

SUP JUNE 2014

PROJECT ID: 1580-12-72

COUNTY: RUSK

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 144

STATE OF WISCONSIN

DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

C LADYSMITH, WEST NINTH STREET

MINER AVENUE TO FLAMBEAU AVENUE

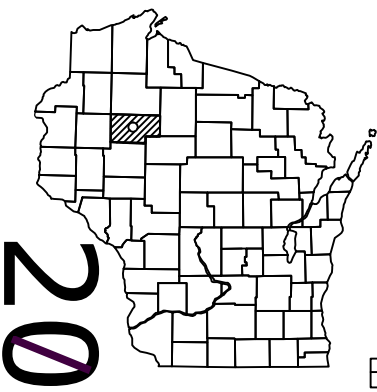
STH 27

RUSK COUNTY

STATE PROJECT NUMBER

1580-12-72

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1580-12-72	WISC 2014246	1



BEGIN CONSTRUCTION (USH 8)

STA. 320+36

N=563535.3866
E=809859.3663

DESIGN DESIGNATION

A.A.D.T. 2014	=	8300
A.A.D.T. 2024	=	9700
D.H.V.	=	12.3%
D.D.	=	58/42
T.	=	15.4%
DESIGN SPEED	=	25 MPH
ESALS	=	5,686,700

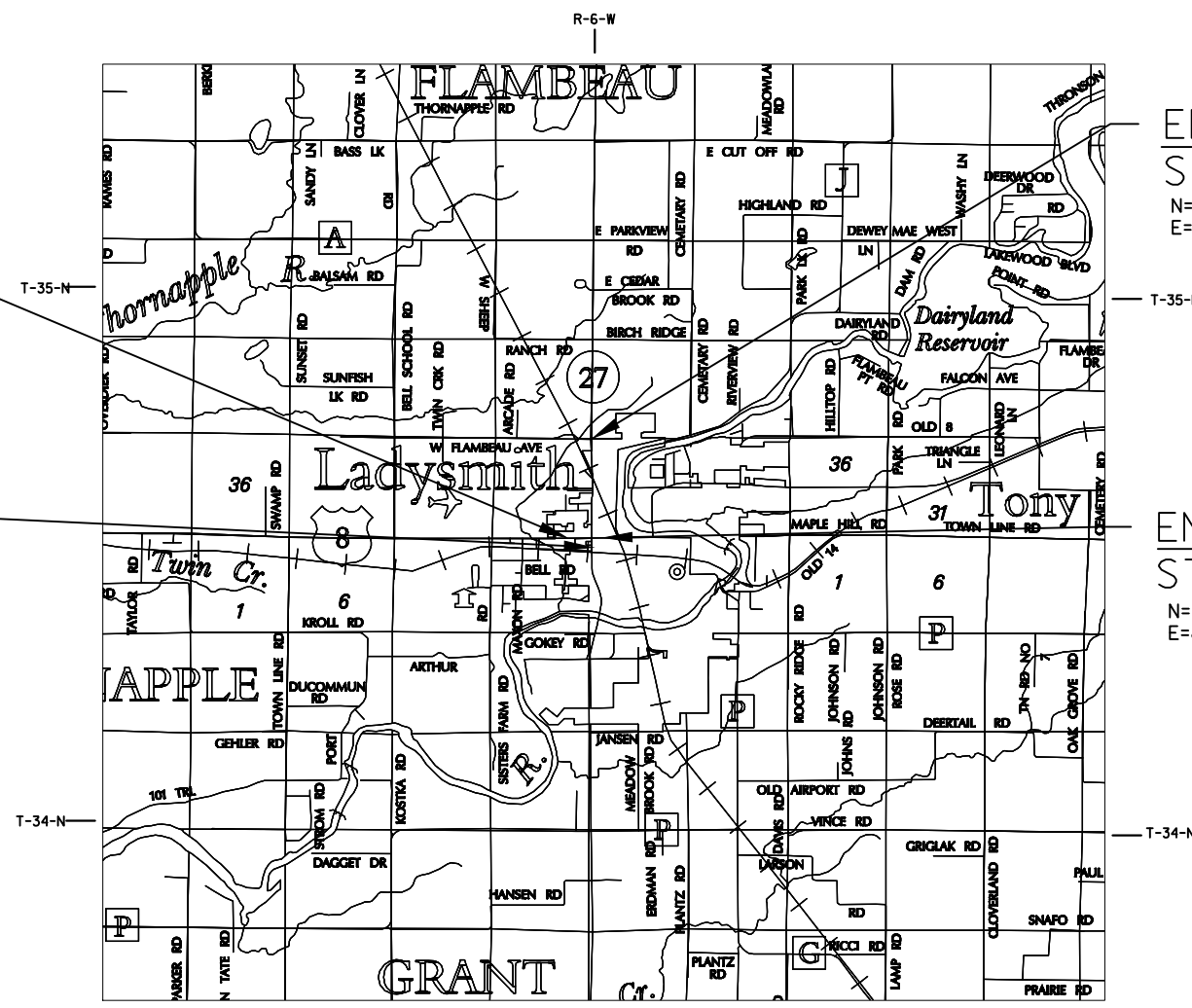
BEGIN PROJECT (STH 27)

STA. 630+00

N=563269.3276
E=810573.7761

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
	STORM SEWER
	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE
MARSH AREA	
WOODED OR SHRUB AREA	



LAYOUT

SCALE 0 1.0

TOTAL NET LENGTH OF CENTERLINE = 1.203

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, RUSK COUNTY, NAD83 (1991), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

END PROJECT (STH 27)

STA. 687+58

N=568931.2199
E=810492.1788

END CONSTRUCTION (USH 8)

STA. 326+28

N=563553.6158
E=810848.9497

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor: MICHAEL PILLER

Designer: MICHAEL PEARSON

Project Manager: PHILLIP KEPPERS

Regional Examiner: DANIEL OJIBWAY

Regional Supervisor: DAVID OSTROWSKI

APPROVED FOR THE DEPARTMENT

DATE: 1/14/14

(Signature)

E

LIST OF STANDARD ABBREVIATIONS

ABUT.	ABUTMENT
AGG.	AGGREGATE
AH.	AHEAD
APPROX.	APPROXIMATE
A.E.W.	APRON ENDWALL
ASPH.	ASPHALTIC
A.D.T.	AVERAGE DAILY TRAFFIC
AZ.	AZIMUTH
BK.	BACK
BEG.	BEGIN
B.M.	BENCH MARK
C/L	CENTER LINE
CONC.	CONCRETE
CONST.	CONSTRUCTION
CO.	COUNTY
C.T.H.	COUNTY TRUNK HIGHWAY
X-SEC.	CROSS SECTION
CR.	CRUSHED
CFS	CUBIC FEET/SECOND
C.Y., CU. YD.	CUBIC YARD
CULV.	CULVERT
C.P.	CULVERT PIPE
D.O.T.	DEPARTMENT OF TRANSPORTATION
D.H.V.	DESIGN HOUR VOLUME
DIA.	DIAMETER
D.	DIRECTIONAL DISTRIBUTION
DISCH. OR DIS.	DISCHARGE
EA.	EACH
ELECT.	ELECTRIC
EL. OR ELEV.	ELEVATION
EMB.	EMBANKMENT
E.B.S.	EXCAVATION BELOW SUBGRADE
EXIST.	EXISTING
FERT.	FERTILIZE
F.E.	FIELD ENTRANCE
FIN.	FINISHED
FT.	FOOT
F.L.	FLOW LINE
GA.	GAUGE
HORIZ.	HORIZONTAL
CWT.	HUNDREDWEIGHT
•INL.	INLET
LT.	LEFT
L.H.F.	LEFT-HAND FORWARD
LIN.	LINEAR
LIN. FT.	LINEAR FOOT
L.S.	LUMP SUM
MAX.	MAXIMUM
MI.	MILE
MISC.	MISCELLANEOUS
N.E.	NORTH EAST
N.W.	NORTH WEST
PAV'T	PAVEMENT
P.C.	POINT OF CURVATURE
P.I.	POINT OF INTERSECTION
P.T.	POINT OF TANGENCY
P.O.T.	POINT ON TANGENT
LB.	POUND
P.E.	PRIVATE ENTRANCE
PROJ.	PROJECT
R.	RANGE
REQ'D	REQUIRED
RT.	RIGHT
R.H.F.	RIGHT-HAND FORWARD
R/W	RIGHT OF WAY
RD.	ROAD
SHR.	SHRINKAGE
SL.	SLOPE
STD.	STANDARD
S.D.D.	STANDARD DETAIL DRAWINGS
S.T.H.	STATE TRUNK HIGHWAY
STA.	STATION
S.P.P.A.	STRUCTURAL PLATE PIPE ARCH
STRUCT.	STRUCTURE
SURF.	SURFACE
TEL.	TELEPHONE
TN.	TOWN
T.	TRUCKS (PERCENT OF)
UNCL.	UNCLASSIFIED
U.G.	UNDERGROUND
V.	VELOCITY OR DESIGN SPEED
V.C.	VERTICAL CURVE

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY FACILITIES AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

CROSS SLOPES AS SHOWN ON THE TYPICAL SECTION WILL VARY AT THE INTERSECTIONS. SEE DETAIL SHEETS AND CROSS SECTIONS FOR SLOPES AND GRADES.

WHEN THE QUANTITY OF BASE AGG. DENSE AND ASPHALTIC PAVEMENT ARE MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS AS SHOWN ON THE PLAN IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND UPON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER IN THE FIELD.

CURVE DATA SHOWN ON THE PLAN IS "ARC DEFINITION".

ALL DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS BETWEEN THE SUBGRADE SHOULDER POINTS, SHALL BE COVERED WITH SEED, FERTILIZER AND E-MAT.

THE EXACT LOCATION OF PRIVATE ENTRANCES ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

CONTROL POINTS ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM RUSK COUNTY. HOIZONTAL DATUM NAD83 BENCHMARK ELEVATIONS ARE REFERENCED TO NAVD 88.

EXPANSION JOINTS TO BE CONSTRUCTED AT ALL RADIUS POINTS IN CURB AND GUTTER OR AT LOCATIONS SHOWN ON THE PLAN.

ALL RADII ARE MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE SHOWN OR NOTED ON THE PLAN.

ITEMS SHOWN ON THE PLANS AND NOT INCLUDED IN THE ESTIMATE OF QUANTITIES ARE NOT PART OF THIS CONTRACT.

INLET GRATE ELEVATIONS REFERRED TO ON INLET NOTES ARE UNADJUSTED GUTTER FLOW LINE ELEVATIONS.

THE STATE WILL FURNISH MOUNTING HARDWARE FOR TYPE II SIGNS THAT ARE MOUNTED ON POLES. CONTACT MORRIS LUKE (715) 392-7886

SUPERELEVATED SECTIONS ARE TO BE MILLED AND RESURFACED AT THE EXISTING CROSS SLOPE.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

THE ACTUAL LOCATION OF BUTT JOINTS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

PROVIDE DOCUMENTATION OF ANY ADJUSTMENTS TO MONITORING WELLS TO THE DNR ACCORDING TO SPECIAL PROVISIONS.

AFTER UTILITY LOCATION, PRIOR TO EXCAVATION FOR NEW SIGN SUPPORT BASES, CONTACT CHARTER COMMUNICATIONS FOR RELOCATION OF UTILITIES THAT INTERFERE WITH CONCRETE BASE EXCAVATION.

RAILROAD

WISCONSIN CENTRAL LTD
1625 DEPOT ST.
STEVENS POINT, WI 54481
ATTN: JACKIE MACEWICZ
PHONE: (715) 345-2503
JIM.ARQUETTE@CENTURYLINK.COM

UTILITIES

CENTURYLINK

P.O. BOX 13
SHELDON, WI 54766
ATTN: JIM ARQUETTE
PHONE: (715) 452-5168
JIM.ARQUETTE@CENTURYLINK.COM

JUMP RIVER ELEC. COOP.
1102 W. NINTH ST. NORTH
P.O. BOX 99
LADYSMITH, WI 54848-099
ATTN: HANK LEW
PHONE: (715) 532-5524

CHARTER COMMUNICATIONS

2304 MAIN ST.
RICE LAKE, WI 54868
ATTN: TOM HAASE
PHONE: (715) 234-5341 EXT 252
CELL 715-370-1601

LADYSMITH MUNICIPLE WATER

120 MINER AVE.
P.O. BOX 431
LADYSMITH, WI 54848-0431
ATTN: KURTIS GORSEGNER
PHONE: (715) 532-2603

XCEL ENERGY

29270 COUTNY RD G
PHILLIPS, WI 54555
ATTN: JASON MCROBERTS
PHONE: (715) 737-1198

WE ENERGIES (GAS/PETROLEUM)
1921 8TH ST. SOUTH
WISCONSIN RAPIDS, WI 54494
ATTN: THOMAS KROSTAG
PHONE: (715) 421-7268

DNR CONTACT

ANDREW BARTA
WDNR - NORTHWEST DISTRICT
HEADQUARTERS
810 WEST MAPLE STREET
SPOONER, WI 54801
PHONE: (715) 635-4071

DESIGN CONTACT

MICHAEL PEARSON
WISDOT NORTHWEST REGION
1701 N. 4TH STREET
SUPERIOR WI, 54880
PHONE: (715) 395-3024

RUSK COUNTY HIGHWAY

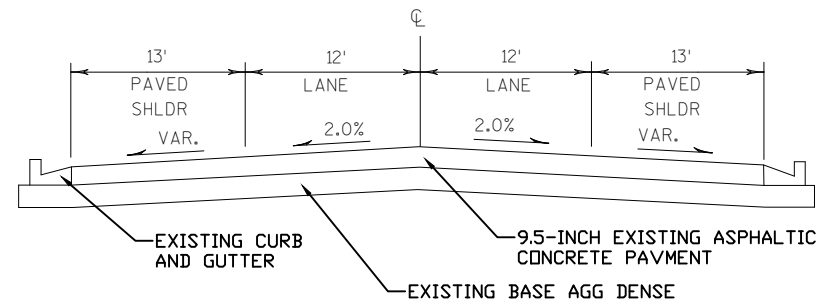
COMMISSIONER
PHILLIP MONTWILL
N4711 HWY 27
LADYSMITH, WI 54848
PHONE: (715) 532-2633

CITY OF LADYSMITH

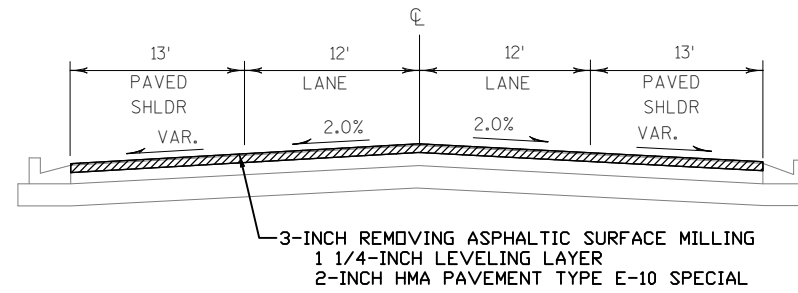
120 MINER AVE.
LADYSMITH, WI 54848
PHONE: (715) 532-2600



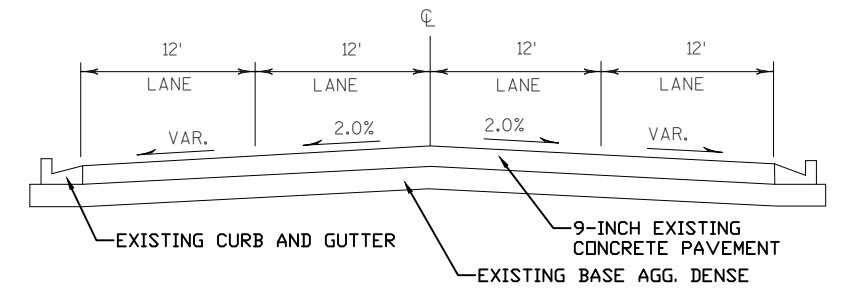
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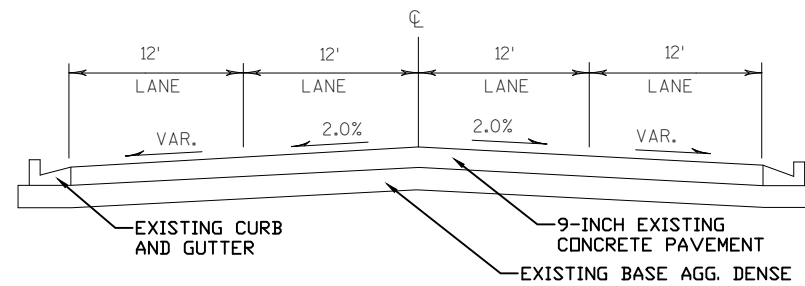
USH 8 EXISTING TYPICAL SECTION
STA 320+36 TO STA 323+25



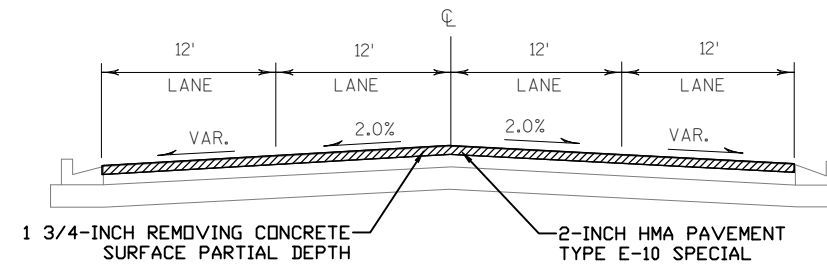
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STA 320+36 TO STA 323+25



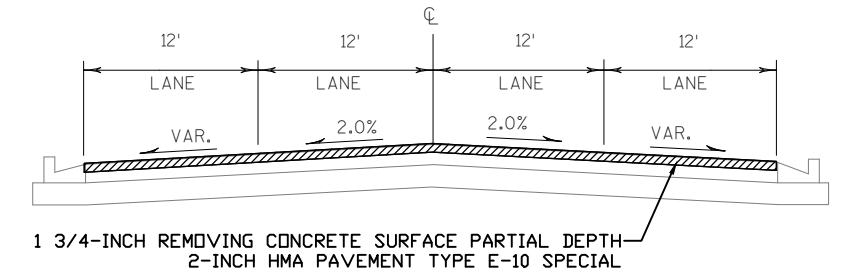
STH 27 EXISTING TYPICAL SECTION
STA 632+20 TO STA 633+75



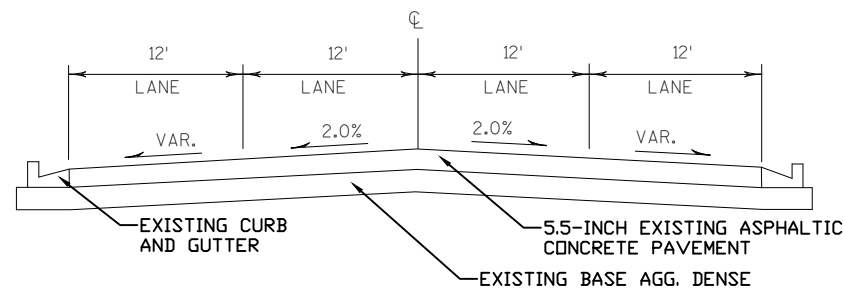
USH 8 EXISTING TYPICAL SECTION
STA 323+25 TO STA 326+28



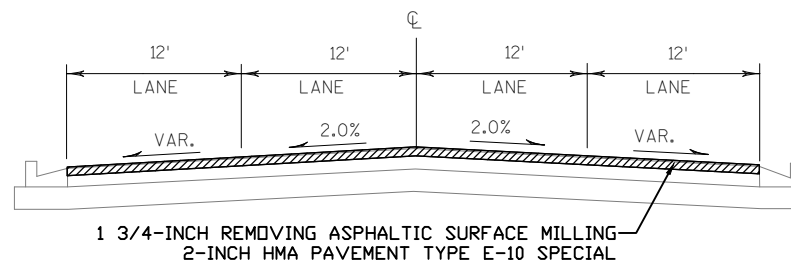
USH 8 FINISHED TYPICAL SECTION
STA 323+25 TO STA 326+28



STH 27 FINISHED TYPICAL SECTION
STA 632+20 TO STA 633+75



STH 27 EXISTING TYPICAL SECTION
STA 630+00 TO STA 630+85



STH 27 FINISHED TYPICAL SECTION
STA 630+00 TO STA 630+85

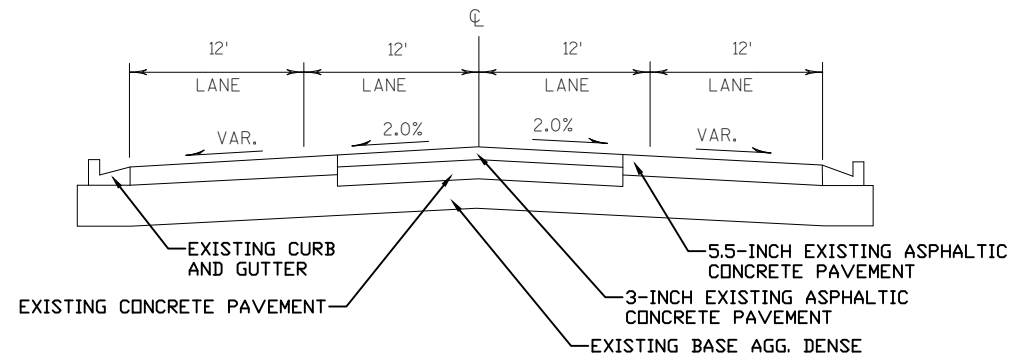
PROJECT NO: 1580-12-72

HWY: STH 27

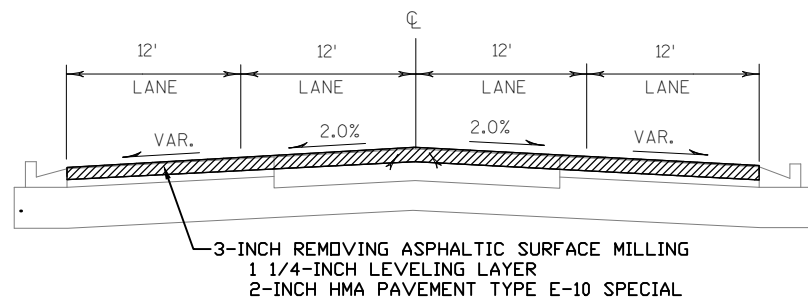
COUNTY: RUSK

PLAN: TYPICAL SECTIONS

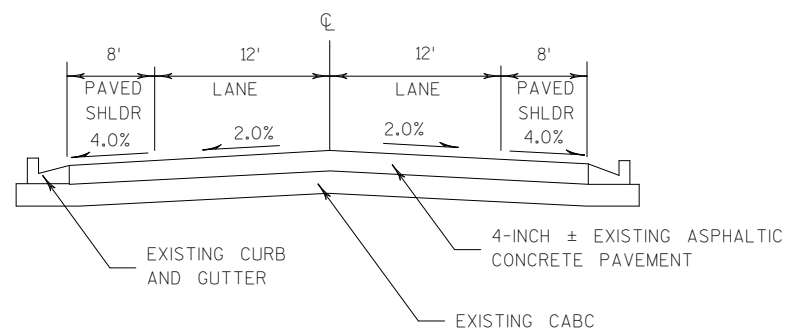
SHEET	-----	E
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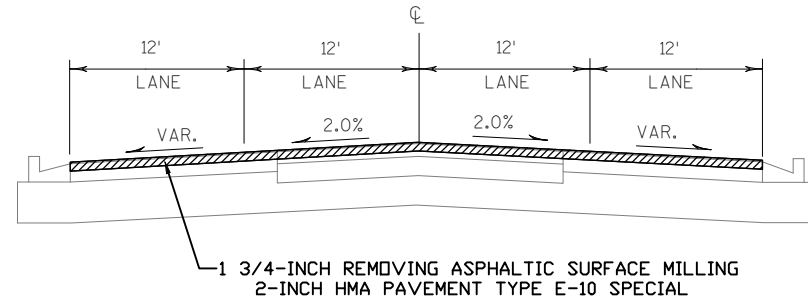
STH 27 EXISTING TYPICAL SECTION
STA 630+85 TO STA 632+20



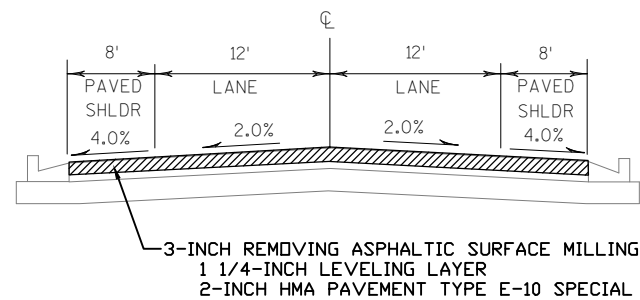
STH 27 FINISHED TYPICAL SECTION
STA 631+10 TO STA 632+20



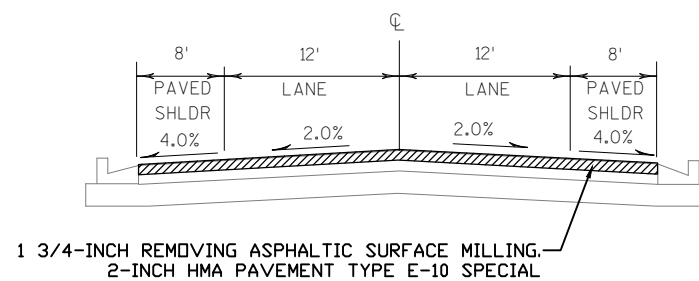
STH 27 EXISTING TYPICAL SECTION
STA 635+00 TO STA 647+19



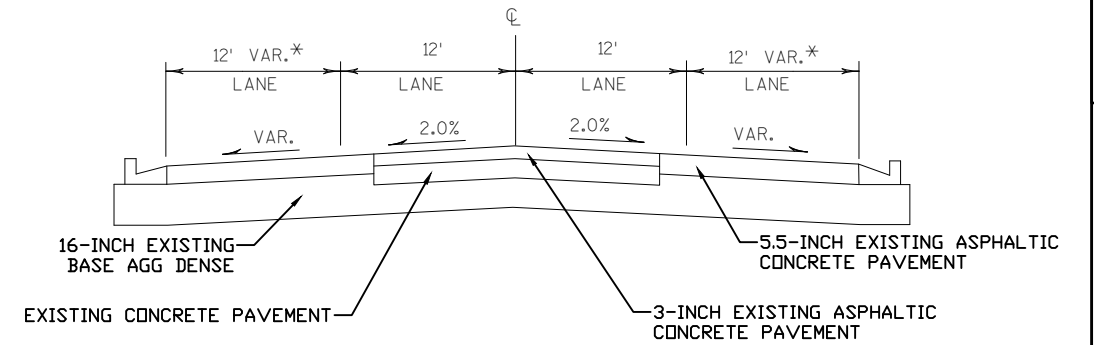
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STA 630+85 TO STA 631+10



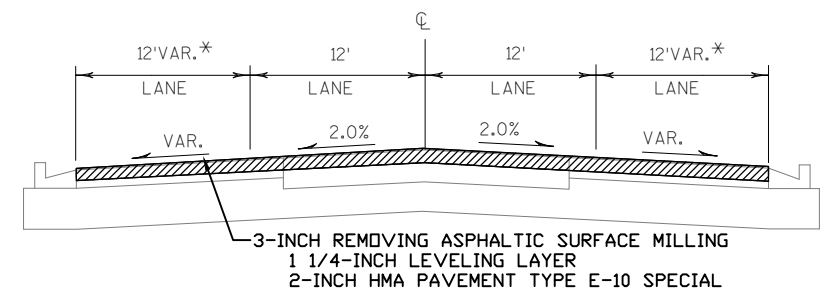
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STA 635+00 TO STA 636+95



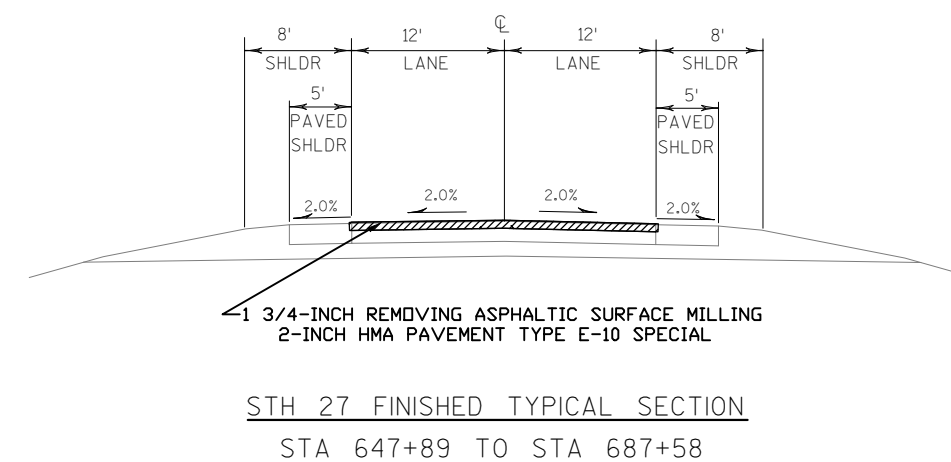
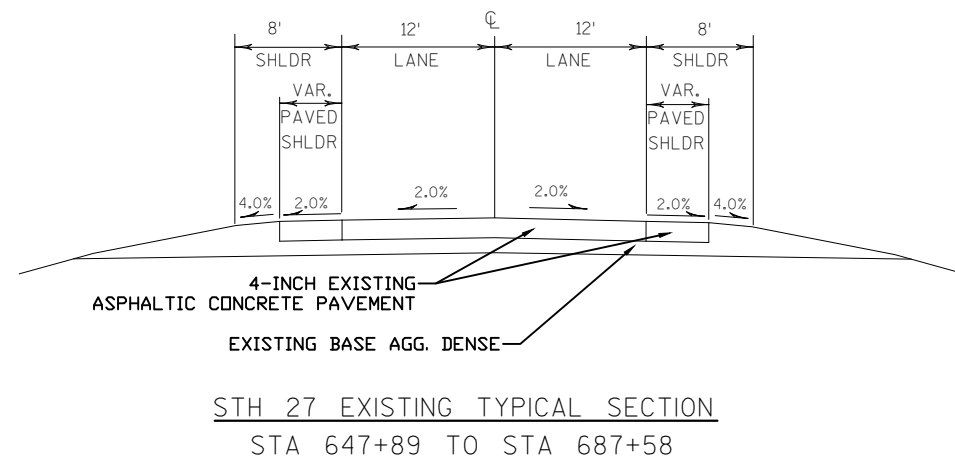
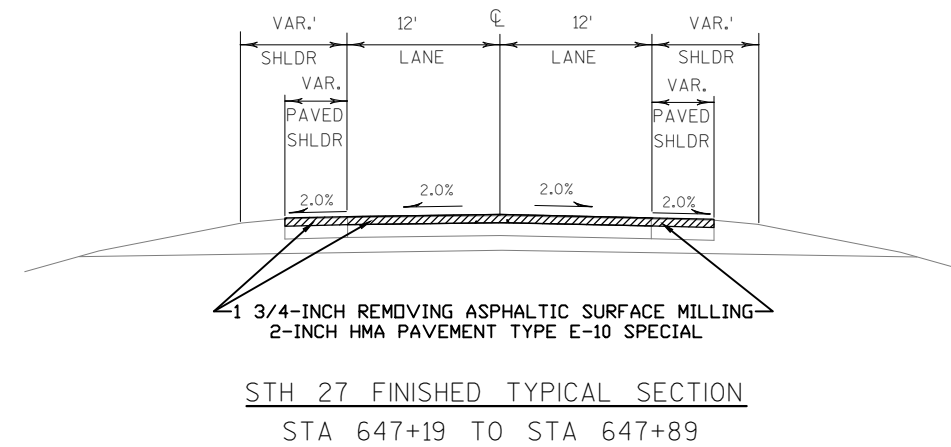
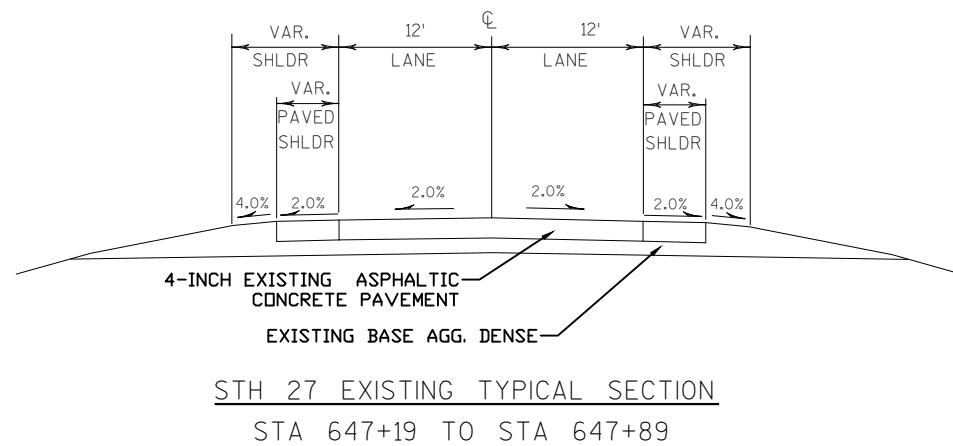
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STA 636+95 TO STA 647+19

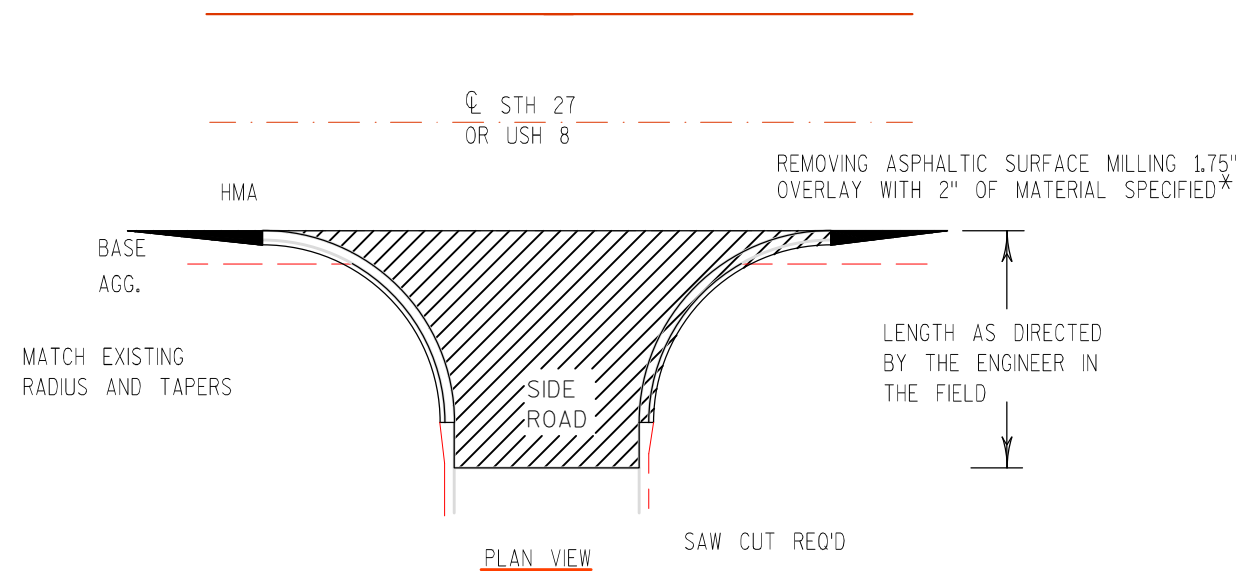


STH 27 EXISTING TYPICAL SECTION
STA 633+75 TO STA 634+30
*STA 634+30 TO STA 635+00



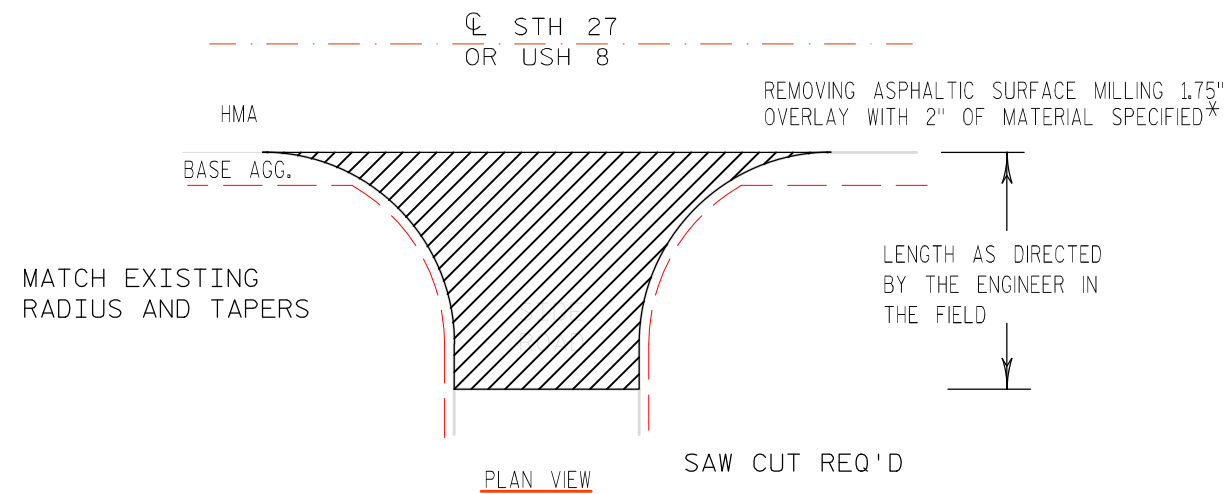
STH 27 FINISHED TYPICAL SECTION
STA 633+75 TO STA 634+30
*STA 634+30 TO STA 635+00





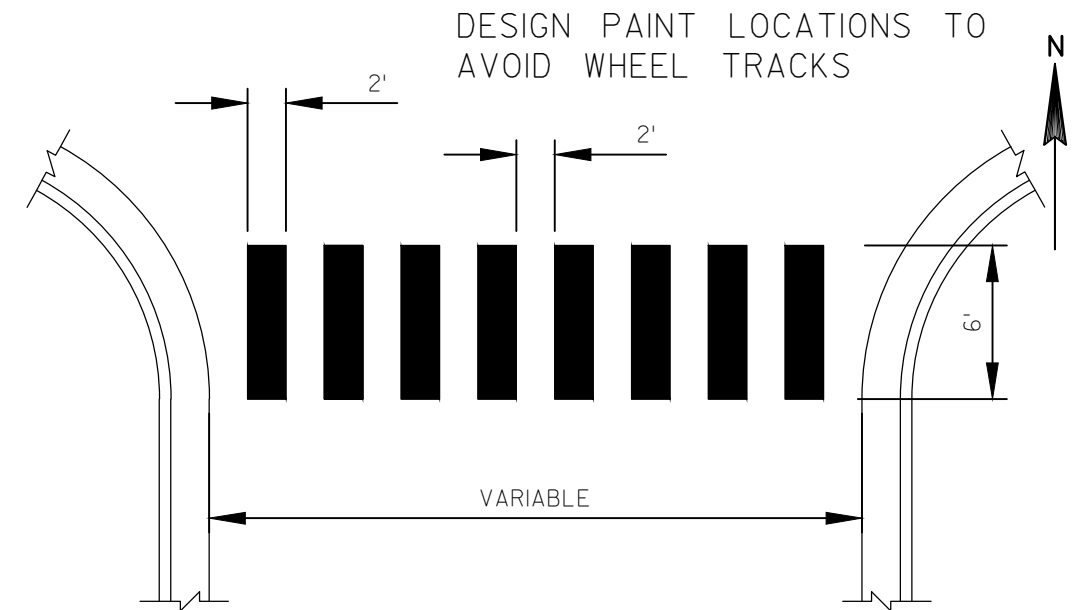
SIDEROAD AND DRIVEWAY PAVING* DETAIL WITH CURB & GUTTER

* USE "ASPHALTIC SURFACE SPEICIAL" FOR SIDEROAD PAVING,
USE "ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES"
FOR PRIVATE ENTRANCES.

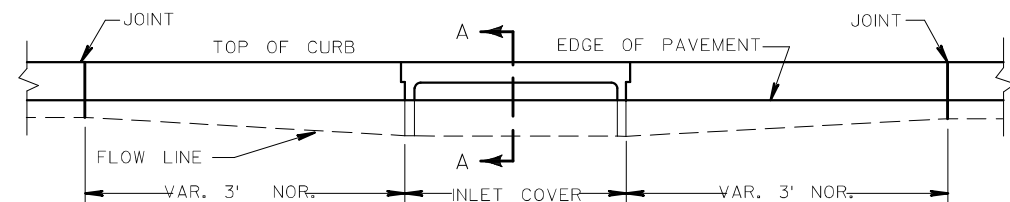
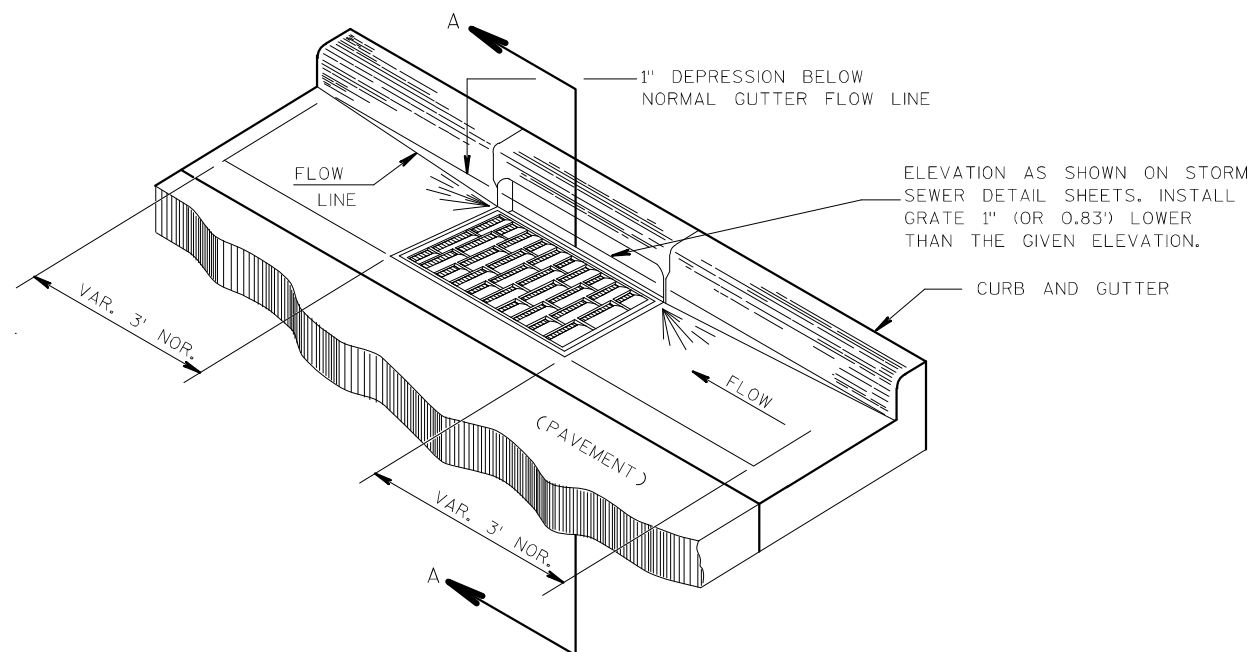
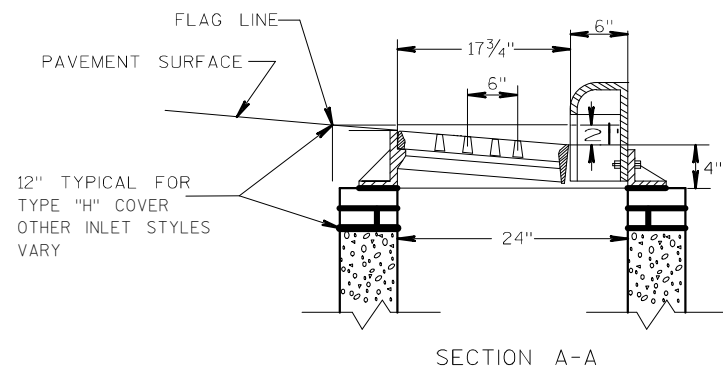


SIDEROAD AND DRIVEWAY PAVING* DETAIL W/O CURB & GUTTER

* USE "ASPHALTIC SURFACE SPEICIAL" FOR SIDE ROAD PAVING,
USE "ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES"
FOR PRIVATE ENTRANCES.



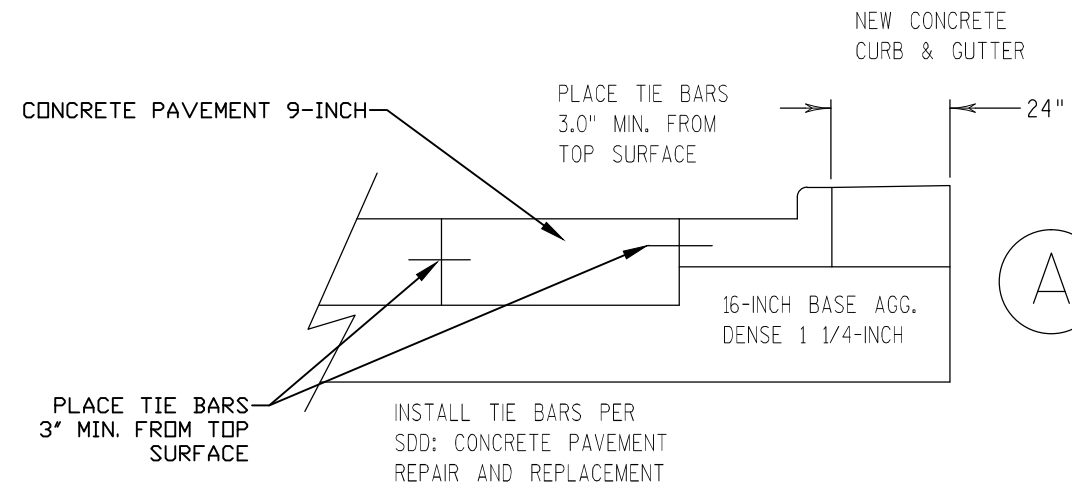
CROSSWALK DESIGN DETAIL STA. 630+00



ELEVATION VIEW

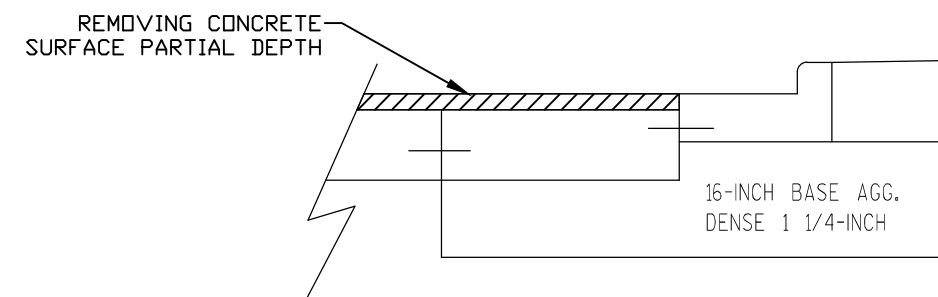
DETAIL OF CURB AND GUTTER AT INLETS

(INTENDED TO SHOW ELEVATION DROP AT INLET.
3-H INLET SHOWN FOR DEMONSTRATION ONLY)



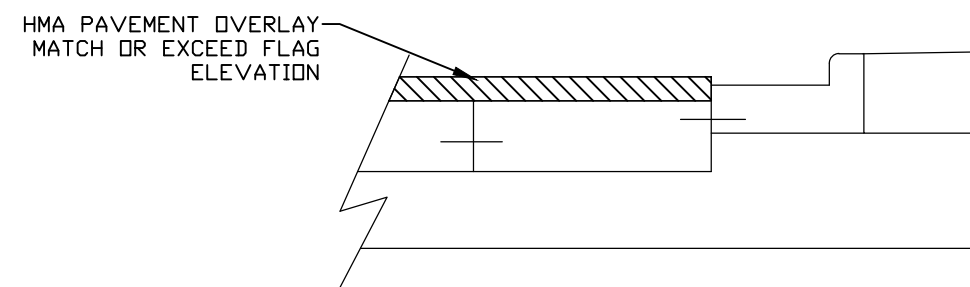
A

NEW CONCRETE PAVEMENT
9-INCH TO MATCH EXISTING
CONCRETE PAVEMENT AND
FLAG ELEVATIONS



B

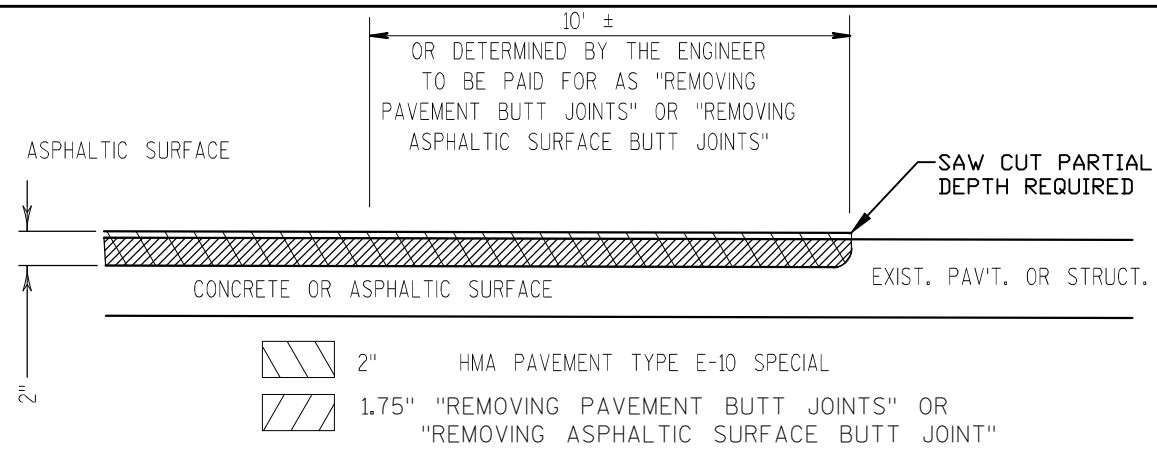
REMOVING CONCRETE SURFACE
PARTIAL DEPTH 1.75"



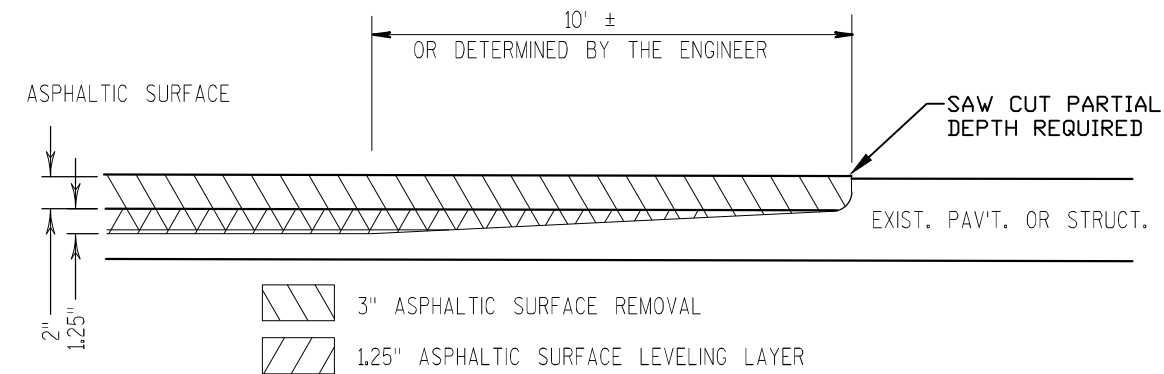
C

OVERLAY WITH 2" HMA
TYPE E-10 SPECIAL

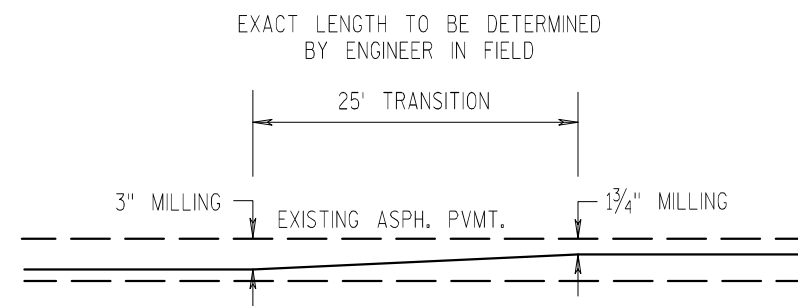
STH 27/USH 8 RADIUS WIDENING TYPICAL
AND CONSTRUCTION SEQUENCE



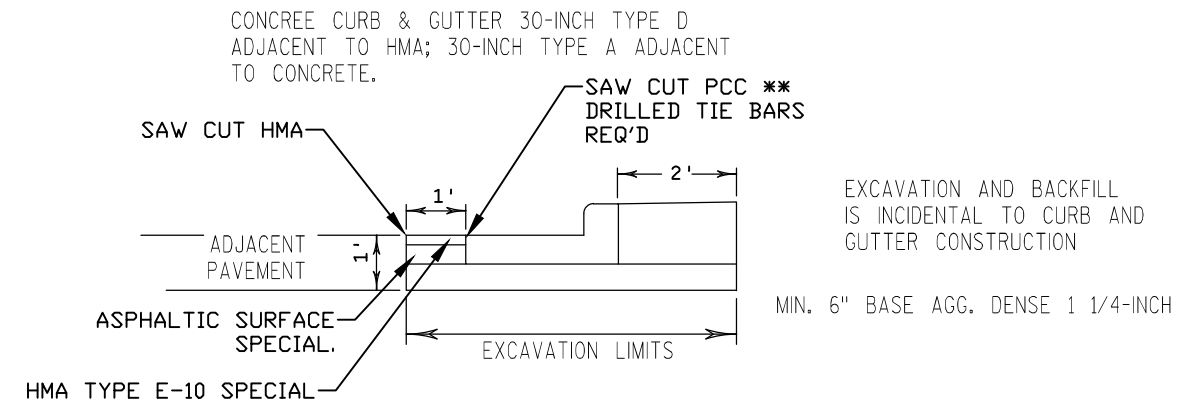
BUTTED JOINT DETAIL PROJECT TERMINALS NORTH, SOUTH AND EAST



BUTTED JOINT DETAIL USH 8 TERMINUS WEST



REMOVING ASPHALTIC SURFACE MILLING DEPTH TRANSITION



CONCRETE CURB & GUTTER REPLACEMENTS

STA. 321+30 - 322+75 RT USH 8
 STA. 321+75 - 323+22 LT USH 8
 STA. 324+75 - 325+25 LT USH 8 (DRIVEWAY)
 STA. 633+71 - 634+15 LT
 STA. 633+71 - 636+30 RT STH 27 (DRIVEWAY)
 STA. 643+45 - 643+75 LT "
 STA. 645+00 - 647+00 LT "
 STA. 646+80 - 647+20 RT "

** SEE "RADIUS WIDENING TYPICAL AND
 CONSTRUCTION SEQUENCE DETAIL"



2

CROSS SECTIONS ARE CUT
ALONG THE EXISTING CURB
ALIGNMENT. ELEVATIONS ARE
SHOWN ON THE NEW CURB
ALIGNMENT.

MATCH EXISTING
GRADES

PT: 202+21.84

MATCH EXISTING

MATCH EXISTING

PT: 102+21.77

MATCH EXISTING
GRADES

N

2

NEW R/W
SEE PLAT
SECTION LOCATIONS

NEW CURB LINE

MATCH EXISTING GRADE
FOR CURB REPLACEMENT

PC: 201+40.17

201+00

MATCH EXISTING

STA 201+62.9
= STA 10+20NW
EL. 1140.26MATCH EXISTING
PAVEMENTSTA 201+78.55
= STA 10+40NW
EL. 1140.80STA 201+93.02
= STA 10+60NW
EL. 1141.40STA 202+12.67
= STA 10+80NW
EL. 1142.20STA 102+10.2
= STA 12+20NE
EL. 1141.8

MATCH EXISTING

PT: 102+21.77

STA 102+10.2
= STA 12+20NE
EL. 1141.8STA 102+07
= STA 12+07NE
EL. 1141.7STA 101+91.22
= STA 12+00NE
EL. 1142.1MATCH EXISTING
PAVEMENTSTA 101+77.05
= STA 11+80NE
EL. 1142.5STA 101+60.5
= STA 11+60NE
EL. 1143.1

MATCH EXISTING

MATCH EXISTING GRADES
FOR CURB REPLACEMENT

325+00

325+00

USH 8

MATCH EXISTING GRADES
FOR CURB REPLACEMENT

00+662

MATCH EXISTING

STA 298+28.601
= STA 32+00SW
EL. 1139.72STA 298+09.59
= STA 31+80SW
EL. 1139.99MATCH EXISTING
PAVEMENTSTA 297+94.75
= STA 31+60SW
EL. 1140.19 PKSTA 297+78.11
= STA 31+40SW
EL. 1140.0

MATCH EXISTING

NEW CURB LINE

NEW R/W
SEE PLAT

EXISTING R/W

PC: 297+57.78

STH 27

PROJECT NO: 1580-12-72

HWY: STH 27

COUNTY: RUSK

PLAN: INTERSECTION DETAILS

SHEET

E

FILE NAME : N:\PDS\C3D\15801201\SHETS0THER\ID\021101.ID.DWG

PLOT DATE : 1/29/2014 1:43 PM

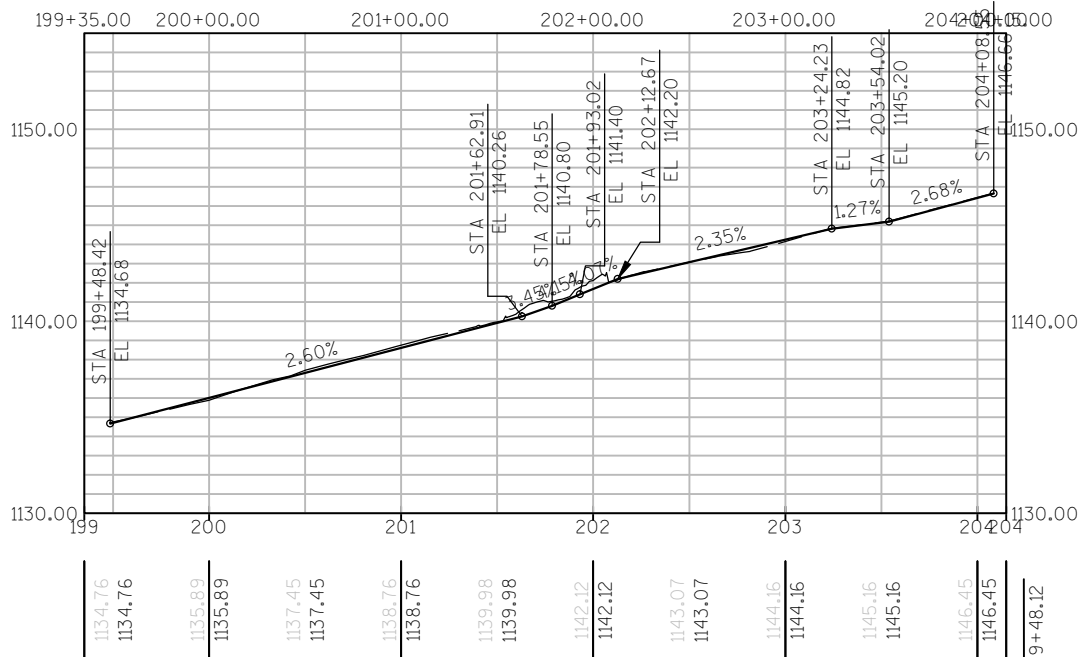
PLOT BY : PEARSON, MICHAEL R

PLOT NAME :

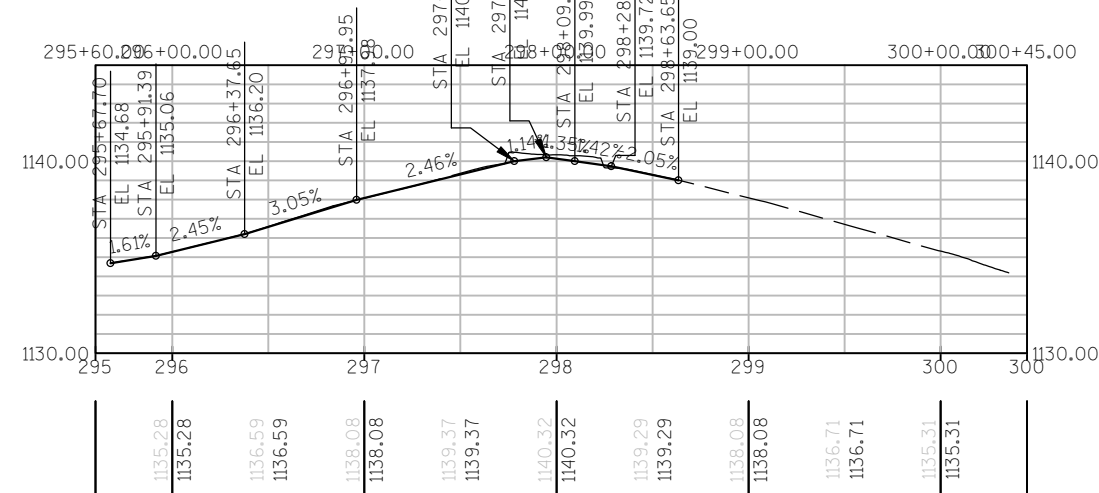
PLOT SCALE : 0.054325

WISDOT/CADDs SHEET 42

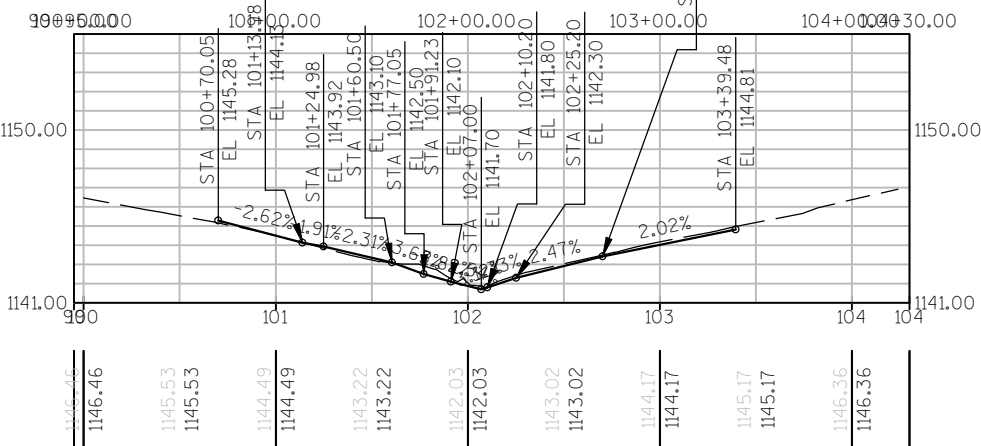
Profile View of Alignment - C3D-AC-NW



Profile View of Alignment - (C3D-SW-CURB)



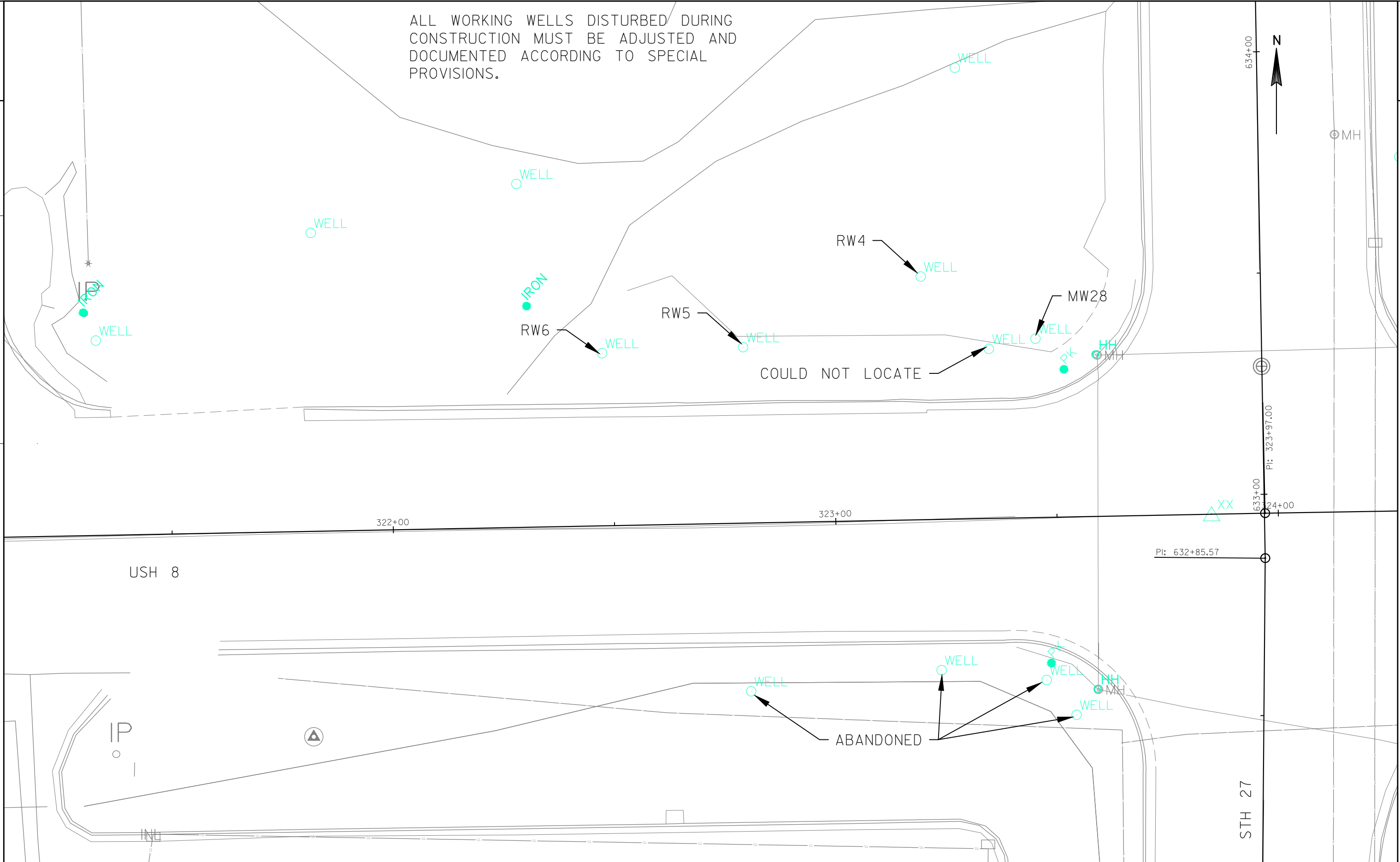
Profile View of Alignment (NE Corner)



2

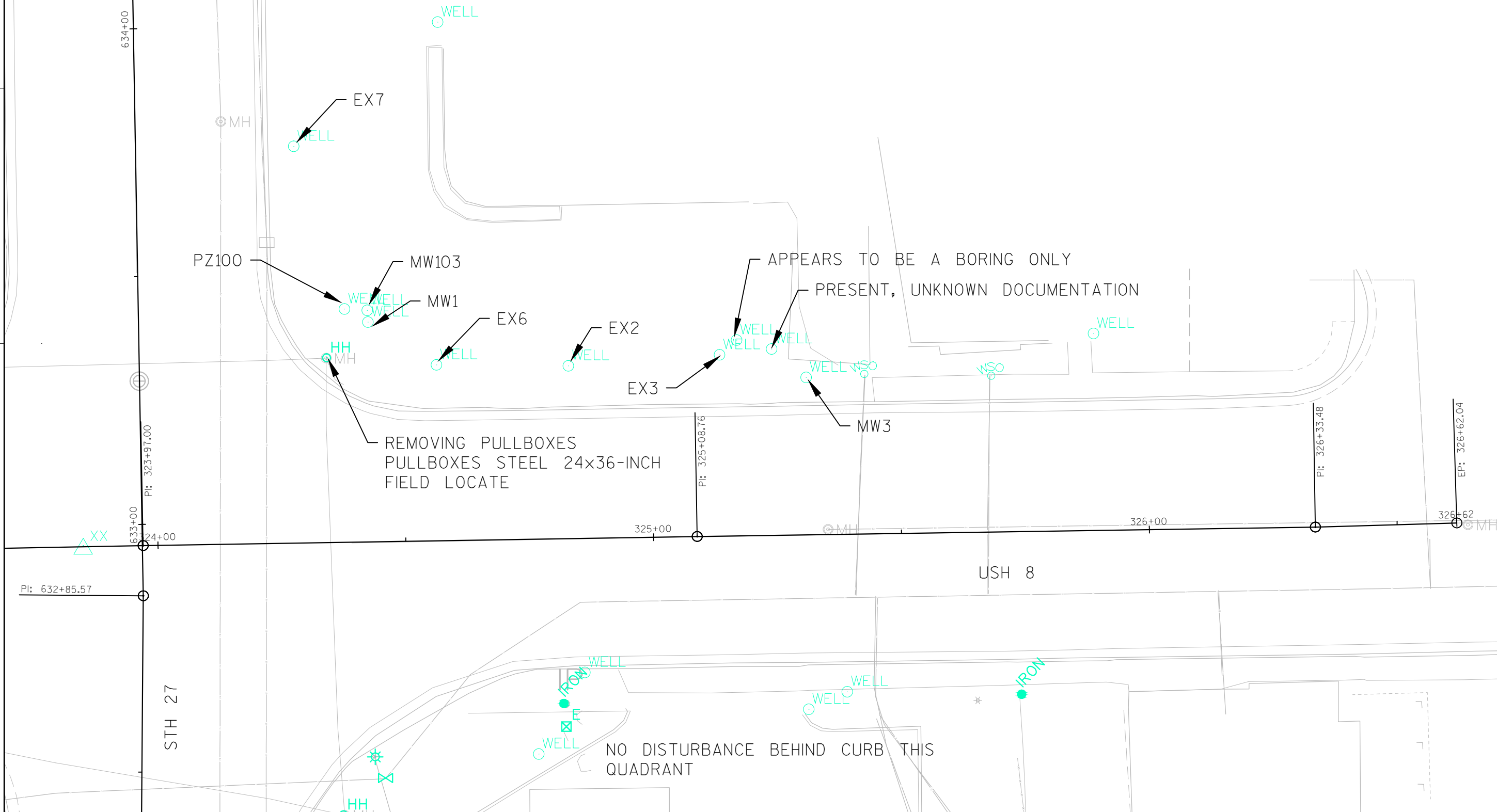
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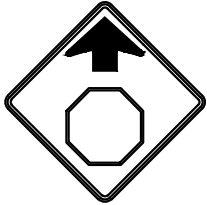

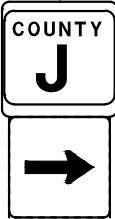



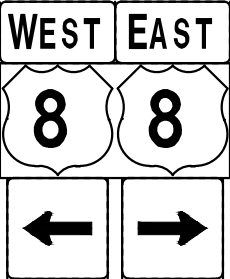






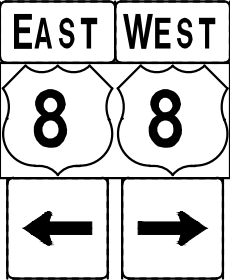






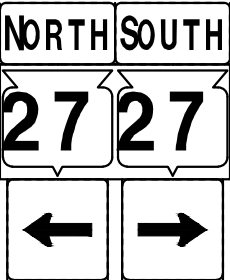

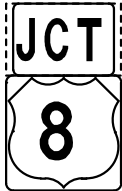
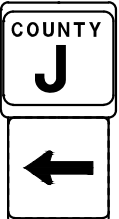
ALL WORKING WELLS DISTURBED/ DURING
CONSTRUCTION MUST BE ADJUSTED AND
DOCUMENTED ACCORDING TO SPECIAL
PROVISIONS.

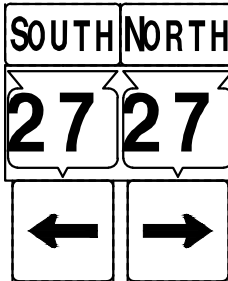





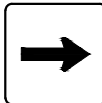

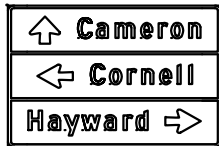


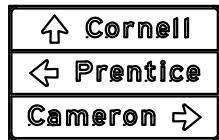


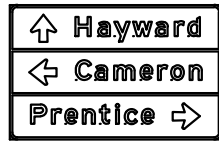

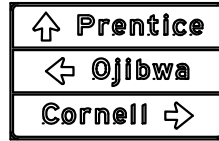



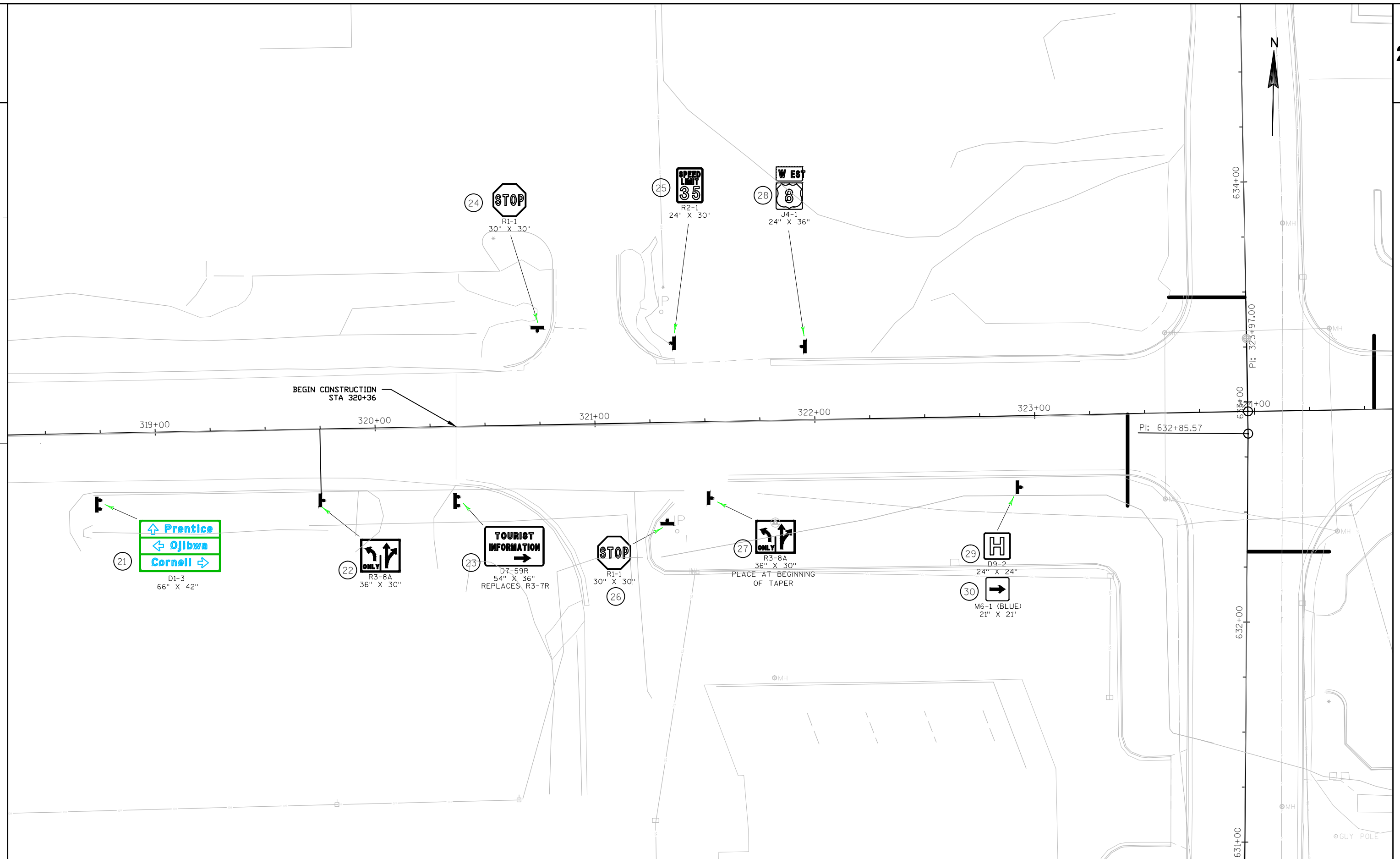
PROJECT NO: 1580-12-72	HWY: STH 27	COUNTY: RUSK	PLAN: INTERSECTION DESIGN - WELLS	SHEET	-----	E
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ALL WORKING WELLS DISTURBED
DURING CONSTRUCTION MUST BE
ADJUSTED AND DOCUMENTED
ACCORDING TO SPECIAL PROVISIONS.



