--	--	9.31	MH 3A	MH 2B	782.59	781.90
INL 3A	21+10, 20.5' RT	791.77	38	--	12.76	--	--	1	1	--	--	--	4.00	INL 3A	MH 3A	787.77	782.97
INL 3B	21+24, 23.9' LT	791.71	9	--	6.85	--	--	1	1	--	--	--	4.56	INL 3B	MH 3A	787.15	786.50
INL 3C	21+71, 38.0' LT	791.74	49	--	1.00	--	--	1	1	--	--	--	4.00	INL 3C	INL 3B	787.74	787.25
INL 3D	22+50, 20.5' LT	791.22	15	--	0.50	--	--	1	1	--	--	--	3.30	INL 3D	MH 3B	787.92	787.84
INL 3E	22+50, 20.5' RT	791.22	41	--	0.50	--	--	1	1	--	--	--	3.00	INL 3E	INL 3D	788.22	788.02
MH 3B	22+36, 16.4' LT	791.43	--	--	0.03	1	--	--	--	1	--	--	8.62	MH 3B	MH 3A	782.81	782.77
MH 4A	24+61, 16.8' LT	792.23	--	--	0.03	1	--	--	--	1	--	--	9.35	MH 4A	MH 3B	782.88	782.81
EX MH 4B	25+25, 20.3' LT	791.98	--	--	0.13	--	--	1	--	--	--	1	8.79	MH 4B	MH 4A	783.19	783.11
INL 4	25+12, 23.0' RT	791.88	45	--	8.00	--	--	1	1	--	--	--	4.00	INL 4	MH 4B	787.88	784.27
MH 5	27+92, 18.7' LT	790.98	--	--	0.09	1	--	--	--	1	--	--	8.27	MH 5	EX MH 4B	783.44	783.20
INL 5A	27+80, 23.3' LT	790.82	13	--	3.05	--	--	1	1	--	--	--	2.92	INL 5A	MH 5	787.90	787.50
INL 5B	27+82, 22.2' RT	790.80	42	--	1.00	--	--	1	1	--	--	--	4.00	INL 5B	MH 5	786.80	786.38
INL 5C	28+43, 24.8' RT	791.08	50	--	0.50	--	--	1	1	--	--	--	4.00	INL 5C	INL 5D	787.08	786.83
INL 5D	28+41, 24.8' LT	791.04	49	--	0.50	--	--	1	1	--	--	--	4.31	INL 5D	MH 5	786.73	786.48
INL 5E	27+85, 36.5' LT	791.00	14	--	0.70	--	1	--	--	--	1	--	4.08	INL 5E	INL 5A	788.00	787.90
MH 6	31+22, 18.4' LT	791.76	--	--	0.27	1	--	--	--	1	--	--	8.02	MH 6	MH 5	784.20	783.31
INL 6A	31+01, 20.5' RT	791.74	44	--	5.00	--	--	1	1	--	--	--	4.00	INL 6A	MH 6	787.74	785.54
INL 6B	31+27, 36.8' LT	791.32	19	--	2.00	--	--	1	1	--	--	--	2.80	INL 6B	MH 6	788.52	788.14
MH 7	34+51, 18.3' LT	790.37	--	--	0.06	1	--	--	--	1	--	--	6.22	MH 7	MH 6	784.46	784.26
INL 7A	34+29, 20.5' RT	790.30	45	--	1.93	--	--	1	1	--	--	--	3.00	INL 7A	MH 7	787.30	786.44
INL 7B	34+53, 35.8' LT	789.52	18	--	4.13	--	--	1	1	--	--	--	2.70	INL 7B	MH 7	786.82	786.09
INL 7C	34+30, 20.5' LT	790.34	21	--	1.64	--	--	1	1	--	--	--	3.00	INL 7C	MH 7	787.34	787.00
MH 8	36+34, 16.3' LT	789.60	--	--	0.08	1	--	--	--	1	--	--	5.89	MH 8	MH 7	784.45	784.30
INL 8A	36+34, 20.5' RT	789.44	37	--	3.37	--	--	1	1	--	--	--	3.00	INL 8A	MH 8	786.44	785.20
INL 8B	36+54, 20.5' LT	789.36	20	--	5.72	--	--	1	1	--	--	--	3.00	INL 8B	MH 8	786.36	785.20
	TOTALS		639	8		9	1	21	20	8	1	2					

ALL ITEMS ARE CATEGORY 0010
UNLESS OTHERWISE NOTED.

R/W PROJECT NUMBER	SHEET NUMBER	TOTAL NUMBER
5911-00-01	4.01	6
FEDERAL PROJECT NUMBER		

PLAT OF RIGHT OF WAY REQUIRED FOR

CITY OF BRODHEAD, WEST 17TH STREET

(WEST 8TH AVENUE TO 1ST CENTER AVENUE)

CTH GG — GREEN COUNTY

CONSTRUCTION PROJECT NUMBER 5911-00-70

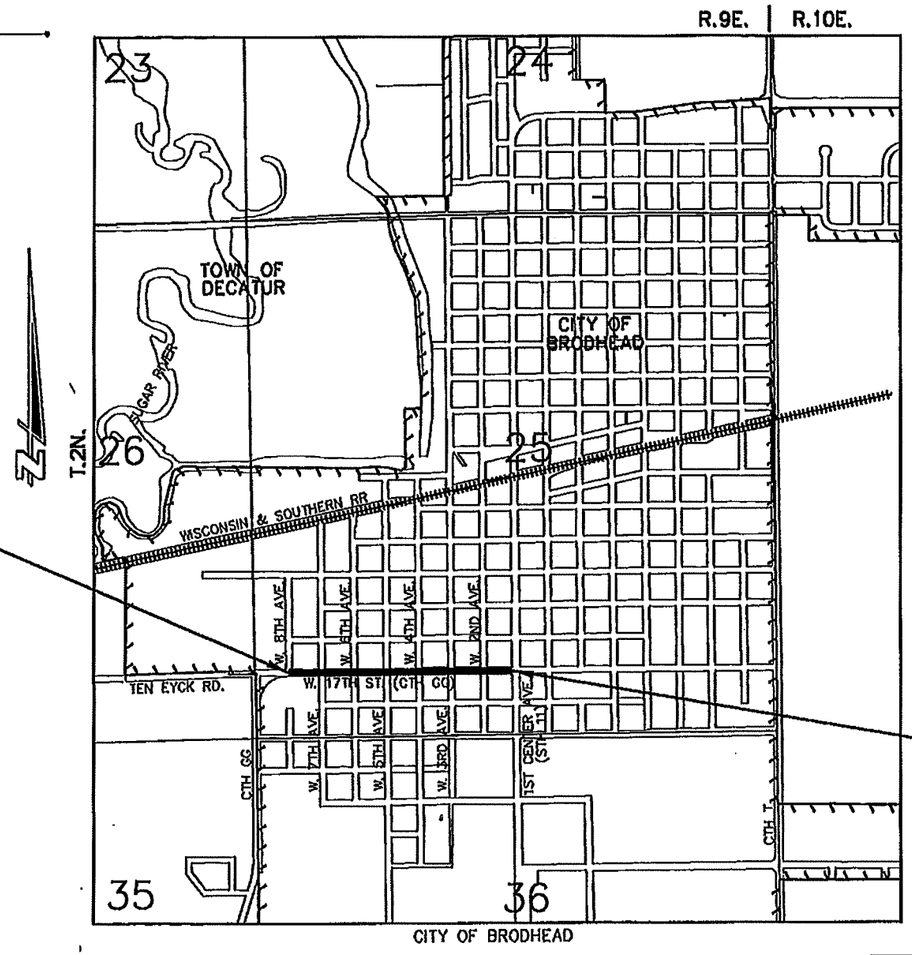
CONVENTIONAL SYMBOLS

- SECTION LINE
- 1/4 LINE
- COUNTY LINE
- CORPORATE LIMITS
- PROPERTY LINE
- SIXTEENTH LINE
- TOWN OR RANGE LINE
- TEMPORARY LIMITED EASEMENT
- PERMANENT LIMITED EASEMENT
- EXISTING RIGHT OF WAY
- EXISTING EASEMENT
- NEW RIGHT OF WAY
- EXISTING FENCE
- REFERENCE LINE
- SLOPE INTERCEPT
- TREE
- TREELINE
- MARSH
- SECTION MONUMENT
- IRON PIPE
- REBAR
- SIGN

- COMBUSTIBLE FLUIDS (UNDER PRESSURE) — CAUTION
- UTILITIES
  - SANITARY SEWER — SAN
  - GAS — GAS
  - UNDERGROUND ELECTRIC — E
  - TELEPHONE — TEL
  - FIBER OPTIC — FO
  - OVERHEAD UTILITIES — OE
  - TELEPHONE PEDESTAL (COMPENSABLE)
  - POWER POLE (COMPENSABLE)
  - SANITARY MANHOLE — SAN
- RIGHT-OF-WAY MARKERS
- SECTION CORNER — T.2N. 28 27 28 27 R.9E.
- TEMPORARY LIMITED EASEMENT

CONVENTIONAL ABBREVIATIONS

AC	ACRES	PI	POINT OF INTERSECTION
℄	CENTERLINE	PT	POINT OF TANGENCY
CTH	COUNTY TRUNK HIGHWAY	PL	PROPERTY LINE
D	DEGREE OF CURVATURE	MI	MILE
DOC	DOCUMENT	R	RADIUS / RANGE
E	EAST	R/L	REFERENCE LINE
H	HOUSE	R/W	RIGHT OF WAY
IP	IRON PIPE	SEC	SECTION
L	LENGTH OF CURVE	S	SOUTH
LC	LONG CHORD	SE	SOUTHEAST
LCB	LONG CORD BEARING	SW	SOUTHWEST
MON	MONUMENT	STH	STATE TRUNK HIGHWAY
N	NORTH	STA	STATION
NE	NORTHEAST	T	TANGENT / TOWNSHIP
NO	NUMBER	USH	UNITED STATES HIGHWAY
NW	NORTHWEST	V	VOLUME
P	PAGE	PLE	PERMANENT LIMITED EASEMENT
PC	POINT OF CURVATURE	TLE	TEMPORARY LIMITED EASEMENT
		W	WEST



BEGIN RELOCATION ORDER
STA 15+00.00
669.86 FEET NORTH AND 347.71 FEET EAST OF THE SOUTHWEST CORNER OF SEC. 25, T2N, R9E

END RELOCATION ORDER
STA 37+70.00
660.05 FEET NORTH AND 28.82 FEET WEST OF THE SOUTH 1/4 CORNER OF SEC. 25, T2N, R9E

SCALE, FEET: 0 750 1500

TOTAL NET LENGTH OF CENTERLINE = 0.430 MI.

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, GREEN COUNTY, NAD 83 (1991) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

NO NEW RIGHT-OF-WAY MONUMENTS WILL BE PLACED. THIS PLAT ONLY INCLUDES TEMPORARY INTERESTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD".

APPROVED FOR CITY OF BRODHEAD

11/13/13 DATE — CITY MAYOR

ORIGINAL PLAT PREPARED BY

Cedar corporation
604 Wilson Avenue
Menomonie, Wisconsin 54751
715-235-9081
800-472-7372
FAX 715-235-2727
www.cedarcorp.com
engineers • architects • planners • environmental specialists
land surveyors • landscape architects • civil engineers

WISCONSIN
BRYON J. MOTSZKO
S-2846
MENOMONIE WI
LAND SURVEYOR

11/11/13 DATE — Bryon Motszko (Signature)

REVISION DATE:
1/2/2014

E

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE CITY.

PARCEL NUMBER	SHEET NUMBER	OWNER	INTEREST REQUIRED	R/W AREA REQUIRED			TLE AREA (S.F.)
				NEW	EXIST.	TOTAL	
1	4.03	CAROL J. FOESCH	TLE	–	–	–	1352
2	4.03	KUHN KNIGHT INC.	TLE	–	–	–	1017
3	4.03	CINDY L. WAGNER	TLE	–	–	–	3036
4	4.03 & .04	SCOTT A. & SHAWN M. COOK	TLE	–	–	–	1301
5	4.03 & .04	CHRISTOPHER K. & MICHELLE L. MCLAIN	TLE	–	–	–	352
6	4.04	TOMMY M. BUTTS & SUSAN A. COPLIEN	TLE	–	–	–	1980
7	4.04	ROBERT R. COPLIEN	TLE	–	–	–	660
8	4.04	ST. PETER'S EVANGELICAL LUTHERAN CHURCH	TLE	–	–	–	6072
9	4.04 & .05	PIERCE REVOCABLE TRUST	TLE	–	–	–	2012
10	4.04 & .05	VINCE J. DEMMON	TLE	–	–	–	1576
11	4.05	JMS HOLDINGS LLC	TLE	–	–	–	2772
12	4.05	LARRY J. BOOHER	TLE	–	–	–	1190
13	4.05	BRUCE A. WAGNER	TLE	–	–	–	3189
14	4.05	KENNETH J. JOYCE	TLE	–	–	–	1329
15	4.06	GENE D. PRUDHON	TLE	–	–	–	924
16	4.06	MATTHEW A. BUSS	TLE	–	–	–	1056
17	4.06	WILLIAM S. & SUSAN F. KNIGHT REVOCABLE LIVING TRUST	TLE	–	–	–	1056
18	4.06	ERIC WOODRUFF	TLE	–	–	–	935
19	4.06	TIMOTHY J. ARN & LAURIE J. KETTLE	TLE	–	–	–	1026
20	4.06	EDWARD M. GRATTAN	TLE	–	–	–	750
21	4.06	NEWCOMER LIVING TRUST	TLE	–	–	–	290
22	4.06	RONALD W. PHILLIPS	TLE	–	–	–	536
23	4.03	DAVID J. & SANDRA ELSNER	TLE	–	–	–	1741
24	4.06	RANDALL KUNDERT	TLE	–	–	–	924
25	4.05	ROBERT A. & TAMMY L. POWELL	TLE	–	–	–	391
50	4.03	FRONTIER COMMUNICATIONS	TEMPORARY RELEASE OF RIGHTS				

NOTE:
R/W ESTABLISHED FROM ORIGINAL PLAT TO THE CITY OF BRODHEAD.

CITY OF BRODHEAD
SW/SW
SEC. 25-2-9

WEST 8TH AVENUE

EX. R/W

66'

LOT 8 | LOT 7 | LOT 6 | LOT 5

ORIGINAL PLAT, BLOCK 195

BLDG

① CAROL J. FOESCH

② KUHN KNIGHT INC.

50 FRONTIER COMMUNICATIONS

PL

15+24.93 33.0/41.0°

16+93.92 41.0

17+92.92 33.0/41.0°

TLE FOR GRADING

8'

EX. R/W

WEST 7TH AVENUE

EX. R/W

66'

LOT 8 | LOT 7 | LOT 6

ORIGINAL PLAT, BLOCK 194

BLDG

23 DAVID J. & SANDRA ELSNER

PL

⑤ CHRISTOPHER K & MICHELLE L. MCLAIN LOT 5 & PART LOT 6

18+58.91 33.0/43.0°

20+33.00 37.0/43.0°

TLE FOR GRADING

SLOPE INTERCEPT

10'

4'

IP

BRODHEAD WATER & LIGHT
CHARTER COMMUNICATIONS

CITY OF BRODHEAD

AMERICAN TRANSMISSION COMPANY

15+00 | 66' | 16+00 | 17+00 | 18+00 | 19+00 | 20+00

R/L = N 89° 34' 24" E

MATCH LINE STA 20+50

WEST 17TH STREET

WE ENERGIES

TLE FOR GRADING

BRODHEAD WATER & LIGHT

EX. R/W

IP

BEGIN RELOCATION ORDER
STA 15+00.00
Y = 141955.83
X = 680090.61

N26°36'53"E
753.99'

17+77.94 33.0/48.0°

17+92.94 33.0/48.0°

18+58.94 33.0/56.0°

19+90.94 43.0

10'

23'

BLDG

② KUHN KNIGHT INC.

③ CINDY L. WAGNER

④ SCOTT A. & SHAWN M. COOK

EX. R/W

EX. R/W

PL

BLDG

LOT 4 | LOT 3 | LOT 2 | LOT 1

ORIGINAL PLAT, BLOCK 211

LOT 4 | LOT 3 | LOT 2

ORIGINAL PLAT, BLOCK 210

T 02 N R 09 E
26 25
35 36

SEC. LINE =
N89°20'49"E 2646.61'

T 02 N R 09 E
25
36

ALUMINUM MONUMENT
Y = 141281.74
X = 679752.83

ALUMINUM MONUMENT
Y = 141311.90
X = 682399.27

REVISION DATE 1/2/2014 – N.C.	DATE 11/11/2013	SCALE, FEET 0 10 20 40	HWY: CTH GG	STATE R/W PROJECT NUMBER: 5911-00-01	PLAT SHEET NO: 4.03
	GRID FACTOR N/A		COUNTY: GREEN	CONSTRUCTION PROJECT NUMBER: 5911-00-70	PS&E SHEET NO. 36

NOTE:
R/W ESTABLISHED FROM ORIGINAL PLAT TO THE CITY OF BRODHEAD.

CITY OF BRODHEAD
SW/SW
SEC. 25-2-9

CITY OF BRODHEAD
SE/SW
SEC. 25-2-9

ORIGINAL PLAT, BLOCK 194

ORIGINAL PLAT, BLOCK 193

ORIGINAL PLAT, BLOCK 192

LOT 5 | LOT 8 | LOT 7 | LOT 6 | LOT 5 | LOT 8

BRODHEAD WATER & LIGHT

BLDG

⑤ CHRISTOPHER K & MICHELLE L. MCLAIN

⑥ TOMMY M. BUTTS & SUSAN A. COPLIEN

⑦ ROBERT R. COPLIEN

⑨ PIERCE REVOCABLE TRUST

EX. R/W

66'

21+21.00 33.0'/37.0'

21+87.00 33.0'/48.0'

23+19.00 38.0'/48.0'

24+51.00 33.0'/38.0'

26+17.00 33.0'/38.0'

PL

4'

15'

5'

IP

TLE FOR GRADING

BRODHEAD, WATER & LIGHT
CHARTER COMMUNICATIONS

FRONTIER COMMUNICATIONS

MATCH LINE STA 20+50

21+00 | 22+00 | 23+00 | 24+00 | 25+00 | 26+00

R/L = N 89° 39' 24" E

MATCH LINE STA 26+00

WEST 17TH STREET

CITY OF BRODHEAD

WE ENERGIES

STA 24+74.78

SLOPE INTERCEPT

23'

8'

10'

TLE FOR GRADING

PRIVATE SIGN (TO REMAIN)

TIE LINE = N00°55'08"W 666.36'

25+17.00 33.0'/41.0'

④ SCOTT A. & SHAWN M. COOK

⑧ ST. PETER'S EVANGELICAL LUTHERAN CHURCH

⑩ VINCE J. DEMMON

24+51.00 33.0'/56.0'

21+87.00 33.0'/56.0'

21+21.00 33.0'/43.0'

ØW

ØT

WEST 6TH AVENUE

WEST 5TH AVENUE

LOT 1 | LOT 4 | LOT 3 | LOT 2 | LOT 1 | LOT 4

ORIGINAL PLAT, BLOCK 210

ORIGINAL PLAT, BLOCK 209

ORIGINAL PLAT, BLOCK 208

T 2 N R 9 E
26 25
35 36

SEC. LINE = N89°20'49"E 1323.31'

SEC. LINE = N89°20'49"E 1323.31'

T 2 N R 9 E
25
36

ALUMINUM MONUMENT
Y = 141281.74
X = 679752.83

ALUMINUM MONUMENT
Y = 141311.90
X = 682399.27

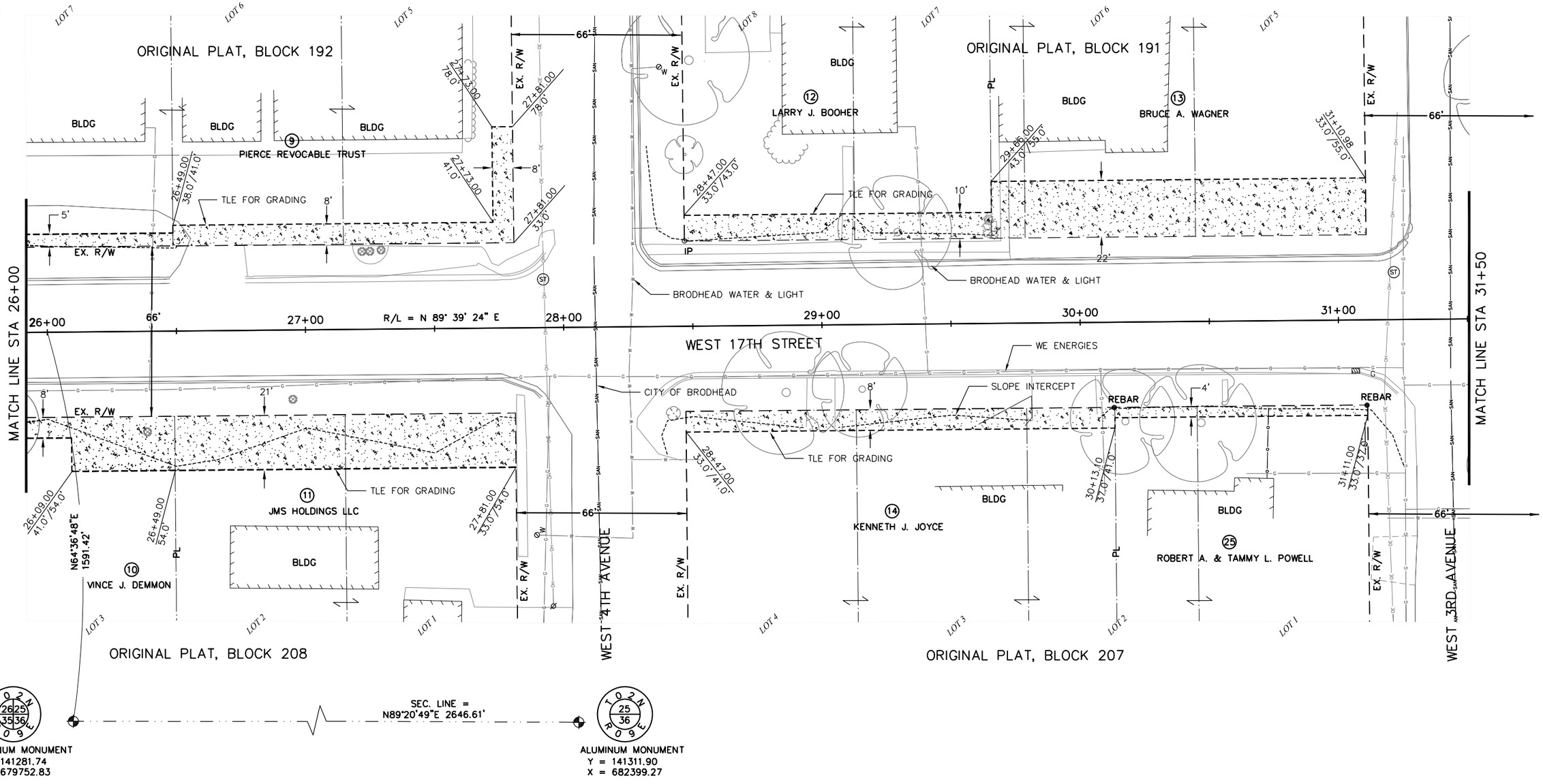
REVISION DATE 1/2/2014 – N.C.	DATE 11/11/2013	SCALE, FEET 0 10 20 40	HWY: CTH GG	STATE R/W PROJECT NUMBER: 5911-00-01	PLAT SHEET NO: 4.04
	GRID FACTOR N/A		COUNTY: GREEN	CONSTRUCTION PROJECT NUMBER: 5911-00-70	PS&E SHEET NO. 37 E

4

4

NOTE:
R/W ESTABLISHED FROM ORIGINAL PLAT TO THE CITY OF BRODHEAD.

CITY OF BRODHEAD
SE/SW
SEC. 25-2-9



SEC. LINE = N89°20'49"E 2646.61'

ALUMINUM MONUMENT
Y = 141281.74
X = 679752.83

ALUMINUM MONUMENT
Y = 141311.90
X = 682399.27

REVISION DATE 1/2/2014	DATE 11/11/2013	SCALE, FEET 0 10 20 40	HWY: CTH GG	STATE R/W PROJECT NUMBER: 5911-00-01	PLAT SHEET NO: 4.05
	GRID FACTOR N/A		COUNTY: GREEN	CONSTRUCTION PROJECT NUMBER: 5911-00-70	PS&E SHEET NO. 38 E

NOTE:
R/W ESTABLISHED FROM ORIGINAL PLAT TO THE CITY OF BRODHEAD.

CITY OF BRODHEAD
SE/SW
SEC. 25-2-9

ORIGINAL PLAT, BLOCK 190

ORIGINAL PLAT, BLOCK 189

WEST 3RD AVENUE

MATCH LINE STA 31+50

1ST CENTER AVENUE (STH 11)

WEST 17TH STREET

WEST 2ND AVENUE

LOT 8 LOT 7 LOT 6 LOT 5 LOT 4 LOT 5 LOT 7

66' EX. R/W

PL

BLDG

(15) GENE. D. PRUDHON

(24) RANDALL KUNDERT

(18) ERIC WOODRUFF

(21) NEWCOMER LIVING TRUST

(22) RONALD W. PHILLIPS

LOT 6

5'

8'

10'

31+77.00
33.0'/40.0'

33+09.00
40.0'

34+41.00
33.0'/40.0'

35+06.85
33.13'/42.13'

35+62.83
36.34'/43.34'

36+38.86
38.62'

36+96.85
38.84'/41.84'

37+63.83
34.09'/42.09'

TLE FOR GRADING

7'

EX. R/W

(ST) BRODHEAD WATER & LIGHT

PI: 34+71.75
Y = 141970.51
X = 682062.31

32+00 R/L = N 89° 39' 24" E 33+00 34+00 35+00 R/L = N 89° 47' 12" E 36+00 37+00

CITY OF BRODHEAD

WE ENERGIES

END RELOCATION ORDER
STA 37+70.00
Y = 141971.62
X = 682360.55

SLOPE INTERCEPT

8'

IP

REBAR

3'

6'

31+77.00
33.0'/41.0'

33+09.00
41.0'

34+41.00
33.0'/41.0'

35+07.17
32.87'/42.87'

35+97.17
35.53'/42.53'

36+39.15
35.38'/38.38'

37+64.15
31.91'/37.91'

TLE FOR GRADING

BLDG

(16) MATTHEW A. BUSS

(17) WILLIAM S. & SUSAN F. KNIGHT REVOCABLE LIVING TRUST

(19) TIMOTHY J. ARN & LAURIE J. KETTLE

(20) EDWARD M. GRATTAN

LOT 1

N03°21'32"W
660.86'

LOT 4 LOT 3 LOT 2 LOT 1 LOT 3 LOT 2 LOT 8

ORIGINAL PLAT, BLOCK 206

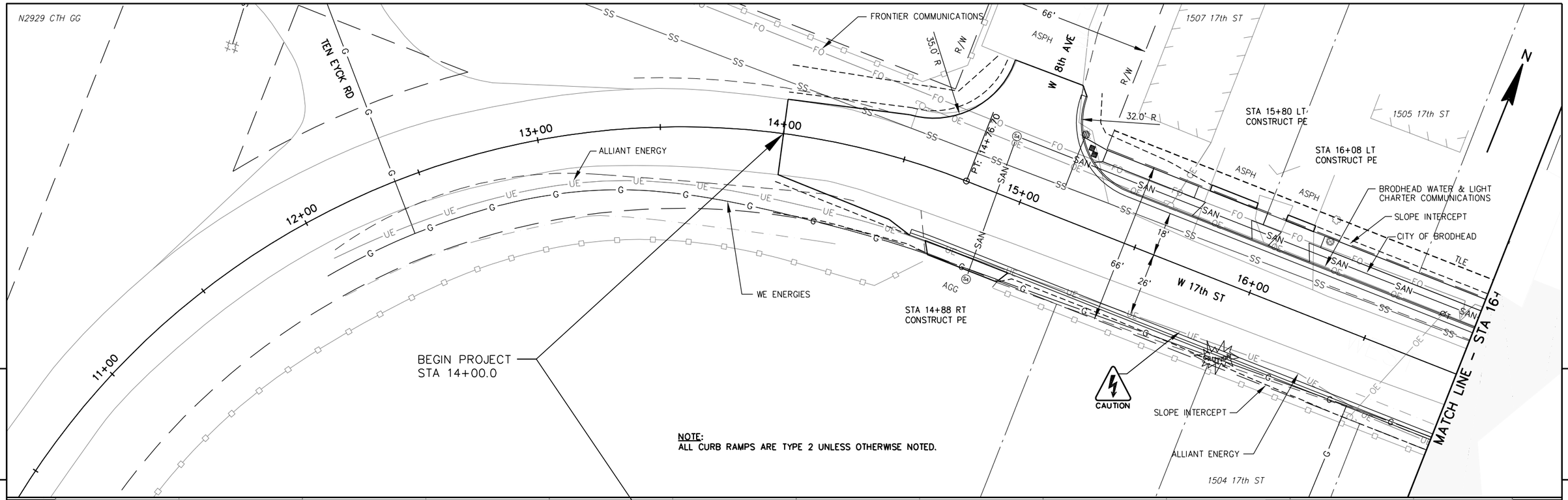
ORIGINAL PLAT, BLOCK 205

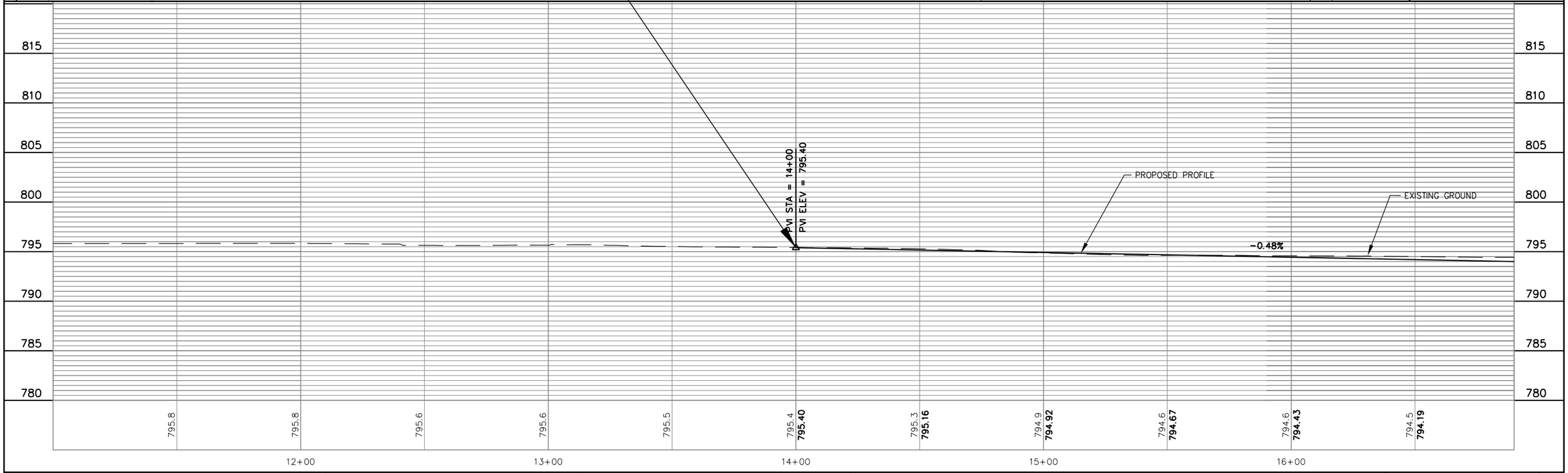
SEC. LINE =
N89°20'49"E 2646.61'

ALUMINUM MONUMENT
Y = 141281.74
X = 679752.83

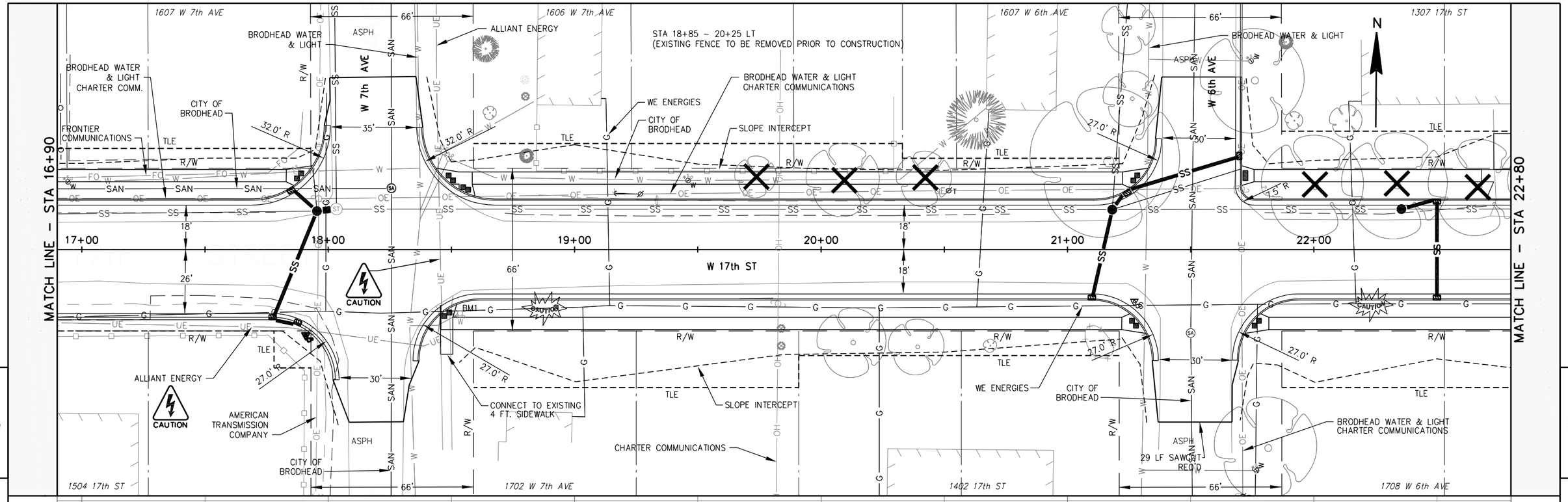
ALUMINUM MONUMENT
Y = 141311.90
X = 682399.27