BILL OF MATERIALS															
ESTIMATE OF QUANTITIES FOR PERMANENT SIGNING															
SIGN	SIGN CODE	SIGN SIZE	NO REQ'D.	SIGN	SIGN CODE	SIGN SIZE	NO REQ'D.	SIGN	SIGN CODE	SIGN SIZE	NO REQ'D.	SIGN	SIGN CODE	SIGN SIZE	NO REQ'D.
	R2-1	24"X30"	3		W3-1	36"X36"	4		J1-1	24"X39"	1		J13-1	24"X45"	1
	R2-1	24"X30"	6		W10-1	36"X36"	3		J4-1	24"X36"	1		J2-2	48"X57"	1
	R2-1	24"X30"	1		W10-1A	24"X12"	1		J4-1	24"X36"	1				
	R1-1	30"X30"	17		W11-2	30"X30"	4		J1-1	24"X39"	2		J2-2	48"X57"	1
	R1-1	36"X36"	8		W14-3	48"X36"	1		J4-1	24"X36"	1				
	R3-8A	36"X30"	7		R1-3P	24"X9"	4		J4-1	24"X36"	1		J2-2	48"X57"	1
	R3-50L	30"X36"	4		J1-1	24"X39"	1		J13-1	24"X45"	1				

BILL OF MATERIALS																
ESTIMATE OF QUANTITIES FOR PERMANENT SIGNING																
SIGN	SIGN CODE	SIGN SIZE	NO REQ'D.	SIGN	SIGN CODE	SIGN SIZE	NO REQ'D.	SIGN	SIGN CODE	SIGN SIZE	NO REQ'D.		SIGN	SIGN CODE	SIGN SIZE	NO REQ'D.
	J2-2	48"X57"	1		I2-3	66" X 24"	1		D1-1	90" X 24"	1					
					D9-2	24"X24"	3		D1-1	90" X 24"	1					
	I55-56	30"X36"	1	 BLUE BACKGROUND	M6-1	21"X21"	1		D1-56SL	66"X24"	1					
	D1-3	66" X 42"	1	 BLUE BACKGROUND	M6-1	21"X21"	1		D7-59R	54"X36"	1					
	D1-3	66" X 42"	1	 BLUE BACKGROUND	M6-1	21"X21"	1		D7-59L	54"X36"	1	GENERAL NOTES FOR SIGNING: * SEE SIGN DETAIL SHEETS FOR SPECIFIC SIGN FABRICATION DETAILS. ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. (WMUTCD) BEFORE INSTALLING ANY SIGN POSTS, LOCATIONS ARE TO BE CLEARED WITH UTILITIES THROUGH DIGGER'S HOTLINE 1-800-242-8511 WHEN INSTALLING ANY SIGN POSTS, A TRIBAL MONITOR SHALL BE PRESENT. ALL SIGNS, OTHER THAN NO PASSING ZONE SIGNS, ARE TO BE PLACED AT EXISTING SIGN LONGITUDINAL LOCATIONS UNLESS OTHERWISE DIRECTED BY ENGINEER. LATERALLY POSITION THE SIGNS AS THE PLAN SHOWS OR AS DIRECTED BY THE ENGINEER. NO PERMANENT SIGNING WORK SHALL BE PERFORMED WITHOUT THE APPROVAL OF THE ENGINEER.				
	D1-3	66" X 42"	1		W3-5	36"X36"	1									
	D1-3	66" X 42"	1		W16-7L	24"X12"	2									



PROJECT NO: 1580-12-72

HWY: STH 27

COUNTY: RUSK

PLAN: PERMANENT SIGNS

SHEET

E

FILE NAME : N:\PDS\C3D\15801201\SHEET\OTHER\PS\PERMANENT SIGNS.DWG

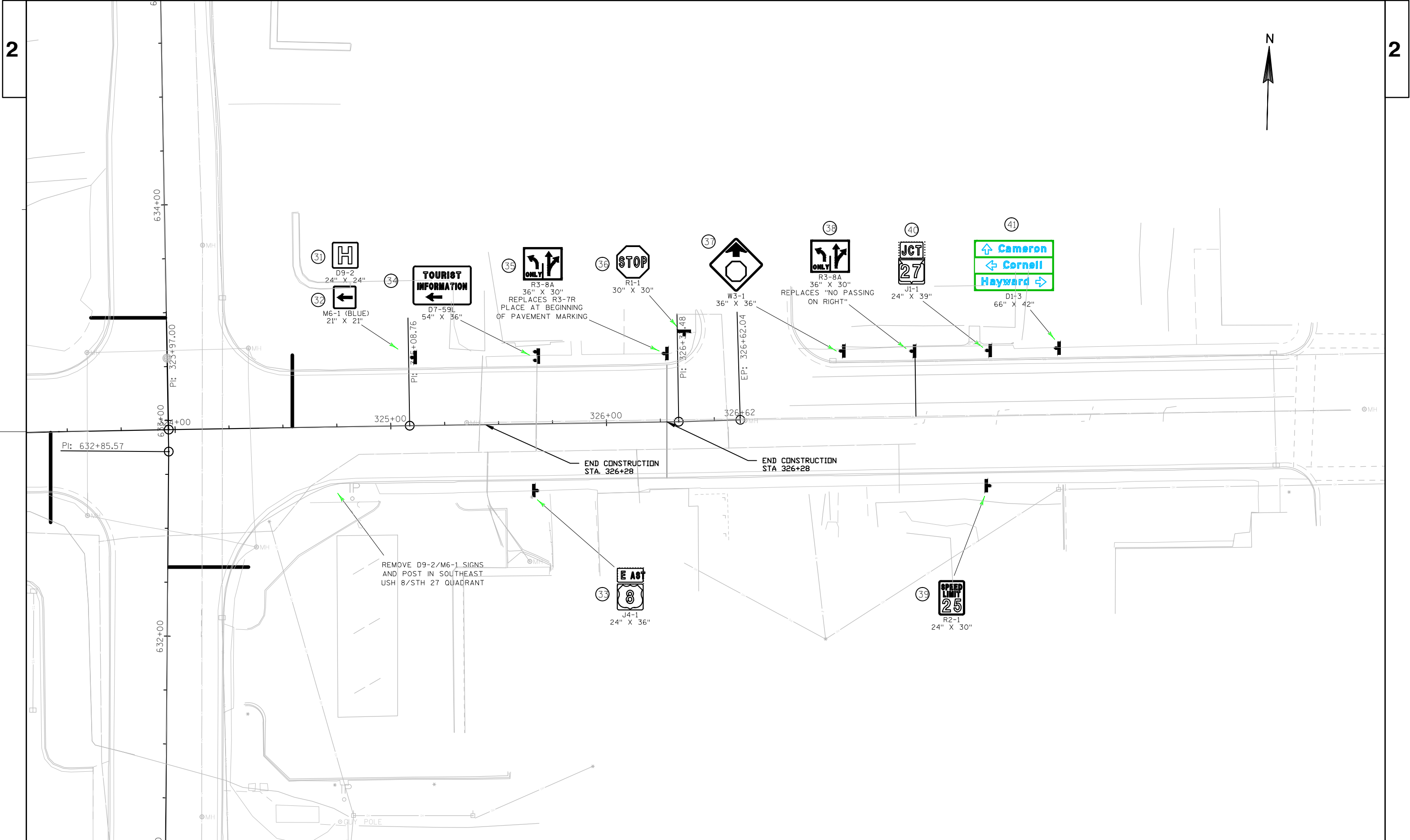
PLOT DATE : 4/8/2013 3:28 PM

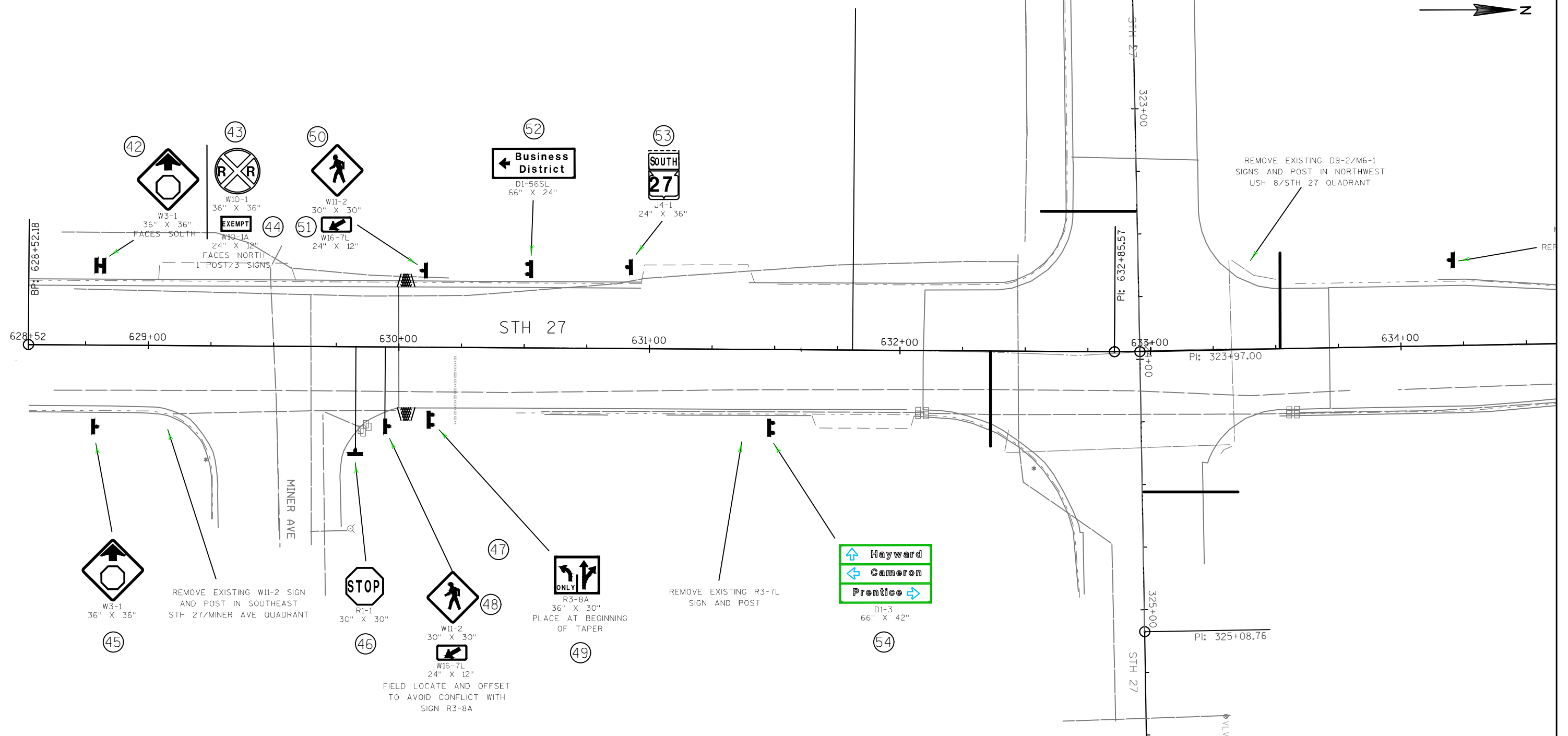
PLOT BY : PEARSON, MICHAEL R

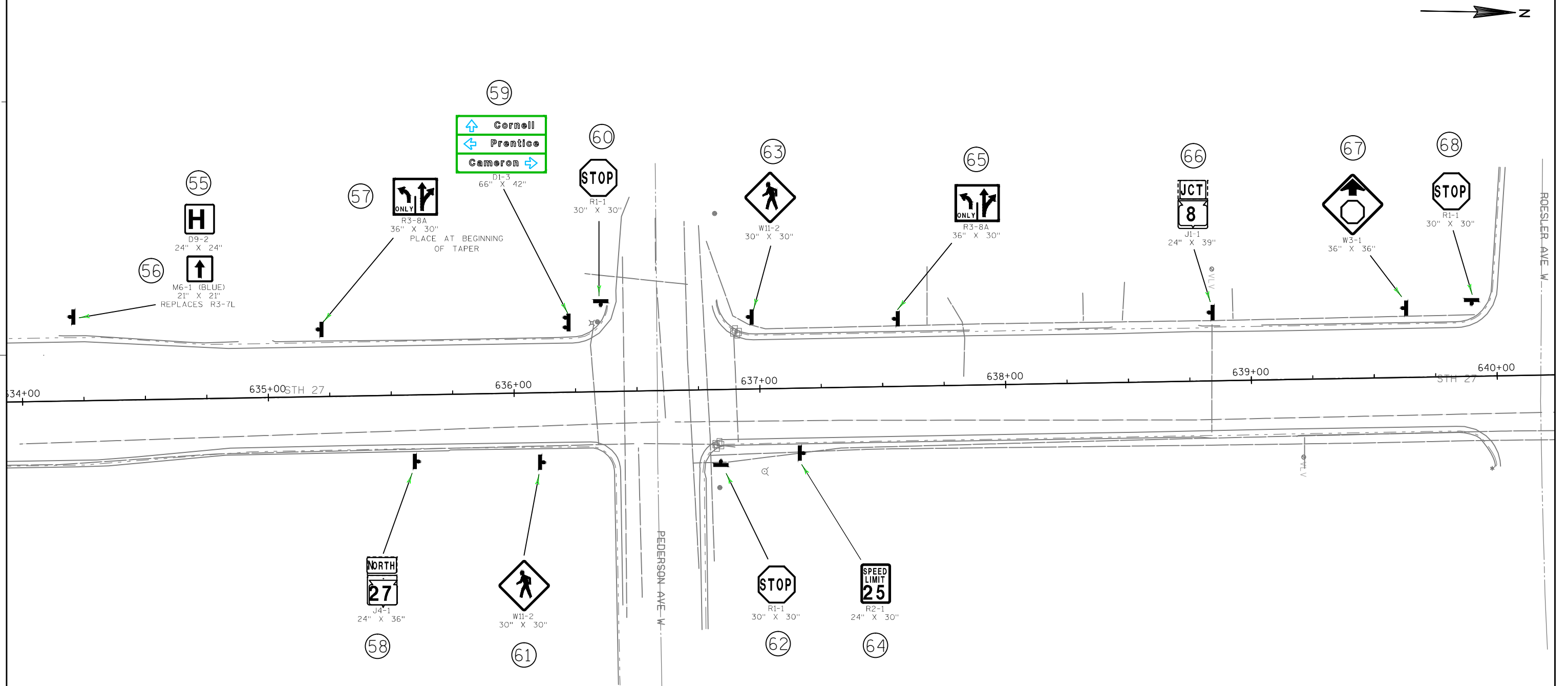
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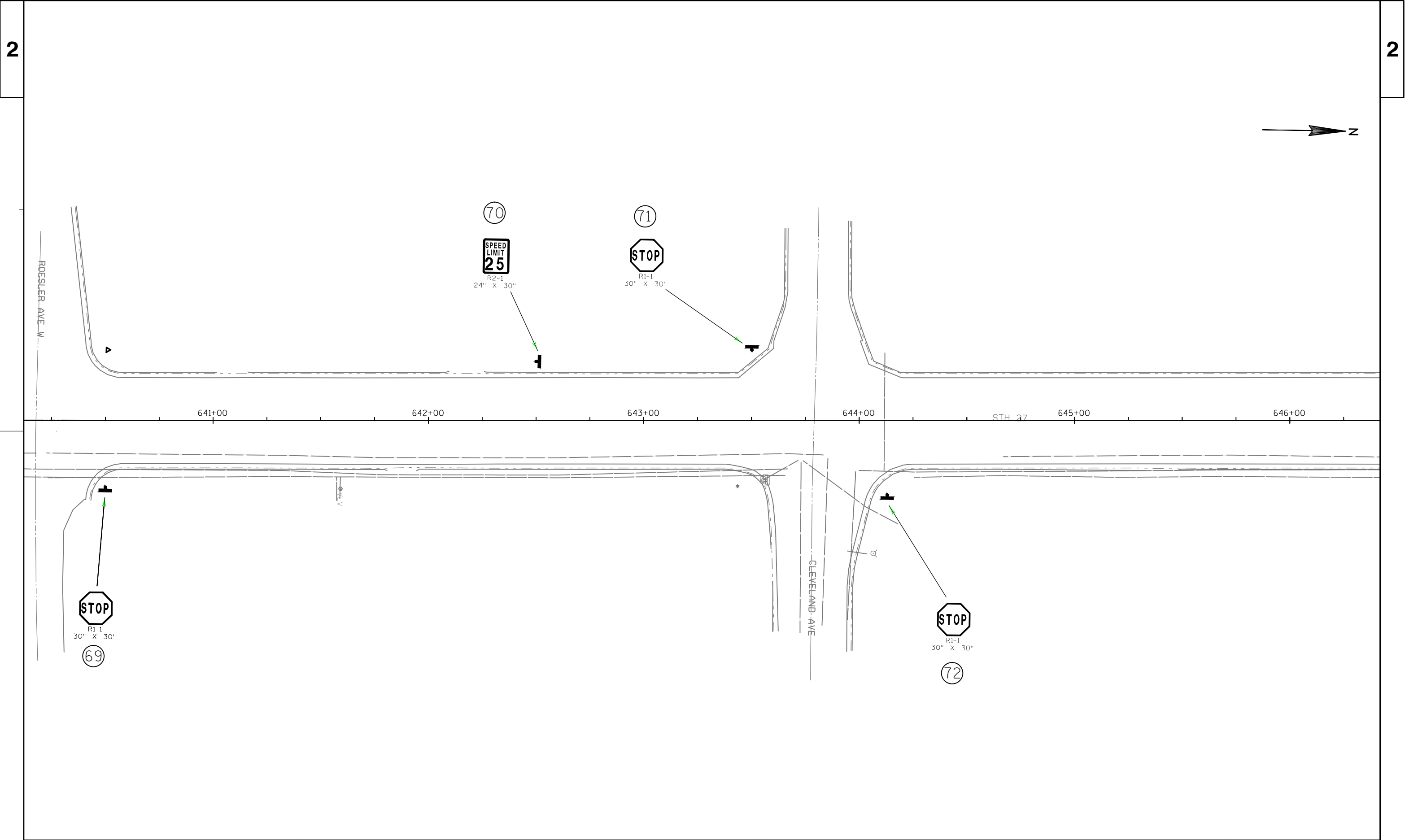
PLOT SCALE : 1 IN:40 FT

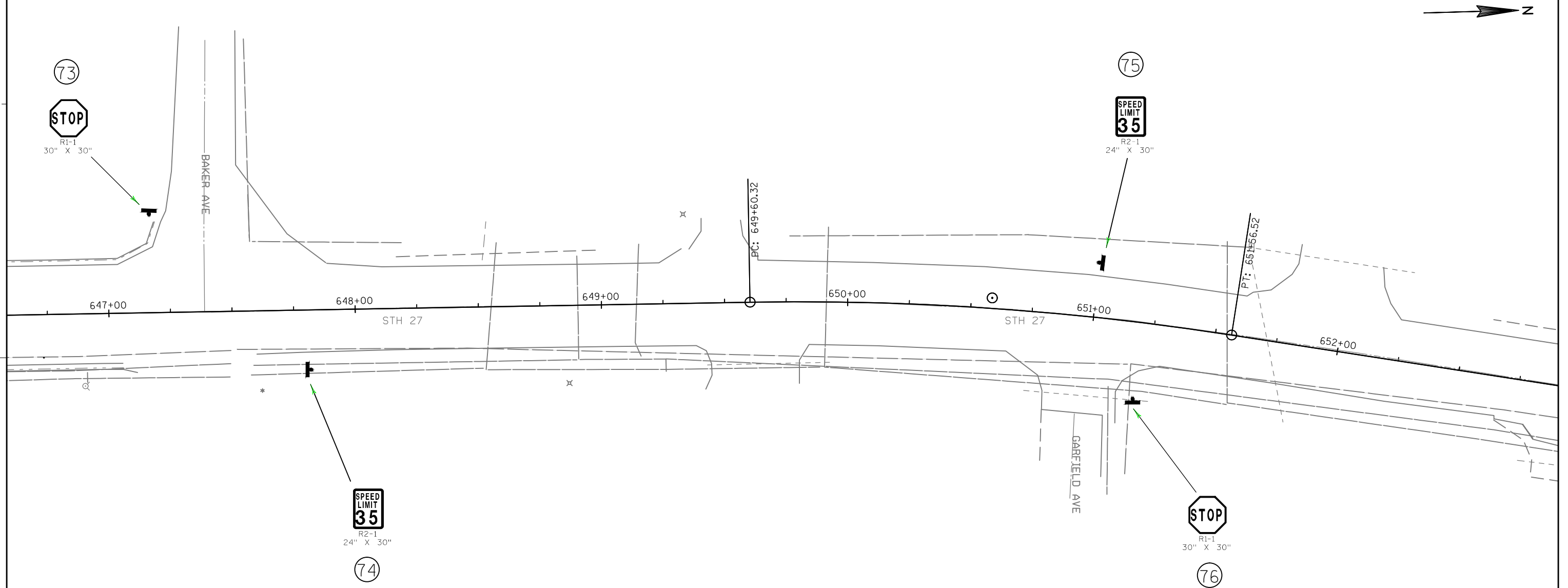
WISDOT/CADDs SHEET 42





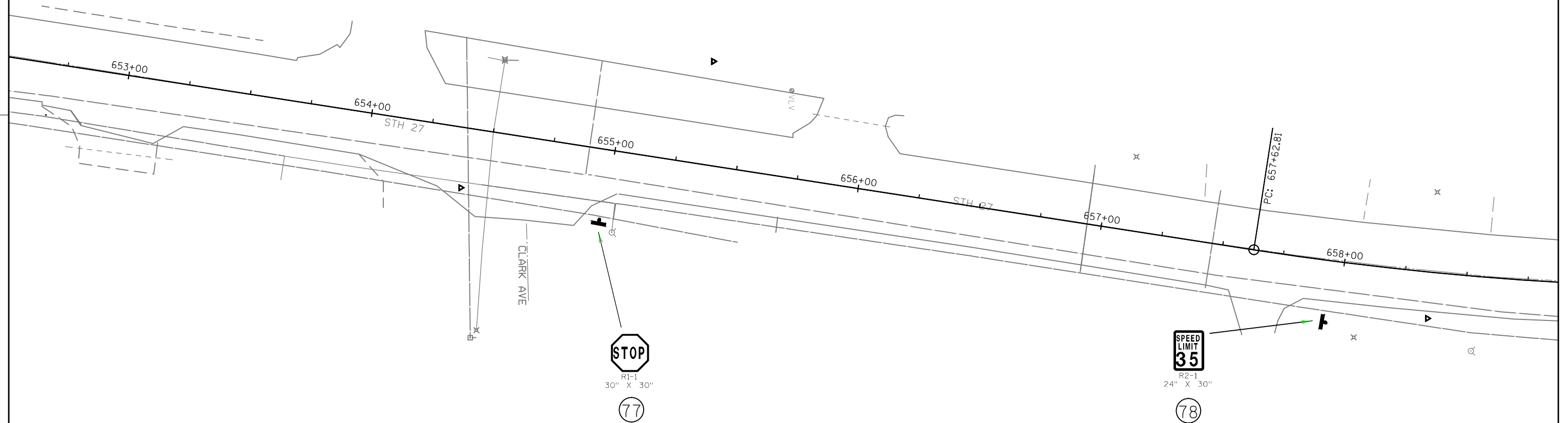






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2 |



PROJECT NO: 1580-12-72

HWY: STH 27

COUNTY: RUSK

PLAN: PERMANENT SIGNS

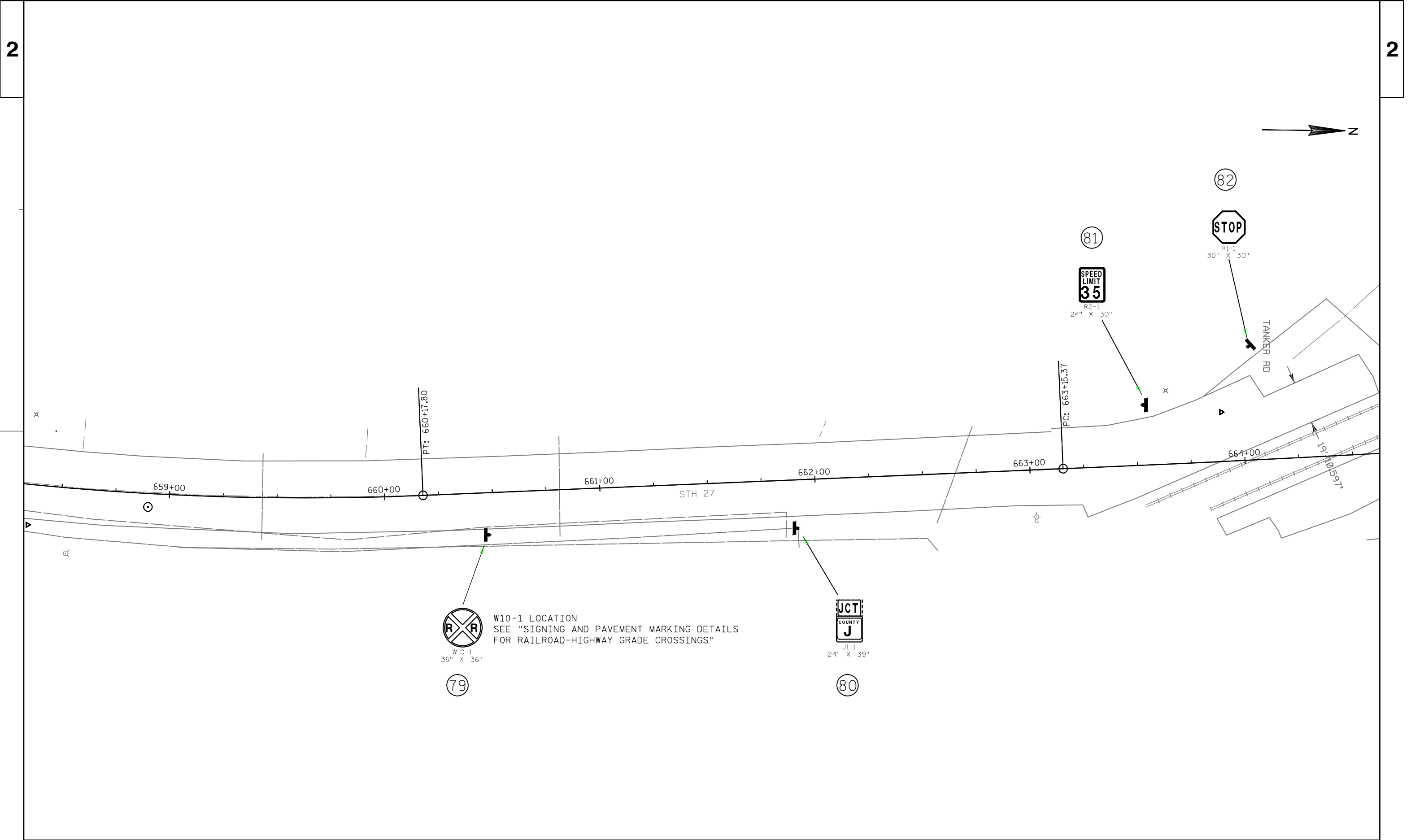
SHEET	-----	E
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FILE NAME = N:\DRS\ORD\15_01201\SUETSOTUE\B\B\EDMANENT STONE27.DWG

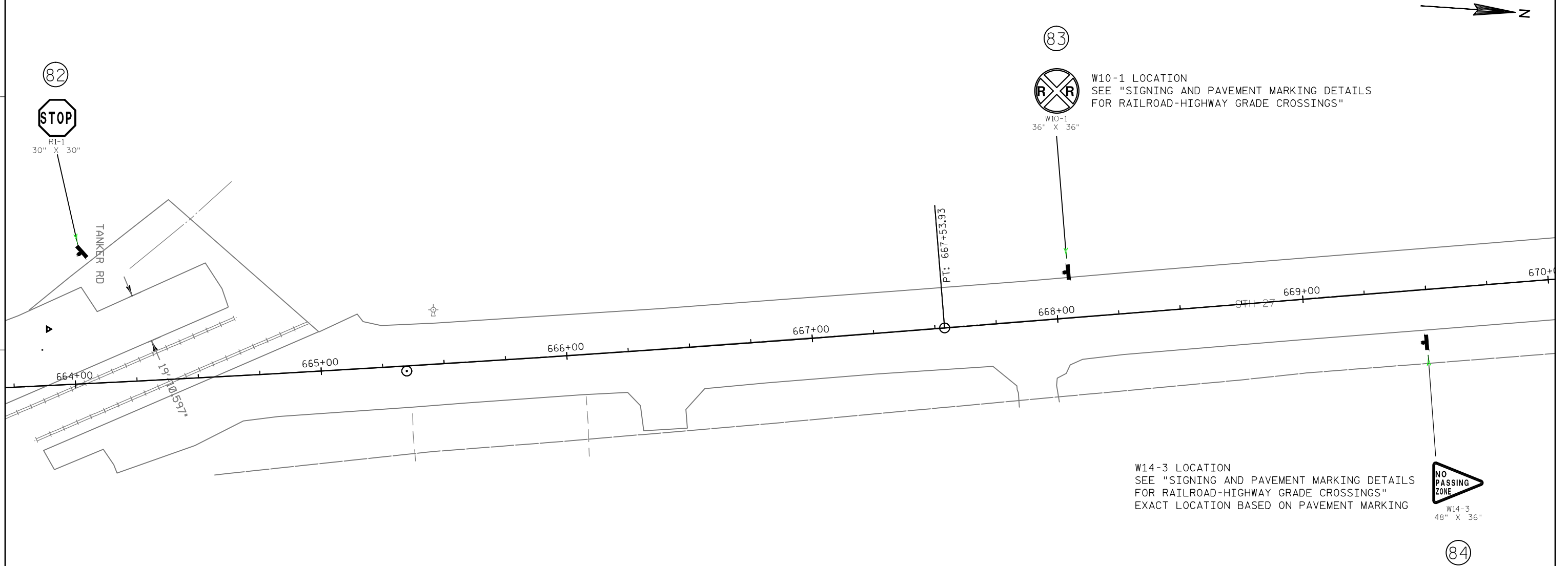
DIOT DATE - 0,2,200 0.35 A

PILOT RV * DEADSONI MTCUACI D PILOT NAME *

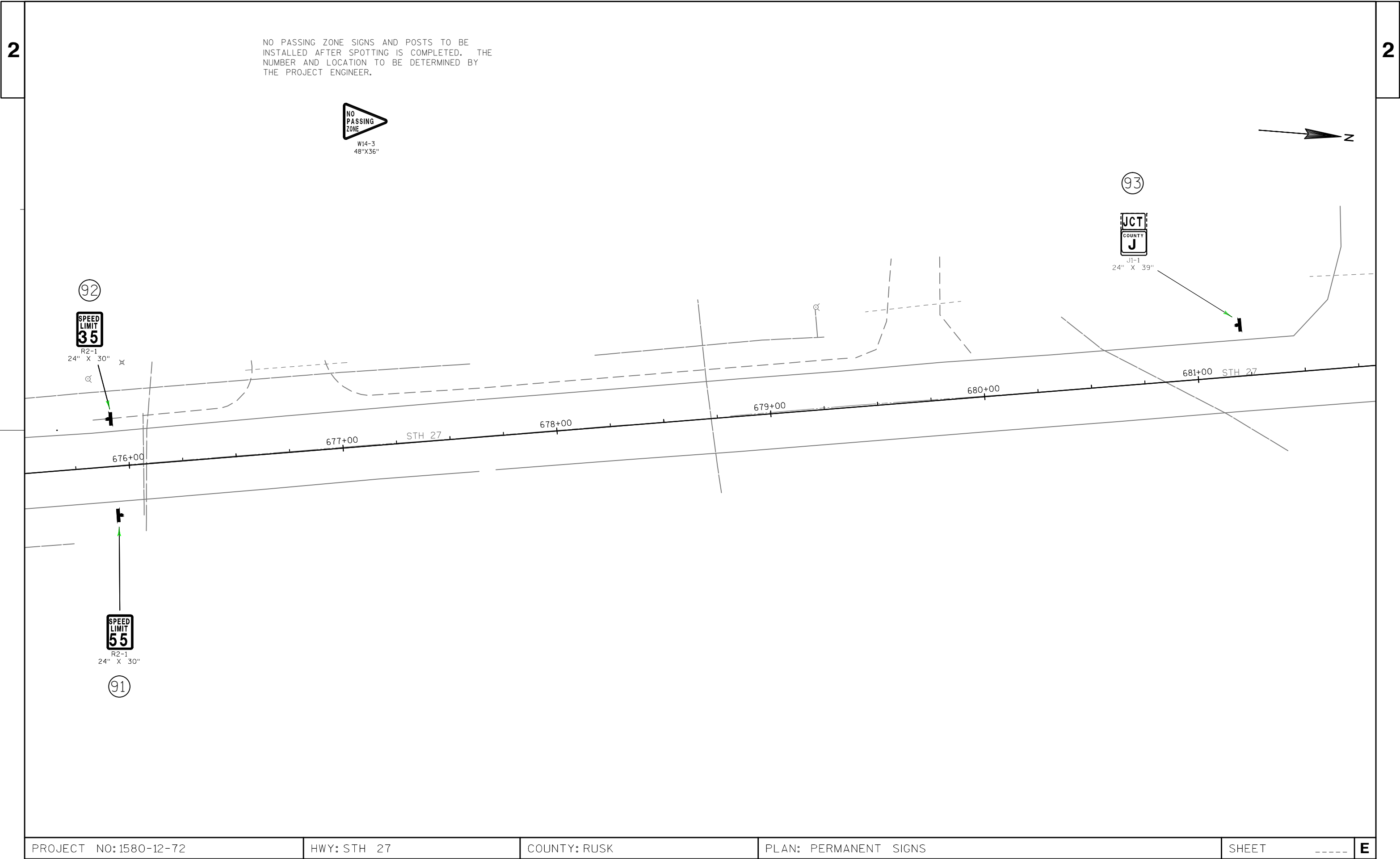
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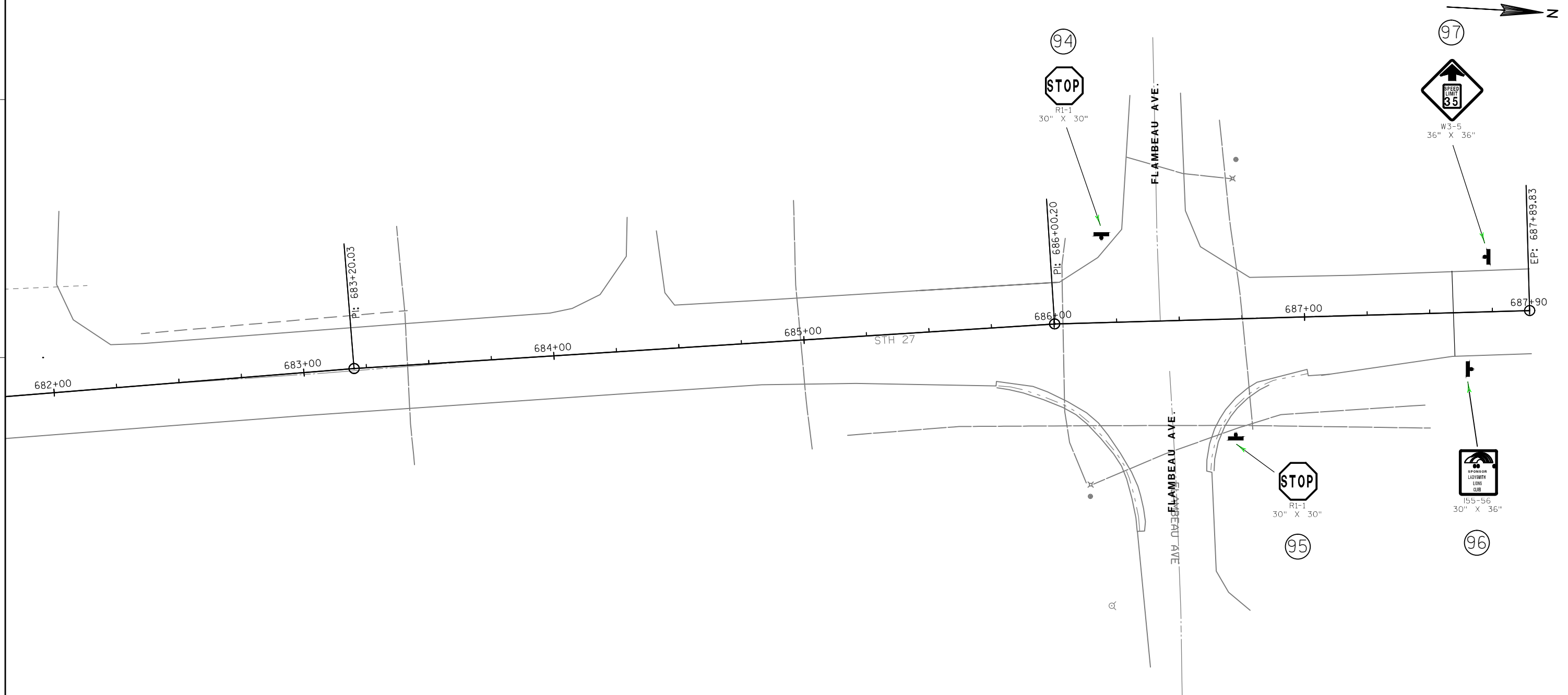


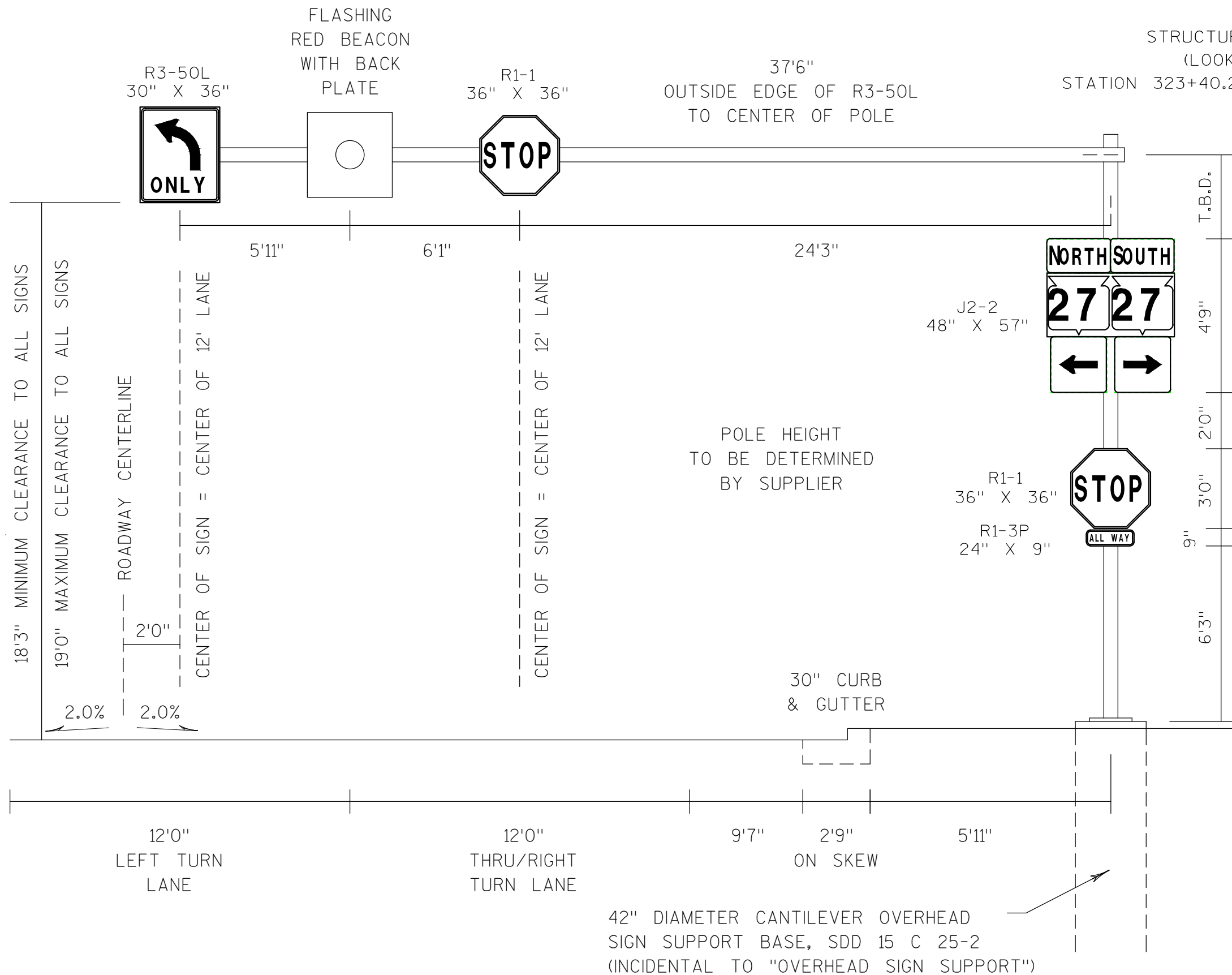
PROJECT NO: 1580-12-72	HWY: STH 27	COUNTY: RUSK	PLAN: PERMANENT SIGNS	SHEET -----	E
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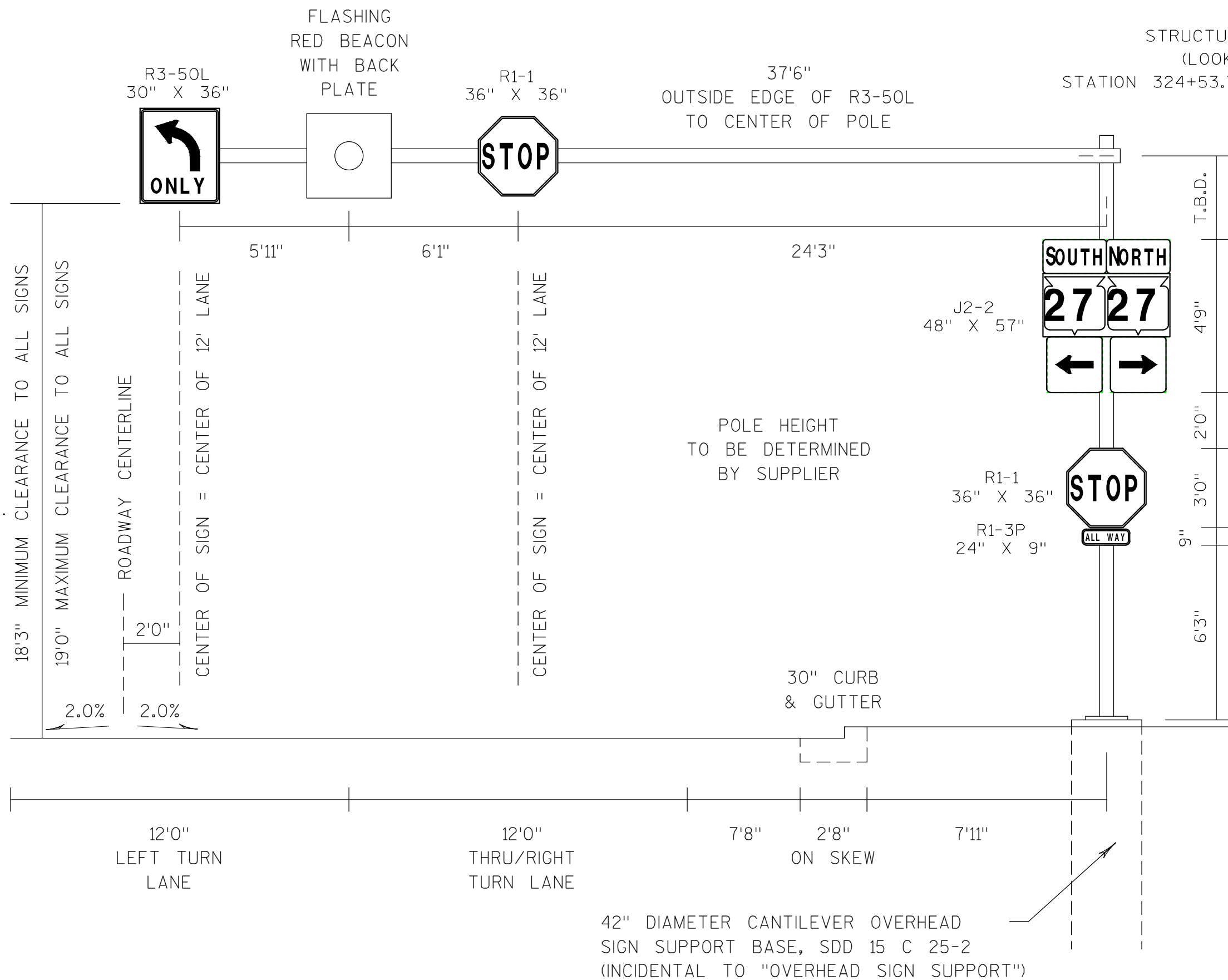










GENERAL NOTES:

DRAWINGS SHALL NOT BE SCALED.

SIGN STRUCTURES SHALL BE DESIGNED TO SUPPORT SIGNS, BEACONS, & BACK PLATES SHOWN.

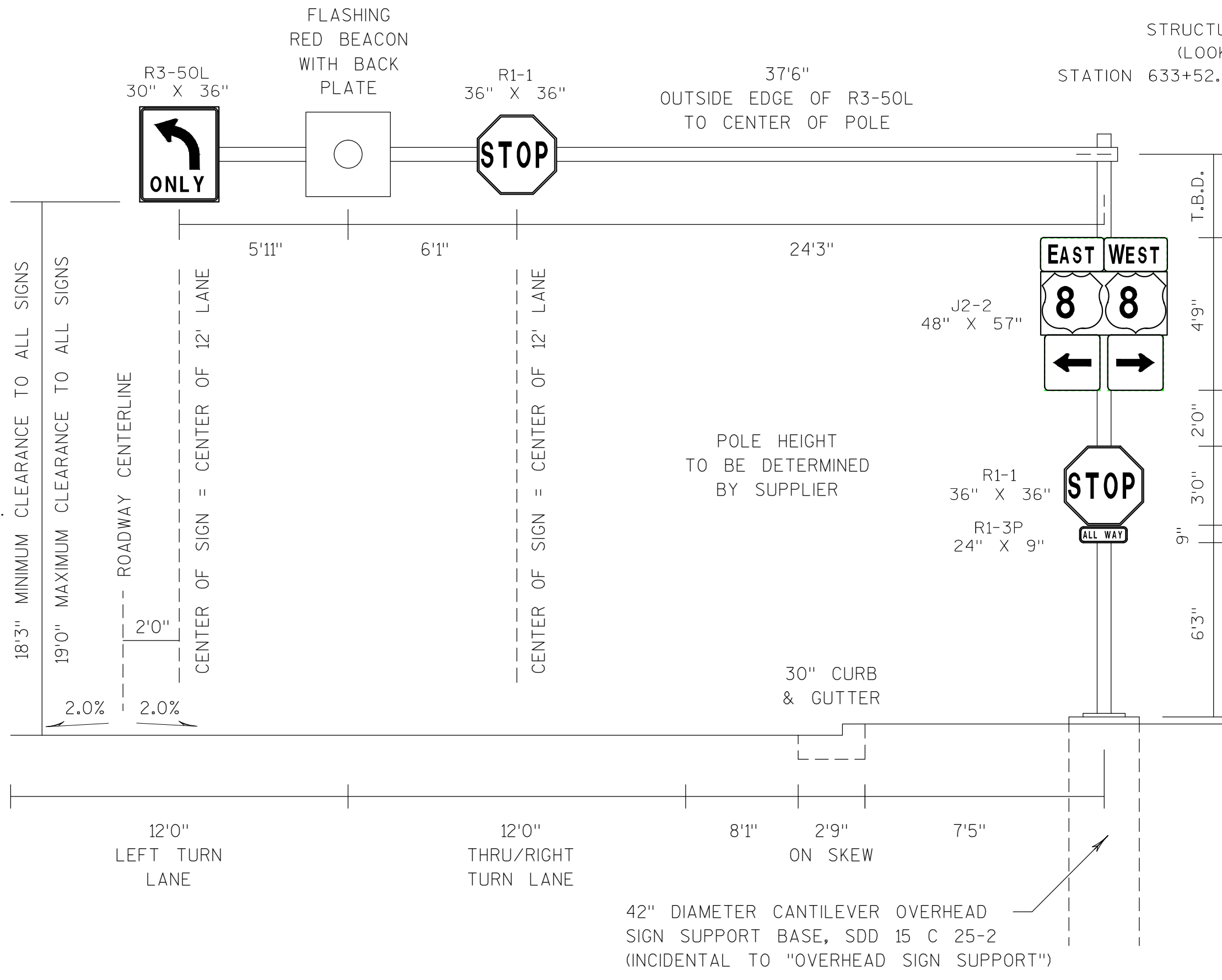
CONTRACTOR SHALL: SUBMIT SHOP DRAWINGS OF OVERHEAD SIGN SUPPORTS, PROVIDE DESIGN CALCULATIONS, SHOW SIGNS ON SHOP DRAWINGS, & PROVIDE AN IDENTIFICATION PLAQUE FOR EACH OVERHEAD SIGN SUPPORT.

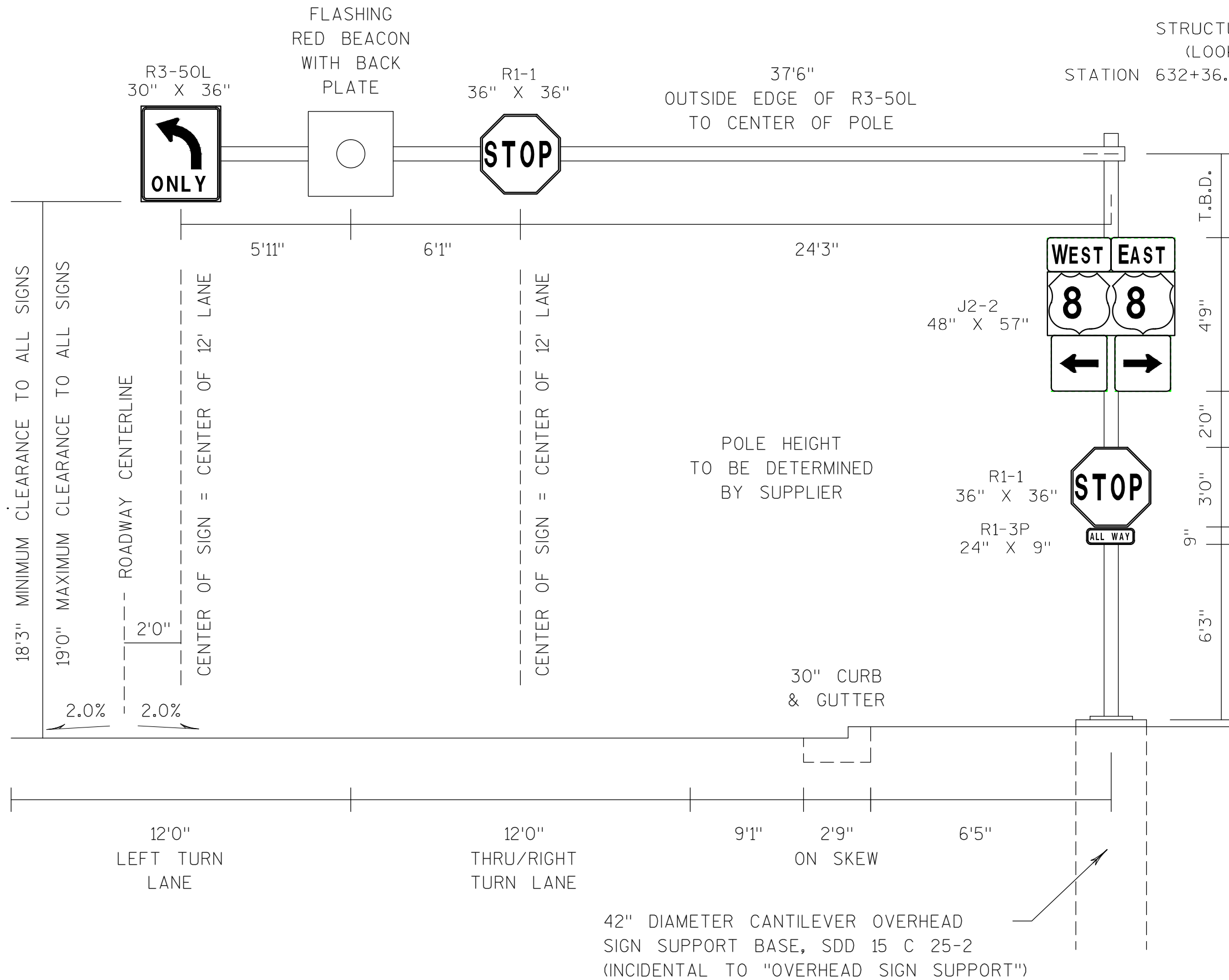
SIGNS, BEACONS, & BACK PLATES SHALL BE INSTALLED ON OVERHEAD SIGN SUPPORTS AT TIME OF ERECTION.

I.D. PLAQUES INCIDENTAL TO "OVERHEAD SIGN SUPPORT" & SIGN MOUNTING SYSTEMS INCIDENTAL TO "SIGNS TYPE II REFLECTIVE H".

DESIGN OVERHEAD SIGN SUPPORTS ACCORDING TO LATEST EDITION OF & SUPPLEMENT TO STATE OF WISCONSIN "STANDARD SPECIFICATIONS FOR HIGHWAY & STRUCTURE CONSTRUCTION" (SECTION 641).

CONSTRUCT OVERHEAD SIGN SUPPORT BASES ACCORDING TO STANDARD DETAIL DRAWING 15 C 25-2.





SEE "PAVEMENT MARKING (LEFT TURN LANE)"
FOR WORK AND ARROW PLACEMENT

SEE "MEDIAN ISLAND MARKING" FOR DIAGONAL
DETAILS

SEE "PAVEMENT MARKING WORDS" FOR WORD
DIMENSIONS

SEE "PAVEMENT MARKING ARROWS" FOR
ARROW DIMENSIONS

PAVEMENT MARKING
ISLAND NOSE EPOXY

PAVEMENT MARKING EPOXY
DIAGONAL 12-INCH YELLOW

PAVEMENT MARKING EPOXY
ARROWS TYPE 2 WHITE

PAVEMENT MARKING
WORDS "ONLY" WHITE

PAVEMENT MARKING EPOXY 4-INCH WHITE
TAPERS: STA 321+51 - 322+51
TANGENT: 322+51 - 323+41
LEFT AND RIGHT

PAVEMENT MARKING EPOXY
4-INCH DOUBLE YELLOW
STA. 320+25 - 321+51

BEGIN CONSTRUCTION
STA. 320+00

PAVEMENT MARKING
EPOXY 4-INCH WHITE
STA 321+37 - 321+51
LEFT AND RIGHT

PAVEMENT MARKING EPOXY
8-INCH WHITE
3' PAINT 9' GAP
STA 321+51 - 322+51

PAVEMENT MARKING EPOXY
4-DOUBLE YELLOW
TAPERS: STA 321+51 - 322+51
MEDIAN: STA 322+51 - 323+41

PAVEMENT MARKING EPOXY
8-INCH WHITE
STA 322+51 - 323+41

PAVEMENT MARKING EPOXY
18-INCH STOP LINE WHITE
CENTERED AT STA 323+40.25

PAVEMENT MARKING EPOXY
18-INCH STOP LINE WHITE
CENTERED AT STA 324+53.75

PAVEMENT MARKING EPOXY
8-INCH WHITE
STA 324+53 - 325+78

PAVEMENT MARKING EPOXY
4-INCH WHITE
STA 324+53 - 326+28
LEFT AND RIGHT

PAVEMENT MARKING EPOXY
ARROWS TYPE 2 WHITE

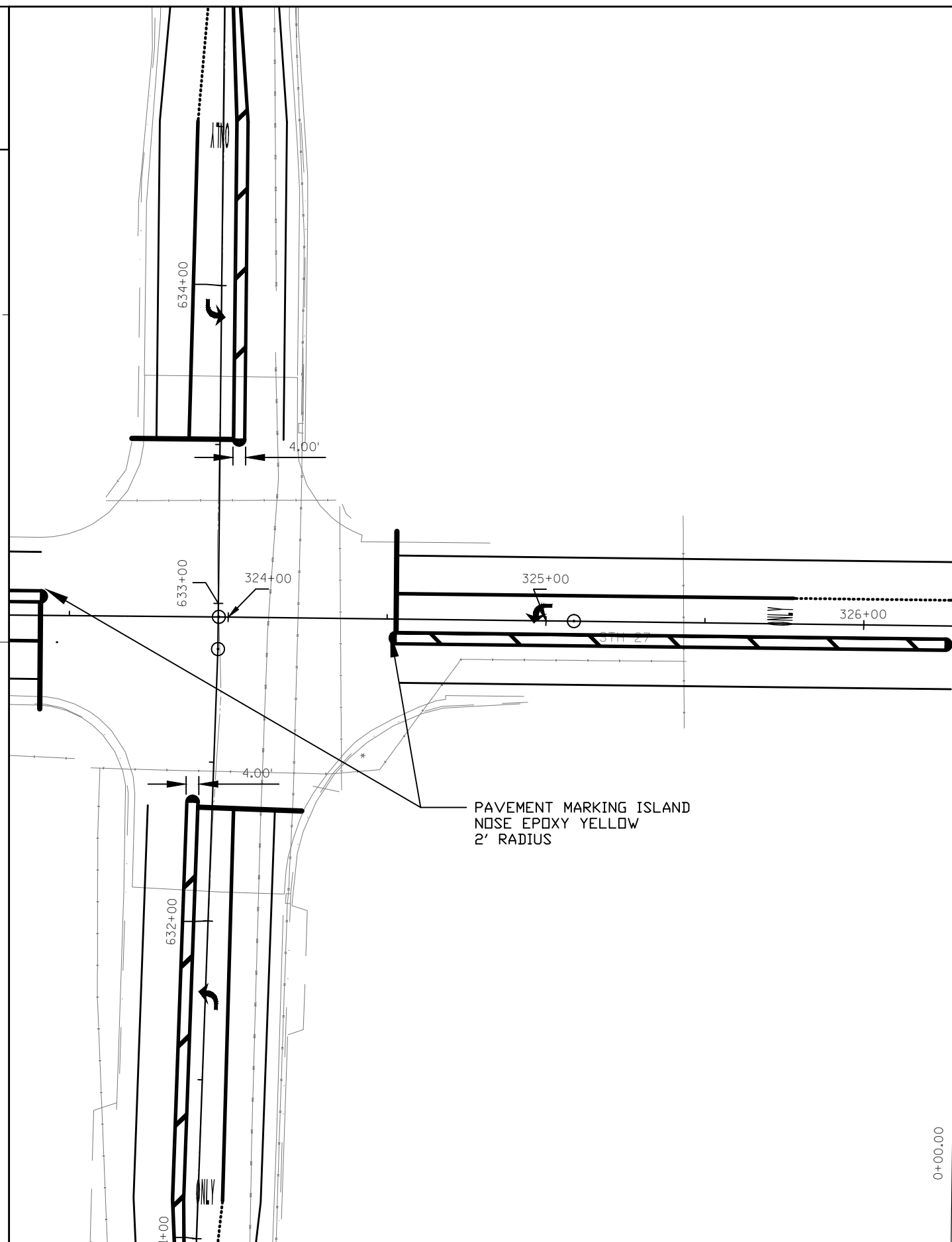
PAVEMENT MARKING
EPOXY WORDS
"ONLY" WHITE

PAVEMENT MARKING
EPOXY 8-INCH WHITE
3' PAINT, 9' GAP
STA 325+78 - 326+28

PAVEMENT MARKING EPOXY
DIAGONAL 12-INCH YELLOW

PAVEMENT MARKING EPOXY
4-INCH DOUBLE YELLOW
STA 324+53 - 326+26





PROJECT NO: 1580-12-72

HWY: STH 27

COUNTY: RUSK

PLAN: PAVEMENT MARKING

SHEET

E

2

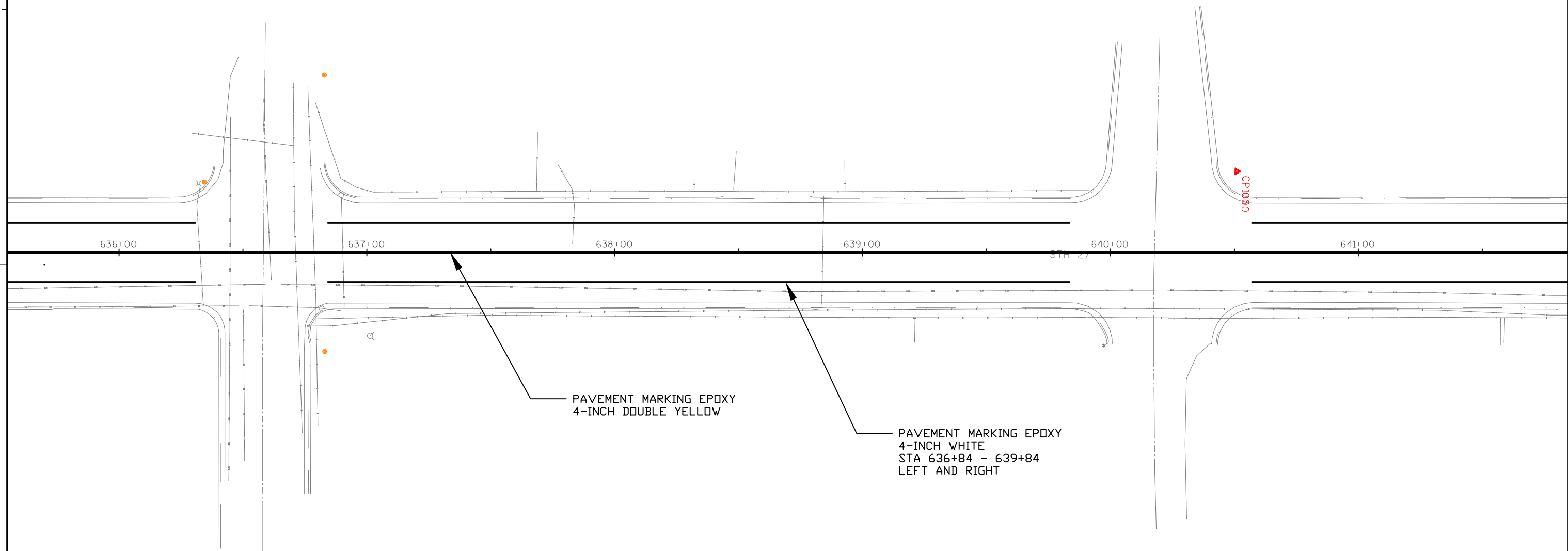


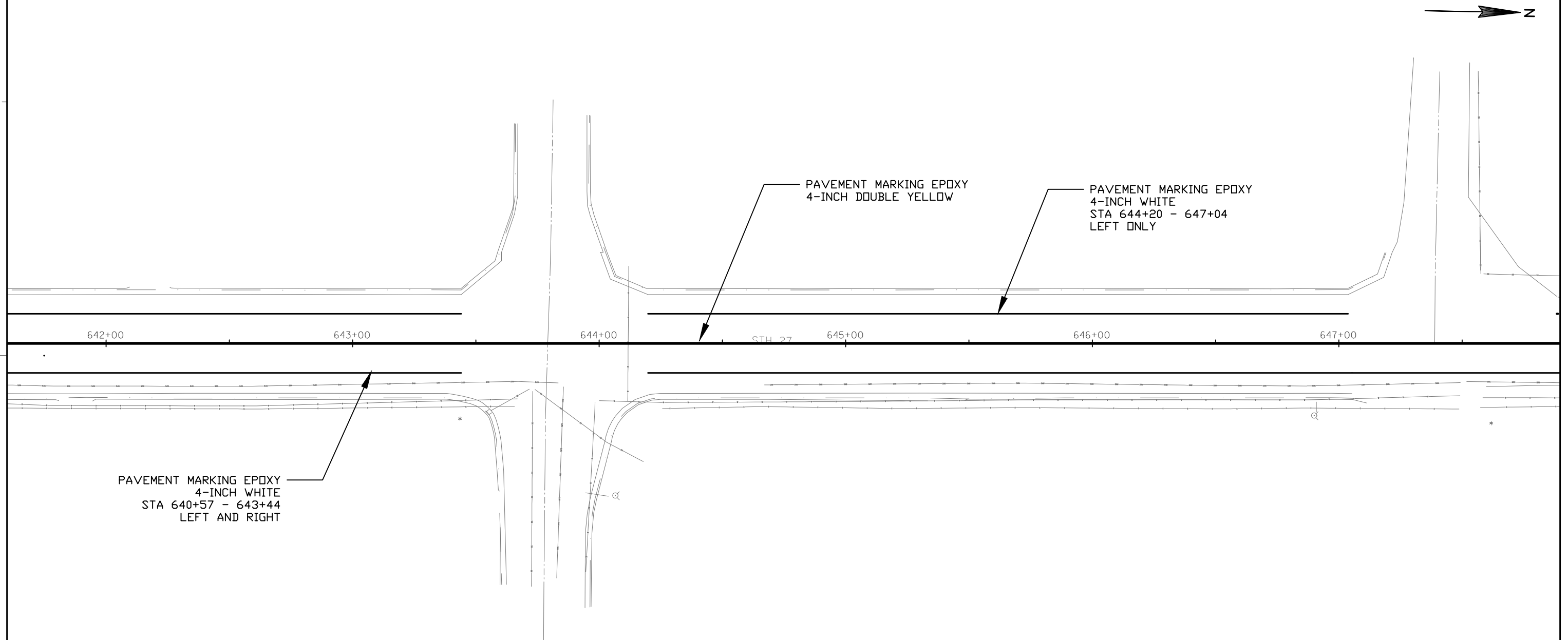
SEE "PAVEMENT MARKING (LEFT TURN LANE)"
FOR WORK AND ARROW PLACEMENT

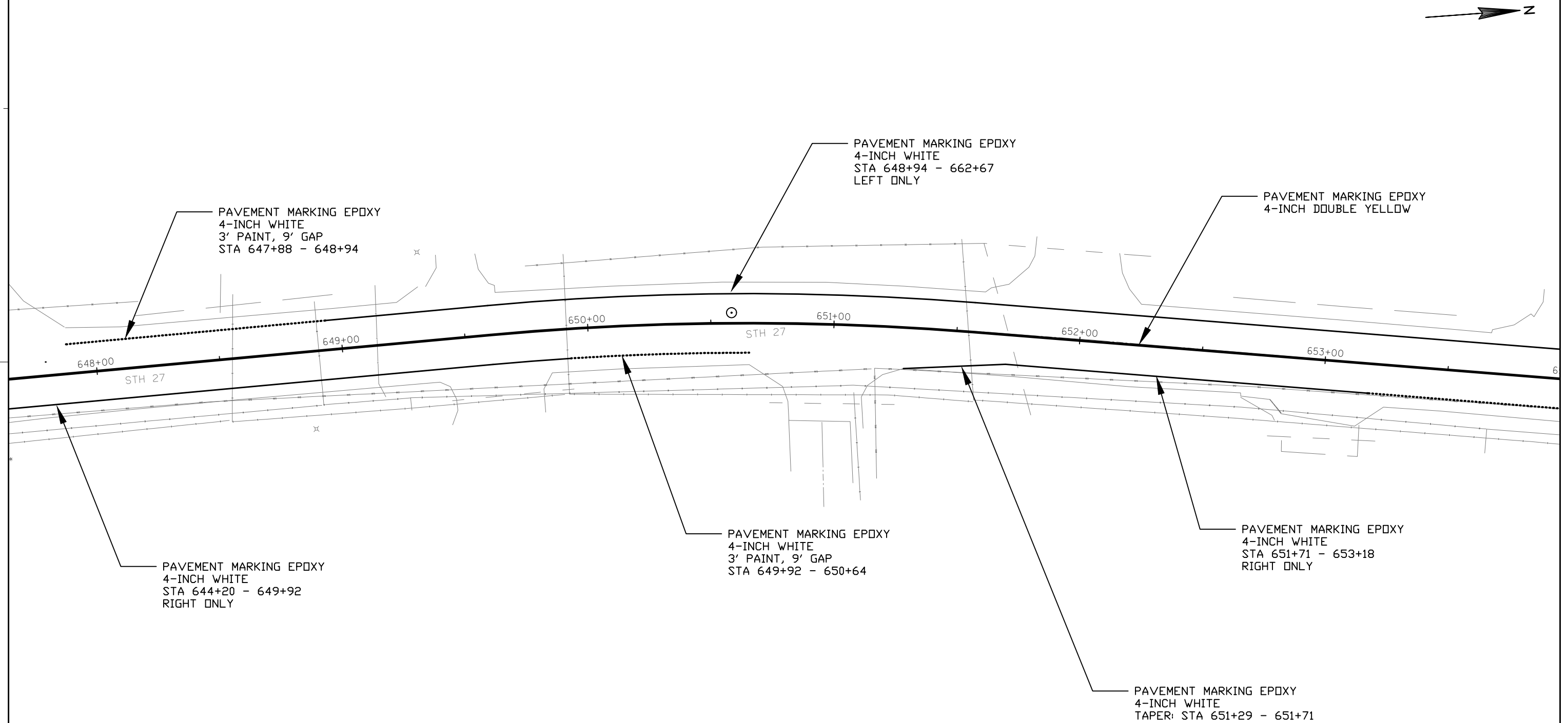
SEE "MEDIAN ISLAND MARKING" FOR DIAGONAL
DETAILS

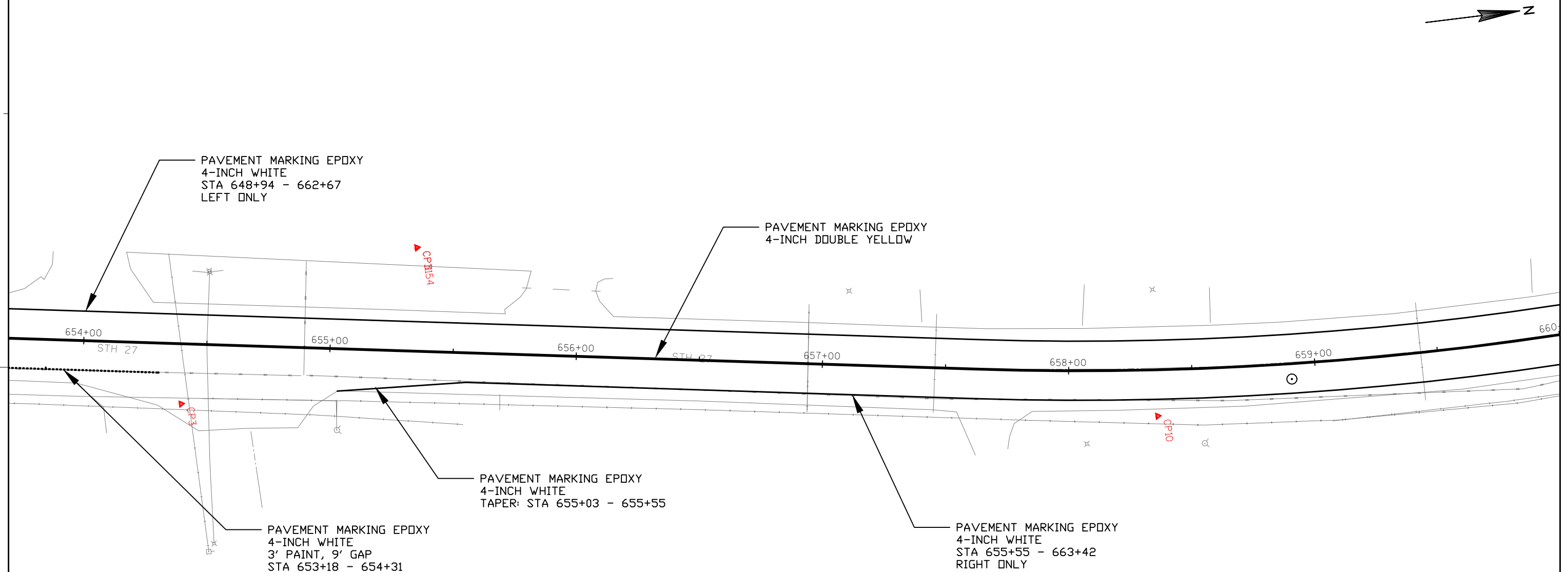
SEE "PAVEMENT MARKING WORDS" FOR WORD
DIMENSIONS

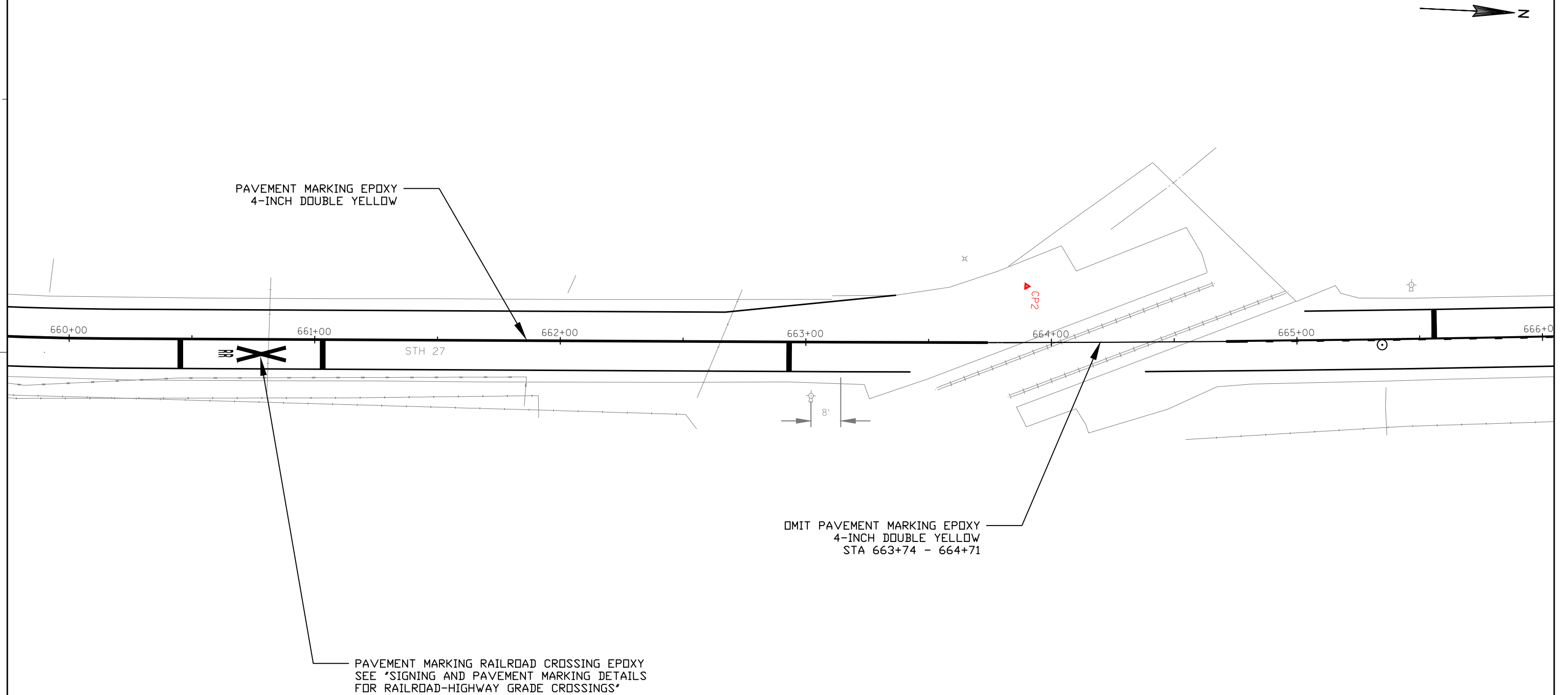
SEE "PAVEMENT MARKING ARROWS" FOR
ARROW DIMENSIONS

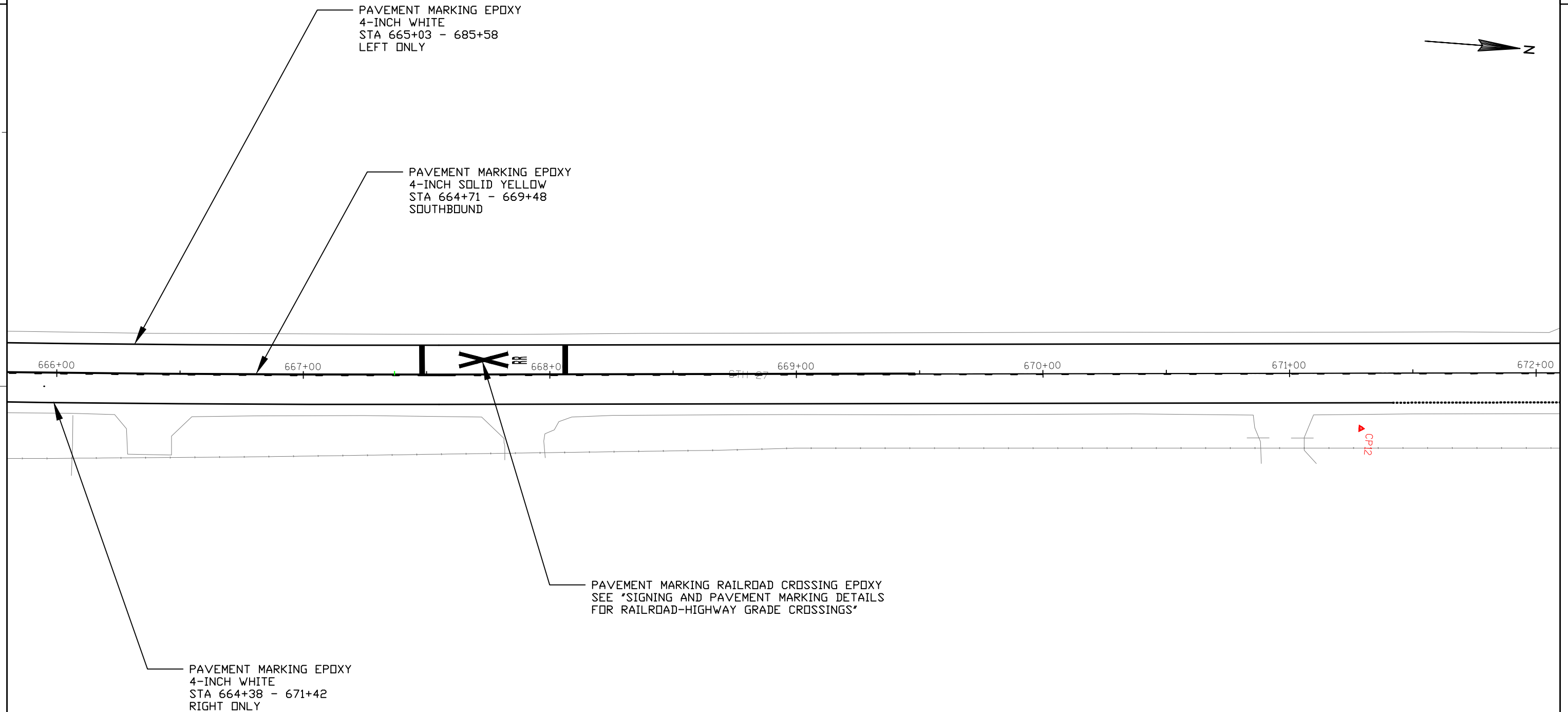












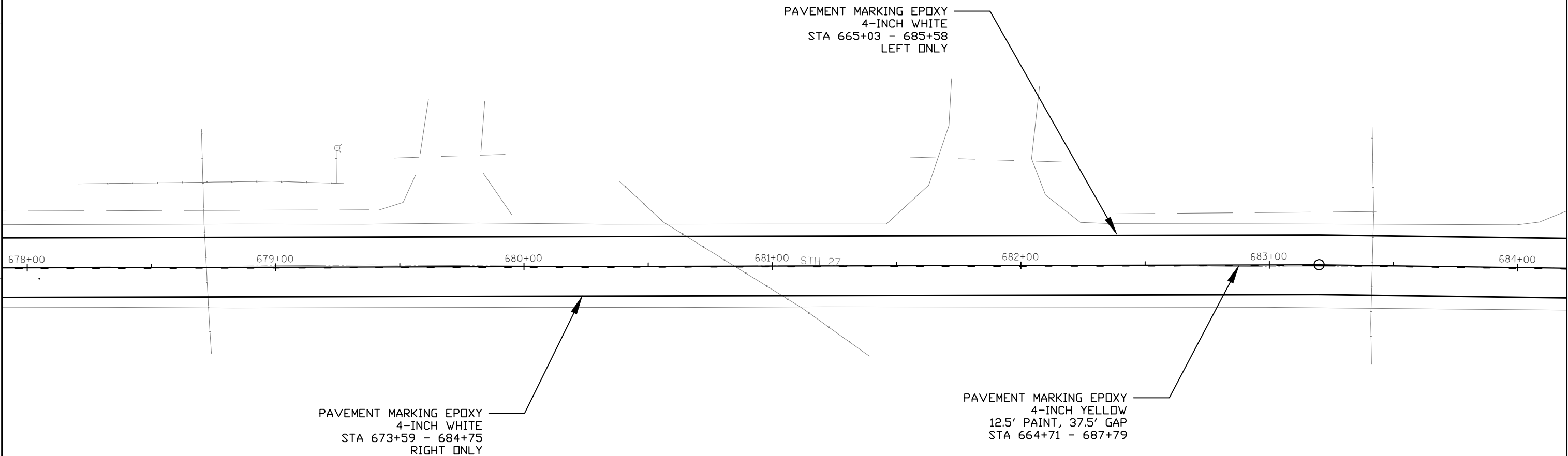


PAVEMENT MARKING EPOXY
4-INCH YELLOW
12.5' PAINT, 37.5' GAP
STA 664+71 - 687+79

672+00 673+00 STH 27 674+00 675+00 676+00 677+00 STH 27 678+00

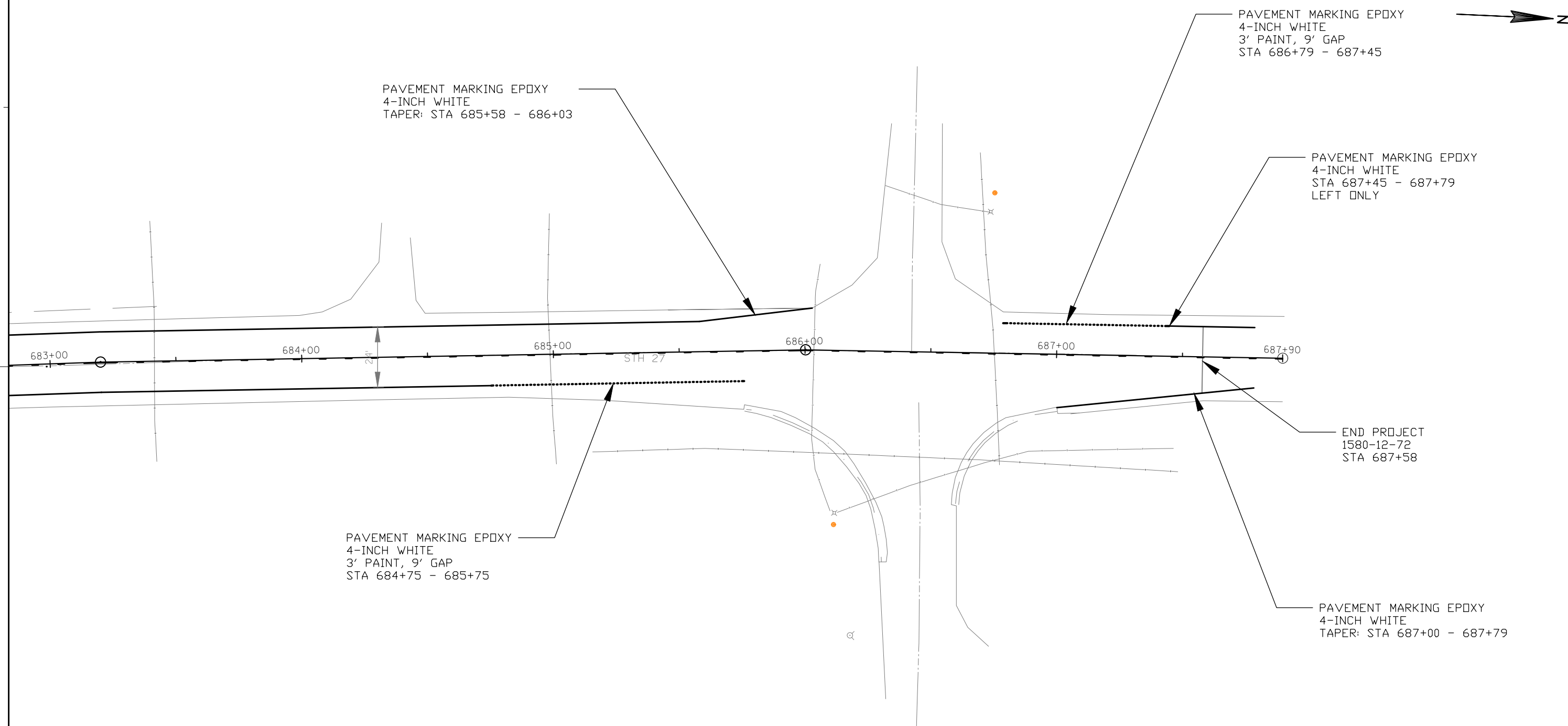
PAVEMENT MARKING EPOXY
4-INCH WHITE
TAPER: STA 673+08 - 673+59

PAVEMENT MARKING EPOXY
4-INCH WHITE
STA 673+59 - 684+75
RIGHT ONLY



2

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PROJECT NO: 1580-12-72

HWY: STH 27

COUNTY: RUSK

PLAN: PAVEMENT MARKING

SHEET	-----	E
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FILE NAME : N:\PDS\C3D\15801201\SHEETS\OTHER\PM\PM-SHEETS.DWG