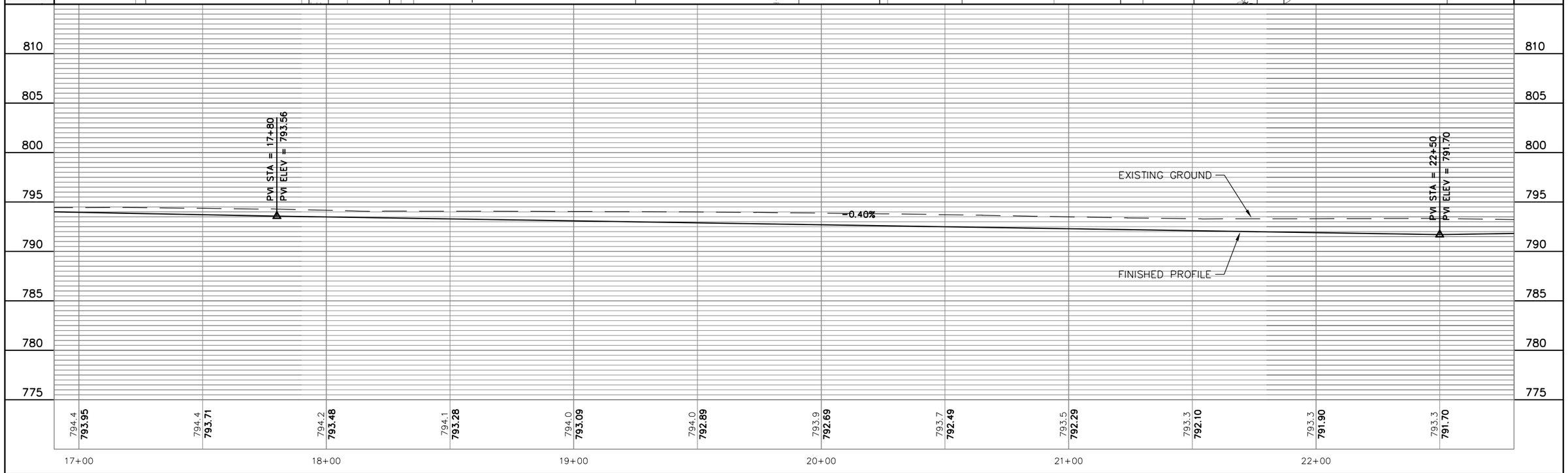
REVISION DATE 1/2/2014 – N.C.	DATE 11/11/2013	SCALE, FEET 0 10 20 40	HWY: CTH GG	STATE R/W PROJECT NUMBER: 5911-00-01	PLAT SHEET NO: 4.06
	GRID FACTOR N/A		COUNTY: GREEN	CONSTRUCTION PROJECT NUMBER: 5911-00-70	PS&E SHEET NO. 39 E





PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	PLAN AND PROFILE:	SHEET 40	E





PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	PLAN AND PROFILE:	SHEET 41	E

MATCH LINE – STA 22+80

MATCH LINE – STA 28+70

5

5

1208 16th ST

1201 17th ST

51 LF SAWCUT REQD

PRIVATE SIGN (TO BE REMOVED PRIOR TO CONSTRUCTION)

STA 26+66 LT CONSTRUCT PE

W 5th AVE

W 4th AVE

W 17th ST

CURB RAMP TYPE 1

SLOPE INTERCEPT

PRIVATE SIGN TO REMAIN

WE ENERGIES

CONNECT TO EXISTING 4 FT. SIDEWALK

FRONTIER COMMUNICATIONS

CHARTER COMMUNICATIONS

CITY OF BRODHEAD

BRODHEAD WATER & LIGHT

1708 W 6th AVE

1208 17th ST

1202 17th ST

23+00 24+00 25+00 26+00 27+00 28+00

PVI STA = 24+81
PVI ELEV = 792.65

PVI STA = 27+80
PVI ELEV = 791.30

0.41%

-0.45%

EXISTING GROUND

FINISHED PROFILE

793.2	791.91	793.2	792.11	793.0	792.32	792.9	792.52	792.9	792.56	792.8	792.34	792.7	792.11	792.6	791.89	792.5	791.66	792.4	791.44	792.6	791.38	792.7	791.58

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	PLAN AND PROFILE:	SHEET 42	E

MATCH LINE – STA 28+70

MATCH LINE – STA 34+60

1107 17th ST

1607 W 3rd AVE

1605 17th ST

1607 W 2nd AVE

W 3rd AVE

W 2nd AVE

W 17th ST

N

CONNECT TO EXISTING 4 FT. SIDEWALK

SLOPE INTERCEPT

SLOPE INTERCEPT

WE ENERGIES

CITY OF BRODHEAD

BRODHEAD WATER & LIGHT

BRODHEAD WATER & LIGHT
CHARTER COMMUNICATIONS

STA 29+00 RT
CONSTRUCT PE

STA 29+93 RT
CONSTRUCT PE

STA 33+75 RT
CONSTRUCT PE

1104 17th ST

1102 17th ST

1008 17th ST

1002 W 2nd AVE

66'

28'

30'

18'

99'

27.0' R

ASPH

TLE

R/W

G

SS

SAN

OHP

OE

W

CAUTION

29+00

30+00

31+00

32+00

33+00

34+00

PVI STA = 30+55
PVI ELEV = 792.40

PVI STA = 32+50
PVI ELEV = 791.62

0.40%

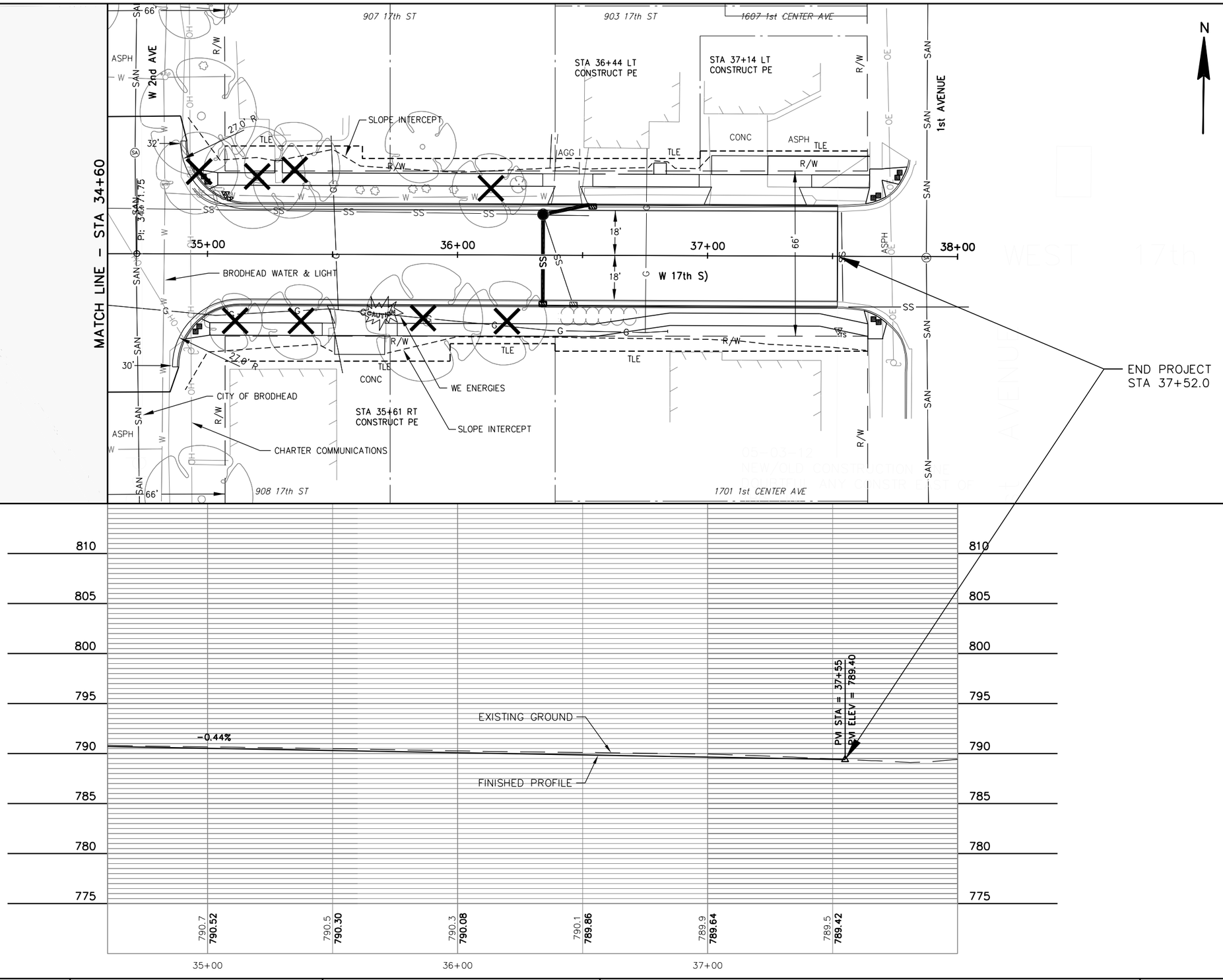
-0.40%

EXISTING GROUND

FINISHED PROFILE

	29+00	30+00	31+00	32+00	33+00	34+00
	792.8	792.7	792.8	792.7	792.6	792.5
	791.78	**791.98**	**792.18**	**792.38**	**792.22**	**792.02**
	792.4	792.2	791.9	791.5	791.1	790.9
	791.82	**791.62**	**791.40**	**791.18**	**790.96**	**790.74**

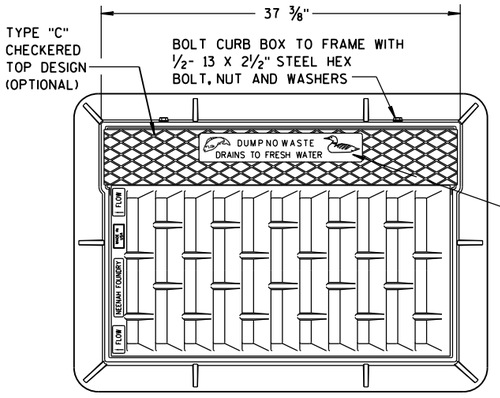
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	PLAN AND PROFILE:	SHEET 43	E



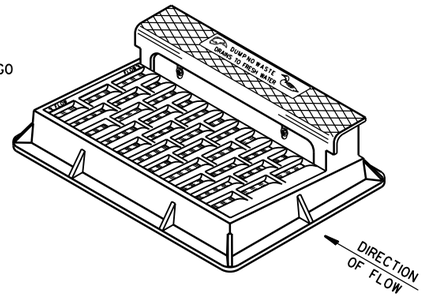
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	PLAN AND PROFILE:	SHEET 44	E

Standard Detail Drawing List

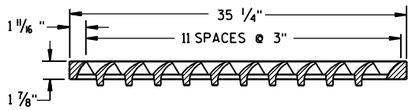
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-19B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08A05-19D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08B09-01	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER
08C06-01	INLETS 3-FT AND 4-FT DIAMETER
08C07-01	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08D01-17	CONCRETE CURB, CONCRETE CURB AND GUTTER AND TIES
08D05-15A	CURB RAMPS TYPES 1 AND 1-A
08D05-15B	CURB RAMPS TYPES 2 AND 3
08D05-15C	CURB RAMPS TYPES 4A AND 4A1
08D05-15D	CURB RAMPS TYPE 4B AND 4B1
08D05-15E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E14-01	TRACKING PAD
14A02-01	TREE PLANTING DETAIL
15C02-05A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-05B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C03-02	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C33-01	STOP LINE AND CROSSWALK PAVEMENT MARKING

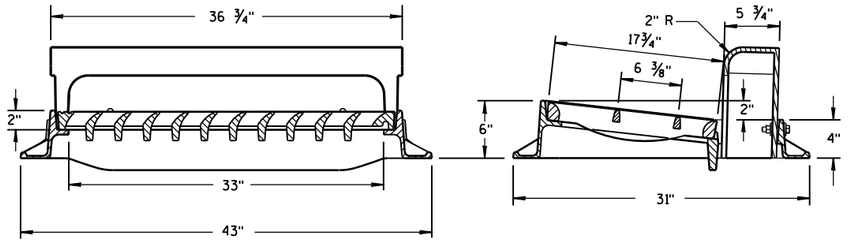


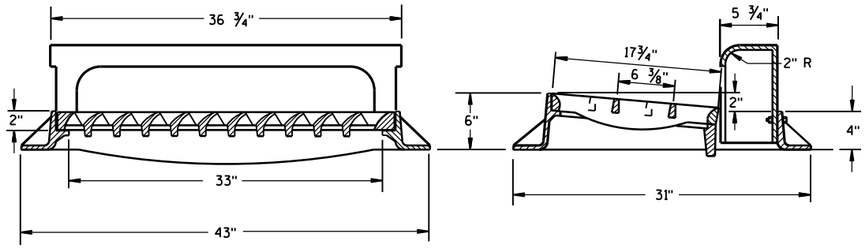
NOTE:
GRATE IS REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

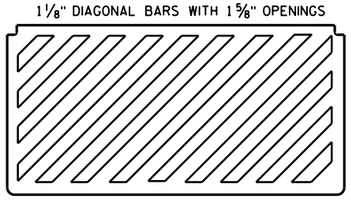






TYPE "H"

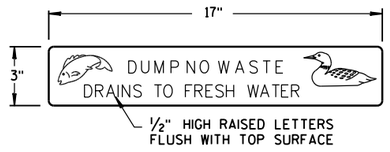
NOTE: EITHER CASTING IS ACCEPTABLE



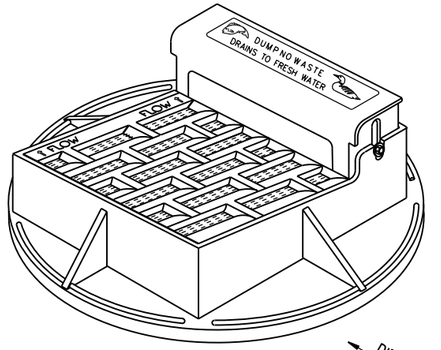
SPECIAL GRATE FOR TYPE "H" COVER

(MEASURES 35 1/4" X 17 3/4" X 2")

(NOTED AS TYPE H-S ON DRAINAGE TABLE)

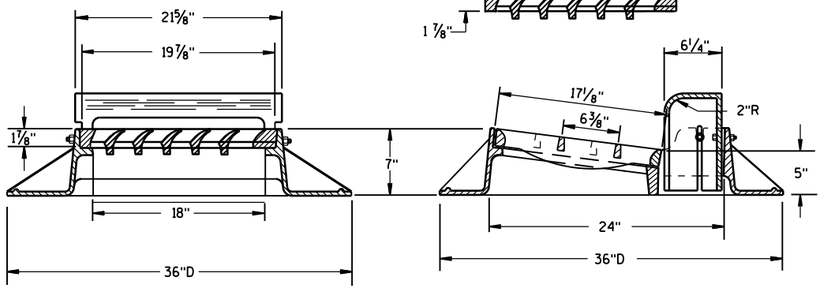


LOGO DETAIL



NOTE: CURB BOX ADJUSTABLE 4" TO 9"

NOTE:
GRATE IS REVERSIBLE.



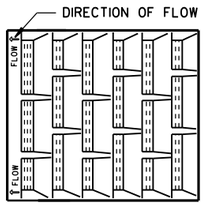
TYPE "A"

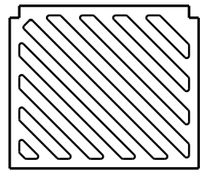
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

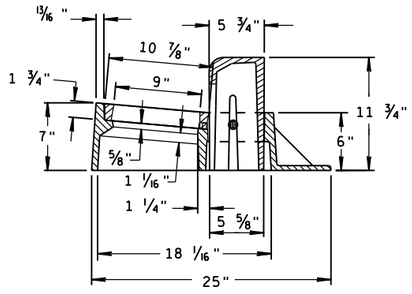


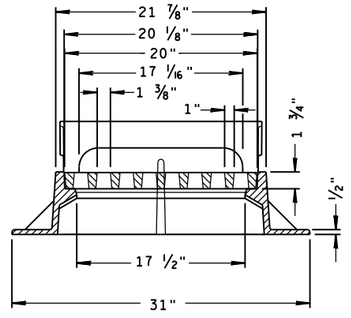


SPECIAL GRATE FOR TYPE "A" COVER

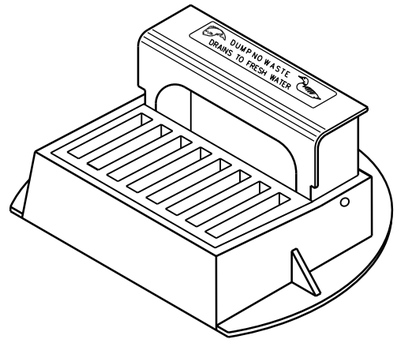
(MEASURES 19 3/4" X 17" X 1 7/8")

(NOTED AS TYPE A-S ON DRAINAGE TABLE)





TYPE "Z"



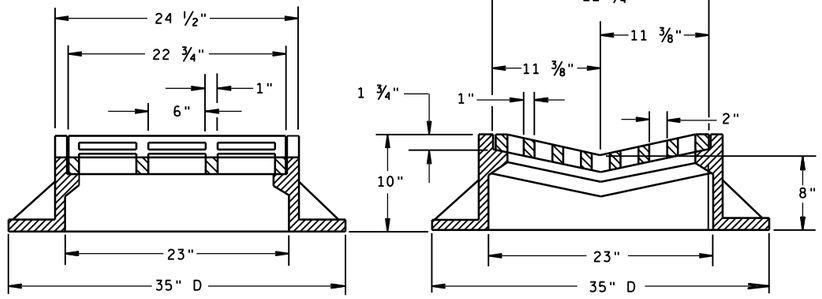
INLET COVERS
TYPE A, H, A-S, H-S & Z

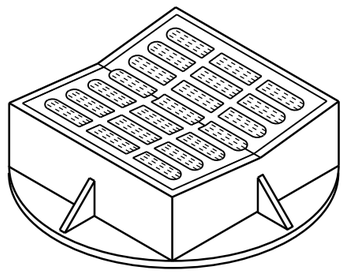
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11-27-13 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

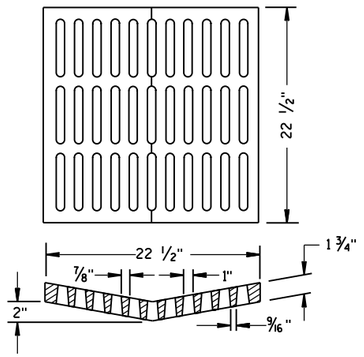
6

S.D.D. 8 A 5-19a



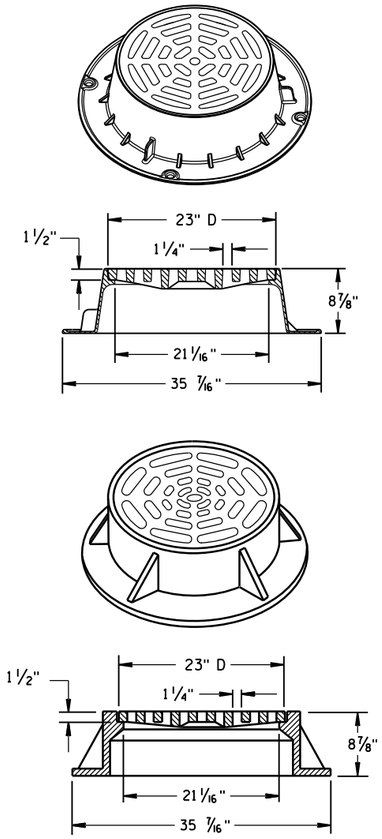


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

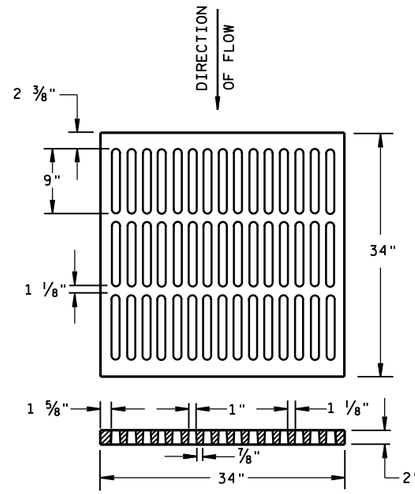
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

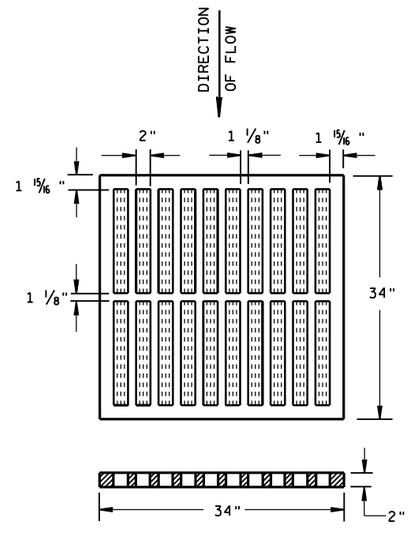
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



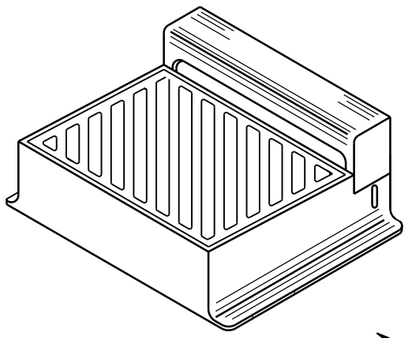
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE



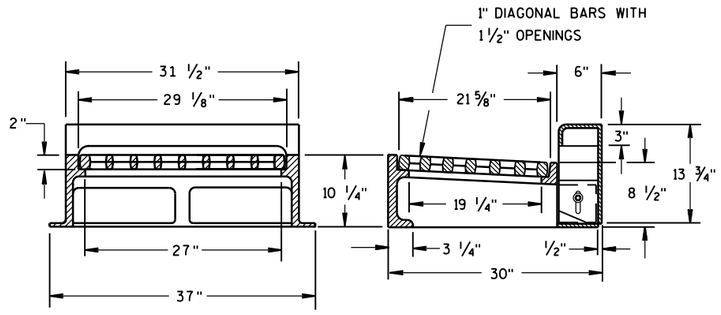
TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



DIRECTION OF FLOW

DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

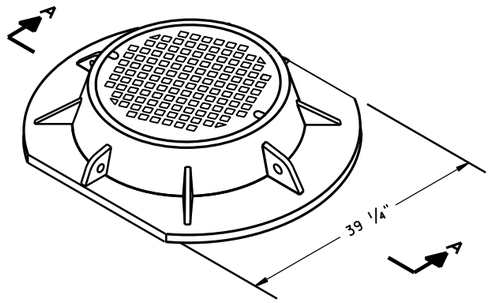
INLET COVERS
TYPE B, B-A, C,
MS, MS-A, & WM

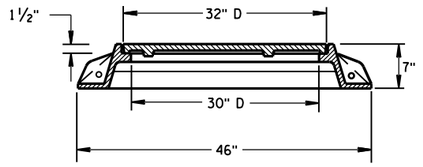
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/27/2013 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

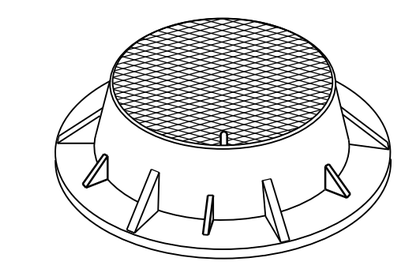
6

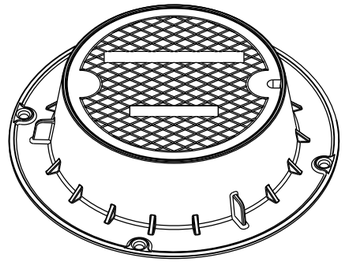
S.D.D. 8 A 5-19b

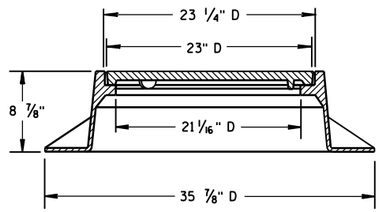


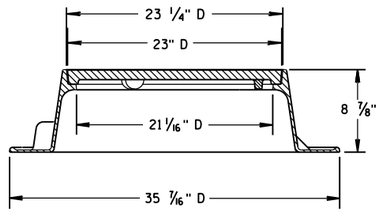


SECTION A-A
TYPE "K"









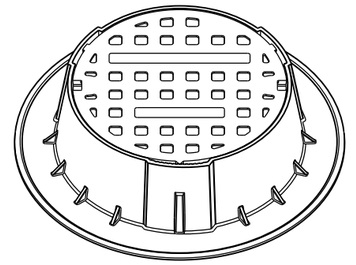
TYPE "J"
NOTE: EITHER CASTING IS ACCEPTABLE

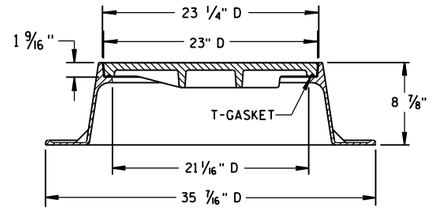
GENERAL NOTES

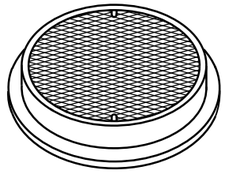
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

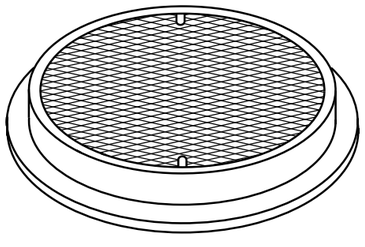
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

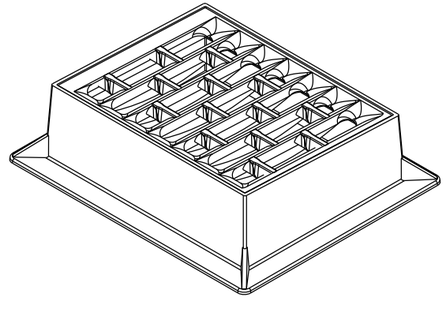
ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

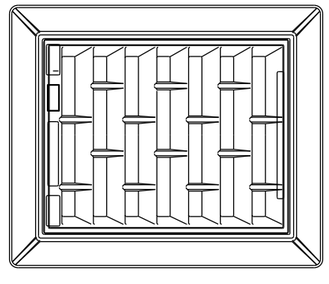


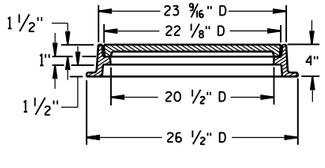




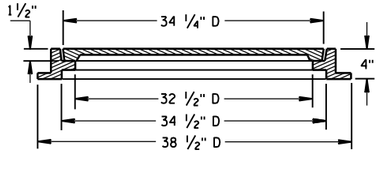




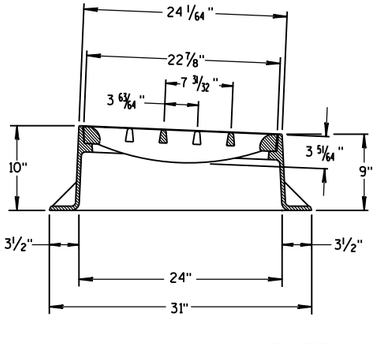


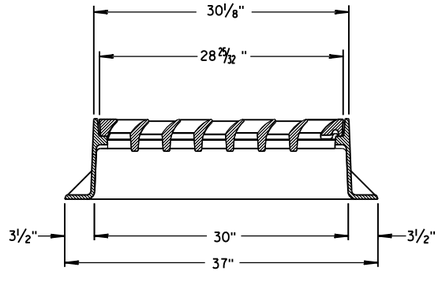


TYPE "L"

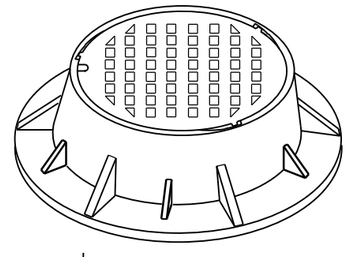


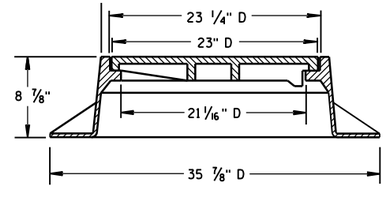
TYPE "M"





INLET COVER TYPE "BW"





TYPE "J" SPECIAL
TYPE "B" NON-ROCKING SELF-SEAL LID
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)
NOTE: EITHER CASTING IS ACCEPTABLE

INLET COVER TYPE BW
MANHOLE COVERS, TYPE K, J, J-S, L & M

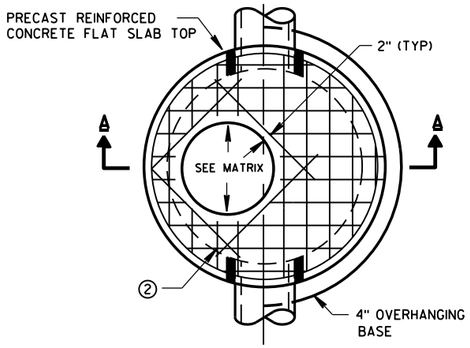
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/27/2013
DATE
FHWA

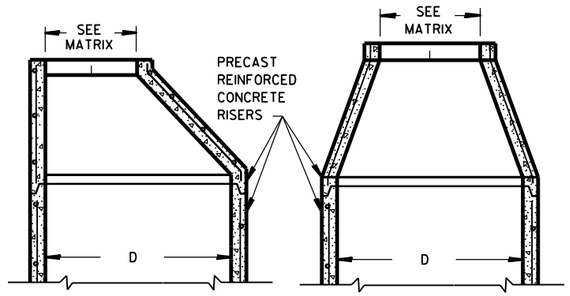
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER

6

S.D.D. 8 A 5-19d

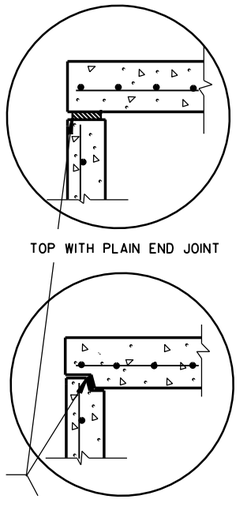


PLAN VIEW CIRCULAR OPENING



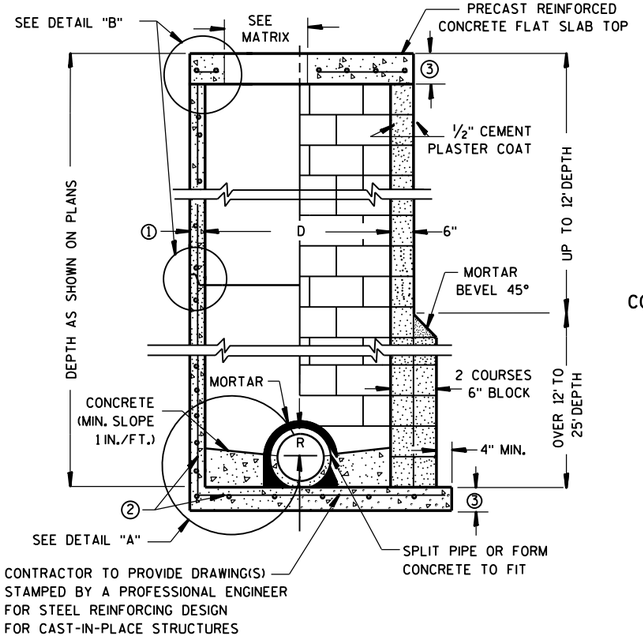
OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP

OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP



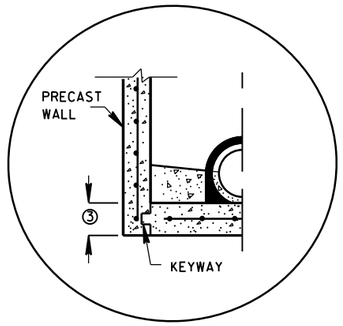
RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

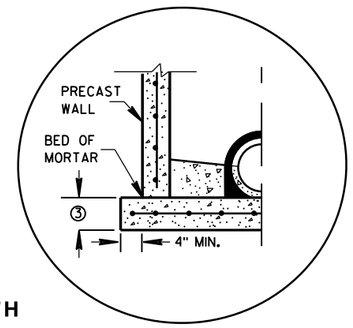


PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②

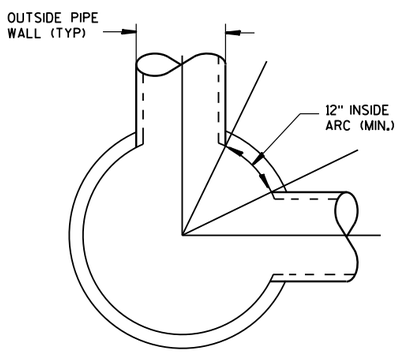


PRECAST REINFORCED
CONCRETE WITH INTEGRAL BASE OPTION



SEPARATE PRECAST REINFORCED
CONCRETE BASE OPTION

DETAIL "A"



DETAIL "C"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES. THE CONE TOPS SHALL BE INSTALLED ON A BED OF MORTAR.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES, AND CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF ½" AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

CONCRETE BLOCK WILL NOT BE PERMITED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M 199.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT, 5 INCHES FOR 4-FT, 6 INCHES FOR 5-FT, 7 INCHES FOR 6-FT, 8 INCHES FOR 7-FT AND 9 INCHES FOR 8-FT DIAMETER PRECAST MANHOLES.

② FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

③ PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS OF 8".