PLOT DATE : 11/20/2013 9:19 AM PLOT BY : PEARSON, MICHAEL R

PLOT BY : PEARSON, MICHAEL R PLOT NAME : _____ PLOT SCALE : 1:40_XREF

WISDOT/CADDS SHEET 42

DATE 22APR14		E S T I M A T E O F Q U A N T I T I E S			
LINE				1580-12-72	
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0010	204.0105	REMOVING PAVEMENT BUTT JOINTS	SY	56.000	56.000
0020	204.0109.S	REMOVING CONCRETE SURFACE PARTIAL DEPTH	SF	23,000.000	23,000.000
0030	204.0115	REMOVING ASPHALTIC SURFACE BUTT JOINTS	SY	168.000	168.000
0040	204.0120	REMOVING ASPHALTIC SURFACE MILLING	SY	17,991.000	17,991.000
0050	204.0150	REMOVING CURB & GUTTER	LF	1,288.000	1,288.000
0060	204.0195	REMOVING CONCRETE BASES	EACH	4.000	4.000
0070	204.0220	REMOVING INLETS	EACH	1.000	1.000
0080	204.9060.S	REMOVING (ITEM DESCRIPTION) 01. EXISTING LIGHT POLES	EACH	4.000	4.000
0090	205.0100	EXCAVATION COMMON	CY	134.000	134.000
0100	211.0100	PREPARE FOUNDATION FOR ASPHALTIC PAVING (PROJECT) 01. 1580-12-72	LS	1.000	1.000
0110	213.0100	FINISHING ROADWAY (PROJECT) 01. 1580-12-72	EACH	1.000	1.000
0120	305.0110	BASE AGGREGATE DENSE 3/4-INCH	TON	14.000	14.000
0130	305.0120	BASE AGGREGATE DENSE 1 1/4-INCH	TON	75.000	75.000
0140	415.0090	CONCRETE PAVEMENT 9-INCH	SY	144.000	144.000
0150	416.0610	DRILLED TIE BARS	EACH	109.000	109.000
0160	440.4410.S	INCENTIVE IRI RIDE	DOL	4,000.000	4,000.000
0170	455.0605	TACK COAT	GAL	741.000	741.000
0180	465.0120	ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES	TON	48.000	48.000
0190	465.0315	ASPHALTIC FLUMES	SY	5.000	5.000
0200	520.8000	CONCRETE COLLARS FOR PIPE	EACH	2.000	2.000
0210	601.0409	CONCRETE CURB & GUTTER 30-INCH TYPE A	LF	332.000	332.000
0220	601.0411	CONCRETE CURB & GUTTER 30-INCH TYPE D	LF	1,143.000	1,143.000
0230	602.0505	CURB RAMP DETECTABLE WARNING FIELD YELLOW	SF	72.000	72.000
0240	608.0412	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 12-INCH	LF	5.000	5.000
0250	608.0418	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 18-INCH	LF	5.000	5.000
0260	611.0624	INLET COVERS TYPE H	EACH	1.000	1.000
0270	611.3004	INLETS 4-FT DIAMETER	EACH	1.000	1.000
0280	611.8110	ADJUSTING MANHOLE COVERS	EACH	3.000	3.000
0290	618.0100	MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) 01. 1580-12-72	EACH	1.000	1.000
0300	619.1000	MOBILIZATION	EACH	1.000	1.000
0310	625.0500	SALVAGED TOPSOIL	SY	130.000	130.000
0320	628.1905	MOBILIZATIONS EROSION CONTROL	EACH	1.000	1.000
0330	628.1910	MOBILIZATIONS EMERGENCY EROSION CONTROL	EACH	1.000	1.000
0340	628.2006	EROSION MAT URBAN CLASS I TYPE A	SY	130.000	130.000
0350	628.7015	INLET PROTECTION TYPE C	EACH	3.000	3.000
0360	629.0210	FERTILIZER TYPE B	CWT	0.200	0.200
0370	630.0110	SEEDING MIXTURE NO. 10	LB	5.000	5.000
0380	630.0200	SEEDING TEMPORARY	LB	10.000	10.000
0390	634.0616	POSTS WOOD 4X6-INCH X 16-FT	EACH	80.000	80.000
0400	637.2210	SIGNS TYPE II REFLECTIVE H	SF	608.920	608.920
0410	637.2230	SIGNS TYPE II REFLECTIVE F	SF	103.210	103.210
0420	638.2602	REMOVING SIGNS TYPE II	EACH	87.000	87.000
0430	638.3000	REMOVING SMALL SIGN SUPPORTS	EACH	69.000	69.000
0440	641.8100	OVERHEAD SIGN SUPPORT (STRUCTURE) 01. 42-INCH S-54-001	LS	1.000	1.000
0450	641.8100	OVERHEAD SIGN SUPPORT (STRUCTURE) 02. 42-INCH S-54-002	LS	1.000	1.000

DATE 22APR14		E S T I M A T E O F Q U A N T I T I E S			
LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	1580-12-72 QUANTITY
0460	641.8100	OVERHEAD SIGN SUPPORT (STRUCTURE) 03. 42-INCH S-54-003	LS	1.000	1.000
0470	641.8100	OVERHEAD SIGN SUPPORT (STRUCTURE) 04. 42-INCH S-54-004	LS	1.000	1.000
0480	642.5201	FIELD OFFICE TYPE C	EACH	1.000	1.000
0490	643.0100	TRAFFIC CONTROL (PROJECT) 01.1580-12-72	EACH	1.000	1.000
0500	643.0300	TRAFFIC CONTROL DRUMS	DAY	1,540.000	1,540.000
0510	643.0715	TRAFFIC CONTROL WARNING LIGHTS TYPE C	DAY	820.000	820.000
0520	643.0900	TRAFFIC CONTROL SIGNS	DAY	630.000	630.000
0530	643.1050	TRAFFIC CONTROL SIGNS PCMS	DAY	32.000	32.000
0540	646.0106	PAVEMENT MARKING EPOXY 4-INCH	LF	9,080.000	9,080.000
0550	646.0126	PAVEMENT MARKING EPOXY 8-INCH	LF	740.000	740.000
0560	646.0406	PAVEMENT MARKING SAME DAY EPOXY 4-INCH	LF	10,710.000	10,710.000
0570	647.0110	PAVEMENT MARKING RAILROAD CROSSINGS EPOXY	EACH	2.000	2.000
0580	647.0166	PAVEMENT MARKING ARROWS EPOXY TYPE 2	EACH	4.000	4.000
0590	647.0356	PAVEMENT MARKING WORDS EPOXY	EACH	4.000	4.000
0600	647.0566	PAVEMENT MARKING STOP LINE EPOXY 18-INCH	LF	120.000	120.000
0610	647.0606	PAVEMENT MARKING ISLAND NOSE EPOXY	EACH	4.000	4.000
0620	647.0726	PAVEMENT MARKING DIAGONAL EPOXY 12-INCH	LF	84.000	84.000
0630	647.0796	PAVEMENT MARKING CROSSWALK EPOXY 24-INCH	LF	138.000	138.000
0640	648.0100	LOCATING NO-PASSING ZONES	MI	1.203	1.203
0650	649.0100	TEMPORARY PAVEMENT MARKING 4-INCH	LF	1,000.000	1,000.000
0660	649.1200	TEMPORARY PAVEMENT MARKING STOP LINE REMOVABLE TAPE 18-INCH	LF	120.000	120.000
0670	650.4000	CONSTRUCTION STAKING STORM SEWER	EACH	1.000	1.000
0680	650.4500	CONSTRUCTION STAKING SUBGRADE	LF	240.000	240.000
0690	650.5000	CONSTRUCTION STAKING BASE	LF	240.000	240.000
0700	650.5500	CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER	LF	1,143.000	1,143.000
0710	650.7000	CONSTRUCTION STAKING CONCRETE PAVEMENT	LF	240.000	240.000
0720	650.8000	CONSTRUCTION STAKING RESURFACING REFERENCE	LF	6,350.000	6,350.000
0730	650.9910	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) 01.1580-12-72	LS	1.000	1.000
0740	652.0225	CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH	LF	200.000	200.000
0750	652.0235	CONDUIT RIGID NONMETALLIC SCHEDULE 40 3-INCH	LF	400.000	400.000
0760	653.0135	PULL BOXES STEEL 24X36-INCH	EACH	4.000	4.000
0770	653.0905	REMOVING PULL BOXES	EACH	4.000	4.000
0780	655.0610	ELECTRICAL WIRE LIGHTING 12 AWG	LF	1,000.000	1,000.000
0790	655.0615	ELECTRICAL WIRE LIGHTING 10 AWG	LF	2,000.000	2,000.000
0800	656.0200	ELECTRICAL SERVICE METER BREAKER PEDESTAL (LOCATION) 01. MINER INTERSECTION	LS	1.000	1.000
0810	658.0210	BACKPLATES SIGNAL FACE 1 SECTION 12-INCH	EACH	4.000	4.000
0820	658.0600	LED MODULES 12-INCH RED BALL	EACH	4.000	4.000
0830	658.5069	SIGNAL MOUNTING HARDWARE (LOCATION) 01. MINER INTERSECTION	LS	1.000	1.000
0840	676.0300	SIGNAL ASSEMBLY ADVANCE FLASHER TYPE 1	EACH	4.000	4.000
0850	690.0150	SAWING ASPHALT	LF	528.000	528.000
0860	690.0250	SAWING CONCRETE	LF	100.000	100.000
0870	ASP.1TOA	ON-THE-JOB TRAINING APPRENTICE AT \$5.00/HR	HRS	1,200.000	1,200.000
0880	ASP.1TOG	ON-THE-JOB TRAINING GRADUATE AT \$5.00/HR	HRS	300.000	300.000

DATE 22APR14		E S T I M A T E O F Q U A N T I T I E S			
LINE					1580-12-72
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0890	SPV. 0060	SPECIAL 01. ADJUSTING WATER VALVES	EACH	5.000	5.000
0900	SPV. 0060	SPECIAL 02. REMOVE EXISTING LIGHTING SERVICE	EACH	1.000	1.000
0910	SPV. 0060	SPECIAL 03. ADJUSTING WELL HEADS	EACH	12.000	12.000
0920	SPV. 0060	SPECIAL 04. ADJUSTING SANITARY SEWER MANHOLES MANHOLES	EACH	9.000	9.000
0930	SPV. 0090	SPECIAL 01. CONCRETE CURB AND GUTTER CURE AND SEAL TREATMENT	LF	1,475.000	1,475.000
0940	SPV. 0170	SPECIAL 01. REHEATING HMA LONGITUDINAL JOINTS SPECIAL	STA	53.000	53.000
0950	SPV. 0195	SPECIAL 01. ASPHALTIC SURFACE SPECIAL	TON	97.000	97.000
0960	SPV. 0195	SPECIAL 02. HMA PAVEMENT TYPE E-10 SPECIAL	TON	2,636.000	2,636.000

MISCELLANEOUS QUANTITIES

REMOVING BUTT JOINTS SUMMARY						
204.0105 204.0115 PAVEMENT ASPHALTIC SURFACE						
CAT	STATION	TO	STATION	LOC	SY	SY REM.
0010	320+36	-	320+46			56
0010	326+48	-	326+58		56	
0010	630+00	-	630+10			56
0010	687+48	-	687+58			56
TOTAL 0010					56	168

REMOVING CONCRETE SURFACE PARTIAL DEPTH					
204.0109.S					
CAT	STATION	TO	STATION	LOC	SF REMARKS
0010	323+25	-	326+28		15000
0010	632+20	-	633+75		8000
TOTAL 0010					23000

REMOVING ASPHALTIC SURFACE MILLING					
204.0120					
CAT	STATION	TO	STATION	LOC	SY REMARKS
0010	630+00	-	631+10		587
0010	631+10	-	632+10		534
0010	633+75	-	636+65		1354
0010	636+65	-	643+90		3223
0010	643+90	-	687+53		11814
0010	320+36	-	323+25		1600
TOTAL 0010					17991

REMOVING CONCRETE BASES					
204.0195					
CAT	STATION	TO	STATION	LOC	EACH REMARKS
0010	632+16	-	632+68	LT	1
0010	633+20	-	633+72	LT	1
0010	633+20	-	633+72	RT	1
0010	632+16	-	632+68	RT	1
TOTAL 0010					4

REMOVING (EXISTING LIGHT POLES)					
204.9060.S					
CAT	STATION	TO	STATION	LOC	EACH REMARKS
0010	632+16	-	632+68	LT	1
0010	633+20	-	633+72	LT	1
0010	633+20	-	633+72	RT	1
0010	632+16	-	632+68	RT	1
TOTAL 0010					4

EXCAVATION COMMON					
205.0100					
CAT	STATION	TO	STATION	LOC	CY REMARKS
0010	632+16	-	632+68	LT	45
0010	633+20	-	633+72	LT	46
0010	633+20	-	633+72	RT	43
TOTAL 0010					134

PREPARE FOUNDATION FOR ASPHALTIC PAVING (1580-12-72)			
211.0100			
CAT	LOCATION	LS	REMARKS
0010	PROJECT	1	
TOTAL 0010		1	

BASE AGGREGATE DENSE SUMMARY						
				305.0110	305.0120	
				BASE AGG DENSE		
				1 1/4-INCH	3/4-INCH	
CAT	STATION	TO	STATION	LOC	TON	TON REMARKS
0010	632+16	-	632+68	LT	25	
0010	633+20	-	633+72	LT	25	
0010	633+20	-	633+72	RT	25	
BEHIND CURB NW QUAD						14
TOTAL 0010					75	14

CONCRETE PAVEMENT 9-INCH							
				415.0090	415.0610		
				CONCRETE	DRI LLED		
				PAVEMENT	TIE		
				9-INCH	BARS		
CAT	STATION	TO	STATION	LOC	SY	EACH	REMARKS
0010	632+16	-	632+68	LT	48	28	*
0010	633+20	-	633+72	LT	49	28	*
0010	633+20	-	633+72	RT	47	28	*
0010	324+75	-	325+25	LT		25	CURB REPLACE
TOTAL 0010					144	109	
*TIE BARS CALCULATED AT 24" C-C							

MISCELLANEOUS QUANTITIES

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HMA SUMMARY										
		455. 0605	SPV. 0195. 02	SPV. 0170. 01	SPV. 0195. 01	465. 0120	465. 0315			
			HMA	REHEATING		ASPHALTIC				
		TACK	PAVEMENT	HMA PAVEMENT	ASPHALTIC	SURFACE	ASPHALTIC			
		COAT	TYPE E-10	LONGITUDINAL	SURFACE	DRIVEWAYS AND				
			SPECIAL	JOINTS SPECIAL	SPECIAL	FIELD ENTRANCES	FLUMES			
CAT	STATION	TO	STATION	GAL	TON	STA	TON	TON	SY	REMARKS

0010	320+36	-	326+28	79	354					
0010	320+36	-	323+40	41	159				5	LEVELING
0010	630+00	-	634+40	59	263					
0010	631+10	-	632+10	13	52					
0010	633+40	-	634+40	13	52					
0010	634+40	-	647+89	127	571	53				
0010	647+89		687+58	375	1185					
0010	SW QUAD PK LOT		ENTRANCES					24		
0010	SIDEROADS			34			97			
0010	NE QUAD BLENDING							24		BEHIND CURB
TOTAL 0010				741	2636	53	97	48	5	

CONCRETE CURB & GUTTER SUMMARY										
		204. 0150	601. 0411	601. 0409	602. 0505	SPV. 0090. 01				
		REMOVING	CONCRETE		CURB RAMP	CONC. C&G				
		CURB &	C&G 30-INCH		DETECT WRNING	CURE&SEAL				
		GUTTER	TYPE D	TYPE A	FIELD YLW	TREATMENT				
CAT	STATION	TO	STATION	LOC	LF	LF	LF	SF	LF	REMARKS

0010	321+40	-	323+17	RT	177	177			177	
0010	321+36	-	323+21	LT	150	185			185	
0010	324+75	-	325+25	LT	50		50		50	
0010	630+00	-	630+00	LT&RT	10			72	0	
	632+16		632+68	LT	90		94		94	
	633+20		633+72	LT	90		94		94	
	633+20		633+72	RT	90		94		94	
0010	633+71	-	636+32	RT	261	261			261	
0010	634+70	-	635+00	LT	30	30			30	
0010	643+40	-	673+70	LT	40	40			40	
0010	645+00	-	647+00	LT	300	300			300	
0010	UNDISTRIBUTED				150	150			150	
TOTAL 0010					1288	1143	332	72	1475	

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STORM SEWER AND UTILITY SUMMARY														
				204. 0220	520. 8000	608. 0412	608. 0418	611. 3004	611. 0624	611. 8110	SPV. 0060. 01	SPV. 0060. 03	SPV. 0060. 04	
				REMOVING	CONCRETE	SS REINFORCE	SS REINFORCED	INLETS	INLET COVER	ADJUSTING	ADJUSTING	ADJUSTING	ADJUSTING	
				INLETS	COLLAR	CONC CL IV	CONC CL IV	4-FOOT	TYPE H	MANHOLE	WATER	WELL	SANITARY	
					FOR PIPE	12-INCH	18-INCH	DIA.		COVERS	VALVES	HEADS	SEWER MANHOLES	
CAT	STATION	OFF	LOC	EACH	EACH	LF	LF	EACH	EACH	EACH	EACH	EACH	EACH	REMARKS
0010	633+56	26'	RT	1	2	5	5	1	1					
0060	PROJECT									5			9	
0010	PROJECT									3		12		
TOTAL 0010				1	2	5	5	1	1	3	5	12	9	

MAINTENANCE AND REPAIR OF HAUL ROADS (1580-12-72)			
618. 0100			
CAT	LOCATION	EACH	REMARKS
0010	PROJECT	1	
TOTAL 0010		1	

EROSION CONTROL SUMMARY											
			625. 0500	628. 1905	628. 1910	628. 2006	628. 7015	629. 0210	630. 0110	630. 0200	
			SALVAGED	MOB.	EMERG.	E-MAT URBAN	INLET PROT.	FERTILIZER	SEED MIX	SEEDING	
			TOPSOIL	EC	MOB. EC	CL 1 TYPE A	TYPE C	TYPE B	NO. 10	TEMPORARY	
CA	STATION	LOC	SY	EACH	EACH	SY	EACH	CWT	LB	LB	REMARKS
0010	USH 8 AND STH 27 INTERSECTION PROJECT		130	1	1	130	1	0. 20	5	10	
			2								
TOTAL 0010			130	1	1	130	3	0. 20	5	10	

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PERMANENT SIGN SUMMARY, PAGE 1										
			634.0616				AREA	637.2210 REFLECTIVE H	637.2230 REFLECTIVE F	
			WOOD POSTS 4" X 6"			SIGNS				
			16'			REFLECTIVE TYPE II				
NO.	STA.	LOCATION	EACH	SIGN CODE	SIZE	MESSAGE	S. F.			REMARKS
1	323+40	S-54-003	0	R3-50L	30"X36"	LEFT ONLY ARROW	7.50	7.50		MOUNT ON OVERHEAD ARM
2			0	R1-1	36"X36"	STOP	7.46	7.46		MOUNT ON OVERHEAD ARM
3			0	J2-2	48"X57"	STH 27 NORTH AND SOUTH	19.00	19.00		MOUNT ON POLE
4			0	R1-1	36"X36"	STOP	7.46	7.46		MOUNT ON POLE
5			0	R1-3P	24"X9"	ALL WAY	1.50	1.50		
6	324+54	S-54-004	0	R3-50L	30"X36"	LEFT ONLY ARROW	7.50	7.50		MOUNT ON OVERHEAD ARM
7			0	R1-1	36"X36"	STOP	7.46	7.46		MOUNT ON OVERHEAD ARM
8			0	J2-2	48"X57"	STH 27 SOUTH AND NORTH	19.00	19.00		MOUNT ON POLE
9			0	R1-1	36"X36"	STOP	7.46	7.46		MOUNT ON POLE
10			0	R1-3P	24"X9"	ALL WAY	1.50	1.50		
11	632+36	S-54-001	0	R3-50L	30"X36"	LEFT ONLY ARROW	7.50	7.50		MOUNT ON OVERHEAD ARM
12			0	R1-1	36"X36"	STOP	7.46	7.46		MOUNT ON OVERHEAD ARM
13			0	J2-2	48"X57"	USH 8 WEST AND EAST	19.00	19.00		MOUNT ON POLE
14			0	R1-1	36"X36"	STOP	7.46	7.46		MOUNT ON POLE
15			0	R1-3P	24"X9"	ALL WAY	1.50	1.50		
16	633+52	S-54-002	0	R3-50L	30"X36"	LEFT ONLY ARROW	7.50	7.50		MOUNT ON OVERHEAD ARM
17			0	R1-1	36"X36"	STOP	7.46	7.46		MOUNT ON OVERHEAD ARM
18			0	J2-2	48"X57"	USH 8 EAST AND WEST	19.00	19.00		MOUNT ON POLE
19			0	R1-1	36"X36"	STOP	7.46	7.46		MOUNT ON POLE
20			0	R1-3P	24"X9"	ALL WAY	1.50	1.50		
21	318+90	RT	2	D1-3	66"X42"	PRENTICE, OJIBWA, CORNELL	19.25	19.25		SEE SIGN PLATES
22	319+75	RT	1	R3-8A	36"X30"	LEFT ONLY, AHEAD & RIGHT ARROWS	7.50	7.50		
23	320+36	RT	1	D7-59R	54"X36"	TOURIST INFORMATION W/ARROW	13.50	13.50		
24	320+75	LT	1	R1-1	30"X30"	STOP	5.18	5.18		STORE EXIT
25	321+40	LT	1	R2-1	24"X30"	SPEED LIMIT 35	5.00	5.00		
26	321+40	RT	1	R1-1	30"X30"	STOP	5.18	5.18		
27	321+50	RT	1	R3-8A	36"X30"	LEFT ONLY, AHEAD & RIGHT ARROWS	7.50	7.50		
28	321+90	LT	1	J4-1	24"X36"	WEST USH 8	6.00	6.00		
29	324+75	RT	1	D9-2	24"X24"	HOSPITAL	4.00	4.00		BLUE
30	324+75	RT	0	MB6-1	21"X21"	LEFT ARROW	3.06	3.06		BLUE
31	325+10	LT	1	D9-2	24"X24"	HOSPITAL	4.00	4.00		
32	325+10	LT	1	MB6-1	21"X21"	RIGHT ARROW	3.06	3.06		
33	325+70	RT	1	J4-1	24"X36"	EAST USH 8	6.00	6.00		
34	325+70	LT	1	D7-59L	54"X36"	TOURIST INFORMATION W/ARROW	13.50	13.50		
35	326+25	LT	1	R3-8A	36"X30"	LEFT ONLY, AHEAD & RIGHT ARROWS	4.50	4.50		
36	326+30	LT	1	R1-1	30"X30"	STOP	5.18	5.18		
37	327+15	LT	1	W3-1	36"X36"	STOP AHEAD (SYMBOLIC)	9.00		9.00	
38	327+43	LT	1	R3-8A	36"X30"	LEFT ONLY, AHEAD & RIGHT ARROWS	7.50	7.50		
39	327+58	RT	1	R2-1	24"X30"	SPEED LIMIT 25	5.00	5.00		
40	327+60	LT	1	J1-1	24"X39"	JCT STH 27	6.50	6.50		
41	328+10	LT	1	D1-3	66"X42"	CAMERON, CORNELL, HAYWARD	19.25	19.25		SEE SIGN PLATES
42	628+75	LT	1	W3-1	36"X36"	STOP AHEAD (SYMBOLIC)	9.00		9.00	
43	628+75	LT	1	W10-1	36"X36"	RAILROAD CROSSING	7.07		7.07	
44	628+75	LT	1	W10-1A	24"X12"	EXEMPT	2.00		2.00	
45	628+75	RT	1	W3-1	36"X36"	STOP AHEAD (SYMBOLIC)	9.00		9.00	
46	629+83	RT	1	R1-1	30"X30"	STOP	5.18	5.18		
47	629+90	RT	1	W11-2	30"X30"	PEDESTRIAN CROSSING	6.25		6.25	
SUB-TOTAL			27				SUB-TOTAL 369.84	327.52	42.32	

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PERMANENT SIGN SUMMARY, PAGE 2											
			634.0616				AREA	637.2210 REFLECTIVE H	637.2230 REFLECTIVE F		
			WOOD POSTS 4" X 6"				SIGNS				
			16'								
NO.	STA.	LOCATION	EACH	SIGN CODE	SIZE	MESSAGE	S. F.			REMARKS	
48	629+90	RT	1	W16-7L	24"X12"	PEDESTRIAN ARROW	2.00		2.00		
49	630+10	RT	1	R3-8A	36"X30"	LEFT ONLY AND RIGHT AHEAD ARROWS	7.50	7.50			
50	630+10	LT	1	W11-2	30"X30"	PEDESTRIAN CROSSING	6.25		6.25		
51	630+10	LT	1	W16-7L	24"X12"	PEDESTRIAN ARROW	2.00		2.00		
52	630+50	LT	1	D1-56SL	66"X24"	BUSINESS DISTRICT	11.00	11.00			
53	630+90	LT	1	J4-1	24"X36"	SOUTH STH 27	6.00	6.00			
54	631+50	RT	2	D1-3	66"X42"	HAYWARD, CAMERON, PRENTICE	19.25	19.25		SEE SIGN PLATES	
55	633+45	LT	1	D9-2	24"X24"	HOSPITAL	4.00	4.00		BLUE	
56	633+45	LT	1	MB6-1	21"X21"	AHEAD ARROW	3.06	3.06		BLUE	
57	635+10	LT	1	R3-8A	36"X30"	LEFT ONLY AND RIGHT AHEAD ARROWS	7.50	7.50			
58	635+75	RT	1	J4-1	24"X36"	NORTH STH 27	6.00	6.00			
59	636+00	LT	2	D1-3	66"X42"	CORNELL, PRENTICE, CAMERON	19.25	19.25		SEE SIGN PLATES	
60	636+25	LT	1	R1-1	30"X30"	STOP	5.18	5.18			
61	636+25	RT	1	W11-2	30"X30"	PEDESTRIAN CROSSING	6.25		6.25		
62	636+90	RT	1	R1-1	30"X30"	STOP	5.18	5.18			
63	637+10	LT	1	W11-2	30"X30"	PEDESTRIAN CROSSING	6.25		6.25		
64	637+20	RT	1	R2-1	24"X30"	SPEED LIMIT 25	5.00	5.00			
65	637+60	LT	1	R3-8A	36"X30"	LEFT ONLY AND RIGHT AHEAD ARROWS	7.50	7.50			
66	638+30	LT	1	J1-1	24"X39"	JCT USH 8	6.50	6.50			
67	639+75	LT	1	W3-1	36"X36"	STOP AHEAD (SYMBOLIC)	9.00		9.00		
68	639+90	LT	1	R1-1	30"X30"	STOP	5.18	5.18			
69	640+50	RT	1	R1-1	30"X30"	STOP	5.18	5.18			
70	642+50	LT	1	R2-1	24"X30"	SPEED LIMIT 25	5.00	5.00			
71	643+40	LT	1	R1-1	30"X30"	STOP	5.18	5.18			
72	644+10	RT	1	R1-1	30"X30"	STOP	5.18	5.18			
73	647+10	LT	1	R1-1	30"X30"	STOP	5.18	5.18			
74	647+75	RT	1	R2-1	24"X30"	SPEED LIMIT 35	5.00	5.00			
75	651+00	LT	1	R2-1	24"X30"	SPEED LIMIT 35	5.00	5.00			
76	651+25	RT	1	R1-1	30"X30"	STOP	5.18	5.18			
77	655+00	RT	1	R1-1	30"X30"	STOP	5.18	5.18			
78	657+90	RT	1	R2-1	24"X30"	SPEED LIMIT 35	5.00	5.00			
79	661+00	RT	1	W10-1	36"X36"	RAILROAD CROSSING	7.07		7.07		
80	661+80	RT	1	J1-1	24"X39"	JCT CTH J	6.50	6.50			
81	663+50	LT	1	R2-1	24"X30"	SPEED LIMIT 35	5.00	5.00			
82	664+00	LT	1	R1-1	30"X30"	STOP	5.18	5.18			
83	668+00	LT	1	W10-1	36"X36"	RAILROAD CROSSING	7.07		7.07		
84	669+10	RT	1	W14-3	48"X36"	NO PASSING ZONE	6.00		6.00		
85	671+60	LT	2	I2-3	66"X24"	CITY OF LADYSMITH	11.00	11.00		SEE SIGN PLATES	
86	672+00	RT	2	D1-1	90"X24"	ARMY RESERVE CENTER	15.00	15.00		SEE SIGN PLATES	
87	672+00	RT	0	J13-1	24"X45"	CTH J, RIGHT ARROW	7.50	7.50			
88	672+80	LT	2	D1-1	90"X24"	ARMY RESERVE CENTER	15.00	15.00		SEE SIGN PLATES	
89	672+80	LT	0	J13-1	24"X45"	CTH J, RIGHT ARROW	7.50	7.50			
90	672+80	RT	1	R1-1	30"X30"	STOP	5.18	5.18			
91	674+50	RT	1	R2-1	24"X30"	SPEED LIMIT 55	5.00	5.00			
92	676+25	LT	1	R2-1	24"X30"	SPEED LIMIT 35	5.00	5.00			
93	684+00	LT	1	J1-1	24"X39"	JCT CTH J	6.50	6.50			
94	686+15	LT	1	R1-1	30"X30"	STOP	5.18	5.18			
95	686+75	RT	1	R1-1	30"X30"	STOP	5.18	5.18			
96	686+60	RT	1	I55-56	30"X36"	ADOPT A HIGHWAY	7.50	7.50		LADYSMITH LIONS CLUB	
97	686+75	LT	1	W3-5	36"X36"	REDUCED SPEED AHEAD, 35 MPH	9.00		9.00		
GRAND TOTAL 0010			80	GRAND TOTAL 0010			712.13	608.92	103.21		

MISCELLANEOUS QUANTITIES

SIGN REMOVAL SUMMARY						
				638. 2602	638. 3000	
				REMOVE	REMOVE	
				SIGNS	SM SIGN	
				TYPE II	SUPPORTS	

CAT	STATION	TO	STATION	LOC	EACH	EACH	REMARKS
0010	319+00	324+00	RT		1	2	PRENTICE, OJIBWA, CORNELL
0010			RT		1	1	RIGHT LANE MUST TURN RIGHT
0010			RT		1	1	STOP
0010			RT		1	1	STOP
0010			RT		1	0	ALL WAY
0010			RT		1	0	STH 27 NORTH, SOUTH W/ARROWS
0010			RT		1	0	RIGHT LANE MUST TURN RIGHT
0010			RT		1	0	STOP
0010			LT		1	1	STOP
0010			LT		1	1	SPEED LIMIT 35
0010			LT		1	1	WEST USH 8
0010	324+00	328+00	RT		1	1	HOSPITAL
0010			RT		1	1	BLUE ARROW
0010			RT		1	0	USH 8 EAST
0010			LT		1	0	STOP
0010			LT		1	0	RIGHT LANE MUST TURN RIGHT
0010			LT		1	0	STH 27 NORTH, SOUTH
0010			LT		1	0	ARROWS
0010			LT		1	0	STOP
0010			LT		1	0	ALL WAY
0010			LT		1	0	RIGHT LANE MUST TURN RIGHT
0010			LT		1	1	STOP
0010			LT		1	1	STOP AHEAD
0010			LT		1	1	NO PASSING ON RIGHT
0010			LT		1	1	CENTER LANE ARROWS ONLY
0010			LT		1	1	CAMERON, CORNELL, HAYWARD
0010			LT		1	0	TOURIST INFORMATION
0010	628+00	633+00	RT		1	1	STOP AHEAD
0010			RT		1	1	PEDESTRIAN CROSSING
0010			RT		1	1	STOP
0010			RT		1	2	RIGHT, LEFT, AHEAD ARROWS
0010			RT		1	1	LEFT LANE MUST TURN LEFT
SUBTOTAL					32	21	

SIGN REMOVAL SUMMARY						
				638. 2602	638. 3000	
				REMOVE	REMOVE	
				SIGNS	SM SIGN	
				TYPE II	SUPPORTS	

CAT	STATION	TO	STATION	LOC	EACH	EACH	REMARKS
0010				RT	1	2	HAYWARD, CAMERON, PRENTICE
0010				RT	1	0	STOP
0010				RT	1	0	RIGHT, LEFT, AHEAD ARROWS
0010				RT	1	0	USH 8, EAST WEST W/ARROWS
0010				RT	1	0	STOP
0010				LT	1	1	STOP AHEAD
0010				LT	1	0	RR CROSSING, EXEMPT
0010				LT	1	1	PEDESTRIAN CROSSING
0010				LT	1	1	STH 27 SOUTH
0010	633+00	636+50	RT		1	1	STH 27 NORTH
0010			RT		1	1	PEDESTRIAN CROSSING
0010			LT		1	0	STOP
0010			LT		1	0	LEFT, RIGHT, AHEAD ARROWS
0010			LT		1	0	USH 8, EAST WEST W/ARROWS
0010			LT		1	0	STOP
0010			LT		1	0	ALL WAY
0010			LT		1	1	LEFT LANE MUST TURN LEFT
0010			LT		1	2	LEFT, RIGHT, AHEAD ARROWS
0010			LT		1	1	CORNELL, PRENTICE, CAMERON
0010	636+50	640+00	LT		1	1	STOP
0010			RT		1	1	STOP
0010			RT		1	1	SPEED LIMIT 25
0010			RT		1	1	STOP
0010			LT		1	1	PEDESTRIAN CROSSING
0010			LT		1	1	USH 8
0010			LT		1	1	STOP
0010			LT		1	1	STOP AHEAD
0010	640+00	645+00	RT		1	1	STOP
0010			RT		1	1	STOP
0010			LT		1	1	SPEED LIMIT 25
0010			LT		1	1	STOP
SUBTOTAL					31	23	

MISCELLANEOUS QUANTITIES

3

SIGN REMOVAL SUMMARY

					638. 2602	638. 3000		
					REMOVE	REMOVE		
					SIGNS	SM SIGN		
					TYPE II	SUPPORTS		
STATION	TO	STATION	LOC		EACH	EACH	REMARKS	
0010	645+00	665+00	RT		1	1	SPEED LIMIT 35	
0010			RT		1	1	STOP	
0010			RT		1	1	STOP	
0010			RT		1	1	RR CROSSING	
0010			RT		1	1	SPEED LIMIT 35	
0010			RT		1	1	JCT CTH J	
0010			LT		1	1	SPEED LIMIT 35	
0010			LT		1	1	SPEED LIMIT 35	
0010			LT		1	1	STOP	
0010	665+00	687+90	RT		1	1	NO PASSING ZONE	
0010			RT		1	2	ARMY RESERVE CENTER	
0010			RT		1	0	CTH J W/ARROW	
0010			RT		1	1	STOP	
0010			RT		1	1	SPEED LIMIT 55	
0010			RT		1	1	STOP	
0010			RT		1	1	ADOPT A HIGHWAY	
0010			LT		1	1	RR CROSSING	
0010			LT		1	2	LADYSMITH	
0010			LT		1	2	ARMY RESERVE CENTER	
0010			LT		1	0	CTH J W/ARROW	
0010			LT		1	1	SPEED LIMIT 35	
0010			LT		1	1	JCT CTH J	
0010			LT		1	1	STOP	
0010			LT		1	1	SPEED LIMIT 35 AHEAD	

GRAND TOTAL 87 69

TRAFFIC CONTROL SUMMARY

			643. 0100	643. 0715			643. 0300			643. 1050
			PROJECT	WARNING LIGHTS			TRAFFIC			SIGNS
				TYPE C steady			DRUMS			PCMS
CAT.	STATION		EACH	EACH	DAYS	DAYS	EACH	DAYS	DAYS	DAY
INTERSECTION										
0010	NW QUADRANT		22	10	220		36	10	360	*
0010	NE QUADRANT		20	10	200		38	10	380	*
0010	SW QUADRANT		22	10	220		40	10	400	*
0010	SE QUADRANT		18	10	180		40	10	400	*
0010										*
0010	PROJECT	1								32

TOTAL 0010 1 82 40 820 154 40 1540 32

* ESTIMATED FOR INTERSECTION MILLING AND PAVING
ACTUAL NUMBERS MAY VARY

OVERHEAD SIGN SUPPORTS, 42"

					641. 8100. 01	641. 8100. 02	641. 8100. 03	641. 8100. 04		
					S-54-001	S-54-002	S-54-003	S-54-004		
					42"	42"	42"	42"		
CAT	STATION		OFFSET	LOC	LS	LS	LS	LS	REMARKS	
0020	323+40	-	37. 75	RT	1					
0030	324+54	-	37. 75	LT		1				
0040	632+36	-	37. 75	RT			1			
0050	633+52	-	37. 75	LT				1		
TOTAL 0010					1	1	1	1		

TRAFFIC CONTROL SIGNS

				643. 0900							
				TRAFFIC		SIGN				SIZE	
				CONTROL SIGNS		CODE					
CAT.	STATION	LOC		EACH	DAYS			MESSAGE	WIDE	HIGH	REMARKS
0010	BEFORE STA. 629+00 SPACED	RT	1	30		W20-1A		ROAD WORK AHEAD	48	48	
0010	ACCORDING TO "TRAFFIC	RT	1	30		W20-4A		ONE LANE ROAD AHEAD	48	48	
0010	CONTROL FOR LANE CLOSURE	RT	1	30		W3-4		BE PREPARED TO STOP	36	36	
0010	(SUITABLE FOR MOVING	RT	1	30		W20-7A		FLAGGER AHEAD	48	48	
0010	OPERATIONS)"	RT	1	30							
0010	629+00	LT	1	30		G20-2A		END ROAD WORK	48	24	
0010	688+00	RT	1	30		G20-2A		END ROAD WORK	48	24	
0010	AFTER STA. 688+00 SPACED	LT	1	30		W01-4L		FLAGGER AHEAD	48	48	
0010	ACCORDING TO "TRAFFIC	LT	1	30		W3-4		BE PREPARED TO STOP	36	36	
0010	CONTROL FOR LANE CLOSURE	LT	1	30		W20-4A		ONE LANE ROAD AHEAD	48	48	
0010	(SUITABLE FOR MOVING	LT	1	30		W20-1A		ROAD WORK AHEAD	48	48	
0010	OPERATIONS)"	LT	1	30							
0010	BEFORE STA. 320+00 SPACED	RT	1	30		W20-1A		ROAD WORK AHEAD	48	48	
0010	ACCORDING TO "TRAFFIC	RT	1	30		W20-4A		ONE LANE ROAD AHEAD	36	36	
0010	CONTROL FOR LANE CLOSURE	RT	1	30		W3-4		BE PREPARED TO STOP	48	48	
0010	(SUITABLE FOR MOVING	RT	1	30		W20-7A		FLAGGER AHEAD	48	48	
0010	OPERATIONS)"	RT	1	30							
0010	319+00	LT	1	30		G20-2A		END ROAD WORK	48	24	
0010	327+00	RT	1	30		G20-2A		END ROAD WORK	48	24	
0010	AFTER STA. 326+00 SPACED	LT	1	30		W20-7A		FLAGGER AHEAD	48	48	
0010	ACCORDING TO "TRAFFIC	LT	1	30		W3-4		BE PREPARED TO STOP	24	24	
0010	CONTROL FOR LANE CLOSURE	LT	1	30		W20-4A		ONE LANE ROAD AHEAD	48	48	
0010	(SUITABLE FOR MOVING	LT	1	30		W20-1A		ROAD WORK AHEAD	48	48	
0010	OPERATIONS)"	LT	1	30							

TOTAL 0010 630

3

MISCELLANEOUS QUANTITIES

PAVEMENT MARKING SUMMARY #1												
					646. 0106	646. 0126	646. 0110	646. 0406	647. 0166	647. 0356	647. 0566	
					EPOXY 4-INCH	EPOXY	RR XING	EPXY SAME DAY	EPOXY ARRW	EPOXY WORDS	EPOXY STOP	
					WHI TE	8-INCH WHT	EPOXY	4-INCH	TYPE 2	ONLY	LI NE 18-INCH	
CAT	STATION	TO	STATION	LOC	LF	LF	EACH	YELLOW LF	EACH	EACH	LF	REMARKS
0010	320+00	-	323+50		550	130		1100	1	1	30	
0010	324+50	-	326+25		350	120		700	1	1	30	
0010	630+00	-	632+40		480	330		960	1	1	30	
0010	633+50	-	636+25		550	130		1100	1	1	30	
0010	636+25	-	647+00					2150				
0010	647+00	-	664+00		3000	15	1	3200				
0010	664+75	-	672+00		1450	15	1	1100				
0010	672+00	-	687+58		2700			400				
TOTAL 0010					9080	740	2	10710	4	4	120	

PAVEMENT MARKING SUMMARY #2											
					647.0606	647.0726	647.0796	648.0100	649.0100	649.1200	
					I SLAND	DIAGONAL	CROSSWALK		TEMP PVT	TEMP STOP LI NE	
					NOSE	EPOXY	EPOXY 24-INCH	LOCATE NO	MARKING	REMOVABLE TAPE	
					EPOXY	12-INCH	WHI TE	PASS ZONE	4-INCH	18-INCH	
CAT	STATION	TO	STATION	LOC	EACH	LF	LF	MI	LF	LF	REMARKS
0010	320+00	-	323+50		1	18				30	
0010	324+50	-	326+25		1	30				30	
0010	630+00	-	632+40		1	18	138			30	
0010	633+50	-	636+25		1	18				30	
0010	636+25	-	687+58					1.203			
PROJECT									1000		
TOTAL 0010					4	84	138	1.203	1000	120	

CONSTRUCTION STAKING SUMMARY												
					650. 4000	650. 4500	650. 5000	650. 5500	650. 7000	650. 8000	650. 9910	
					STORM	SUBGRADE	BASE	CURB AND	CONCRETE	RESURFACE	SUPPLEMENTAL	
					SEWER			GUTTER	PAVEMENT	REFERENCE	CONTROL	
CAT	STATION	TO	STATION	LOC	EACH	LF	LF	LF	LF	LF	LS	REMARKS
0010	633+56	26'	OFF	RT	1							
0010	632+25	-	632+45	LT		80	80		80			SW CURB
0010	633+25	-	633+75	LT		80	80		80			NW CURB
0010	633+25	-	633+75	RT		80	80		80			NE CURB
0010	321+75	-	323+22	LT&RT				300				
0010	324+75	-	325+25	LT				50				
0010	633+75	-	635+75	RT				200				
0010	643+40	-	647+10	LT				240				
0010	PROJECT							353		6350	1	
TOTAL 0010					1	240	240	1143	240	6350	1	

MISCELLANEOUS QUANTITIES

LIGHT AND SIGN POLE SUMMARY #1												
				652.0225	652.0235	653.0135	653.0905	655.0610	655.0615	656.0200	658.5069	
				CONDUIT RIGID		PULL BOX	REMOVING	ELECTRICAL			SIGNAL	
				NON-METALIC SCH 40		STEEL	PULL	WIRE, LIGHTING		SERVICE METR	MOUNTING	
				2-INCH	3-INCH	24 X 36	BOXES	12 AWG	10 AWG	BRKR PED	HARDWARE	
CAT	STATION	TO	LOC	LF	LF	EACH	EACH	LF	LF	EACH	LS	REMARKS
0010	323+40	-	37.75' RT	50	100	1	1	250	500			USH 8 ALI
0010	324+54	-	37.75' LT	50	100	1	1	250	500			USH 8 ALI
0010	632+36	-	37.75' RT	50	100	1	1	250	500			STH 27 ALI
0010	633+52	-	37.75' LT	50	100	1	1	250	500			STH 27 ALI
0010	PROJECT									1	1	
TOTAL 0010				200	400	4	4	1000	2000	1	1	

SAWING ASPHALT				
		690.0150	690.0250	
		SAWING	SAWING	
		ASPHALT	CONCRETE	
CAT	LOCATION	LF	LF	REMARKS
0010	320+36	48		END OF PAVING, PARTIAL DEPTH
0010	326+28		50	"
0010	630+00	48		"
0010	687+58	34		"
0010	NE QUAD	200		"
0010	CURB RAMPS		20	CURB AND SIDEWALK
0010	CURB ENDS		30	VARIOUS LOC.
0010	SIDE STREETS	198		PARTIAL DEPTH
TOTAL 0010		528	100	

LIGHT AND SIGN POLE SUMMARY #2							
			658.0210	658.0600	676.0300	SPV.0060.02	
			BACKPLAT SIGNL	LED MODULE	SGNL ASSY	REMOVE EXIST	
			FACE 1 SECTION	RED BALL	ADV FLASHER	LIGHTING	
			12-INCH	12-INCH	TYPE 1	SERVICE	
CAT	STATION	LOC	EACH	EACH	EACH	EA	REMARKS
0010	323+40	37.75' RT	1	1	1		
0010	324+54	37.75' LT	1	1	1		
0010	632+36	37.75' RT	1	1	1		
0010	633+52	37.75' LT	1	1	1		
0010	PROJECT					1	
TOTAL 0010			4	4	4	1	

ALIGNMENT DATA
S.T.H. 27

POB STA. 628+52.01
Y = 563097.45
X = 810591.89
PI STA. 632+95.70
Y = 563541.13
X = 810595.32
PI STA. 635+95.70
Y = 563841.09
X = 810590.38

ALIGNMENT DATA
U.S.H. 8

POB STA. 316+97.00
Y = 563528.78
X = 809895.43
PI STA. 323+97.00
Y = 563541.13
X = 810595.32
PI STA. 330+97.00
Y = 563552.22
X = 811295.23

UTILITY INTERESTS REQUIRED		
UTILITY #	OWNER(S)	INTEREST REQUIRED
5	CITY OF LADYSMITH	RELEASE OF RIGHTS
6	CHARTER COMMUNICATIONS	RELEASE OF RIGHTS

UTILITY EASEMENT INFORMATION		
PARCEL #	COMPANY NAME	RECORDING DATA
1	NO EASEMENT INFORMATION FOUND	-----
2	NO EASEMENT INFORMATION FOUND	-----

SURVEY SPINDLE

County Coordinates
Y = 566233.82
X = 810692.45

TRANSPORTATION PROJECT PLAT NO: 1580-12-22 - 4.01

AMENDMENT NO: 1

AMENDS PARCEL 1 OF TPP 1580-12-22-4.01, RECORDED AS DOCUMENT 327169. LOCATED IN LOT 1 BLOCK 12 LADYSMITH TOWNSITE COMPANY'S FIRST ADDITION ALSO BEING PART OF THE NE 1/4 - NE 1/4 OF SECTION 4, T34N, R6W, CITY OF LADYSMITH, RUSK COUNTY, WISCONSIN

RELOCATION ORDER STH 27 CITY OF LADYSMITH, WEST NINTH STREET (MINER AVENUE TO FLAMBEAU AVENUE) RUSK COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3) AND 84.09, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:
1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.



SECTION COURSE TABLE		
COURSE	BEARING	DISTANCE
124-125	N 88°59'20" E	2655.20'
124-409	N 88°59'20" E	2386.50'
125-123	N 02°06'34" E	2661.51'
125-126	N 89°05'31" E	2638.62'
125-200	S 01°35'04" W	2540.28'

NOTES:

COORDINATES AND BEARINGS SHOWN ON THIS PLAT ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM, RUSK COUNTY ZONE, NAD83 (2007) ADJUSTMENT. THE COORDINATES SHOWN ARE GRID COORDINATES AND ARE TO BE USED AS GRID OR GROUND VALUES ON THIS PLAT.

RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 1" I.D. X 24" IRON PIPE) AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD".

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: EXISTING HIGHWAY RIGHT-OF-WAY FOR S.T.H. 27 & U.S.H. 8 ESTABLISHED FROM PREVIOUS PROJECT 1580-09-21

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LANDS TO THE DEPARTMENT OF TRANSPORTATION.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE. ALL TLES EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE. BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHT TO MAKE OR CONSTRUCT IMPROVEMENT ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.

CONVENTIONAL ABBREVIATIONS

ACCESS POINT/
DRIVEWAY CONNECTION
ACCESS RIGHTS
ACRES
AND OTHERS
CENTERLINE
CERTIFIED SURVEY MAP
CORNER
DOCUMENT
EASEMENT
HIGHWAY EASEMENT
LAND CONTRACT
MONUMENT
PAGE
PERMANENT LIMITED EASEMENT
PROPERTY LINE
RECORDED AS
REFERENCE LINE

AP
AR
AC.
ET.AL.
C/L
CSM
COR.
DOC.
EASE.
H.E.
LC
MON.
P.
PLE
PL
(100')
R/L

CURVE DATA

LONG CHORD
LONG CHORD BEARING
RADIUS
DEGREE OF CURVE
CENTRAL ANGLE OR DELTA
LENGTH OF CURVE
TANGENT
R/L

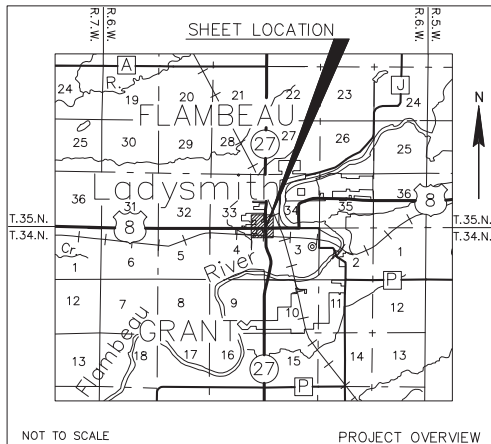
CONVENTIONAL UTILITY SYMBOLS

WATER
GAS
TELEPHONE
OVERHEAD
TRANSMISSION LINES
ELECTRIC
CABLE TELEVISION
FIBER OPTIC
SANITARY SEWER
STORM SEWER

W
G
T
OH
E
TV
FO
SAN
SS
NON
COMPENSABLE
COMPENSABLE

CONVENTIONAL SYMBOLS

(1" UNLESS NOTED)
PROPOSED R/W LINE
EXISTING H.E. LINE
PROPERTY LINE
LOT & TIE LINES
SLOPE INTERCEPTS
CORPORATE LIMITS
NO ACCESS
(BY PREVIOUS ACQUISITION/CONTROL)
ACCESS RESTRICTED
(BY ACQUISITION)
NO ACCESS
(BY STATUTORY AUTHORITY)
SECTION LINE
QUARTER LINE
SIXTEENTH LINE
EXISTING CENTERLINE
PROPOSED REFERENCE LINE
PARALLEL OFFSET



NOT TO SCALE PROJECT OVERVIEW

R/W STATION AND OFFSET TABLE FOR U.S.H. 8		
POINT #	STATION	OFFSET
404	323+58.92	57.36'
405	323+28.44	33.00'
406	322+52.56	33.00'
407	322+52.54	50.00'
408	321+28.04	50.00'
409	321+28.04	33.00'
410	323+26.04	33.00'
411	323+64.08	72.00'
TLE450	323+32.88	38.00'
TLE451	322+52.55	38.00'
TLE453	321+28.04	38.00'
TLE454	323+30.92	38.00'

R/W STATION AND OFFSET TABLE FOR S.T.H. 27		
POINT #	STATION	OFFSET
401	630+63.99	42.41'
402	631+13.97	43.62'
403	631+30.22	34.01'
404	632+27.40	36.36'
405	632+60.97	67.70'
410	633+28.62	71.00'
411	633+67.66	33.00'
412	634+28.66	33.00'
413	634+28.66	5.03'
TLE450	632+56.09	63.14'
TLE454	633+33.62	66.13'
TLE455	633+62.52	38.00'
TLE456	634+62.52	38.00'
TLE457	634+62.52	33.00'

R/W COURSE TABLE		
COURSE	BEARING	DISTANCE
400-401	N 89°33'25" W	36.04'
401-402	N 00°56'37" W	50.00'
402-403	N 31°02'35" E	18.88'
403-404	N 00°56'37" W	97.20'
404-405	N 42°35'14" W	45.93'
405-406	S 88°59'20" W	75.88'
406-407	S 00°56'37" E	17.00'
407-408	S 88°59'20" W	124.50'
408-409	N 01°00'40" W	83.00'
409-410	N 88°59'20" E	198.00'
410-411	N 43°16'46" E	54.48'
411-412	N 00°56'37" W	61.00'
412-413	N 89°03'23" E	38.03'
413-125	S 02°06'34" W	100.10'
125-400	S 01°35'04" W	264.75'

FOUND IRON PIPE/PI

R/W MONUMENT
R/W STANDARD
SIGN
SECTION CORNER MONUMENT
SECTION CORNER SYMBOL
FEE (HATCH VARIES)
TEMPORARY LIMITED
EASEMENT
PERMANENT LIMITED
EASEMENT
R/W BOUNDARY POINT
PARCEL NUMBER
UTILITY INTEREST
SIGN NUMBER
(OFF PREMISE)
BUILDING

I HEREBY CERTIFY THAT THIS PLAT MEETS ALL REQUIREMENTS OF SECTION 84.095, WISCONSIN STATUTES. THIS PLAT WAS PREPARED BY OR UNDER THE DIRECTION OF

KOLBY SCHERTZ
PRINTED NAME
5/28/2012
DATE

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN
DEPARTMENT OF TRANSPORTATION

MICHAEL PILLER
PRINTED NAME
06/04/2012
DATE



2811 8TH STREET SOUTH, SUITE 8
WISCONSIN RAPIDS, WI 54494
715-423-3525

R/W COORDINATE TABLE		
Point #	Y	X
401	563309.75	810551.12
402	563359.74	810550.30
403	563375.92	810560.04
404	563473.11	810558.44
405	563506.92	810527.35
406	563505.58	810451.48
407	563488.59	810451.76
408	563486.39	810327.28
409	563569.38	810325.82
410	563572.87	810523.79
411	563612.53	810561.14
412	563673.53	810560.13
413	563674.15	810598.16
TLE450	563502.00	810531.88
TLE451	563500.58	810451.57
TLE453	563574.38	810325.74
TLE454	563577.95	810528.58
TLE455	563607.32	810556.22
TLE456	563707.30	810554.58
TLE457	563707.38	810559.58

EXISTING MONUMENTS			
Point #	Y	X	DESCRIPTION
113	563586.12	810328.36	3/4" IRON ROD
115	563572.02	810545.37	PK NAIL

FILE NAME : S:\SURVEY\LAND SURVEYS\2012 PROJECTS\W0#1 ID1580-12-22 STH27 TPP MASTER 2\DRAWINGS\TPP SHEET 4.01 AMENDMENT 1.DWG PLOT DATE : May 28, 2012
APPRAISAL PLAT DATE : 06/04/2012

PLOT BY : KOLBY SCHERTZ

PLOT NAME : STH 27 & USH 8 PLOT SCALE : -----

1580-12-22 4.01 Amendment NO:1

TRANSPORTATION PROJECT PLAT NO: 1580-12-22 - 4.02

THAT PART OF THE NW 1/4 - NW 1/4 OF SECTION 3, T34N, R6W AND PART OF LOT 13 BLOCK 11 PLAT OF NEW FLAMBEAU ALSO BEING PART OF THE SW 1/4 OF THE SW 1/4 OF SECTION 34, T35N, R6W ALL IN THE CITY OF LADYSMITH, RUSK COUNTY, WISCONSIN

RELOCATION ORDER STH 27 CITY OF LADYSMITH, WEST NINTH STREET (MINER AVENUE TO FLAMBEAU AVENUE) RUSK COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3) AND 84.09, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:
1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

ACCEPTED FOR RECORDING AND FILING IN
THE OFFICE OF THE REGISTER OF DEEDS
IN RUSK COUNTY, WISCONSIN AT
10:00 AM ON MAY 2, 2012
AS DOCUMENT # 327170 AND
FILED IN VOL. 1 TFP 2, PAGE 2
Linda Ann Effert
SIGNATURE OF REGISTER OF DEEDS

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1580-12-22-4.02
AMENDMENT NO: _____

NOTES:

COORDINATES AND BEARINGS SHOWN ON THIS PLAT ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM, RUSK COUNTY ZONE, NAD83 (2007) ADJUSTMENT. THE COORDINATES SHOWN ARE GRID COORDINATES AND ARE TO BE USED AS GRID OR GROUND VALUES ON THIS PLAT.

RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 1" I.D. X 24" IRON PIPE) AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD".

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

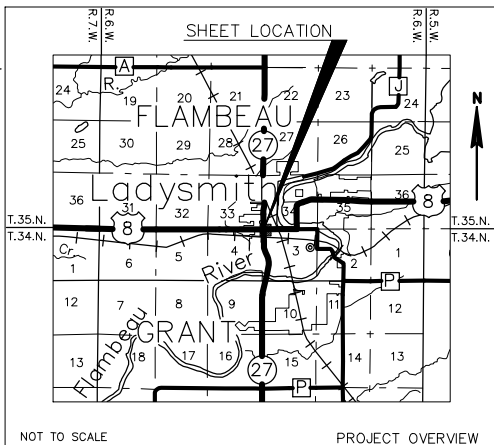
EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
EXISTING HIGHWAY RIGHT-OF-WAY FOR S.T.H. 27 & U.S.H. 8 ESTABLISHED FROM PREVIOUS PROJECT 1580-09-21

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LANDS TO THE DEPARTMENT OF TRANSPORTATION.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE. ALL TLES EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE. BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHT TO MAKE OR CONSTRUCT IMPROVEMENT ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.

R/W STATION AND OFFSET TABLE FOR U.S.H. 8			R/W STATION AND OFFSET TABLE FOR S.T.H. 27		
POINT #	STATION	OFFSET	POINT #	STATION	OFFSET
416	324+29.96	71.00'	400	630+63.99	6.37'
417	324+68.98	33.00'	413	634+28.66	5.03'
418	325+76.02	33.00'	415	634+28.62	33.00'
419	325+76.02	33.00'	416	633+66.68	33.00'
420	324+78.02	33.00'	417	633+28.65	72.00'
421	324+29.05	82.00'	420	632+64.62	81.78'
TLE462	324+34.96	66.13'	421	632+14.48	33.98'
TLE463	324+63.84	38.00'	422	630+63.99	37.62'
TLE464	325+29.98	38.00'	TLE460	634+78.65	33.00'
TLE465	325+29.98	33.00'	TLE461	634+78.65	38.00'
			TLE462	633+61.80	38.00'
			TLE463	633+33.66	66.87'



ALIGNMENT DATA
U.S.H. 8
POB STA. 316+97.00
Y = 563528.78
X = 809895.43
PI STA. 323+97.00
Y = 563541.13
X = 810595.32
PI STA. 330+97.00
Y = 563552.22
X = 811295.23

ALIGNMENT DATA
S.T.H. 27
POB STA. 628+52.01
Y = 563097.45
X = 810591.89
PI STA. 632+95.70
Y = 563541.13
X = 810595.32
PI STA. 635+95.70
Y = 563841.09
X = 810590.38

SECTION COURSE TABLE		
COURSE	BEARING	DISTANCE
124-125	N 88°59'20" E	2655.20'
125-123	N 02°06'34" E	2661.51'
125-126	N 89°05'31" E	2638.62'
126-418	N 89°05'31" E	2459.28'
125-200	S 01°35'04" W	2540.28'

R/W COURSE TABLE		
COURSE	BEARING	DISTANCE
400-125	N 01°35'04" E	264.75'
125-413	N 02°06'34" E	100.10'
413-415	N 89°07'49" E	27.97'
415-416	S 00°56'37" E	61.94'
416-417	S 46°40'10" E	54.47'
417-418	N 89°05'31" E	107.04'
418-419	S 00°54'29" E	66.00'
419-420	S 89°05'31" W	98.00'
420-421	S 44°04'27" W	69.28'
421-422	S 00°56'37" E	150.53'
422-400	S 89°33'25" W	43.99'

CONVENTIONAL SYMBOLS

FOUND IRON PIPE/PIN (1" UNLESS NOTED)
R/W MONUMENT
R/W STANDARD
SIGN
SECTION CORNER MONUMENT
SECTION CORNER SYMBOL
FEE (HATCH VARIES)
TEMPORARY LIMITED EASEMENT
PERMANENT LIMITED EASEMENT
R/W BOUNDARY POINT
PARCEL NUMBER
UTILITY INTEREST
SIGN NUMBER (OFF PREMISE)
BUILDING

PROPOSED R/W LINE
EXISTING H.E. LINE
PROPERTY LINE
LOT & TIE LINES
SLOPE INTERCEPTS
CORPORATE LIMITS
NO ACCESS (BY PREVIOUS ACQUISITION/CONTROL)
ACCESS RESTRICTED (BY ACQUISITION)
NO ACCESS (BY STATUTORY AUTHORITY)
SECTION LINE
QUARTER LINE
SIXTEENTH LINE
EXISTING CENTERLINE
PROPOSED REFERENCE LINE
PARALLEL OFFSET

ACCESS POINT/ DRIVEWAY CONNECTION
ACCESS RIGHTS
ACRES
AND OTHERS
CENTERLINE
CERTIFIED SURVEY MAP
CORNER
DOCUMENT
EASEMENT
HIGHWAY EASEMENT
LAND CONTRACT
MONUMENT
PAGE
PERMANENT LIMITED EASEMENT
PROPERTY LINE
RECORDED AS REFERENCE LINE

CONVENTIONAL ABBREVIATIONS

RELEASE OF RIGHTS
REMAINING
RIGHT-OF-WAY
SECTION
STATION
TEMPORARY LIMITED EASEMENT
VOLUME
LONG CHORD
LONG CHORD BEARING
RADIUS
DEGREE OF CURVE
CENTRAL ANGLE OR DELTA
LENGTH OF CURVE
TANGENT

AP
AR
AC.
ET.AL.
C/L
CSM
COR.
DOC.
EASE.
H.E.
LC
MON.
P.
PLE
PL
R/L

CURVE DATA

CONVENTIONAL UTILITY SYMBOLS

WATER
GAS
TELEPHONE
OVERHEAD
TRANSMISSION LINES
ELECTRIC
CABLE TELEVISION
FIBER OPTIC
SANITARY SEWER
STORM SEWER
NON COMPENSABLE
POWER POLE
TELEPHONE POLE
TELEPHONE PEDESTAL
ELECTRIC TOWER

W
T
T
OH
E
TV
FO
SAN
SS
LCH
LCB
R
D
DELTA
L
TAN

UTILITY EASEMENT INFORMATION		
PARCEL #	COMPANY NAME	RECORDING DATA
3	NO EASEMENT INFORMATION FOUND	-----

SCHEDULE OF LANDS AND INTERESTS REQUIRED		OWNER NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT		R/W S.F. REQUIRED			TLE S.F. REQUIRED	PLE S.F. REQUIRED
PARCEL NUMBER	OWNERS	INTEREST REQUIRED		NEW	EXISTING	TOTAL		
3	VICKI RICHARDSON	PLE, TLE		----	----	----	890	629

QUEST
Civil Engineers, LLC

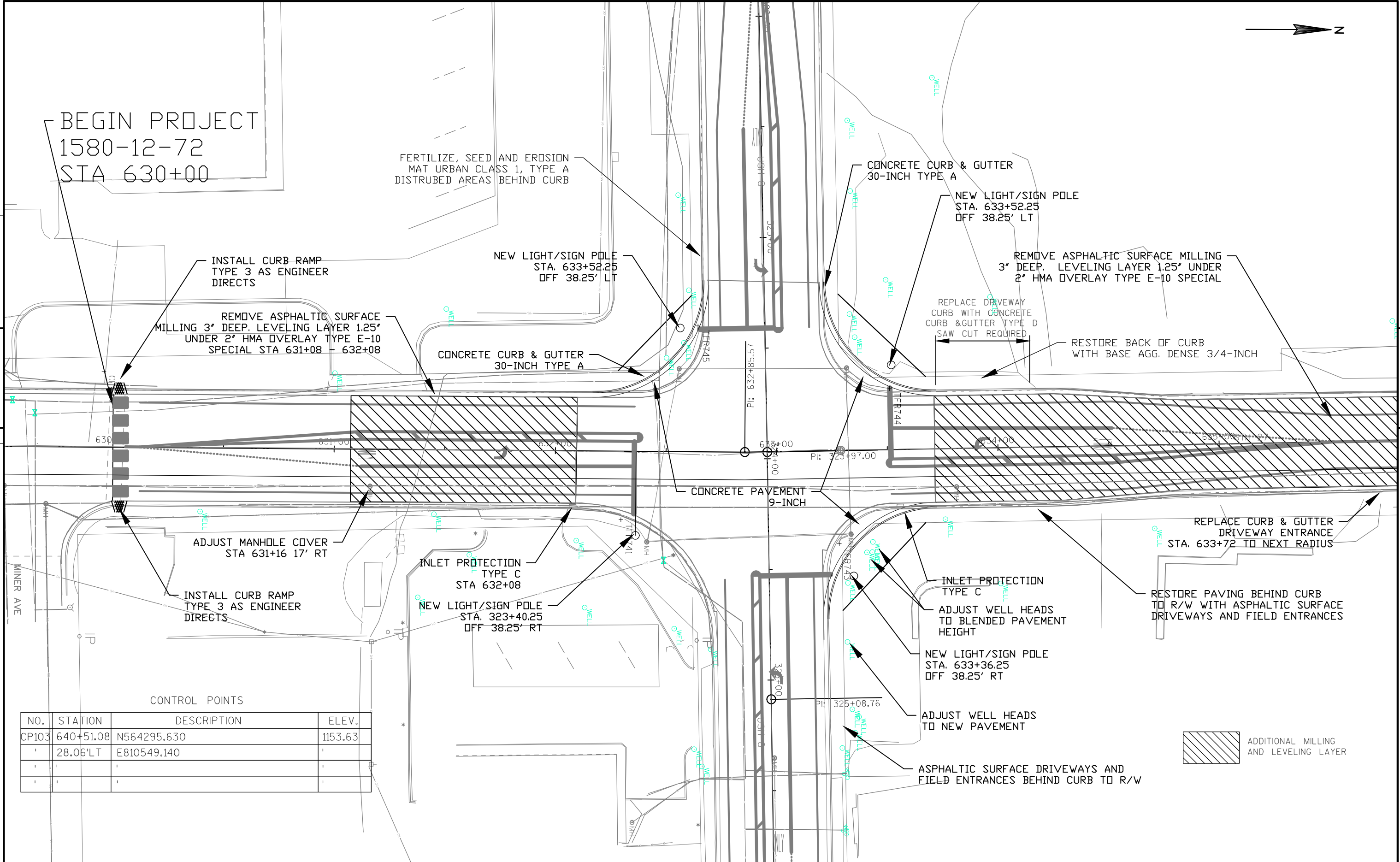
Design It.
Build It.
Live It.