MANHOLE COVER OPENING MATRIX

MANHOLE COVER TYPE	C	ALL J'S	K	L	M
OPENING SIZE (FT)					
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

MANHOLE SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	36
7-FT	48	36
8-FT	60	42

MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER

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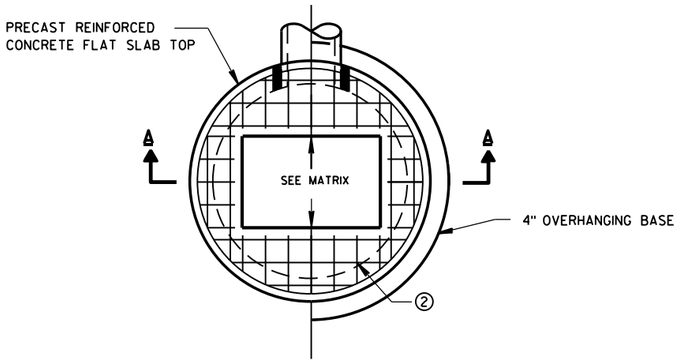
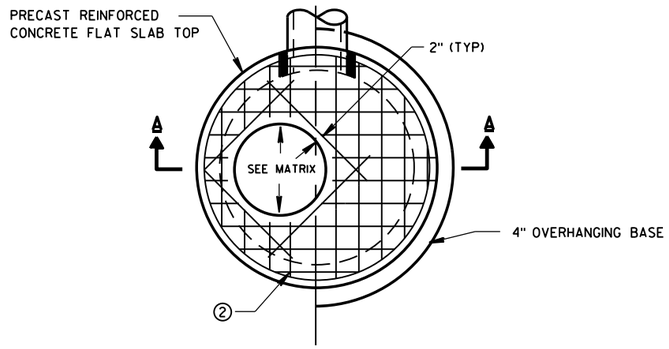
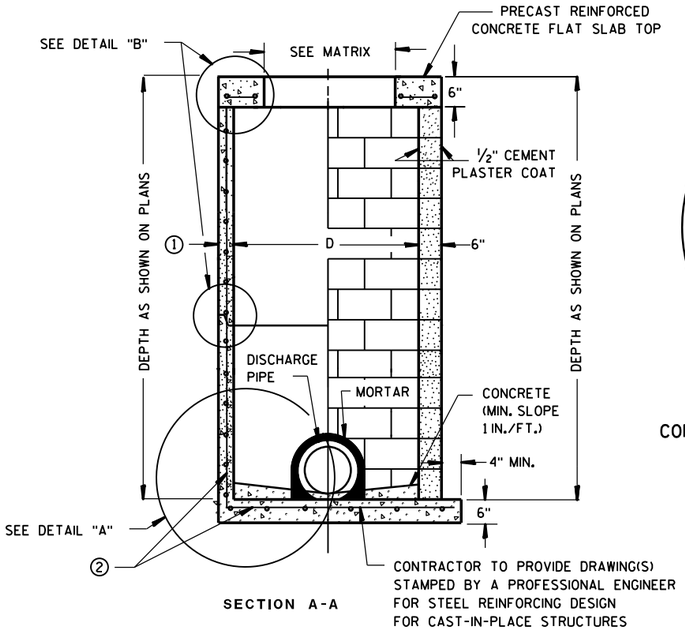


PLAN VIEW RECTANGULAR OPENING



PLAN VIEW CIRCULAR OPENING

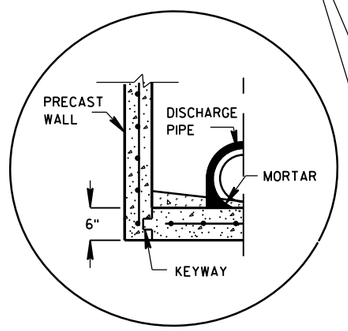


SECTION A-A

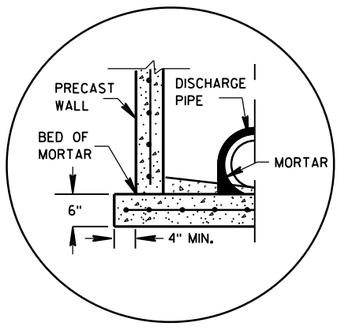
PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②

CIRCULAR INLETS W/ FLAT TOP

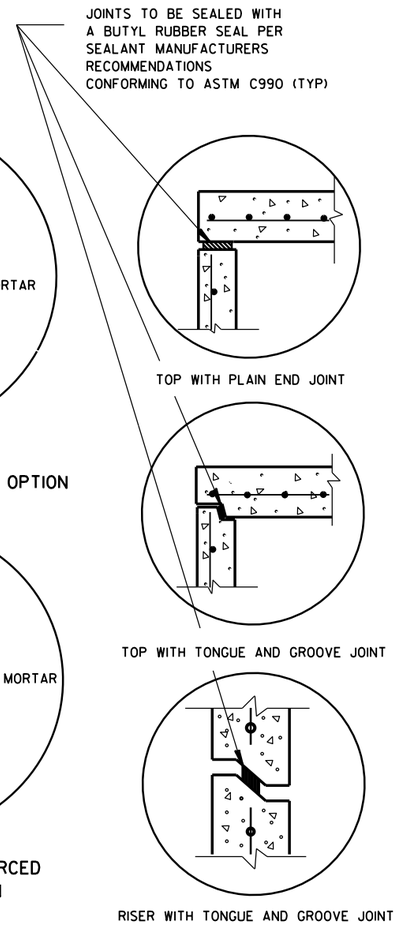


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

DETAIL "A"



DETAIL "B"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

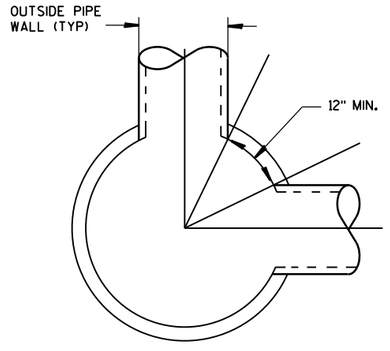
FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

① MINIMUM WALL THICKNESS SHALL BE 4-IN FOR 3-FT DIAMETER AND 5-IN FOR 4-FT DIAMETER PRECAST INLETS.

② FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

INLET COVER OPENING MATRIX

	INLET COVER TYPE	ALL A'S	ALL B'S	BW	C	F	ALL H'S	S	T	V	WM	Z
INLET SIZE	OPENING SIZE (FT)											
3-FT	2 DIA.				X							X
	2X2	X	X					X		X		
4-FT	2 DIA.				X							X
	2X2	X	X					X		X		
	2X2.5			X				X	X	X	X	
	2X3						X					
	2.5X3					X						



DETAIL "C"

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18

INLETS 3-FT AND 4-FT DIAMETER

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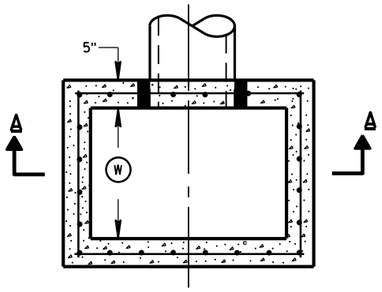
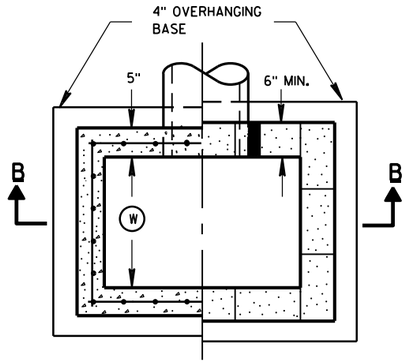
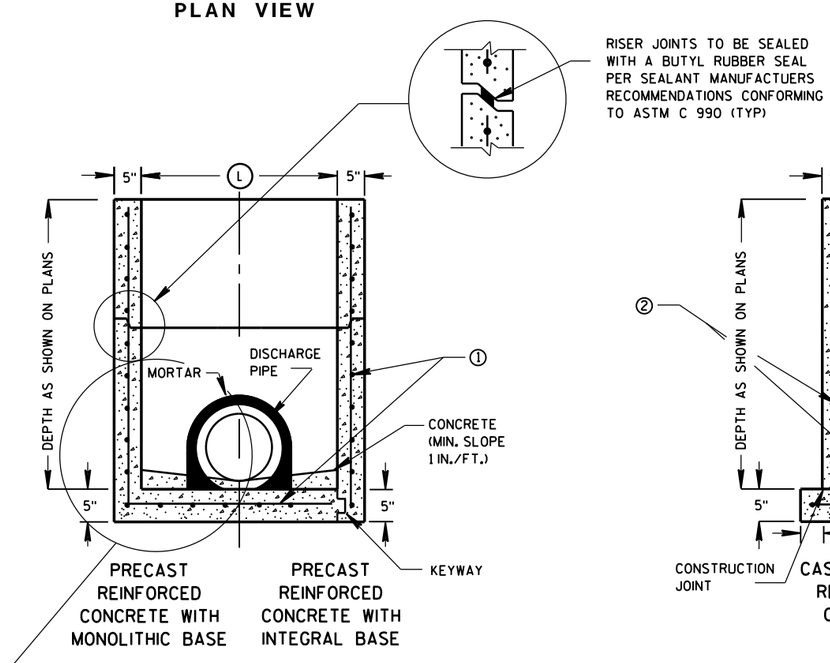


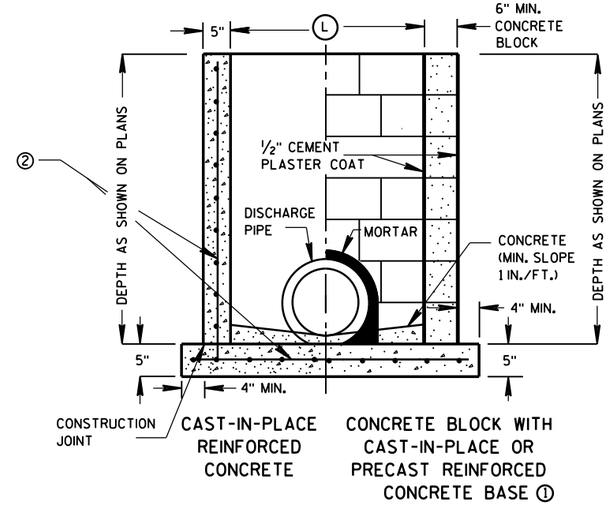
PLAN VIEW



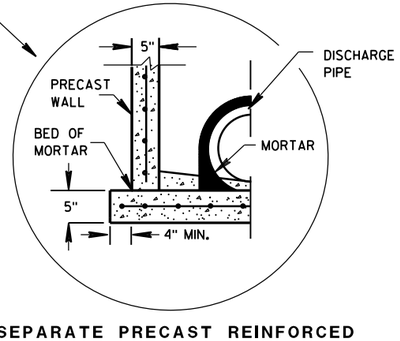
PLAN VIEW



SECTION A-A



SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS.
4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED.
OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

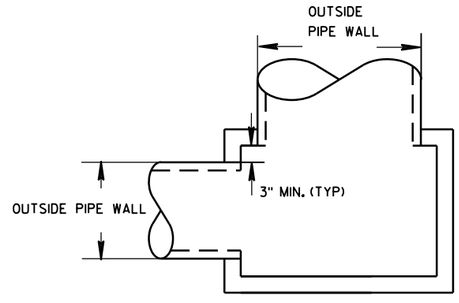
② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

INLET SIZE		INLET COVER TYPE	ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM
	WIDTH (W) (FT)	LENGTH (L) (FT)									
2X2-FT	2	2	X	X				X		X	
2X2.5-FT	2	2.5			X			X	X	X	X
2X3-FT	2	3					X				
2.5X3-FT	2.5	3				X					

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

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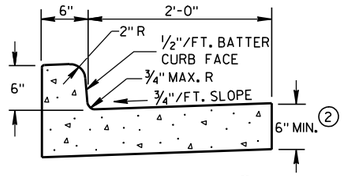
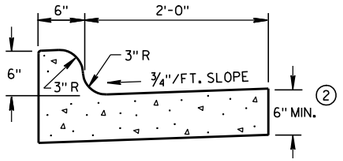
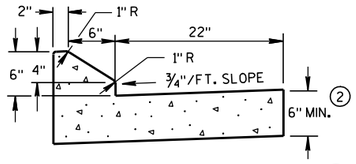


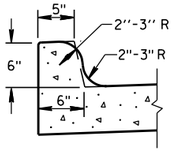
TYPES A & D (1)



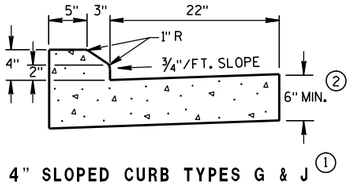
TYPES K & L (1)



6" SLOPED CURB TYPES G & J (1)

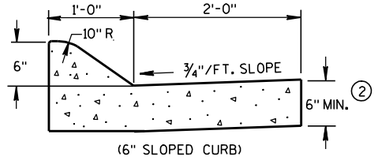


OPTIONAL CURB SHAPE FOR TYPES K & L (1)

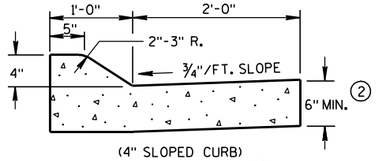


4" SLOPED CURB TYPES G & J (1)

CONCRETE CURB & GUTTER 30"

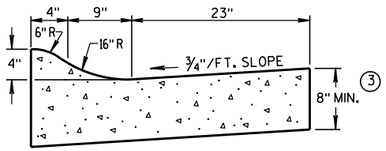


(6" SLOPED CURB)



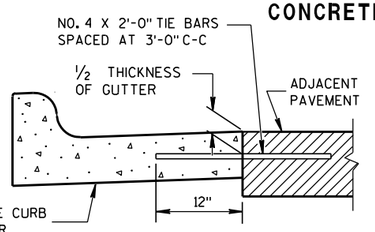
(4" SLOPED CURB)

TYPES A & D (1)

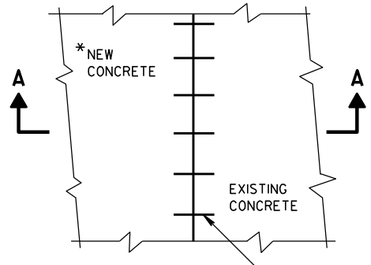


4" SLOPED CURB TYPES R & T (1) (4)

CONCRETE CURB & GUTTER 36"

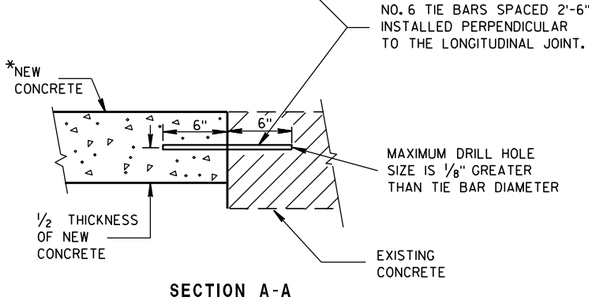


TYPICAL TIE BAR LOCATION (1)



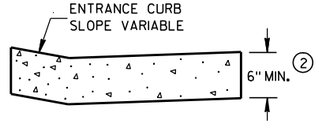
*NEW CURB & GUTTER, SURFACE DRAINS, CONCRETE PAVEMENT OR OTHER NEW CONCRETE.

PLAN VIEW

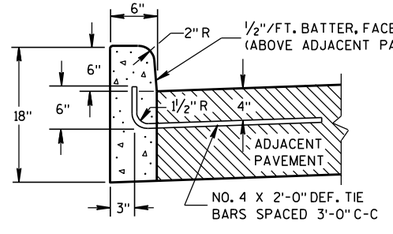


SECTION A-A

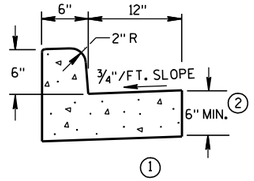
TIE BARS DRILLED INTO EXISTING PAVEMENT



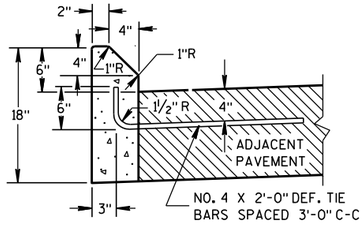
DRIVEWAY ENTRANCE CURB
(WHEN DIRECTED BY THE ENGINEER)



(1)
TYPES A & D

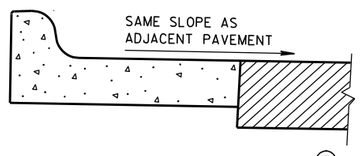


(1)
TYPES A & D
CONCRETE CURB & GUTTER 18"



(1)
TYPES G & J

CONCRETE CURB



REVERSE SLOPE GUTTER (5)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE. A LONGITUDINAL CONSTRUCTION JOINT IS NOT REQUIRED WITH INTEGRAL CURB AND GUTTER.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

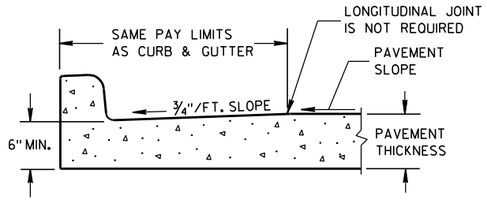
(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K AND R.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

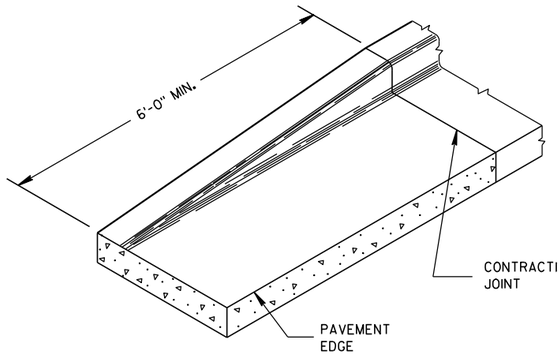
(3) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(4) THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

(5) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB & GUTTER



END SECTION CURB & GUTTER

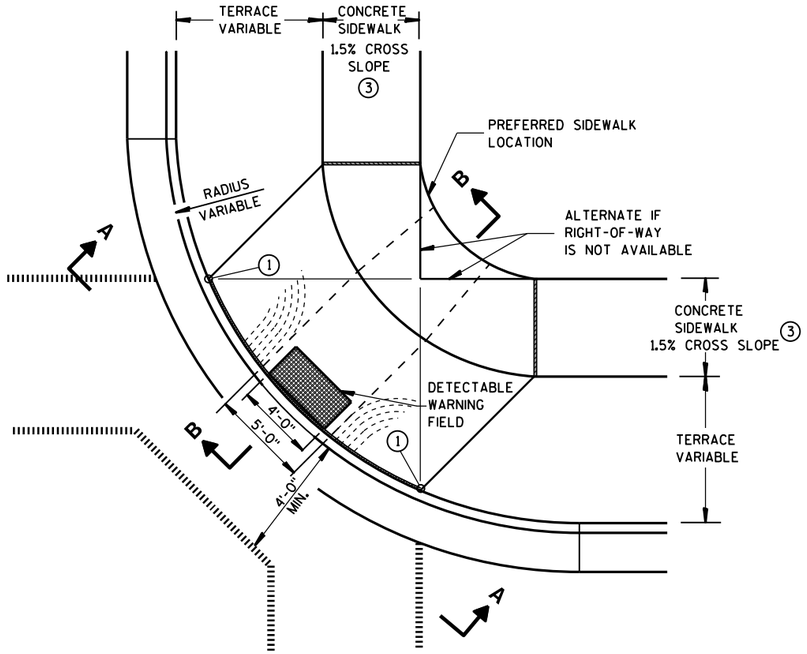
CONCRETE CURB, CONCRETE CURB & GUTTER AND TIES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
9/4/08 DATE
/S/ Jerry H. [illegible]
ROADWAY STANDARDS ENGINEER
FHWA

6

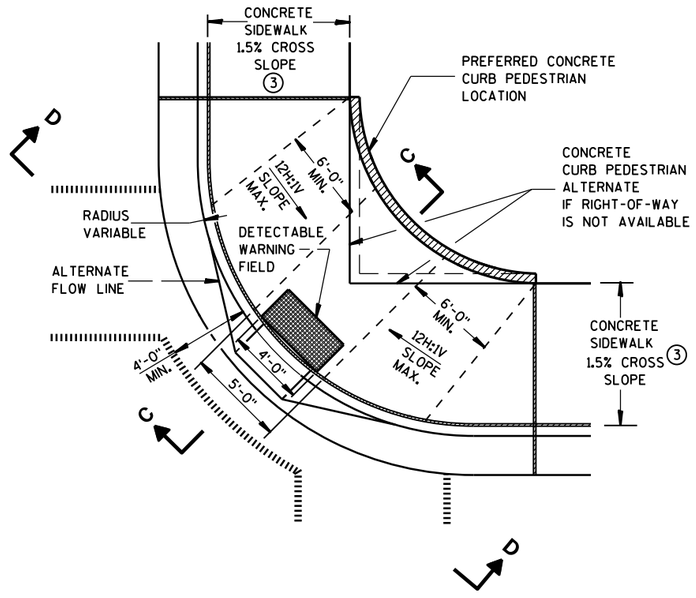
S.D.D. 8 D 1-17



PLAN VIEW

TYPE 1 RAMP

(CENTER OF CORNER RADIUS)



PLAN VIEW

TYPE 1-A RAMP

(NO TERRACE)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

RAMPS SHALL BE BUILT AT 12H:1V OR FLATTER. WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAL FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD".

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

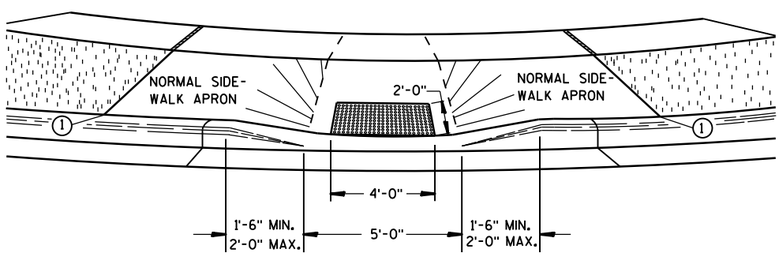
① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB.

② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE.

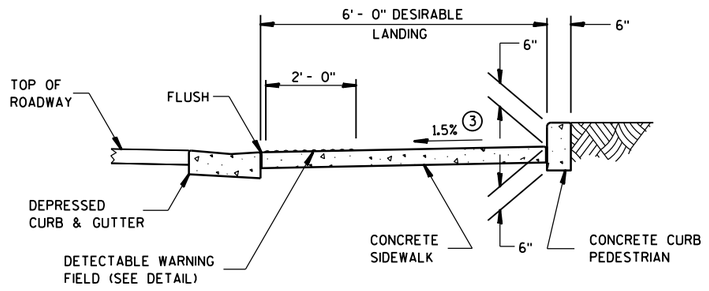
③ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

LEGEND

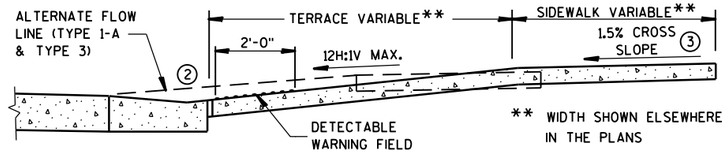
- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



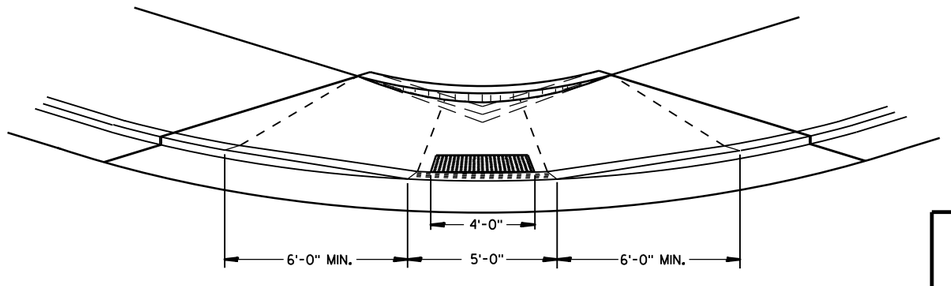
VIEW A-A



SECTION C-C



SECTION B-B



VIEW D-D

CURB RAMPS TYPES 1 AND 1-A

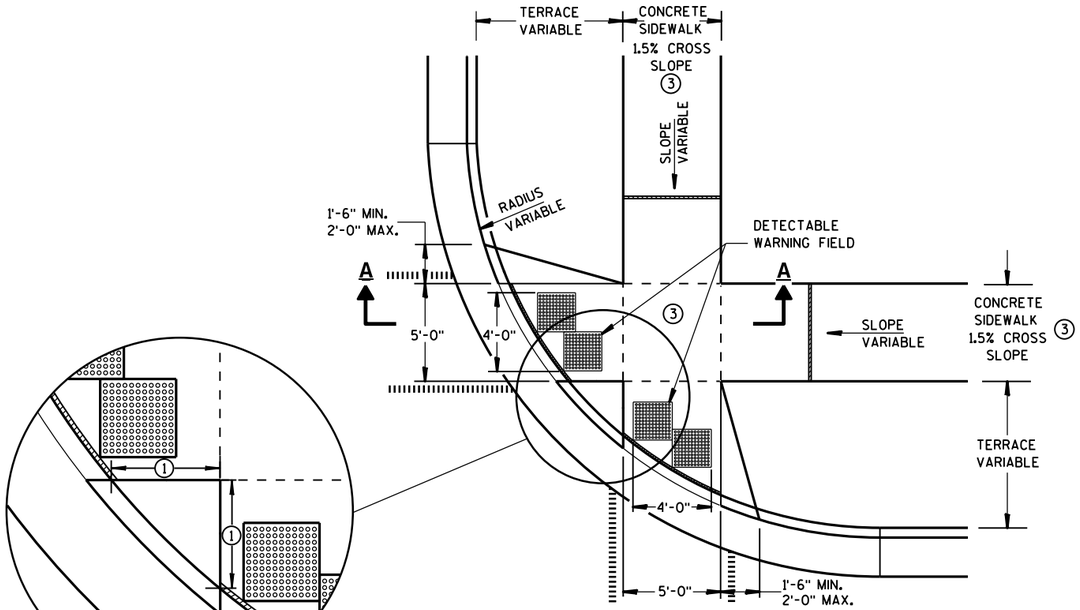
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

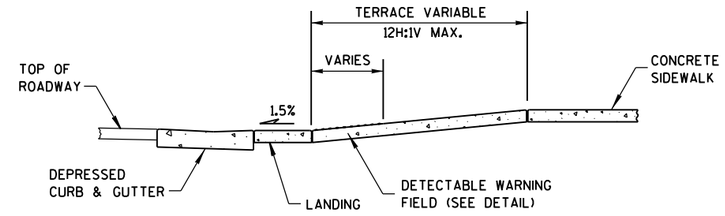
S.D.D. 8 D 5-15a

6

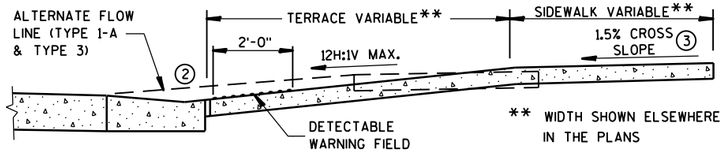
S.D.D. 8 D 5-15a



PLAN VIEW
TYPE 2 RAMP
(ON LINE WITH SIDEWALK)



SECTION A-A



SECTION B-B

GENERAL NOTES

USE THE TYPE 3 RAMP ONLY WHEN A TYPE 1 OR TYPE 2 CANNOT BE ACHIEVED BECAUSE OF FIELD CONDITIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

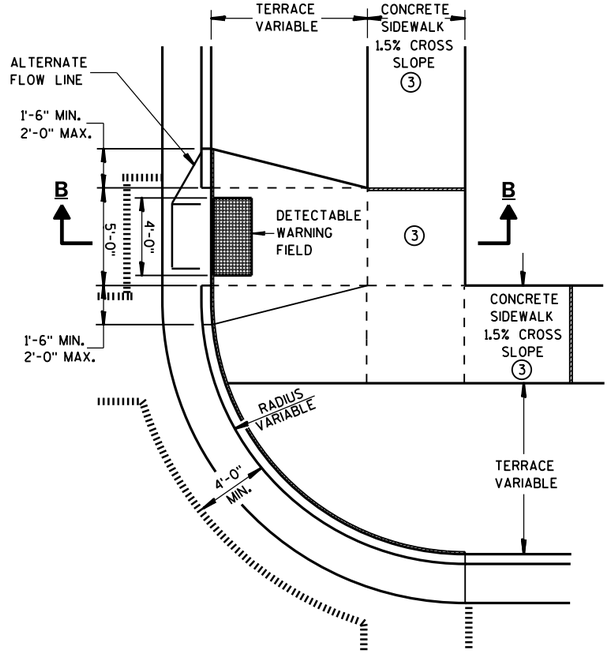
① WHEN THIS DISTANCE IS LESS THAN 6'-0" IT MAY BE DIFFICULT TO ACHIEVE A 12H:1V SLOPE, OR FLATTER, ON THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 12H:1V SLOPE, OR FLATTER, ON RAMP. 2" MINIMUM CURB HEIGHT.

② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE.

③ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



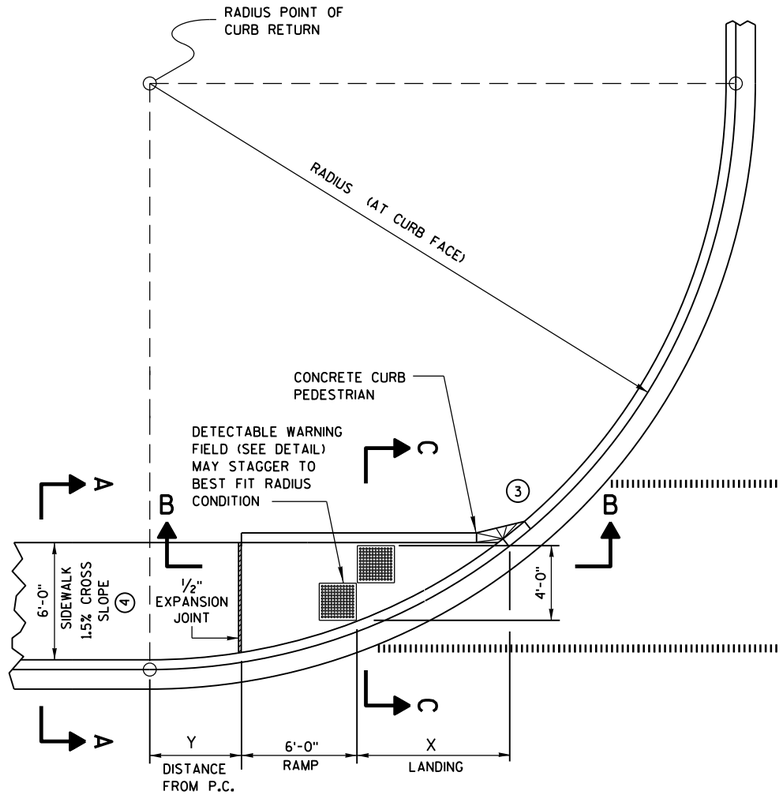
PLAN VIEW
TYPE 3 RAMP
(OUTSIDE OF CROSSWALK AREA)

CURB RAMPS
TYPES 2 AND 3

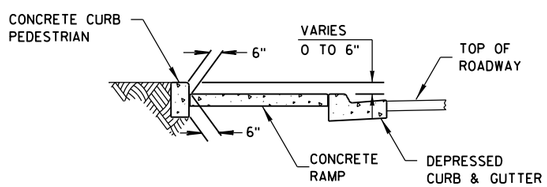
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

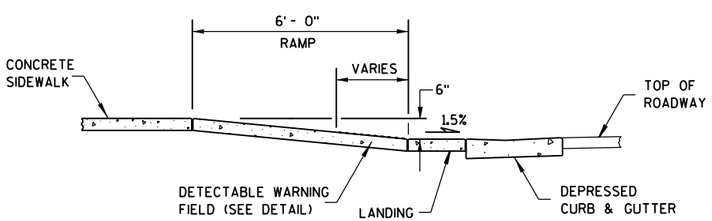
S.D.D. 8 D 5-15b



CURB RAMP TYPE 4A
PLAN VIEW



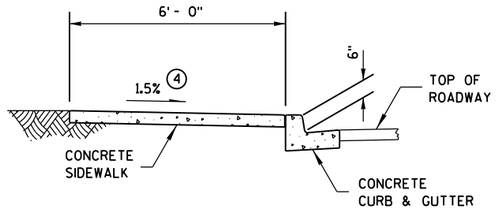
SECTION C-C FOR TYPE 4A



SECTION B-B FOR TYPE 4A

RADIUS (AT CURB FACE)	X	Y
20 FEET	6'-1¾"	2'-7¼"
30 FEET	7'-11¾"	4'-8¼"
40 FEET	9'-5¼"	6'-5"
50 FEET	10'-8¾"	7'-11¼"
60 FEET	11'-10¼"	9'-3½"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A-A FOR TYPE 4A

GENERAL NOTES

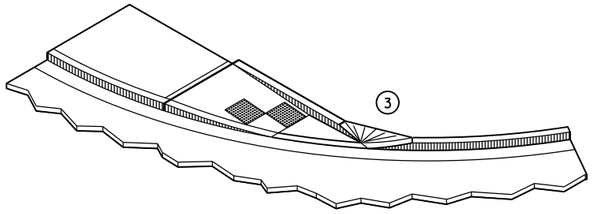
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

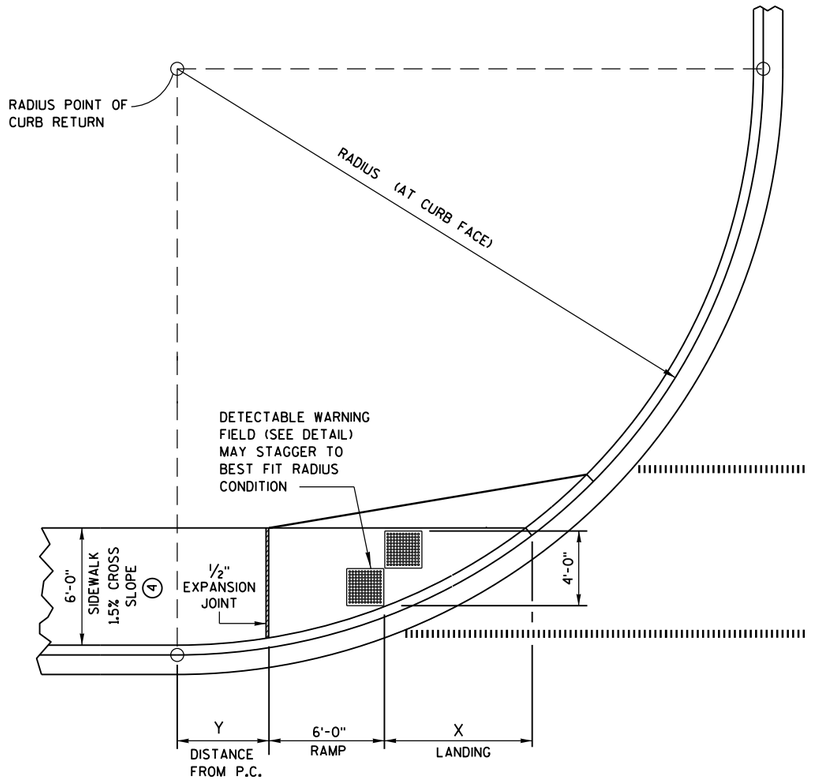
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

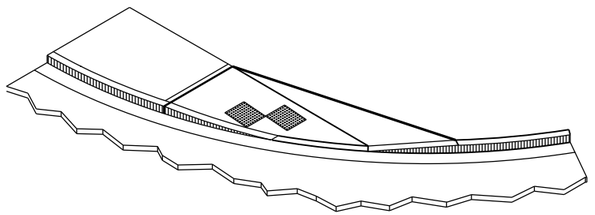
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