2811 8TH STREET SOUTH, SUITE 8
WISCONSIN RAPIDS, WI 54494
715-423-3525

I HEREBY CERTIFY THAT THIS PLAT MEETS ALL REQUIREMENTS OF SECTION 84.095, WISCONSIN STATUTES. THIS PLAT WAS PREPARED BY OR UNDER THE DIRECTION OF
KOLBY SCHERTZ
PRINTED NAME
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION
MICHAEL PILLER
PRINTED NAME
DATE
4/26/2012
DATE
4/30/2012
DATE

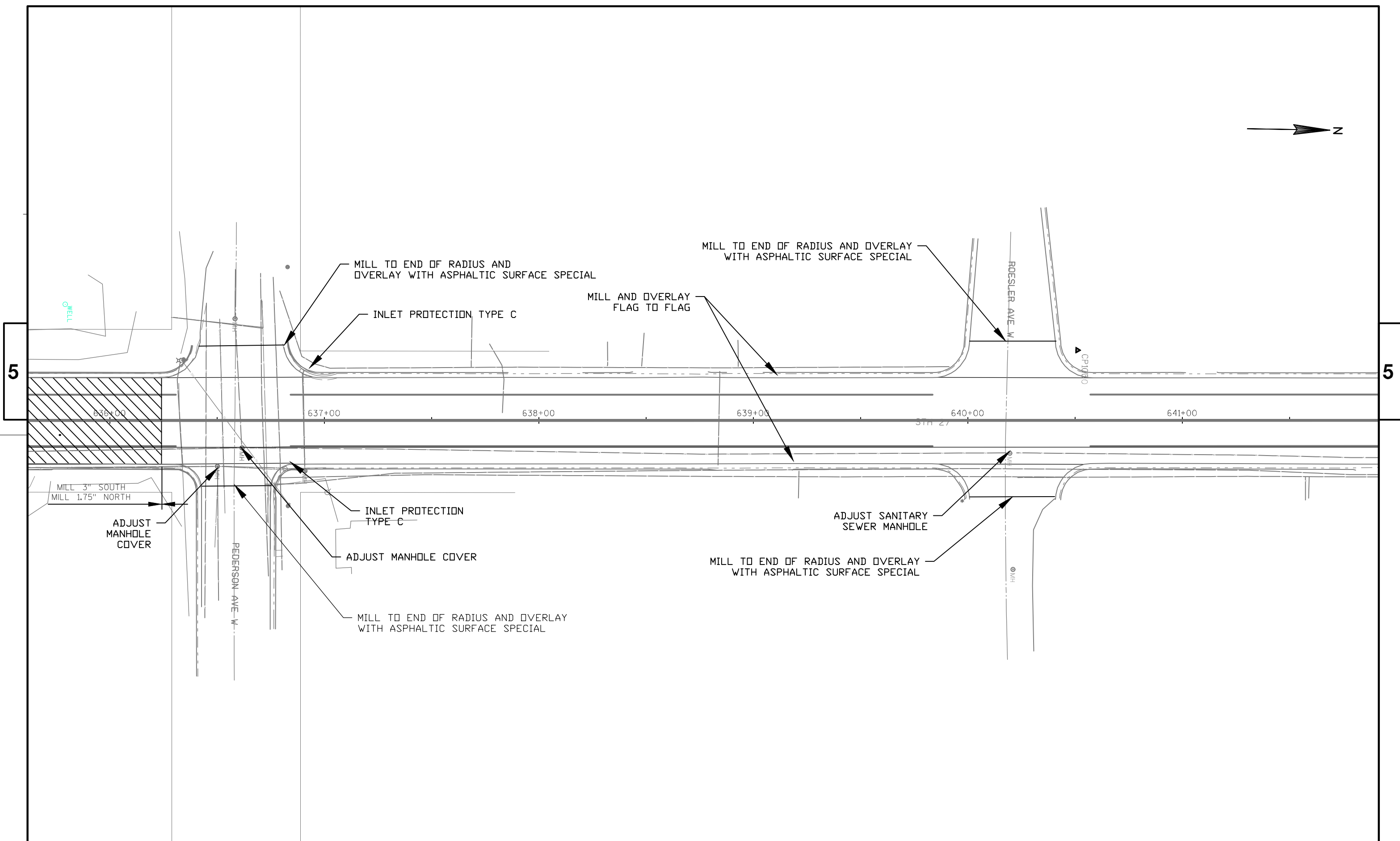
R/W COORDINATE TABLE		
Point #	Y	X
400	563309.47	810587.16
413	563674.15	810598.16
415	563674.57	810626.13
416	563612.64	810627.15
417	563575.26	810666.77
418	563576.96	810773.80
419	563510.97	810774.84
420	563509.41	810676.85
421	563459.64	810628.67
422	563309.13	810631.15
TLE460	563724.59	810625.30
TLE461	563724.68	810630.30
TLE462	563607.85	810632.23
TLE463	563580.18	810661.55
TLE464	563581.23	810727.68
TLE465	563576.23	810727.76

EXISTING MONUMENTS			
Point #	Y	X	DESCRIPTION
108	563510.82	810772.66	3/4" IRON ROD
109	563509.02	810680.37	3/4" IRON ROD

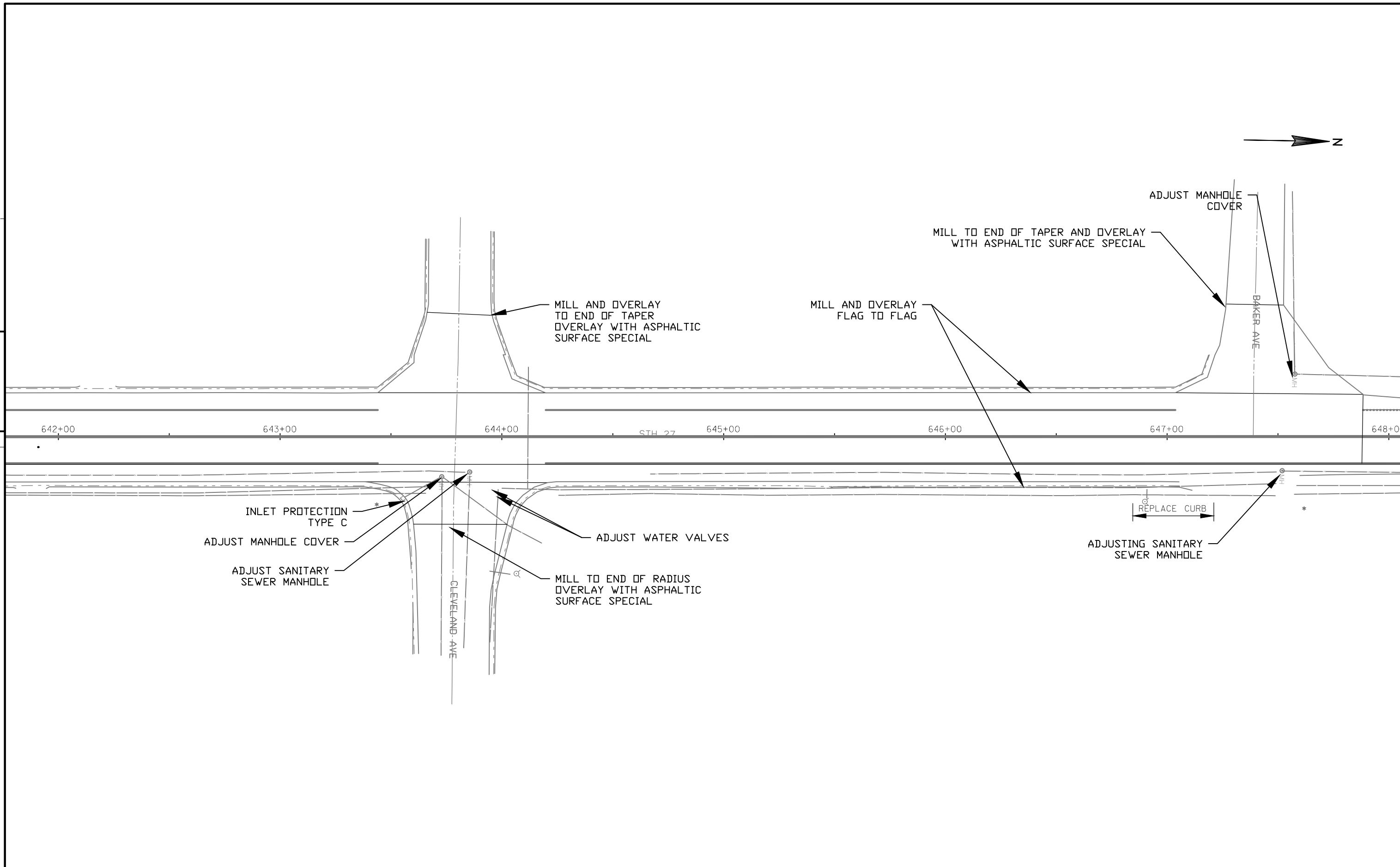


CONTROL POINTS

NO.	STATION	DESCRIPTION	ELEV.
CP103	640+51.08	N564295.630	1153.63
	28.06'LT	E810549.140	



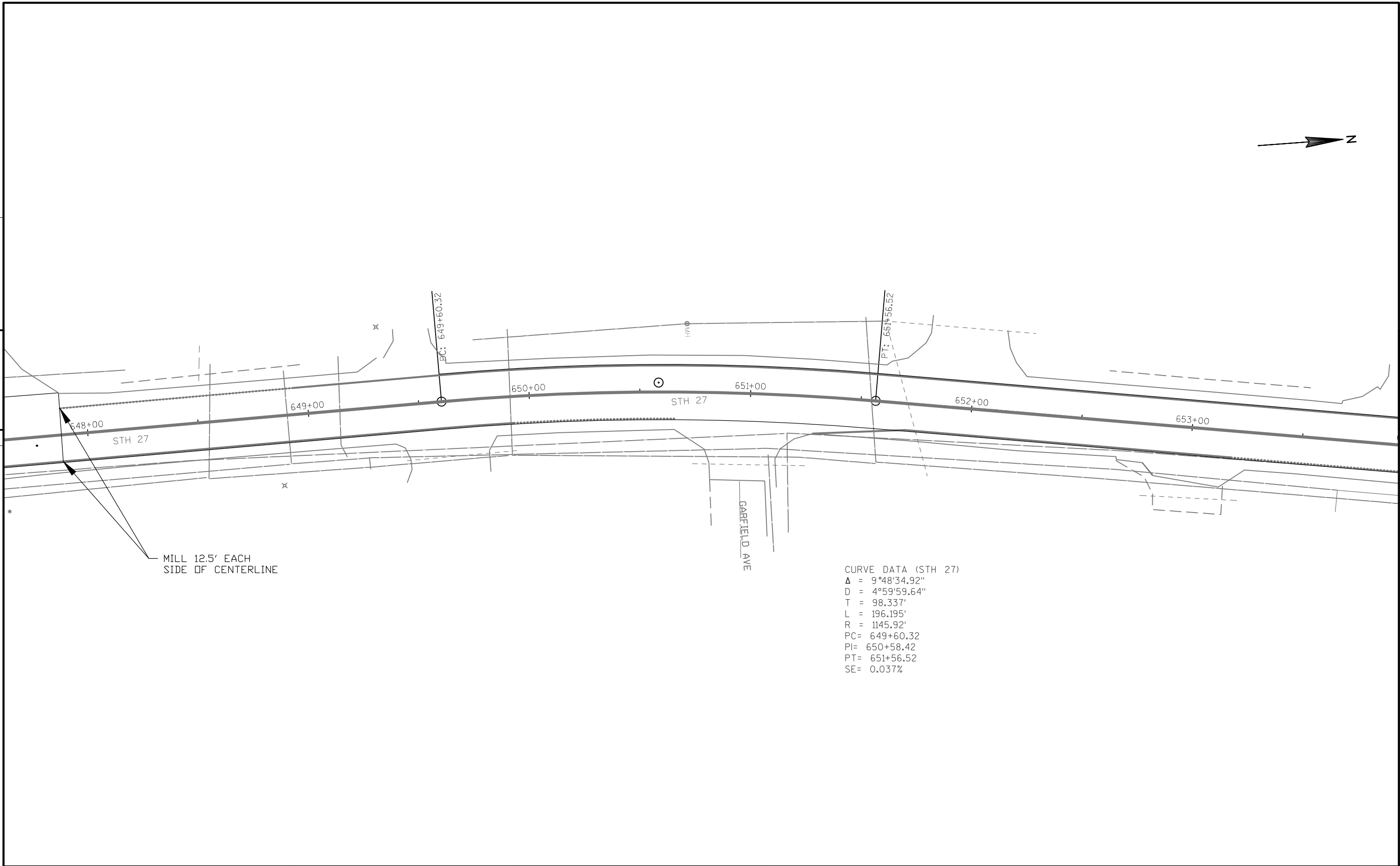
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5

PROJECT NO: 1580-12-72	HWY: STH 27	COUNTY: RUSK	PLAN STH 27	SHEET	E
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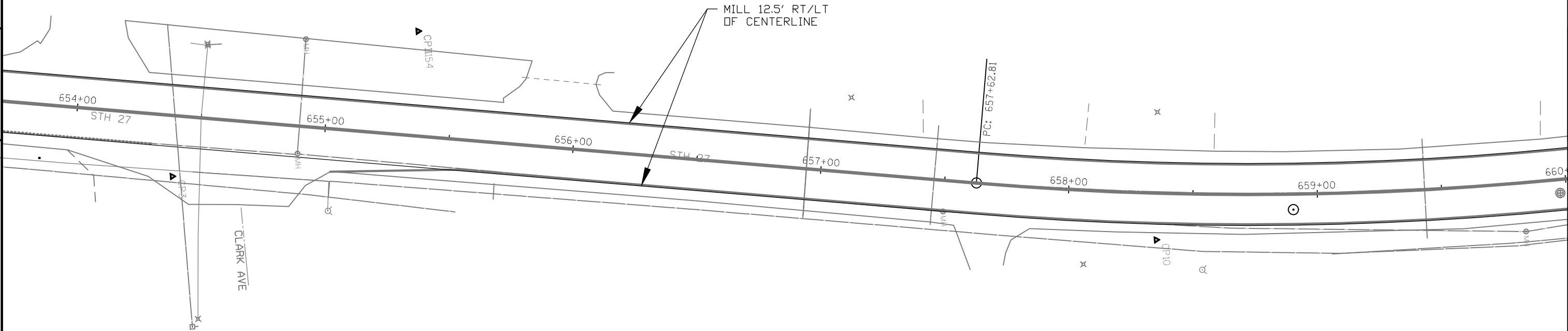
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5

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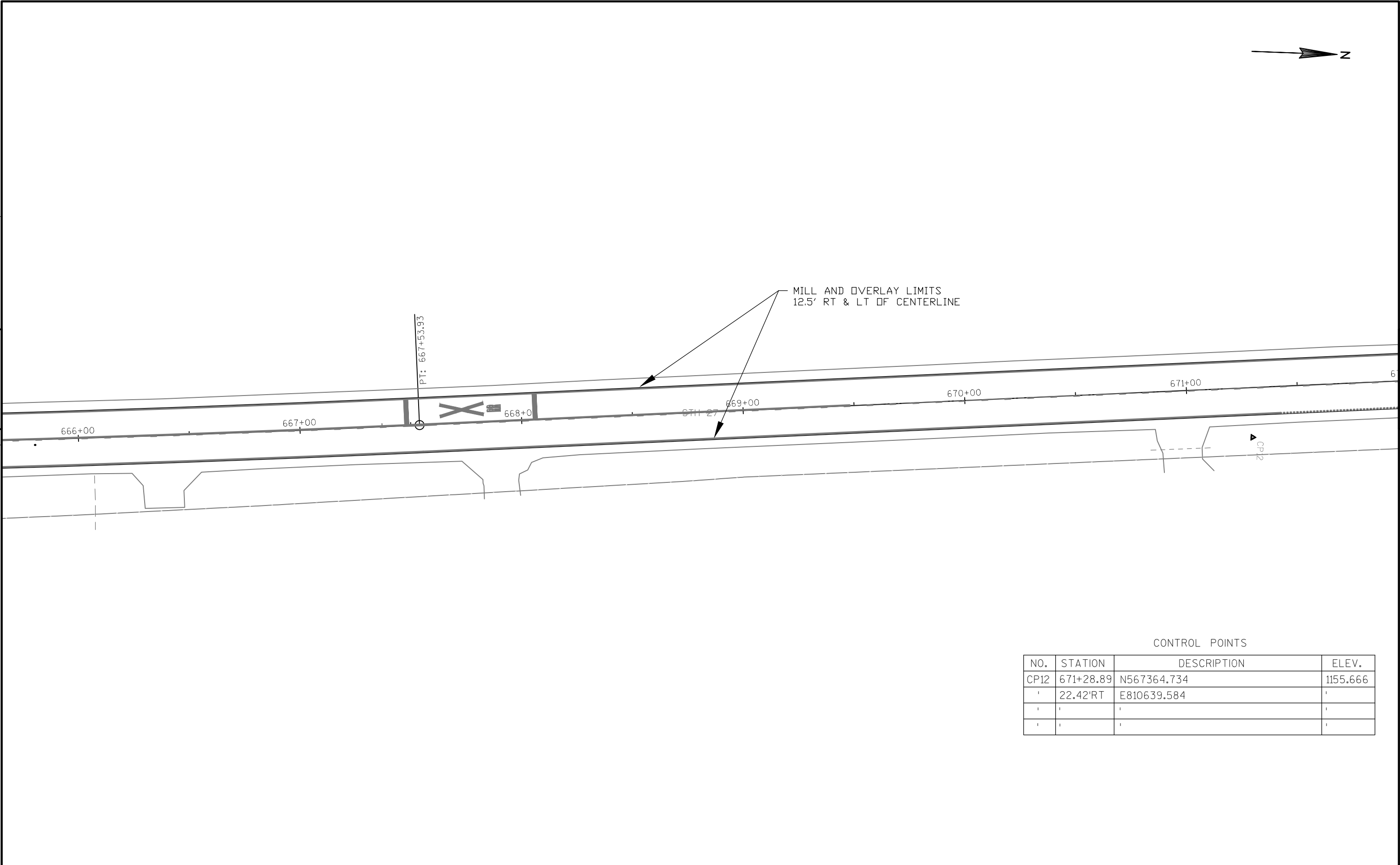


CURVE DATA (STH 27)
Δ = 11°11'55.3"
D = 4°23'30.46"
T = 127.903'
L = 254.992'
R = 1304.61'
PC= 657+62.8
PI= 658+90.3
PT= 660+17.8
SE= 0.034%

CONTROL POINTS

NO.	STATION	DESCRIPTION	ELEV.
CP11	655+33.9	N565780.235	1149.149
'	42.026'LT	E810595.520	'
CP10	658+35.75	N566070.490-----PD:CHEVY	1155.019
'	18.79'RT	E810699.932	'
CP2	663+90	N566624.916-----PD:MAG-HUB	1158.115
'	22.93'LT	E810647.635	'

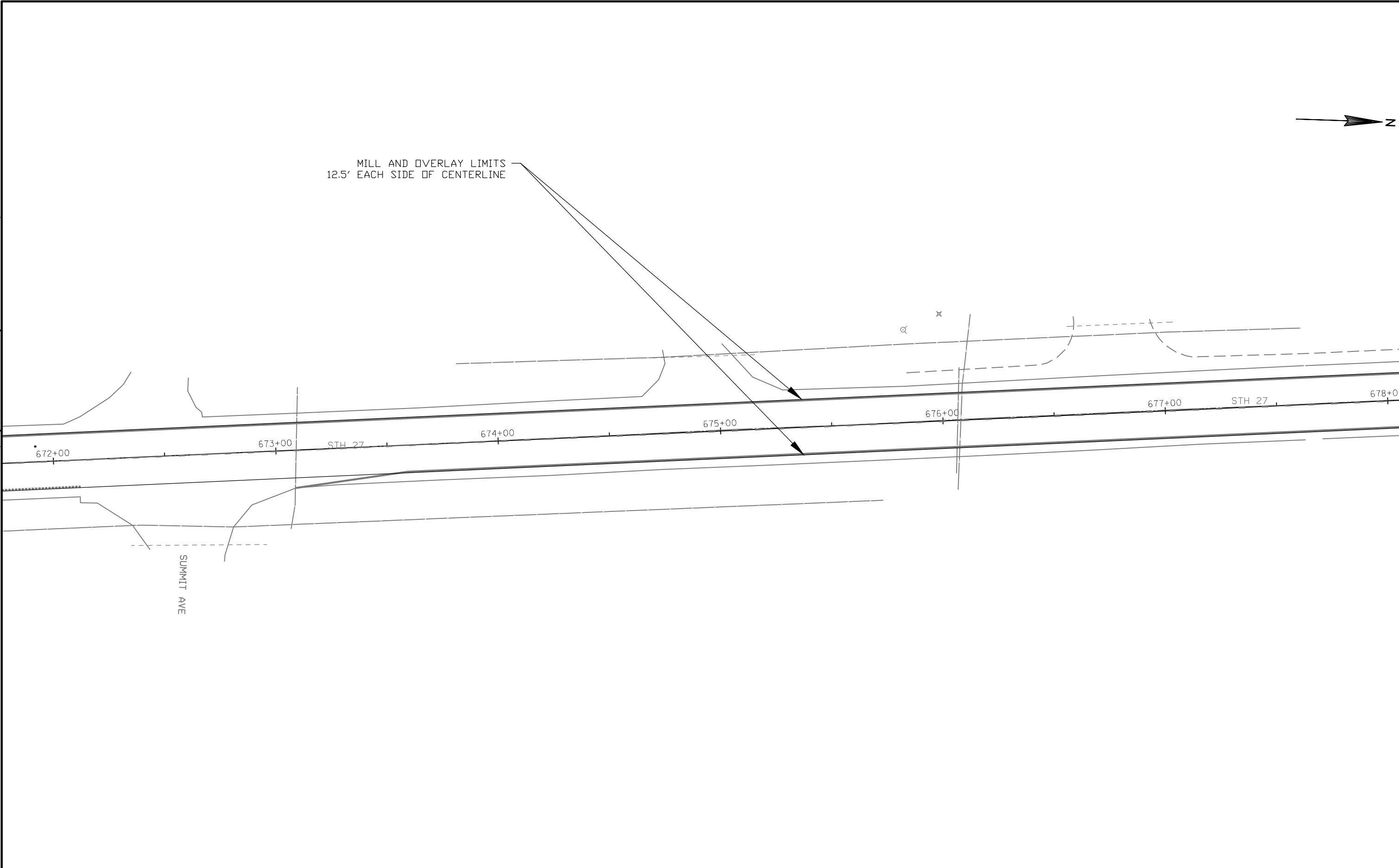
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5

CONTROL POINTS			
NO.	STATION	DESCRIPTION	ELEV.
CP12	671+28.89	N567364.734	1155.666
'	22.42'RT	E810639.584	'
'	'	'	'
'	'	'	'

5



5

PROJECT NO: 1580-12-72	HWY: STH 27	COUNTY: RUSK	PLAN STH 27	SHEET	E
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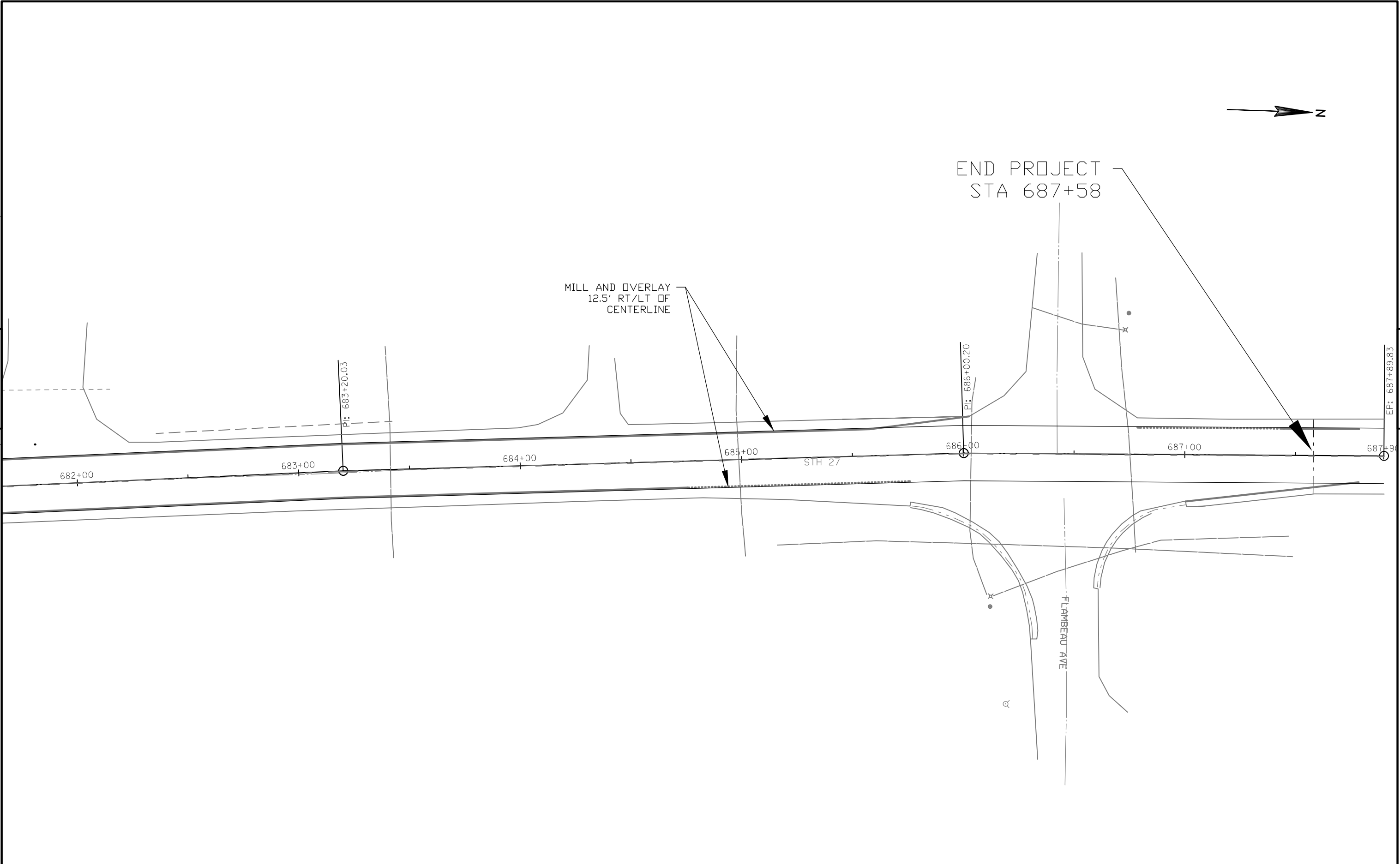
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5

PROJECT NO: 1580-12-72	HWY: STH 27	COUNTY: RUSK	PLAN STH 27	SHEET	E
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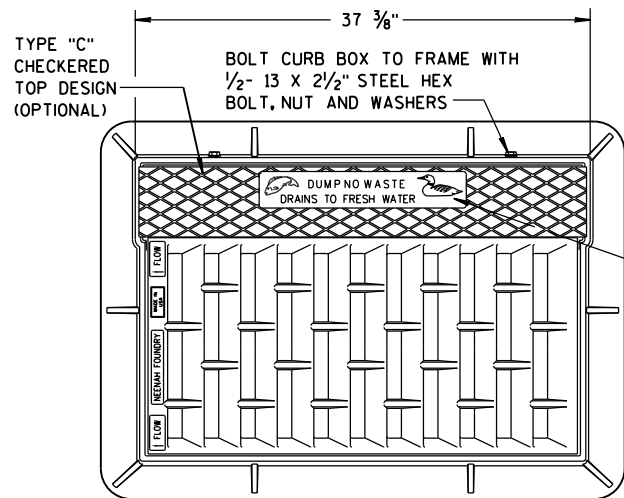


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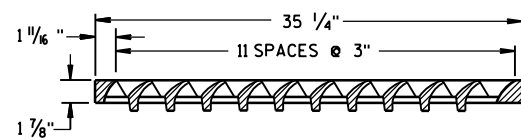
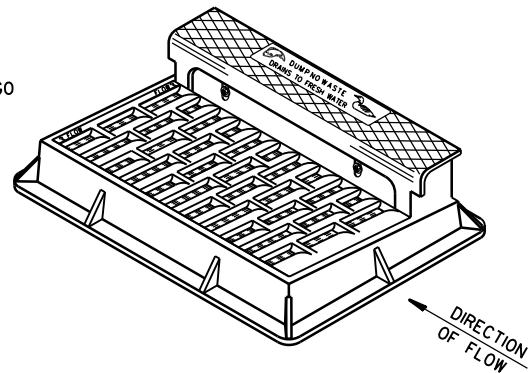
PROJECT NO: 1580-12-72	HWY: STH 27	COUNTY: RUSK	PLAN STH 27	SHEET	E
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Standard Detail Drawing List

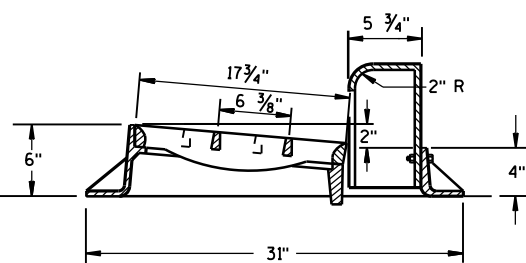
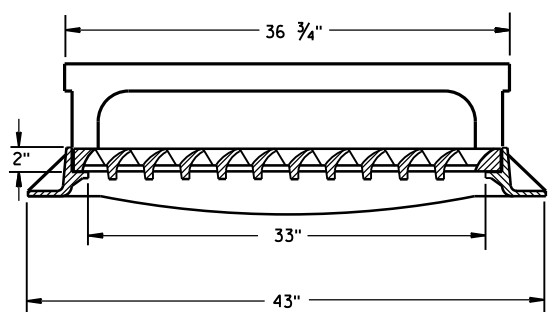
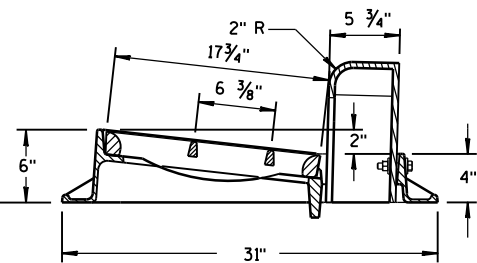
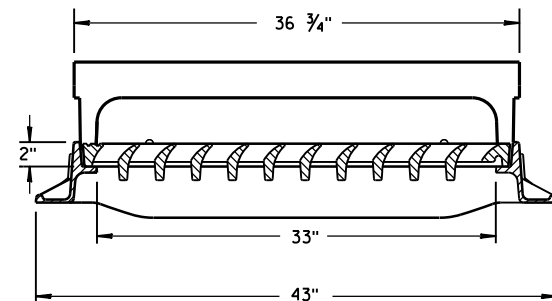
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08C06-01	INLETS 3-FT AND 4-FT DIAMETER
08D01-17	CONCRETE CURB, CONCRETE CURB AND GUTTER AND TIES
08D04-05	CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES
08D05-15A	CURB RAMPS TYPES 1 AND 1-A
08D05-15B	CURB RAMPS TYPES 2 AND 3
08D05-15C	CURB RAMPS TYPES 4A AND 4A1
08D05-15D	CURB RAMPS TYPE 4B AND 4B1
08D05-15E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08F04-07	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
09B02-07	CONDUIT
09B04-10	PULL BOX
09D01-04	CABINET SERVICE INSTALLATION (METER BREAKER PEDESTAL)
09D02-02	SIGNAL OR LIGHTING CONTROL CABINET
10A03-03	CIRCUIT IDENTIFICATION PLAQUES SIGN BRIDGES
13B01-10	PAVEMENT DETAILS FOR RAILROAD APPROACH
13C01-16	CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES
13C09-11A	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
13C09-11B	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
13C09-11C	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
13C11-11A	RURAL DOWELED CONCRETE PAVEMENT
13C11-11B	RURAL DOWELED CONCRETE PAVEMENT
13C13-08	URBAN DOWELED CONCRETE PAVEMENT
15C04-02	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-02	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C07-12B	PAVEMENT MARKING WORDS
15C07-12C	PAVEMENT MARKING ARROWS
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C08-16B	PAVEMENT MARKING (INTERSECTIONS)
15C08-16E	PAVEMENT MARKING (LEFT TURN LANE)
15C08-16F	PAVEMENT MARKING (ISLANDS)
15C09-09A	SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD-HIGHWAY GRADE CROSSINGS
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C18-03	MEDIAN ISLAND MARKING
15C19-02A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C25-02	42" DIAMETER CANTILEVER OVERHEAD SIGN SUPPORT BASE
15C33-01	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D20-02	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY



**NOTE:
GRATE IS REVERSIBLE.**

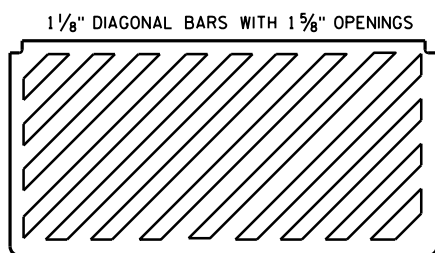


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"



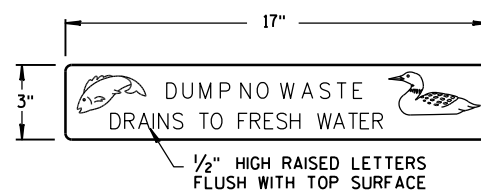
TYPE "H"

NOTE: EITHER CASTING IS ACCEPTABLE

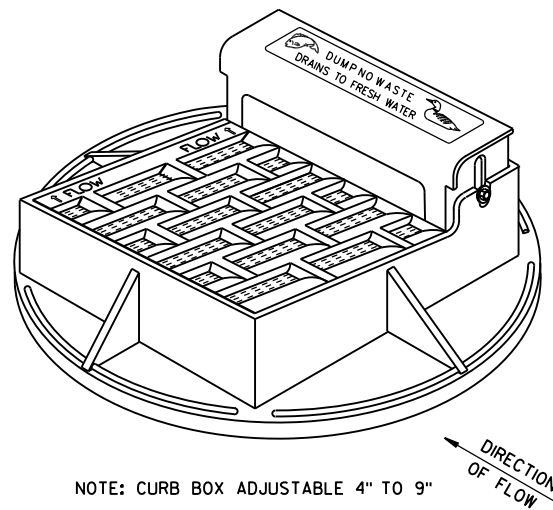


**SPECIAL GRATE FOR
TYPE "H" COVER**

(MEASURES 35 1/4" X 17 3/4" X 2")
(NOTED AS TYPE H-S ON DRAINAGE TABLE)

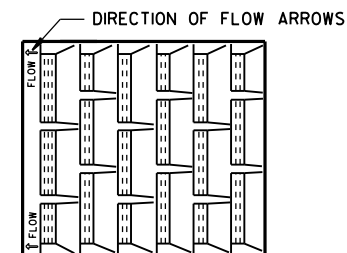


LOGO DETAIL

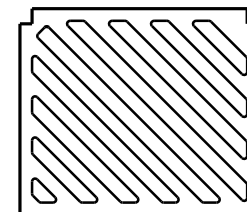


NOTE: CURB BOX ADJUSTABLE 4" TO 9"

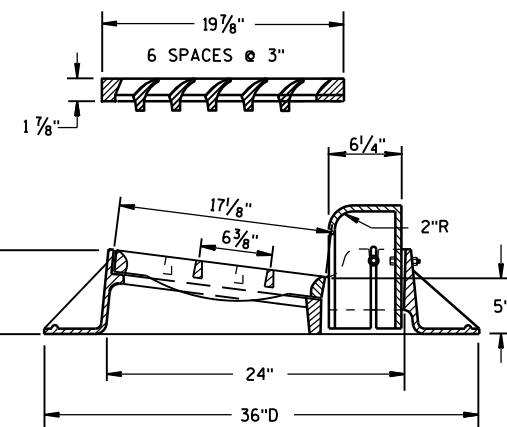
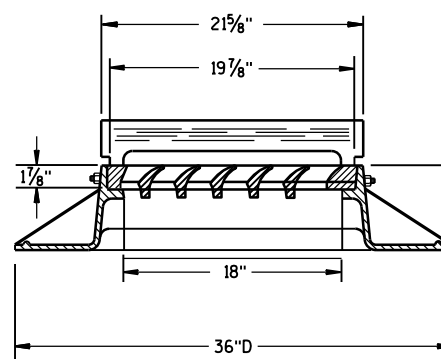
**NOTE:
GRATE IS REVERSIBLE.**



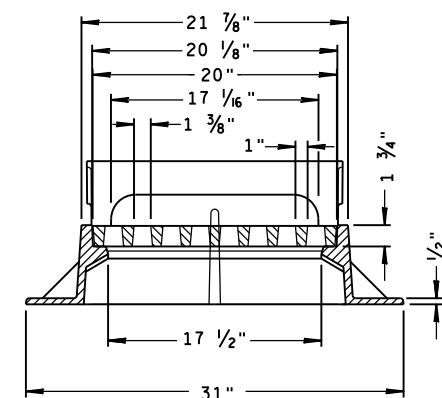
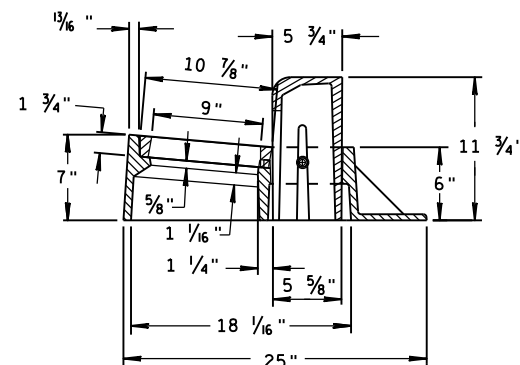
1" DIAGONAL BARS
WITH 1 1/2" OPENINGS



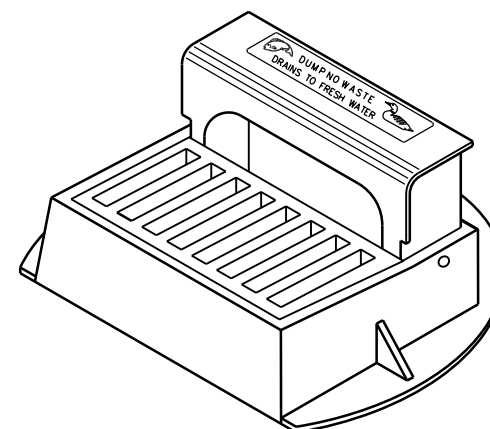
**SPECIAL GRATE FOR
TYPE "A" COVER**
(MEASURES 19 3/4" X 17" X 1 1/8")
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"



TYPE "Z"

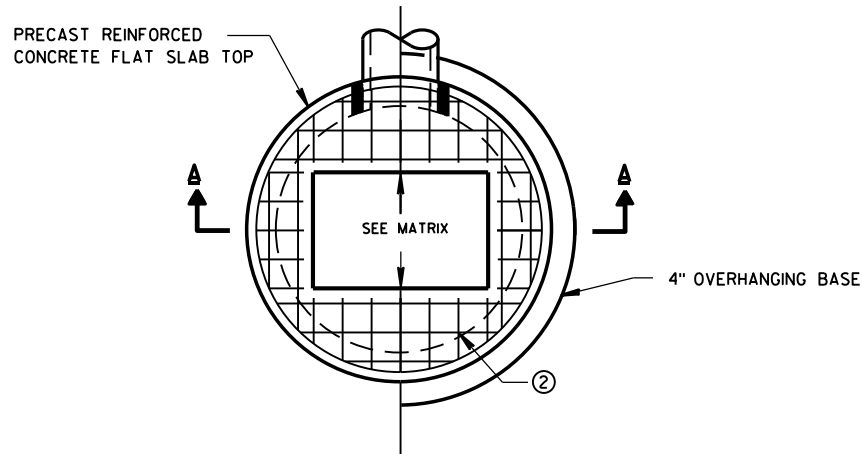


**INLET COVERS
TYPE A, H, A-S, H-S & Z**

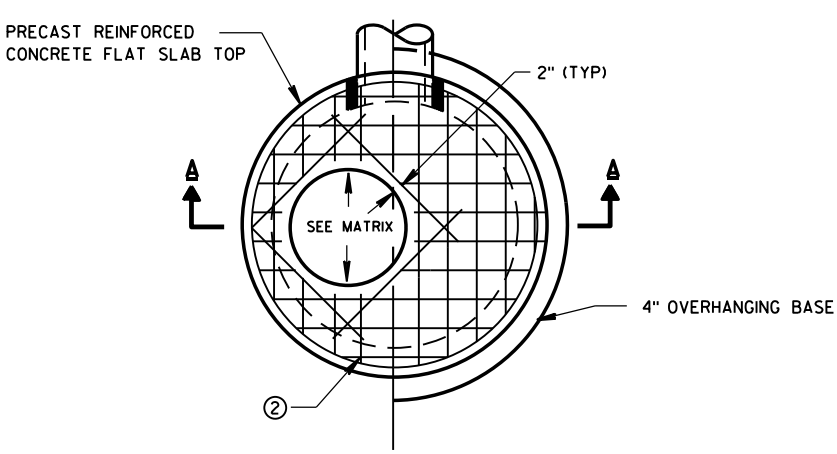
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
II-27-13
DATE
FHWA

/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

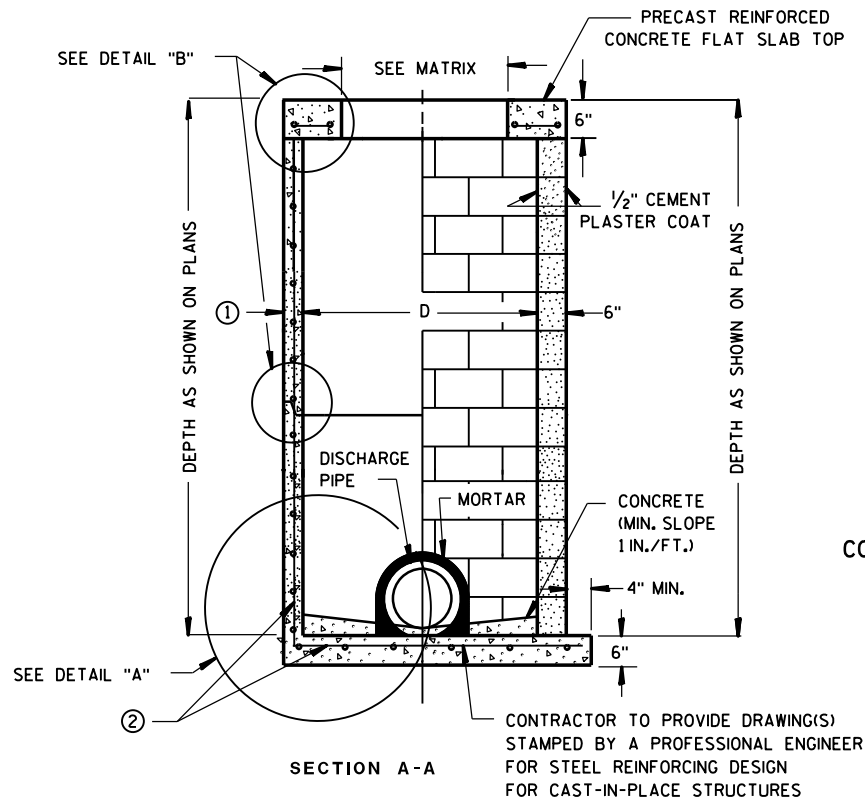


PLAN VIEW RECTANGULAR OPENING



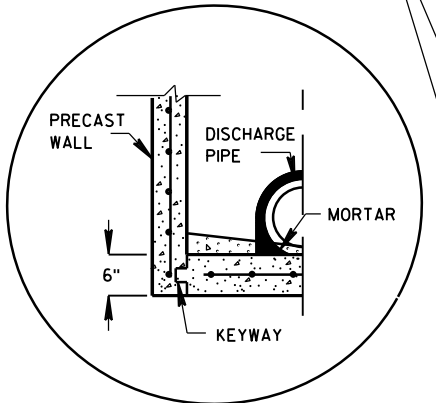
PLAN VIEW CIRCULAR OPENING

JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C990 (TYP)

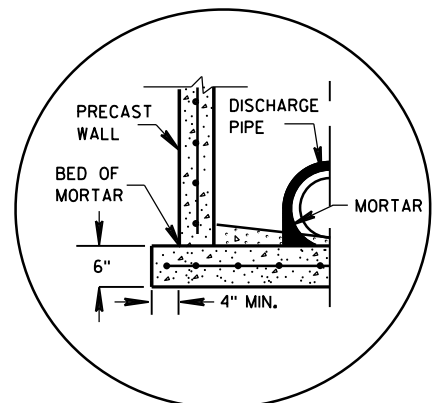


PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE **CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②**

CIRCULAR INLETS W/ FLAT TOP

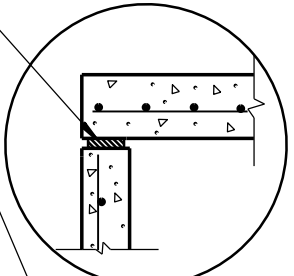


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

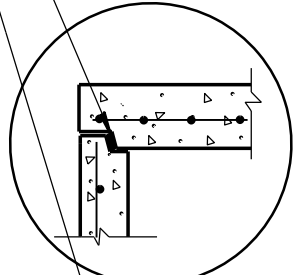


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

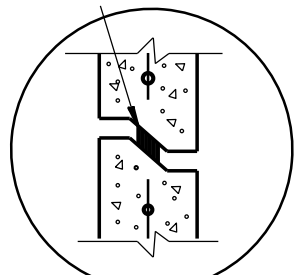
DETAIL "A"



TOP WITH PLAIN END JOINT



TOP WITH TONGUE AND GROOVE JOINT



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

INLETS 3-FT AND 4-FT DIAMETER

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

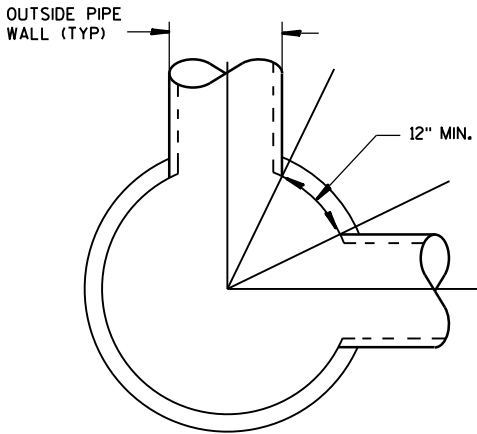
4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

- ① MINIMUM WALL THICKNESS SHALL BE 4-IN FOR 3-FT DIAMETER AND 5-IN FOR 4-FT DIAMETER PRECAST INLETS.
- ② FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

INLET COVER OPENING MATRIX

	INLET COVER TYPE	ALL A'S	ALL B'S	BW	C	F	ALL H'S	S	T	V	WM	Z
INLET SIZE	OPENING SIZE (FT)											
3-FT	2 DIA.				X							X
	2X2	X	X					X		X		
4-FT	2 DIA.				X							X
	2X2	X	X					X	X	X	X	
	2X2.5			X								
	2X3						X					
	2.5X3					X						



DETAIL "C"

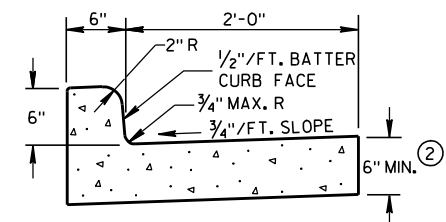
PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18

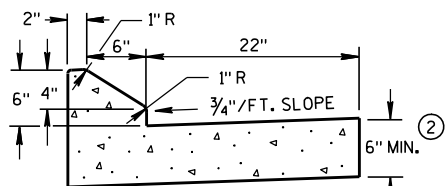
INLETS 3-FT AND 4-FT DIAMETER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

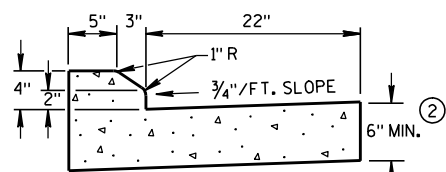
APPROVED
6/5/2012 DATE /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA



TYPES A & D ①

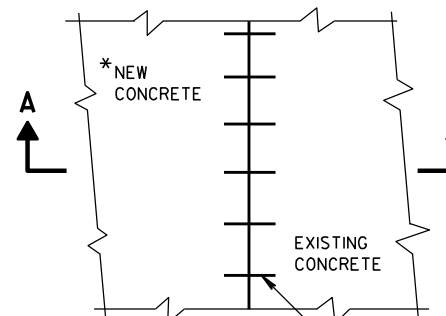


6" SLOPED CURB TYPES G & J ①



4" SLOPED CURB TYPES G & J ①

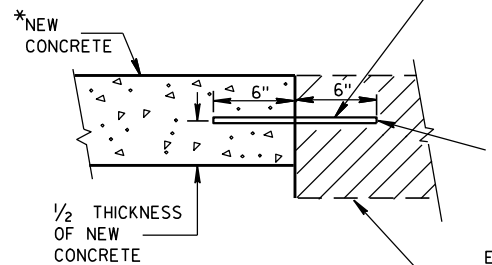
CONCRETE CURB & GUTTER 30"



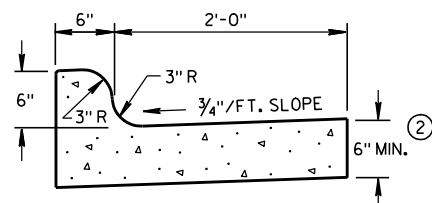
PLAN VIEW

* NEW CURB & GUTTER,
SURFACE DRAINS,
CONCRETE PAVEMENT
OR OTHER NEW CONCRETE.

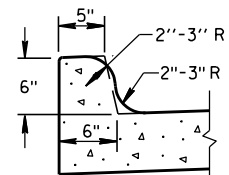
NO. 6 TIE BARS SPACED 2'-6" C-C,
INSTALLED PERPENDICULAR
TO THE LONGITUDINAL JOINT.



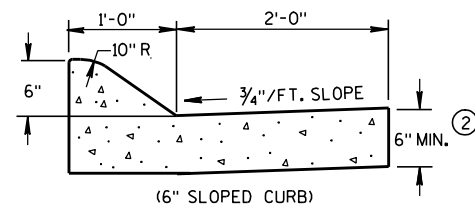
SECTION A-A
TIE BARS DRILLED
INTO EXISTING PAVEMENT



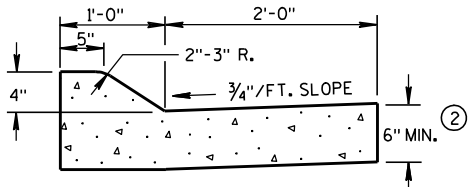
TYPES K & L ①



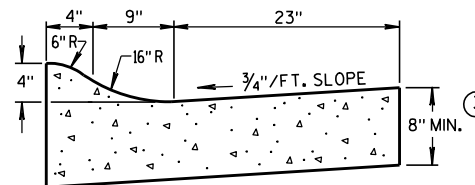
OPTIONAL CURB SHAPE
FOR TYPES K & L ①



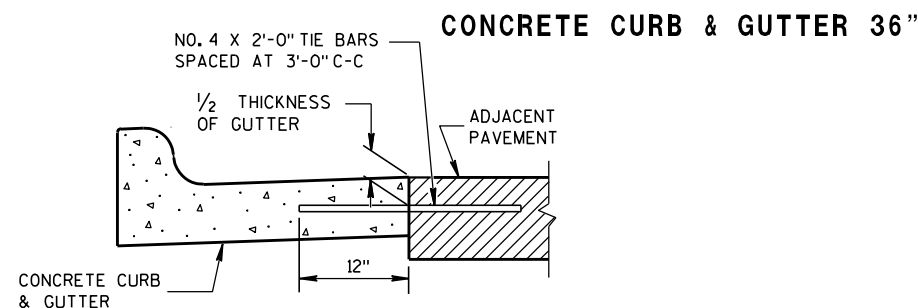
(6" SLOPED CURB)



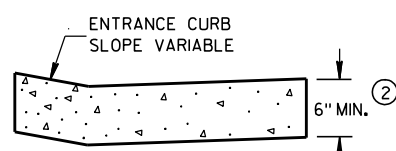
TYPES A & D ①



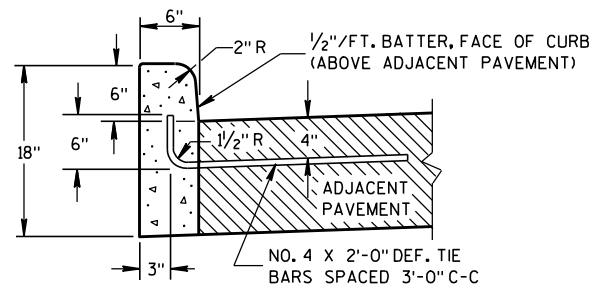
4" SLOPED CURB TYPES R & T ① ④



TYPICAL TIE BAR LOCATION ①

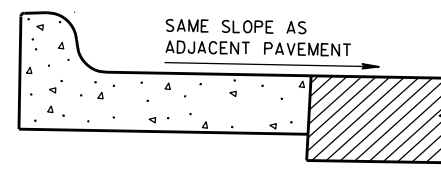


DRIVEWAY ENTRANCE CURB
(WHEN DIRECTED BY THE ENGINEER)

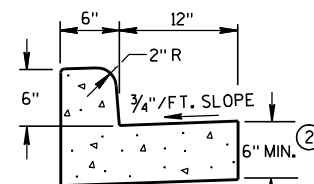


TYPES A & D ①

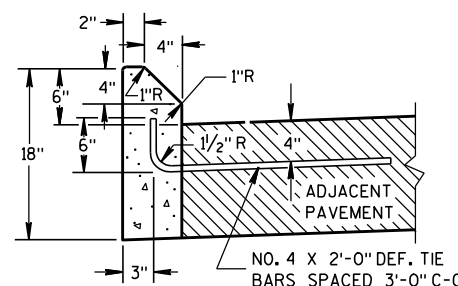
CONCRETE CURB



REVERSE SLOPE GUTTER ⑤
(TYPICAL FOR ALL CURB & GUTTER TYPES)



TYPES A & D
CONCRETE CURB & GUTTER 18"



TYPES G & J ①

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

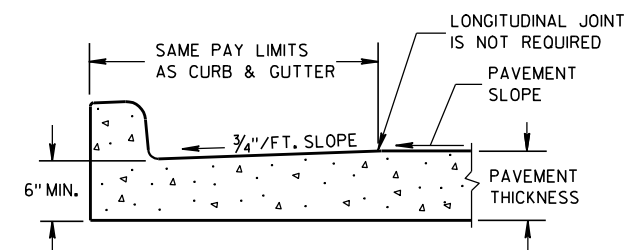
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE. A LONGITUDINAL CONSTRUCTION JOINT IS NOT REQUIRED WITH INTEGRAL CURB AND GUTTER.

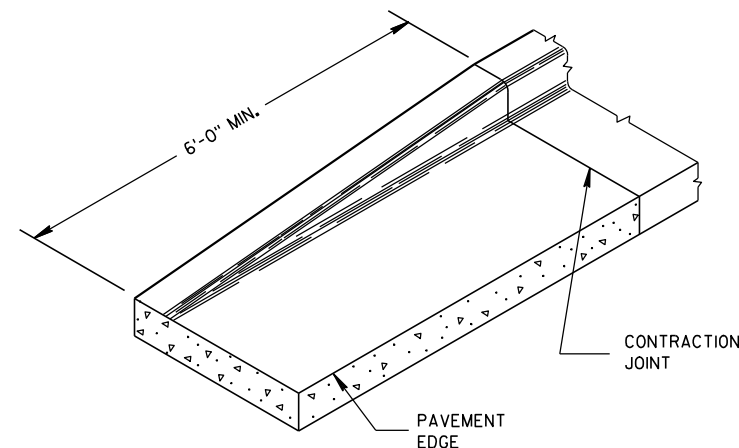
WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K AND R.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ④ THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑤ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.



PARTIAL SECTION OF PAVEMENT
WITH INTEGRAL CURB & GUTTER



END SECTION CURB & GUTTER

CONCRETE CURB, CONCRETE
CURB & GUTTER AND TIES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

9/4/08

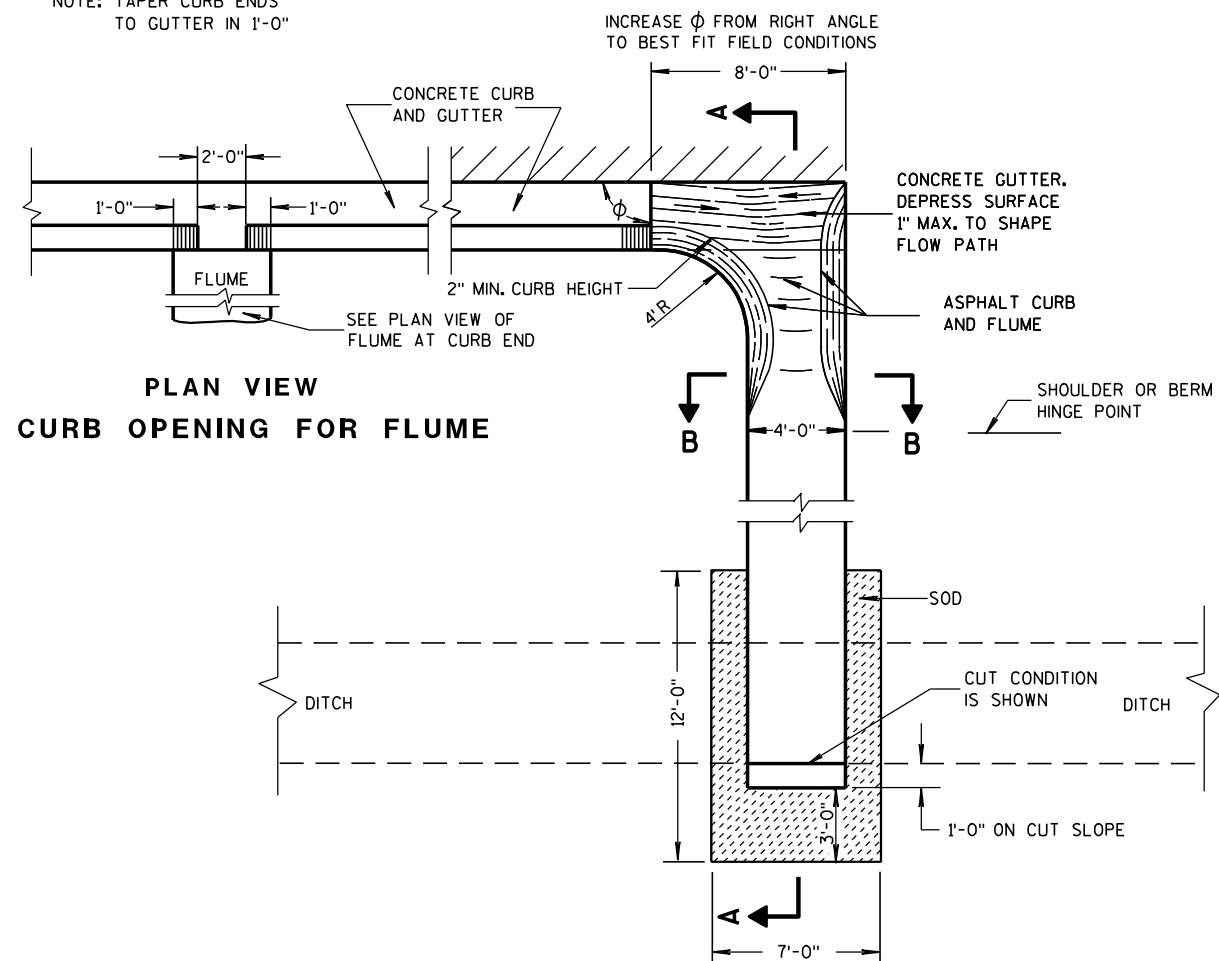
DATE

FHWA

/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

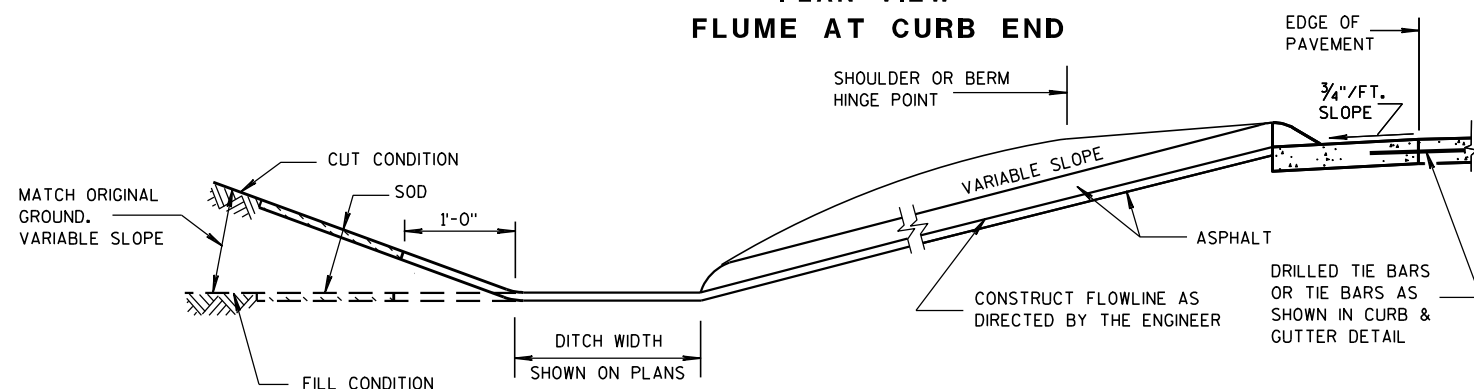
ASPHALTIC FLUME

NOTE: TAPER CURB ENDS
TO GUTTER IN 1'-0"

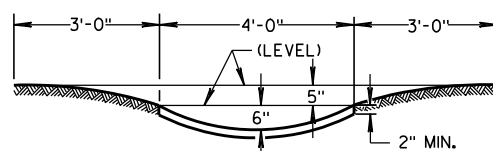


PLAN VIEW
CURB OPENING FOR FLUME

PLAN VIEW
FLUME AT CURB END



SECTION A-A



SECTION B-B

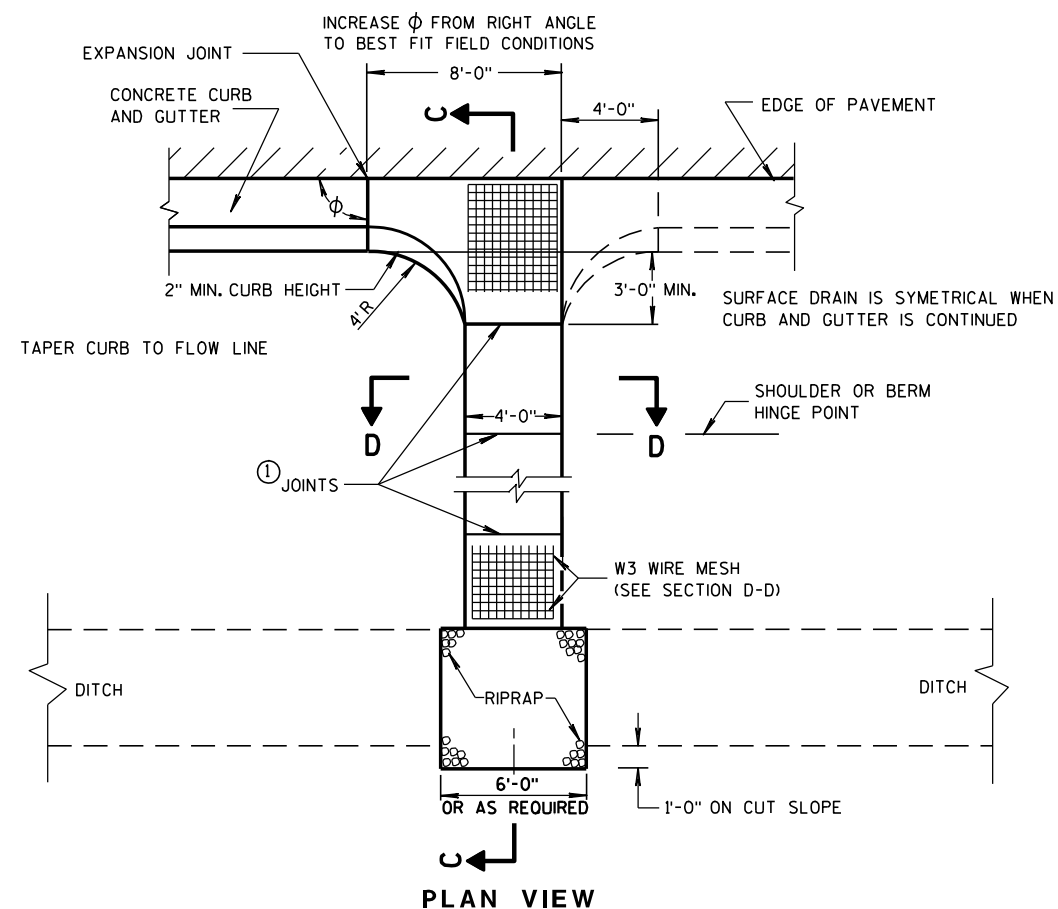
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

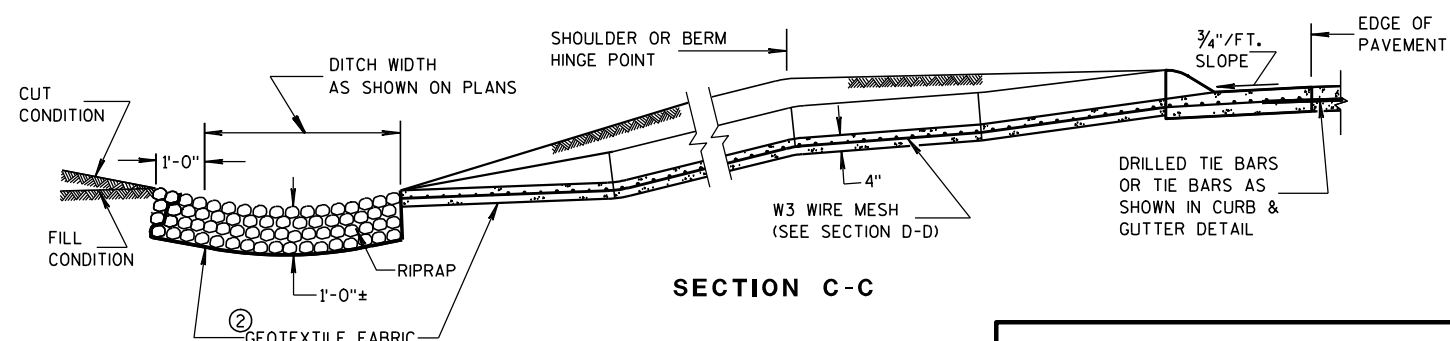
WELDED STEEL WIRE FABRIC SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.

- ① JOINTS SHALL BE $\frac{1}{8}$ TO $\frac{1}{4}$ INCH WIDE BY $1\frac{1}{2}$ INCHES DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.
- ② GEOTEXTILE FABRIC TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.
- ③ CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED

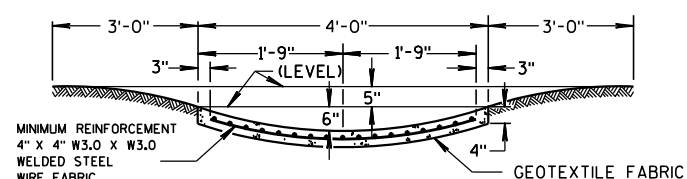
③ CONCRETE SURFACE DRAIN



PLAN VIEW



SECTION C-C



SECTION D-D

CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

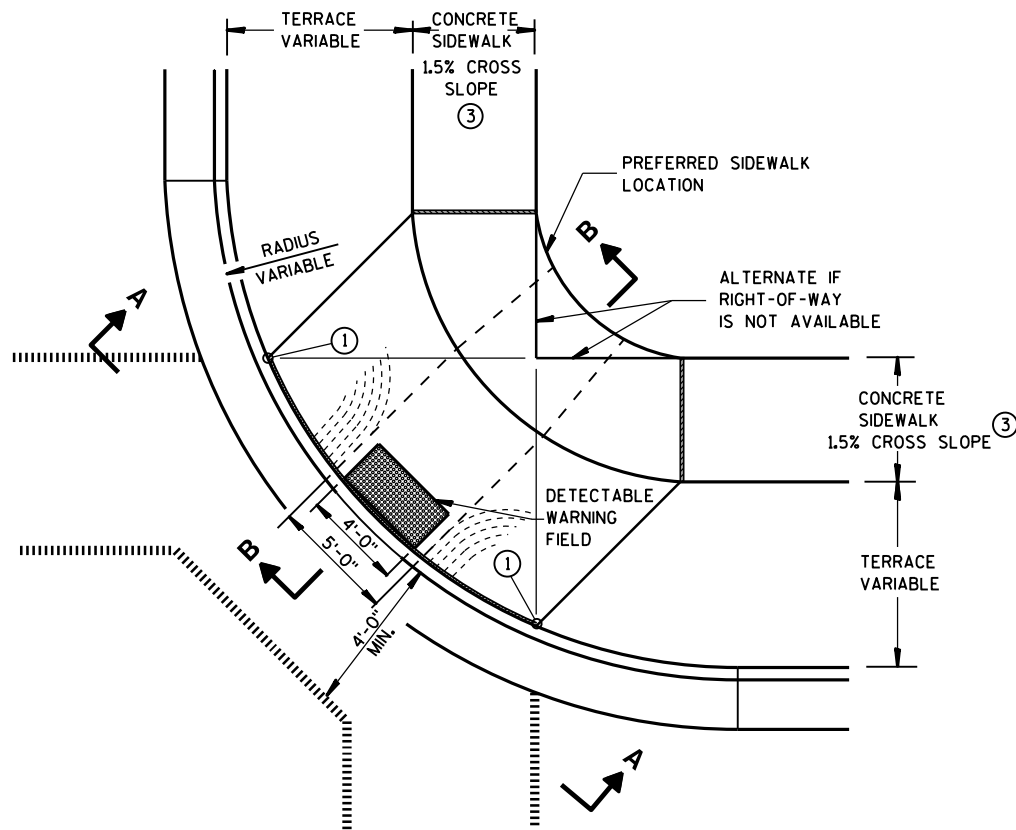
APPROVED

9-4-08

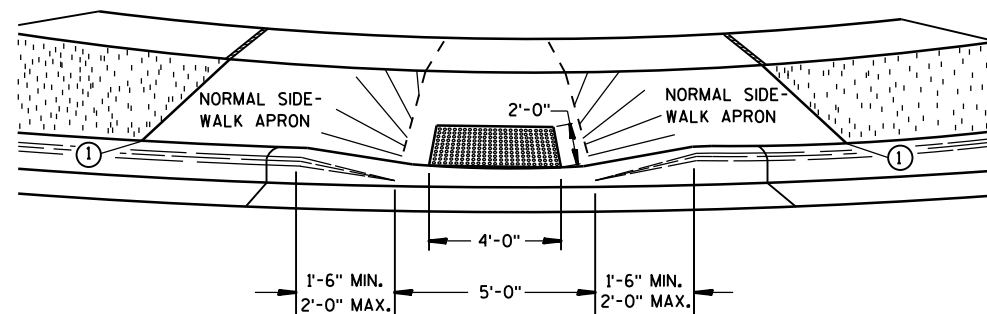
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FHWA

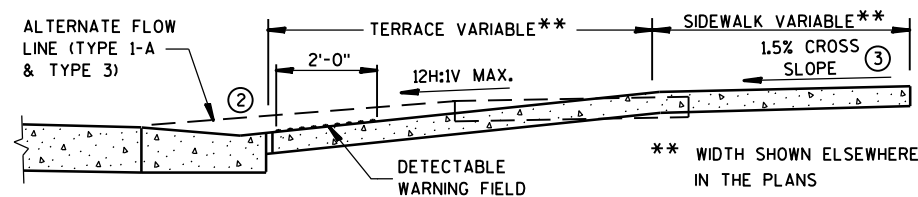
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



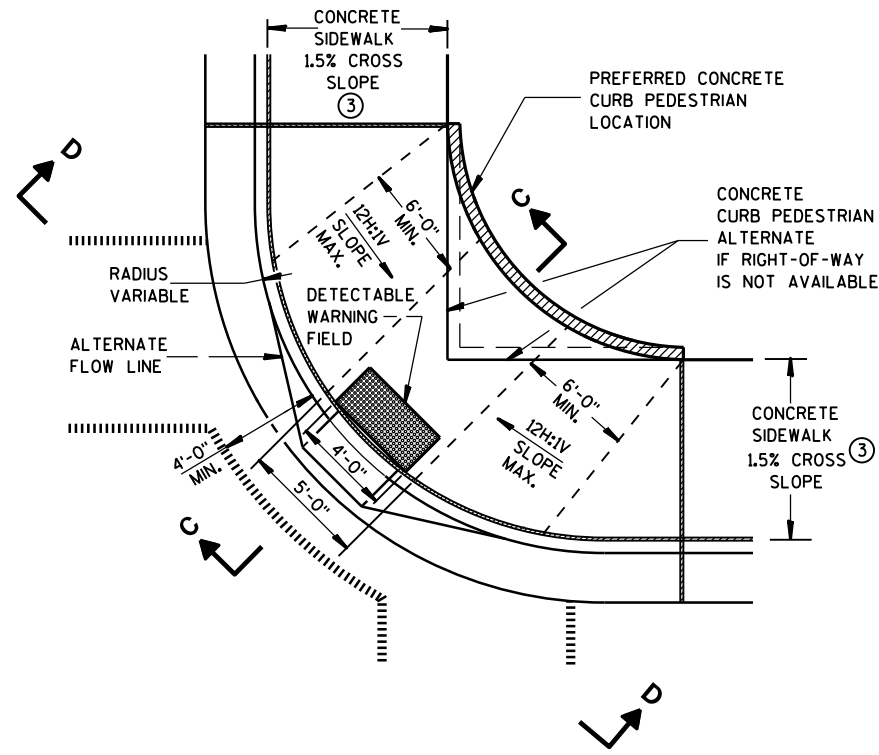
**PLAN VIEW
TYPE 1 RAMP**
(CENTER OF CORNER RADIUS)



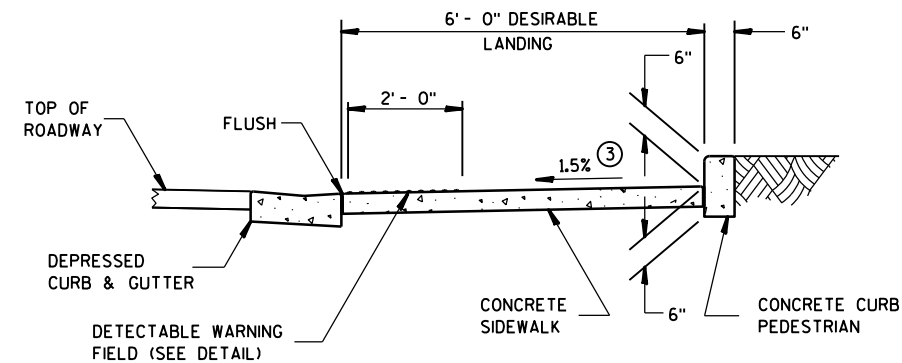
VIEW A-A



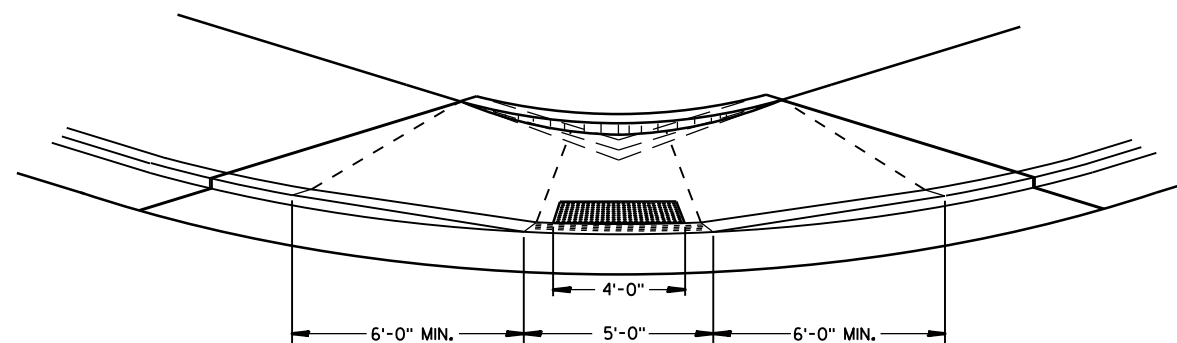
SECTION B-B



**PLAN VIEW
TYPE 1-A RAMP**
(NO TERRACE)



SECTION C-C



VIEW D-D

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

RAMPS SHALL BE BUILT AT 12H:1V OR FLATTER. WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAL FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD".