ISOMETRIC VIEW FOR TYPE 4A



CURB RAMP TYPE 4A1
PLAN VIEW



ISOMETRIC VIEW FOR TYPE 4A1

LEGEND

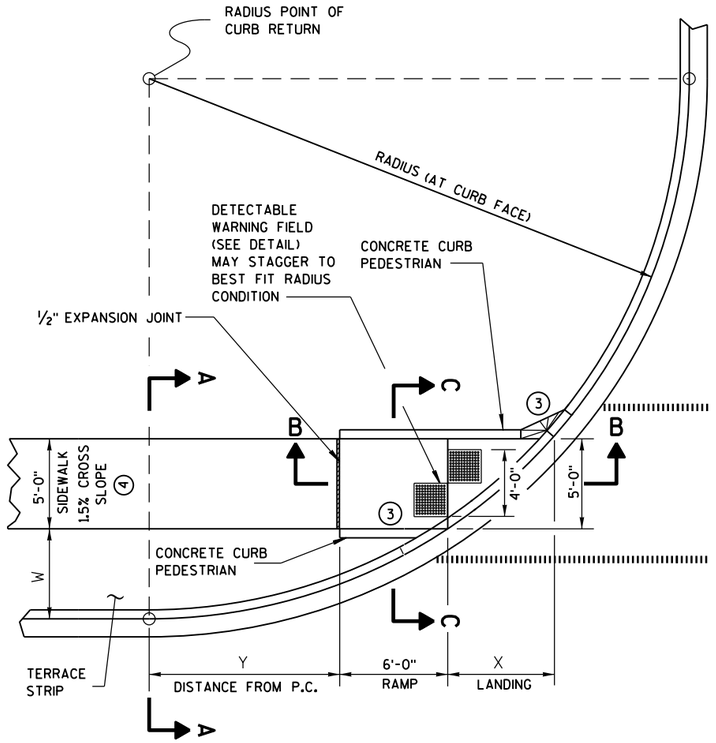
- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPES 4A AND 4A1

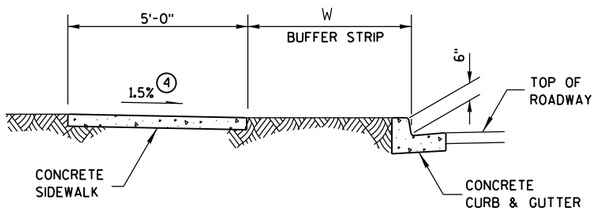
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

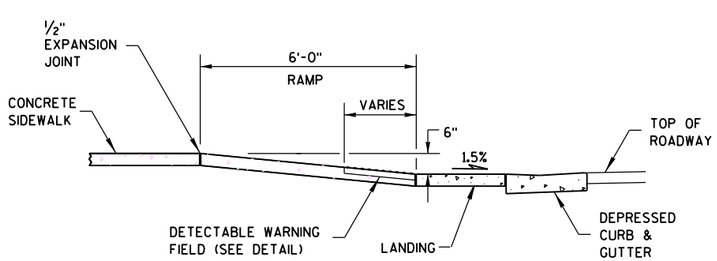
S.D.D. 8 D 5-15c



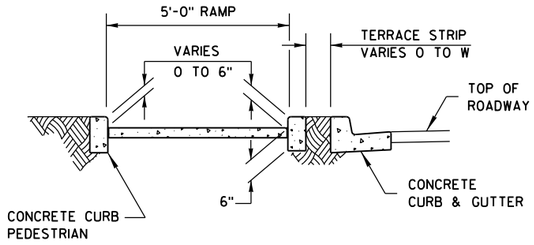
CURB RAMP TYPE 4B
PLAN VIEW



SECTION A-A FOR TYPE 4B



SECTION B-B FOR TYPE 4B



SECTION C-C FOR TYPE 4B

LEGEND

½" EXPANSION JOINT-SIDEWALK

CONTRACTION JOINT FIELD LOCATED

PAVEMENT MARKING CROSSWALK (WHITE)

RADIUS (AT CURB FACE)	W = 3'-0"		W = 4'-0"		W = 5'-0"		W = 6'-0"		W = 7'-0"	
	X	Y	X	Y	X	Y	X	Y	X	Y
20 FEET	5'-5½"	4'-6½"	4'-8½"	6'-0"	4'-1"	7'-2¾"	3'-7"	8'-3½"	3'-1½"	9'-2½"
30 FEET	7'-3¾"	7'-1"	6'-5½"	8'-11½"	5'-9¼"	10'-7"	5'-2½"	12'-0"	4'-8¾"	13'-3¼"
40 FEET	8'-9½"	9'-2½"	7'-10"	11'-5¼"	7'-1"	13'-4½"	6'-5¾"	15'-¾"	5'-11½"	16'-7¼"
50 FEET	10'-¾"	11'-¾"	9'-¼"	13'-7¼"	8'-2½"	15'-9½"	7'-6½"	17'-9"	6'-11¾"	19'-6¼"
60 FEET	11'-2½"	12'-8¾"	10'-¾"	15'-6½"	9'-2¼"	17'-11¾"	8'-5¾"	20'-1¾"	7'-10½"	22'-1½"

INTERMEDIATE RADII CAN BE INTERPOLATED

GENERAL NOTES

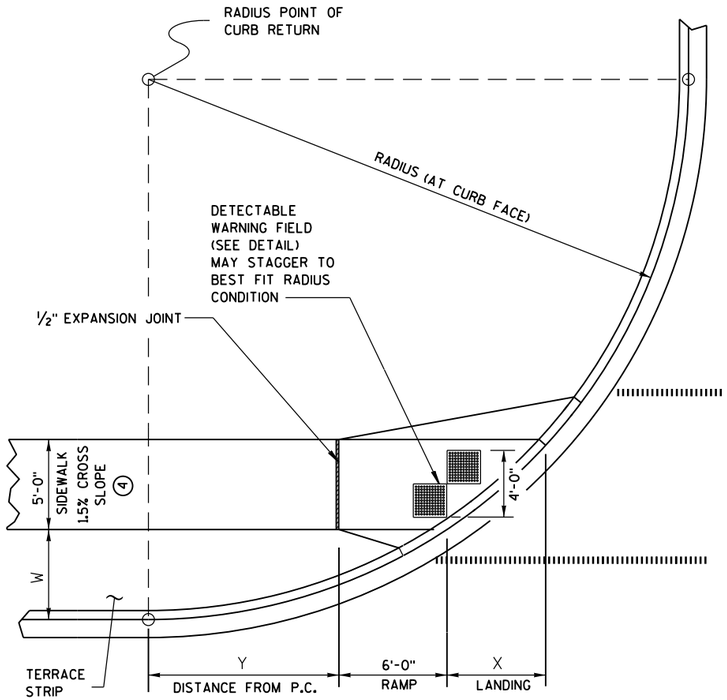
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

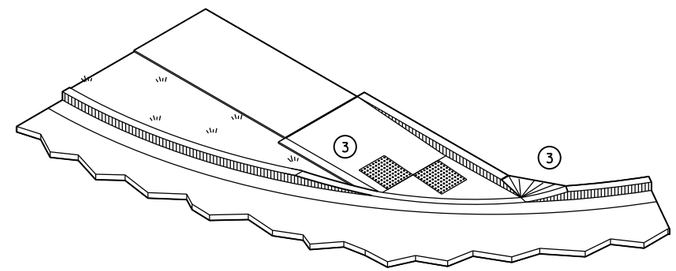
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

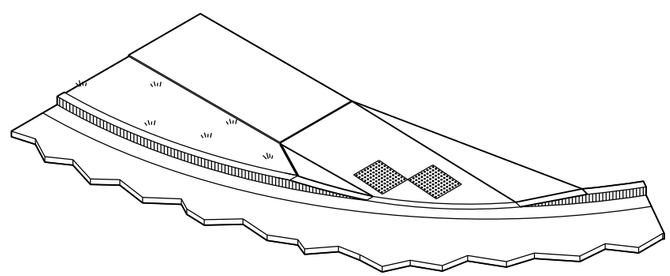
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



CURB RAMP TYPE 4B1
PLAN VIEW



ISOMETRIC VIEW FOR TYPE 4B



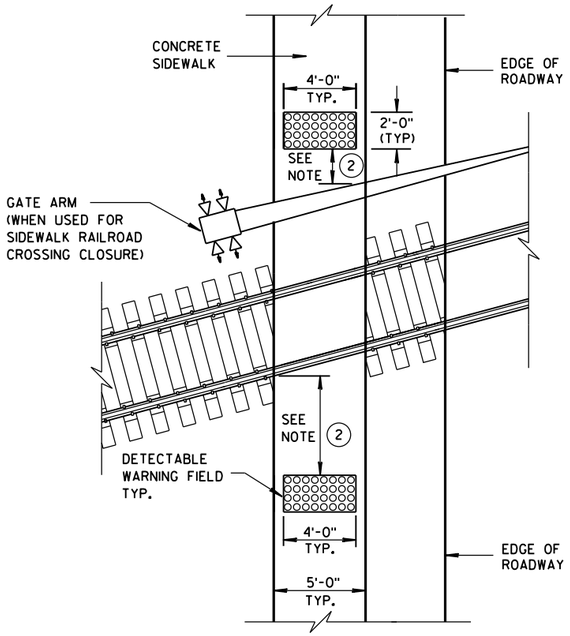
ISOMETRIC VIEW FOR TYPE 4B1

CURB RAMPS TYPE 4B AND 4B1

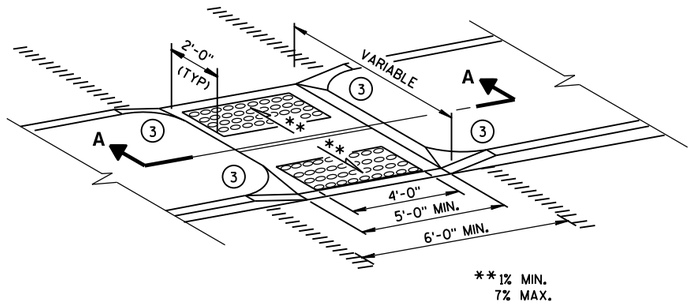
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 8 D 5-15d



TYPE 8
DETECTABLE WARNINGS AT RAILROAD CROSSING



MEDIAN ISLAND
NON-ELEVATED CROSSING
TYPE 5

GENERAL NOTES

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

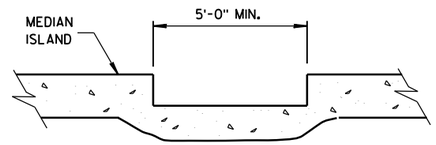
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(1) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

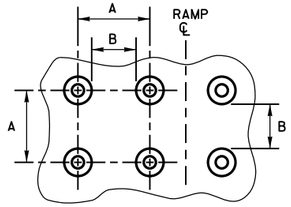
(2) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ± 0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



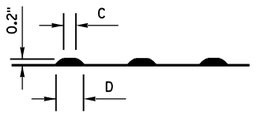
SECTION A-A



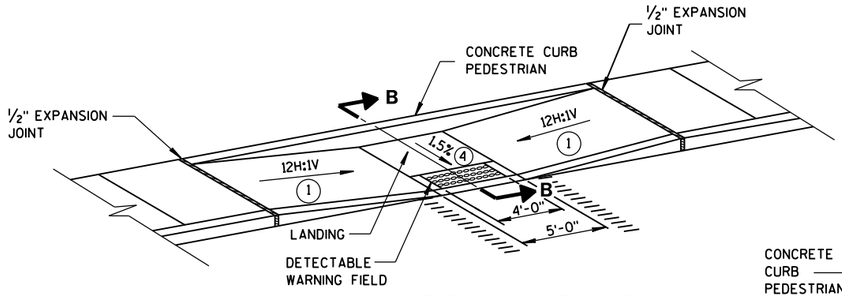
PLAN VIEW

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

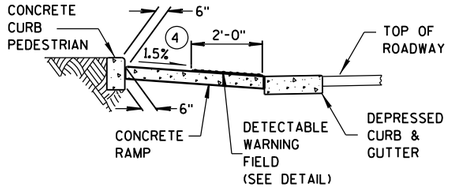
* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.



ELEVATION VIEW

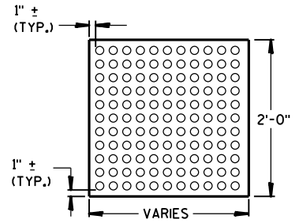


MID-BLOCK CROSSING
TYPE 7A



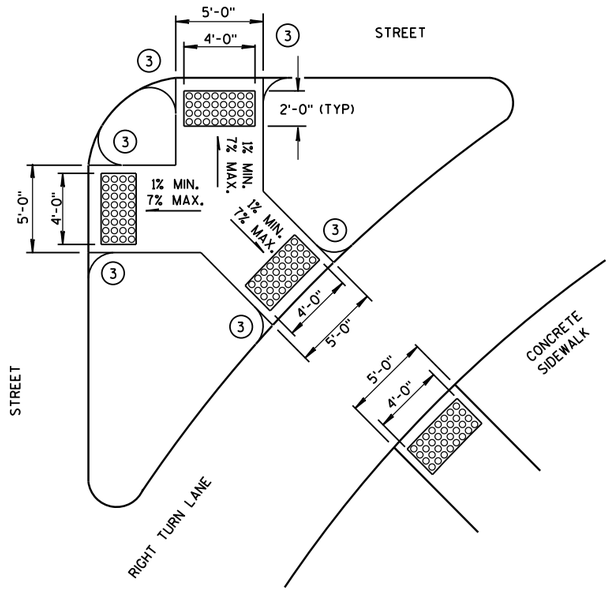
SECTION B-B

TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL

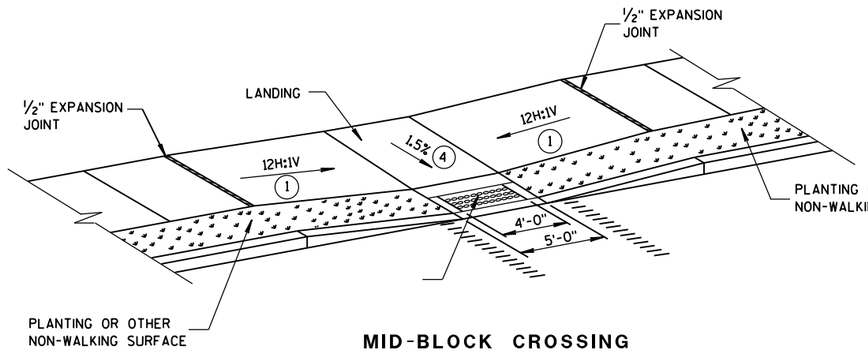


PLAN VIEW

DETECTABLE WARNING FIELD (TYPICAL)



TYPE 6
DETECTABLE WARNING AT ISLANDS



MID-BLOCK CROSSING
TYPE 7B

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPES 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

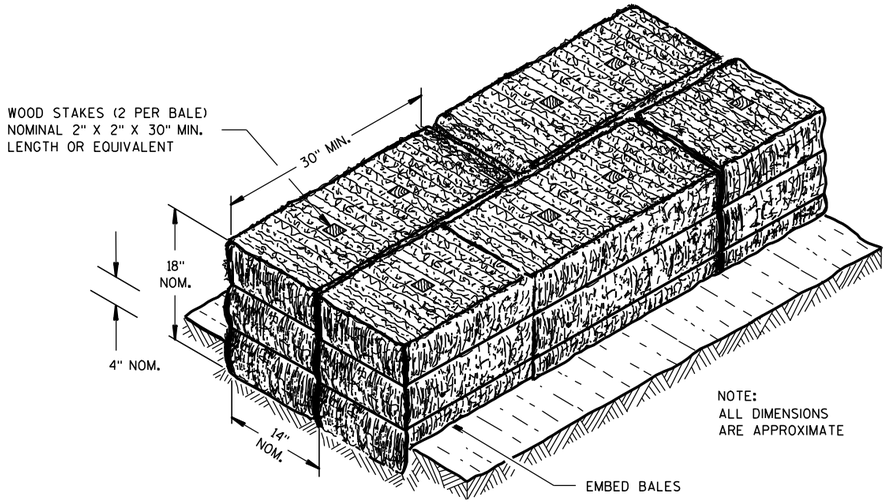
APPROVED
2-6-2013 DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

6

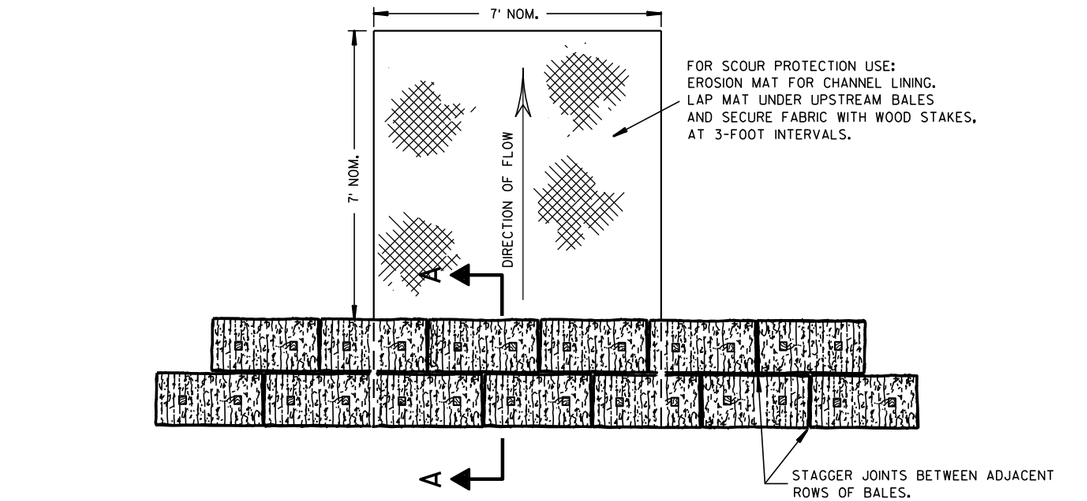
S.D.D. 8 D 5-15e

6

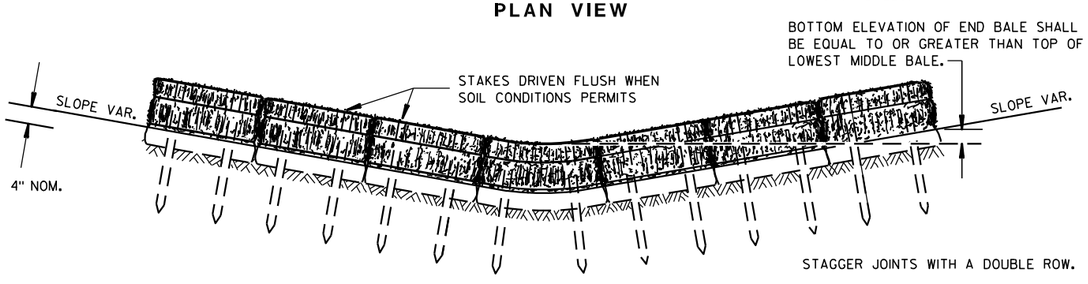
S.D.D. 8 D 5-15e



SECTION A-A



PLAN VIEW



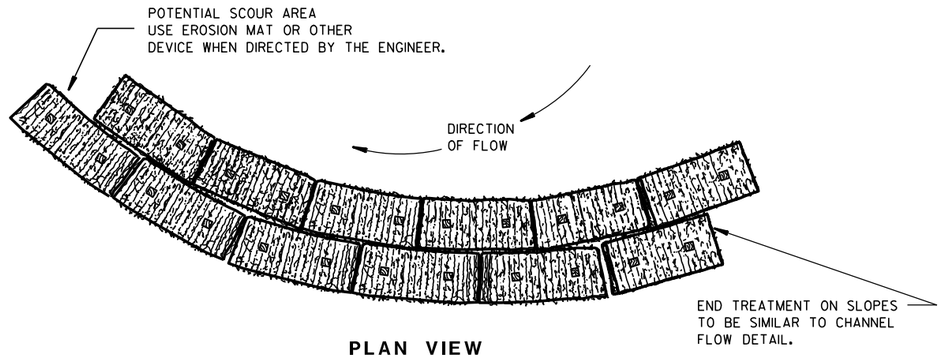
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES (1)

GENERAL NOTES

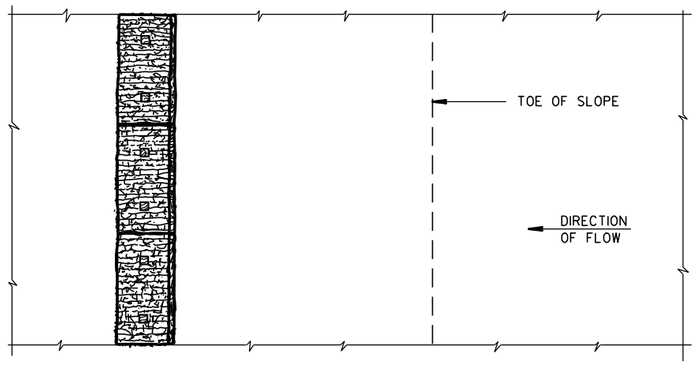
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

(1) TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

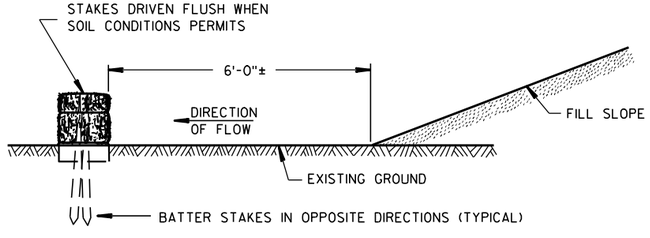


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

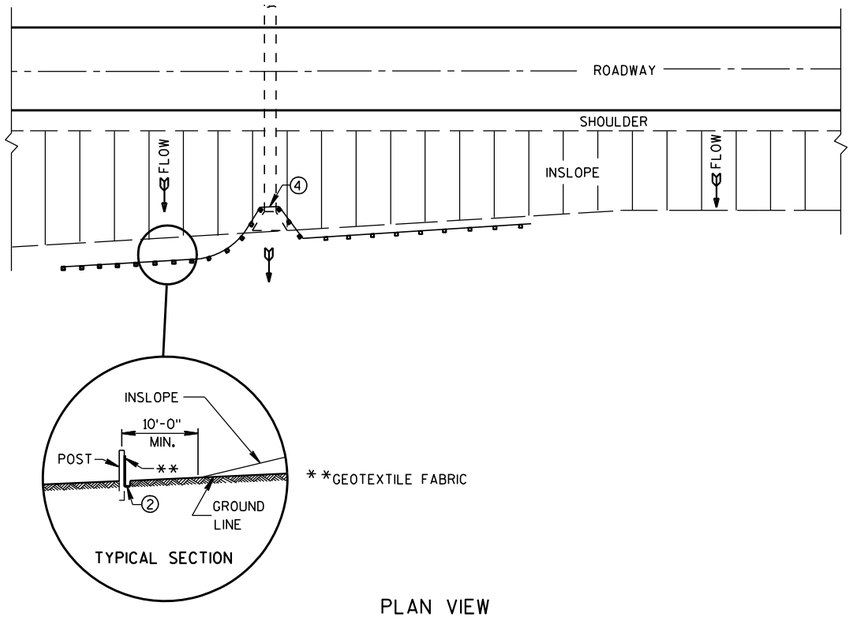
APPROVED

6/04/02 DATE — /S/ Beth Cannestra — CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

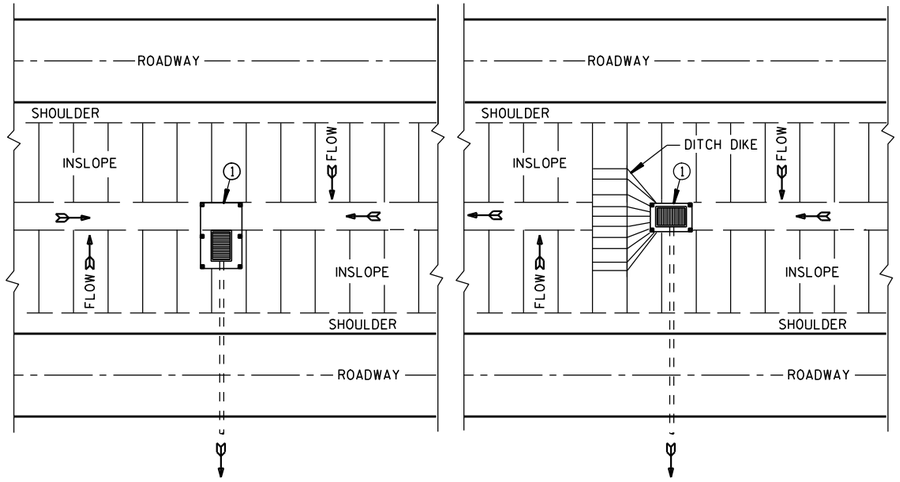
6

S.D.D. 8 E 8-3



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

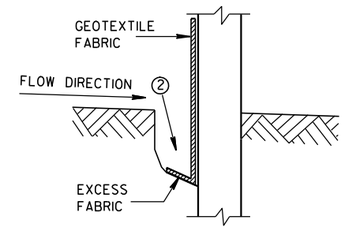
① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.

② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.

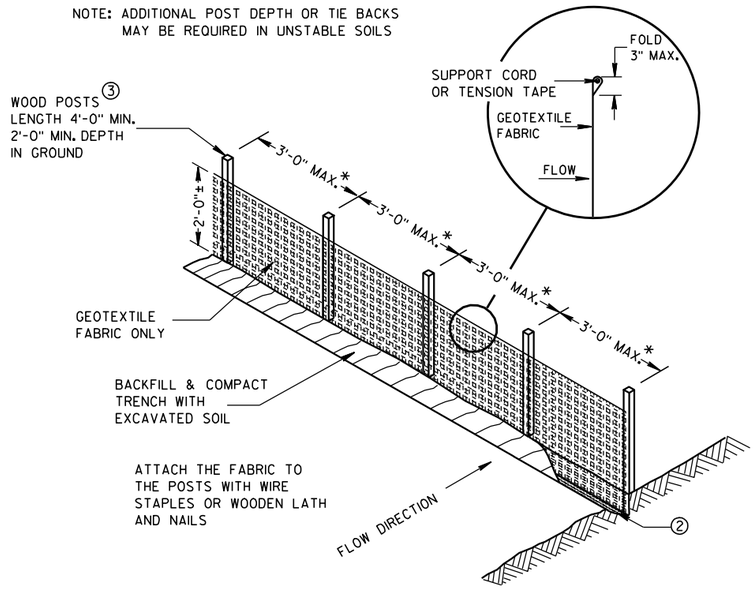
③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.

④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.

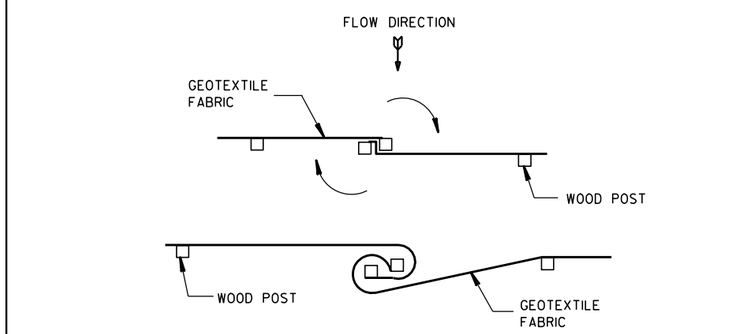
⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



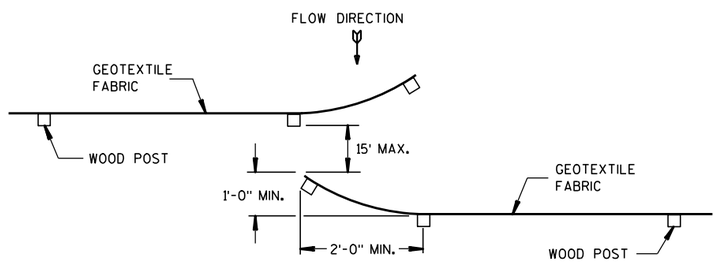
TRENCH DETAIL



SILT FENCE

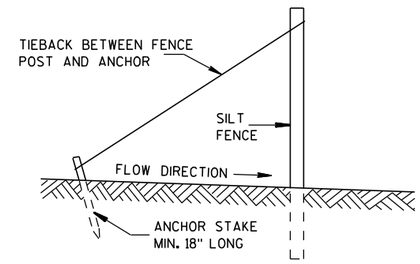


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

S.D.D. 8 E 9-6

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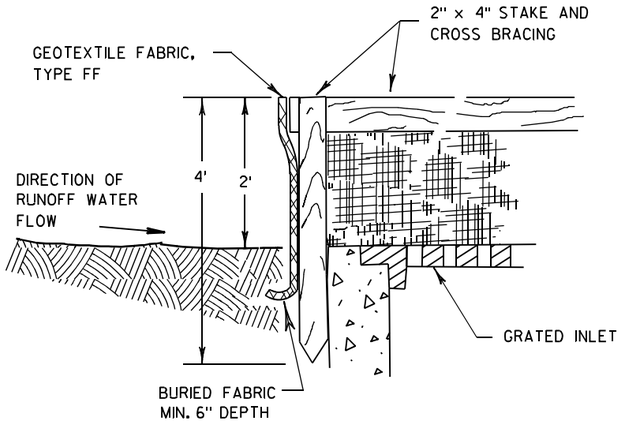
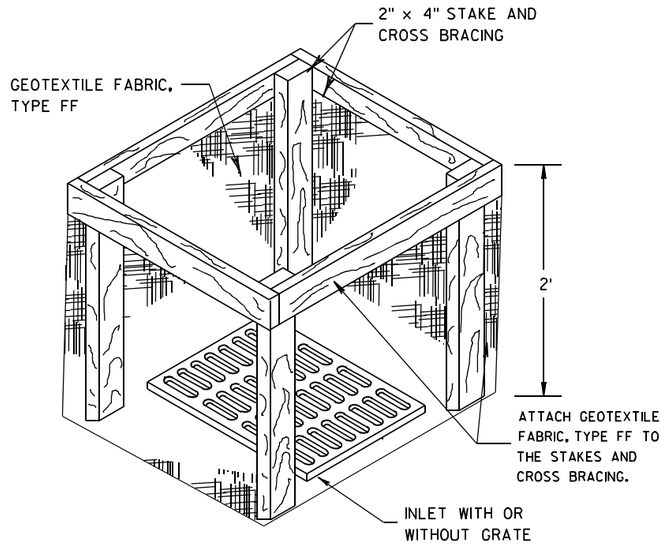
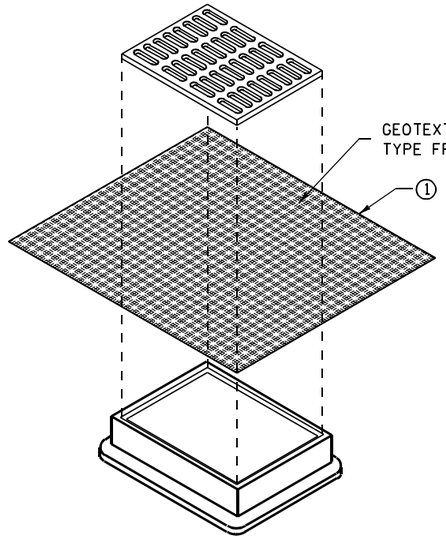
S.D.D. 8 E 9-6



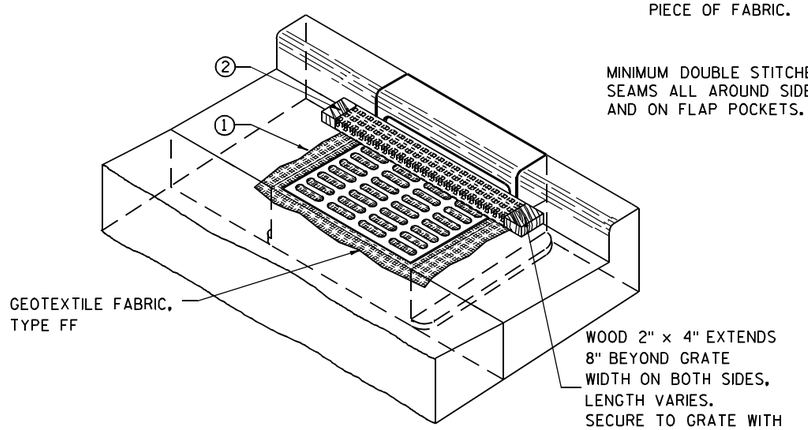


INLET PROTECTION, TYPE A

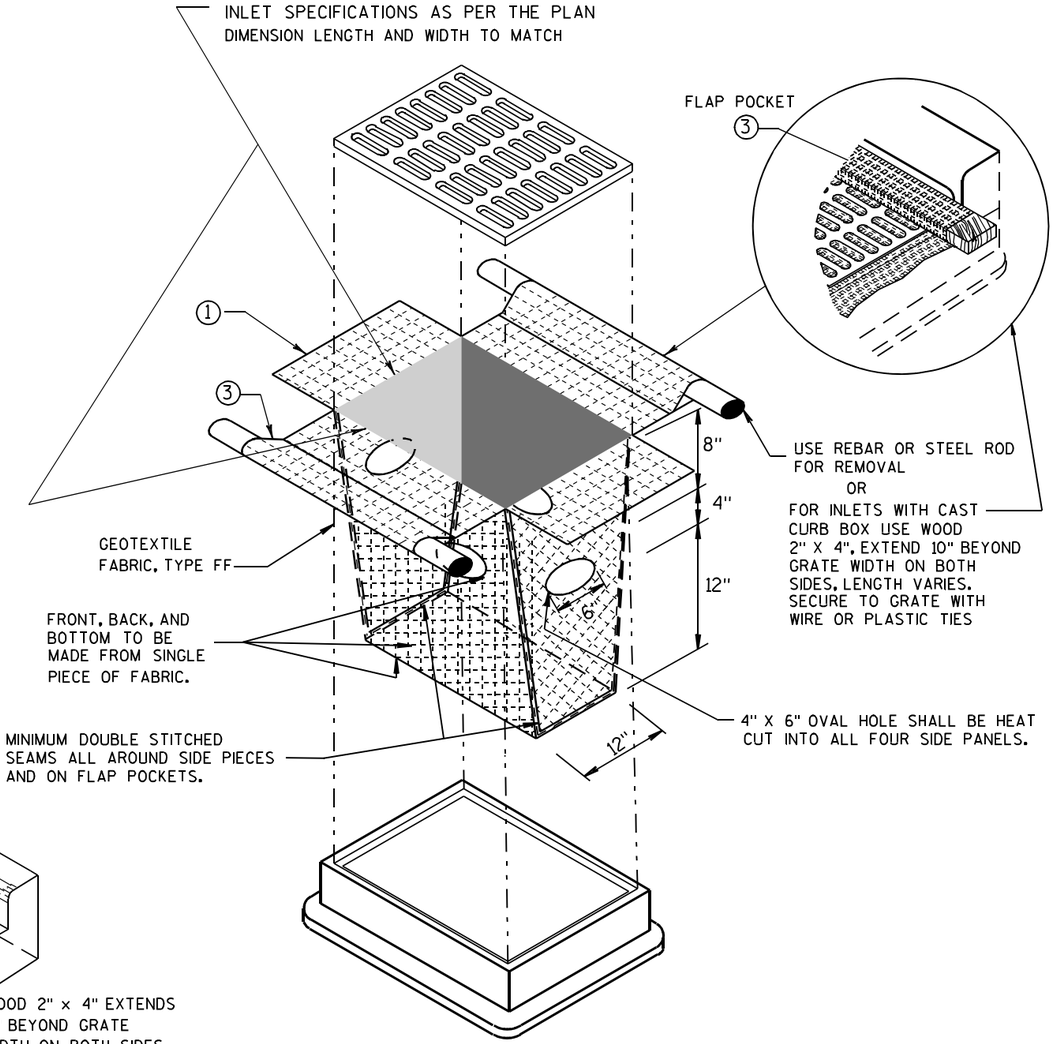


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

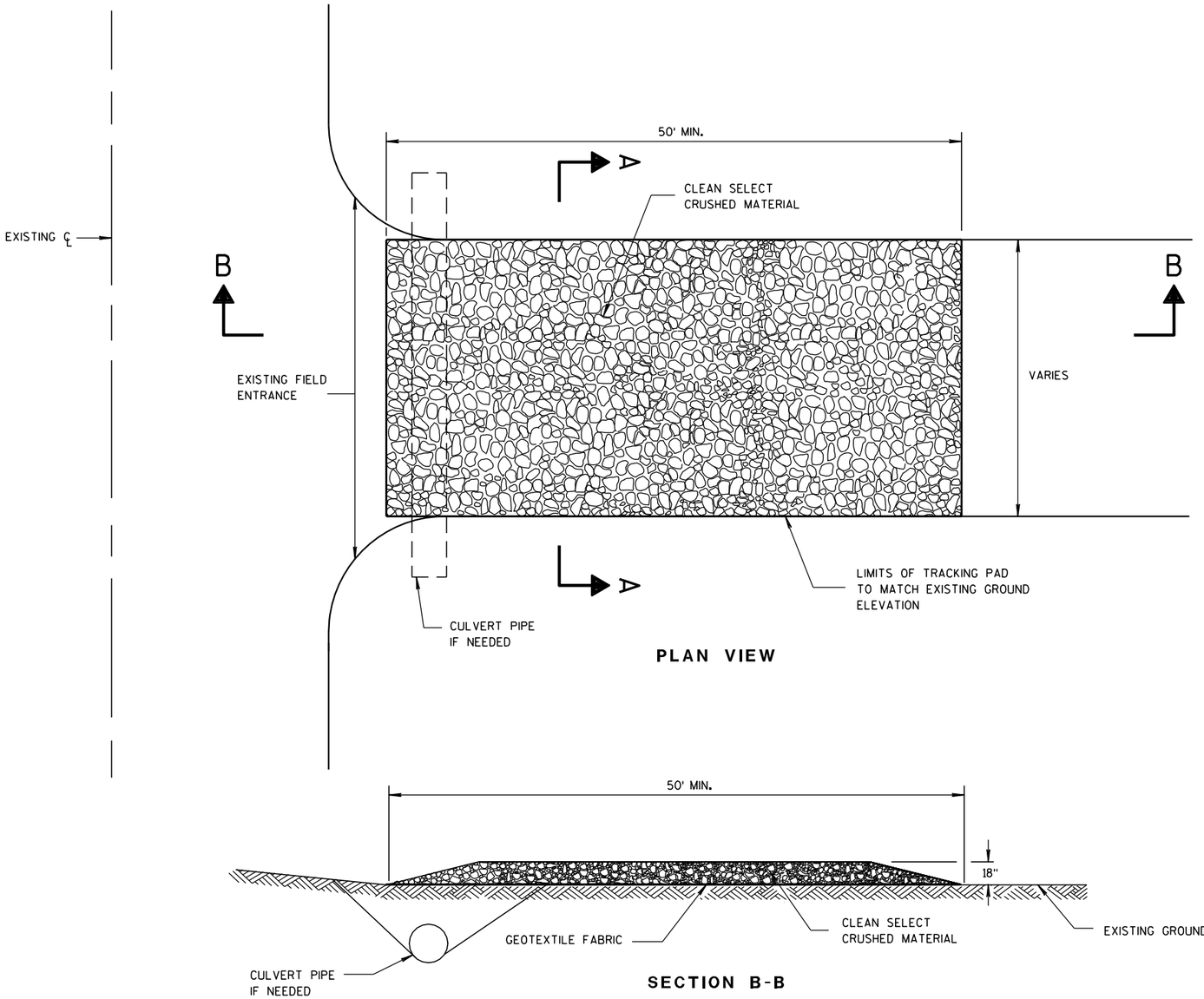
APPROVED

10/16/02
DATE

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

6
S.D.D. 8 E 10-2



PLAN VIEW

SECTION B-B

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TRACKING PAD SHALL BE INSPECTED DAILY. DEFICIENT AREAS SHALL BE REPAIRED OR REPLACED IMMEDIATELY.

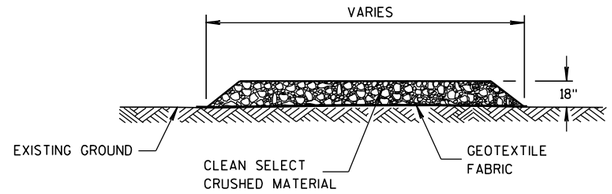
TRACKING PAD TO BE REMOVED AFTER CONSTRUCTION IS COMPLETED.

TRACKING PAD SHALL BE THE FULL WIDTH OF THE EGRESS POINT.

SURFACE WATER MUST BE PREVENTED FROM PASSING THROUGH THE TRACKING PAD. FLOWS SHALL BE DIVERTED AWAY, AROUND OR CONVEYED UNDER THE TRACKING PAD.

CULVERT PIPE OR OTHER BMP USED TO DIVERT WATER AWAY, AROUND OR UNDER THE TRACKING PAD SHALL BE DESIGNED TO CONVEY THE 2 YEAR - 24 HOUR EVENT.

THE COST OF ADDITIONAL BMP TO DIVERT WATER ARE INCIDENTAL TO THE TRACKING PAD BID ITEM.



SECTION A-A

TRACKING PAD

TRACKING PAD

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
3/24/2011 — DATE
/S/ Jerry H. Zoda — ROADWAY STANDARDS ENGINEER
FHWA

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S.D.D. 8 E 14-1

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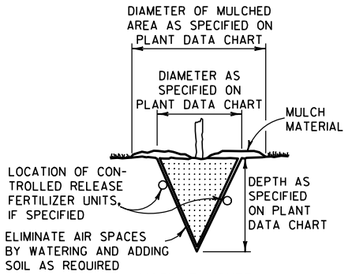
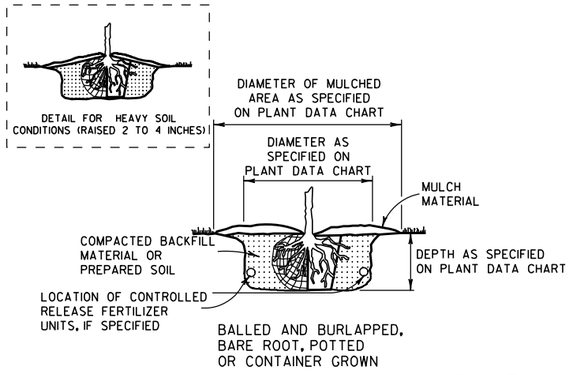
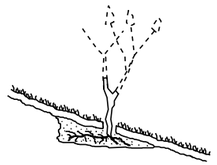


MACHINE MOVED

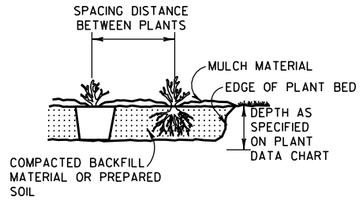


BALLED AND BURLAPPED, BARE ROOT, POTTED OR CONTAINER GROWN



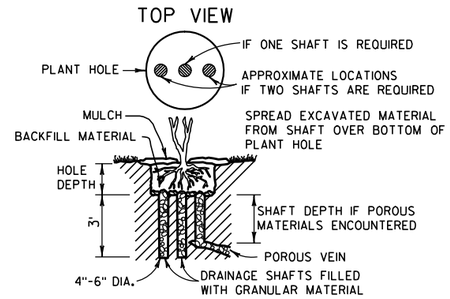
ACCOMMODATE ROOTS (SMOOTH AND STAGHORN SUMAC)

NOTE:
1) ENGINEER SHALL REQUIRE 3 SLITS IN POT TO SPEED DETERIORATION
2) METAL, PLASTIC OR OTHER NONDEGRADABLE POTS SHALL BE REMOVED PRIOR TO PLANTING



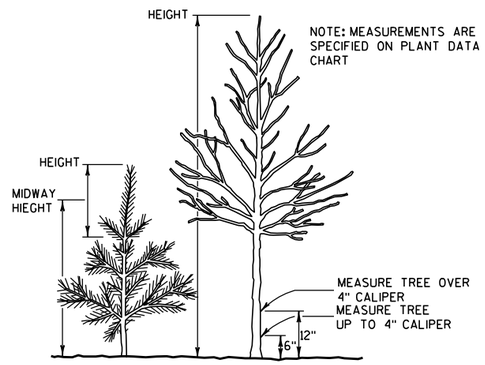
PREPARED BED (BARE ROOT AND POTTED)

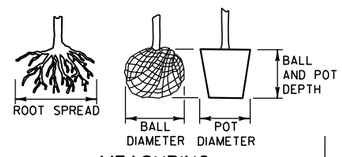
PLANTING



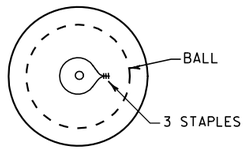
NOTE:
DRAINAGE SHAFT AS SPECIFIED ON PLANT DATA CHART

DRAINING

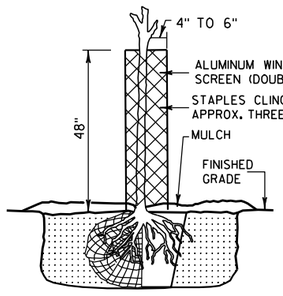




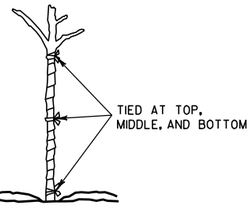
MEASURING



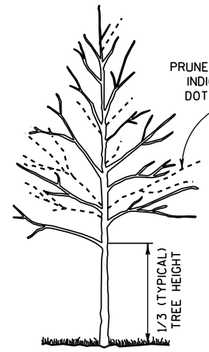
TOP VIEW



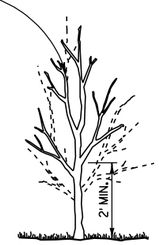
RODENT PROTECTION



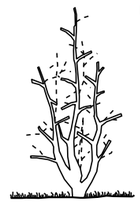
WRAPPING



TALL DECIDUOUS TREES (ASH, MAPLE, LINDEN, OAK)



SINGLE STEMMED LOW DECIDUOUS TREES (CRABS AND OTHER SINGLE STEM TYPE)

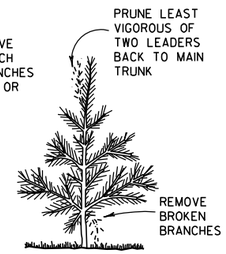


MULTI-STEMMED LOW DECIDUOUS TREES OR LARGE SHRUBS (HAWTHORN, SERVICEBERRY)

NOTE: WHEN PRUNING, PRESERVE CHARACTER AND SHAPE OF TREE. AVOID LEAVING STUBS - REMOVE BRANCH OR TWIG BACK TO THE NEAREST CROTCH
1) PRUNE TO REMOVE DEAD AND BROKEN BRANCHES
2) PRUNE TO REMOVE BRANCHES THAT TOUCH OR ARE TOO CLOSE TO OTHER BRANCHES

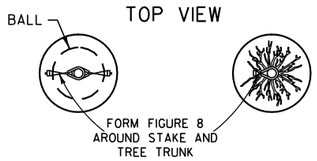
SHRUBS (GRAY DOGWOOD ARROWWOOD VIBURNUM)

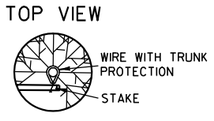
SUMAC

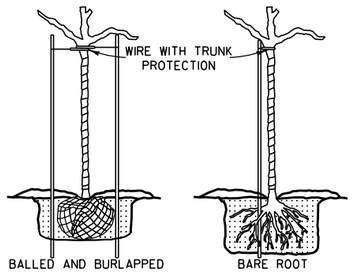


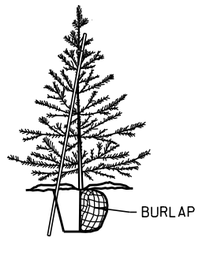
TREE TYPE EVERGREENS (PINE, SPRUCE, FIR) EVERGREENS USUALLY ARE NOT PRUNED

PRUNING



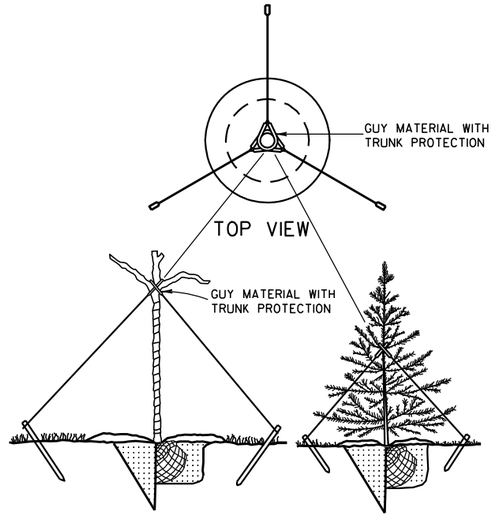




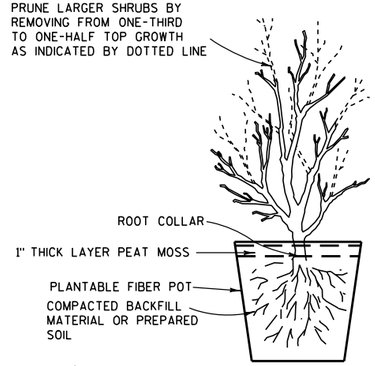


NOTE: BRACING STAKE
1) SHALL BE DRIVEN INTO THE GROUND AS CLOSE TO THE TREE AS POSSIBLE WITHOUT DAMAGING THE BRANCHES.
2) MAY BE DRIVEN AT SUCH AN ANGLE THAT IT DOES NOT PENETRATE THE BALL OR POT.
3) SHALL NOT PROTRUDE ABOVE THE TOP OF THE TREE; AND
4) SHALL HAVE A HOLE NEAR THE TOP TO HOLD THE WIRE IN PLACE.

BRACING



GUYING



POTTING

NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

BRACING, WRAPPING, GUYING, RODENT PROTECTION, FERTILIZER AND MULCH SHALL BE USED ONLY WHEN SPECIFIED ON THE PLANT DATA CHART (PART OF PLAN) OR SPECIAL PROVISIONS.

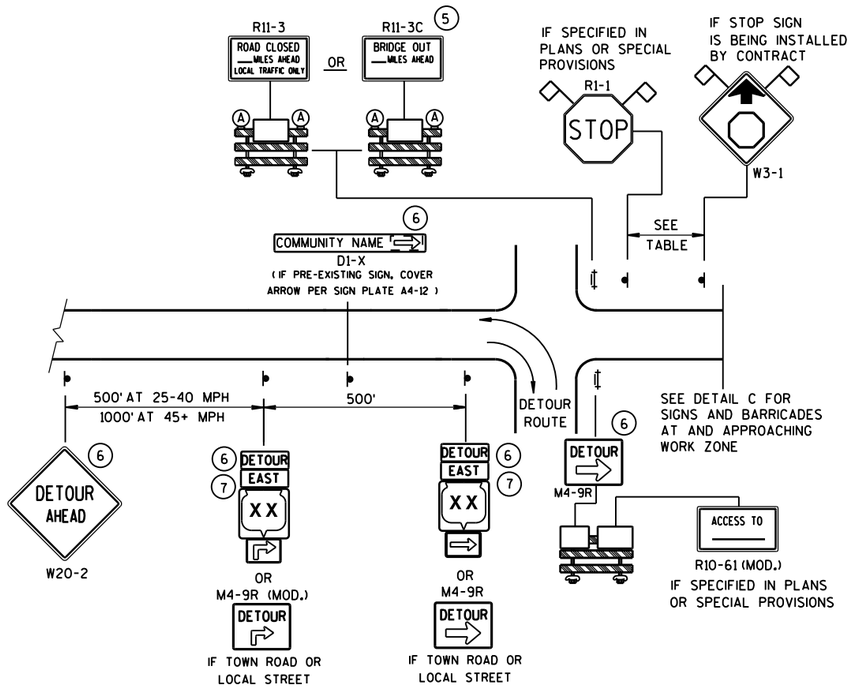
TREE PLANTING DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4/11/94 — DATE
/S/ Rory L. Rhinesmith — CHIEF METHODS DEVELOPMENT ENGINEER
FHWA

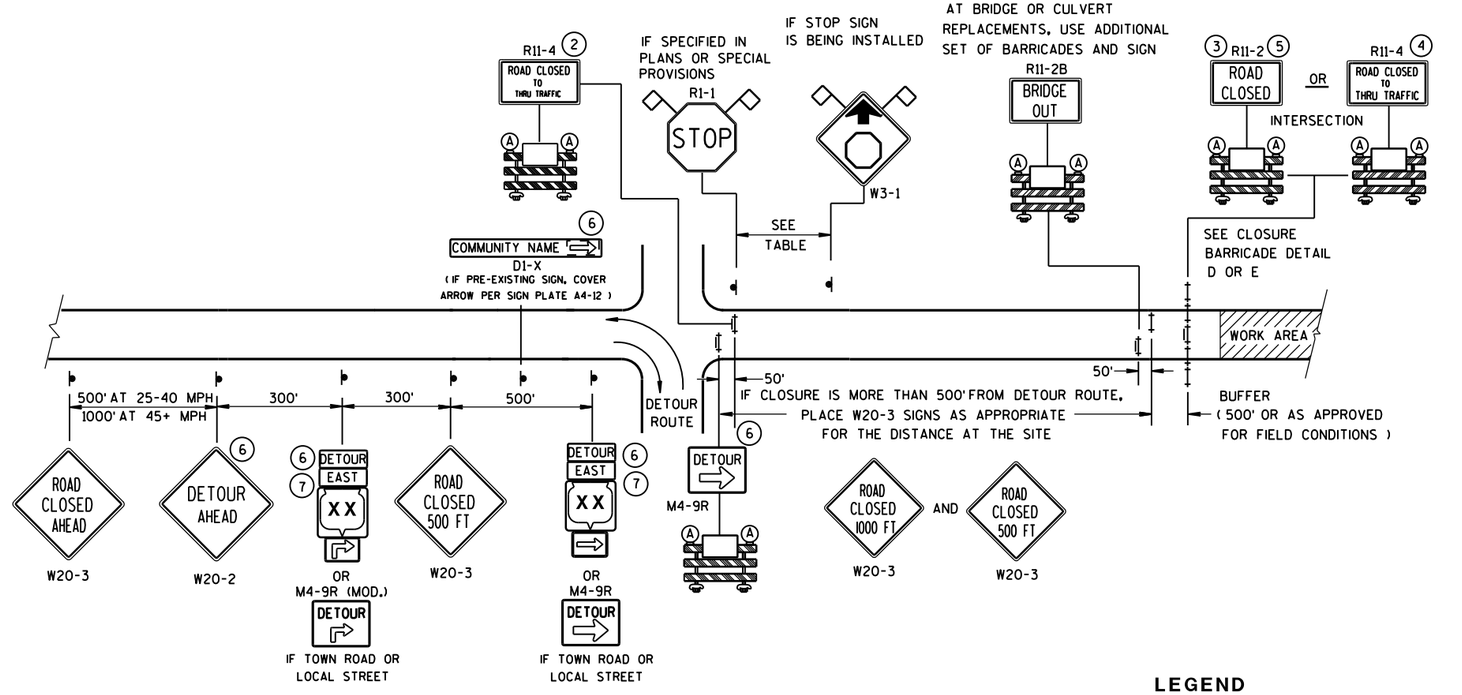
6

S.D.D. 14 A 2-1



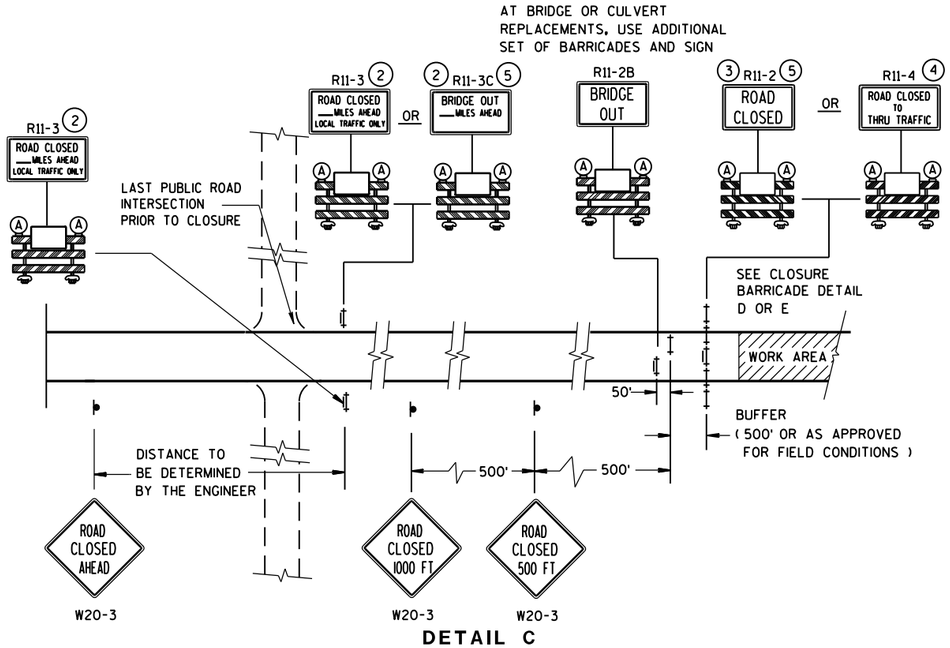
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "**b**" FOR GENERAL NOTES AND FOOTNOTES (1) THROUGH (7)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- DETOUR / EAST — M4-8 / M3-X
- XX — M1-4; COUNTY XX — M1-5A; XX — M1-6
- M05-1 OR M06-1
- FLAGS, 16" X 16" MIN., (ORANGE)

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

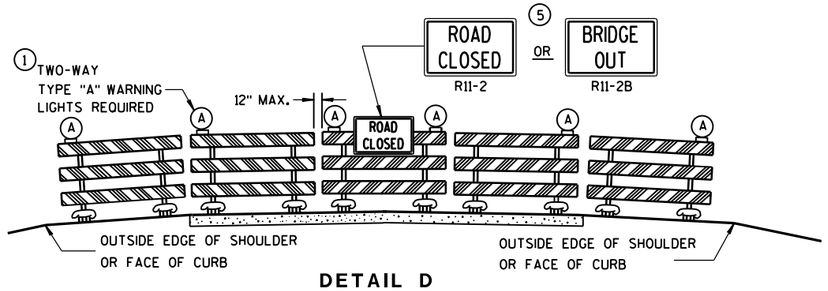
8/2013 DATE

/S/ Travis Feltes
STATE TRAFFIC ENGINEER

FHWA

6

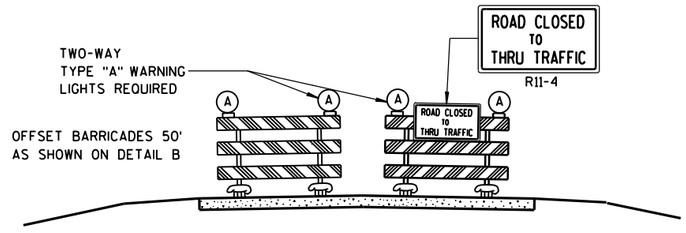
S.D.D. 15 C 2-5a



DETAIL D

ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW



DETAIL E

LANE CLOSURE BARRICADE DETAIL

APPROACH VIEW

SEE SDD 15C2-SHEET "**a**" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

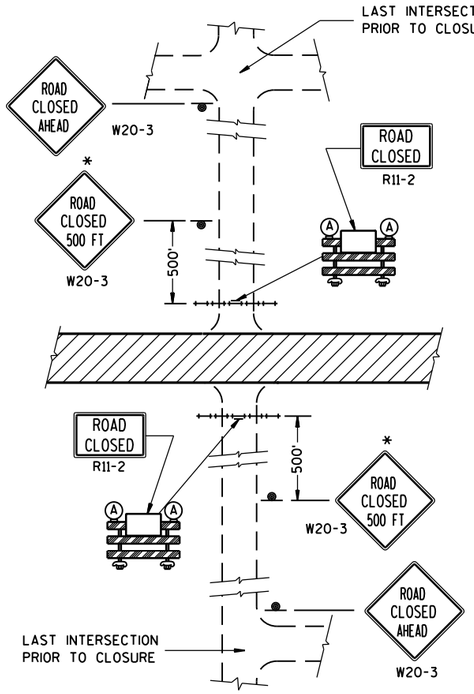
8/2013 DATE

/S/ Travis Feltes
STATE TRAFFIC ENGINEER

FHWA

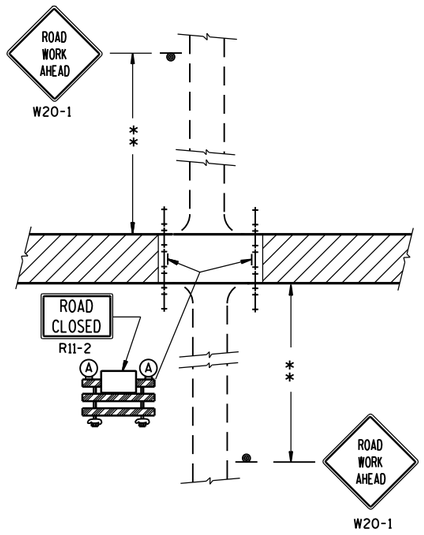
6

S.D.D. 15 C 2-5b



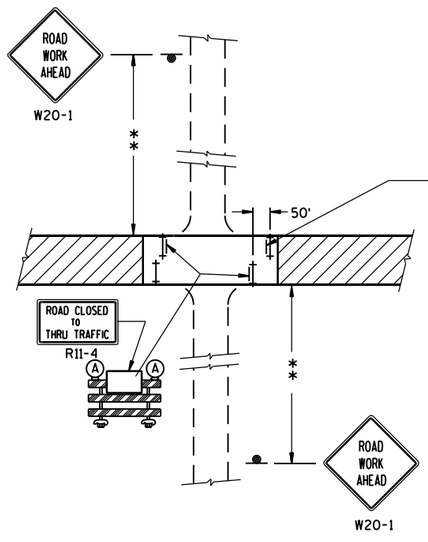
DETAIL 1

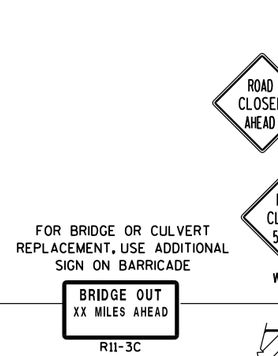
(NO ACCESS TO PROJECT)



DETAIL 2

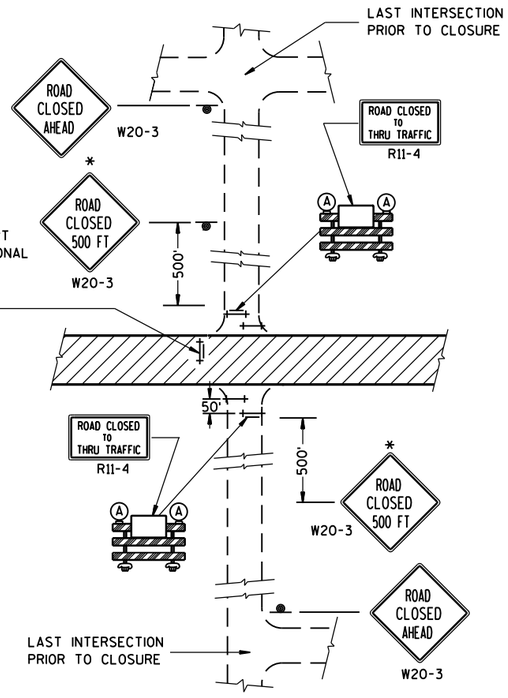
(PUBLIC CROSS-TRAFFIC MAINTAINED. NO ACCESS TO PROJECT).





DETAIL 3

(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).



DETAIL 4

(CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

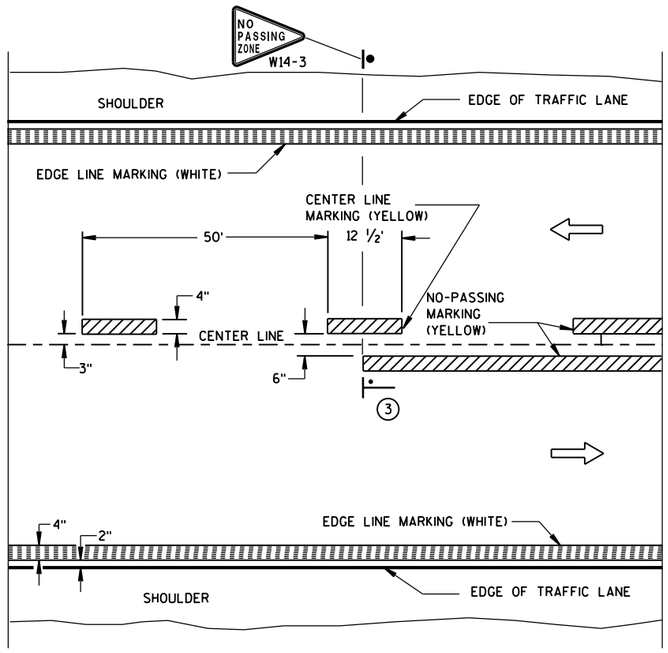
APPROVED
8/2013 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

6

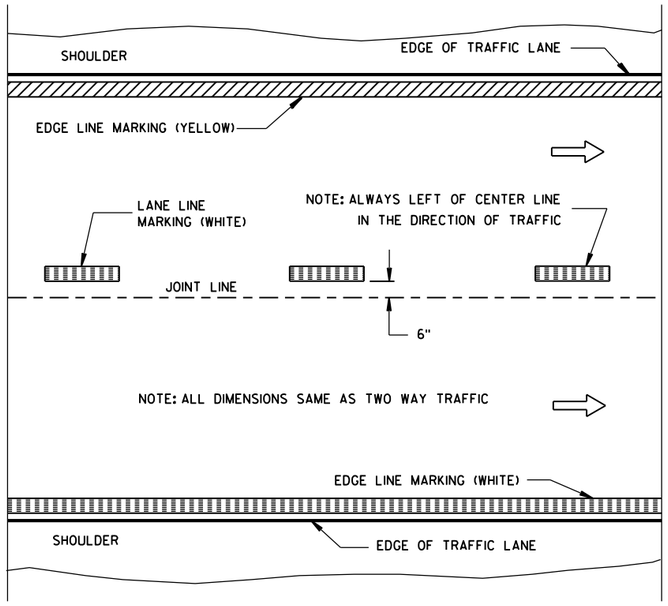
S.D.D. 15 C 3-2

6

S.D.D. 15 C 3-2

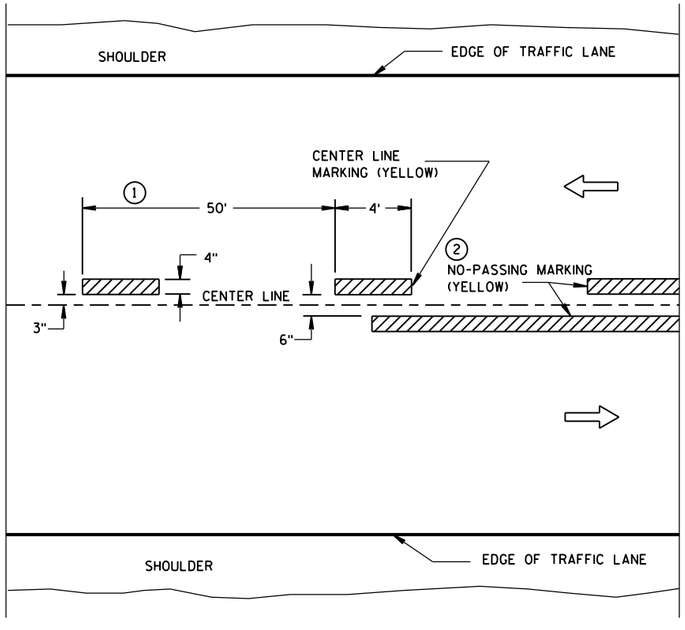


TWO WAY TRAFFIC

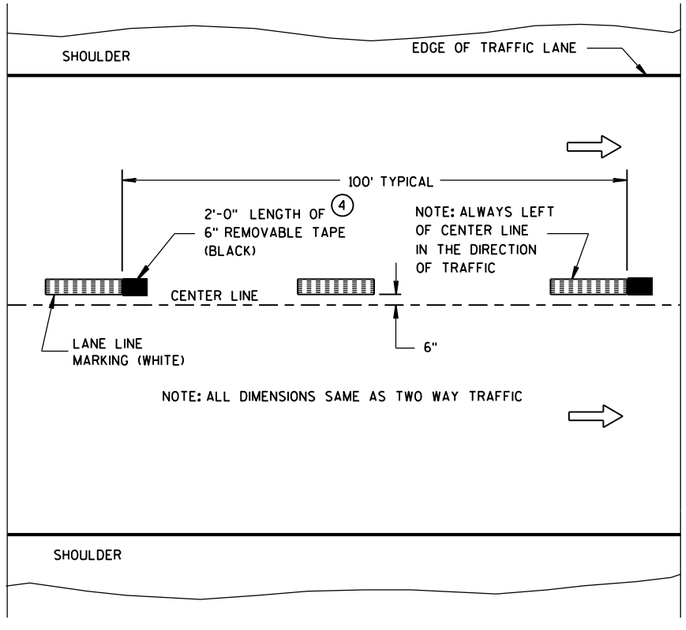


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING

(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

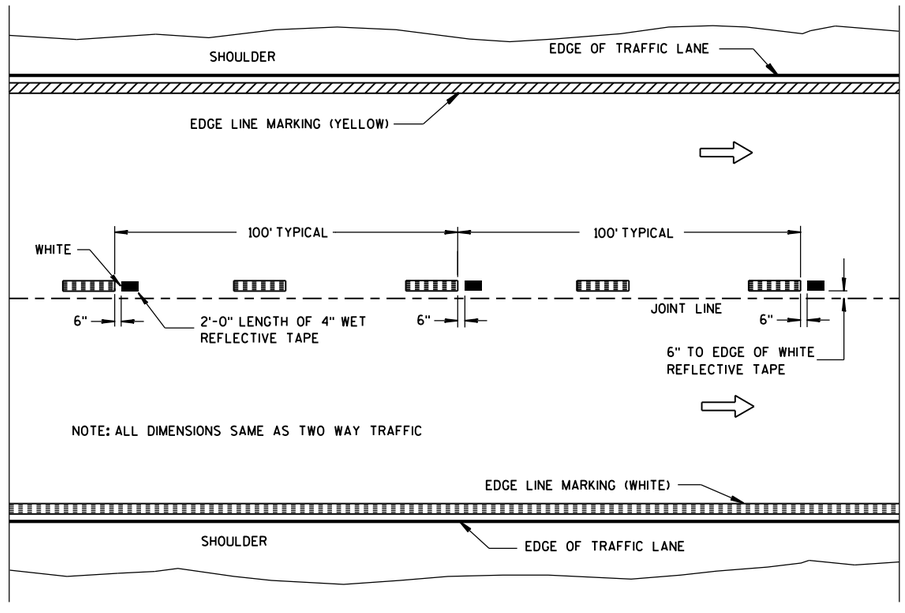
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

1. HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
2. NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
3. NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
4. CONCRETE ONLY.