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

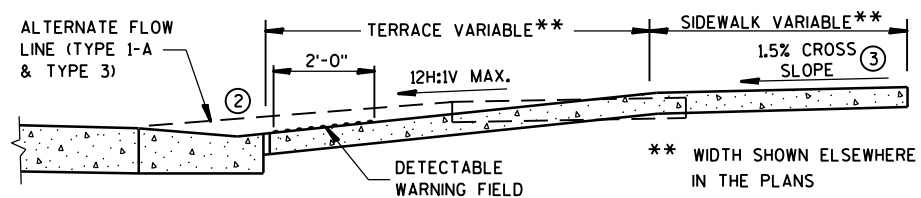
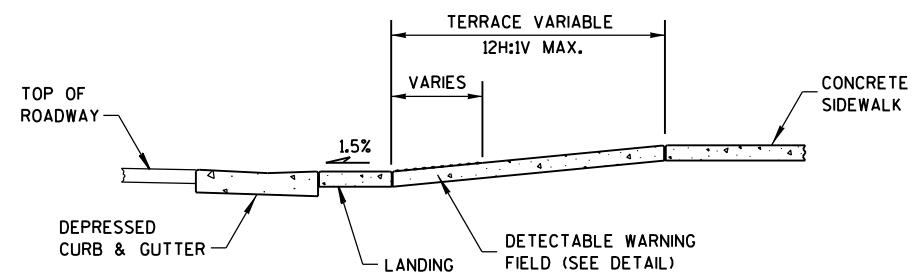
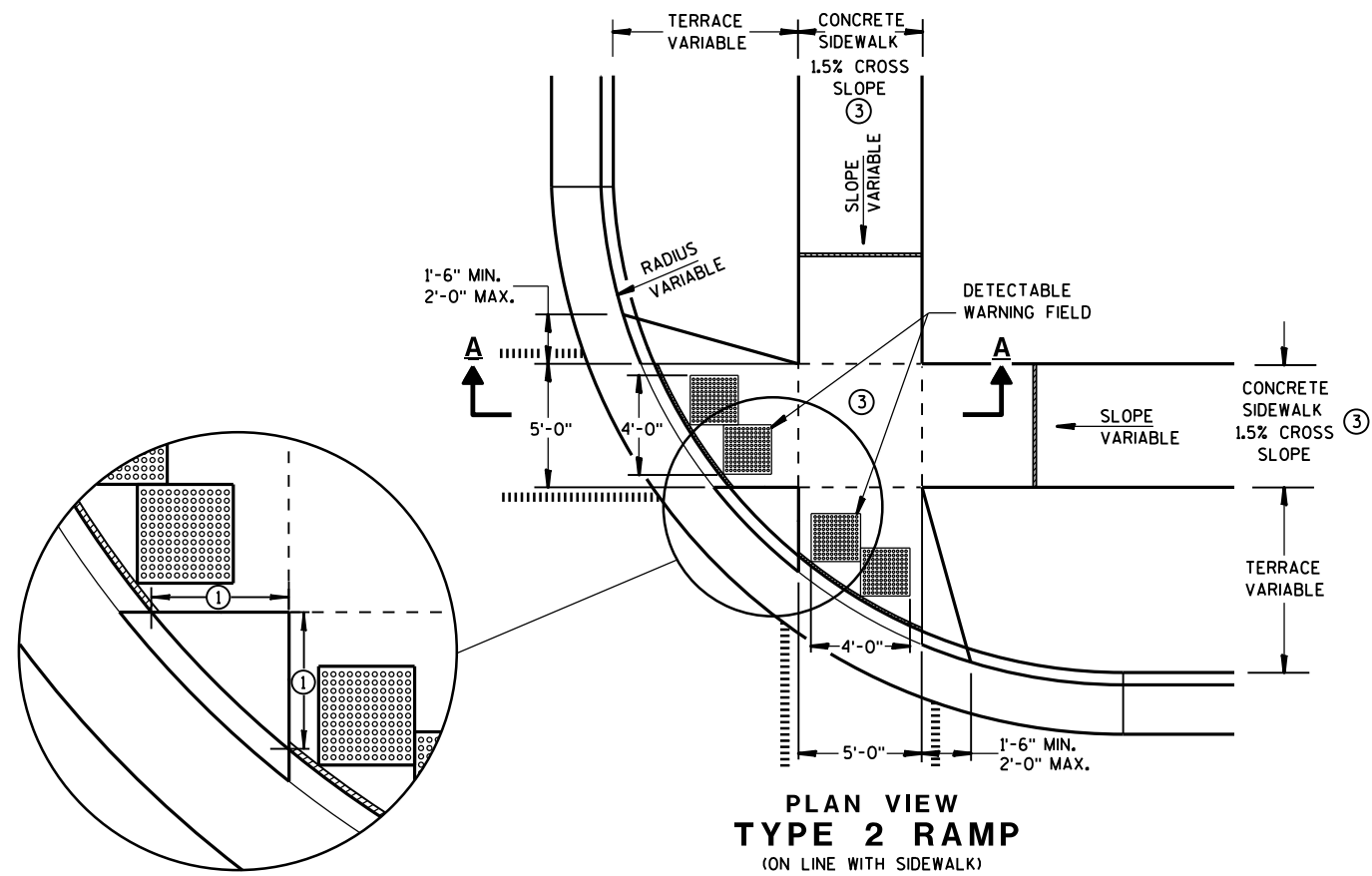
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE.
- ③ $\pm 0.5\%$ CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

LEGEND

- 1/2" EXPANSION JOINT-SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT

**CURB RAMPS
TYPES 1 AND 1-A**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



GENERAL NOTES

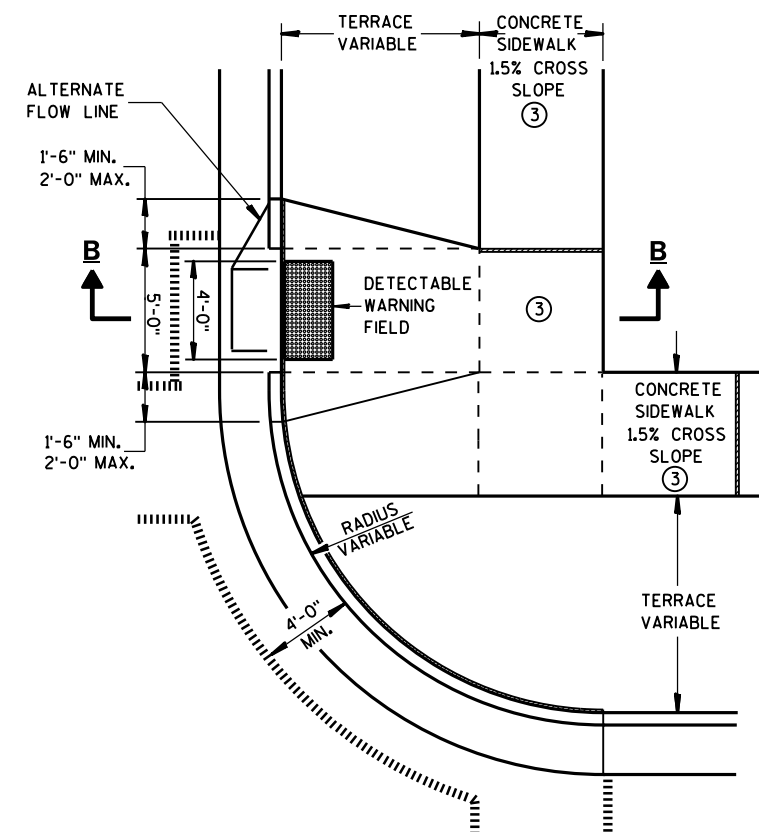
USE THE TYPE 3 RAMP ONLY WHEN A TYPE 1 OR TYPE 2 CANNOT BE ACHIEVED BECAUSE OF FIELD CONDITIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ① WHEN THIS DISTANCE IS LESS THAN 6'-0" IT MAY BE DIFFICULT TO ACHIEVE A 12H:1V SLOPE, OR FLATTER, ON THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 12H:1V SLOPE, OR FLATTER, ON RAMP. 2" MINIMUM CURB HEIGHT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE.
- ③ $\pm 0.5\%$ CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

LEGEND

- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



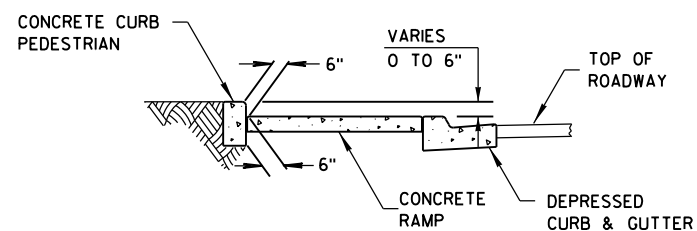
**PLAN VIEW
TYPE 3 RAMP
(OUTSIDE OF CROSSWALK AREA)**

**CURB RAMPS
TYPES 2 AND 3**

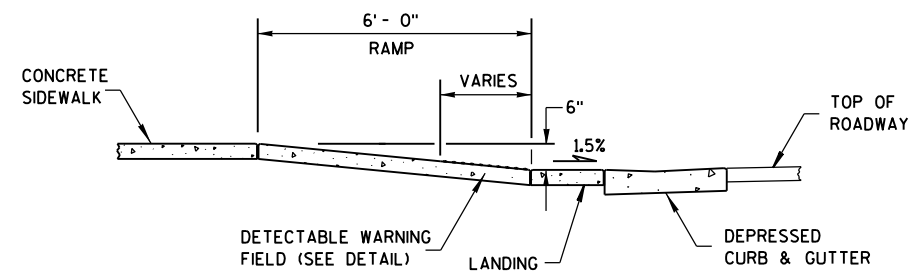
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CURB RAMP TYPE 4A
PLAN VIEW



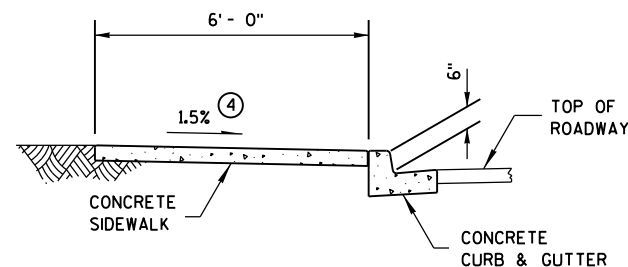
SECTION C-C FOR TYPE 4A



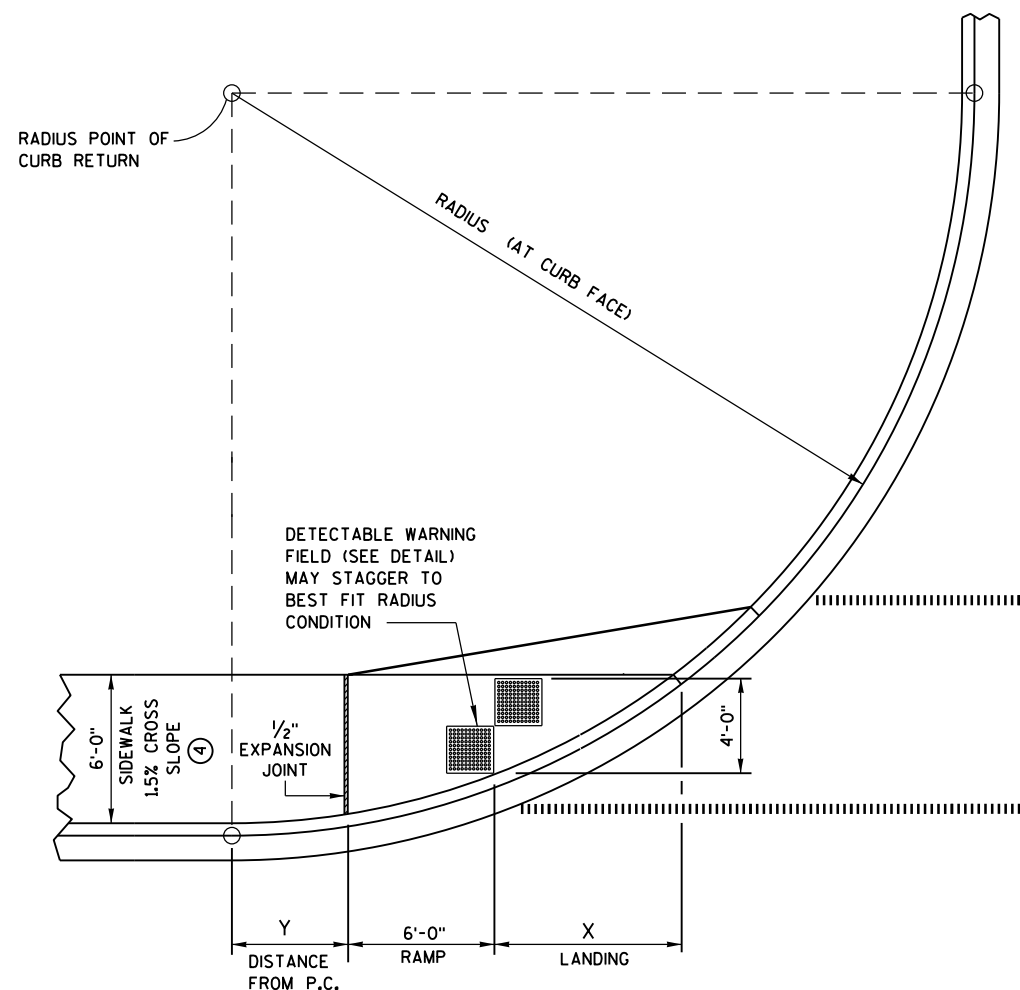
SECTION B-B FOR TYPE 4A

RADIUS (AT CURB FACE)	X	Y
20 FEET	6'-1 $\frac{3}{4}$ "	2'-7 $\frac{1}{4}$ "
30 FEET	7'-11 $\frac{3}{4}$ "	4'-8 $\frac{1}{4}$ "
40 FEET	9'-5 $\frac{1}{4}$ "	6'-5"
50 FEET	10'-8 $\frac{3}{4}$ "	7'-11 $\frac{1}{4}$ "
60 FEET	11'-10 $\frac{1}{4}$ "	9'-3 $\frac{1}{2}$ "

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A-A FOR TYPE 4A



CURB RAMP TYPE 4A1
PLAN VIEW

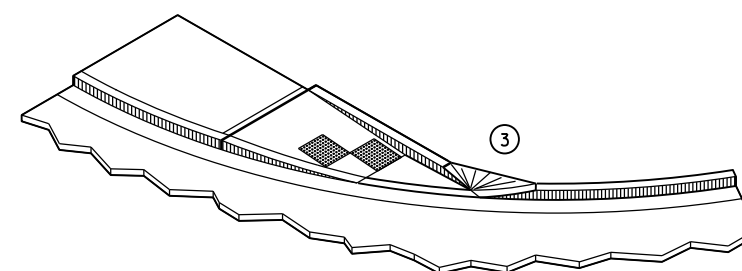
GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

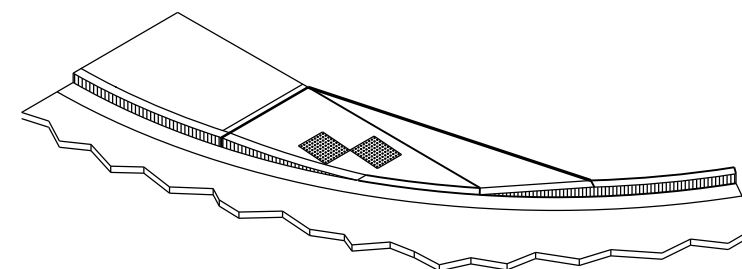
RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ③ INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.)
DO NOT MARK TRANSITION NOSE.
- ④ $\pm 0.5\%$ CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.





ISOMETRIC VIEW FOR TYPE 4A



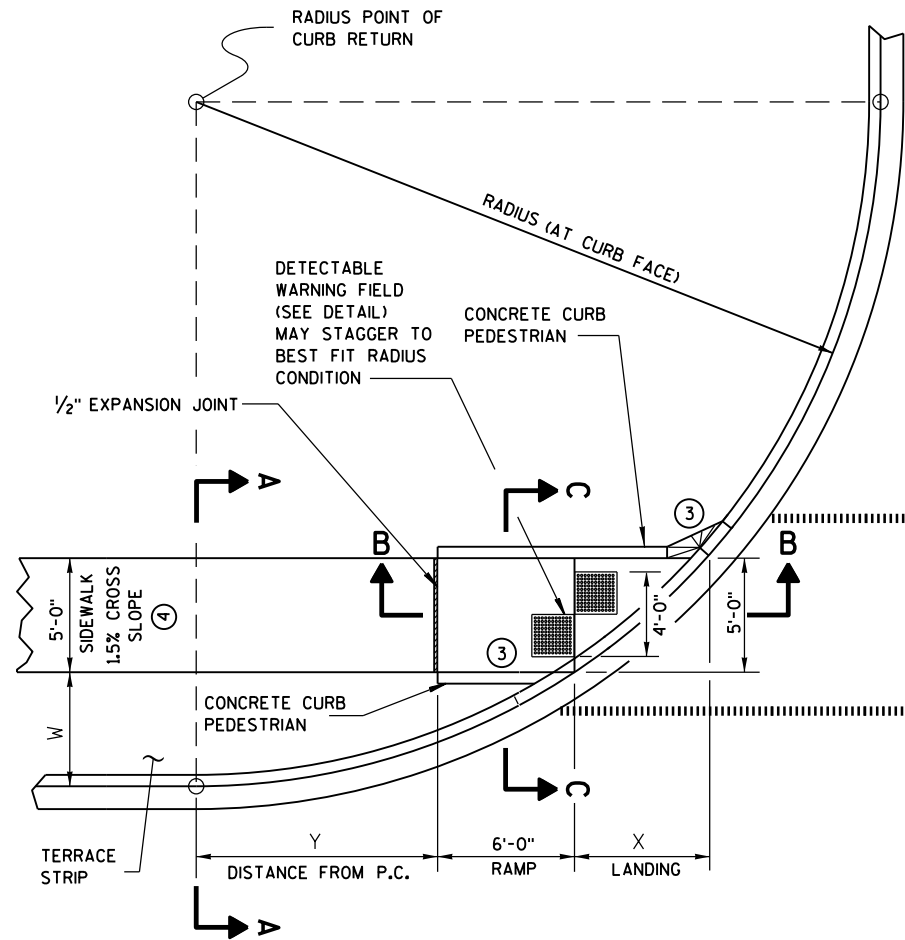
ISOMETRIC VIEW FOR TYPE 4A1

LEGEND

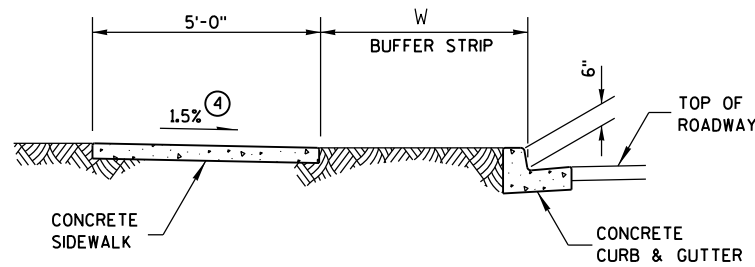
- | | |
|---|------------------------------------|
| | 1/2" EXPANSION JOINT-SIDEWALK |
|  | CONTRACTION JOINT FIELD LOCATED |
|  | PAVEMENT MARKING CROSSWALK (WHITE) |

CURB RAMPS TYPES 4A AND 4A1

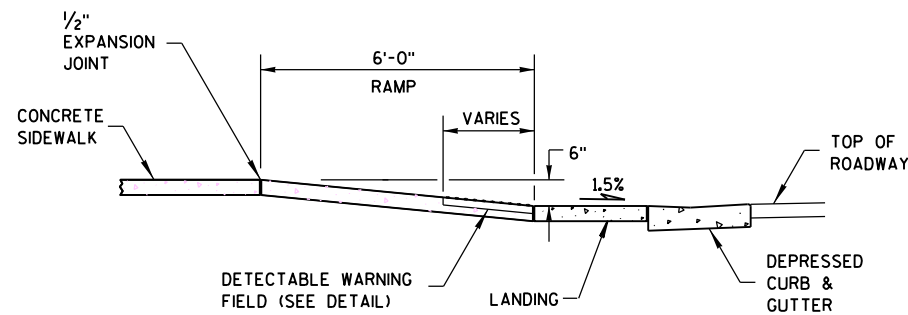
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**CURB RAMP TYPE 4B
PLAN VIEW**



SECTION A-A FOR TYPE 4B

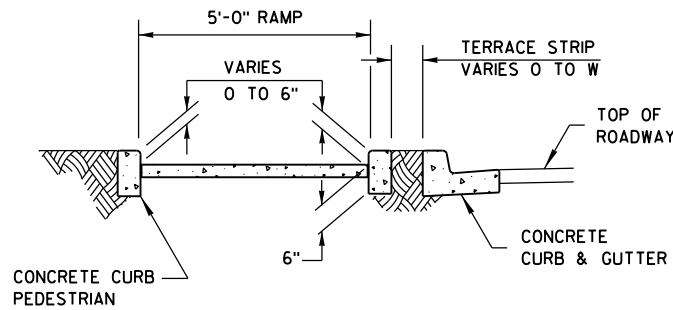


SECTION B-B FOR TYPE 4B

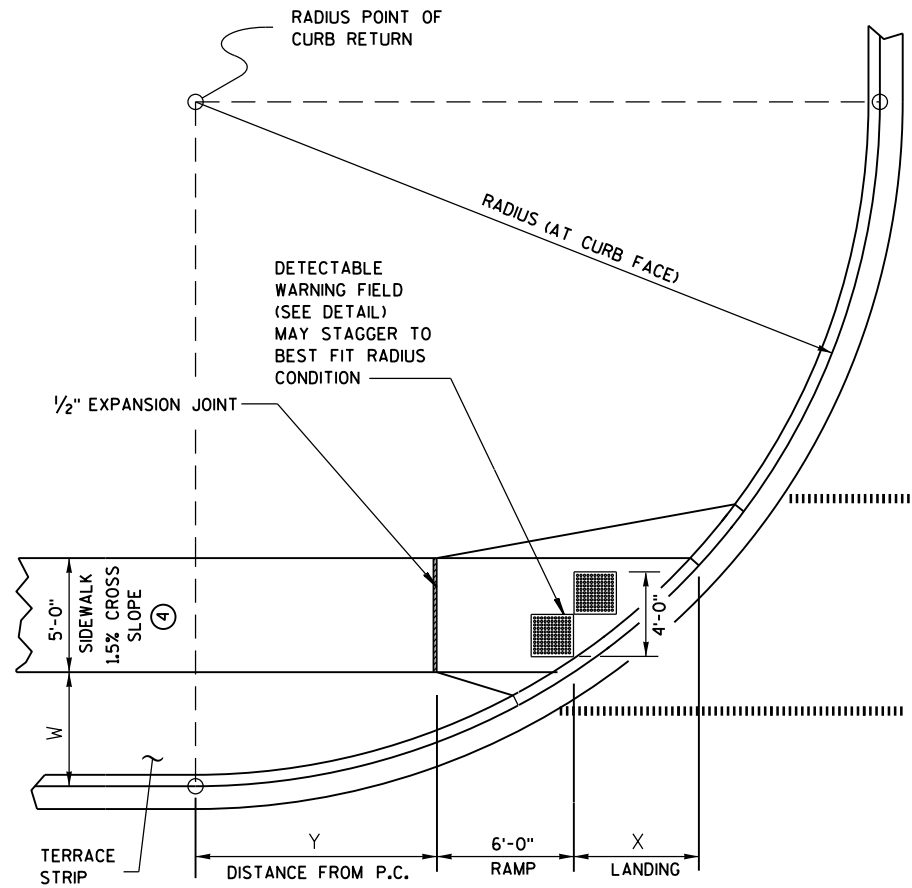
- LEGEND**
- 1/2" EXPANSION JOINT-SIDEWALK
 - CONTRACTION JOINT FIELD LOCATED
 - PAVEMENT MARKING CROSSWALK (WHITE)

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y
20 FEET	5'-5 1/2"	4'-6 1/2"	4'-8 1/2"	6'-0"	4'-1"	7'-2 3/4"	3'-7"	8'-3 1/2"	3'-1 1/2"	9'-2 1/2"
30 FEET	7'-3 3/4"	7'-1"	6'-5 1/2"	8'-11 1/2"	5'-9 1/4"	10'-7"	5'-2 1/2"	12'-0"	4'-8 3/4"	13'-3 1/4"
40 FEET	8'-9 1/2"	9'-2 1/2"	7'-10"	11'-5 1/4"	7'-1"	13'-4 1/2"	6'-5 3/4"	15'-3/4"	5'-11 1/2"	16'-7 1/4"
50 FEET	10'-3/4"	11'-3/4"	9'-1/4"	13'-7 1/4"	8'-2 1/2"	15'-9 1/2"	7'-6 1/2"	17'-9"	6'-11 3/4"	19'-6 1/4"
60 FEET	11'-2 1/2"	12'-8 3/4"	10'-3/4"	15'-6 1/2"	9'-2 1/4"	17'-11 3/4"	8'-5 3/4"	20'-1 3/4"	7'-10 1/2"	22'-1 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION C-C FOR TYPE 4B



**CURB RAMP TYPE 4B1
PLAN VIEW**

GENERAL NOTES

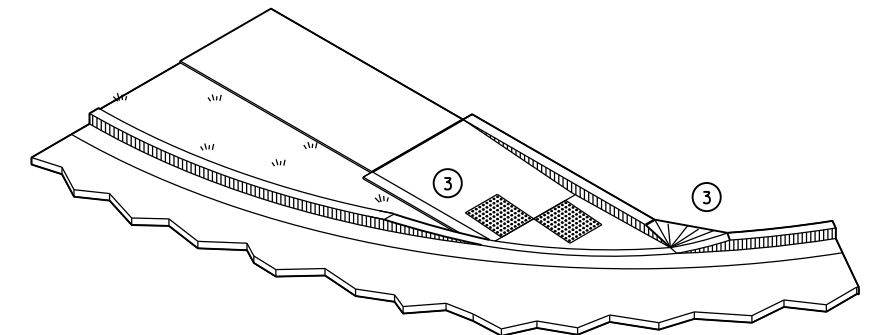
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

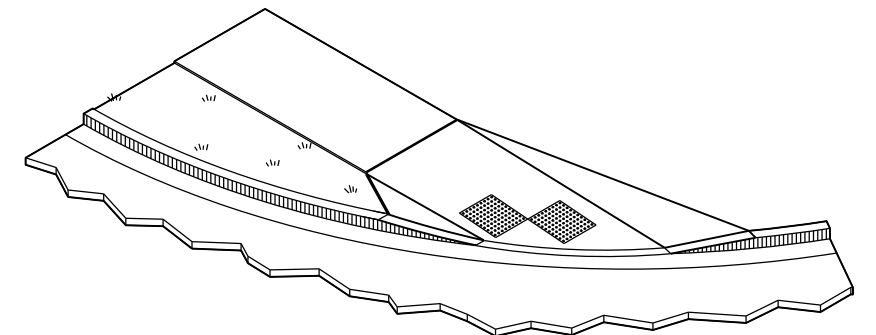
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

③ INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



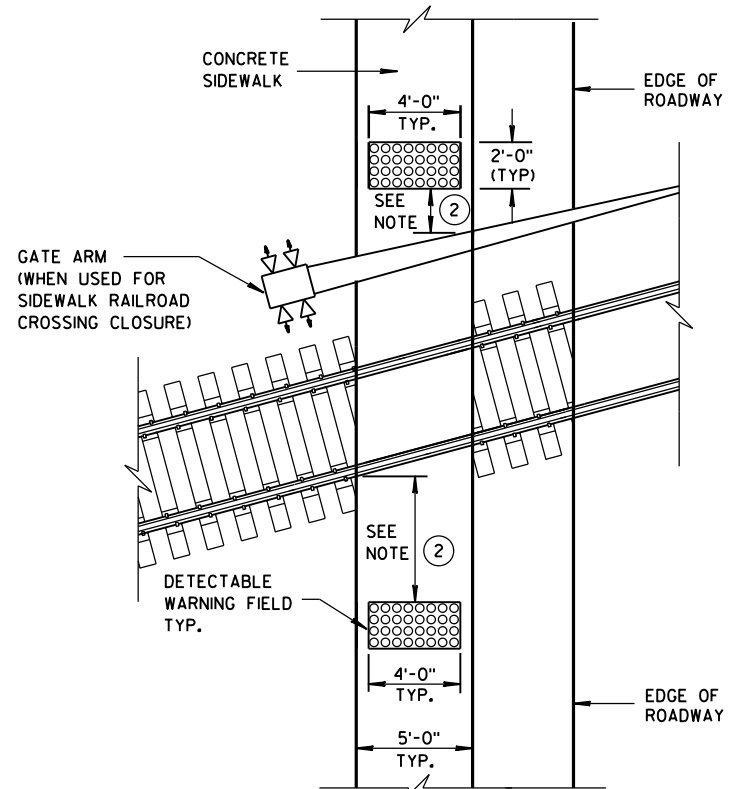
ISOMETRIC VIEW FOR TYPE 4B



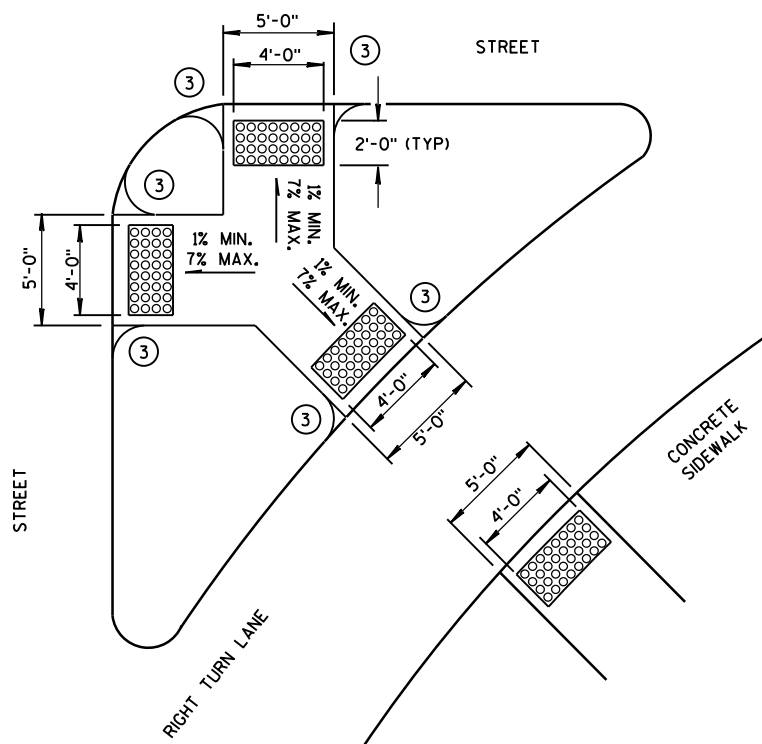
ISOMETRIC VIEW FOR TYPE 4B1

**CURB RAMPS
TYPE 4B AND 4B1**

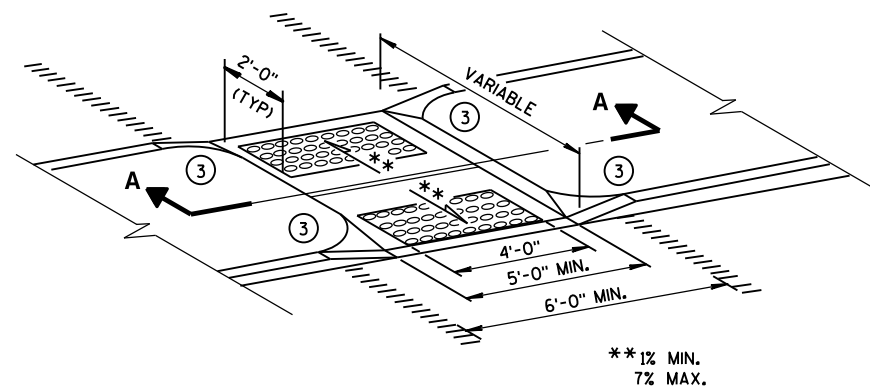
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



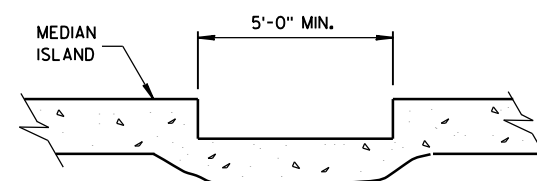
TYPE 8
DETECTABLE WARNINGS
AT RAILROAD CROSSING



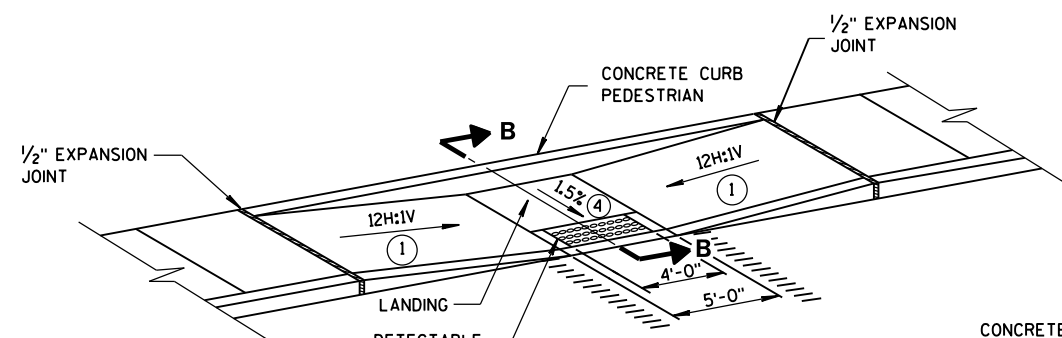
TYPE 6
DETECTABLE WARNING AT ISLANDS



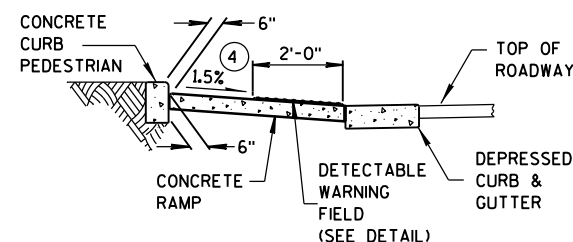
MEDIAN ISLAND
NON-ELEVATED CROSSING
TYPE 5



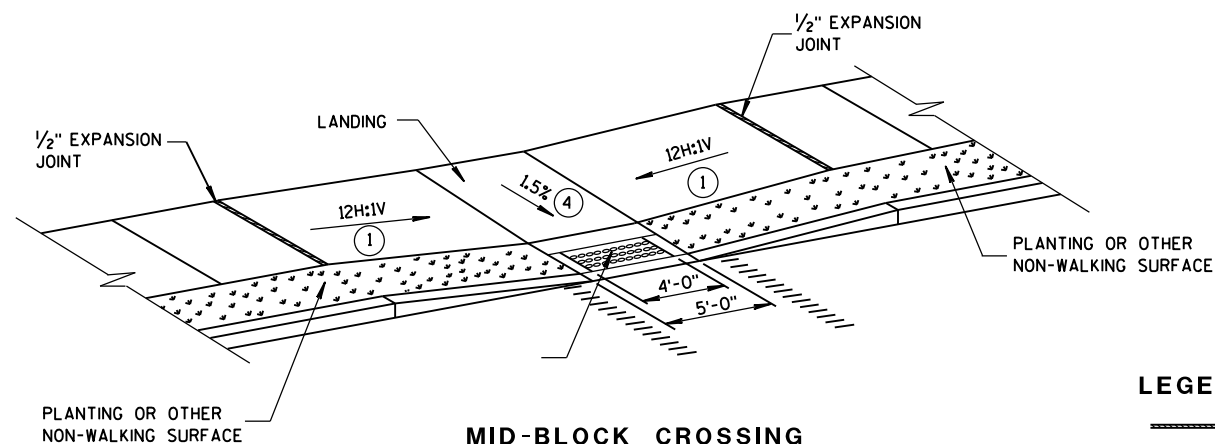
SECTION A-A



MID-BLOCK CROSSING
TYPE 7A



SECTION B-B



MID-BLOCK CROSSING
TYPE 7B

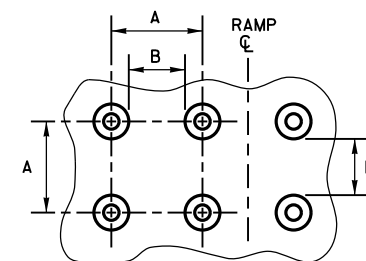
NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

GENERAL NOTES

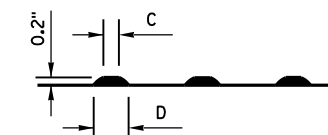
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ① SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ② THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET \pm 0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- ③ INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.
- ④ \pm 0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



PLAN VIEW



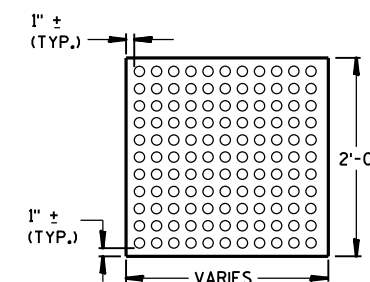
ELEVATION VIEW

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

TRUNCATED DOMES

DETECTABLE WARNING PATTERN DETAIL



PLAN VIEW
DETECTABLE WARNING
FIELD (TYPICAL)

LEGEND

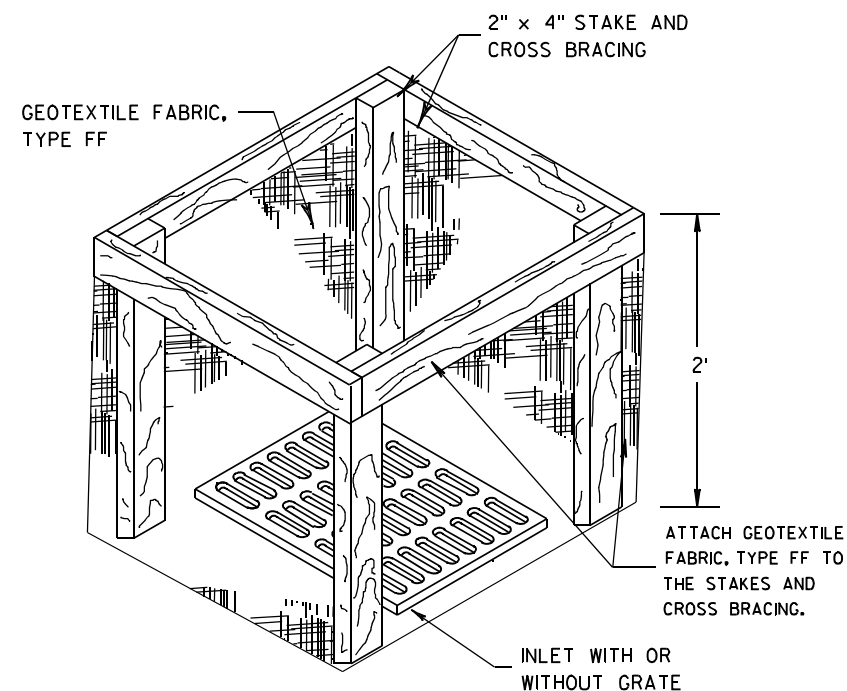
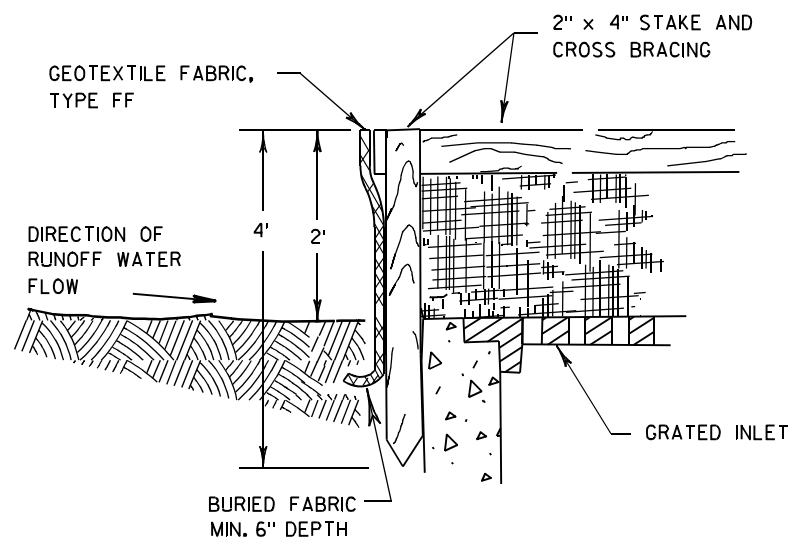
- 1/2" EXPANSION JOINT-SIDEWALK
- - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPES 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
2-6-2013
DATE
FHWA

/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



INLET PROTECTION, TYPE A

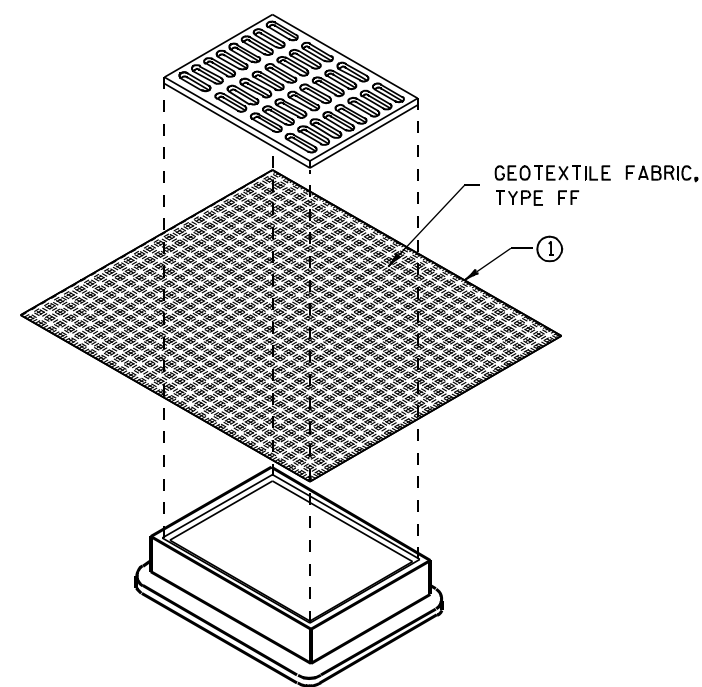
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

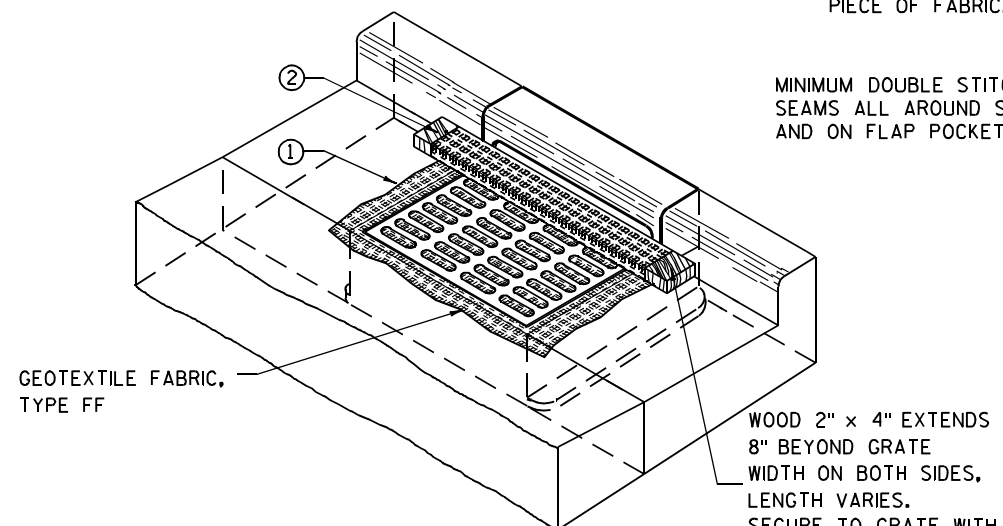
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

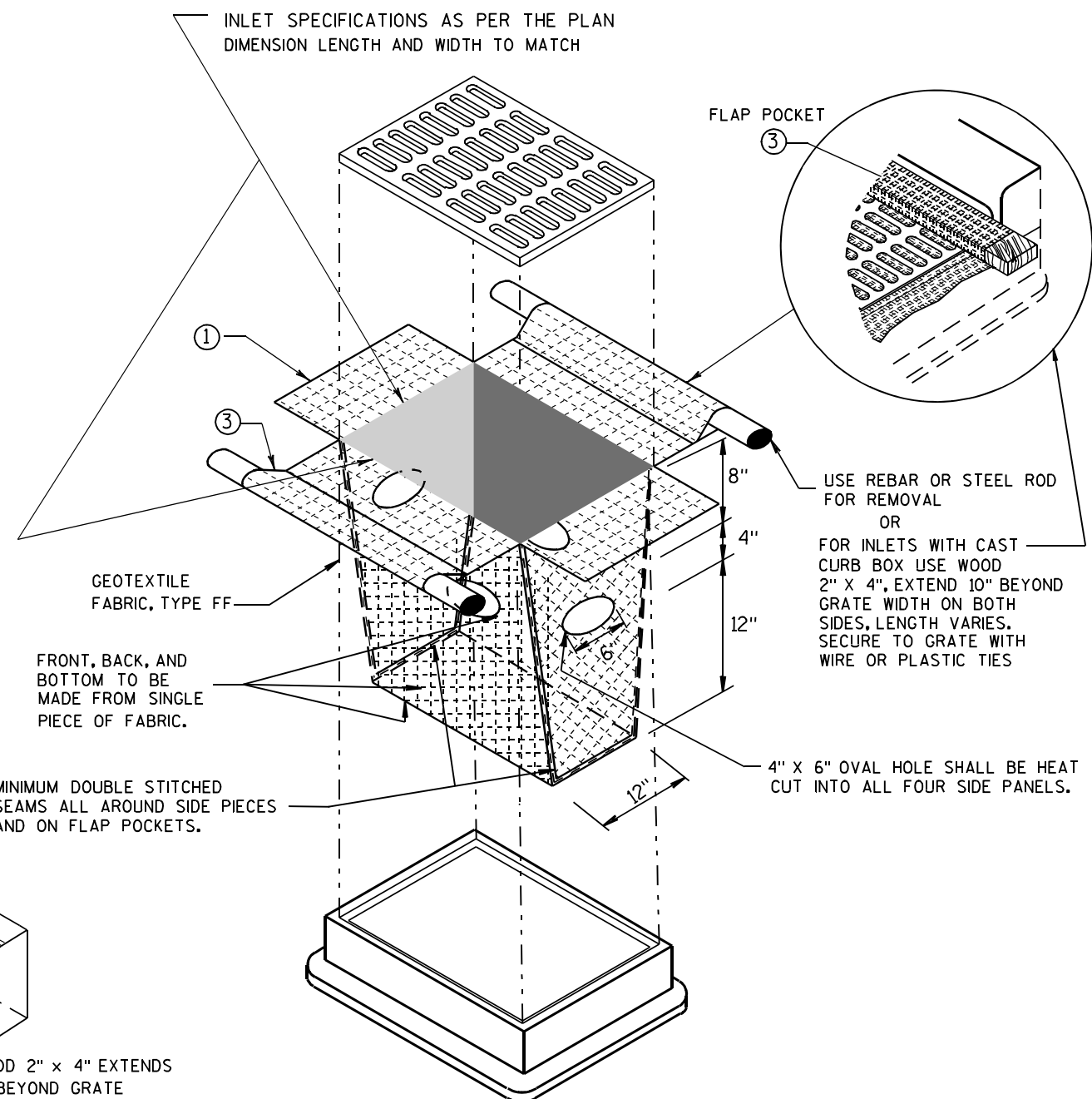
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



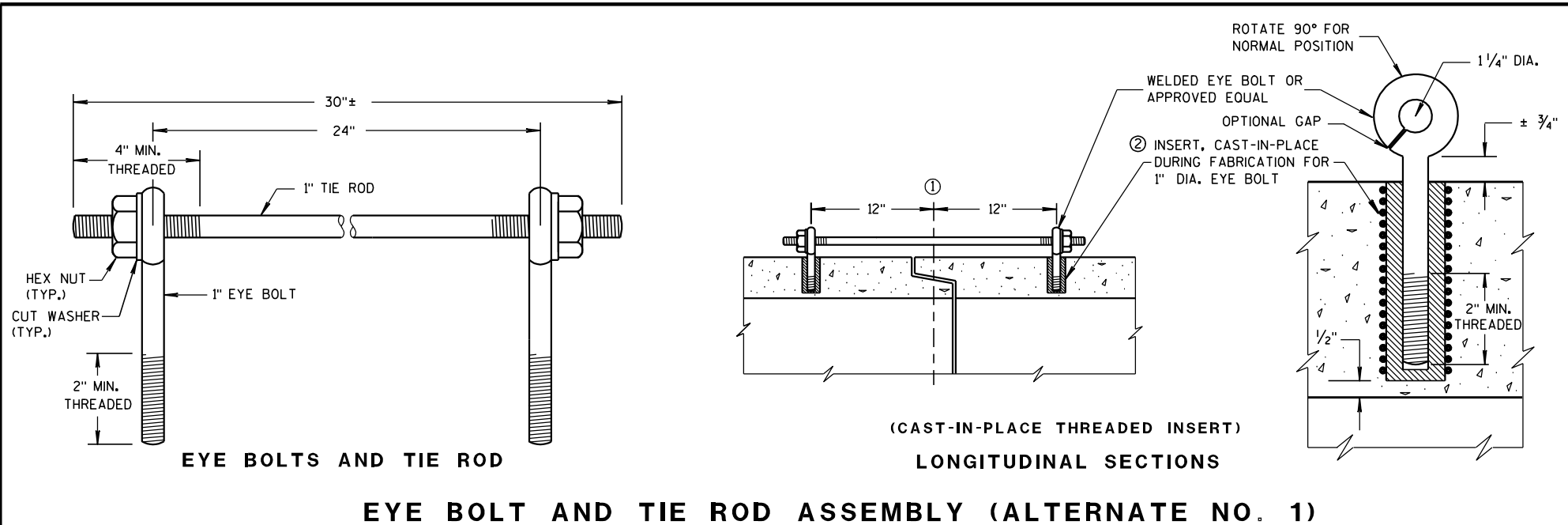
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Cannestra
DATE
FHWA CHIEF ROADWAY DEVELOPMENT ENGINEER



GENERAL NOTES

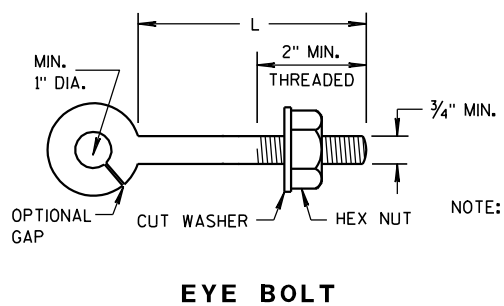
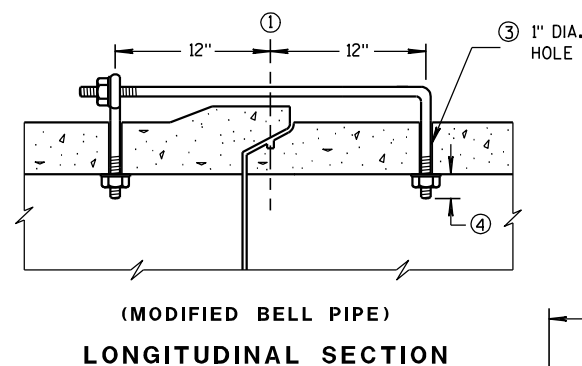
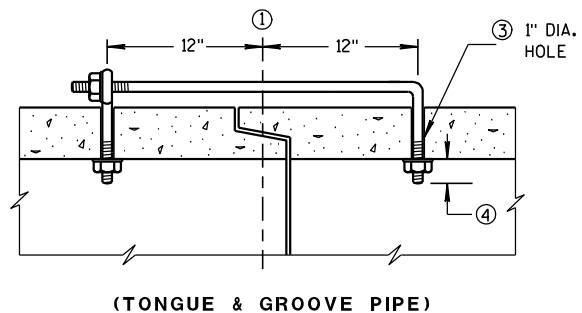
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

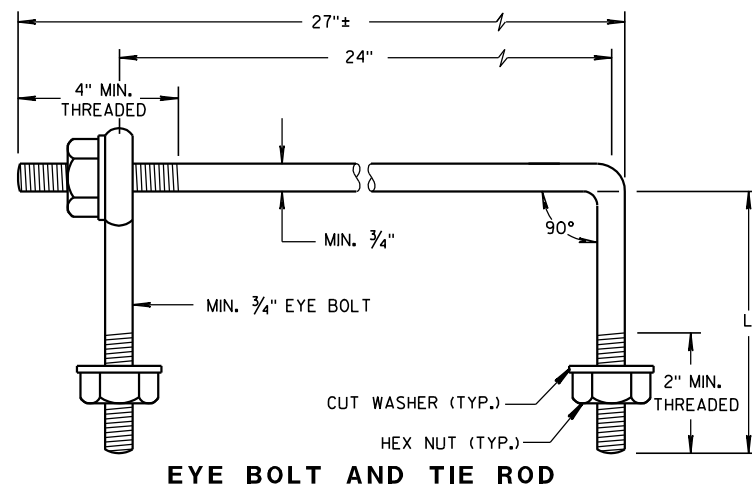
JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

- ① ϕ OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- ② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- ③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED 12 INCHES FROM ϕ OF TONGUE AND GROOVE.
- ④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- ⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.



NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.

(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)
EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)



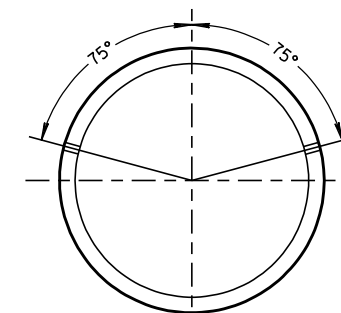
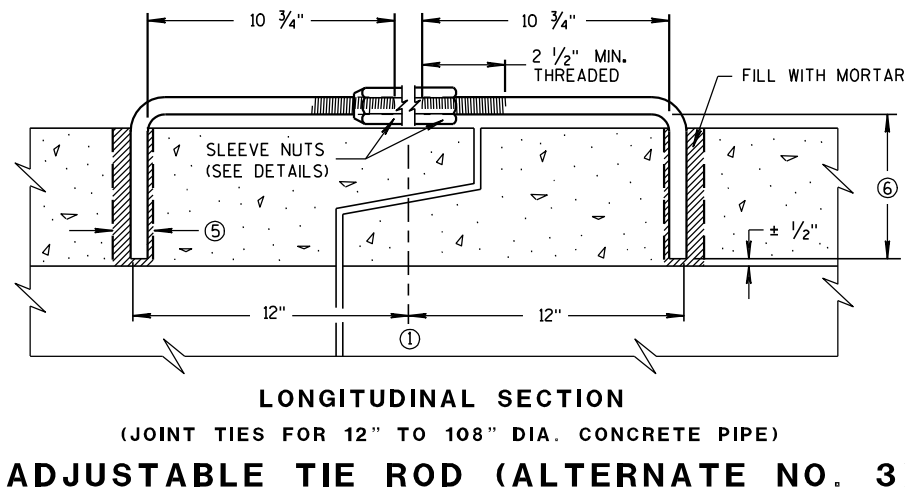
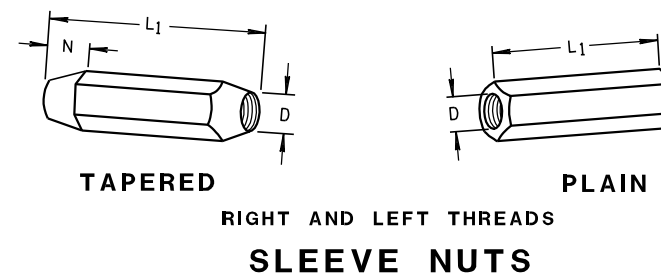
EYE BOLT DIMENSION TABLE

PIPE SIZE	L = LENGTH	
	TONGUE & GROOVE PIPE	MODIFIED BELL PIPE
18" TO 24"	4 1/2"	6 1/4"
30"	5"	7"
36"	5 1/2"	7"
42"	6"	
48"	6 1/2"	
60"	7 1/2"	
66"	8"	

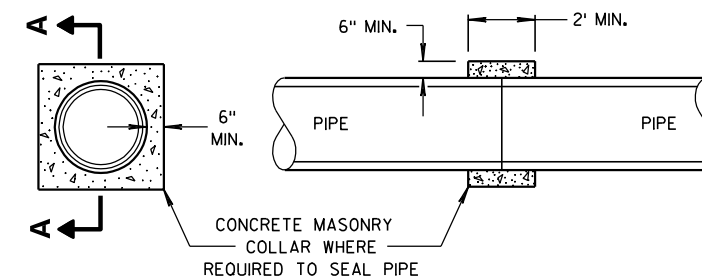
ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L ₁	N
12-60	5/8	5/8	5	1/2
66-84	3/4	3/4	5	1/2
90-108	1	1	7	1 1/16

DIMENSIONS SHOWN ARE IN INCHES



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

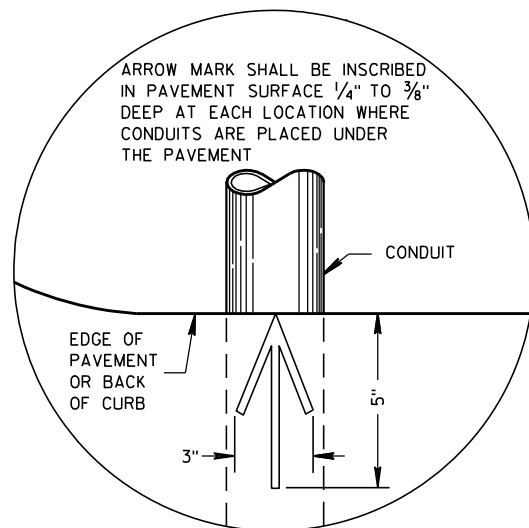


CONCRETE COLLAR DETAIL

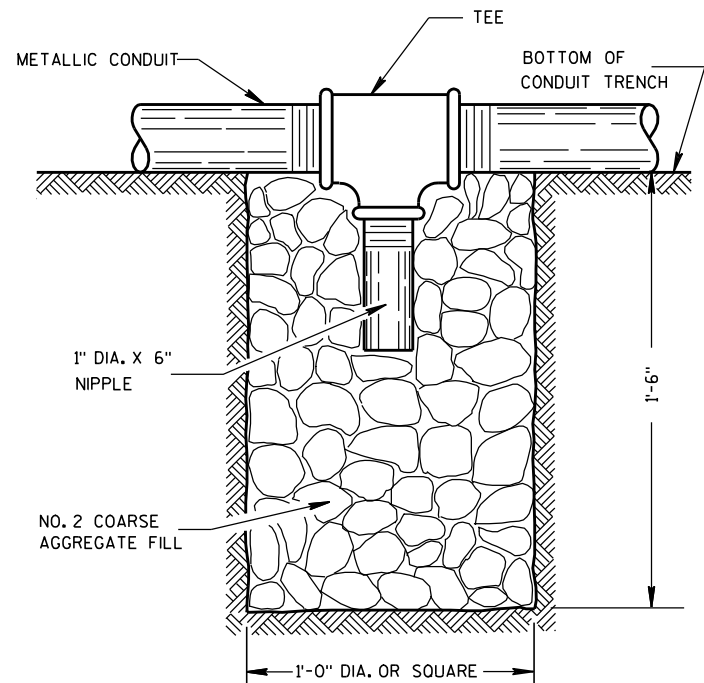
JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/5/2012 /S/ Jerry H. Zogg
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

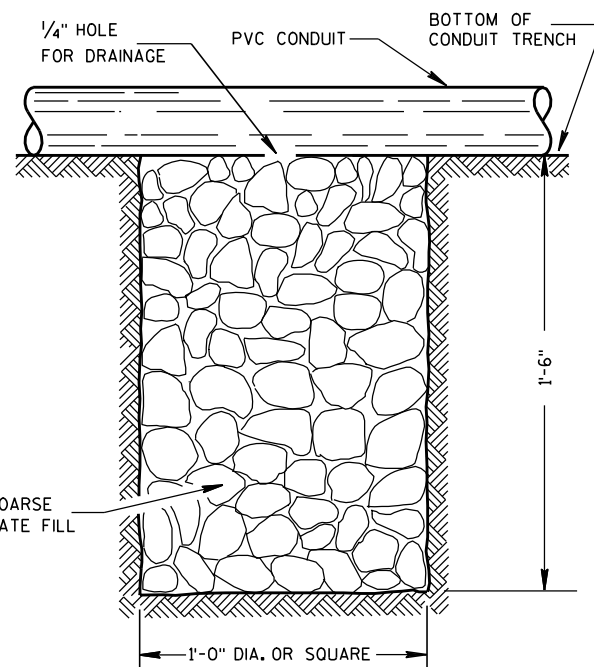


PLAN VIEW
ARROW MARK



NOTE: INSTALL AT LOCATIONS WHERE METALLIC CONDUITS CANNOT BE PITCHED TO DRAIN INTO A PULL BOX.

DRAIN SUMP FOR METALLIC CONDUIT



NOTE: INSTALL AT LOCATIONS WHERE PVC CONDUITS CANNOT BE PITCHED TO DRAIN INTO A PULL BOX.

DRAIN SUMP FOR PVC CONDUIT

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSON TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

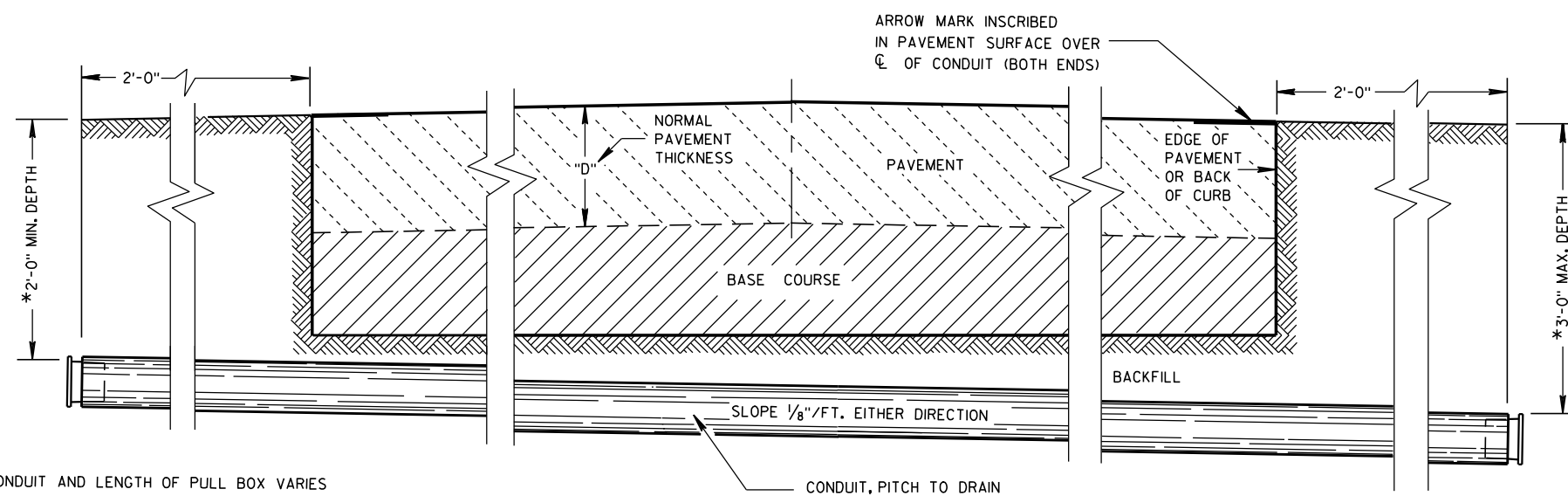
PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REINSTALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY ATTACHED.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

POLY ROPE OR A PULL WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.



*DEPTH OF CONDUIT AND LENGTH OF PULL BOX VARIES WITH HEIGHT OF CURB USED. ALSO SEE PULL BOX S.D.D. 9B4

SIDE ELEVATION
DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS

CONDUIT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

10/23/03

DATE

FHWA

/S/ Balu Ananthanarayanan
STATE ELECTRICAL ENGINEER FOR HWYS

TABLE OF NOMINAL DIMENSIONS AND WEIGHTS

DIMENSION IN INCHES		CORRUGATED STEEL PIPE								
PIPE DIAMETER (INSIDE)	A	12	12	12	18	18	18	24	24	24
PIPE LENGTH **	B	24	30	36	24	30	36	36	42	48
WALL THICKNESS	C	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064
COVER	D	10 1/4	10 1/4	10 1/4	16 1/4	16 1/4	16 1/4	22 1/4	22 1/4	22 1/4
FRAME	E	14 1/2	14 1/2	14 1/2	20 1/2	20 1/2	20 1/2	26 1/2	26 1/2	26 1/2
FRAME	F	8 1/2	8 1/2	8 1/2	14 1/2	14 1/2	14 1/2	20 1/2	20 1/2	20 1/2
FRAME	G	11 1/2	11 1/2	11 1/2	17 1/2	17 1/2	17 1/2	23 1/2	23 1/2	23 1/2
WEIGHT IN POUNDS *										
FRAME AND COVER		60	60	60	110	110	110	155	155	155

* THE ACTUAL WEIGHT OF THE MANHOLE FRAME AND COVER MAY VARY WITHIN 5 PERCENT PLUS OR MINUS OF THE WEIGHTS SHOWN.

** NORMALLY USED LENGTHS. THE PROJECT ENGINEER SHALL DETERMINE IF PIPE LENGTHS, OTHER THAN THOSE SPECIFIED, SHALL BE USED, TO A MAXIMUM OF 48" (CONTINUOUS LENGTH, NON-SPLICED). THE ADDITIONAL LENGTH SHALL BE INCIDENTAL TO THE PULL BOX BID PRICE.

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL FRAMES AND COVERS SHALL BE HEAVY DUTY TYPE, SUITABLE FOR VEHICULAR TRAFFIC LOADS.

PULL BOXES LOCATED IN THE ROADWAYS SHALL HAVE LOCKING COVERS.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN 1/4".

THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

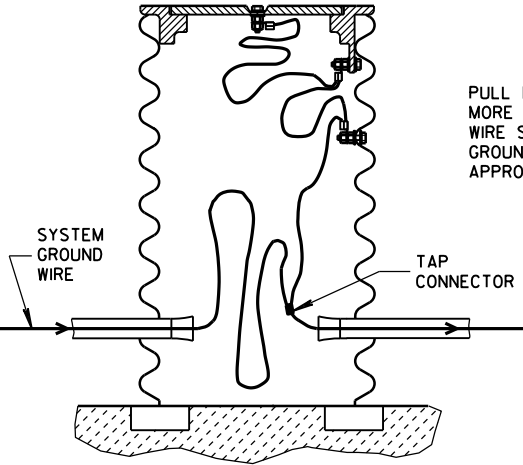
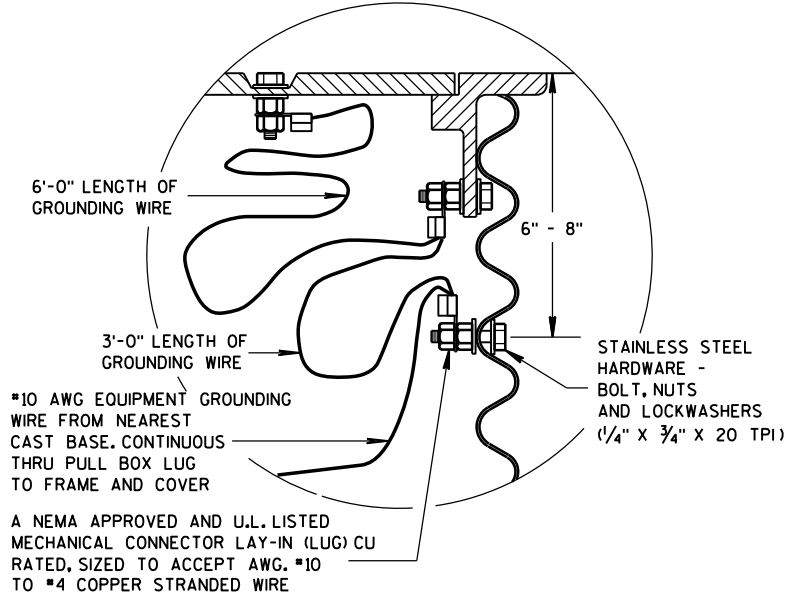
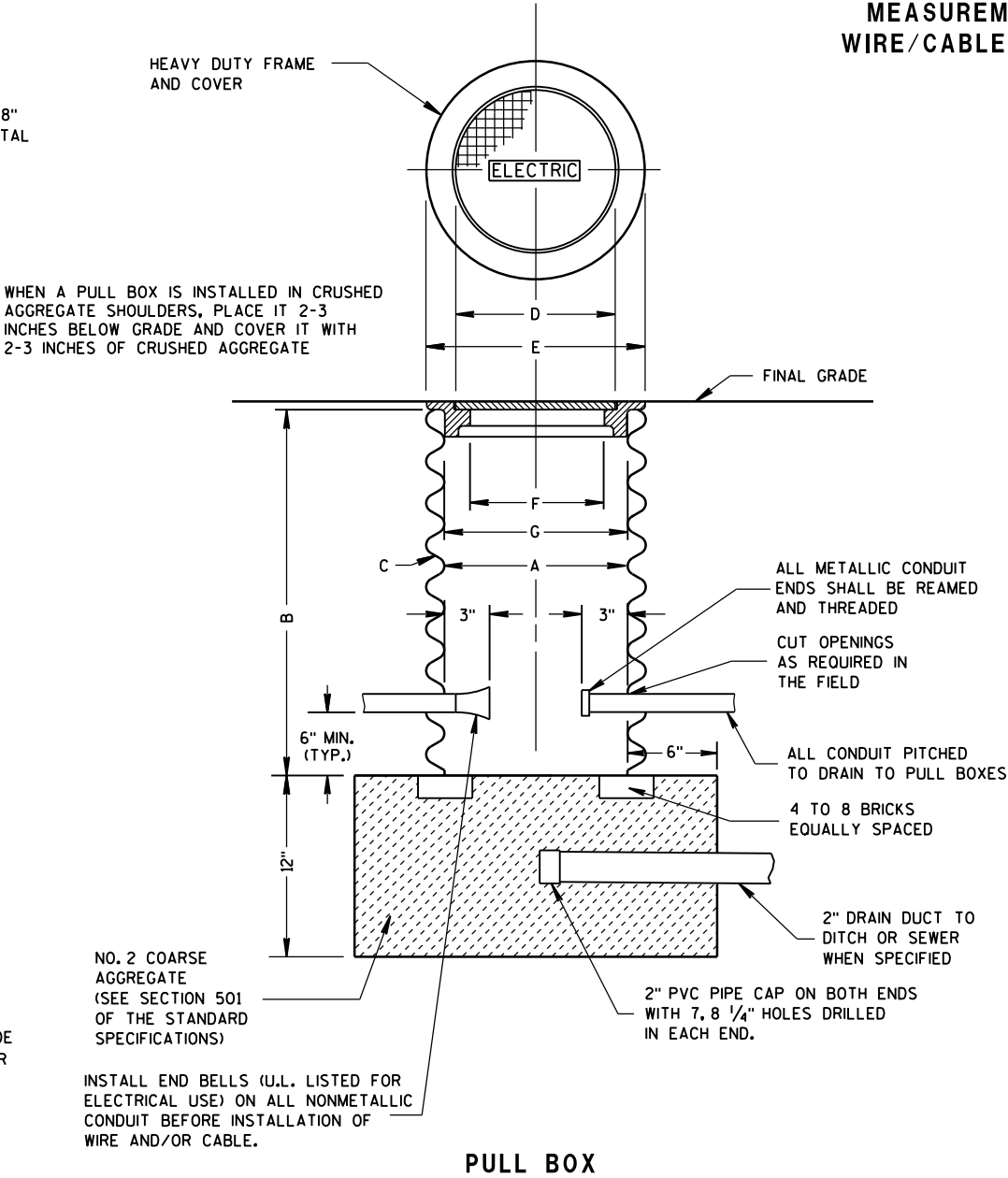
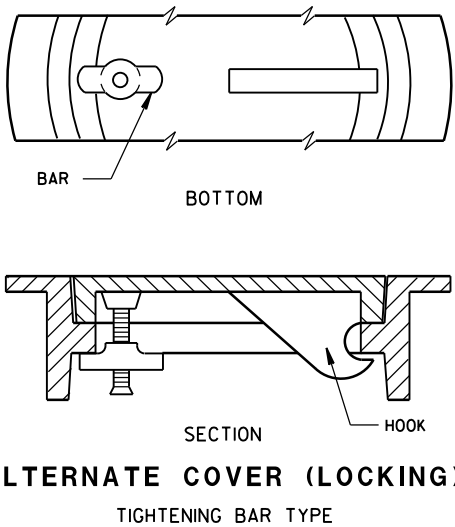
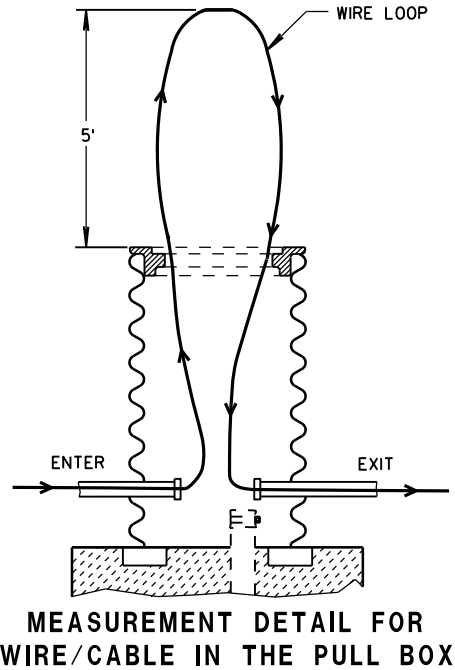
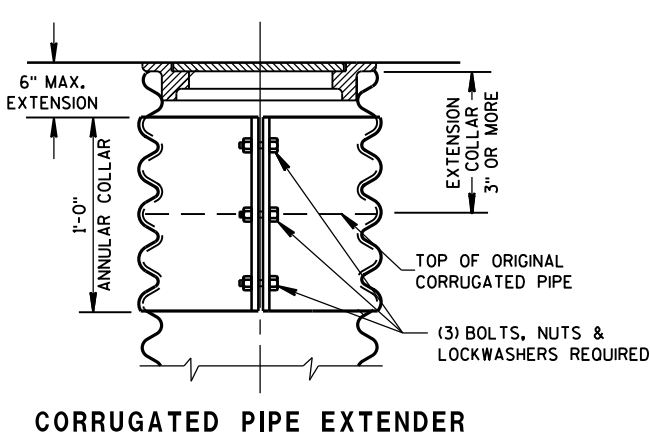
GROUNDING LUGS (MECHANICAL CONNECTORS) SHALL BE U.L. LISTED AND APPROVED FOR USE WITH COPPER WIRE.

GROUNDING LUGS ARE NOT REQUIRED IN PULL BOXES WHEN VOLTAGES OF LESS THAN 50 VOLTS AC ARE THE ONLY VOLTAGES ENCOUNTERED IN THE BOXES.