NOTE

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL



WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE

LEGEND

"T" MARKING

POST MOUNTED SIGN

PAVEMENT MARKING (MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5-13-2013 DATE
/S/ Travis Feltes STATE TRAFFIC E
FHWA

6

S.D.D. 15 C 8-16a

LEGEND

SIGN ON PORTABLE OR PERMANENT SUPPORT

DIRECTION OF TRAFFIC

WORK AREA

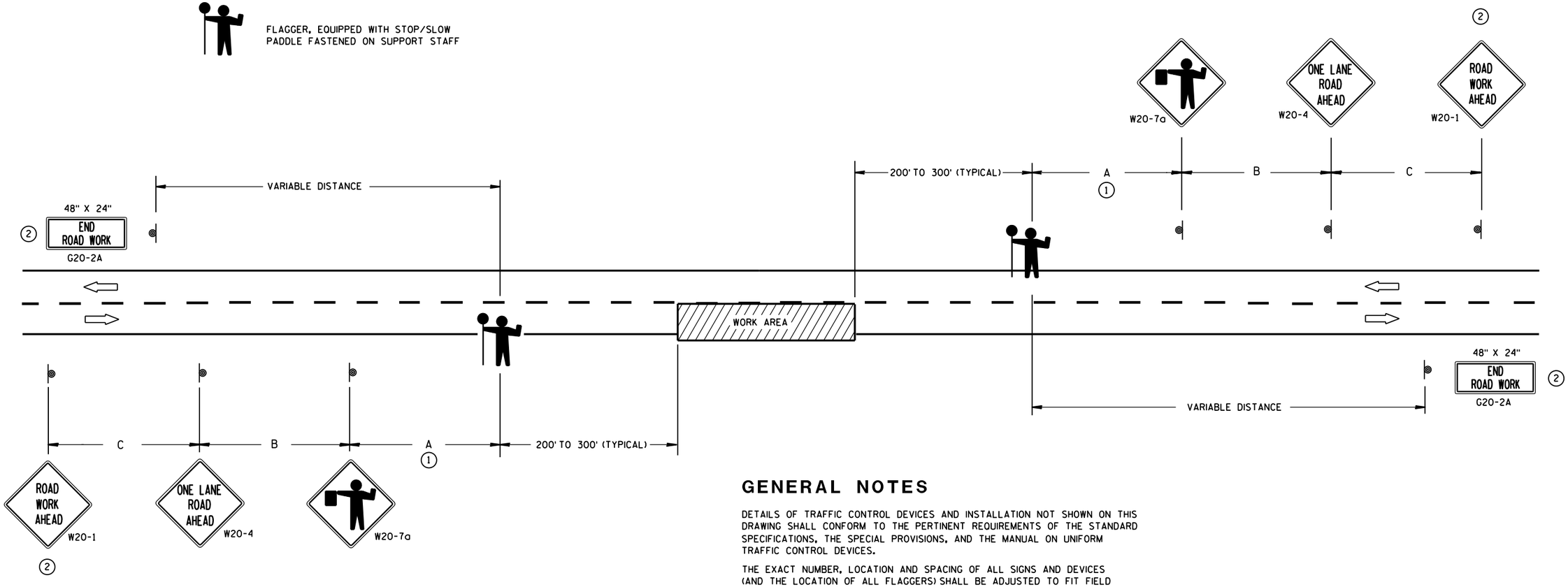
FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'

W3-4

USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



(1) FOR A MOVING WORK OPERATION,
SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

(2) SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

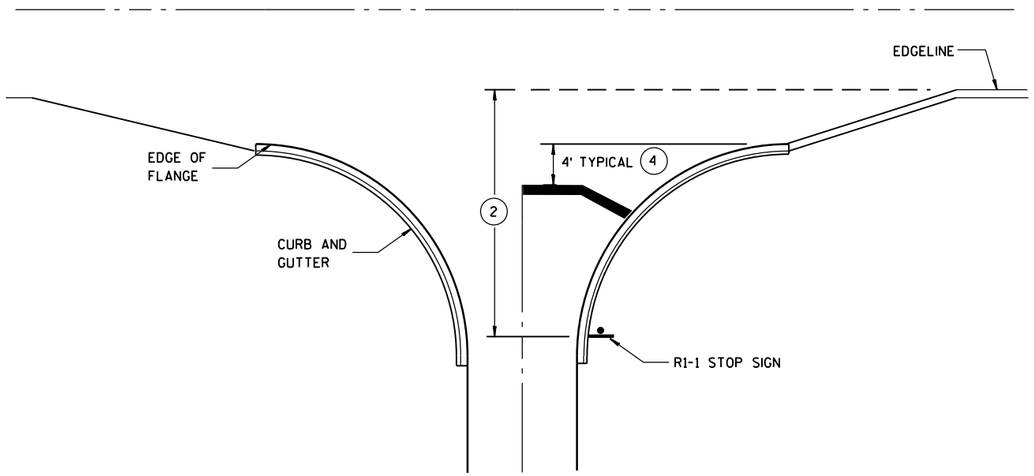
APPROVED
8/2013 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

6

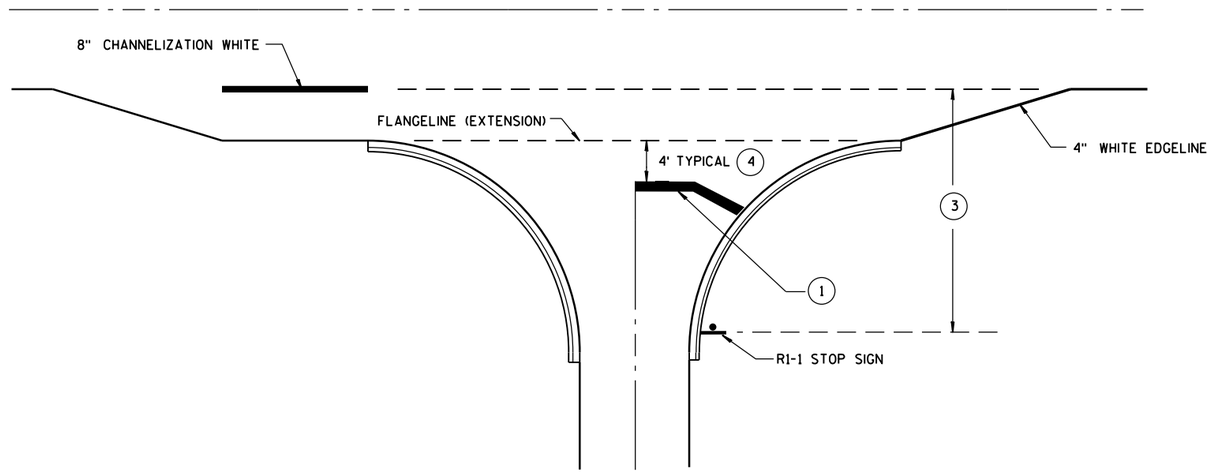
S.D.D. 15 C 12-4

6

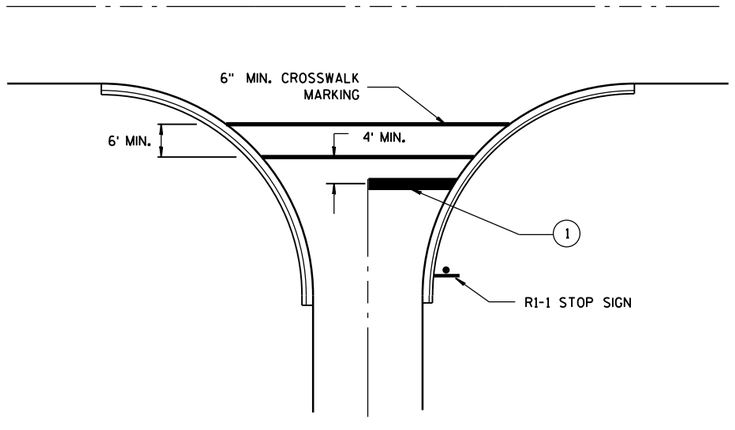
S.D.D. 15 C 12-4



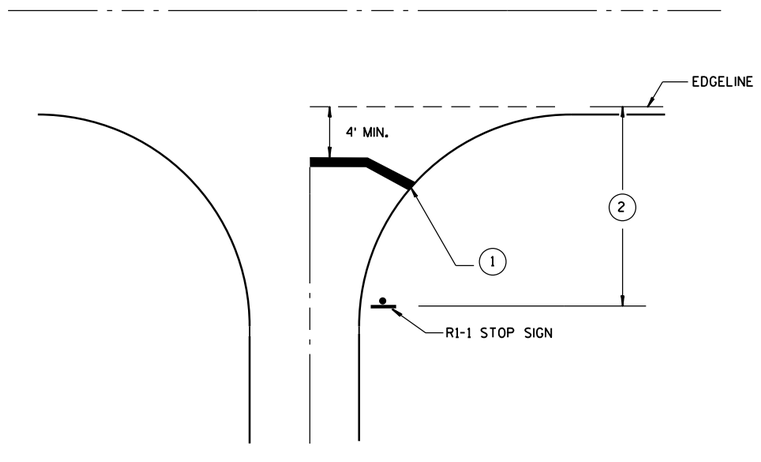
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
3. IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
4. MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES.

STOP LINE AND CROSSWALK PAVEMENT MARKING

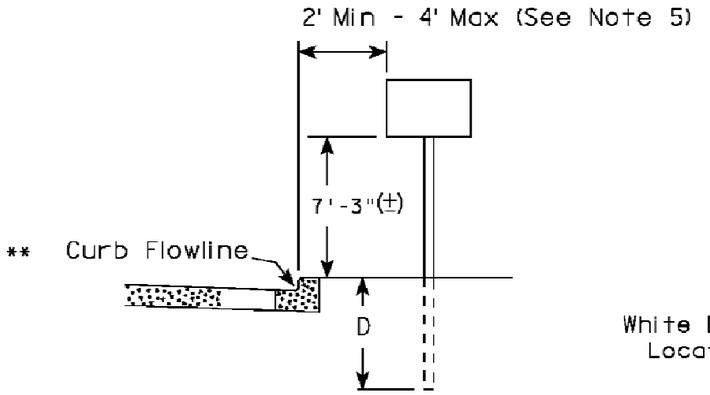
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4/30/2013 DATE
/S/ Travis Feltes
STATE TRAFFIC E
FHWA

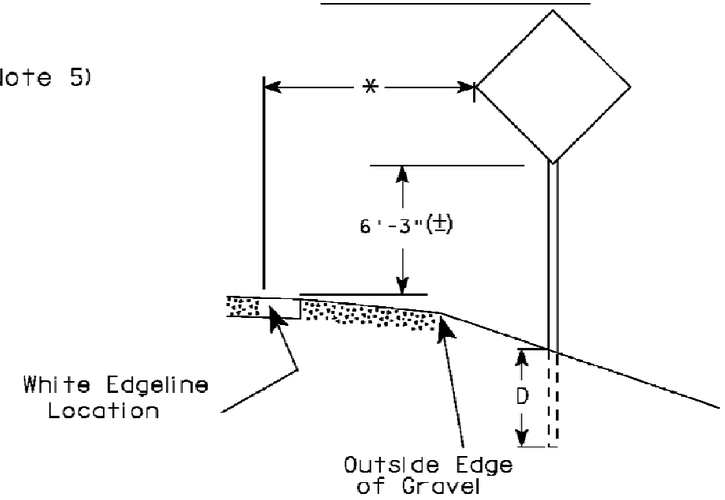
6

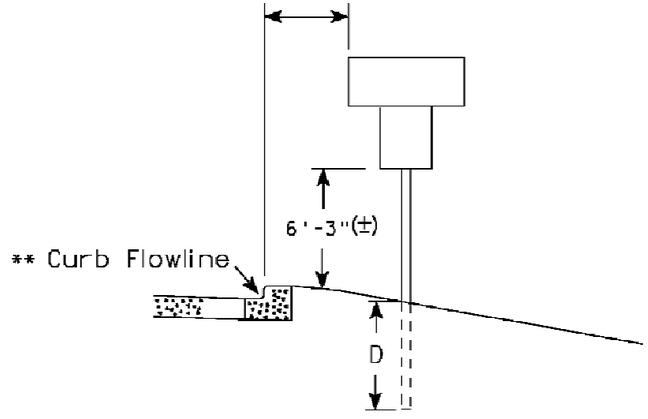
S.D.D. 15 C 33-1

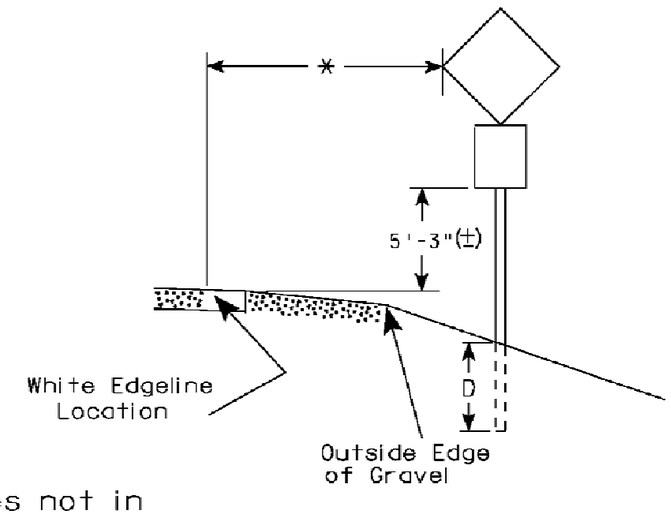
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

GENERAL NOTES

1. Signs wider than 4 feet or larger than 20 sq. ft. shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series) & End of Road Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (±).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

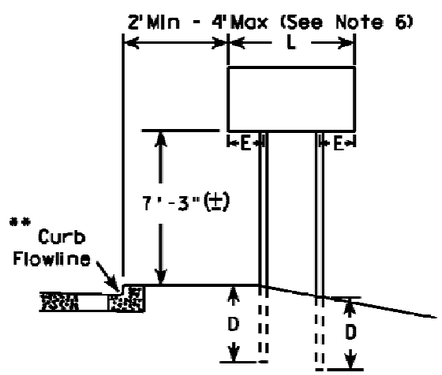
WISCONSIN DEPT OF TRANSPORTATION

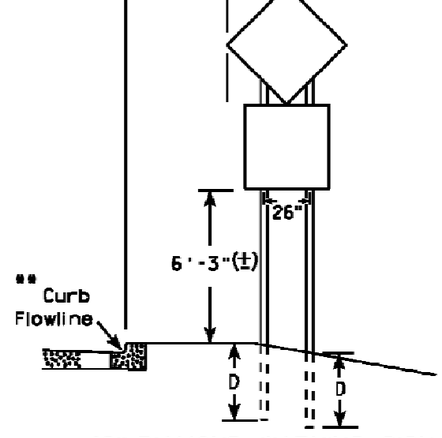
APPROVED Matthew L Rauch for State Traffic Engineer

DATE 9/21/2011 PLATE NO. A4-3.16

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	SIGN PLATES	SHEET 69	E

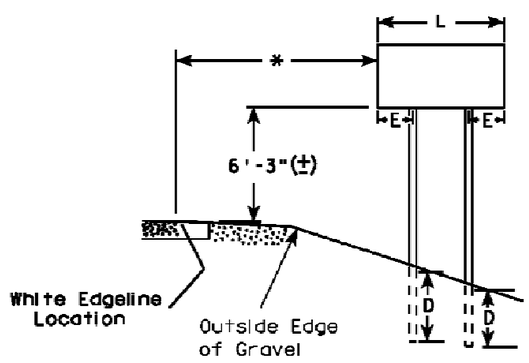
URBAN AREA

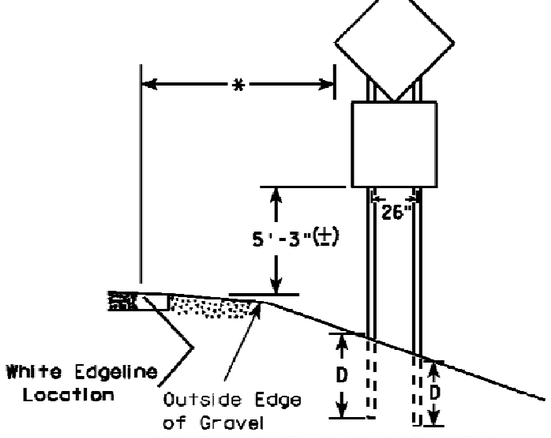




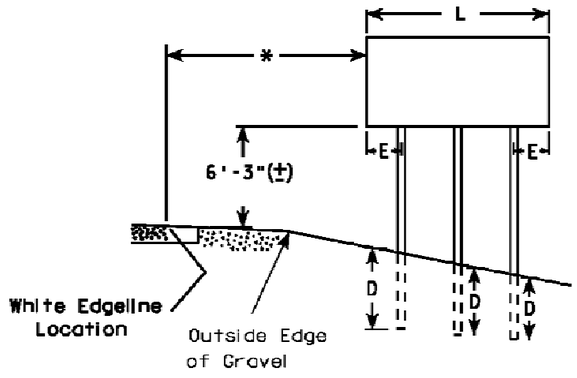
48" DIAMOND WARNING SIGN

RURAL AREA (See Note 3)





48" DIAMOND WARNING SIGN



GENERAL NOTES

1. For multiple post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series) & End of Road Markers (W5-56 & W5-56A) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width or 20 S.F. or less in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 120"	L/5

***

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

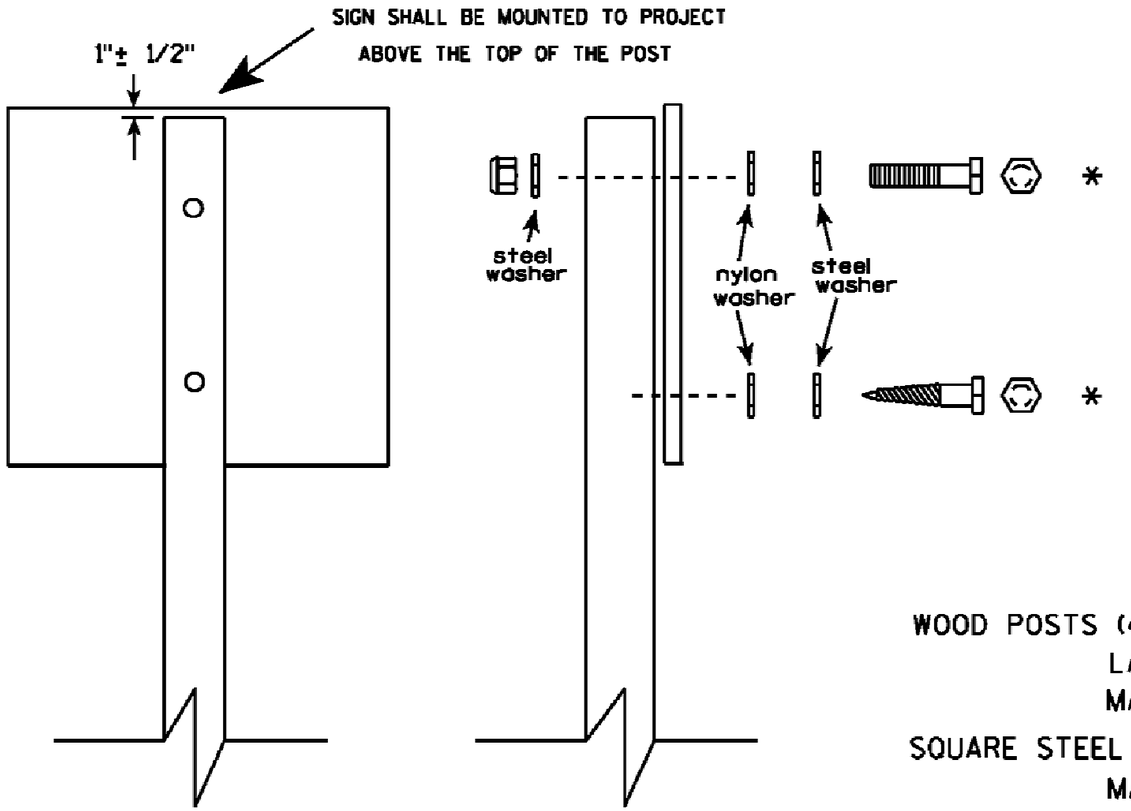
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 9/21/2011 PLATE NO. A4-4.11

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	SIGN PLATES	SHEET 70	E

7

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\070101_SP_48930008.DWG PLOT DATE : 1/28/2014 9:12 PM PLOT BY : KATHRYN BLASER PLOT NAME : PLOT SCALE : ##########



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

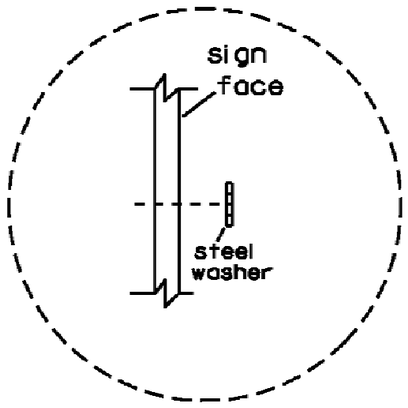
b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

WOOD POSTS (4" x 4" or 4" x 6")
LAG SCREWS - 3/8" X 3"
MACHINE BOLTS - 5/16" X 6-1/2" or 7" Length w/ nuts

SQUARE STEEL POSTS (2" x 2")
MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts
RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON for all Type H signs.



Washer Placement when Sign Has Other Than Type H or Type F Face

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

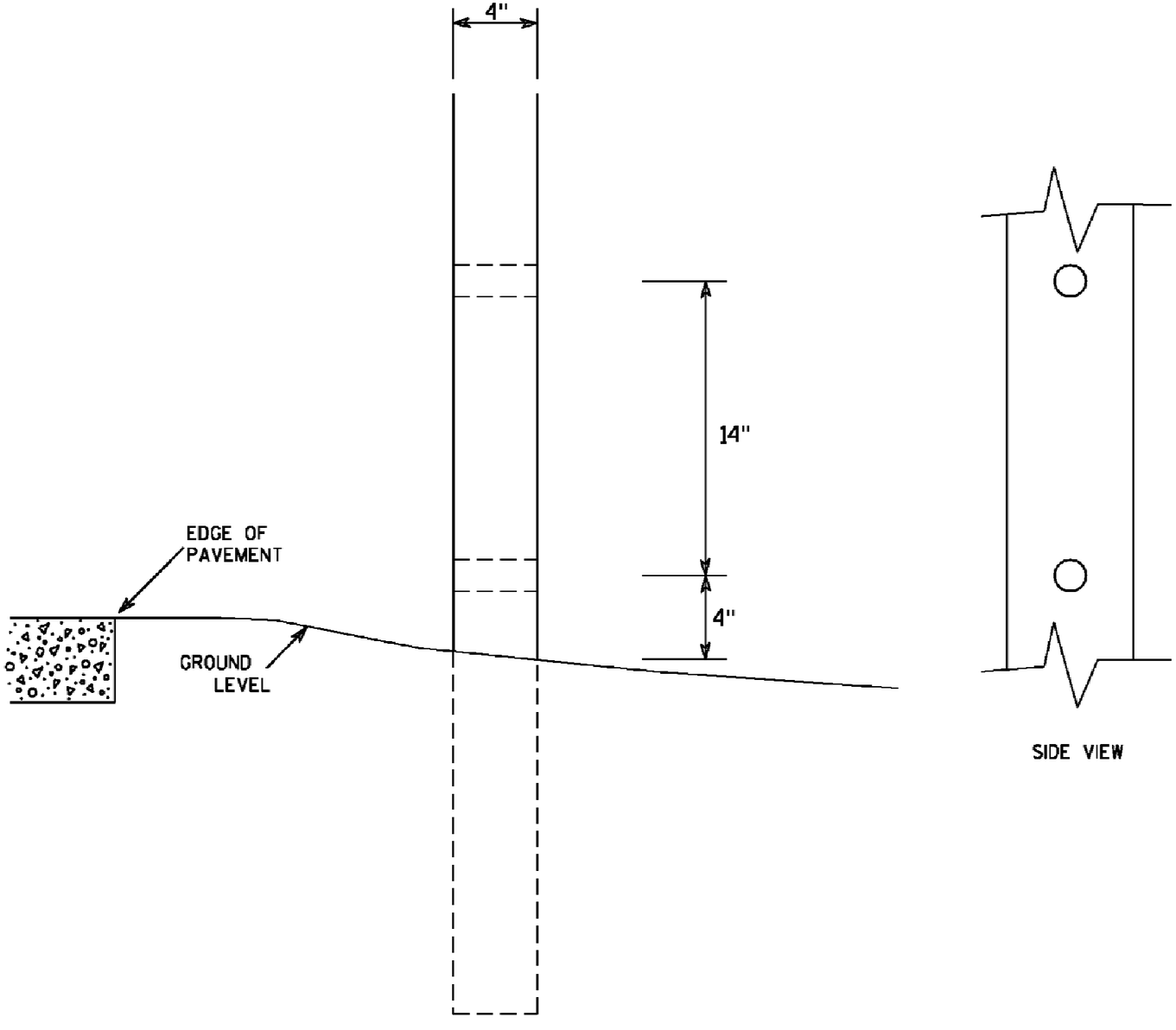
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/23/10 PLATE NO. A4-8.7

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	SIGN PLATES	SHEET 71	E

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\070101_SP_48930008.DWG PLOT DATE : 1/28/2014 9:14 PM PLOT BY : KATHRYN BLASER PLOT NAME : PLOT SCALE : ##########



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

7

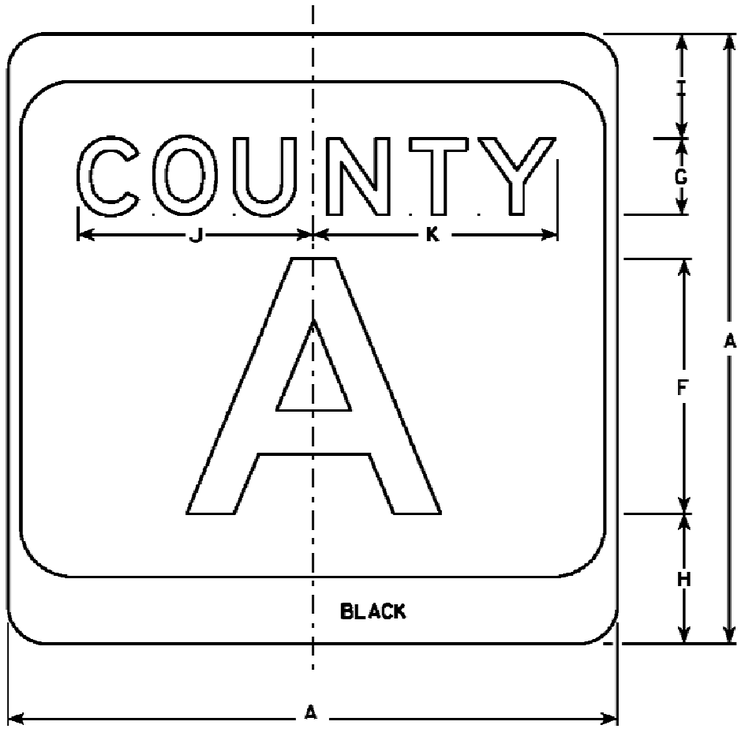
4 X 6 WOOD POST MODIFICATIONS

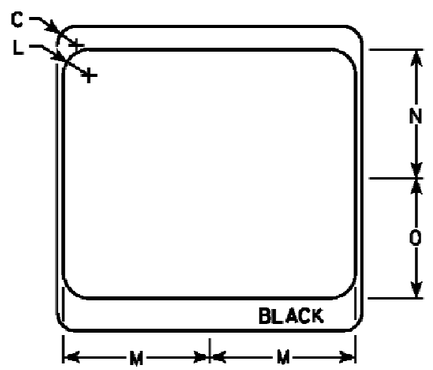
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Chester J Spang for State Traffic Engineer

DATE 3/27/97 PLATE NO. A4-11.2

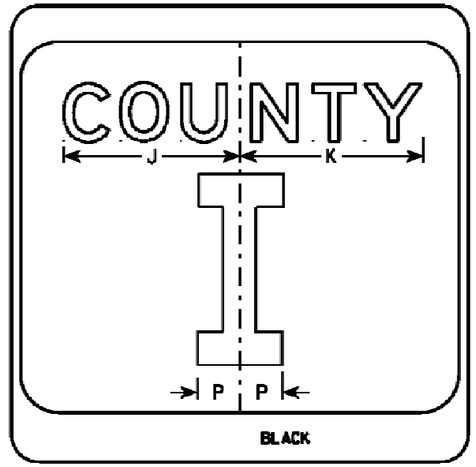
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	SIGN PLATES	SHEET 72	E

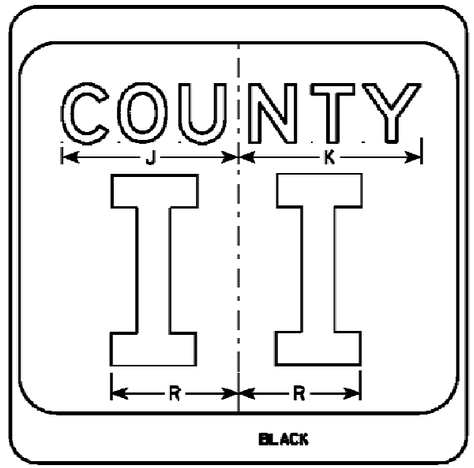




NOTES

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White & Black - See Note 7
   Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.
   Message Series D for 2 letters unless message is too big then Series C.
   Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs
   Background - Type H Reflective
   Detour or temporary Signs
   Background - Reflective





SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 ½			10	3	5 ⅛	4 ⅛	9 ¼	9 ⅝	2	11 ½	10 ⅛	9 ⅜	2 ¼		6 ⅝									4.0
3	36		2 ¼			16	4	7 ⅝	5 ⅝	12 ¼	12 ⅞	3	17 ⅛	15 ¼	14	3 ⅜		10									9.0
4	36		2 ¼			16	4	7 ⅝	5 ⅝	12 ¼	12 ⅞	3	17 ⅛	15 ¼	14	3 ⅜		10									9.0
5	36		2 ¼			16	4	7 ⅝	5 ⅝	12 ¼	12 ⅞	3	17 ⅛	15 ¼	14	3 ⅜		10									9.0

CTH MARKER
M1-5A FOR ASSEMBLIES

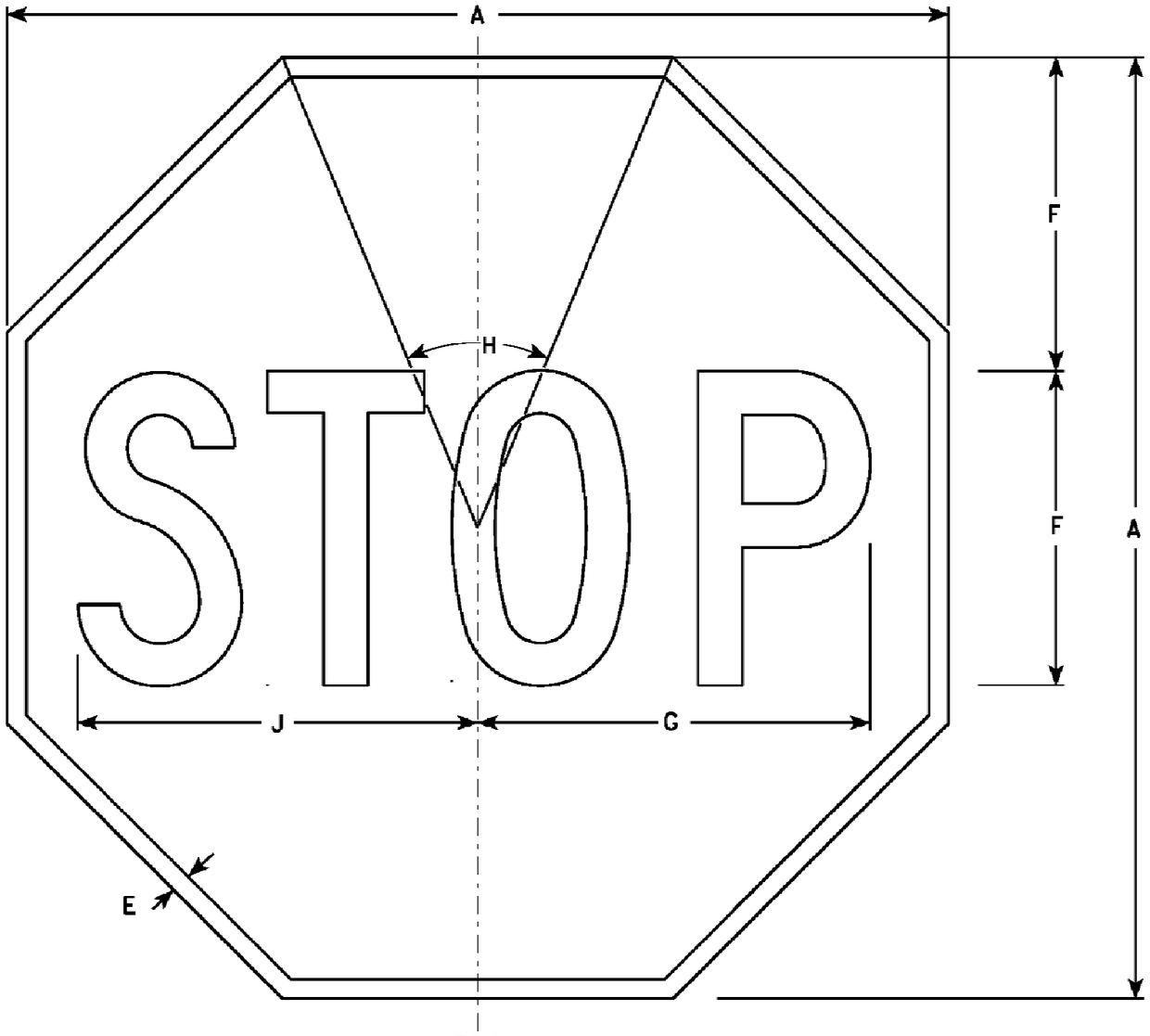
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 9/27/11 PLATE NO. M1-5A.8