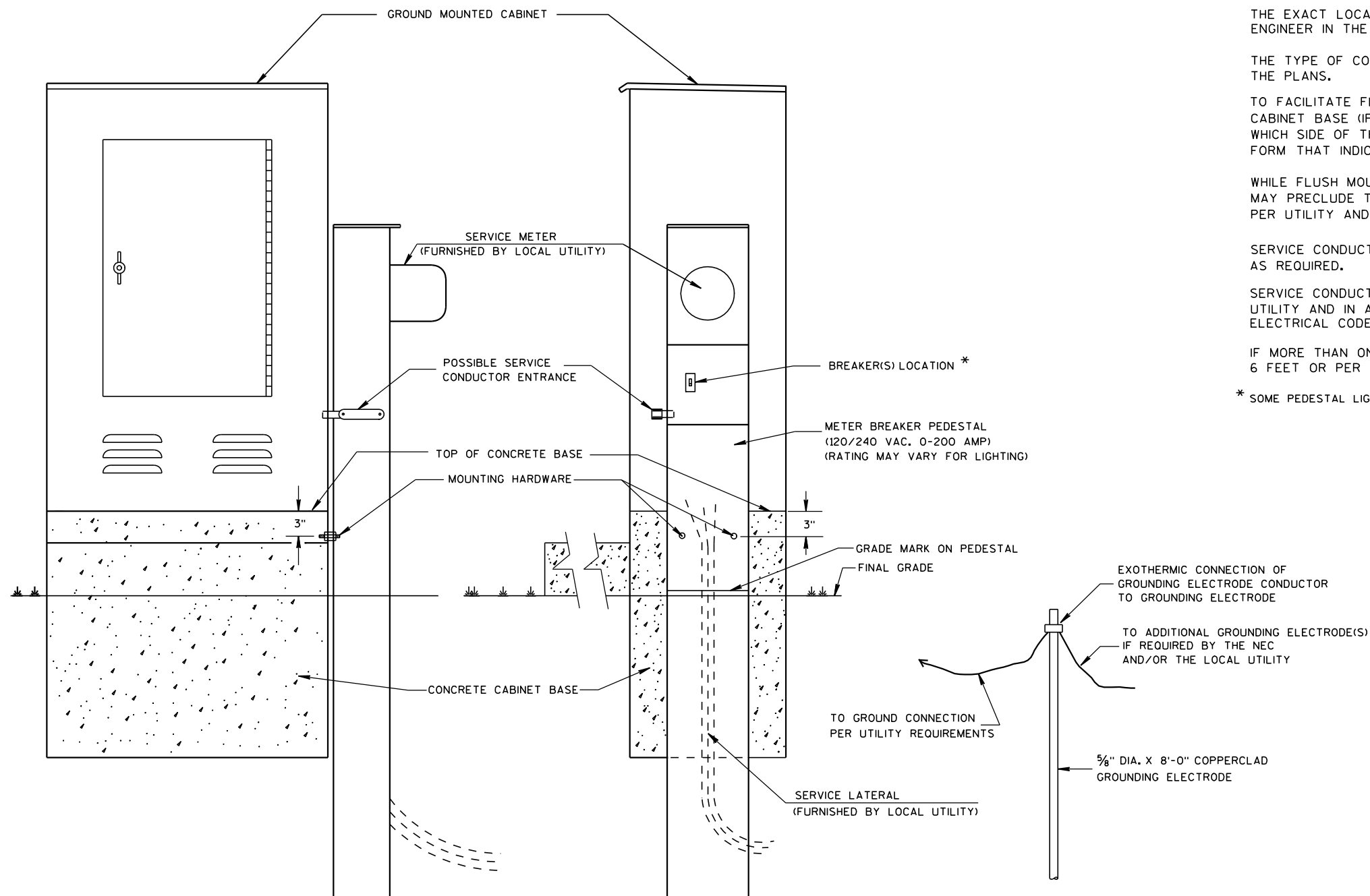
ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED, SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

S.D.D. 9B2, "CONDUIT", APPLIES TO THIS DRAWING.

WHEN PULL BOXES ARE INSTALLED FOR FUTURE USE, DO NOT INSTALL THE EQUIPMENT GROUNDING LUG. THE EQUIPMENT GROUNDING LUG, THE EQUIPMENT GROUNDING ELECTRODE AND THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE REQUIRED AND INSTALLED UNDER A FUTURE WIRING CONTRACT.



PULL BOX	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 2-7-2013 DATE	/S/ Ahmet Demirbilek STATE ELECTRICAL ENGINEER
FHWA	



TYPICAL CABINET SERVICE INSTALLATION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

THE EXACT LOCATION OF THE METER BREAKER PEDESTAL SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

THE TYPE OF CONCRETE CABINET BASE TO BE INSTALLED SHALL BE AS CALLED FOR IN THE PLANS.

TO FACILITATE FLUSH MOUNTING OF THE METER BREAKER PEDESTAL AGAINST THE SIDE OF THE CABINET BASE (IF FLUSH MOUNTING POSSIBLE, CONFER WITH THE LOCAL UTILITY TO DETERMINE WHICH SIDE OF THE CONCRETE BASE THE ELECTRICAL SERVICE LATERAL WILL APPROACH, THEN FORM THAT INDICATED SIDE FOR FULL SIDE DEPTH.

WHILE FLUSH MOUNTING IS THE MOST DESIRABLE MOUNTING CONFIGURATION UTILITY REQUIREMENTS MAY PRECLUDE THIS OPTION. CONTRACTOR MUST PROVIDE UTILITY APPROVED PEDESTAL AND INSTALL PER UTILITY AND MANUFACTURERS REQUIREMENTS.

SERVICE CONDUCTOR ENTRANCES SHALL BE RIGID METALLIC CONDUIT, NIPPLES AND/OR CONDULETS AS REQUIRED.

SERVICE CONDUCTOR ENTRANCES SHALL BE SIZED AND LOCATED AS REQUIRED BY THE LOCAL UTILITY AND IN ACCORDANCE WITH APPROPRIATE ARTICLES OF THE LATEST ACCEPTED NATIONAL ELECTRICAL CODE.

IF MORE THAN ONE GROUNDING ELECTRODE IS REQUIRED, THE DISTANCE APART SHALL BE 6 FEET OR PER LOCAL UTILITY REGULATIONS.

* SOME PEDESTAL LIGHTING PLANS SHOW MAIN LUGS ONLY.

CABINET SERVICE INSTALLATION
(METER BREAKER PEDESTAL)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

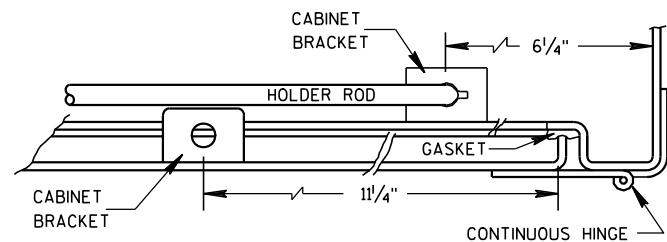
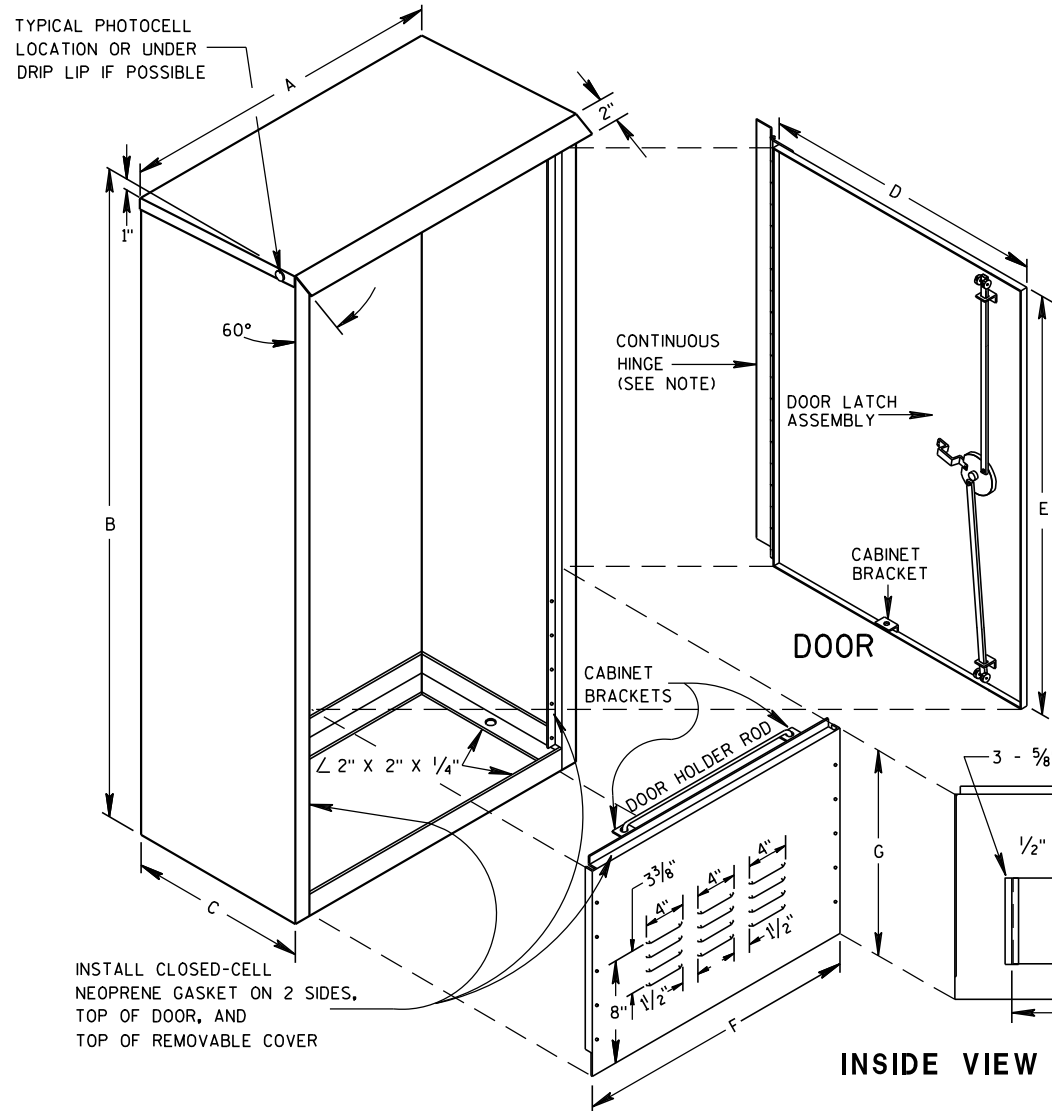
10/27/09

DATE

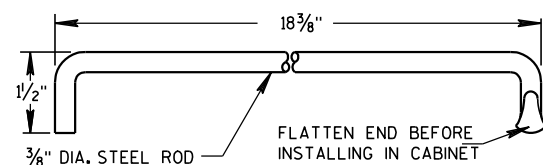
FHWA

/S/ Joanna L. Bush

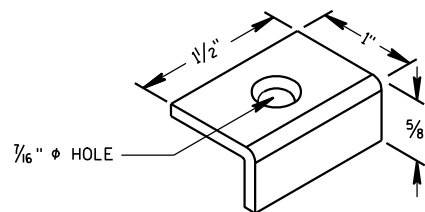
STATE ELECTRICAL ENGINEER FOR HWYS



HINGE & DOOR HOLDER



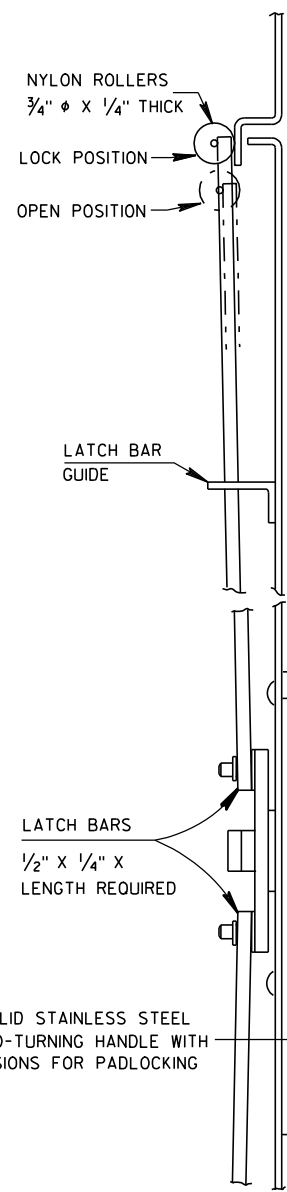
HOLDER ROD



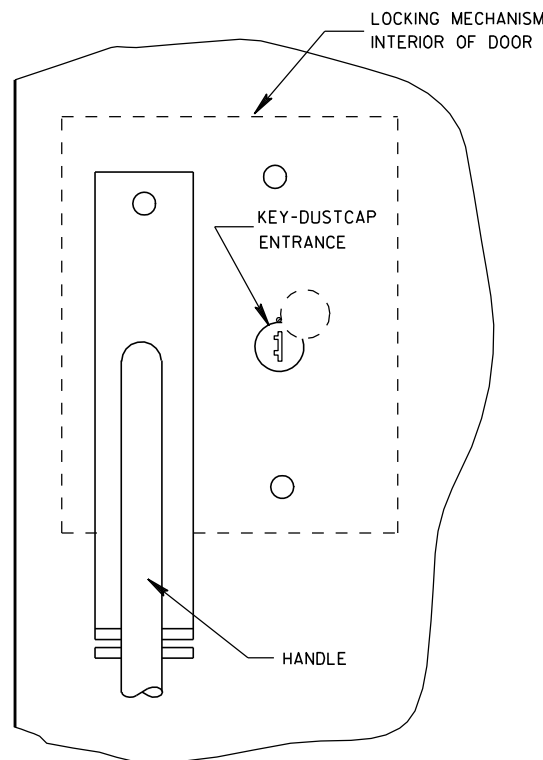
CABINET BRACKET

TABLE OF DIMENSIONS (INCHES)

MARK	CABINET TYPE		
	3060	3860	3866
A	30	38	38
B	60	60	66
C	16 1/2	16 1/2	24
D	26 1/2	34 3/4	33 3/4
E	38 3/4	38 3/4	38 3/4
F	26 1/2	34 3/4	33 3/4
G	19	19	25
H	16 1/2	16 1/2	24
H/2	8 1/4	8 1/4	12
J	30	38	38
J/2	15	19	19
K	13 3/4	13 3/4	21 1/4
L	27 1/2	35 1/2	35 1/2

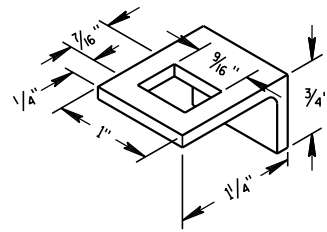


SIDE VIEW



FRONT VIEW

LATCH ASSEMBLY



LATCH BAR GUIDE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PRIME WITH PHOSPHATE TREATMENT AND PRIMER.

FINISH EXTERIOR SURFACES WITH RUSTOLEUM #906 SILVER GRAY OR APPROVED EQUAL.

FINISH INTERIOR WITH RUSTOLEUM #2766 HIGH GLOSS WHITE ENAMEL OR APPROVED EQUAL.

ALL SHEET METAL PARTS SHALL BE .125 INCH THICK ALUMINUM.

ALL SEAMS SHALL BE CONTINUOUSLY WELDED.

ALUMINUM SHALL BE TYPE 5052-H32.

CONTINUOUS HINGE SHALL BE HEAVY GAUGE ALUMINUM WITH 1/4" DIAMETER STAINLESS STEEL HINGE PIN. HINGE IS SECURED WITH 1/4" X 20 TPI STAINLESS STEEL CARRIAGE BOLTS AND STAINLESS STEEL NYLOCK NUTS.

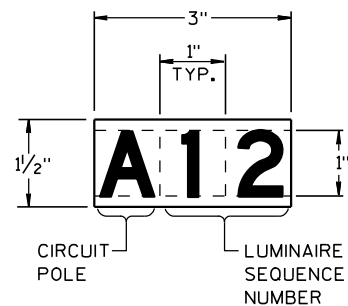
A SINGLE PHOTOCELL SHALL BE LOCATED ON THE NORTH-NORTHEAST SIDE OF THE CABINET UNLESS OTHERWISE CALLED FOR IN THE SPECIAL PROVISIONS. THE PHOTOCELL SHALL BE PLACED AS SHOWN AND SHALL BE AN APPROVED TYPE.

DOOR LATCH ASSEMBLY TO BE PROVIDED WITH THREE-POINT LOCKING MECHANISM.

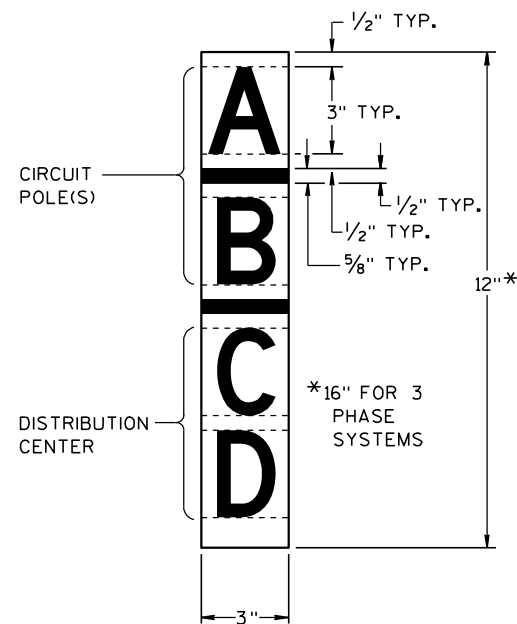
SIGNAL OR LIGHTING CONTROL CABINET

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/21/96
DATE
/S/ Balu Ananthanarayanan
STATE ELECTRICAL ENGINEER FOR HWYS
FHWA

**SEQUENCE DECAL**

(MOUNT ON LUMINAIRE)

**SIGN BRIDGE CIRCUIT PLAQUE****GENERAL NOTES**

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN IN THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PLAQUES SHALL BE INCIDENTAL TO ALL NEW INSTALLATIONS.

WHERE SHOWN IN THE PLANS, REPLACEMENT PLAQUES WILL BE MEASURED AND PAID SEPARATELY.

FASTEN TOP, CENTER AND BOTTOM OF PLAQUE TO POLE OR OTHER LOCATION AS FOLLOWS:

GALVANIZED STEEL SHAFT - 3 STAINLESS STEEL POP RIVETS

A588 STEEL SHAFT - SHIM FOR DRAINAGE WITH STAINLESS WASHERS;
FASTEN WITH STAINLESS SELF-TAPPING SCREWS

ALUMINUM SHAFTS - 3 ALUMINUM POP RIVETS

MOUNTING HEIGHT SHALL BE APPROXIMATELY 5.0' ABOVE CURB OR SHOULDER. ADJUST IF IT IS KNOWN THAT REQUIRED TRAFFIC SIGNS WILL OBSTRUCT.

PLAQUE MATERIALS:

BASE - SHEET ALUMINUM, 0.060" THICK.

FACE - WHITE, SELF-ADHESIVE VINYL SHEETING, NON-RETROREFLECTIVE

LINES - BLACK, 1/2" WIDE, SELF-ADHESIVE

CHARACTERS - BLACK, SELF-ADHESIVE, SERIES "D", SIZE AS SHOWN

WITH THE APPROVAL OF THE ENGINEER, THE BASE MATERIAL MAY BE OMITTED AND THE FACE ADHERED DIRECTLY TO THE SURFACE, IN CASES SUCH AS SMOOTH, CLEAN ALUMINUM STRUCTURES.

ALTERNATIVE COMPUTER-GENERATED SIGN LETTERING MAY BE ACCEPTED IF THE ENGINEER FINDS IT TO BE EQUIVALENT.

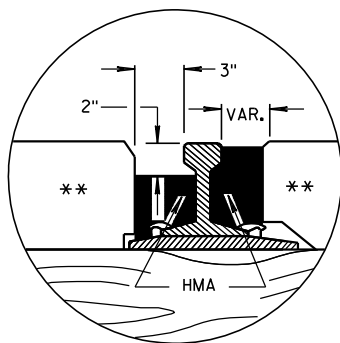
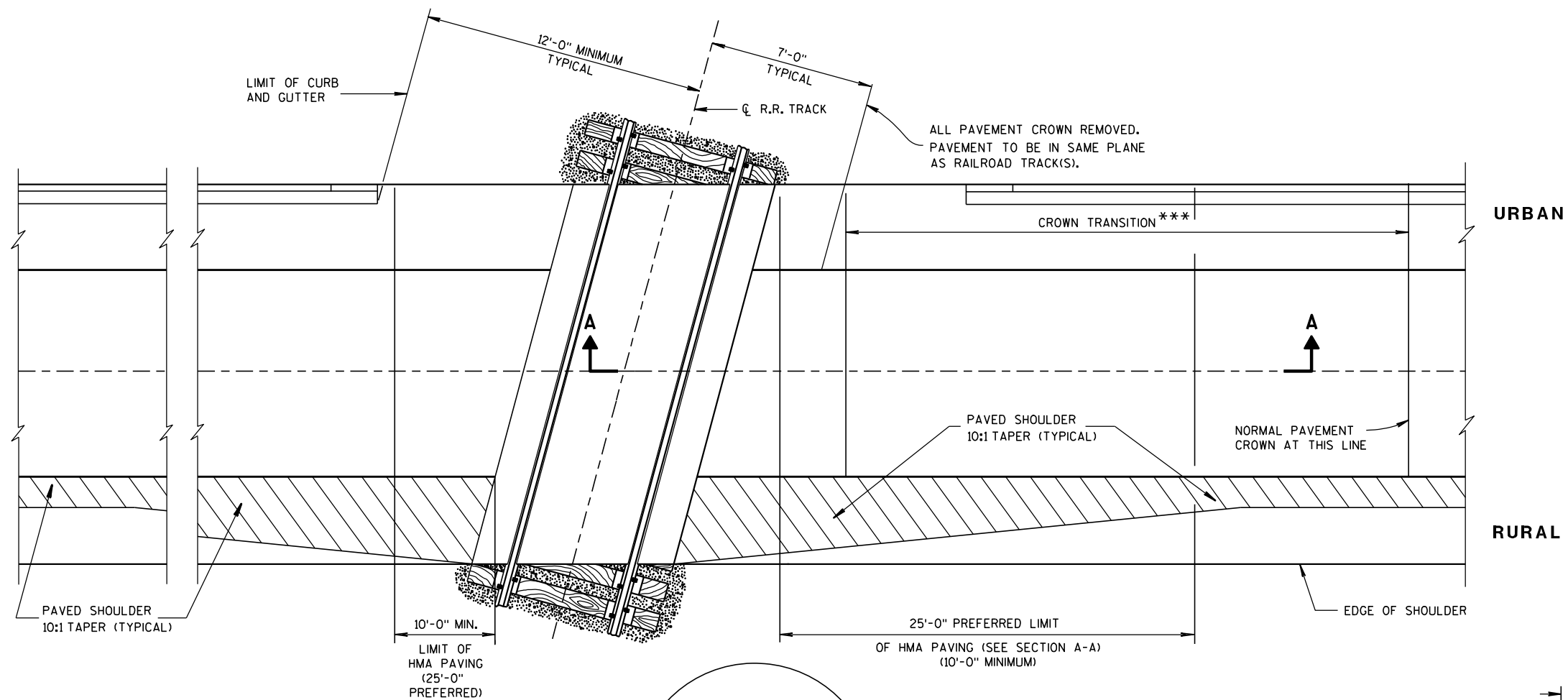
ALL SIGN BRIDGE STRUCTURES MUST ALSO HAVE STRUCTURE ID PLAQUES AS SHOWN IN THE STRUCTURE DETAILS.

CIRCUIT PLAQUES SHALL BE MOUNTED IN THE STEM WHICH HAS THE ELECTRICAL CIRCUIT, FACING TRAFFIC.

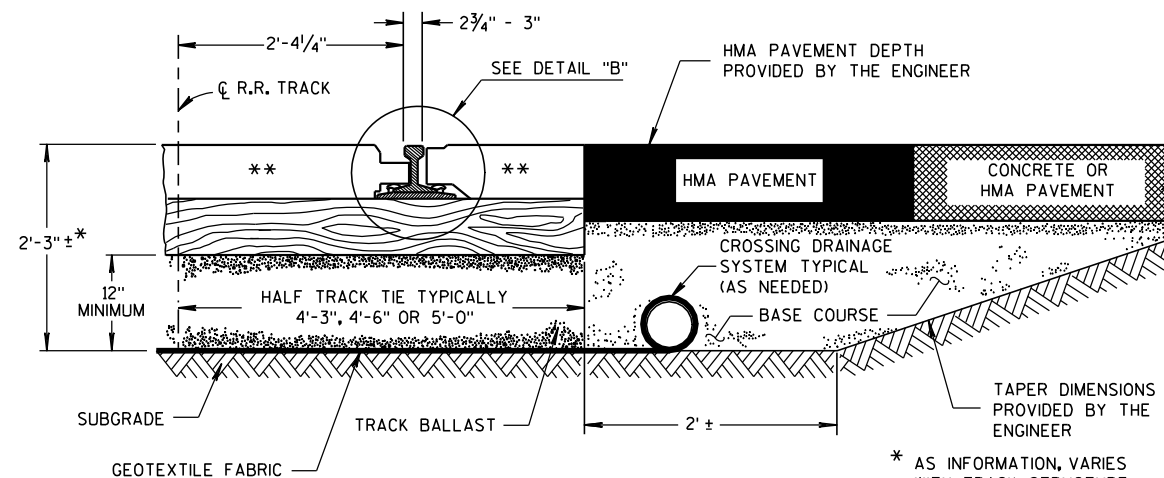
**CIRCUIT IDENTIFICATION PLAQUES
SIGN BRIDGES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

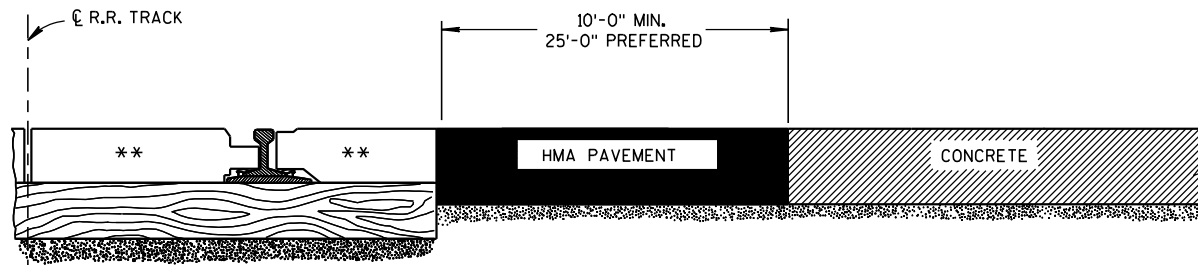
APPROVED
10/25/2010 /S/ John Corbin
DATE STATE ELECTRICAL ENGINEER FOR HWYS
FHWA



DETAIL B
HMA FLANGEWAY
AND FIELD FILLERS



TYPICAL HALF SECTION



SECTION A-A
CONCRETE PAVEMENT APPROACH



SECTION A-A
HMA PAVEMENT APPROACH

EXAMPLES OF PAVEMENT APPROACHES

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TIMBER, CONCRETE OR RUBBER CROSSING SURFACE MATERIAL, RAILS, TIES, BALLAST, GEOTEXTILE FABRIC AND CROSSING DRAINAGE SYSTEM BY OTHERS UNLESS OTHERWISE PROVIDED.

HMA PAVEMENT APPROACHES AND HMA PAVEMENT CROSSING SURFACES TO BE PLACED BY CONTRACTOR UNLESS OTHERWISE PROVIDED.

HMA FLANGEWAY AND FIELD FILLERS TO BE PLACED AND THOROUGHLY HAND COMPACTED BY THE CONTRACTOR WHEN NOT PROVIDED BY OTHERS. SEE DETAIL B. HMA FILLERS NOT REQUIRED WHEN RUBBER FILLERS ARE PROVIDED.

HMA PAVEMENT SHALL BE ROLLED PARALLEL TO THE TRACK.

** CROSSING SURFACE MAY BE TIMBER, RUBBER, CONCRETE, HMA PAVEMENT OR A COMBINATION OF SUCH MATERIALS.

*** CROWN TRANSITION LENGTH SHOWN ELSEWHERE IN THE PLAN.

PAVEMENT DETAILS FOR RAILROAD APPROACH

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

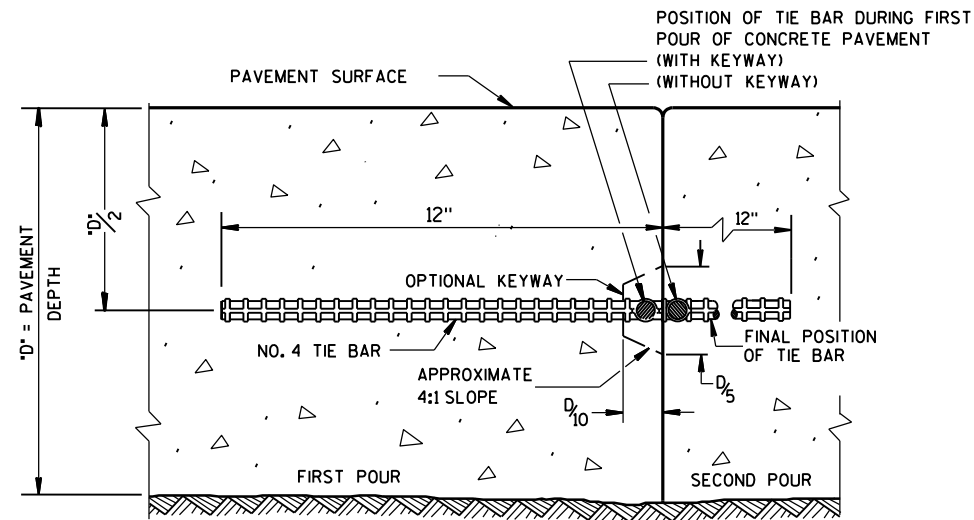
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DATE

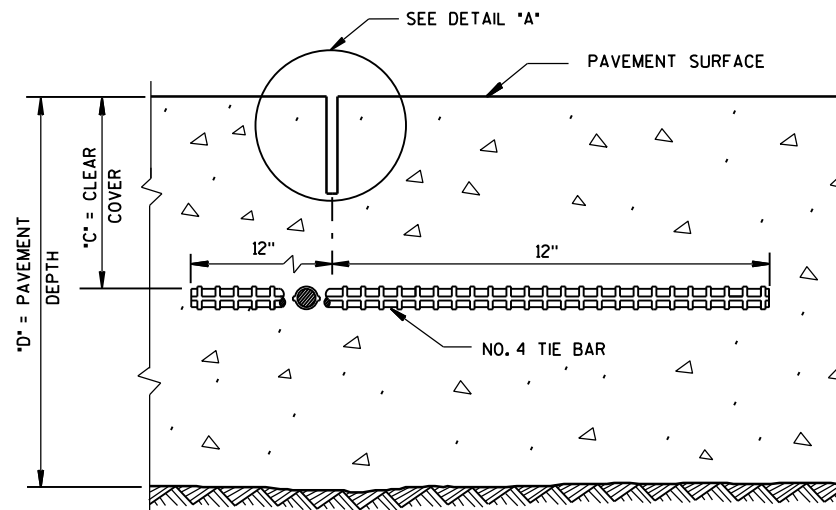
FHWA

/S/ Ronald E. Adams

CHIEF, RAILROADS & HARBORS SECTION



CONSTRUCTION JOINT



SAWED JOINT

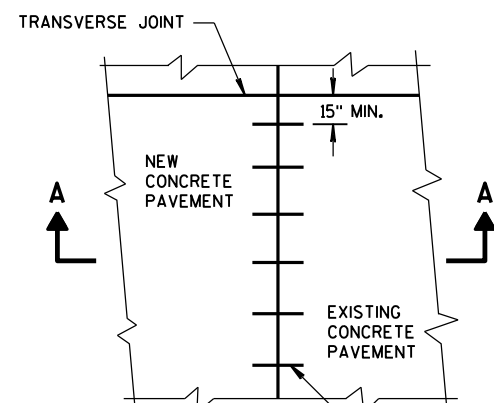
GENERAL NOTES

DO NOT SEAL OR FILL LONGITUDINAL JOINTS.

CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.

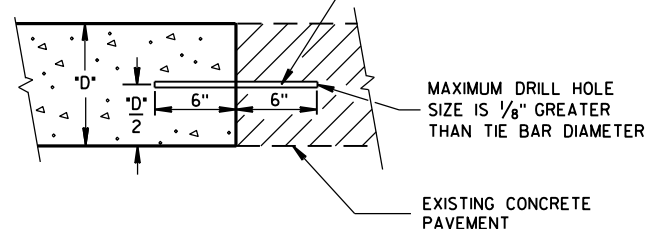
CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

- ① ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

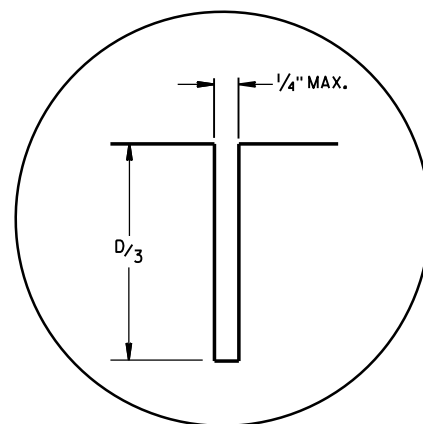


PLAN VIEW

NO. 6 TIE BARS SPACED 30" C-C, INSTALLED PERPENDICULAR TO THE LONGITUDINAL JOINT. ①



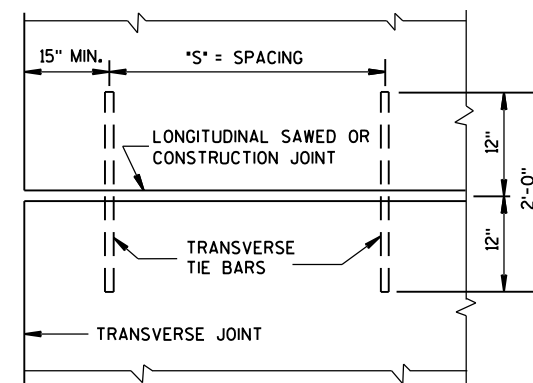
SECTION A-A
LONGITUDINAL CONSTRUCTION JOINT
TIE BARS ANCHORED
INTO EXISTING PAVEMENT



DETAIL "A"

TIE BAR TABLE

PAVEMENT DEPTH "D"	CLEAR COVER "C"	MAXIMUM TIE BAR SPACING "S"	
		PAVEMENT WIDTH 24' OR 26'	≥ 30'
6, 6 1/2"	3" ± 1/2"	48"	42"
7, 7 1/2"	3 1/4" ± 1"	45"	36"
8, 8 1/2"	3 3/4" ± 1"	39"	30"
9, 9 1/2"	4 1/4" ± 1"	33"	27"
10, 10 1/2"	4 3/4" ± 1"	30"	24"
11, 11 1/2"	5 1/4" ± 1"	27"	21"
12"	5 3/4" ± 1"	24"	21"



PLAN VIEW
SHOWING LOCATION OF TIE BARS

CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES

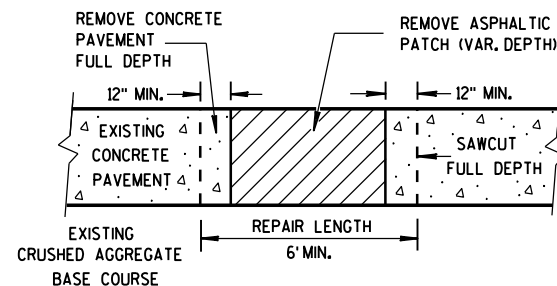
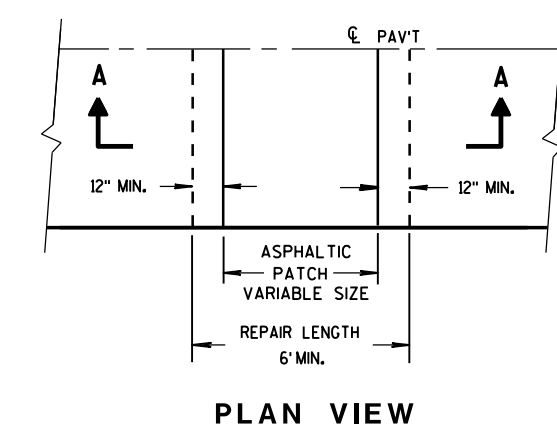
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

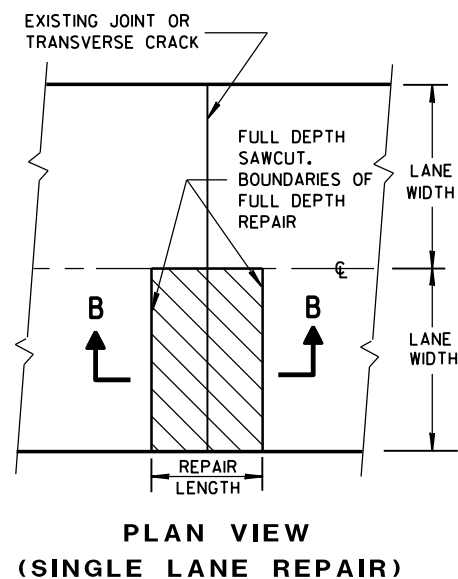
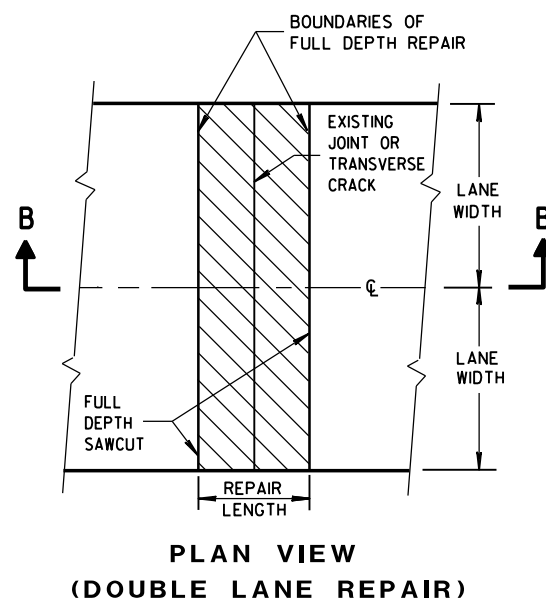
5-3-2013
DATE

/S/ Deb Bischoff
PAVEMENT POLICY & DESIGN ENGINEER

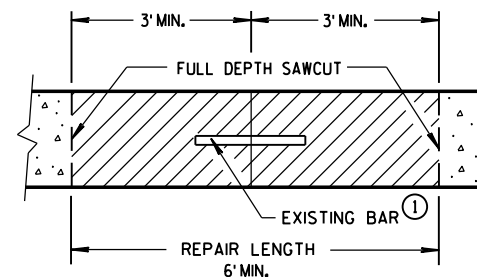
FHWA



SECTION A-A
HMA PATCH REMOVAL



FULL DEPTH CONCRETE PAVEMENT REMOVAL
(SEE NOTE)



SECTION B-B
CONCRETE REMOVAL

GENERAL NOTES

SAW CUT, DRILL, AND LIFT OUT EXISTING CONCRETE PAVEMENT WITHIN THE BOUNDARIES OF CONCRETE REPAIR AREAS. THE CONTRACTOR MAY MAKE ADDITIONAL SAW CUTS INSIDE THE REPAIR LIMITS TO REDUCE WEIGHT AND SIZE OF CONCRETE PIECES. ADDITIONAL SAW CUTS ARE NOT PAID FOR BY THE DEPARTMENT.

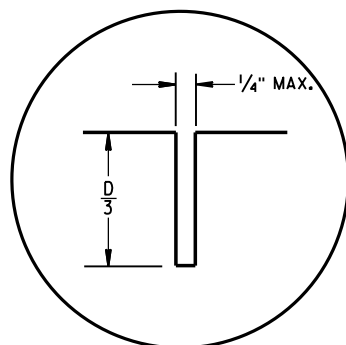
PROVIDE A 6-FOOT MINIMUM DISTANCE FROM BOUNDARIES OF CONCRETE REPAIR AREAS TO ADJACENT TRANSVERSE JOINT OR CRACK IN THE SAME LANE.

THE LENGTH OF THE REPAIRS MAY VARY FROM THE DIMENSIONS SHOWN IF THE EXISTING CONCRETE PAVEMENT IS NONDOWELED AND THE PAVEMENT IS TO BE OVERLAID AFTER REPAIRING.

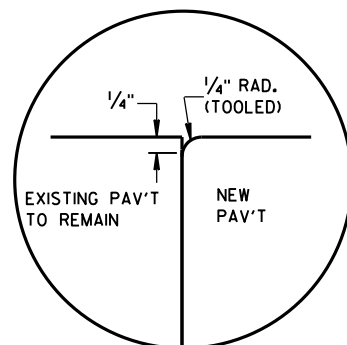
① DOWEL BARS MIGHT NOT EXIST.

TIE BAR TABLE

PAVEMENT DEPTH "D"	CLEAR COVER "C"	MAXIMUM TIE BAR SPACING "S"	
		PAVEMENT WIDTH 24' OR 26'	≥ 30'
6, 6 1/2"	3" ± 1/2"	48"	42"
7, 7 1/2"	3 1/4" ± 1"	45"	36"
8, 8 1/2"	3 3/4" ± 1"	39"	30"
9, 9 1/2"	4 1/4" ± 1"	33"	27"
10, 10 1/2"	4 3/4" ± 1"	30"	24"
11, 11 1/2"	5 1/4" ± 1"	27"	21"
12"	5 3/4" ± 1"	24"	21"

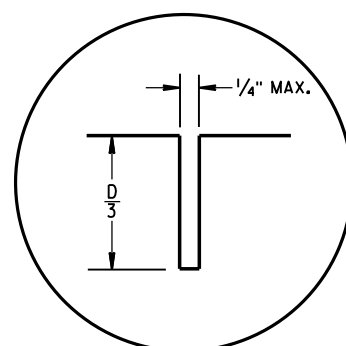


C1

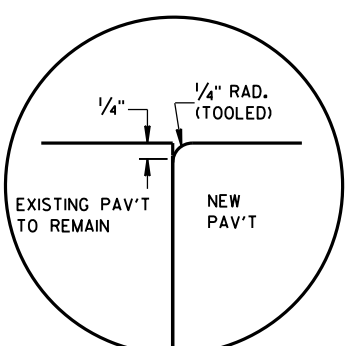


C2

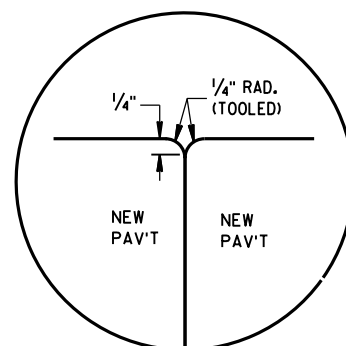
TRANSVERSE JOINTS



L1

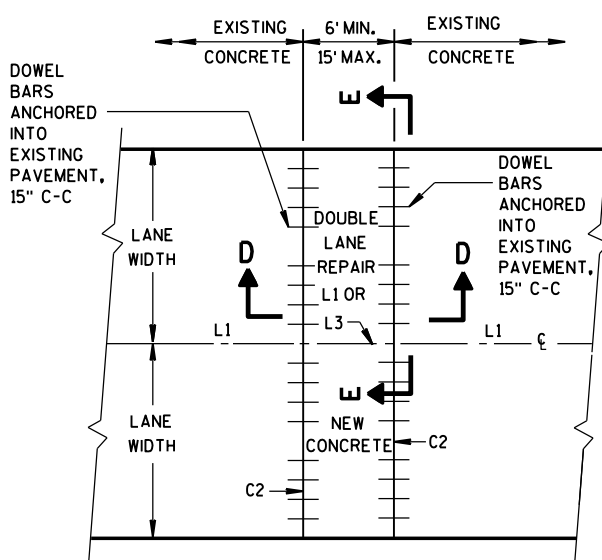


L2



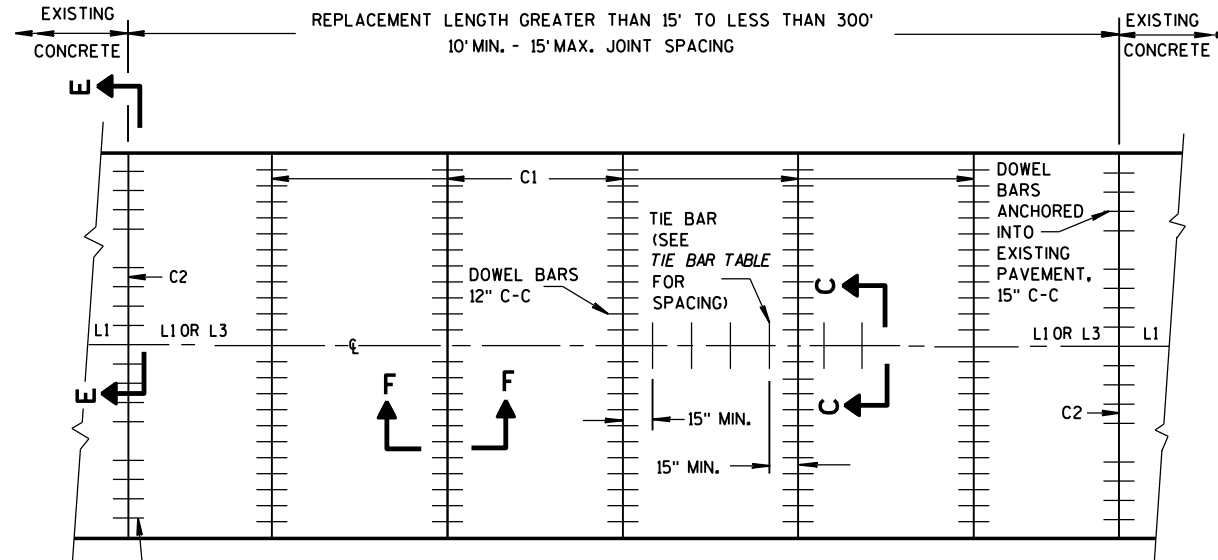
L3

LONGITUDINAL JOINTS



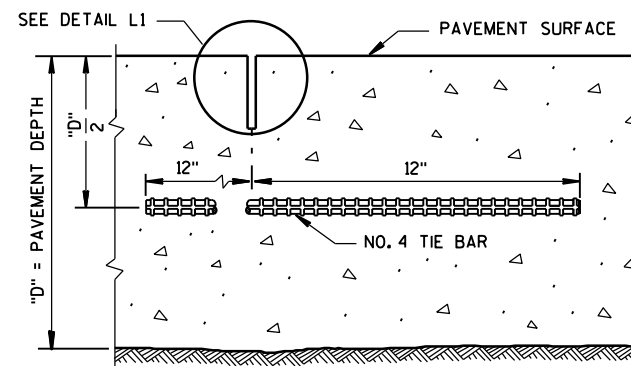
PLAN VIEW

MULTI-LANE CONCRETE PAVEMENT REPAIR



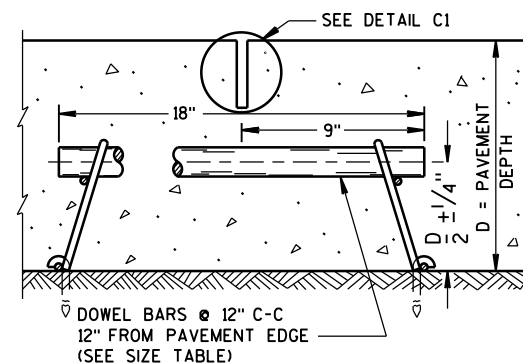
PLAN VIEW

MULTI-LANE CONCRETE PAVEMENT REPLACEMENT



SECTION C-C

SAWED LONGITUDINAL JOINT

SECTION F-F
CONTRACTION JOINT

GENERAL NOTES

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

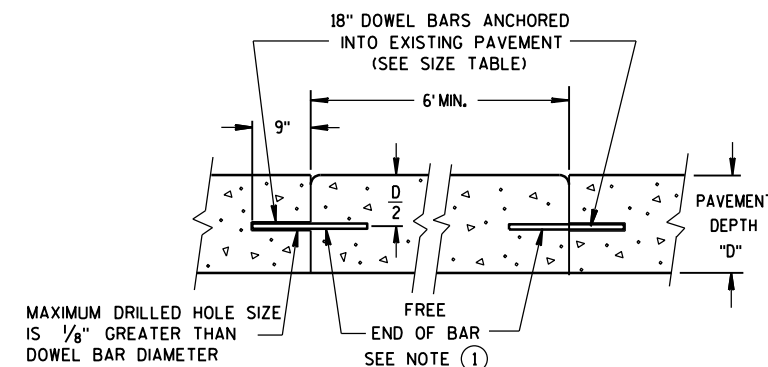
CONCRETE PAVEMENT REPAIRS OF EXISTING NONDOWELED CONCRETE PAVEMENTS DO NOT NEED TO BE DOWELED.

DO NOT SEAL OR FILL JOINTS.

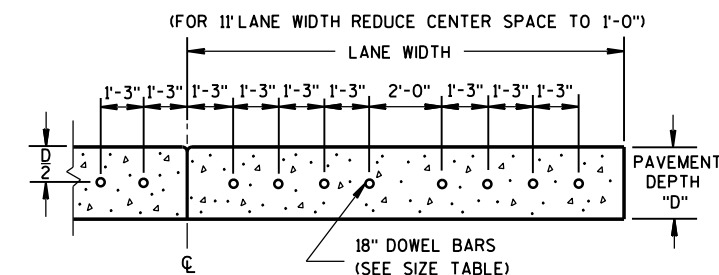
ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

FOR MULTI-LANE CONCRETE PAVEMENT REPLACEMENTS, PROVIDE A MINIMUM DISTANCE OF 15 INCHES FROM ALL TRANSVERSE JOINTS OR EDGES OF REPLACEMENT TO THE CENTER OF THE TIE BAR NEAREST THAT JOINT OR EDGE.

- ① APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.



SECTION D-D



SECTION E-E

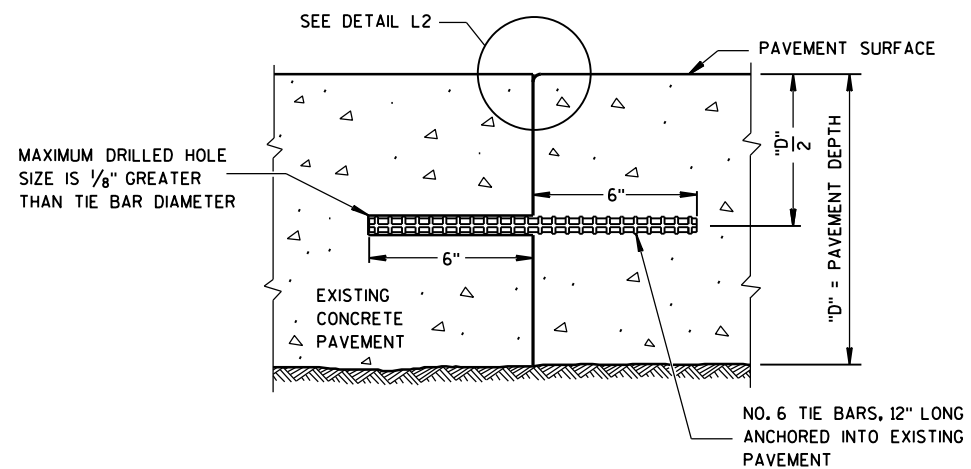
DRILLED DOWEL BAR CONSTRUCTION JOINT

PAVEMENT DEPTH, DOWEL BAR SIZE
AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
5 1/2", 6, 6 1/2"	NONE	12'
7", 7 1/2"	1"	14'
8", 8 1/2"	1 1/4"	15'
9", 9 1/2"	1 1/4"	15'
10" & ABOVE	1 1/2"	15'

CONCRETE PAVEMENT
REPAIR AND REPLACEMENT

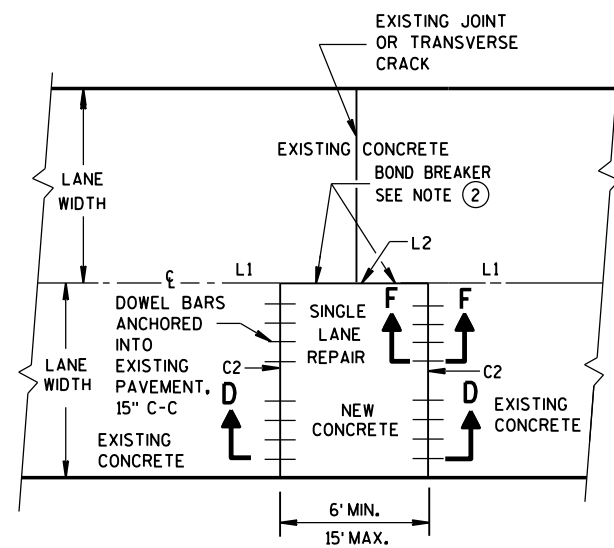
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



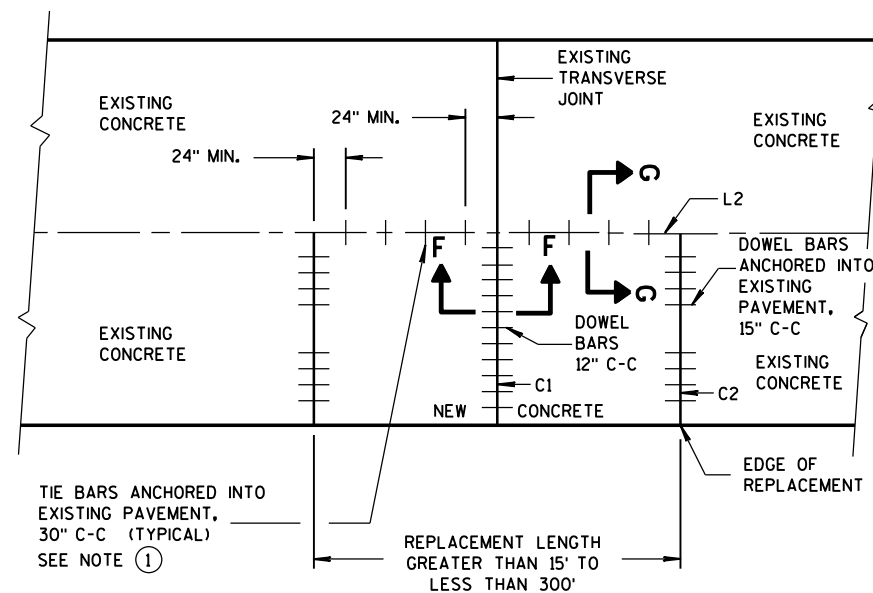
SECTION G-G
TIE BARS ANCHORED
INTO EXISTING PAVEMENT

GENERAL NOTES

- ① WITH THE APPROVAL OF THE ENGINEER, FOR SINGLE LANE PAVEMENT REPLACEMENTS LESS THAN 30 FEET IN LENGTH, THE CONTRACTOR MAY INSTALL DRILLED TIE BARS ON 6:1 SKEW HORIZONTALLY, DIRECTION OF SKEW ALTERNATING WITH EACH SUCCESSIVE BAR. DRIVE SKEWED TIE BARS TO A DEPTH OF 6 INCHES AND TO SUCH A DIAMETER AS TO PROVIDE A TIGHT DRIVEN FIT.
- ② USE AN ENGINEER-APPROVED BOND BREAKER (E.G. RELEASE AGENT, CURING COMPOUND) FOR SINGLE LANE REPAIRS UP TO 15 FEET IN LENGTH.



PLAN VIEW
SINGLE LANE
CONCRETE PAVEMENT REPAIR



PLAN VIEW
SINGLE LANE
CONCRETE PAVEMENT REPLACEMENT

CONCRETE PAVEMENT REPAIR AND REPLACEMENT

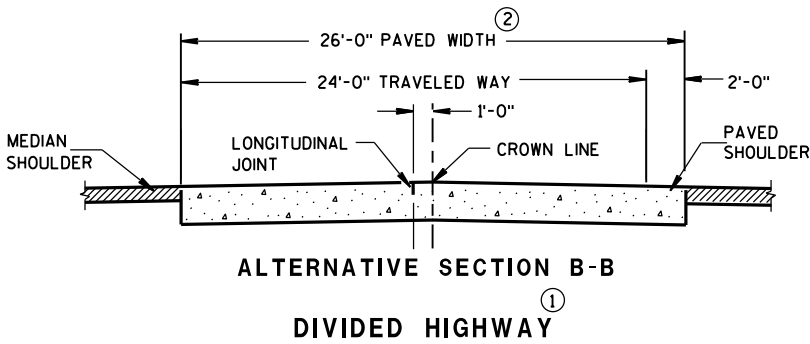
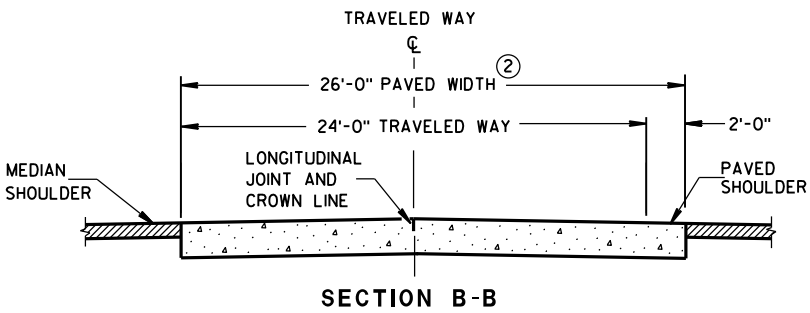
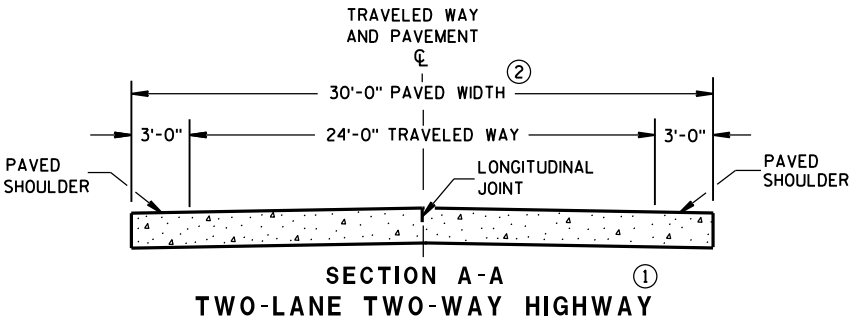
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

12-2013
DATE

FHWA

/S/ Deb Bischoff
PAVEMENT POLICY & DESIGN ENGINEER



GENERAL NOTES

CONTRACTION JOINTS

CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT SEAL OR FILL CONTRACTION JOINTS.

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES AND A MAXIMUM OF 18 INCHES FROM THE FREE EDGE OF PAVEMENT.

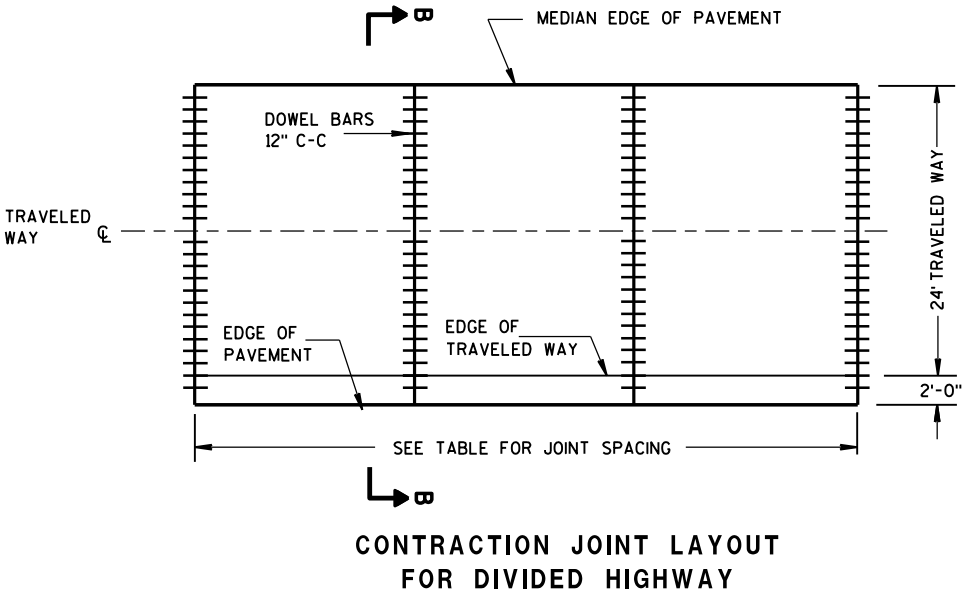
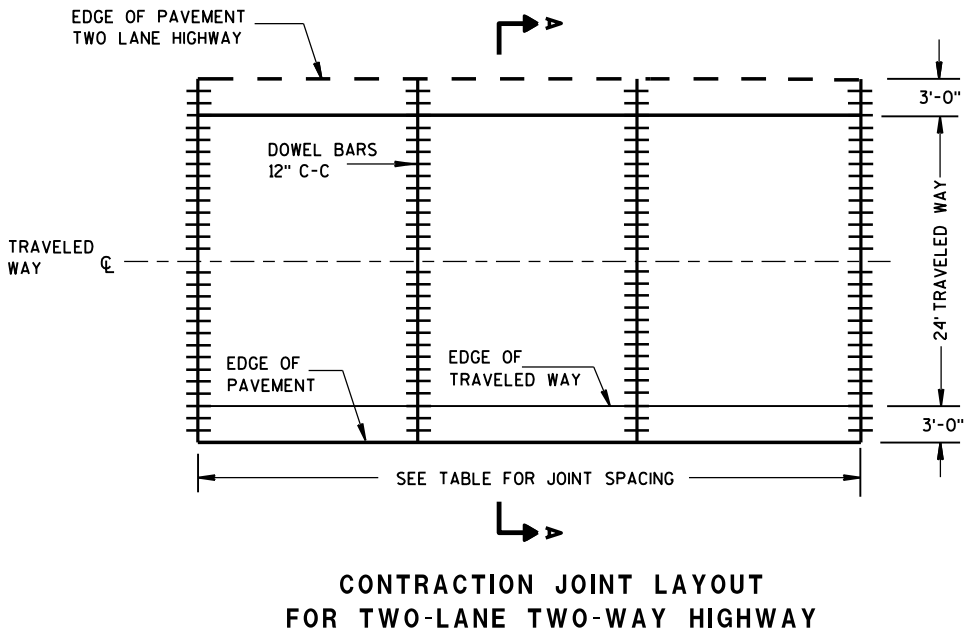
CONSTRUCTION JOINTS

LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.

- ① REFER TO TYPICAL CROSS SECTIONS FOR ADDITIONAL DETAILS.
- ② MEASURE THE ENTIRE PAVED WIDTH INCLUDING THE PORTION(S) LABELED PAVED SHOULDER AS CONCRETE PAVEMENT.

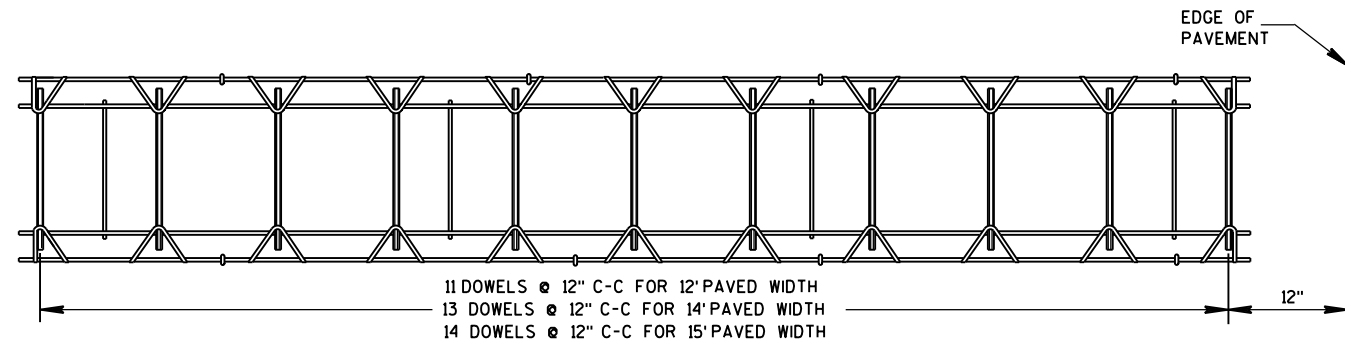
PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
5 1/2", 6", 6 1/2"	NONE	12'
7", 7 1/2"	1"	14'
8", 8 1/2"	1 1/4"	15'
9", 9 1/2"	1 1/4"	15'
10" & ABOVE	1 1/2"	15'

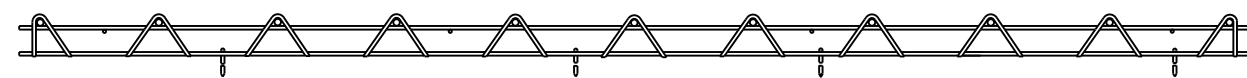


RURAL DOWELED
CONCRETE PAVEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



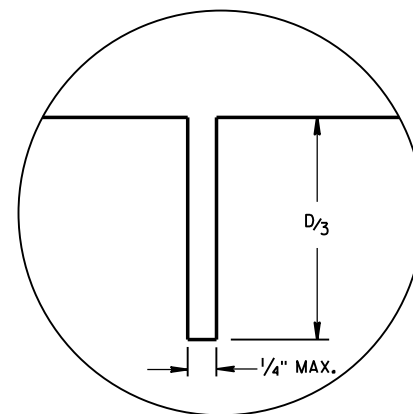
PLAN VIEW



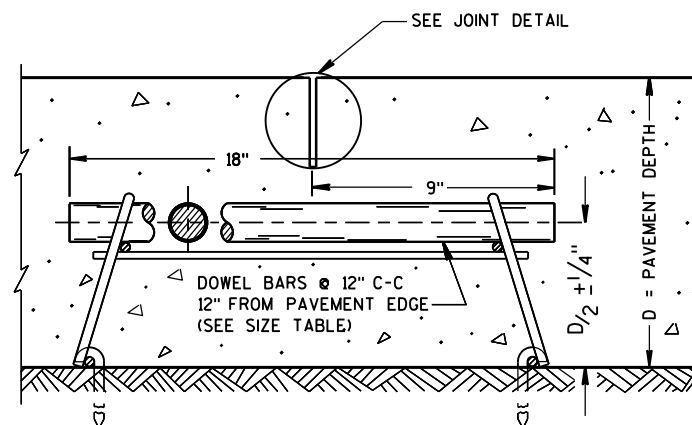
SIDE VIEW

(NORMAL TO CENTERLINE)

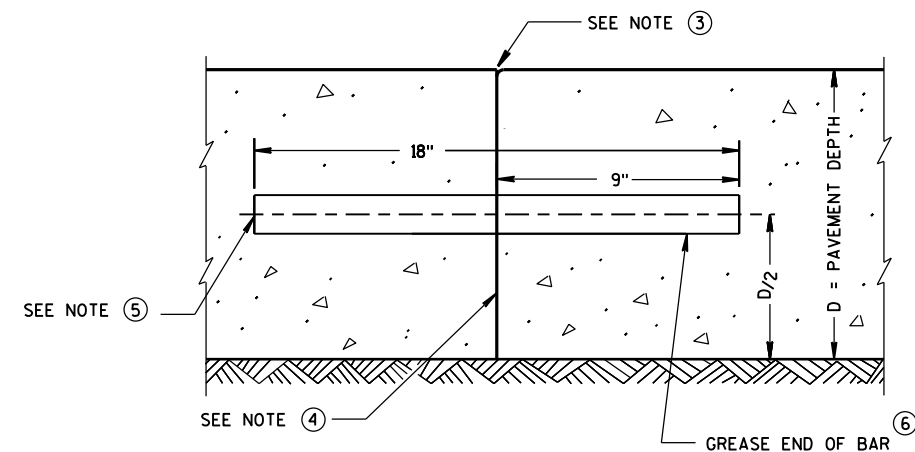
CONTRACTION JOINT DOWEL ASSEMBLY ①



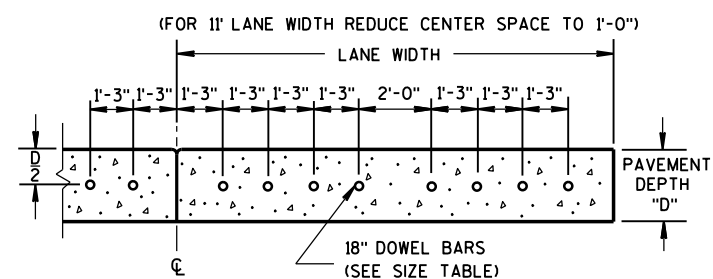
JOINT DETAIL



DOWELED CONTRACTION JOINT



TRANSVERSE CONSTRUCTION JOINT



DRILLED DOWEL BAR CONSTRUCTION JOINT ⑦

GENERAL NOTES

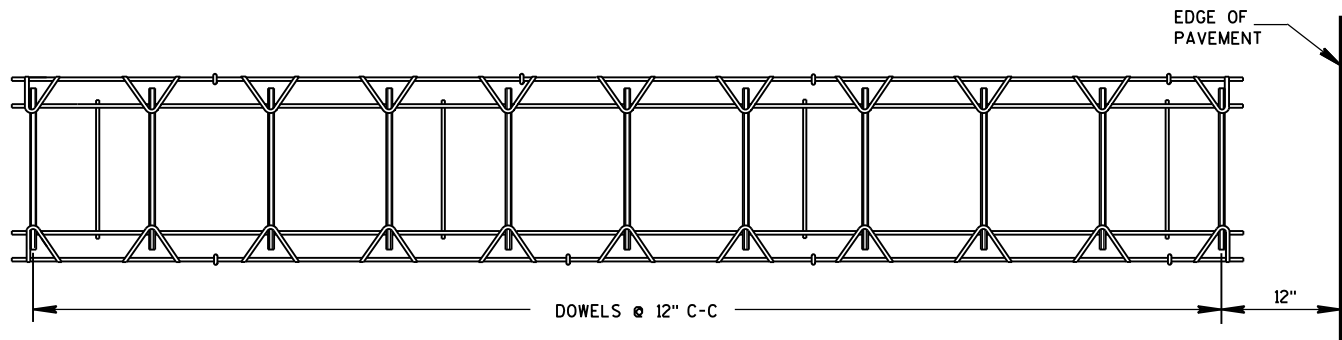
- ① OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTING CONTRACTION JOINTS.
- ② SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION, NUMBER AND LENGTH OF ANCHORS ARE DEPENDENT UPON FIELD CONDITIONS.
- ③ FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A $1/4$ -INCH RADIUS AT FORMED JOINTS.
- ④ PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.
- ⑤ INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING. INSTALL FORMED DOWEL BARS 12 INCHES C-C AND 12 INCHES FROM PAVEMENT EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL BARS ACCORDING TO *DRILLED DOWEL BAR CONSTRUCTION JOINT* DETAIL.
- ⑥ APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.
- ⑦ ANCHOR DOWEL BARS INTO DRILLED HOLES WITH AN EPOXY. MAXIMUM DRILLED HOLE SIZE IS $1/8$ -INCH GREATER THAN DOWEL BAR DIAMETER, 9 INCHES IN LENGTH.

RURAL DOWELED
CONCRETE PAVEMENTSTATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

5/3/2013
DATE/S/ Deb Bischoff
PAVEMENT POLICY & DESIGN ENGINEER

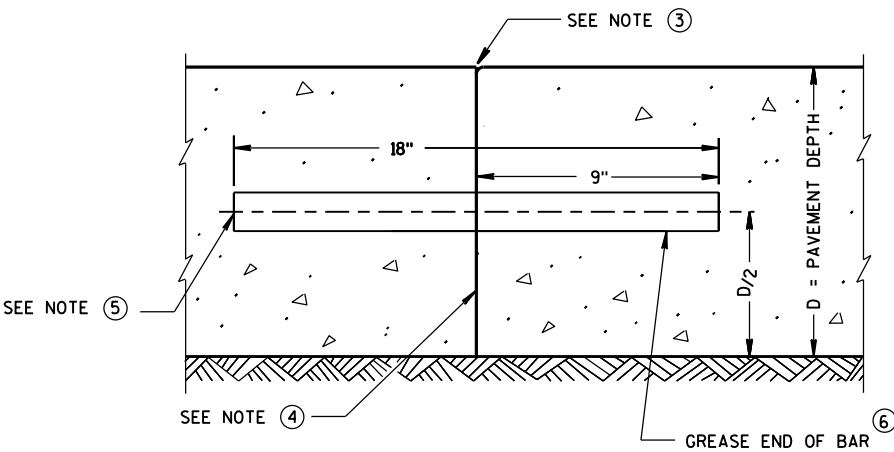
FHWA



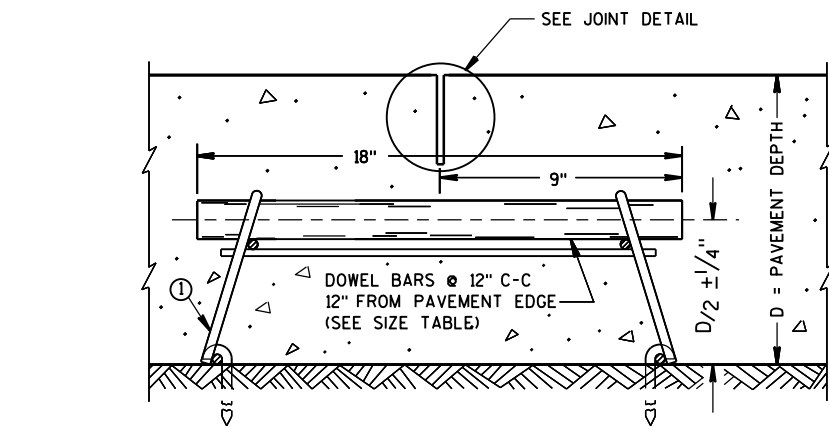
PLAN VIEW



SIDE VIEW
CONTRACTION JOINT DOWEL ASSEMBLY



TRANSVERSE CONSTRUCTION JOINT



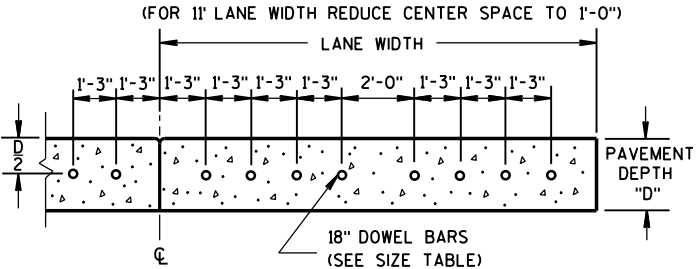
DOWELED CONTRACTION JOINT

PAVEMENT DEPTH, DOWEL BAR SIZE
AND JOINT SPACING TABLE

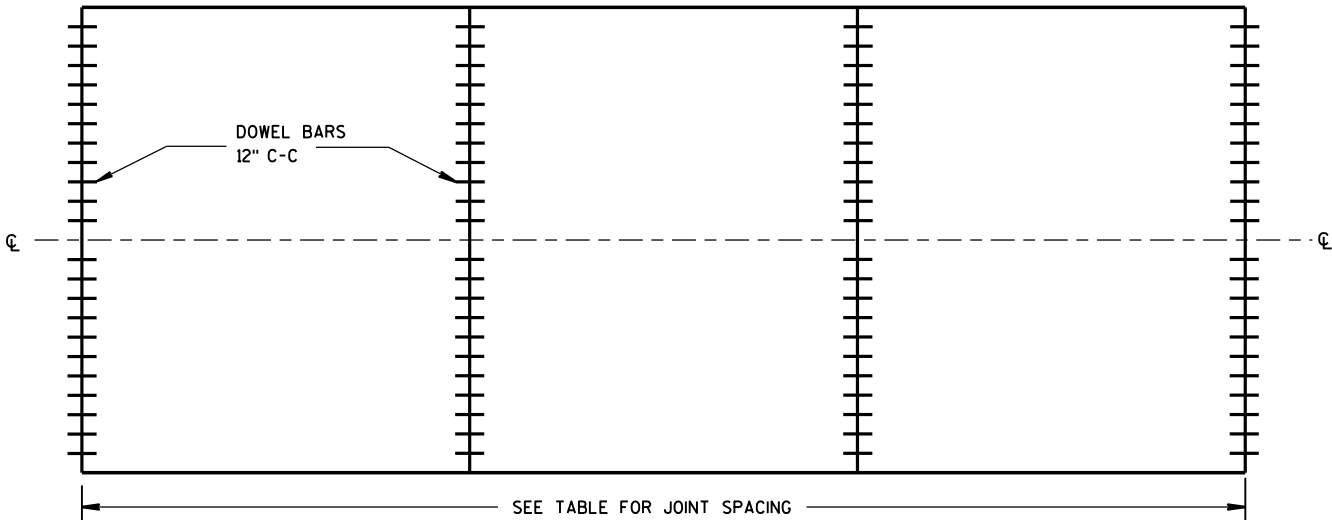
PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
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GENERAL NOTES

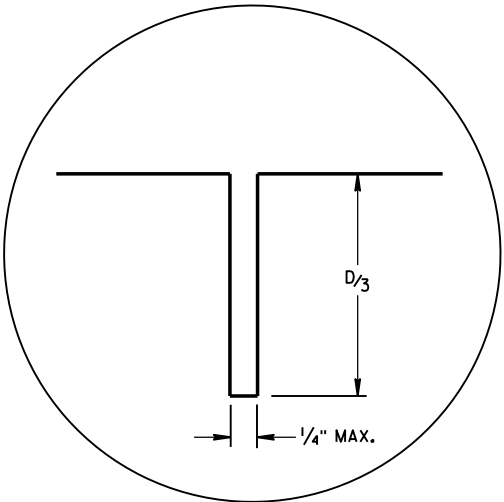
- CONTRACTION JOINTS**
- CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- DO NOT SEAL OR FILL CONTRACTION JOINTS.
- INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.
- FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES AND A MAXIMUM OF 18 INCHES FROM THE LONGITUDINAL JOINT AND THE FREE EDGE OF PAVEMENT.
- CONSTRUCTION JOINTS**
- LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.
- OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTING CONTRACTION JOINTS.
 - SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION, NUMBER AND LENGTH OF ANCHORS ARE DEPENDENT UPON FIELD CONDITIONS.
 - FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A 1/4-INCH RADIUS AT FORMED JOINTS.
 - PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.
 - INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING. INSTALL FORMED DOWEL BARS 12 INCHES C-C AND 12 INCHES FROM PAVEMENT EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL BARS ACCORDING TO *DRILLED DOWEL BAR CONSTRUCTION JOINT* DETAIL.
 - APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.
 - ANCHOR DOWEL BARS INTO DRILLED HOLES WITH AN EPOXY. MAXIMUM DRILLED HOLE SIZE IS 1/8-INCH GREATER THAN DOWEL BAR DIAMETER, 9 INCHES IN LENGTH.



DRILLED DOWEL BAR CONSTRUCTION JOINT



CONTRACTION JOINT LOCATIONS

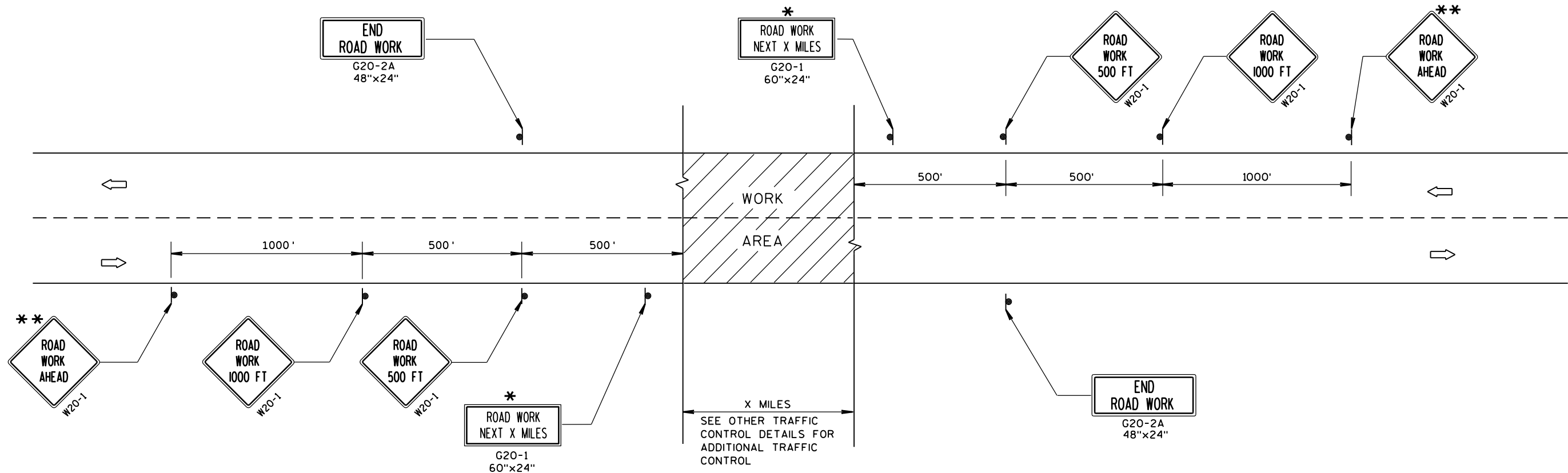


JOINT DETAIL

URBAN DOWELED
CONCRETE PAVEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5/3/2013 /S/ Deb Bischoff
DATE PAVEMENT POLICY & DESIGN ENGINEER
FHWA



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

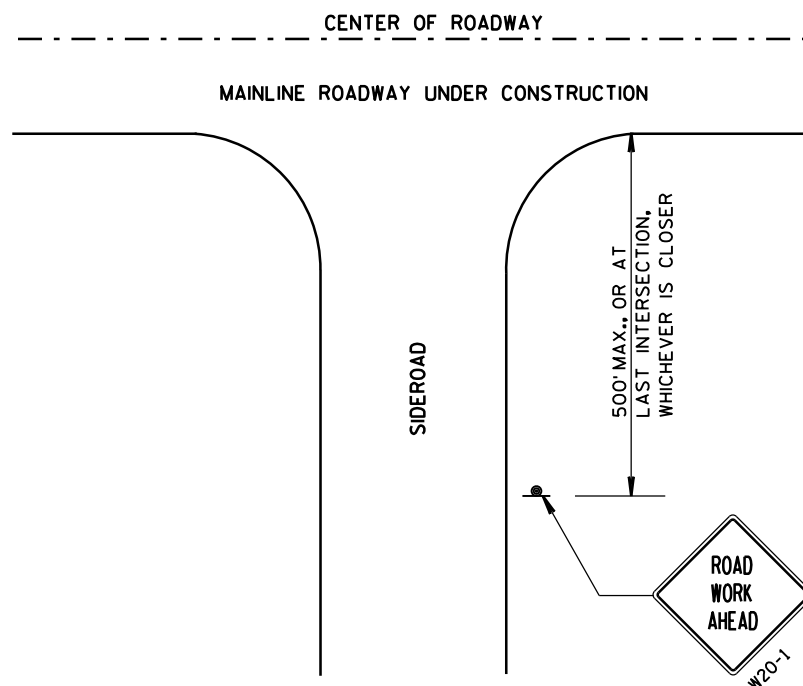
ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

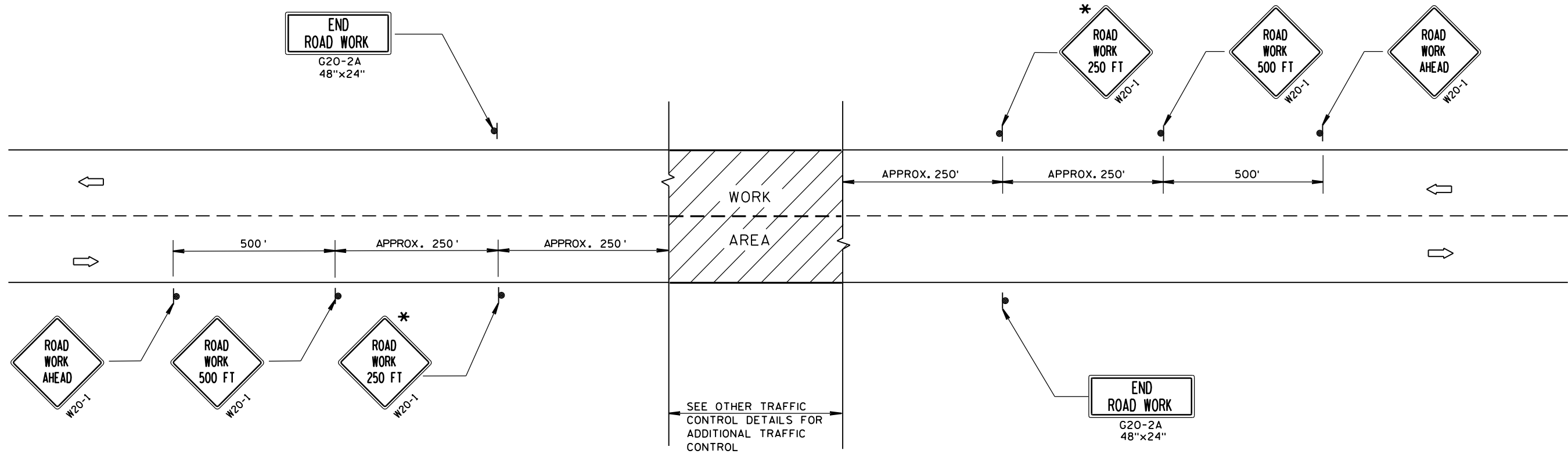
** PLACE ADDITIONAL W20-1 "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

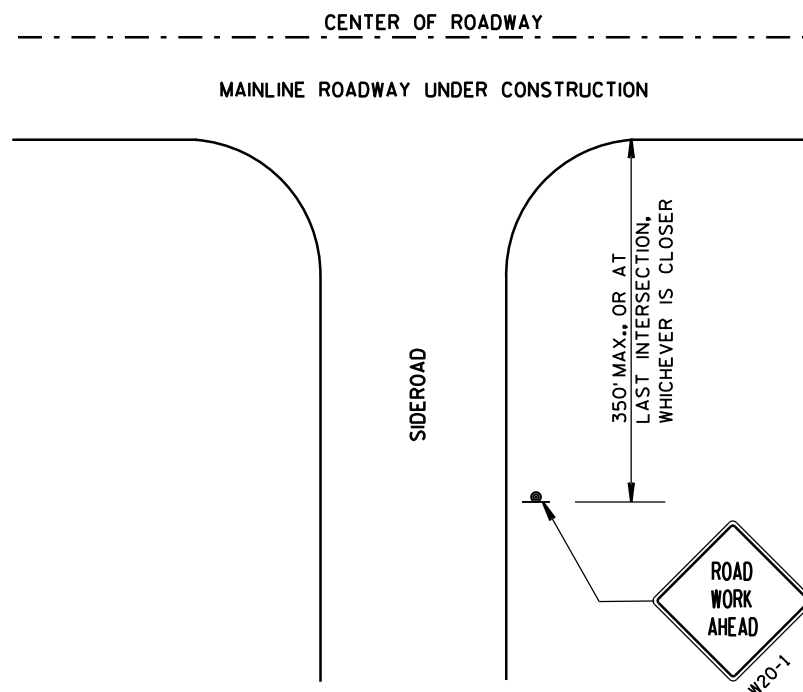
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL, ADVANCE
WARNING SIGNS 40 M.P.H.
OR LESS TWO-WAY UNDIVIDED
ROAD OPEN TO TRAFFIC

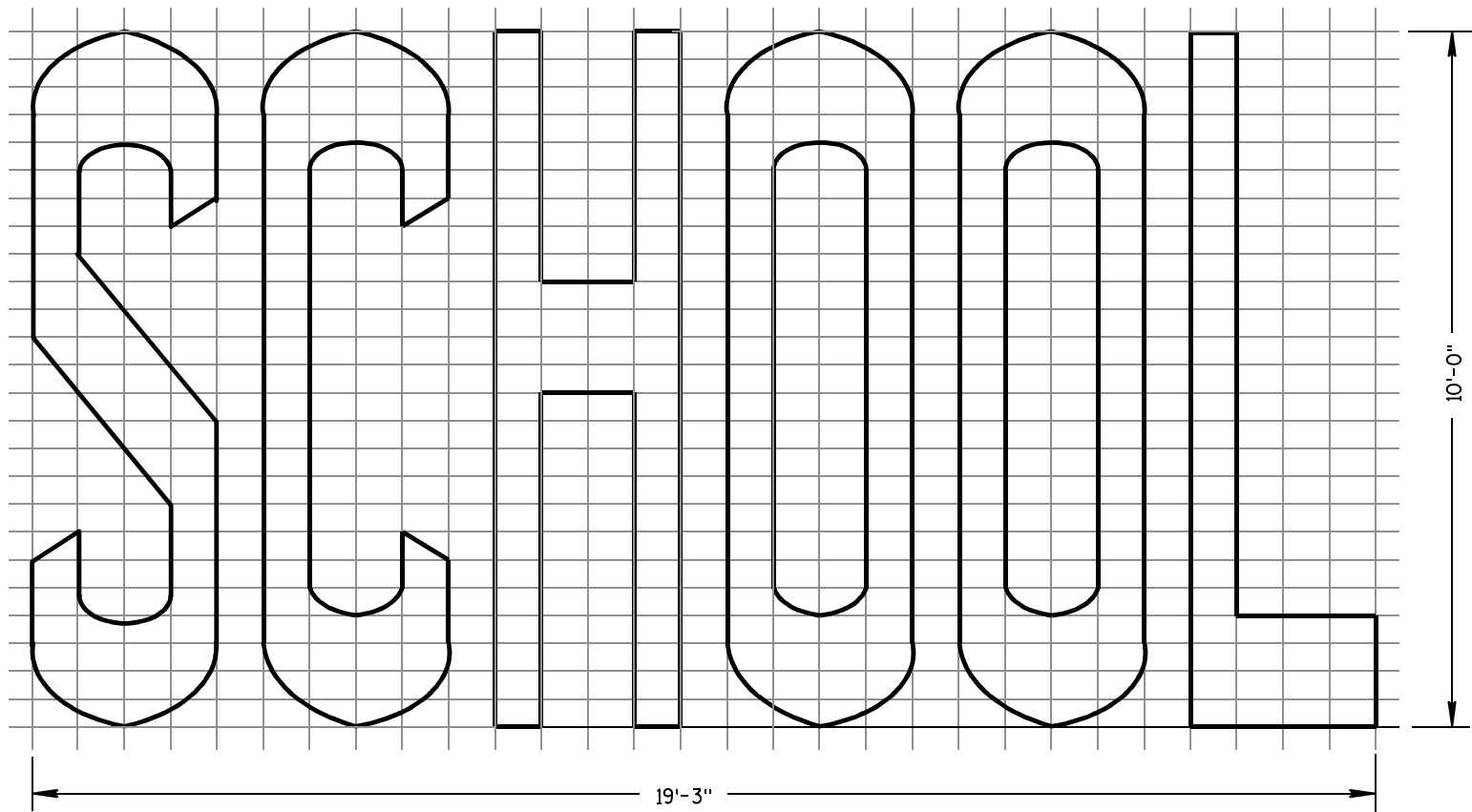
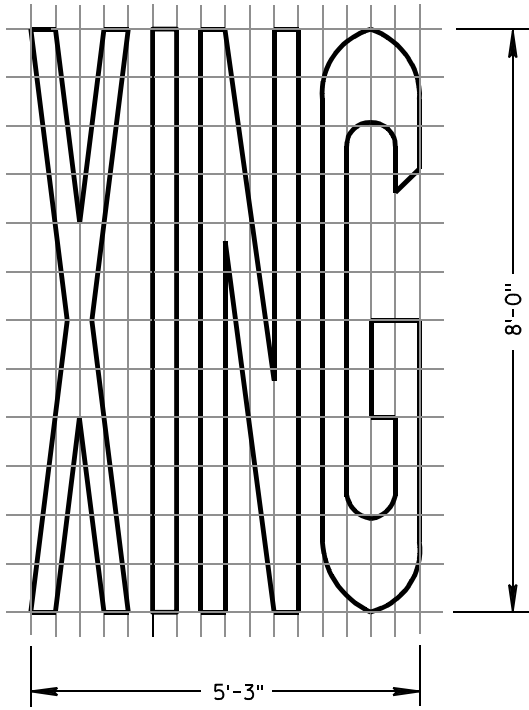
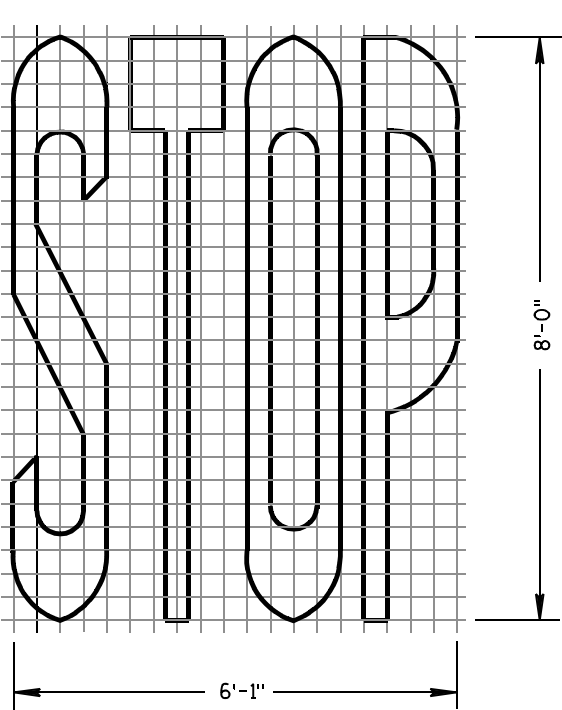
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

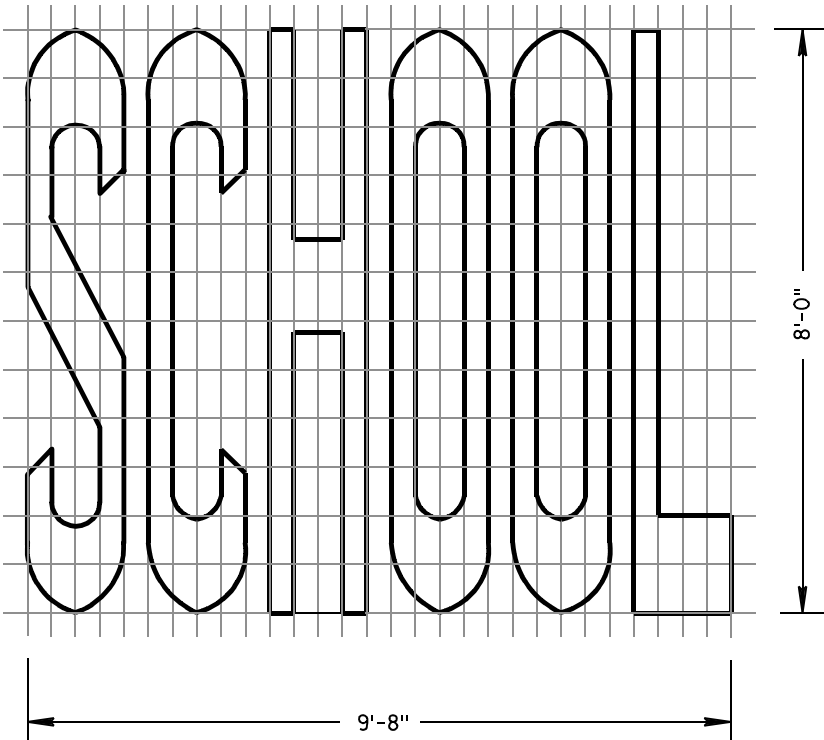
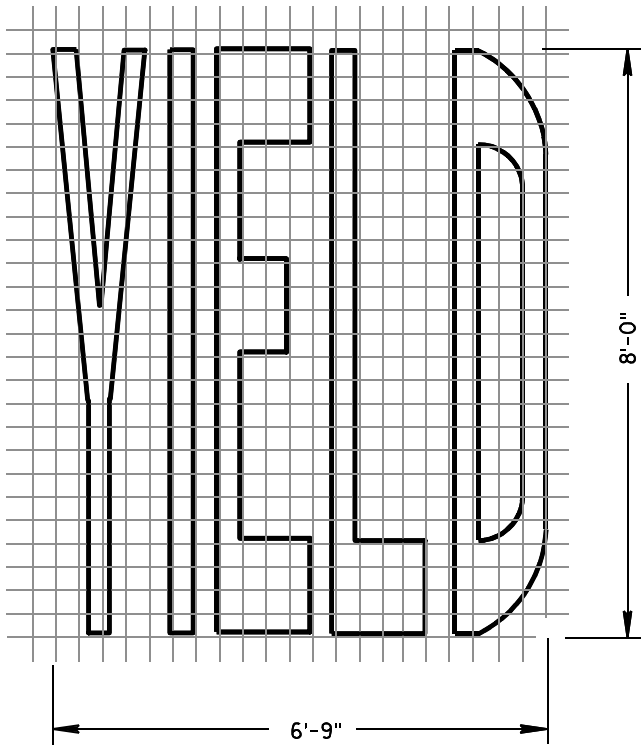
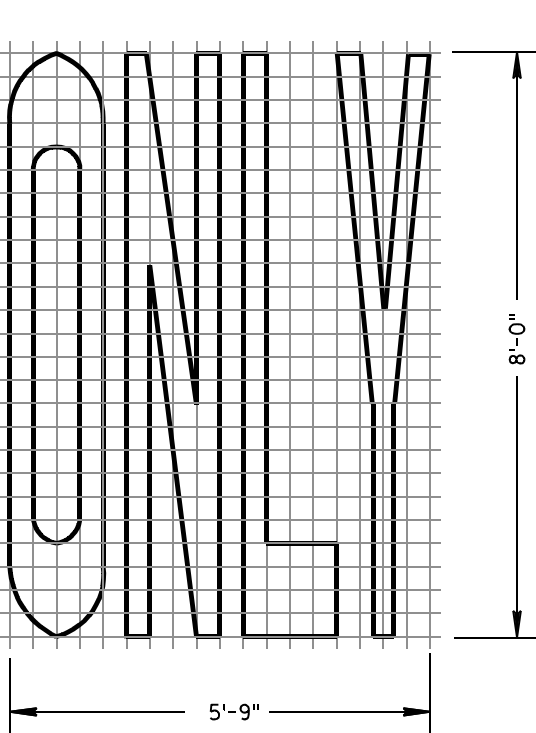
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

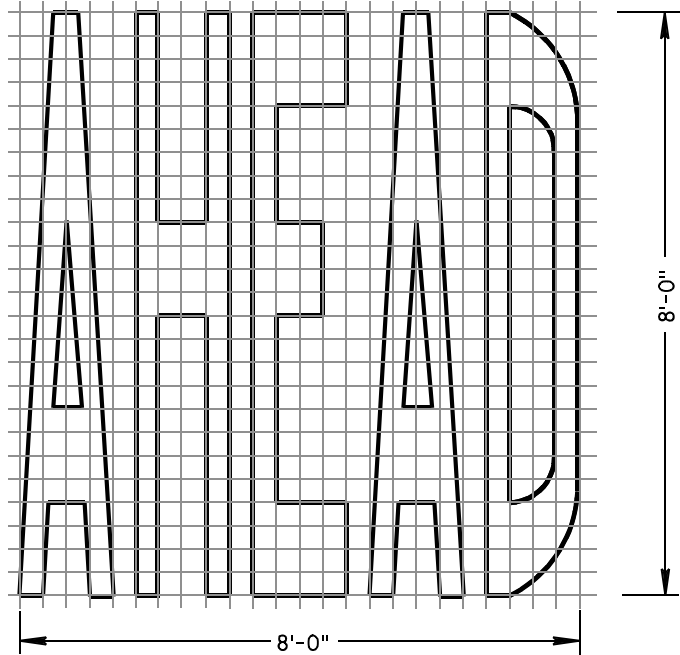
ALL LETTERS, ARROWS AND SYMBOLS SHALL BE IN CONFORMANCE WITH REQUIREMENTS INCLUDED IN "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKING" BOOK BY THE FEDERAL HIGHWAY ADMINISTRATION. ALL LETTERS, ARROWS AND SYMBOLS SHALL BE WHITE AND REFLECTORIZED. SMALL DIFFERENCES IN DIMENSIONS WITHIN THE TOLERANCES OF THAT BOOK ARE ACCEPTABLE.



TWO-LANE



SINGLE-LANE



PAVEMENT MARKING WORDS

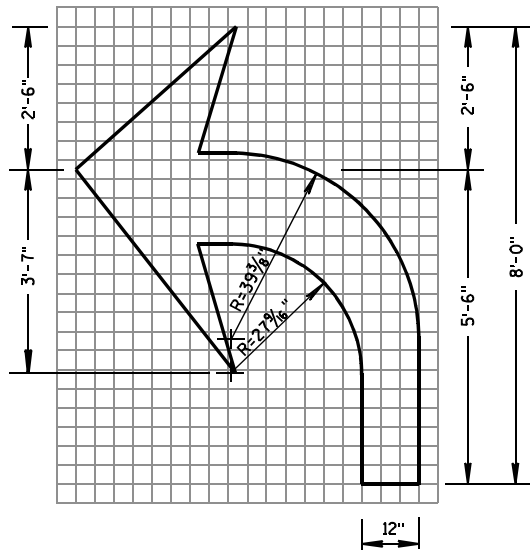
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

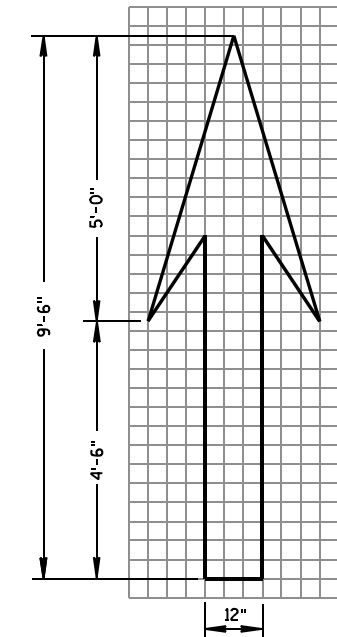
7-1-11
DATE

/S/ Thomas N. Notbohm
STATE TRAFFIC ENGINEER OF DESIGN

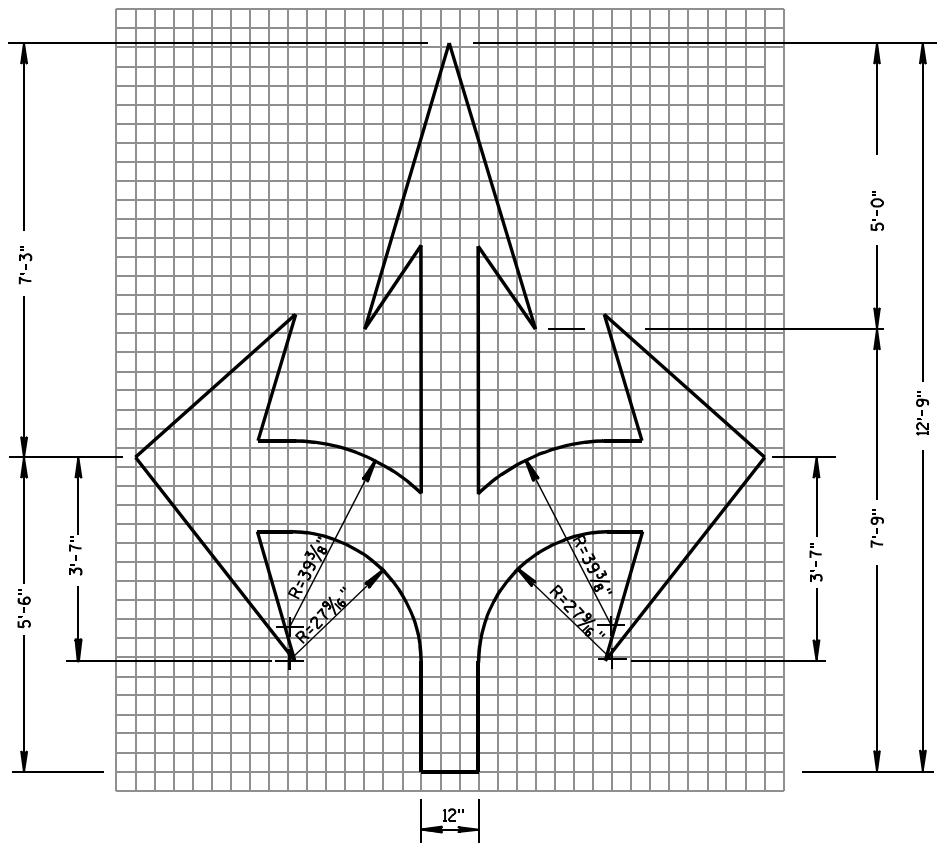
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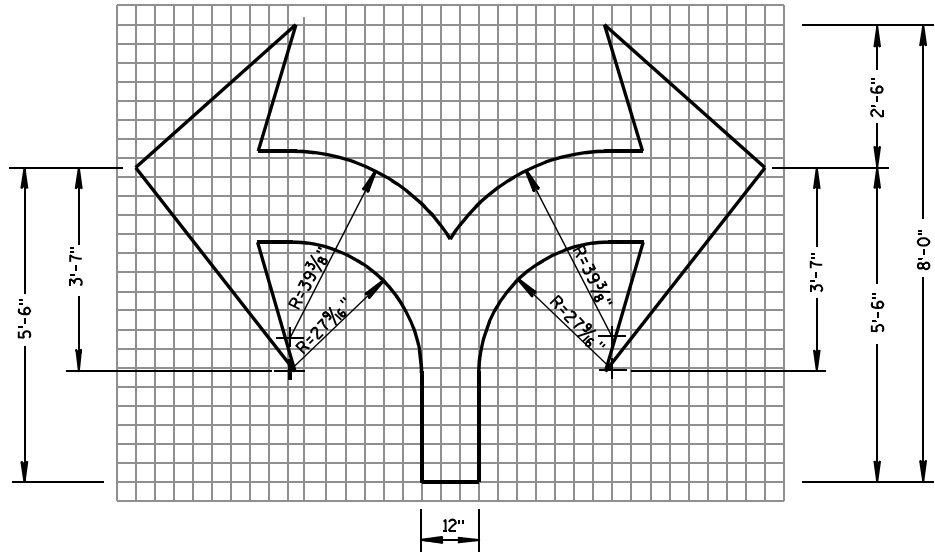
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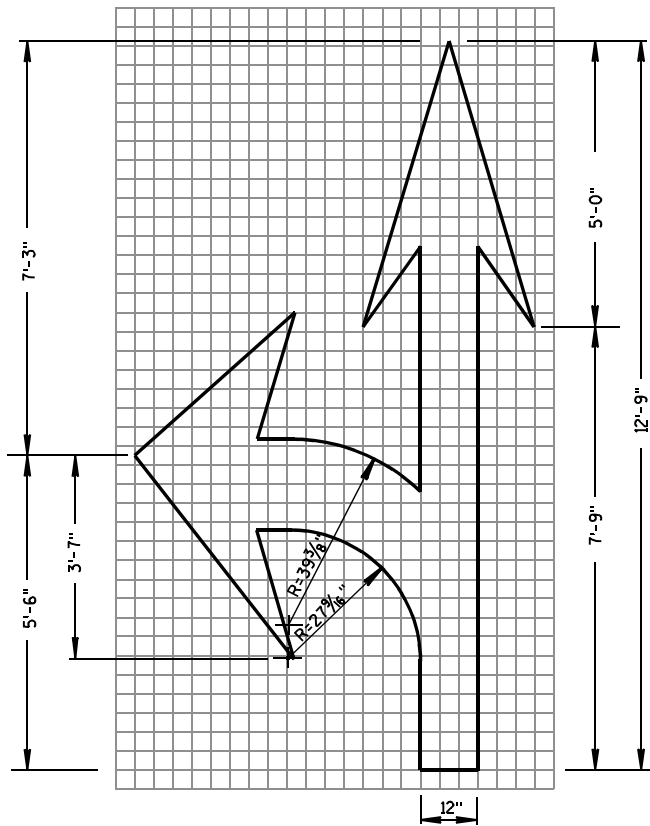
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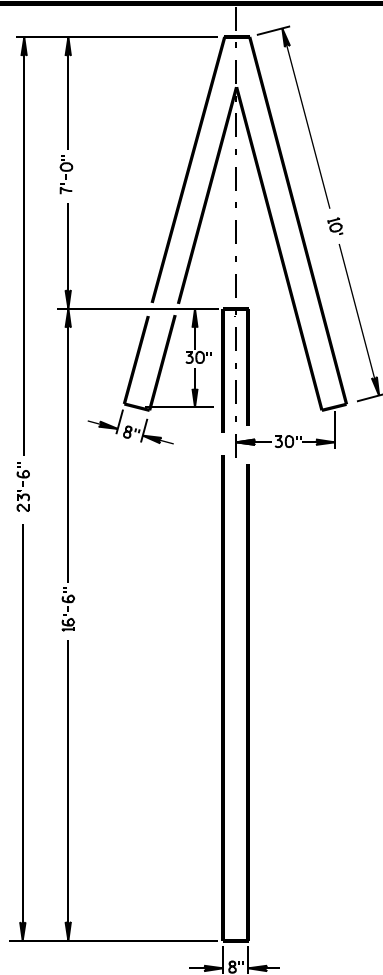
TYPE 6



TYPE 7



TYPE 3

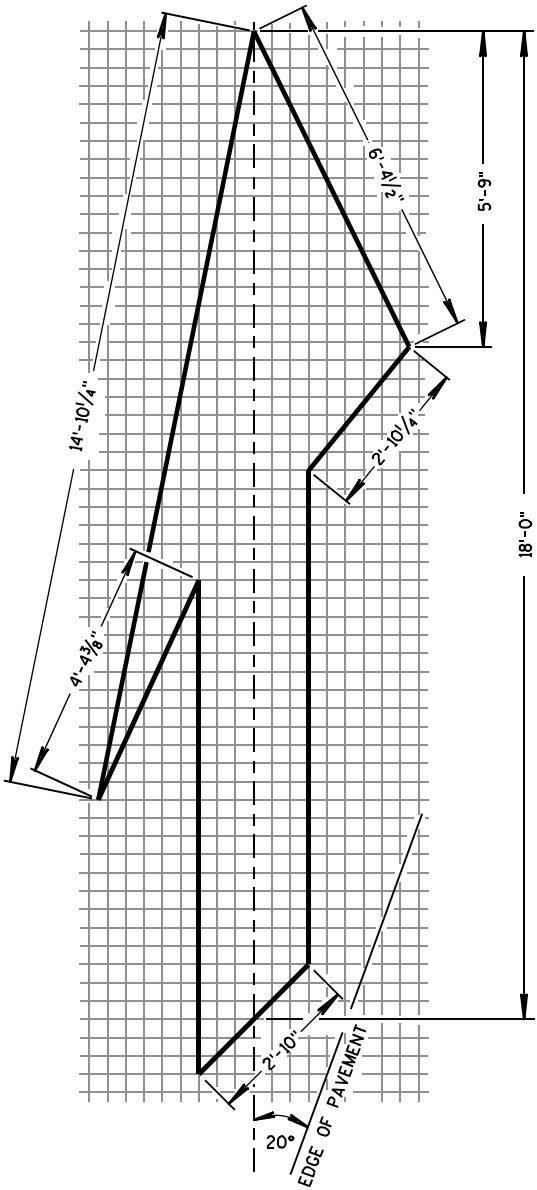


TYPE 4

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

ALL LETTERS, ARROWS AND SYMBOLS SHALL BE IN CONFORMANCE WITH REQUIREMENTS INCLUDED IN "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKING" BOOK BY THE FEDERAL HIGHWAY ADMINISTRATION. ALL LETTERS, ARROWS AND SYMBOLS SHALL BE WHITE AND REFLECTORIZED. SMALL DIFFERENCES IN DIMENSIONS WITHIN THE TOLERANCES OF THAT BOOK ARE ACCEPTABLE.



TYPE 5 LANE DROP ARROW

PAVEMENT MARKING ARROWS

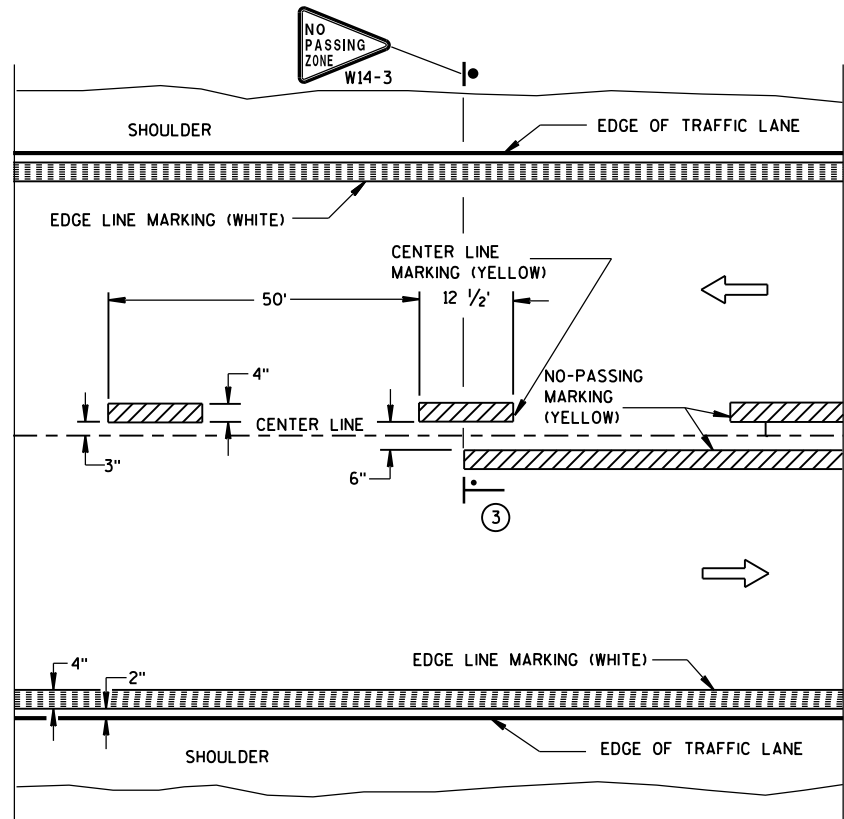
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

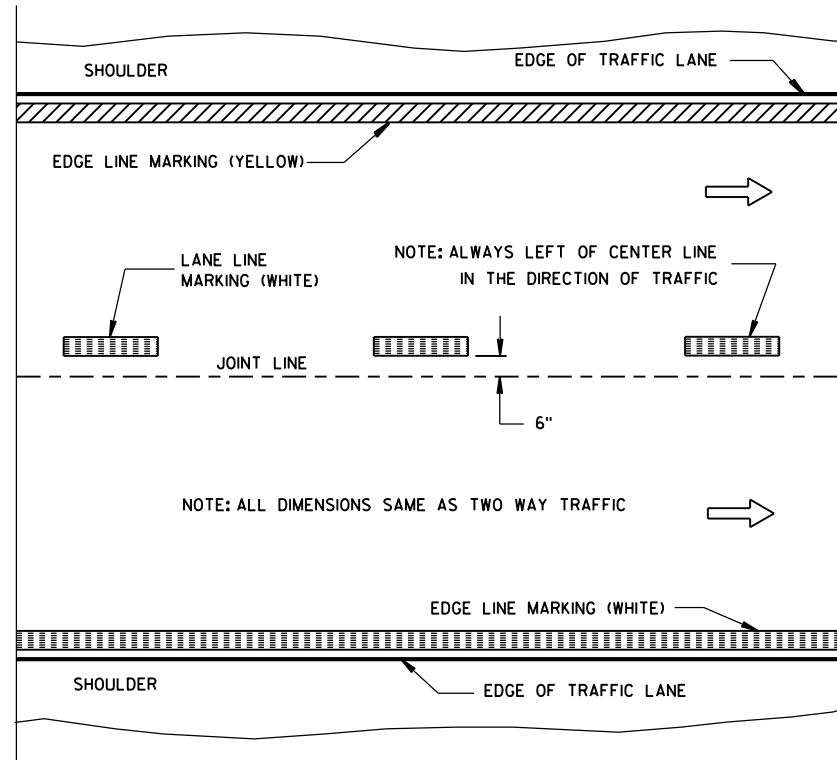
7/1/11
DATE

/S/ Thomas N. Notbohm
STATE TRAFFIC ENGINEER OF DESIGN

FHWA

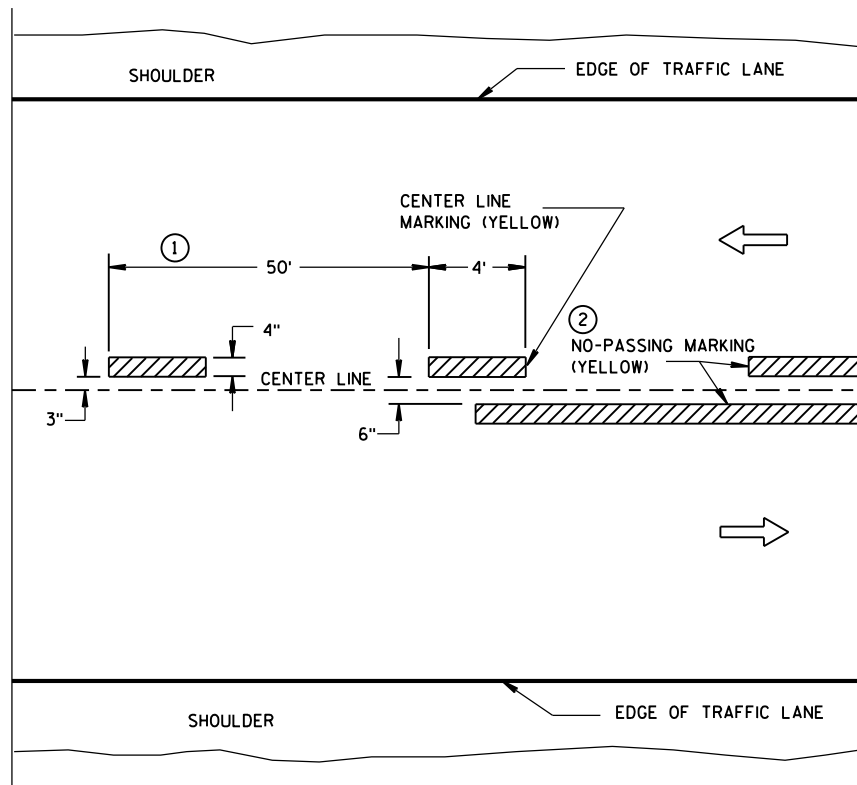


TWO WAY TRAFFIC

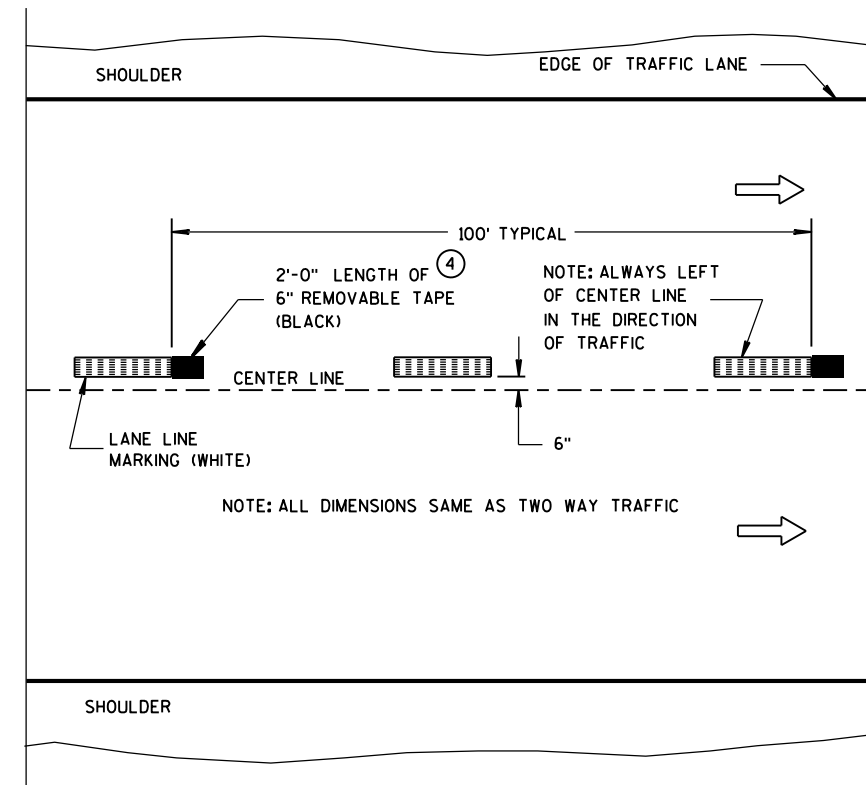


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING
(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

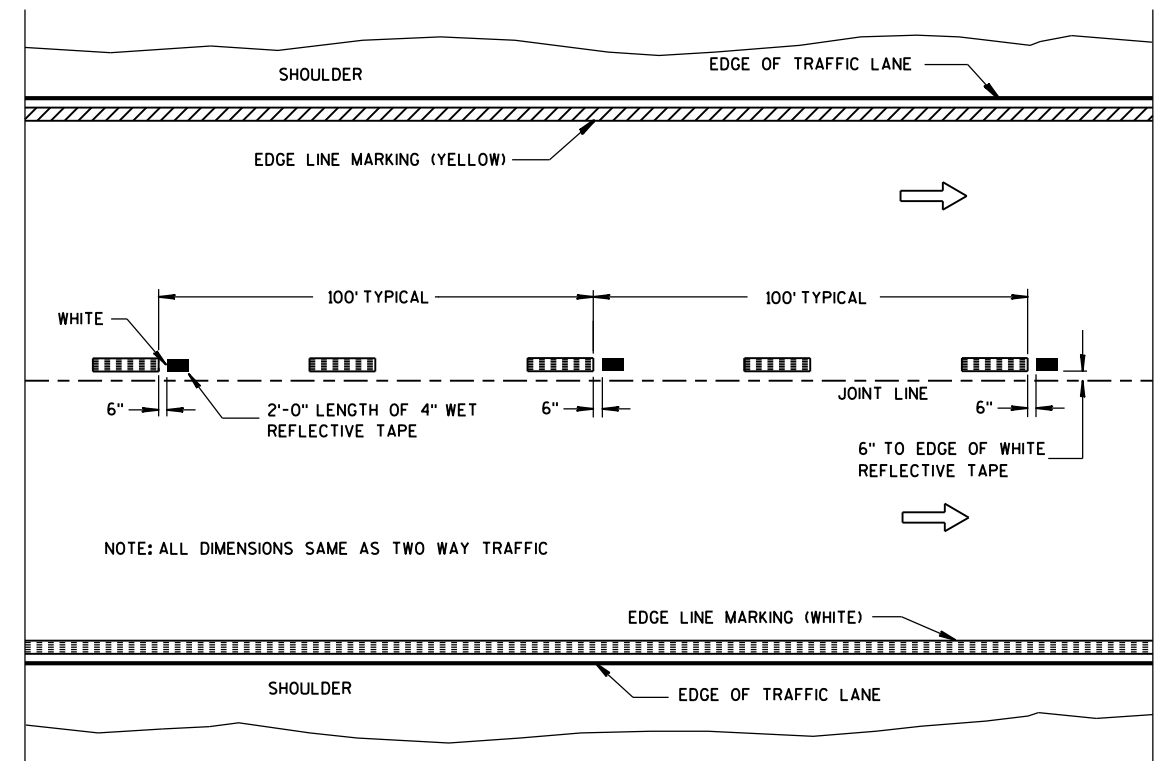
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- 1 HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- 2 NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- 3 NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
- 4 CONCRETE ONLY.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL



WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE

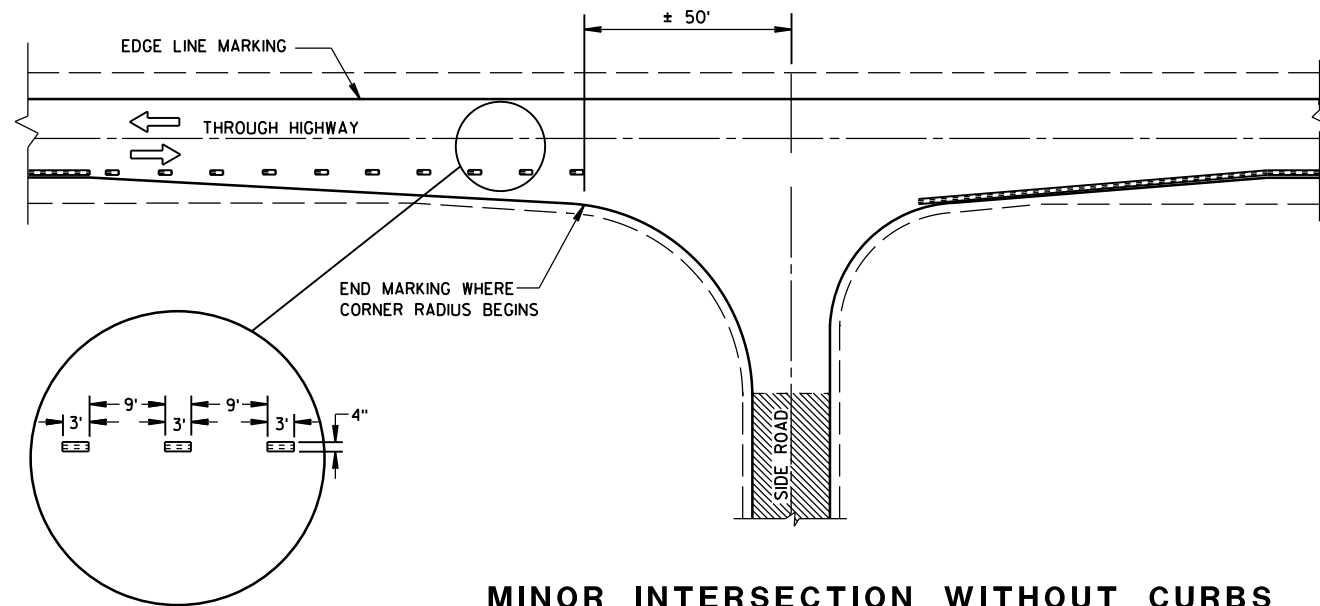
LEGEND

- "T" MARKING
- POST MOUNTED SIGN

PAVEMENT MARKING
(MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

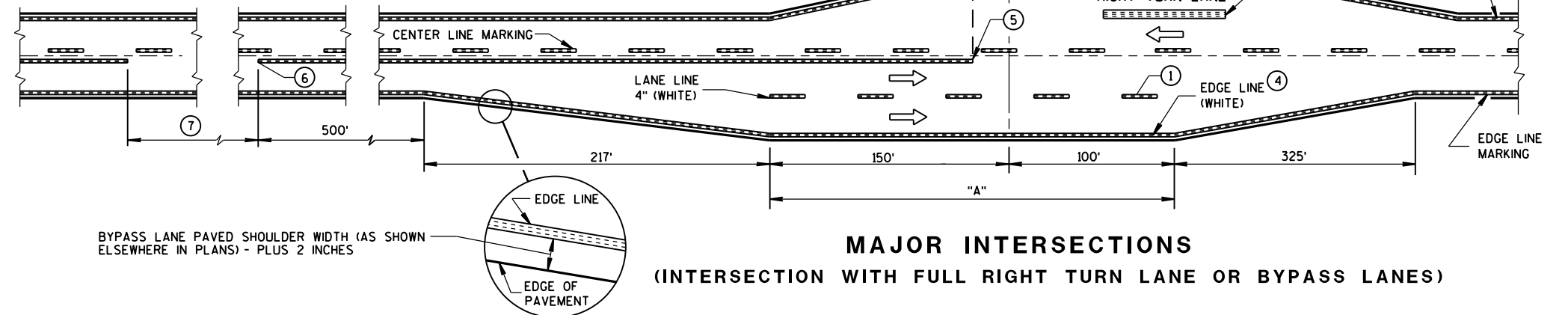
APPROVED
5-13-2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER
FHWA



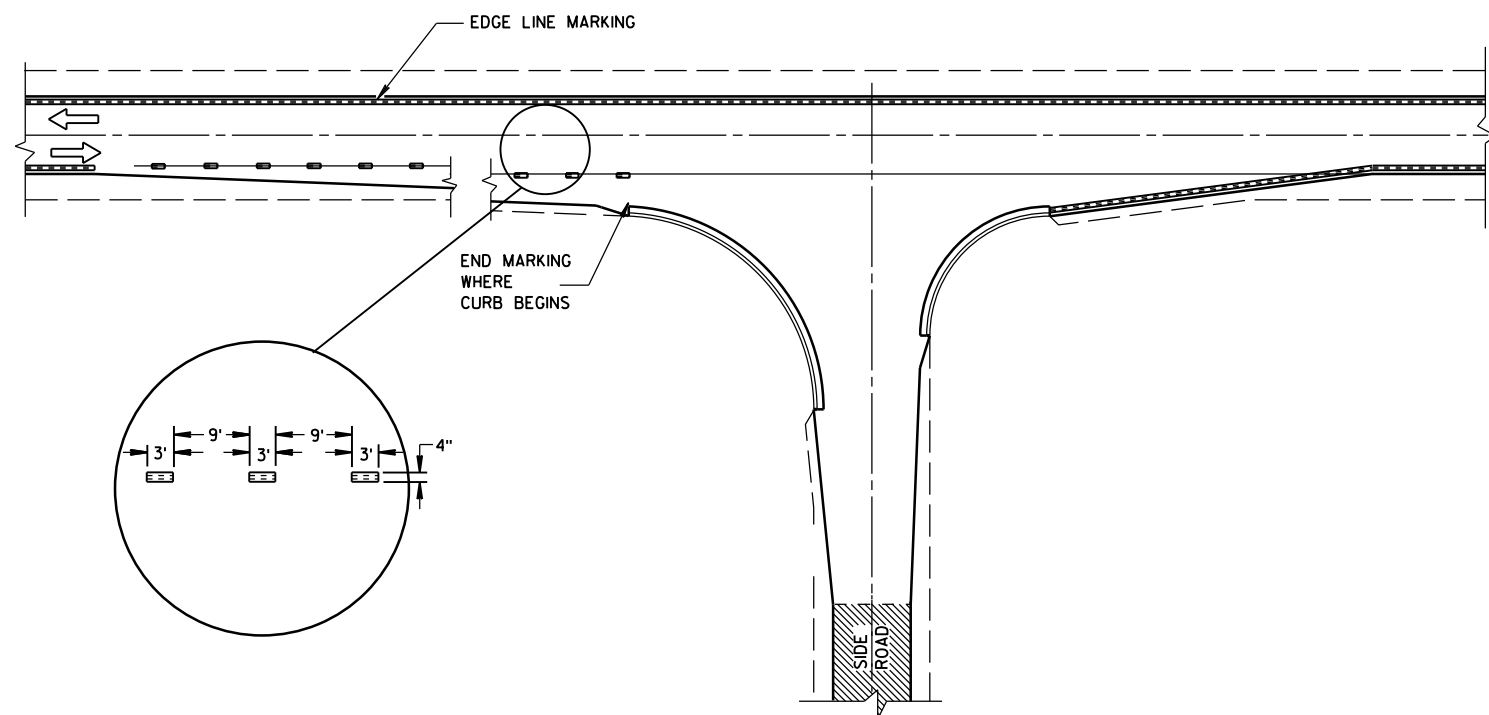
MINOR INTERSECTION WITHOUT CURBS

⑦

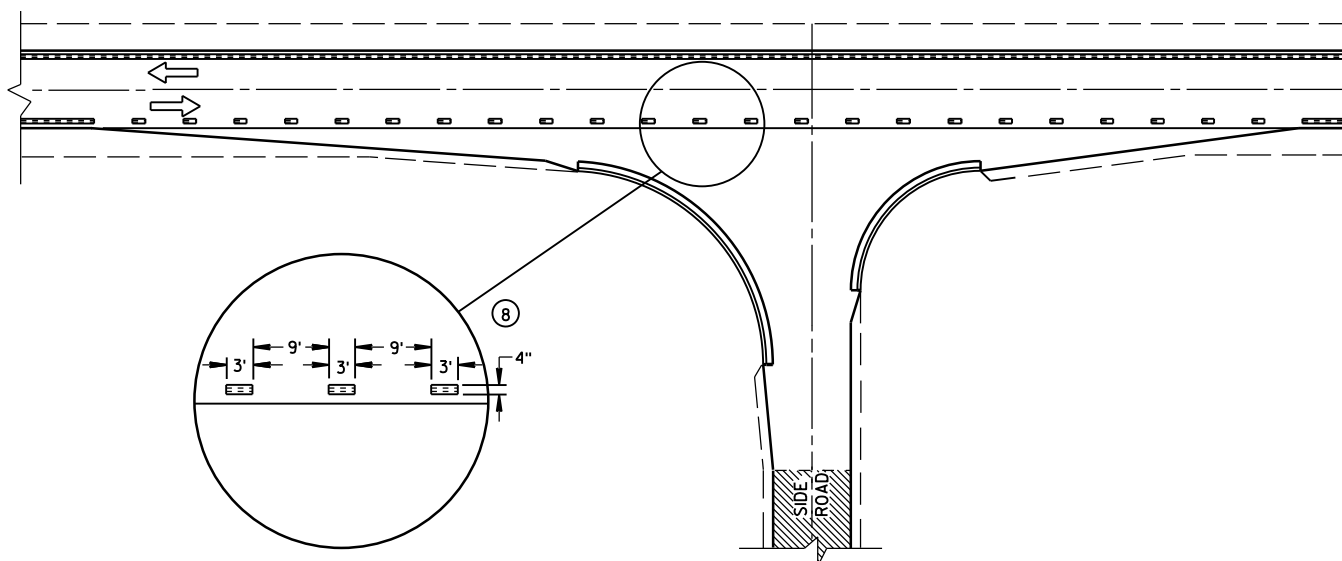
POSTED SPEED (MPH)	MINIMUM DISTANCE BETWEEN ZONES (FEET)
25 - 30	528
35 - 40	528
45 - 50	686
55	792



MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)



MINOR INTERSECTION WITH CURBS
(TYPICAL MARKING)



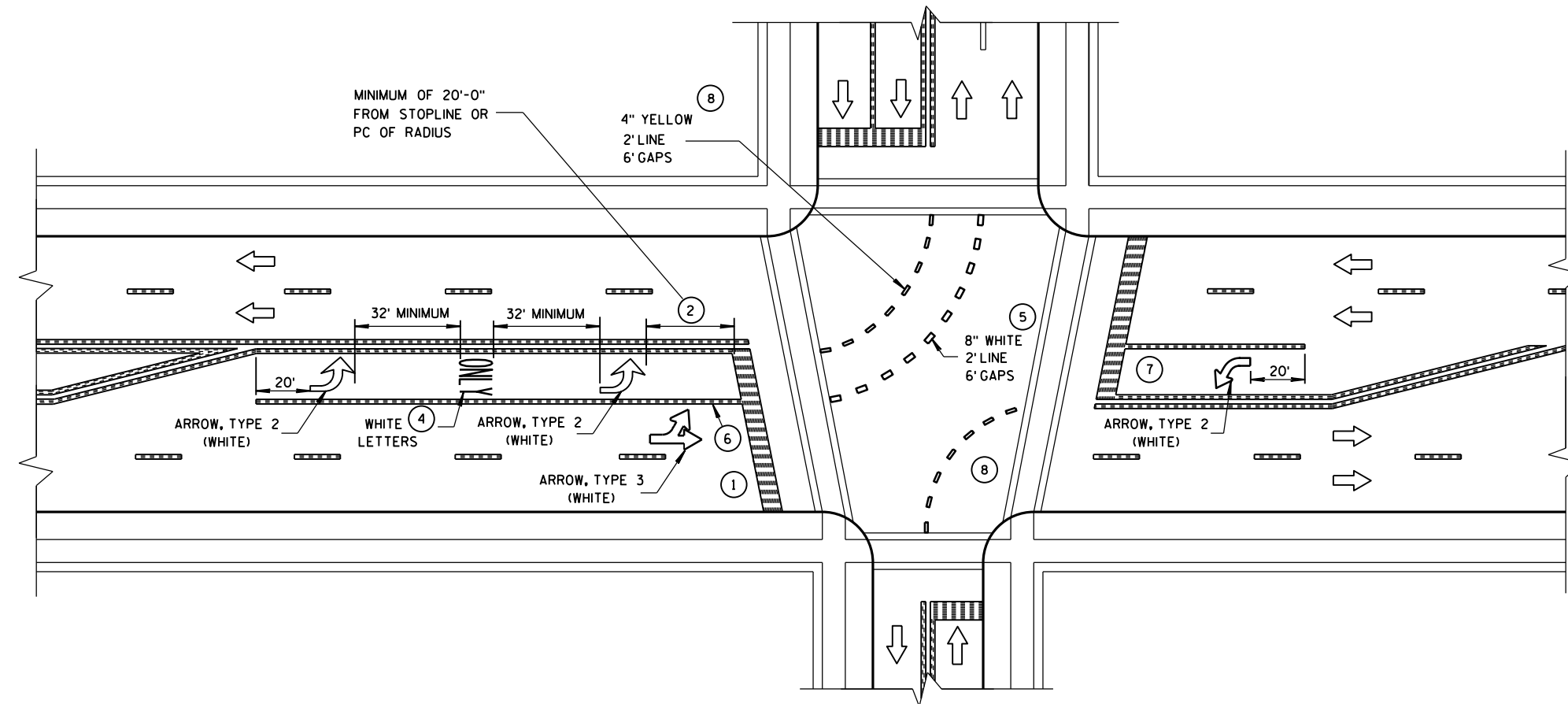
MINOR INTERSECTION WITH CURBS
③ (FOR SPECIAL CONDITIONS AS SPECIFIED)

GENERAL NOTES

- EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
 - ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
 - ③ ALTERNATIVE MARKING SHALL BE PROVIDED WHEN SPECIFIED IN THE CONTRACT. TYPICAL SITUATIONS WHERE THIS MARKING MAY BE REQUIRED ARE WHERE THE INTERSECTION IS ON A SHARP HORIZONTAL CURVE OR CREST VERTICAL CURVE IN AN UNLIGHTED AREA SUCH THAT THE EDGE LINE MAY BE MISLEADING TO THE MOTORIST OR DISAPPEAR FROM SIGHT.
 - ④ THE EDGE LINE IN THE TAPER AREAS OF THE BYPASS LANE AND THE BYPASS LANE SHALL BE LOCATED 1-FOOT FROM EDGE OF PAVEMENT TO THE OUTSIDE EDGE OF EDGE LINE.
 - ⑤ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.
 - ⑥ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
 - ⑦ IF THE DISTANCE BETWEEN 2 SUCCESSIVE NO-PASSING ZONES IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES, CONNECT THE 2 ZONES.
 - ⑧ 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

PAVEMENT MARKING
(INTERSECTIONS)

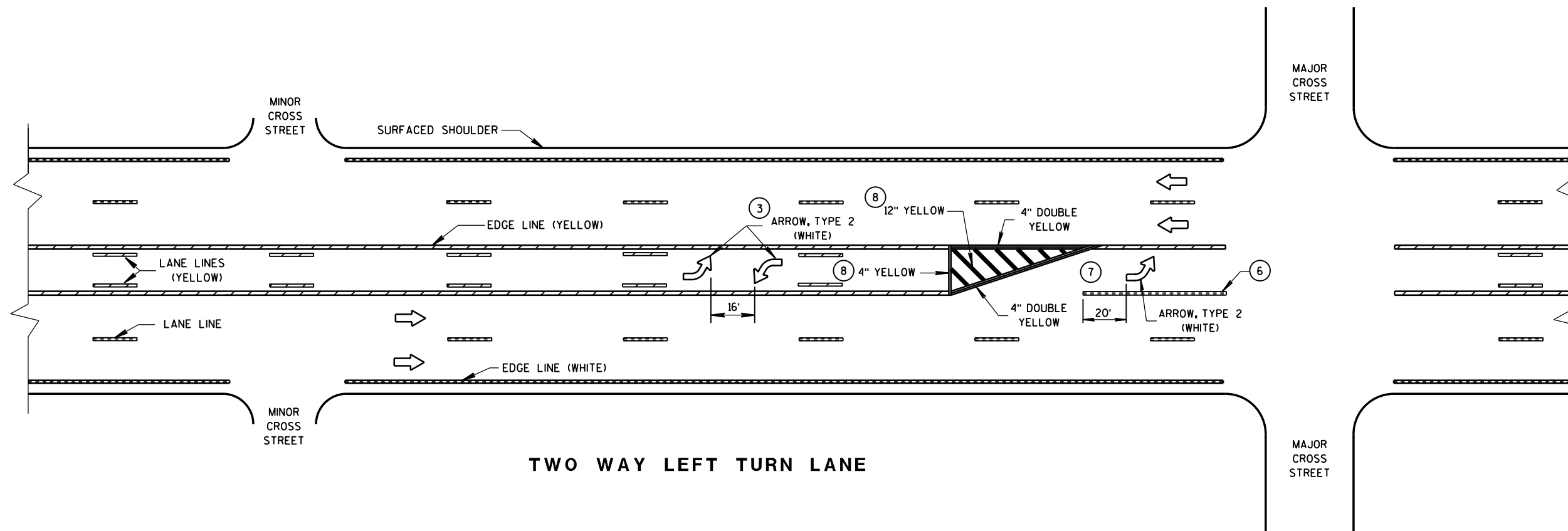
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



GENERAL NOTES

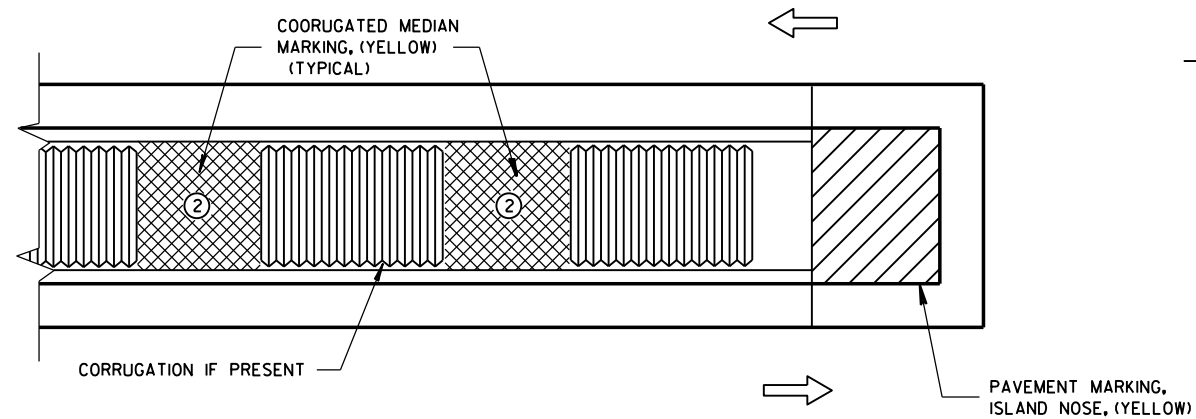
- ① STOP BAR IS REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.
- ② DISTANCE MAY BE ADJUSTED TO ACCOMMODATE SHORT LEFT TURN LANES, AS APPROVED BY THE ENGINEER.
- ③ A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ④ ADD EXTRA SETS OF ONE ARROW AND ONE ONLY PER 160 FEET OR WHEN ON A CURVE.
- ⑤ 8" WHITE WITH 2' LINE 6' GAPS FOR DUAL TURN LANE.
- ⑥ 8" WHITE
- ⑦ ADD SECOND ARROW WHEN TURN BAY IS GREATER THAN OR EQUAL TO 108 FEET.
- ⑧ REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.

NOTE:
ARROW SYMBOL (➡)
SHOWS DIRECTION OF TRAVEL

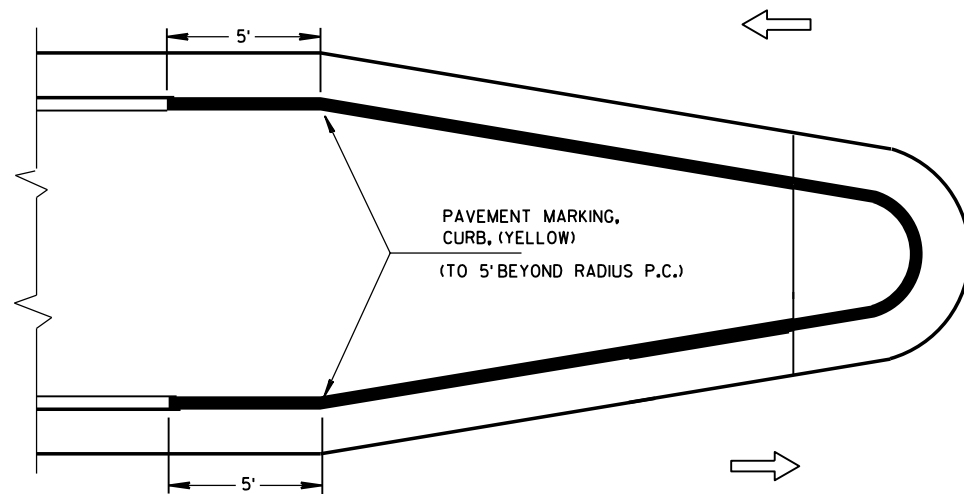


PAVEMENT MARKING
(LEFT TURN LANE)

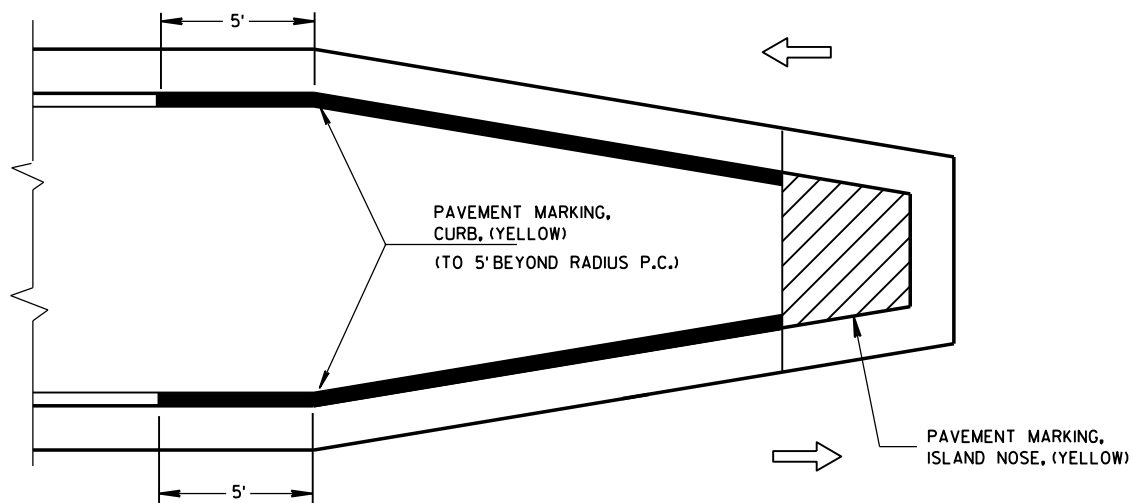
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



MEDIAN ISLAND WITH SQUARE BLUNT NOSE

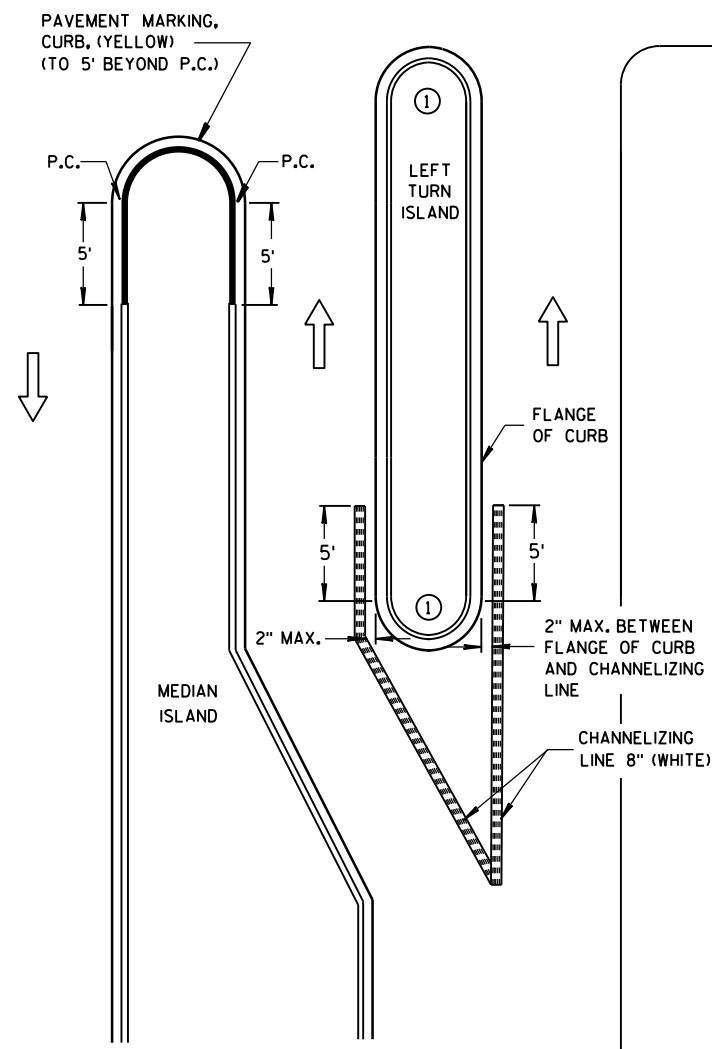


MEDIAN ISLAND WITH ROUND BLUNT NOSE



MEDIAN ISLAND WITH SLOPED NOSE

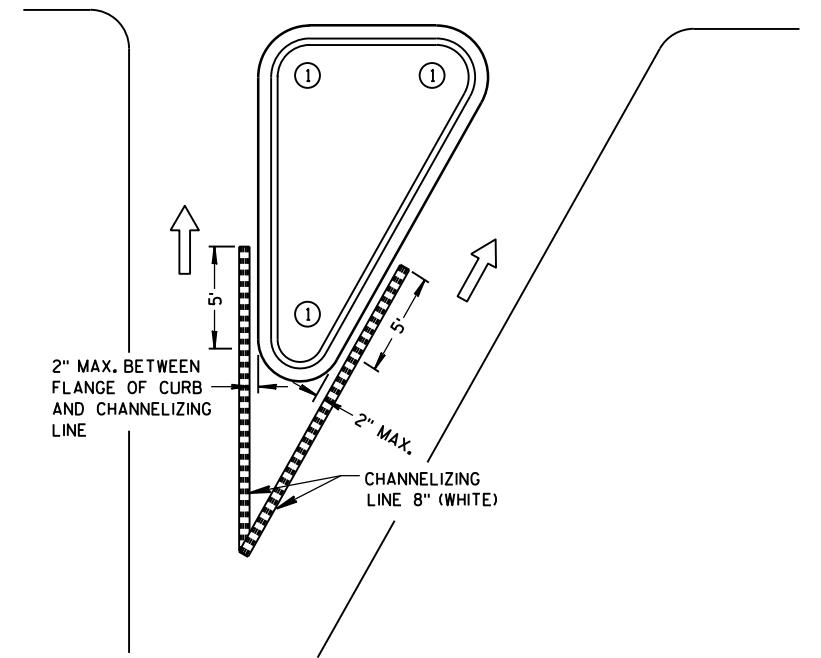
TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS



LEFT TURN & MEDIAN ISLAND

GENERAL NOTES

- 1 DO NOT MARK CURB NOSES THAT SEPARATE LANES OF TRAFFIC TRAVELING IN THE SAME DIRECTION.
- 2 WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN, THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.