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	SIGN PLATES	SHEET 73	E

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\070101_SP_48930008.DWG PLOT DATE : 1/28/2014 9:56 PM PLOT BY : KATHRYN BLASER PLOT NAME : PLOT SCALE : ##########



R1-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area Sq. Ft.
1	24				3/8	8	10	45°		10 1/4																	3.31
2S	30				3/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

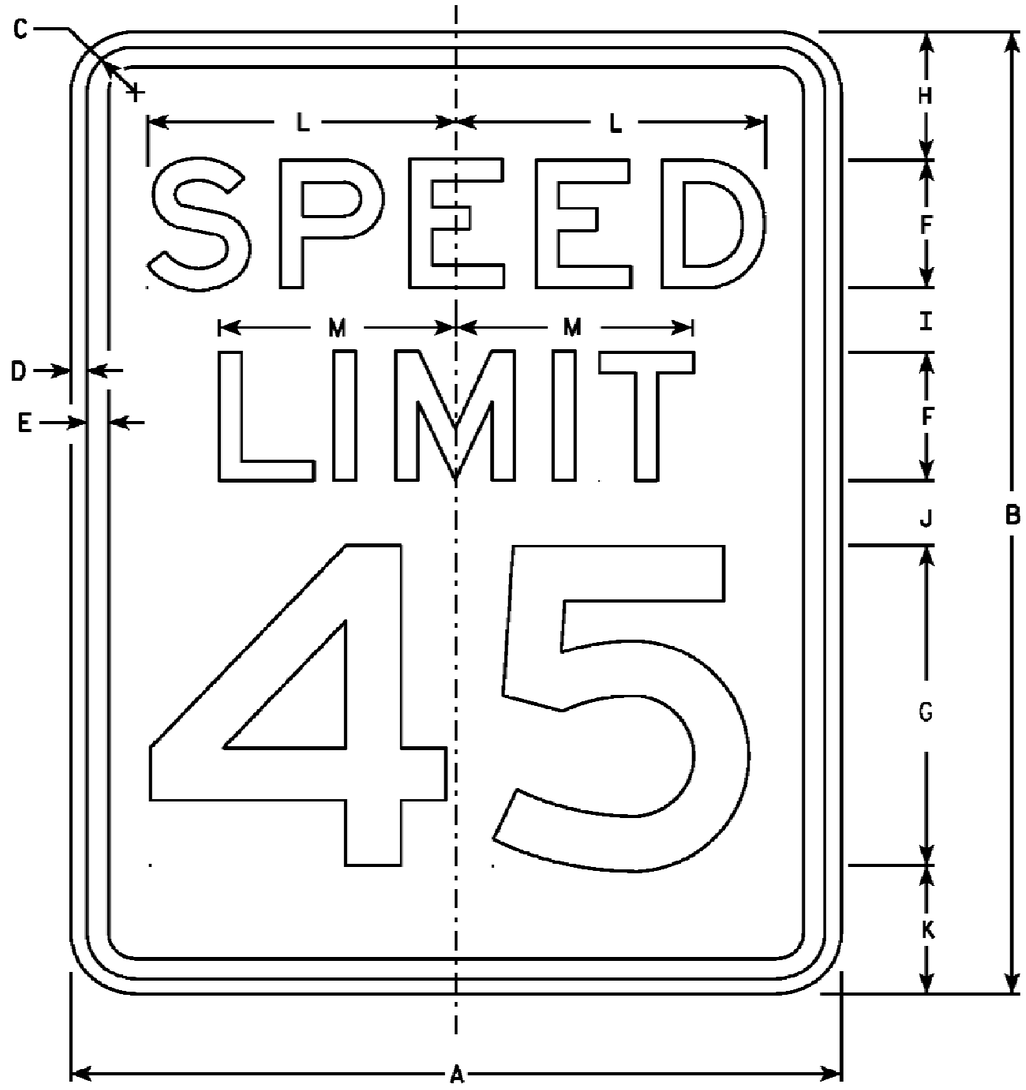
STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1.12

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	SIGN PLATES	SHEET 74	E



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN
R2-1

WISCONSIN DEPT OF TRANSPORTATION

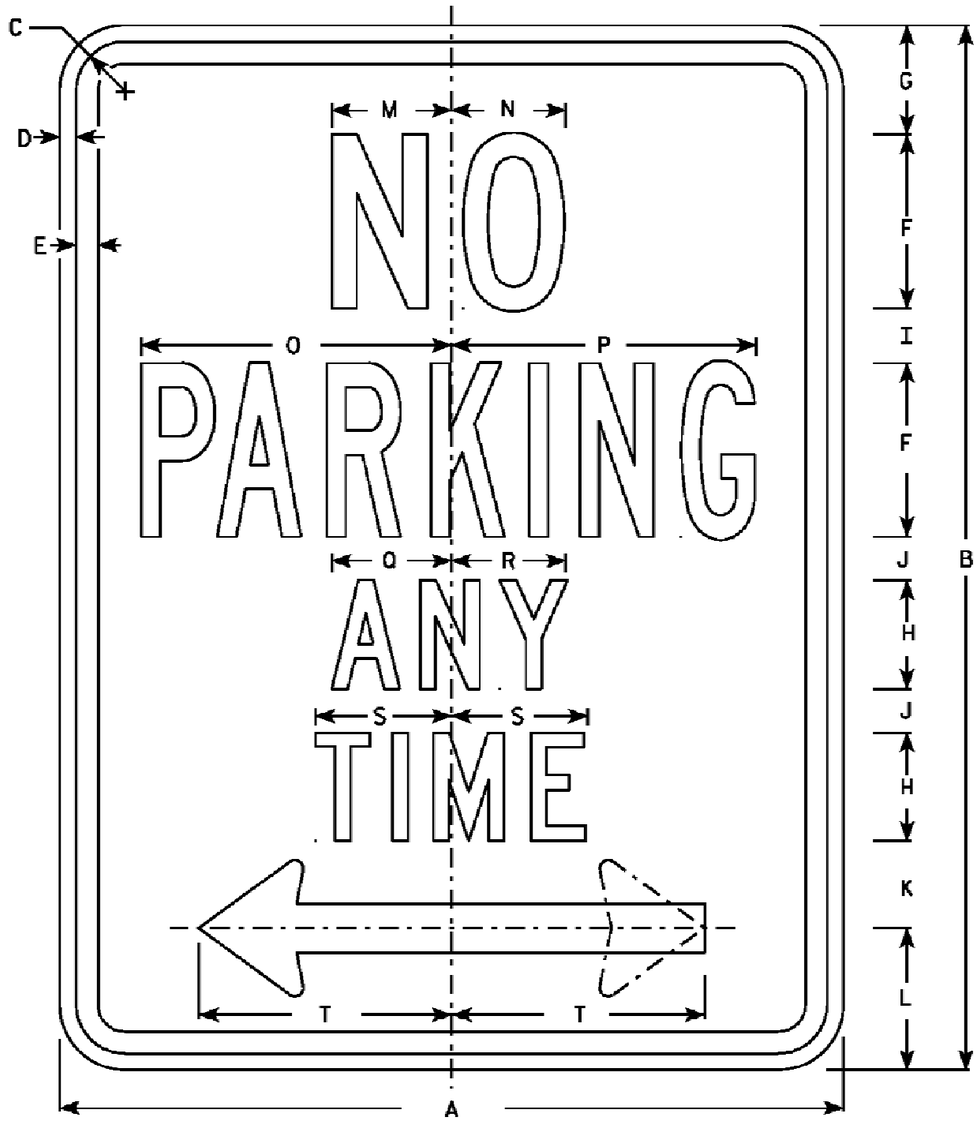
APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-L13

PROJECT NO: 5911-00-70 | HWY: LOCAL STREET | COUNTY: GREEN | SIGN PLATES | SHEET 75 | E

7

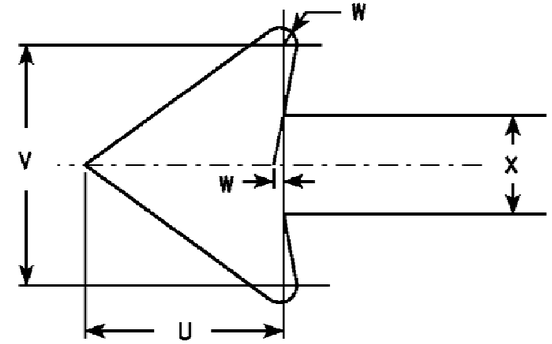
FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\070101_SP_48930008.DWG PLOT DATE : 1/28/2014 9:59 PM PLOT BY : KATHRYN BLASER PLOT NAME : PLOT SCALE : ##########



R7-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Red
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 3 and 4 are series C, line 2 is series B.
6. R7-1D (double arrow)
   R7-1L (left arrow)
   R7-1R (right arrow)



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 1/4	2 1/8	2 1/2	3 7/8	1 1/2	1 3/4	1/8	3/4			1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	2 3/4	2 5/8	3 1/8	5 7/8	2 1/4	2 5/8	1/4	1 1/8			3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
4																											
5																											

STANDARD SIGN
R7-1

WISCONSIN DEPT OF TRANSPORTATION

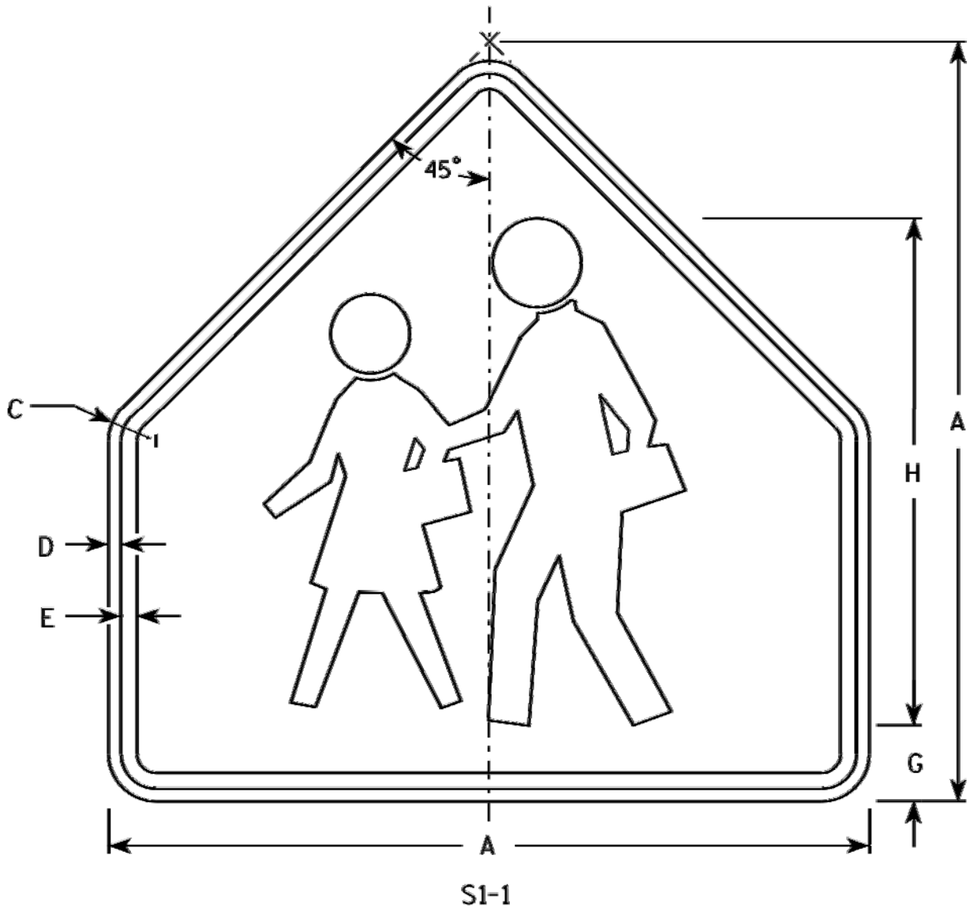
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/31/2011 PLATE NO. R7-1.9

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	SIGN PLATES	SHEET 76	E

7

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\070101_SP_48930008.DWG PLOT DATE : 1/28/2014 10:01 PM PLOT BY : KATHRYN BLASER PLOT NAME : PLOT SCALE : ##########



S1-1

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow-Green
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

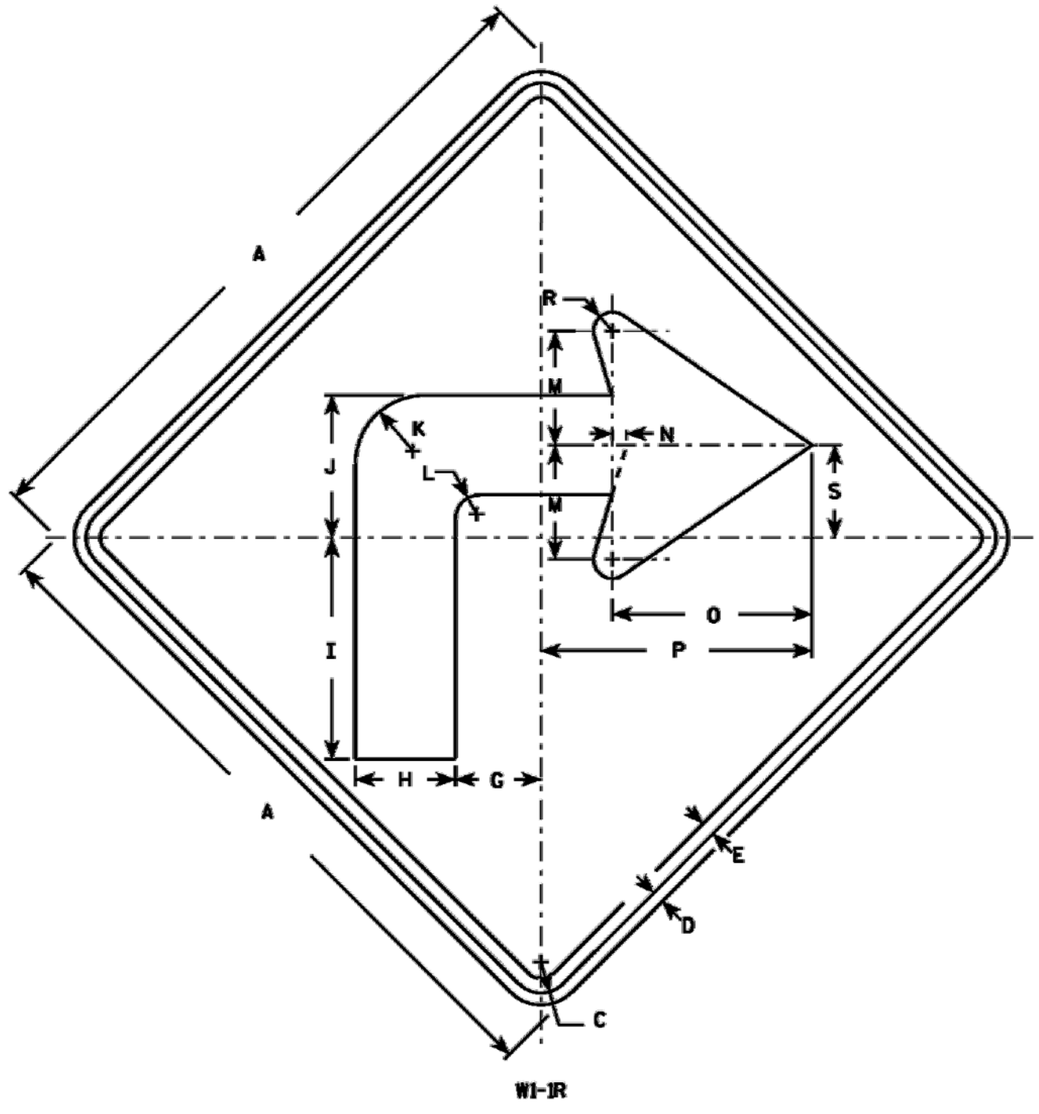
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8		3	20																			4.69
2	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
3	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
4	48		2 1/4	3/4	1		4 3/4	32																			12
5																											

STANDARD SIGN
S1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer
DATE 6/30/05 PLATE NO. SI-1.8

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	SIGN PLATES	SHEET 77	E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-1L is the same as W1-1R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		3	3 1/2	7 3/4	5	2 1/2	7/8	4	1/2	7	9 1/2		5/8	3 1/4								4.0
2S	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
2M	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
3	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
4	48		2 1/4	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0
5	48		2 1/4	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0

STANDARD SIGN
W1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew L Rauch
for State Traffic Engineer

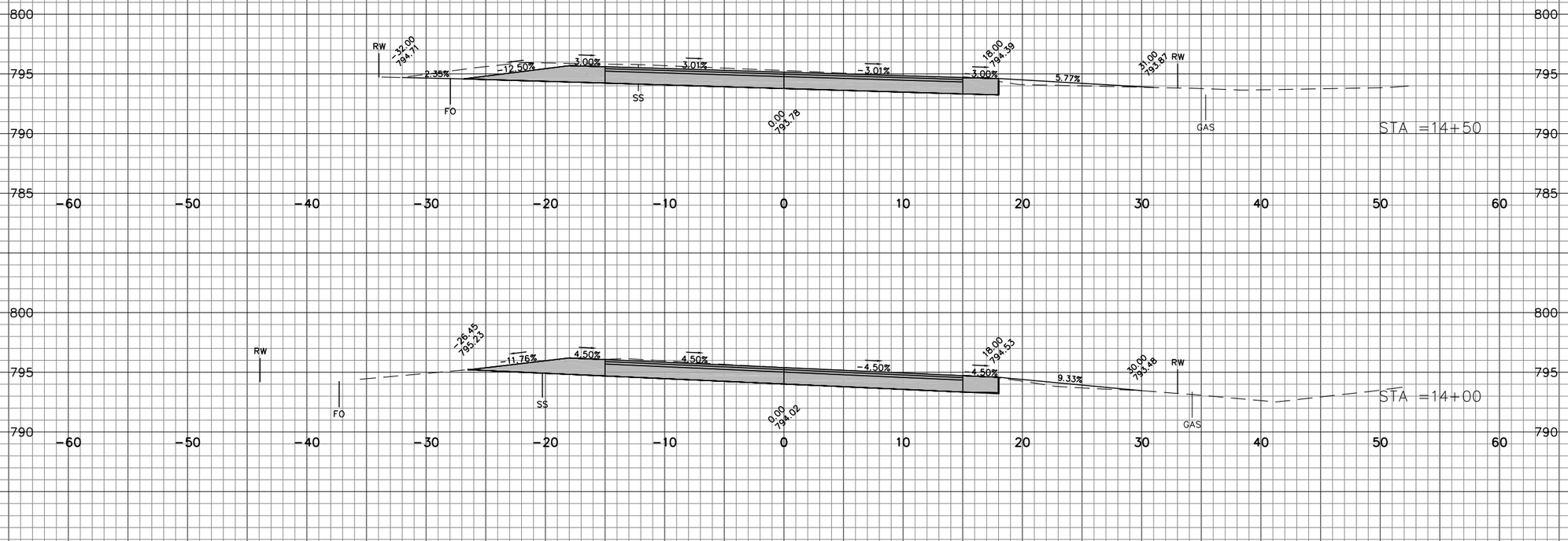
DATE 5/15/12 PLATE NO. W1-1.11

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	SIGN PLATES	SHEET 78	E

7

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\070101_SP_48930008.DWG PLOT DATE : 1/28/2014 10:15 PM PLOT BY : KATHRYN BLASER PLOT NAME : PLOT SCALE : ##########

STATION	Real Station	Distance	Cut	Salvaged/Unusable Pavement Material	Fill	Cut Note 1	Salvaged/Unusable Pavement Material Note 2	Fill Note 3	Cut 1.00 Note 1	Expanded Fill 1.25	Mass Ordinate Note 8
14+00	1400		62	0	0	0	0	0	0	0	0
14+50	1450	50	83	0	0	135	0	0	135	0	135
15+00	1500	50	84	0	0	155	0	0	290	0	289
15+50	1550	50	80	0	1	152	0	1	441	1	440
16+00	1600	50	91	0	0	158	0	1	600	2	598
16+50	1650	50	96	0	0	173	0	0	773	2	770
17+00	1700	50	98	0	1	180	0	1	952	4	949
17+50	1750	50	102	0	1	185	0	2	1137	6	1132
18+00	1800	50	117	0	0	203	0	1	1340	7	1333
18+50	1850	50	130	0	0	228	0	0	1568	7	1561
19+00	1900	50	104	0	2	216	0	2	1784	10	1774
19+50	1950	50	110	0	1	198	0	3	1982	14	1968
20+00	2000	50	119	0	1	212	0	2	2194	16	2178
20+50	2050	50	129	0	0	230	0	1	2424	17	2407
21+00	2100	50	128	0	0	238	0	0	2661	17	2644
21+50	2150	50	196	0	0	299	0	0	2961	17	2944
22+00	2200	50	132	0	1	304	0	1	3264	18	3247
22+50	2250	50	144	0	0	256	0	1	3520	19	3502
23+00	2300	50	126	0	1	250	0	1	3771	20	3750
23+50	2350	50	113	0	0	221	0	1	3992	22	3970
24+00	2400	50	110	0	1	206	0	1	4198	23	4175
24+50	2450	50	87	0	1	182	0	2	4380	25	4355
25+00	2500	50	95	0	0	168	0	1	4549	27	4522
25+50	2550	50	79	0	2	162	0	2	4710	29	4681
26+00	2600	50	92	0	0	159	0	2	4869	31	4838
26+50	2650	50	100	0	2	178	0	2	5047	34	5013
27+00	2700	50	103	0	0	188	0	2	5235	37	5198
27+50	2750	50	109	0	0	197	0	0	5432	37	5395
28+00	2800	50	152	0	0	242	0	1	5674	38	5636
28+50	2850	50	121	0	0	253	0	0	5926	38	5888
29+00	2900	50	117	0	0	220	0	0	6146	38	6108
29+50	2950	50	112	0	0	212	0	0	6358	38	6320
30+00	3000	50	98	0	0	195	0	0	6553	38	6514
30+50	3050	50	82	0	2	167	0	2	6720	40	6679
31+00	3100	50	80	0	1	150	0	3	6869	44	6825
31+50	3150	50	101	0	0	168	0	1	7037	46	6992
32+00	3200	50	108	0	0	194	0	0	7231	46	7185
32+50	3250	50	115	0	0	206	0	0	7437	46	7391
33+00	3300	50	109	0	0	207	0	0	7644	46	7597
33+50	3350	50	97	0	0	191	0	0	7834	47	7788
34+00	3400	50	80	0	0	164	0	0	7999	47	7952
34+50	3450	50	94	0	0	162	0	0	8160	47	8113
35+00	3500	50	109	0	0	188	0	0	8348	47	8301
35+50	3550	50	80	0	0	174	0	0	8522	47	8475
36+00	3600	50	79	0	0	147	0	0	8669	48	8622
36+50	3650	50	84	0	0	152	0	0	8821	48	8773
37+00	3700	50	82	0	0	180	0	0	9001	49	8952
37+50	3750	50	79	0	0	149	0	0	9150	49	9100
						9150	0	40			



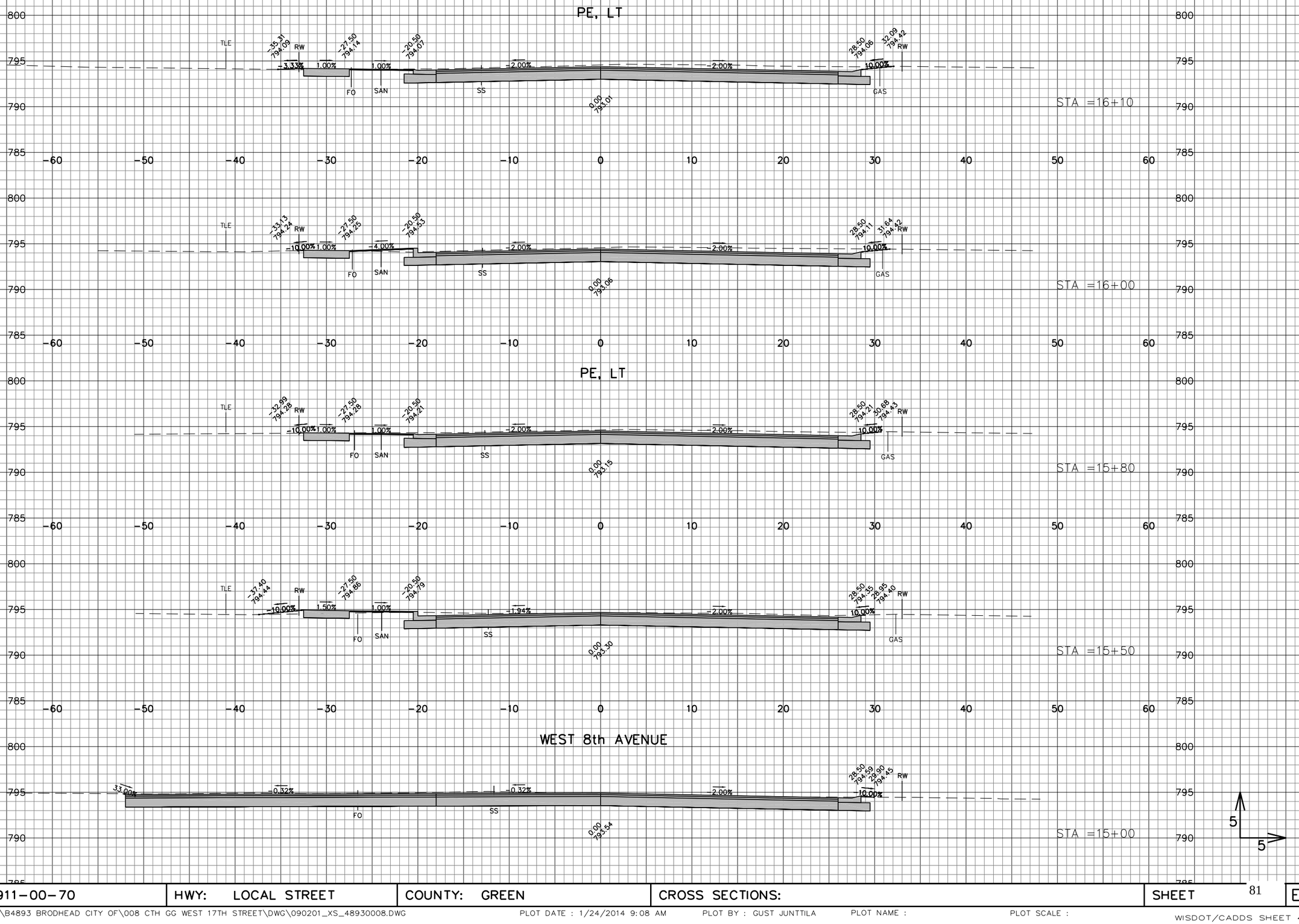
BEGIN CONSTRUCTION – STA 14+00

9

9

PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 80	E

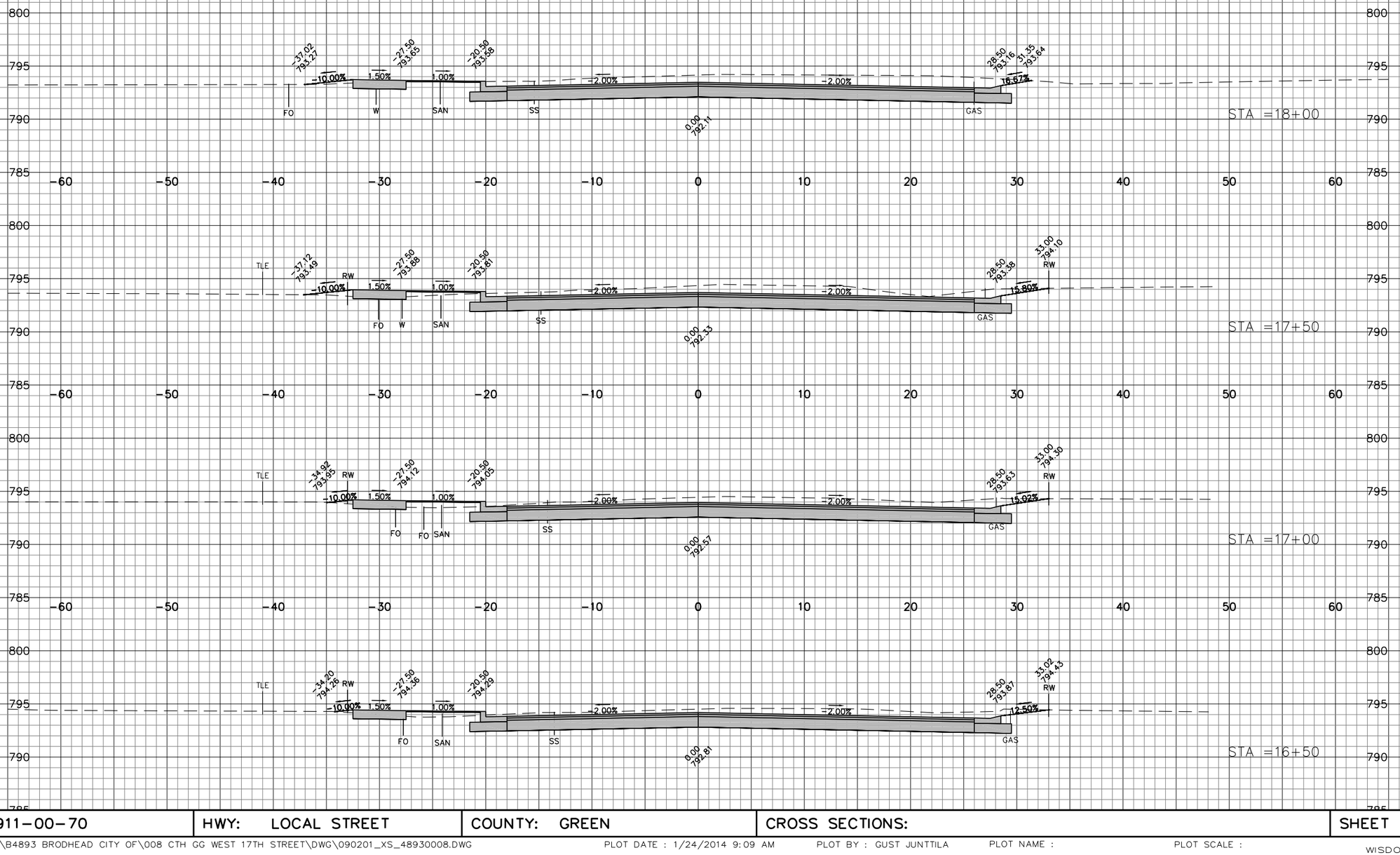
FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\090201_XS_48930008.DWG PLOT DATE : 1/24/2014 9:06 AM PLOT BY : GUST JUNTTILA PLOT NAME : PLOT SCALE :



PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET E

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\090201_XS_48930008.DWG PLOT DATE : 1/24/2014 9:08 AM PLOT BY : GUST JUNTTILA PLOT NAME : PLOT SCALE :

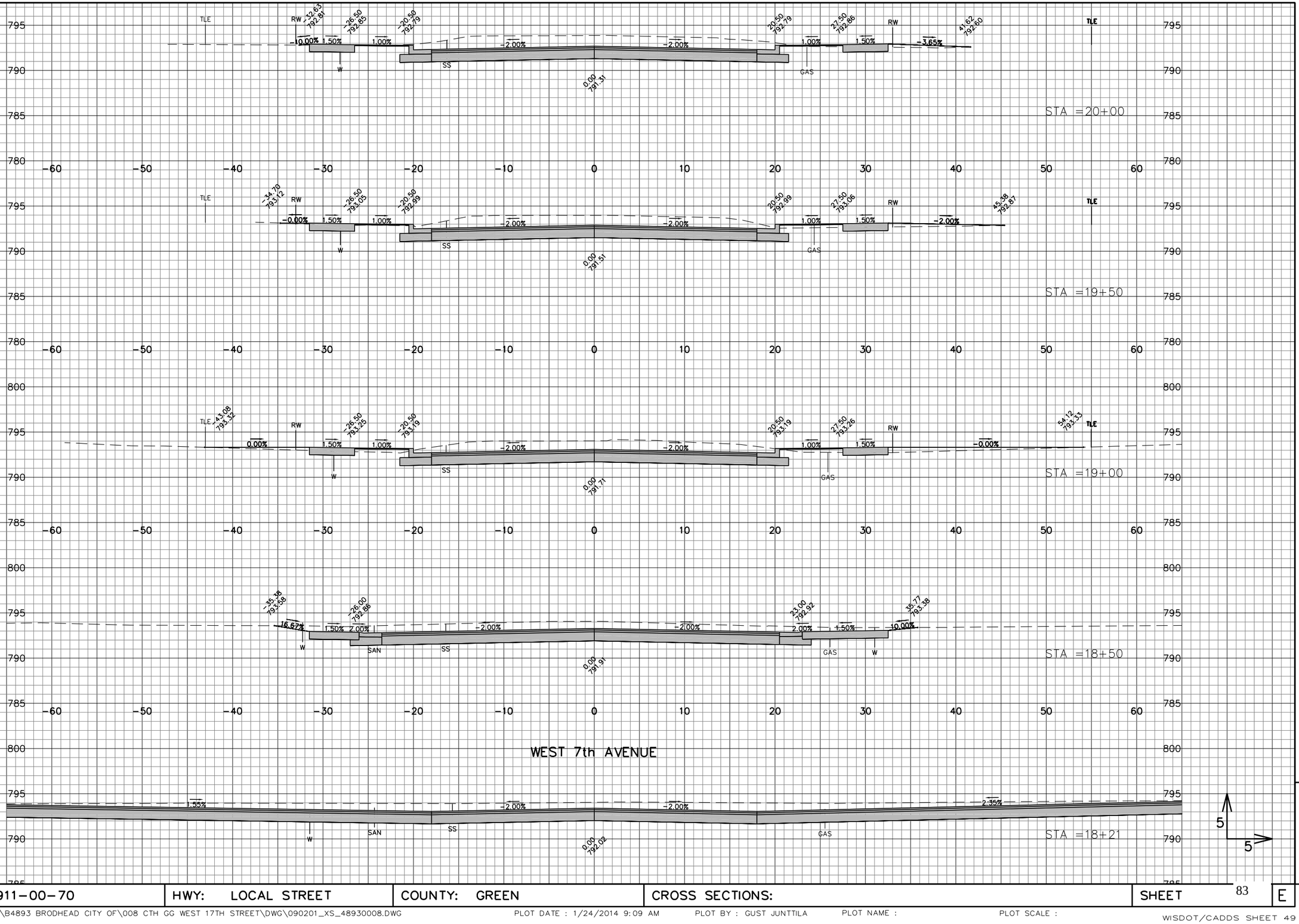
WISDOT/CADDS SHEET 49



9

9

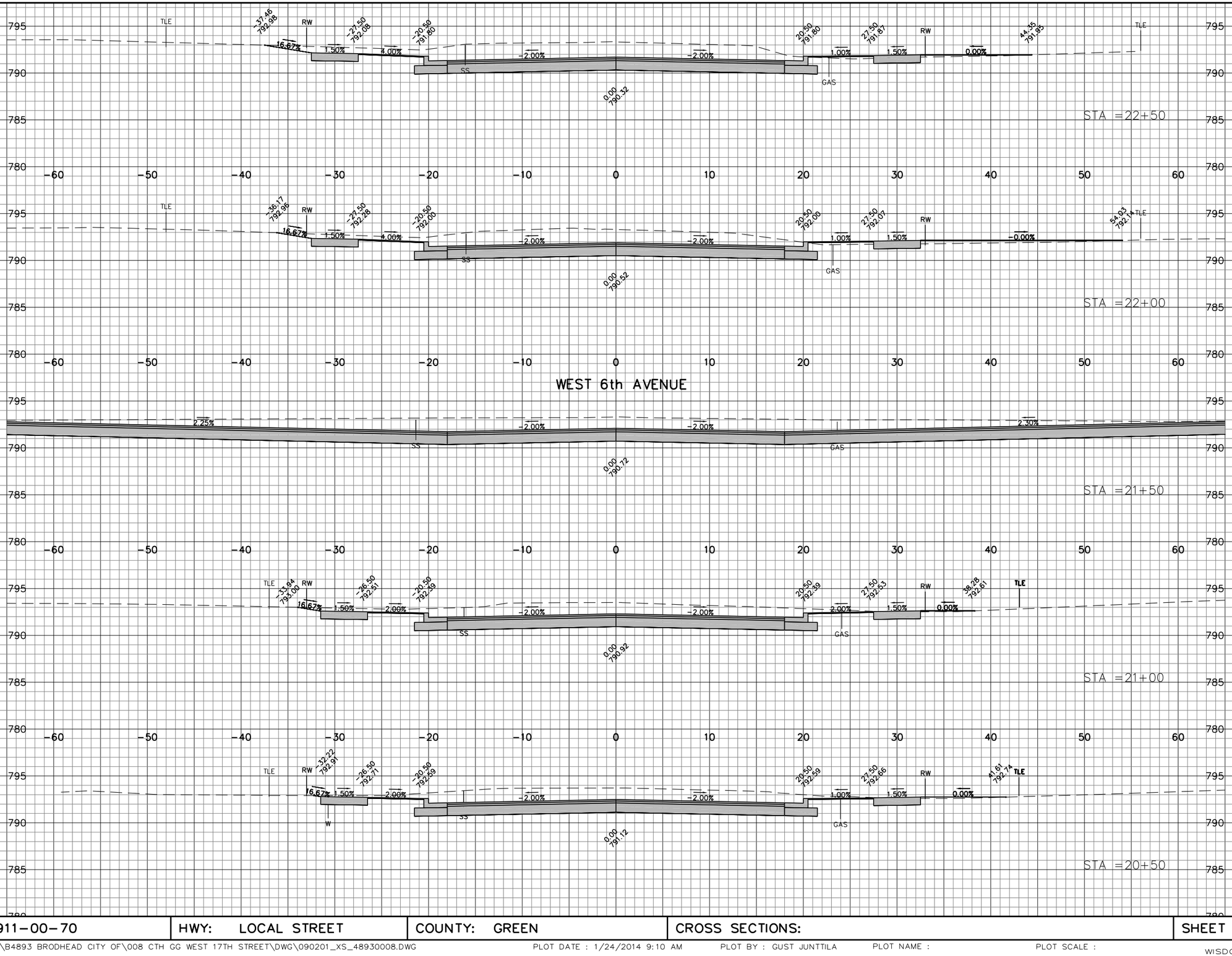
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 82 E



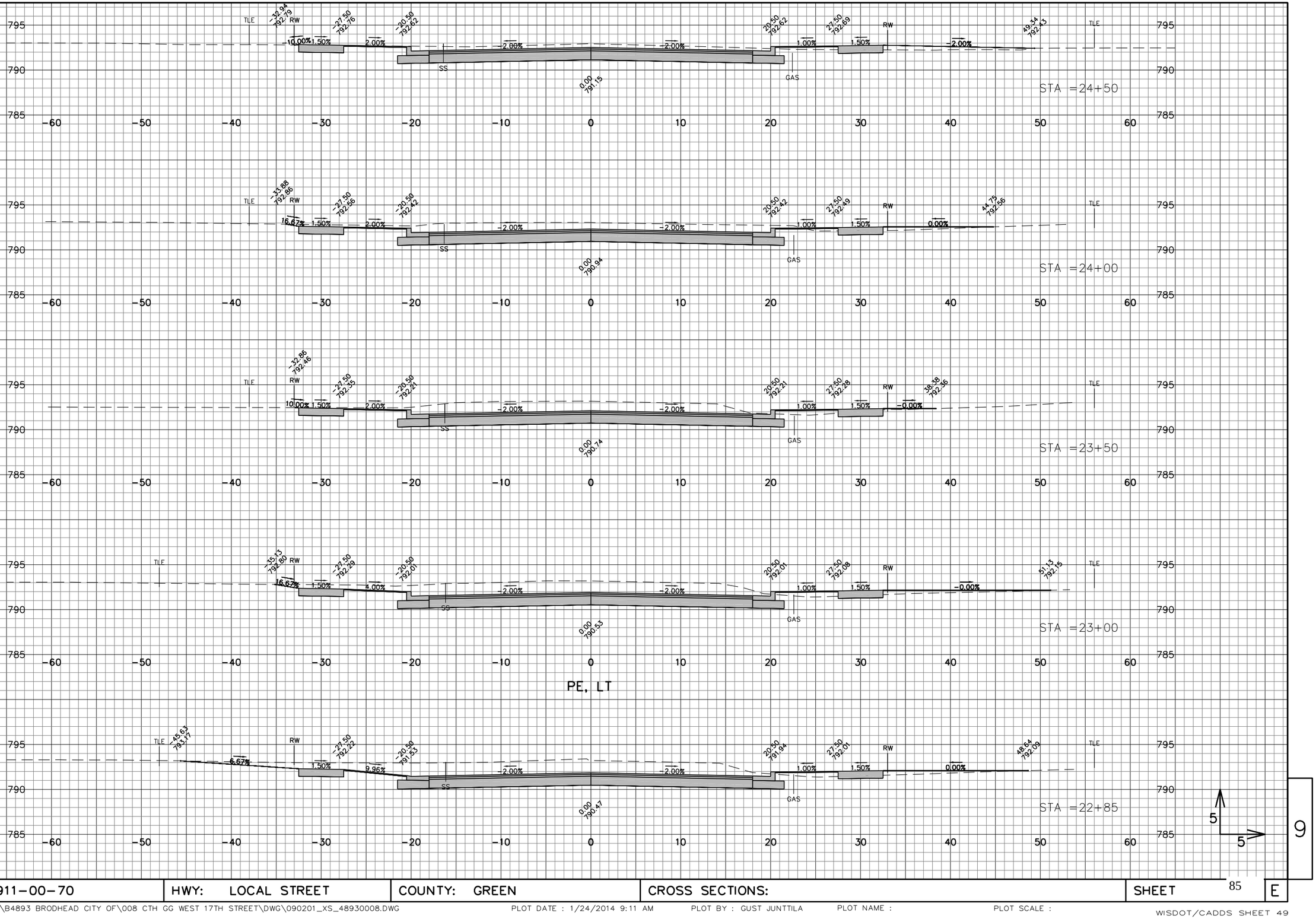
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 83	E

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\090201_XS_48930008.DWG PLOT DATE : 1/24/2014 9:09 AM PLOT BY : GUST JUNTTILA PLOT NAME : PLOT SCALE :

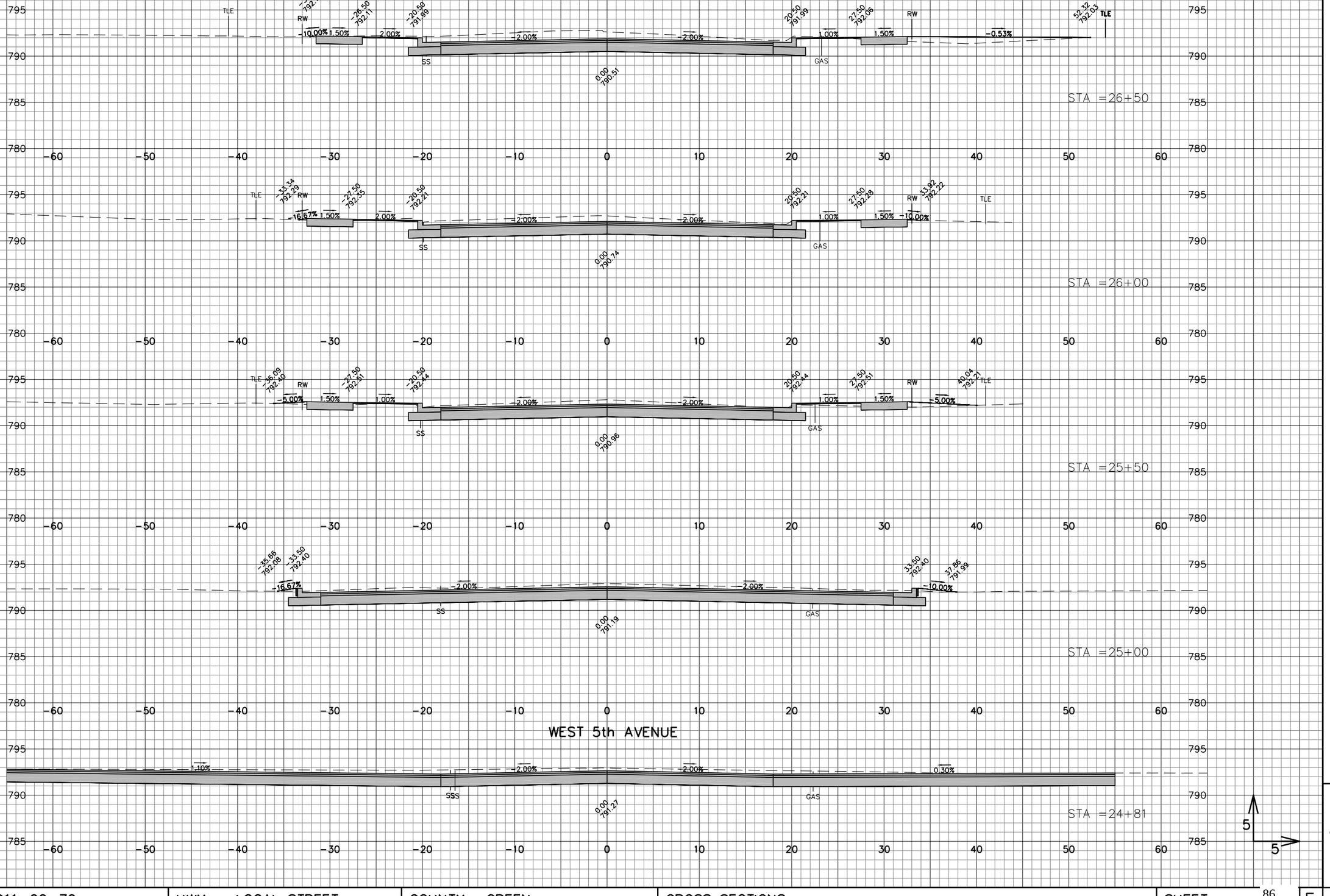
WISDOT/CADDS SHEET 49



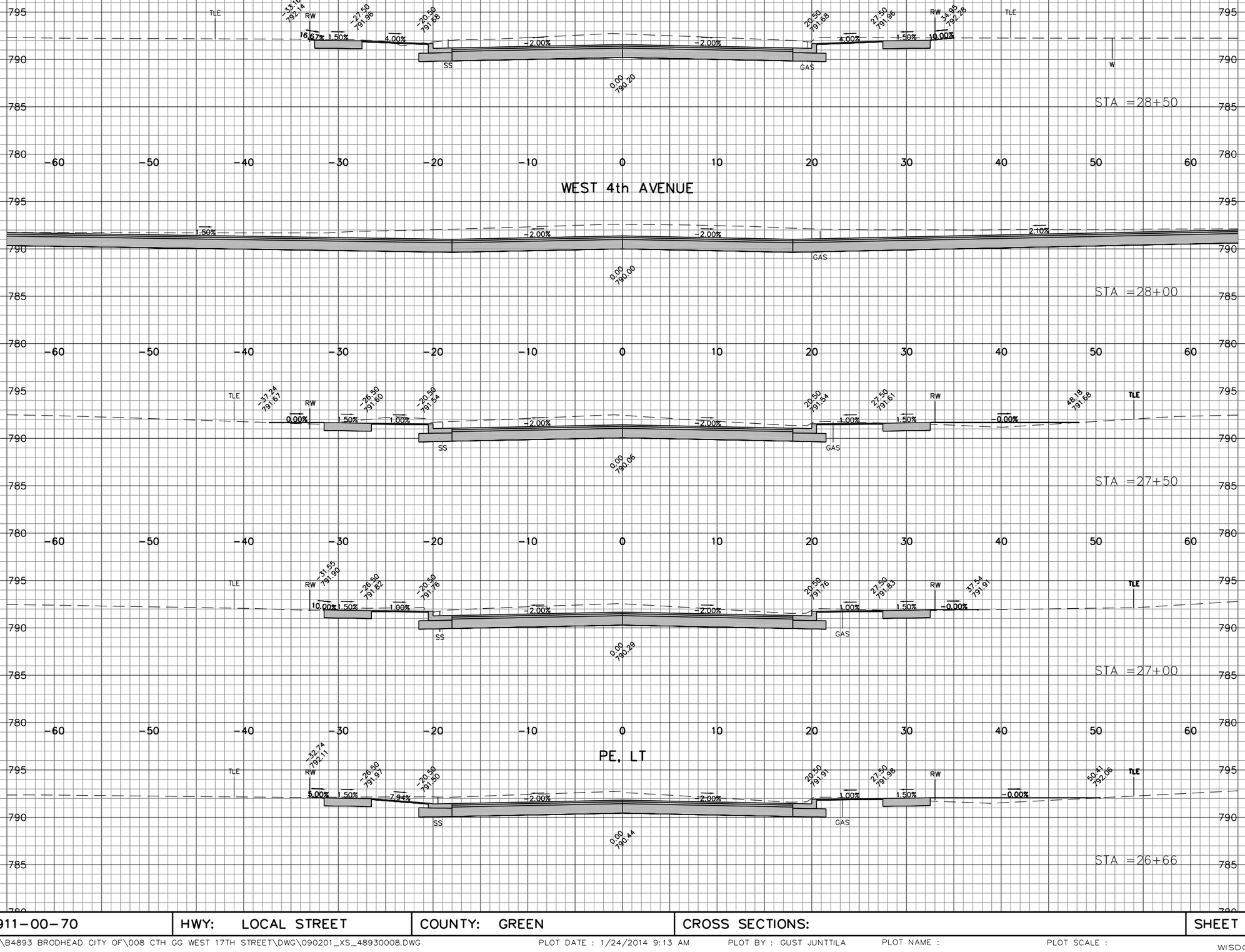
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 84 E



PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 85 E



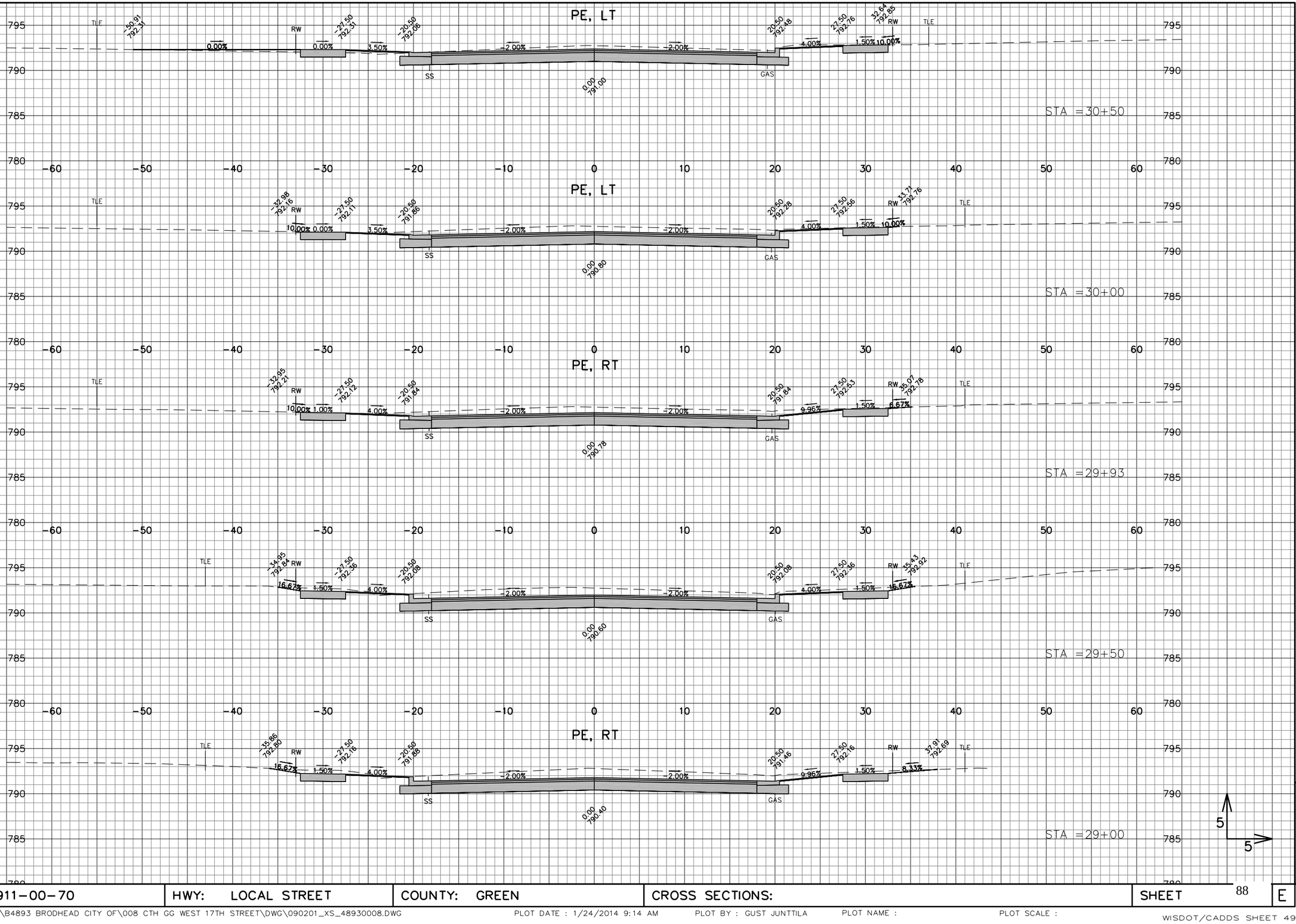
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 86	E



PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 87 E

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\090201_XS_48930008.DWG PLOT DATE : 1/24/2014 9:13 AM PLOT BY : GUST JUNTTILA PLOT NAME : PLOT SCALE :

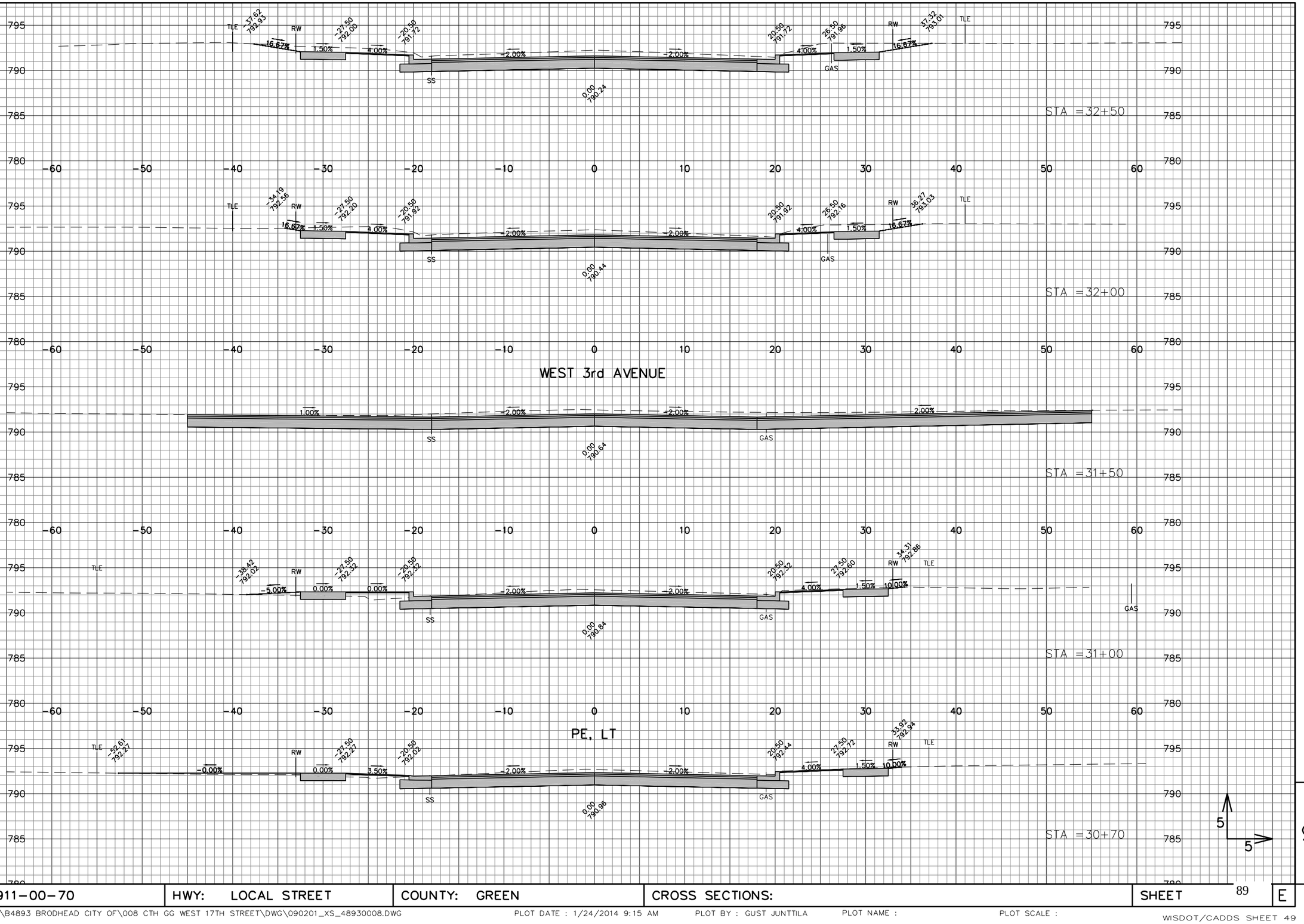
WISDOT/CADDS SHEET 49



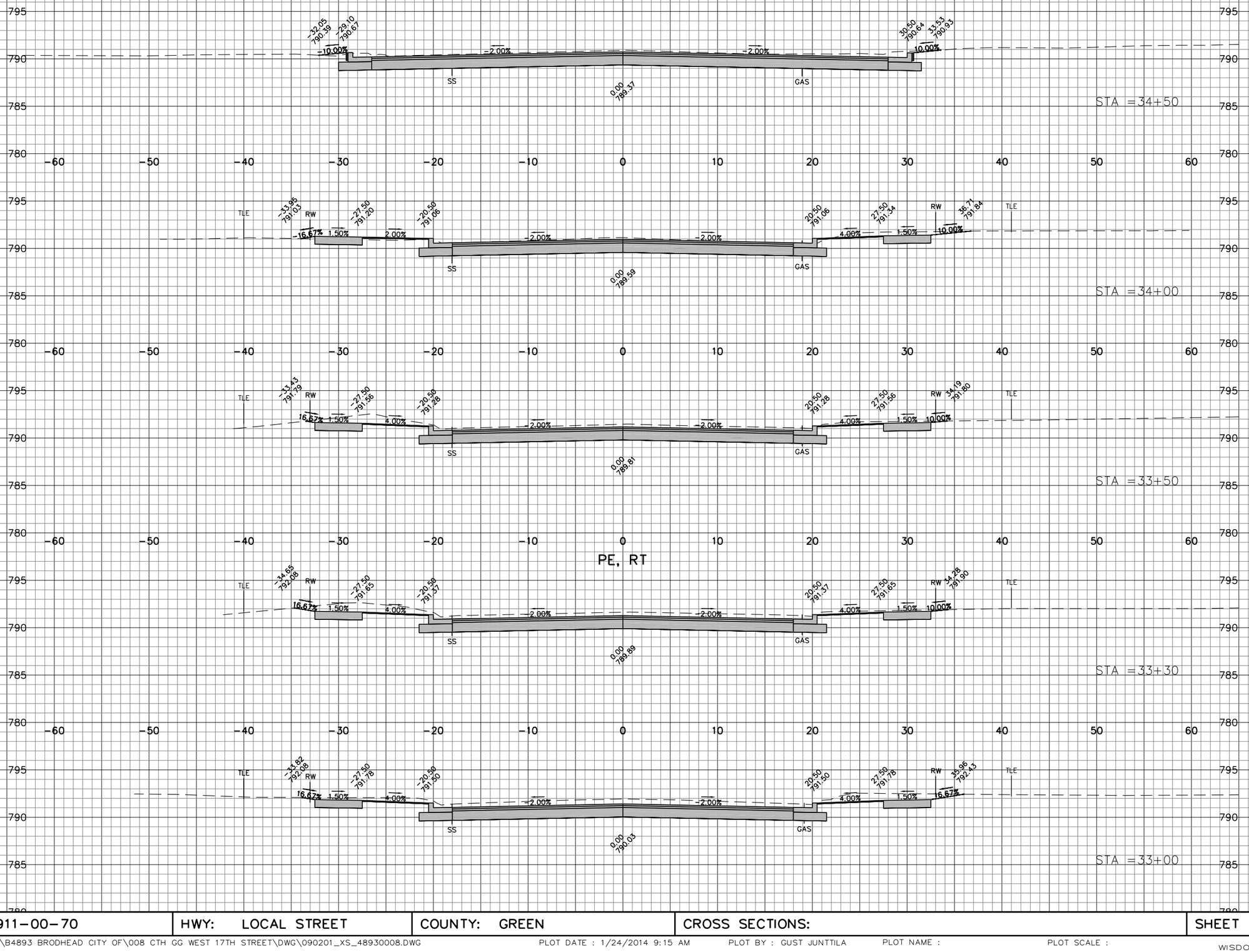
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET E

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\090201_XS_48930008.DWG PLOT DATE : 1/24/2014 9:14 AM PLOT BY : GUST JUNTTILA PLOT NAME : PLOT SCALE :

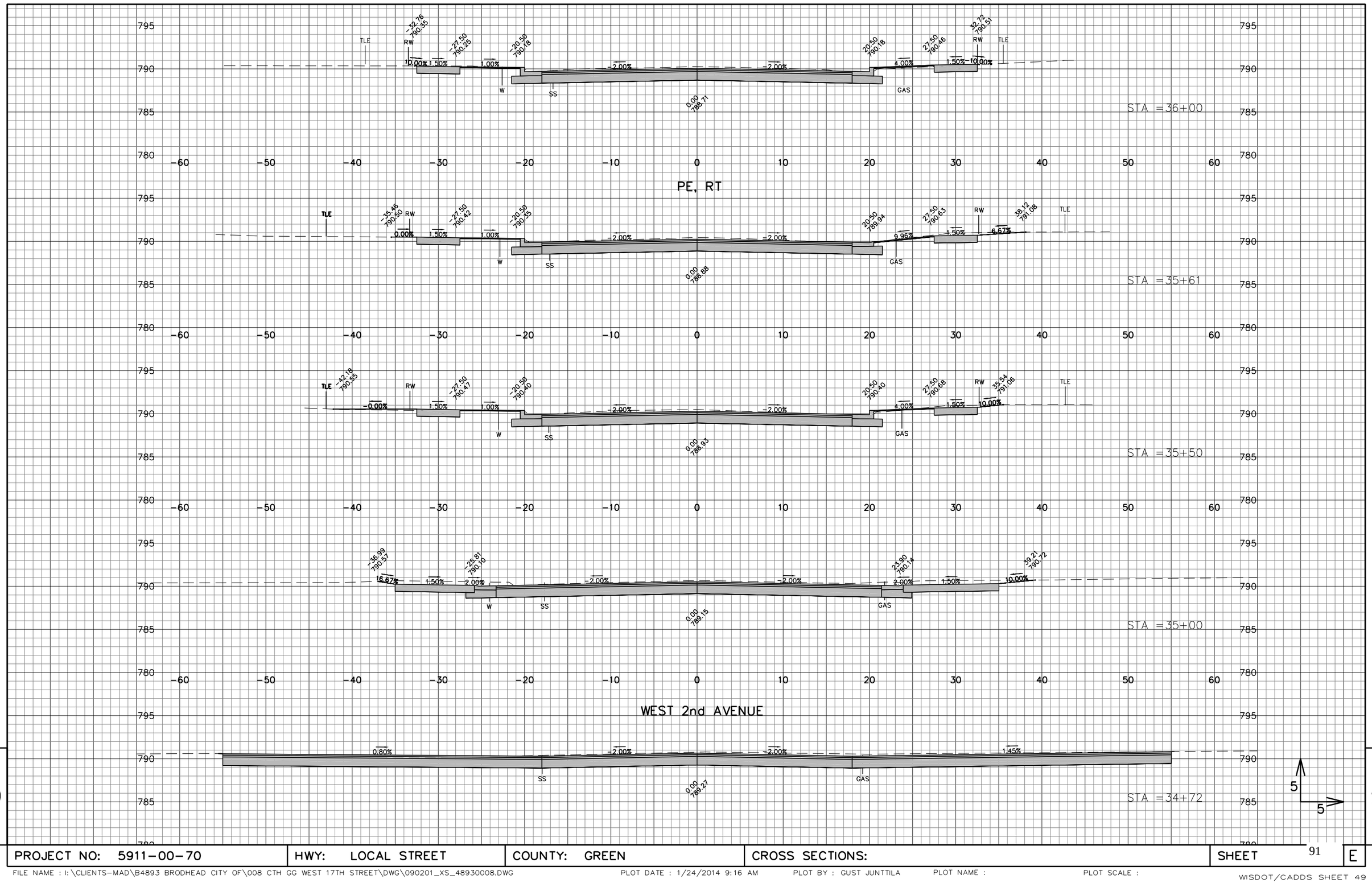
WISDOT/CADDS SHEET 49



PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 89 E



PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 90	E



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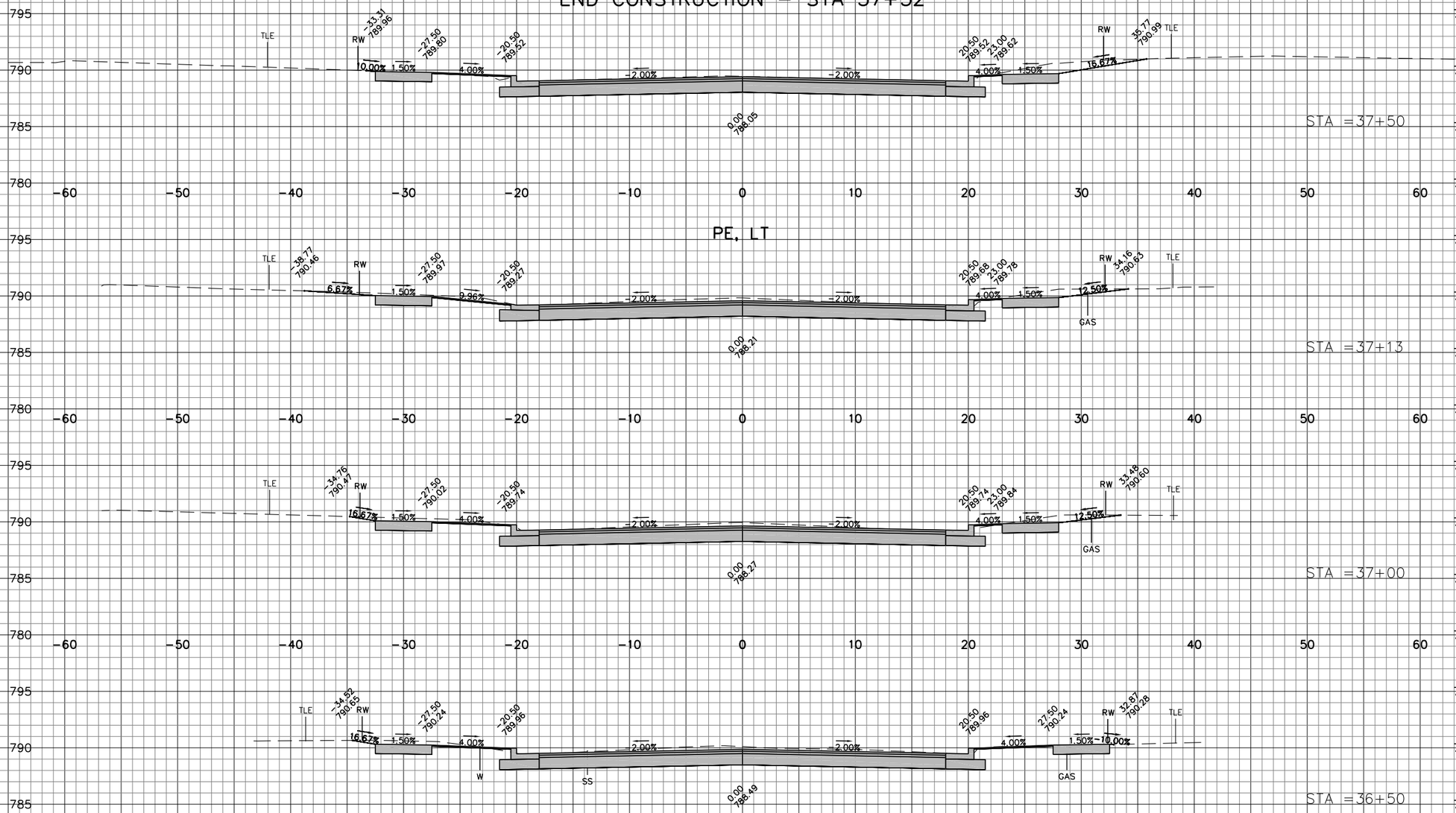
PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 91 E

FILE NAME : I:\CLIENTS-MAD\B4893 BRODHEAD CITY OF\008 CTH GG WEST 17TH STREET\DWG\090201_XS_48930008.DWG PLOT DATE : 1/24/2014 9:16 AM PLOT BY : GUST JUNTTILA PLOT NAME : PLOT SCALE :

WISDOT/CADDS SHEET 49

END CONSTRUCTION – STA 37+52



PROJECT NO: 5911-00-70	HWY: LOCAL STREET	COUNTY: GREEN	CROSS SECTIONS:	SHEET 92 E

Notes

Wisconsin Department of Transportation

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