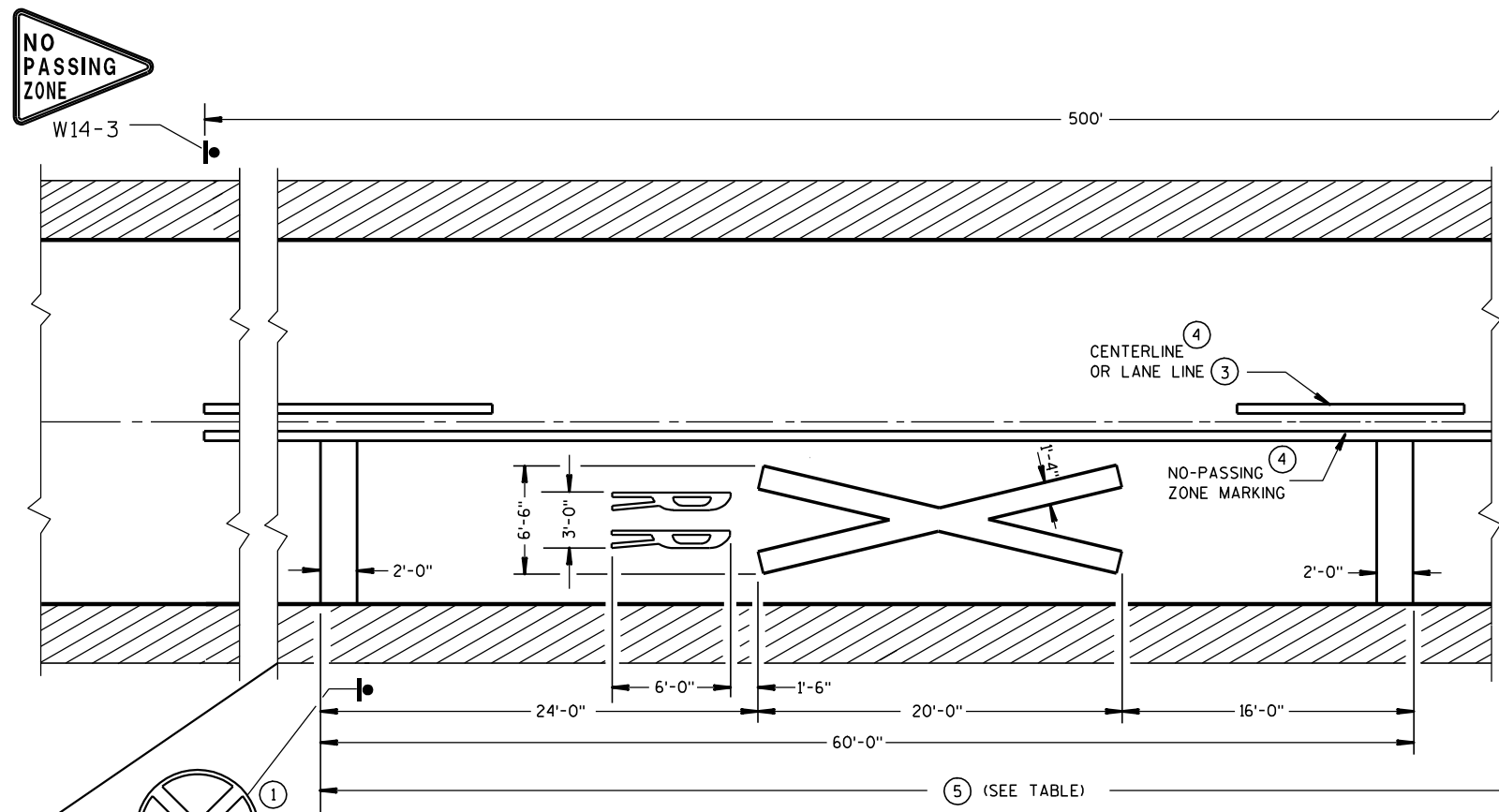
RIGHT TURN ISLAND

LEGEND

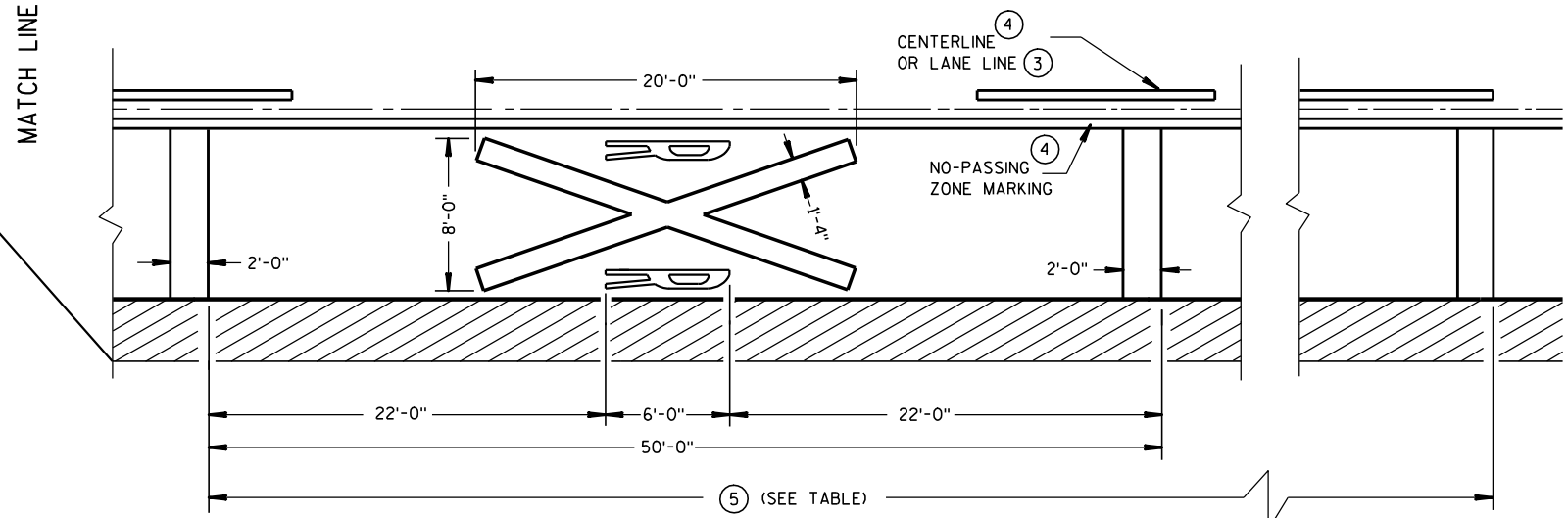
- ISLAND NOSE MARKING
- CURB MARKING
- CORRUGATED MEDIAN MARKING
- DIRECTION OF TRAVEL

PAVEMENT MARKING (ISLANDS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



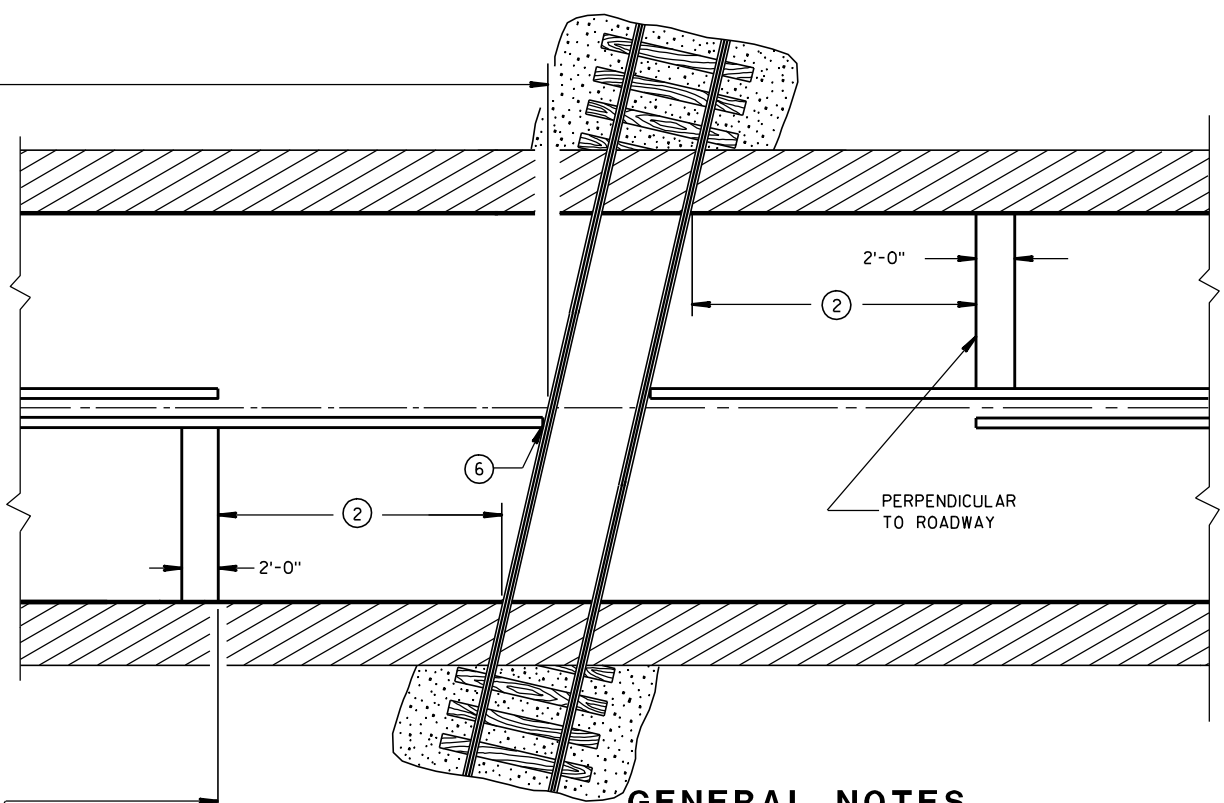
PREFERRED PAVEMENT MARKING



ALTERNATE PAVEMENT MARKING

Posted Speed (M.P.H.)	Dimension Range (Feet)
25	150*- 250
30	200*- 300
35	250*- 450
40	300*- 500
45	400*- 650
50	550*- 800
55	750*- 1000
60	1000*- 1250
65	1000*- 1250

* THE MINIMUM DISTANCES IN THE TABLE ARE DESIRABLE AND SHOULD BE USED. THE DISTANCES MAY BE INCREASED UP TO THE MAXIMUM TO ALLOW FOR FIELD CONDITIONS SUCH AS THE CLOSE PROXIMITY OF DRIVEWAYS, BRIDGES, SIDEROADS OR OTHER FEATURES THAT WOULD PROHIBIT THE MINIMUM DISTANCES FROM BEING USED.



GENERAL NOTES

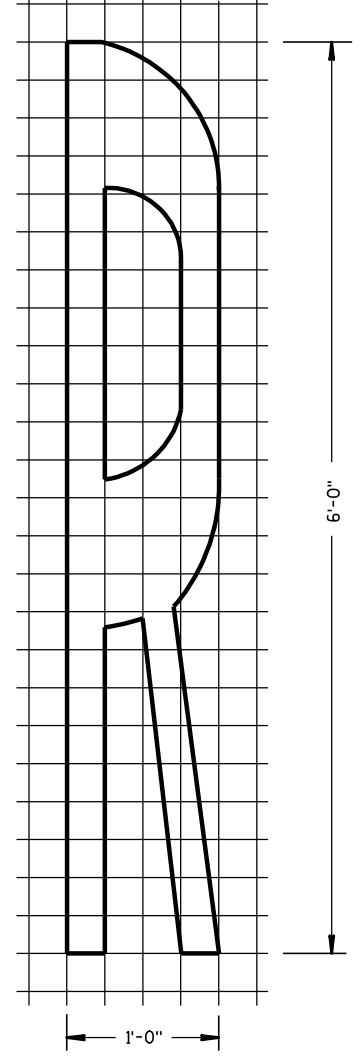
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

A THREE-LANE ROADWAY SHOULD BE MARKED WITH A CENTERLINE FOR TWO-LANE APPROACH OPERATION ON THE APPROACH TO A CROSSING.

ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE. ALL LETTERS AND SYMBOLS SHALL BE IN CONFORMANCE WITH THE "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS" (ADOPTED BY THE FEDERAL HIGHWAY ADMINISTRATION).

CENTER OR LANE LINES AND NO-PASSING ZONE MARKINGS SHOWN ON THIS DRAWING ARE REQUIRED AND PAID FOR UNDER OTHER ITEMS IN THE CONTRACT.

- ① A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).
- ② MINIMUM 8' FROM ANY RAILROAD WARNING DEVICES (SIGNALS, GATES, ETC.) OR 25' FROM THE NEAREST RAIL, WHICHEVER DISTANCE IS GREATER.
- ③ REFLECTIVE WHITE.
- ④ REFLECTIVE YELLOW 500' MINIMUM. MARKING LIMITS MAY BE EXTENDED AS DIRECTED BY THE ENGINEER TO MEET ADJACENT NO-PASSING ZONE MARKINGS.
- ⑤ TABLE BASED UPON 2C-4 WISCONSIN SUPPLEMENT OF MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- ⑥ FOR MULTIPLE TRACK CROSSINGS, THE BARRIER LINE SHALL EXTEND TO THE NEAR RAIL OF THE FURTHEST TRACK IN THE DIRECTION OF HIGHWAY TRAVEL.







SIGNING AND PAVEMENT MARKING
DETAILS FOR RAILROAD-HIGHWAY
GRADE CROSSINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
1-9-2012 /S/ Thomas N. Notbohm
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

LEGEND

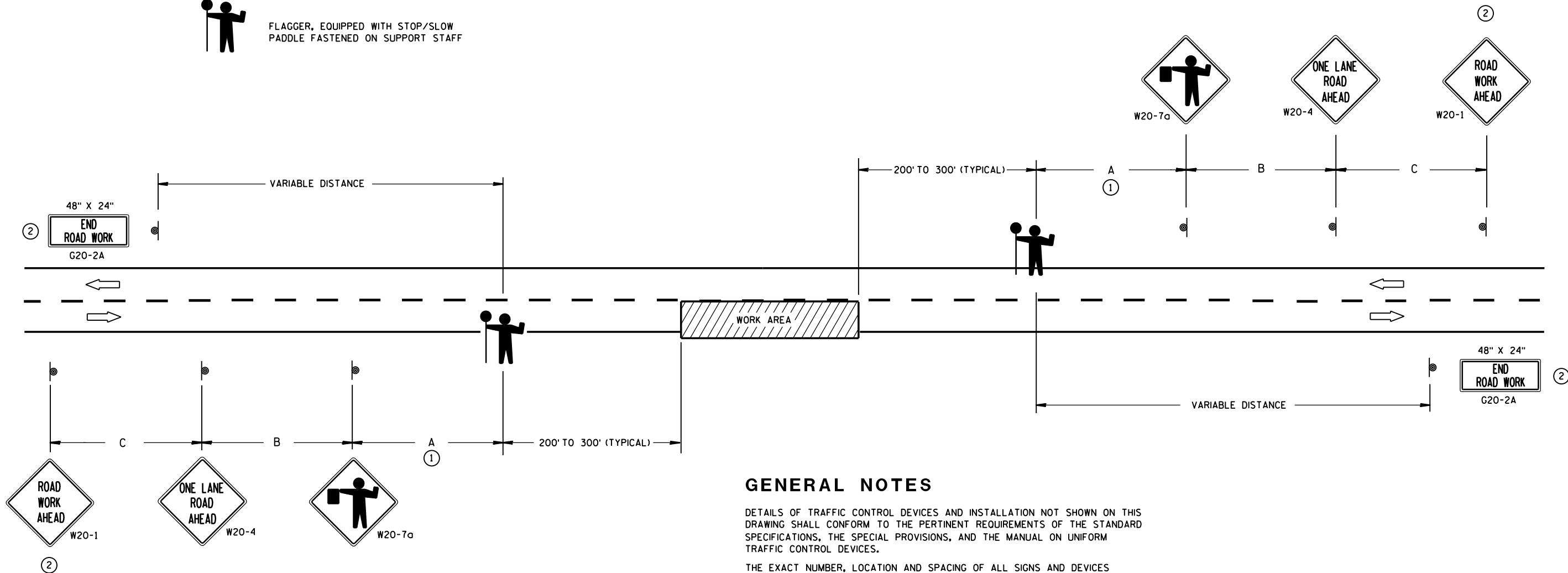
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

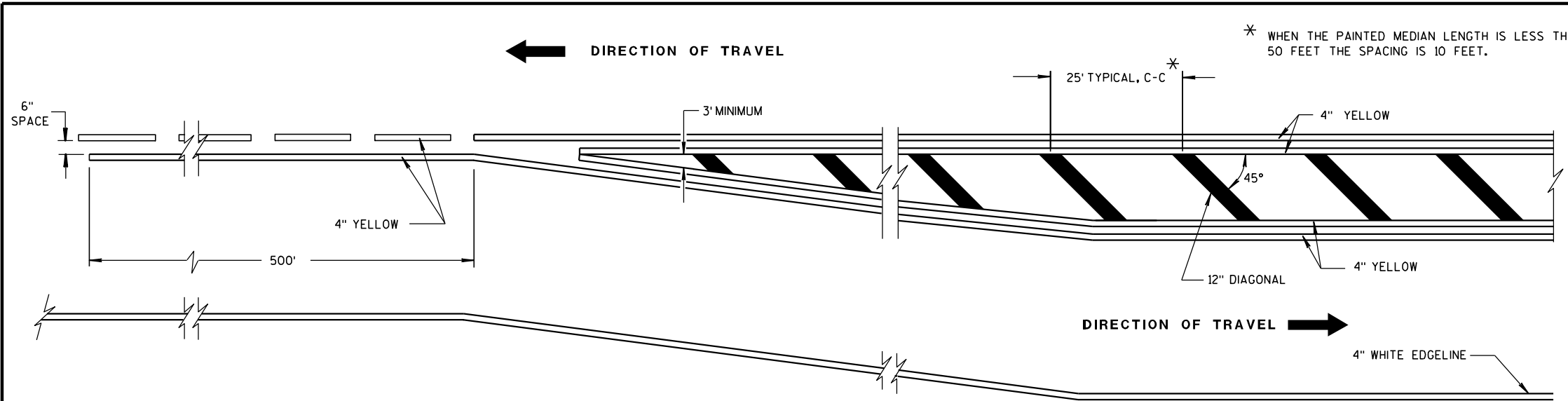
① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

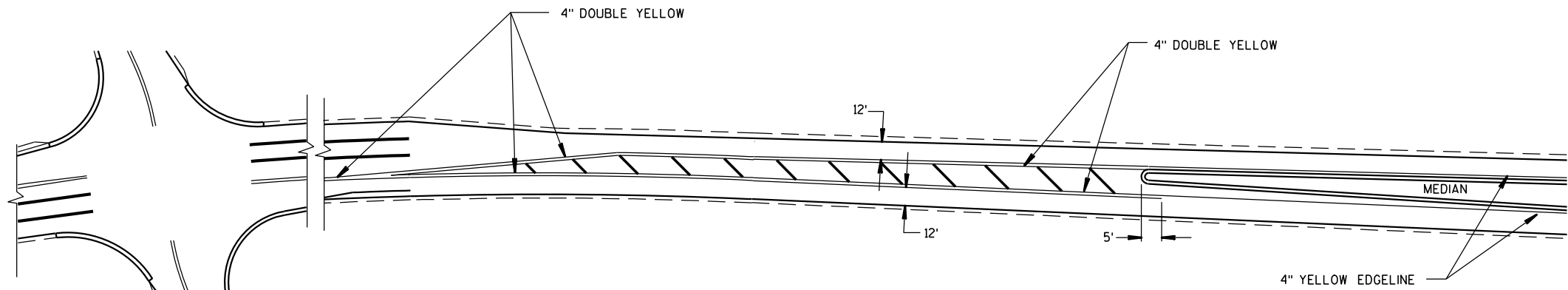
APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



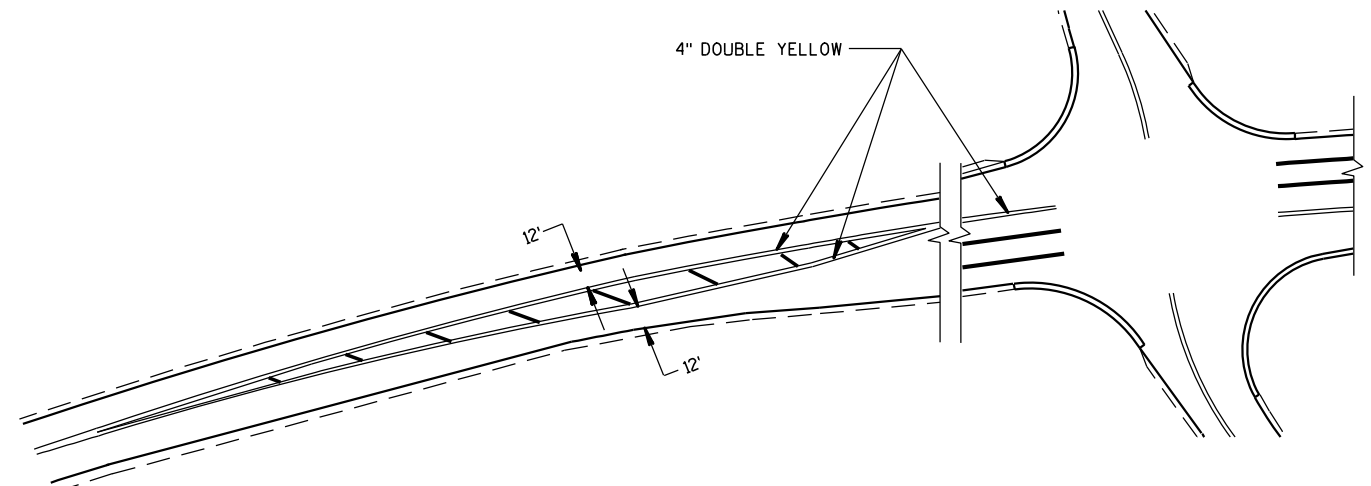
MEDIAN ISLAND DETAIL

GENERAL NOTE

DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT WIDEST POINT.

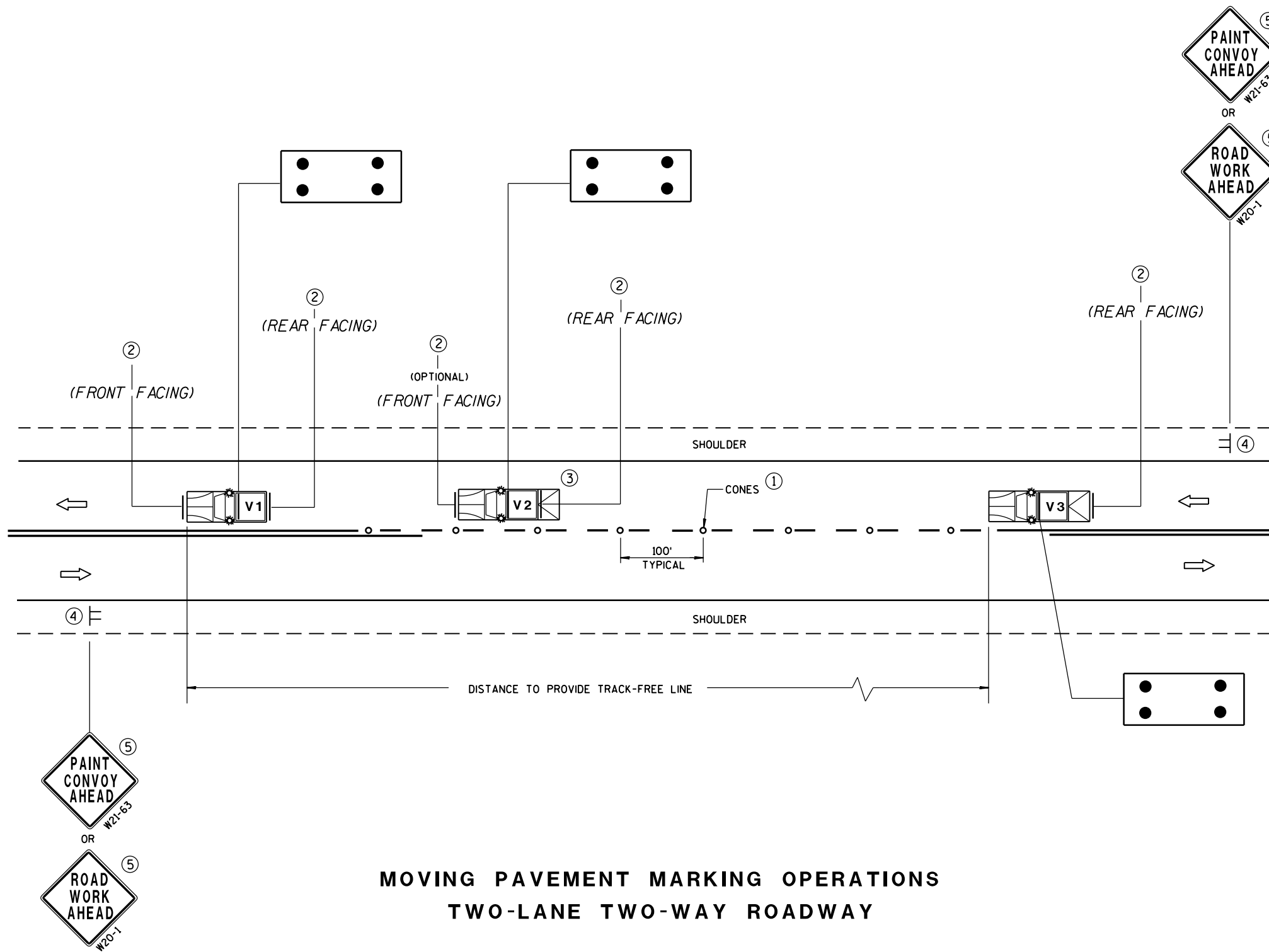


APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON APPROACH MARKINGS

MEDIAN ISLAND MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 2-5-09 DATE	/S/ Thomas N. Notbohm STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



MOVING PAVEMENT MARKING OPERATIONS TWO-LANE TWO-WAY ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

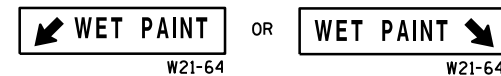
THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.



③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.

④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

V1 LEAD VEHICLE

V2 SHADOW VEHICLE

V3 TRAIL VEHICLE WITH TMA

TMA TRUCK-MOUNTED ATTENUATOR

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC

CONES

FLASHING ARROW PANEL (CAUTION)

MOVING PAVEMENT MARKING
OPERATION
TWO-LANE TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

5/3/2013
DATE

/S/ Travis Feltes
STATE TRAFFIC ENGINEER

FHWA

GENERAL NOTES

ORIENT ANCHOR BOLTS IN FOOTING AND PROVIDE ANCHOR BOLT STICK OUT ABOVE TOP OF CONCRETE FOOTING BASE PER FABRICATION DRAWING.

BENDING DIMENSIONS FOR REINFORCING BARS ARE OUT TO OUT.

USE 3" CLEAR FOR ALL REINFORCEMENT UNLESS NOTED OTHERWISE.

SIGN SUPPORTS SHALL BE LOCATED NORMAL TO ROADWAY.

THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.

WELDING OF ANCHOR BOLTS TO THE CAGE IS UNACCEPTABLE. TEMPLATES SHALL BE USED.

BAR CAGE TO BE ASSEMBLED USING TIE WIRES ONLY, NO WELDING.

BASES (SHAFT) SHALL BE EXCAVATED BY THE USE OF A CIRCULAR AUGER. IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE SOIL, THE FORM SHALL BE REMOVED BEFORE BACK FILLING AROUND THE BASE. ANY REQUIRED BACKFILL SHALL BE WELL COMPACTED IN LAYERS OF 1 FOOT OR LESS. COMPACTION SHALL BE BY MECHANICAL MEANS. CARE SHALL BE TAKEN SO NO DAMAGE OCCURS TO THE CONCRETE BASE DURING COMPACTION.

EXCAVATION OF MATERIALS NOT OCCUPIED BY CONCRETE SHALL BE MINIMIZED TO REDUCE DISTURBANCE OF THE SURROUNDING SOILS.

THE BOTTOM OF THE DRILLED HOLE SHALL BE FIRM AND THOROUGHLY CLEANED SO NO LOOSE OR COMPRESSIBLE MATERIALS ARE PRESENT AT THE TIME OF THE CONCRETE PLACEMENT.

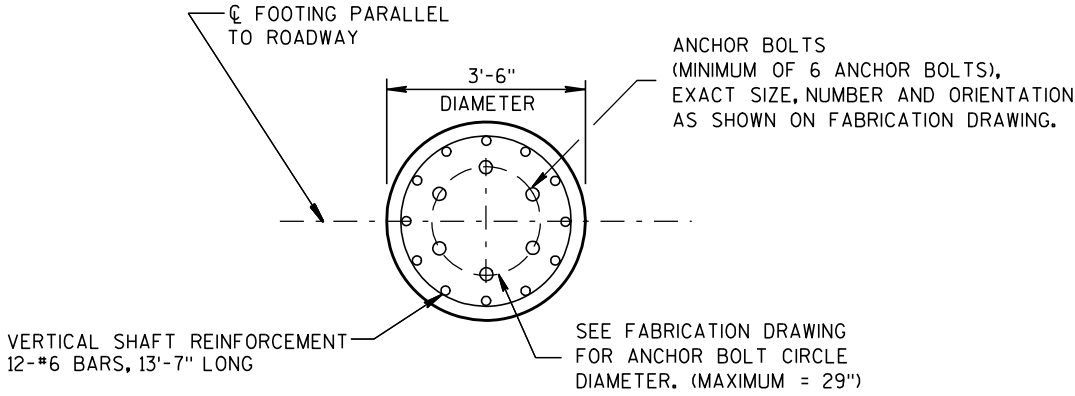
IF THE DRILLED HOLE CONTAINS STANDING WATER, THE CONCRETE SHALL BE PLACED USING A TREMIE TO DISPLACE THE WATER.

THE REINFORCEMENT AND ANCHOR BOLTS SHALL BE ADEQUATELY SUPPORTED IN THE PROPER POSITIONS SO NO MOVEMENT OCCURS DURING CONCRETE PLACEMENT.

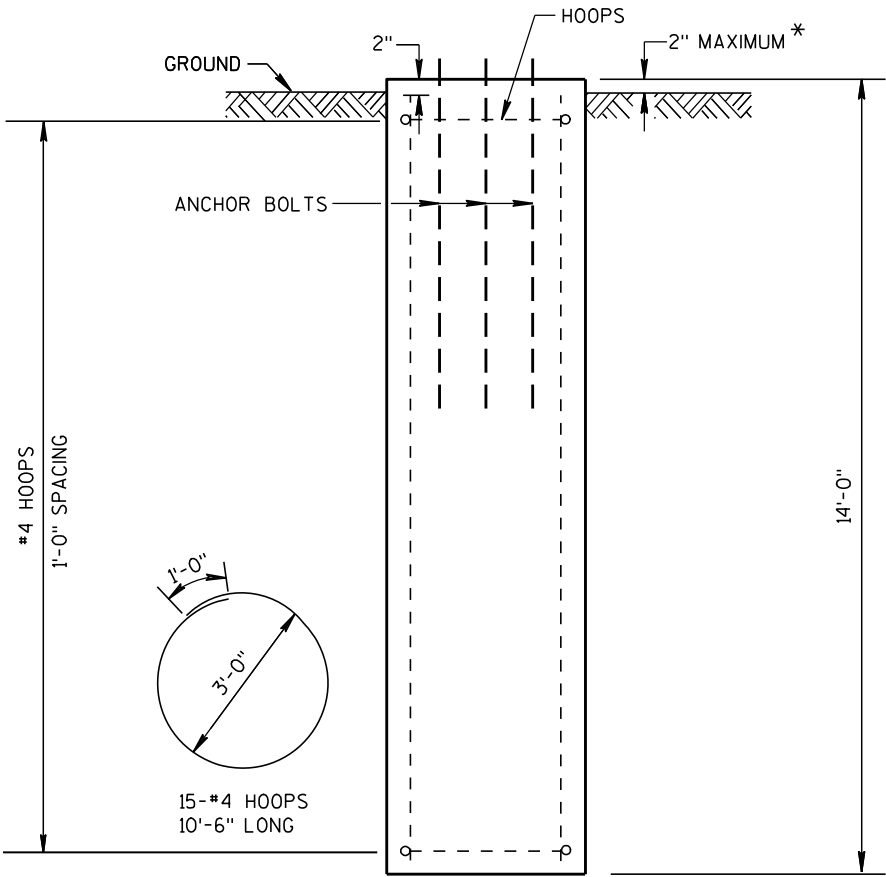
ANY DAMAGE TO THE CONCRETE BASE DURING CONSTRUCTION OPERATIONS SHALL BE REPAIRED AT THE ENGINEER'S DIRECTION, AT THE EXPENSE OF THE CONTRACTOR.

CONCRETE MASONRY ----- $f_c=3,500$ p.s.i.
HIGH STRENGTH BAR STEEL REINFORCEMENT, GRADE 60 ----- $f_y=60,000$ p.s.i.
ANCHOR BOLTS ----- AASHTO M314 GRADE 55

THIS FOOTING HAS BEEN DESIGNED FOR SITES WHERE SOILS EXHIBIT A PHI-ANGLE GREATER THAN OR EQUAL TO 20 DEGREES (GRANULAR SOILS), OR A COHESION VALUE GREATER THAN OR EQUAL TO 350 PSF (COHESIVE SOILS).



PLAN VIEW



ELEVATION VIEW

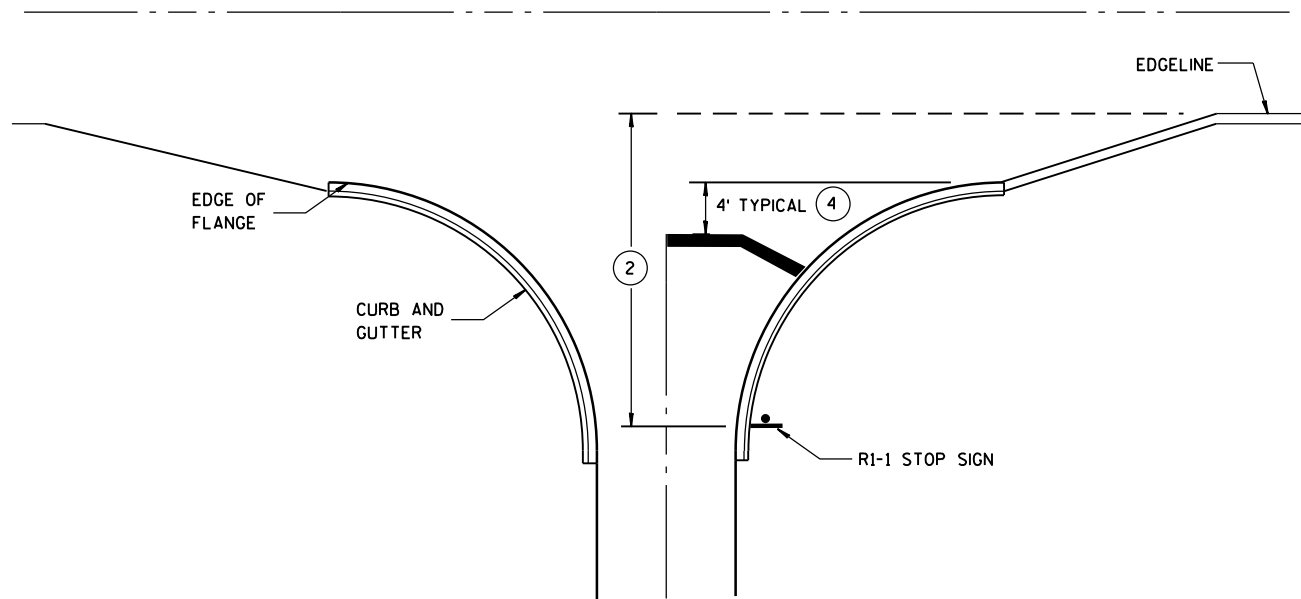
CONCRETE - 5.0 C.Y. PER FOOTING
H.S. REINFORCEMENT - 350 LBS. PER FOOTING

* FOR OVERHEAD SIGN SUPPORTS THAT ARE INSTALLED ADJACENT TO SIDEWALKS, THE TOP OF THE BASE SHALL BE POURED FLUSH WITH THE GROUND.

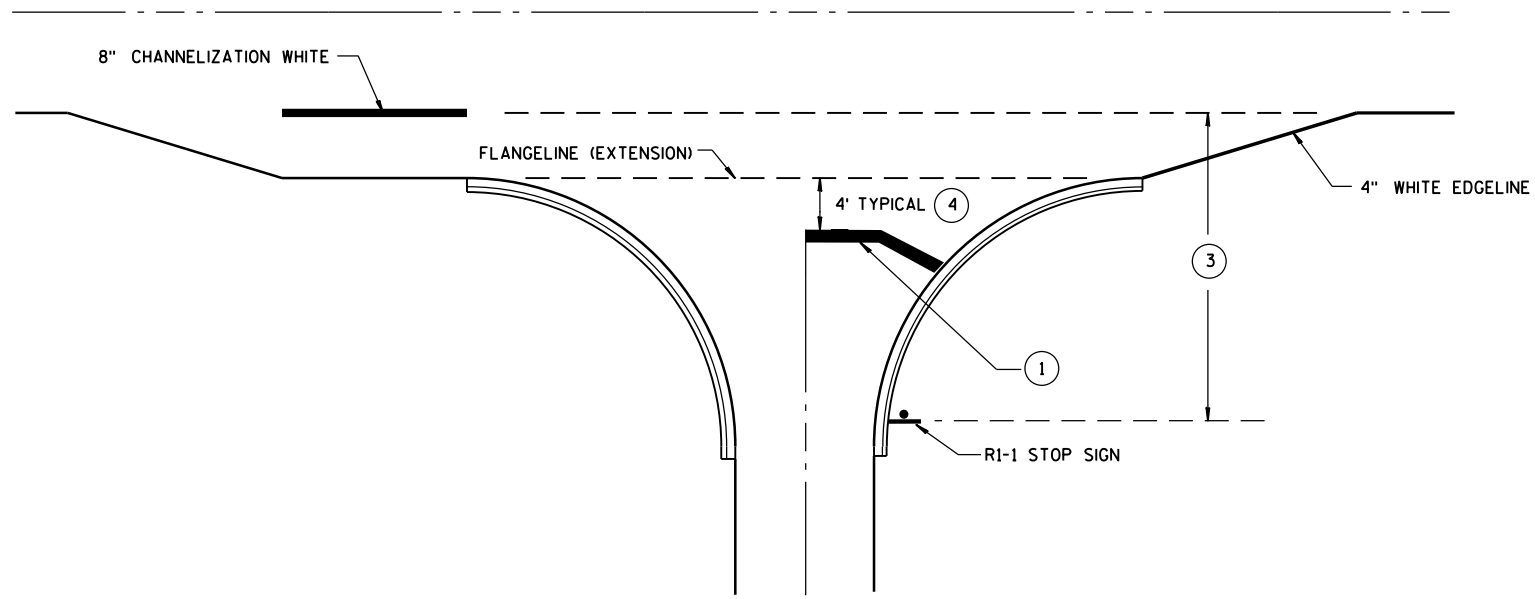
42" DIAMETER CANTILEVER
OVERHEAD SIGN SUPPORT BASE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

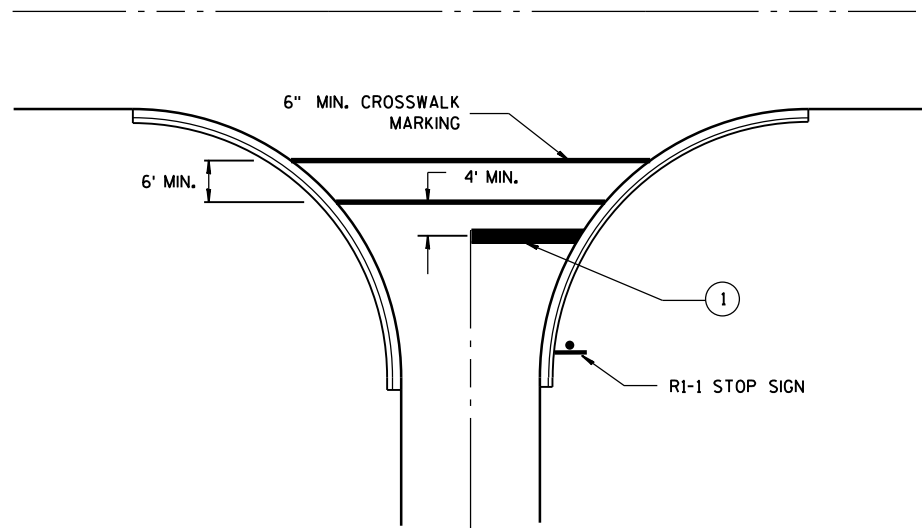
APPROVED
4-17-09 /S/ Thomas N. Notbohm
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



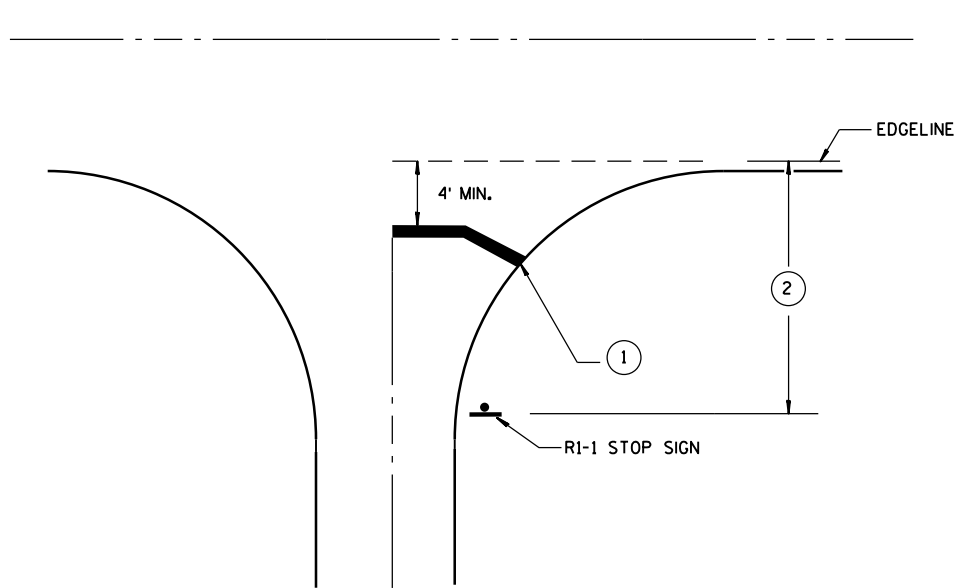
TYPICAL STOP LINE PAVEMENT MARKING
WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH CROSSWALK MARKING

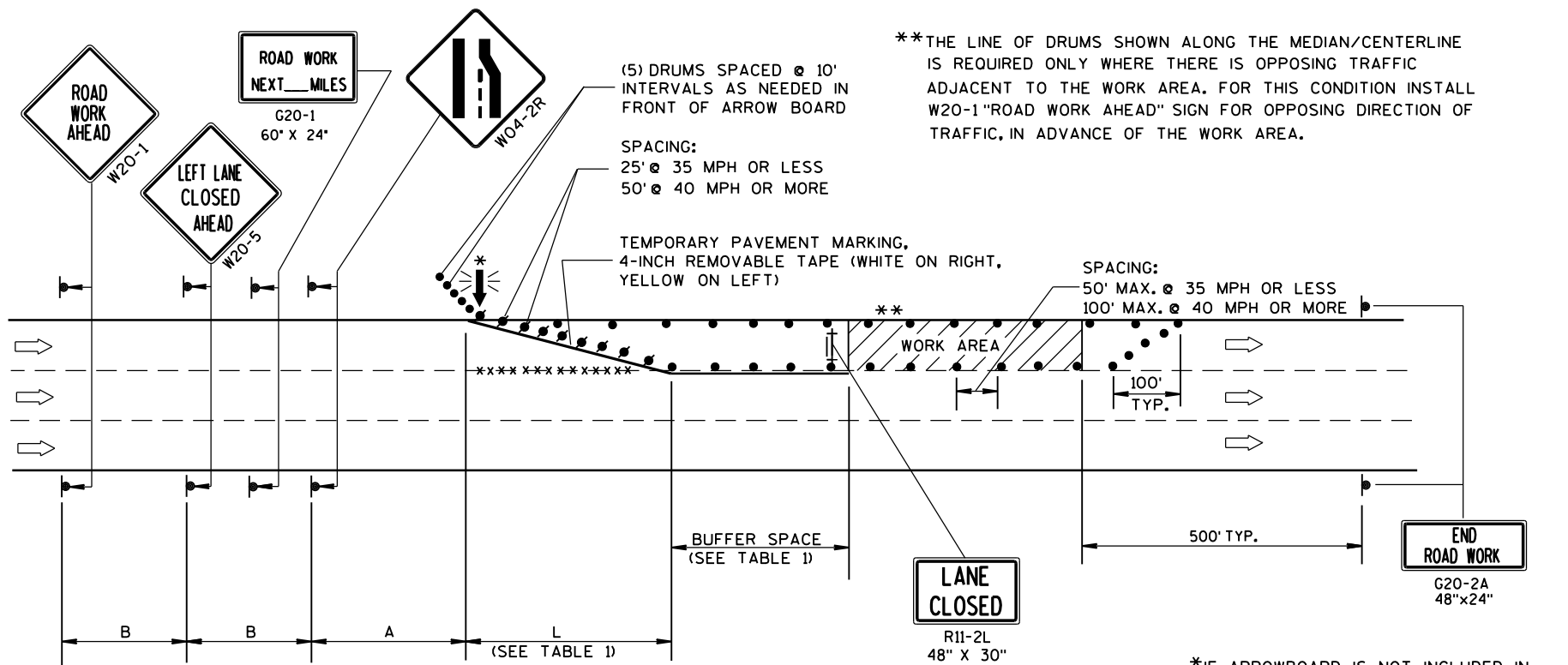


TYPICAL STOP LINE PAVEMENT MARKING
WITHOUT CURB AND GUTTER

GENERAL NOTES

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGE LINE THAN NO STOP LINE IS REQUIRED.
- ③ IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- ④ MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES.

STOP LINE AND CROSSWALK PAVEMENT MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4/30/2013 DATE	/S/ Travis Feltz STATE TRAFFIC ENGINEER
FHWA	



B=400' AT 25-30 MPH
700' AT 35-40 MPH
1000' AT 45-55 MPH

A=200' AT 25-30 MPH
350' AT 35-40 MPH
500' AT 45-55 MPH

TABLE 1
TAPER AND BUFFER SPACE
FOR 12' LANE WIDTH

S	L	BUFFER SPACE
25	125'	55'
30	180'	85'
35	245'	120'
40	320'	170'
45	540'	220'
50	600'	280'
55	660'	335'

FOR LANE WIDTH OTHER THAN 12':

L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

L = TAPER LENGTH IN FEET

S = NON-CONSTRUCTION SPEED LIMIT (MPH)

W = WIDTH OF LANE CLOSURE

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

GENERAL NOTES

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

ON UNDIVIDED ROADWAYS, OMIT THE SIGNS SHOWN ON LEFT SIDE OF ROAD.

W20-1, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.

PLACE THE ARROWBOARD AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE LANE CLOSURE TAPER, PREFERABLY ON THE SHOULDER OR TERRACE.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL,
SINGLE LANE CLOSURE,
NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

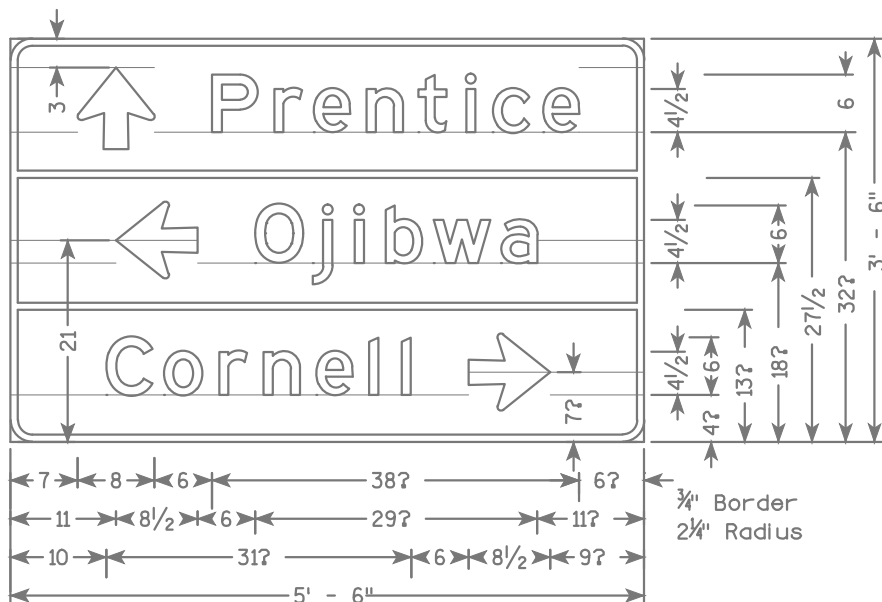
8/2013

DATE

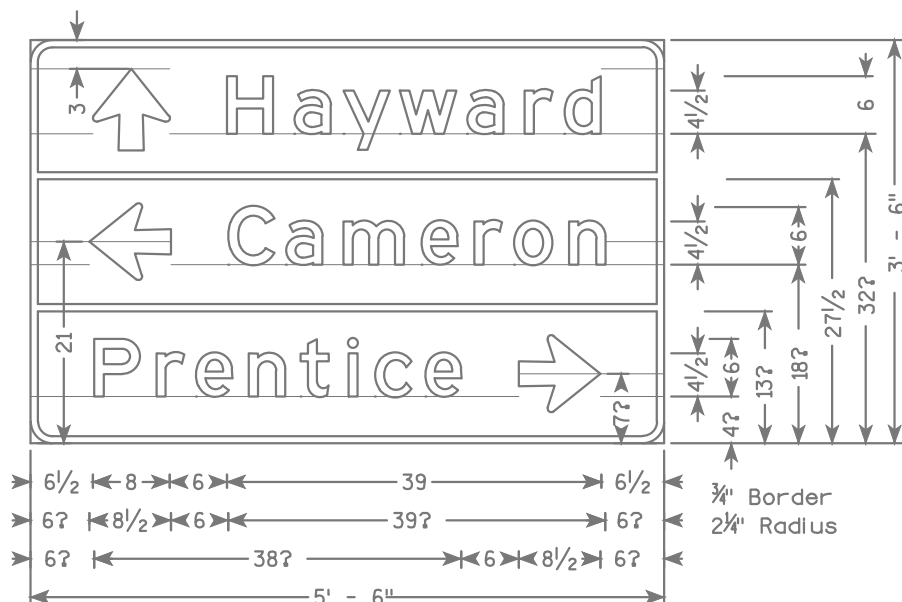
FHWA

/S/ Travis Feltes

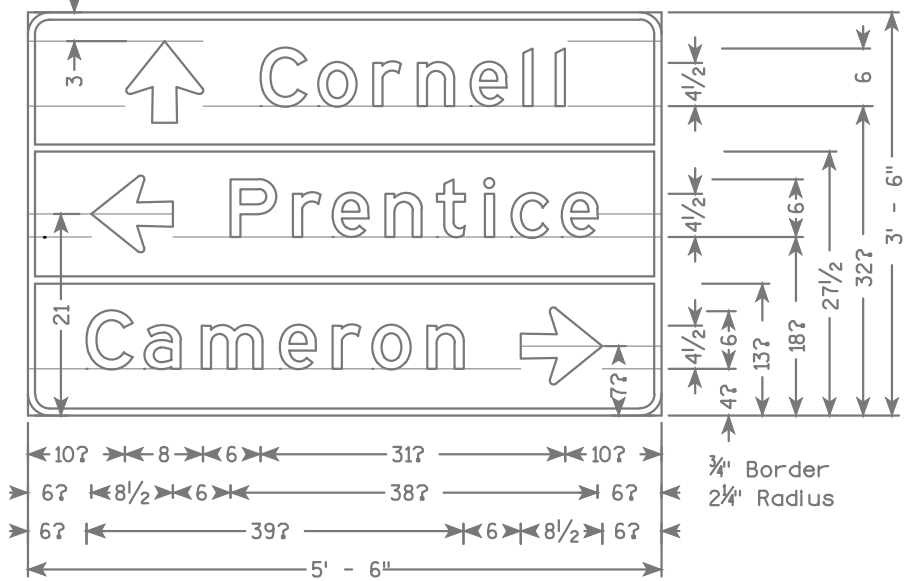
STATE TRAFFIC ENGINEER OF DESIGN



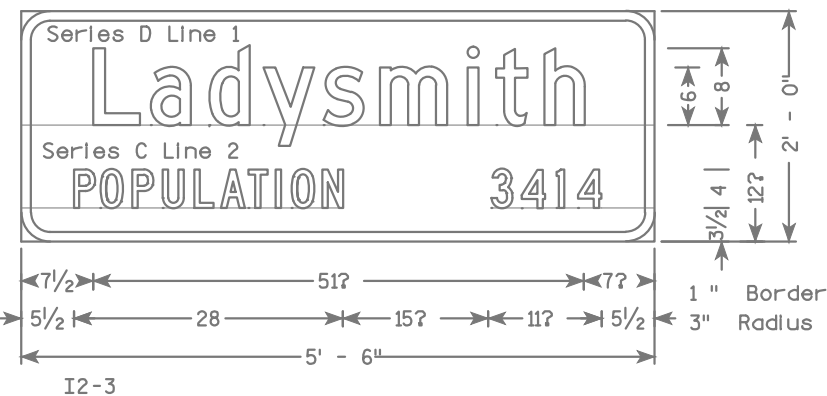
D1-3



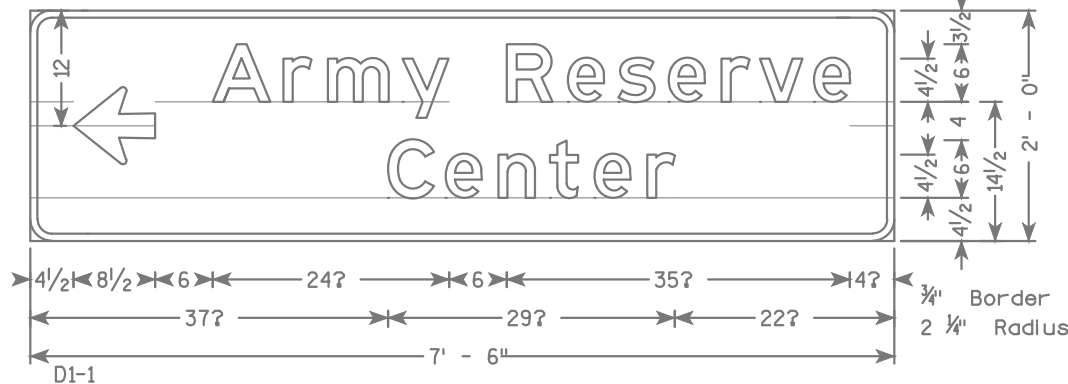
D1-3



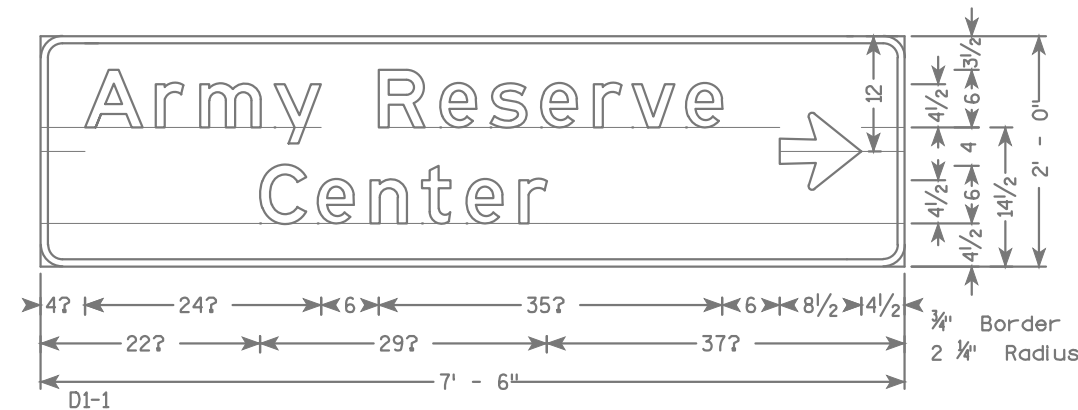
D1-3



I2-3



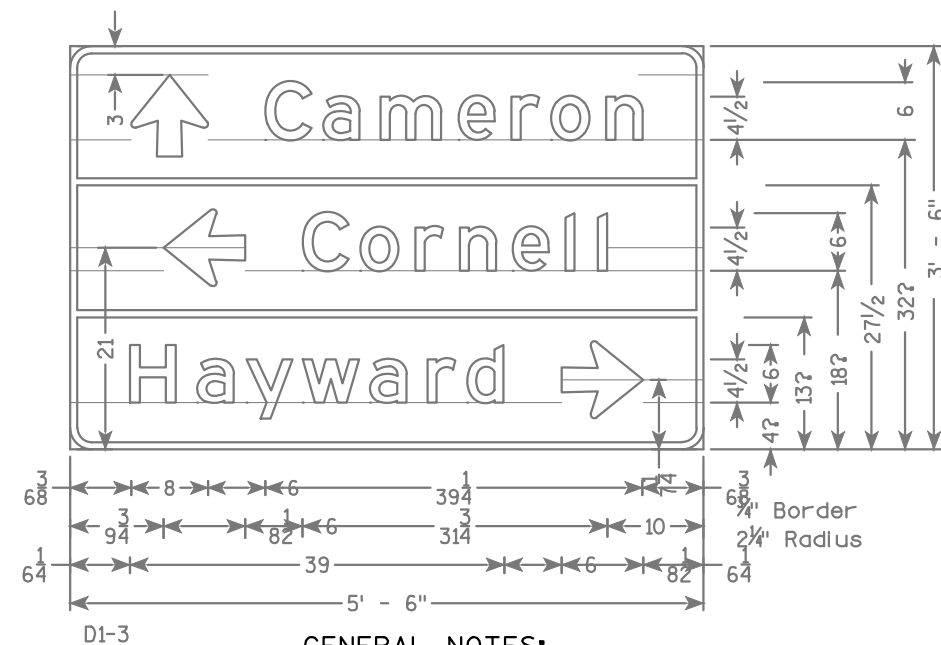
D1-1



D1-1

NOTES

1. All Signs Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Green
Message - White
3. Message Series - E except as noted

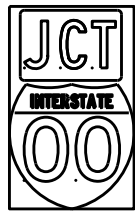


D1-3

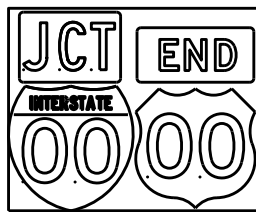
GENERAL NOTES:

1. DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE PLANS.
2. UNLESS OTHERWISE NOTED, ALL SIGNS SHOWN ON THIS SHEET ARE "SIGNS, TYPE XX".
3. UNLESS OTHERWISE NOTED, TYPE I AND TYPE II SIGNS ON THIS SHEET SHALL HAVE "TYPE H REFLECTIVE SHEETING" AND, "TYPE H MESSAGE MATERIAL".
4. UNLESS OTHERWISE NOTED, ALL SIGNS SHOWN ON THIS SHEET SHALL HAVE A GREEN BACKGROUND AND WHITE MESSAGE.
5. ALL UPPER CASE MESSAGE (EXCEPT ON SHIELDS OR WHERE OTHERWISE NOTED) SHALL BE "SERIES E, MODIFIED". ALL LOWERCASE MESSAGE WITH AN INITIAL UPPERCASE LETTER SHALL BE "SERIES E, MODIFIED".
6. UNLESS OTHERWISE NOTED, ALL SIGNS SHOWN ON THIS SHEET SHALL HAVE "TYPE A" OR "TYPE C" ARROWS AS SHOWN. SEE THE STANDARD SIGN PLATES FOR FURTHER DETAILS.
7. SEE THE STANDARD SIGN PLATES FOR FURTHER DETAILS ON ROUTE MARKER SHIELDS.
8. THE SIGN NUMBER IS DENOTED IN THE CIRCLE NEAR EACH DETAIL.
9. DO NOT SCALE.

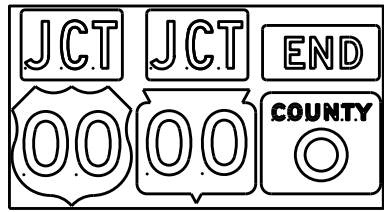
TYPICAL ASSEMBLIES



J1-1



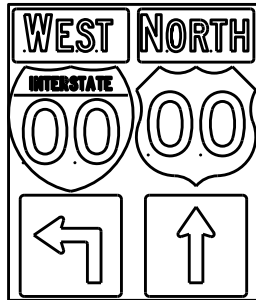
J1-2



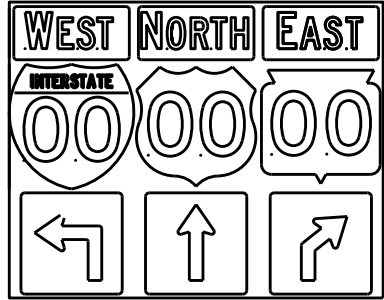
J1-3



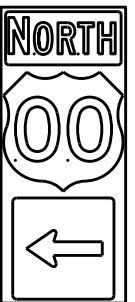
J2-1



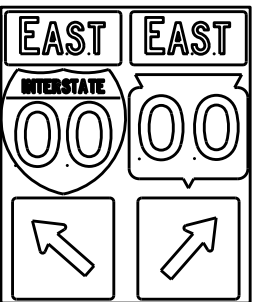
J2-2



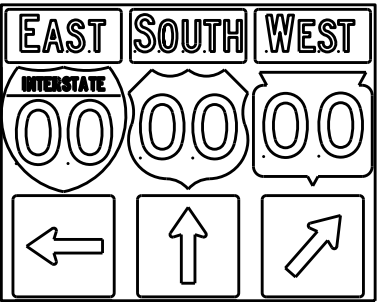
J2-3



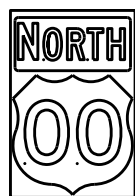
J3-1



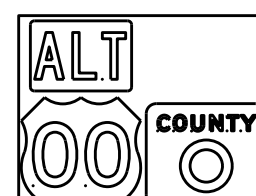
J3-2



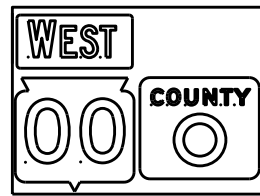
J3-3



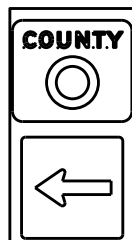
J4-1



J4-2



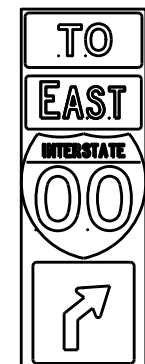
J4-2



J13-1



J12-1



J32-1



J33-1



J23-1

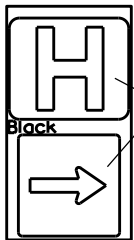


J22-1



JV

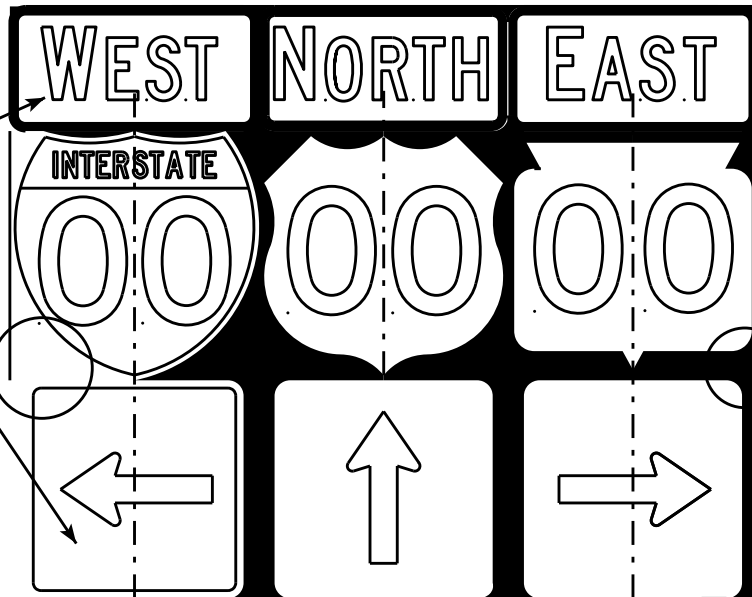
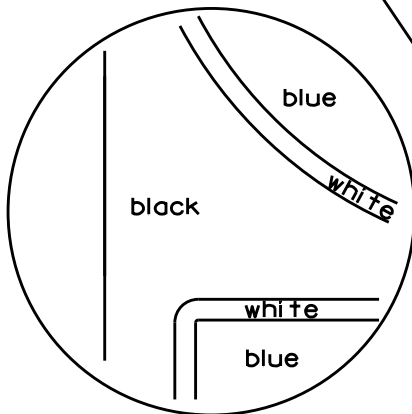
(Typical Vertical J-Assembly
See Note 10 and 11)



JH-1

Blue Background

[blue background
with interstate]



[black background]

ROUTE MARKERS & COMPONENTS
IN TYPICAL ASSEMBLIES

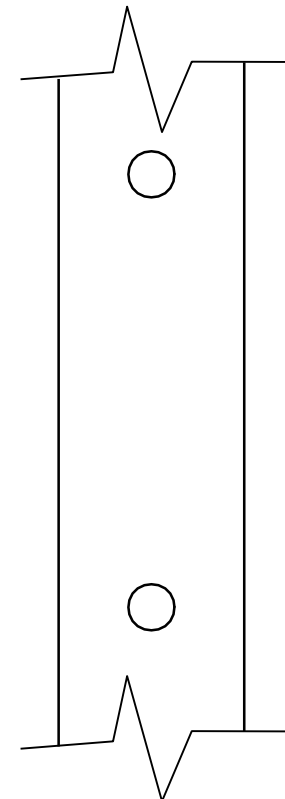
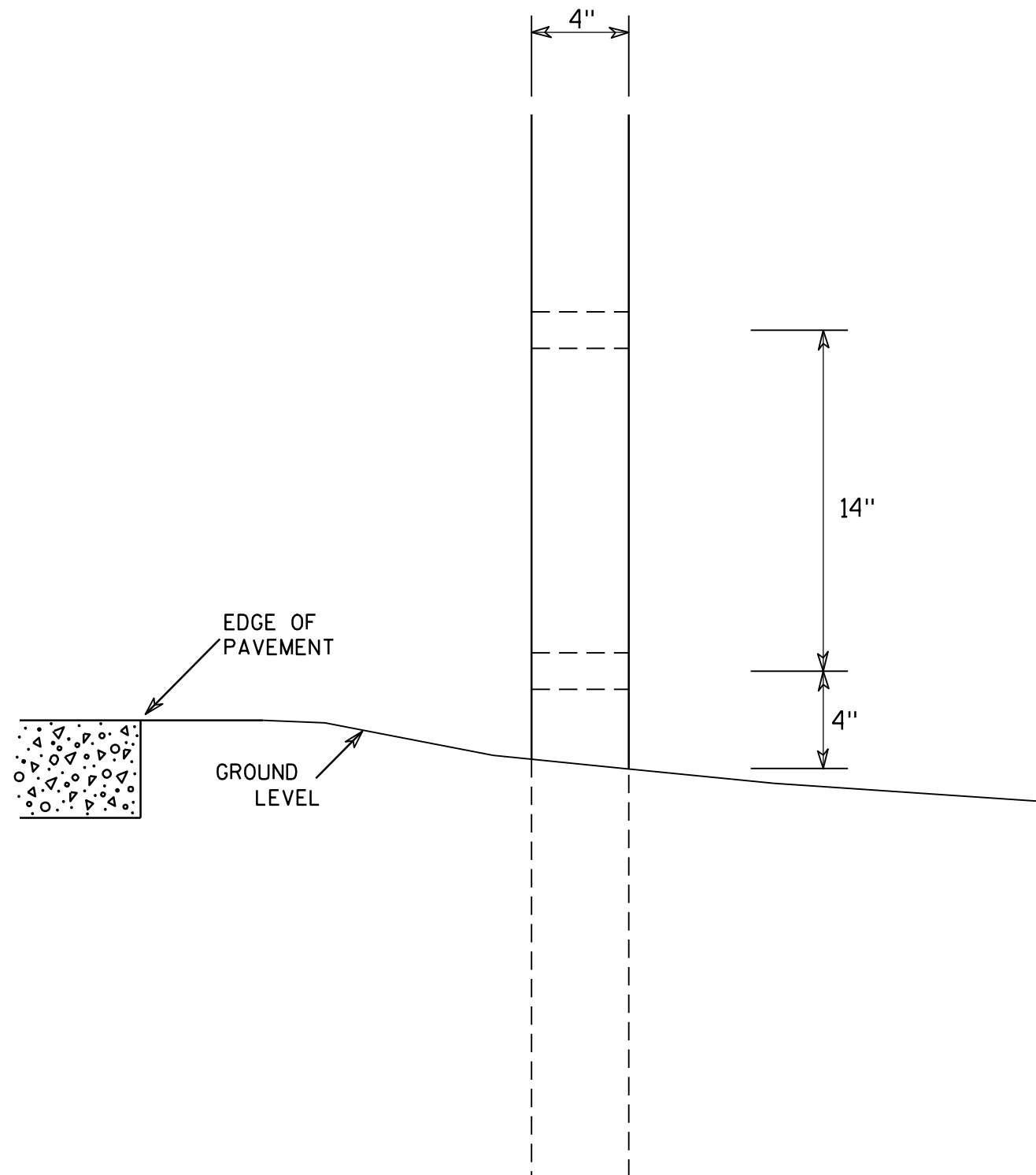
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/06/14 PLATE NO. A2-1S.8

NOTES

1. Signs are Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Black Non-reflective
Message - see Note 5
3. Message Series - See Note 5
4. Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
5. The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
6. Certain marker heads require the component pieces to be the same color. As an example, all the components used with an M1-1 Interstate marker shall be blue.
7. Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
8. Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
9. Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
10. All Vertical J Assemblies are given a Sign Code of JV
11. For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

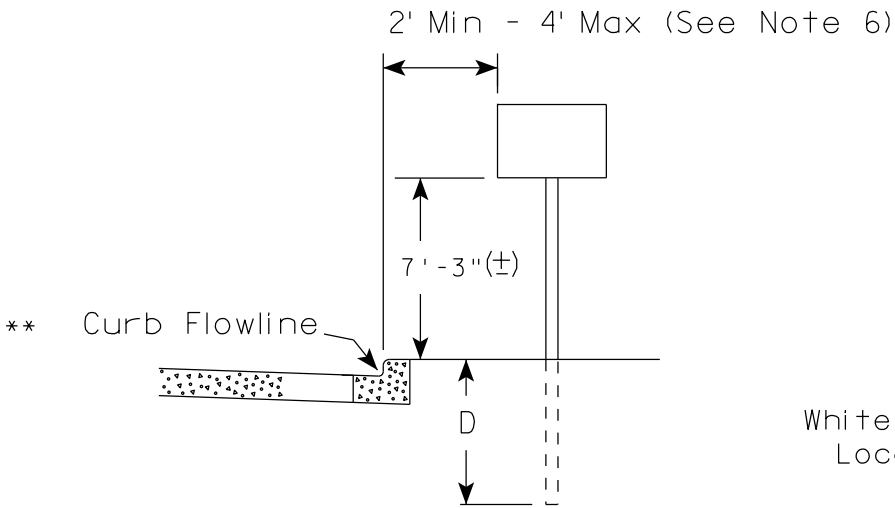
HWY:

COUNTY:

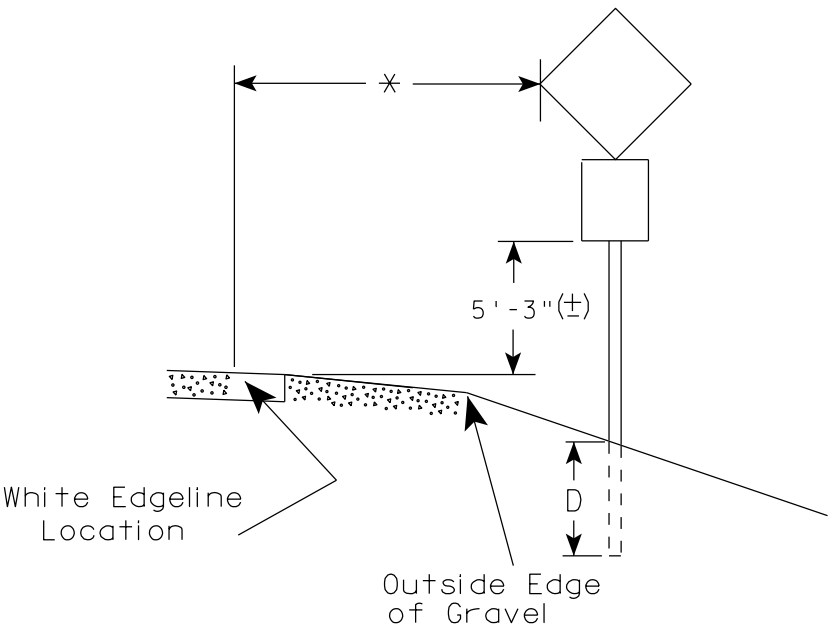
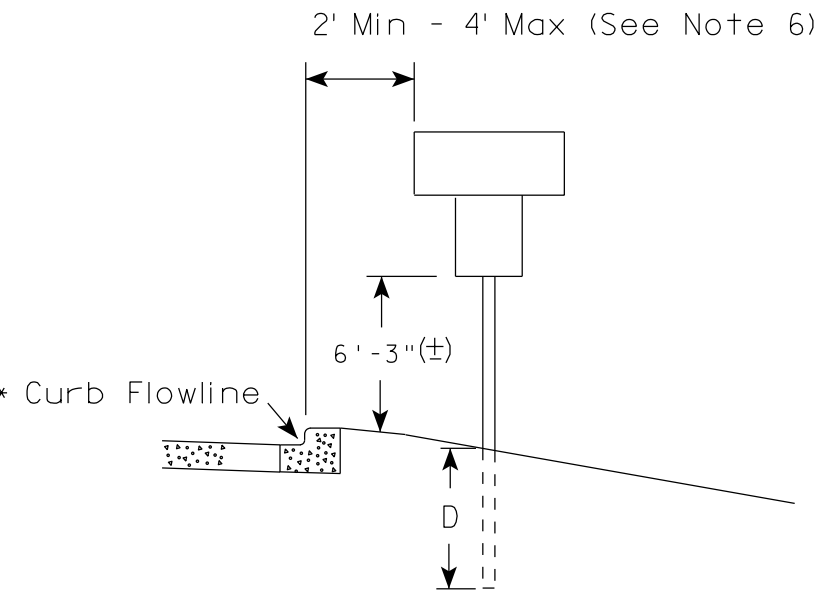
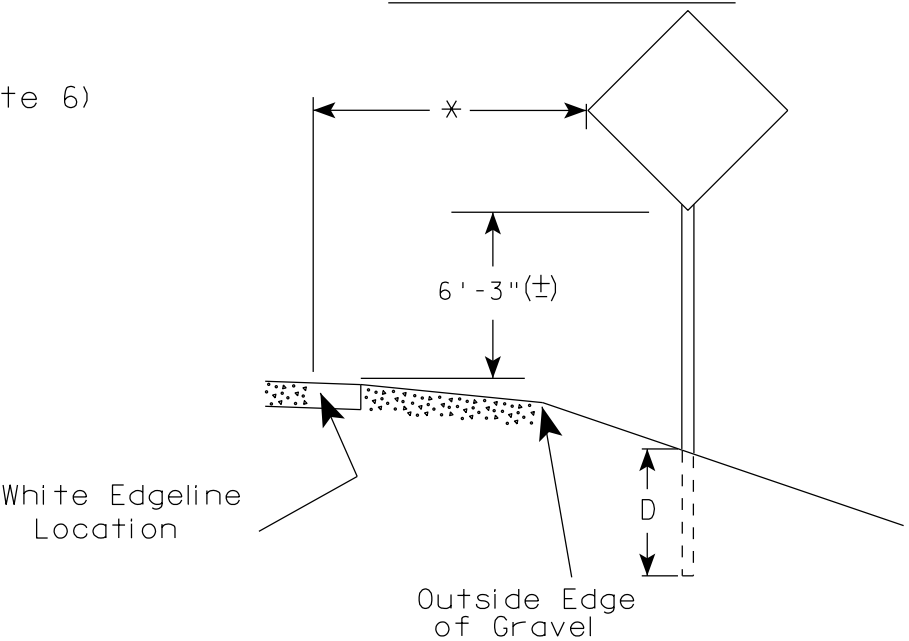
SHEET NO:

E

URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet, 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series) & End of Rod Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (+).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

×× The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

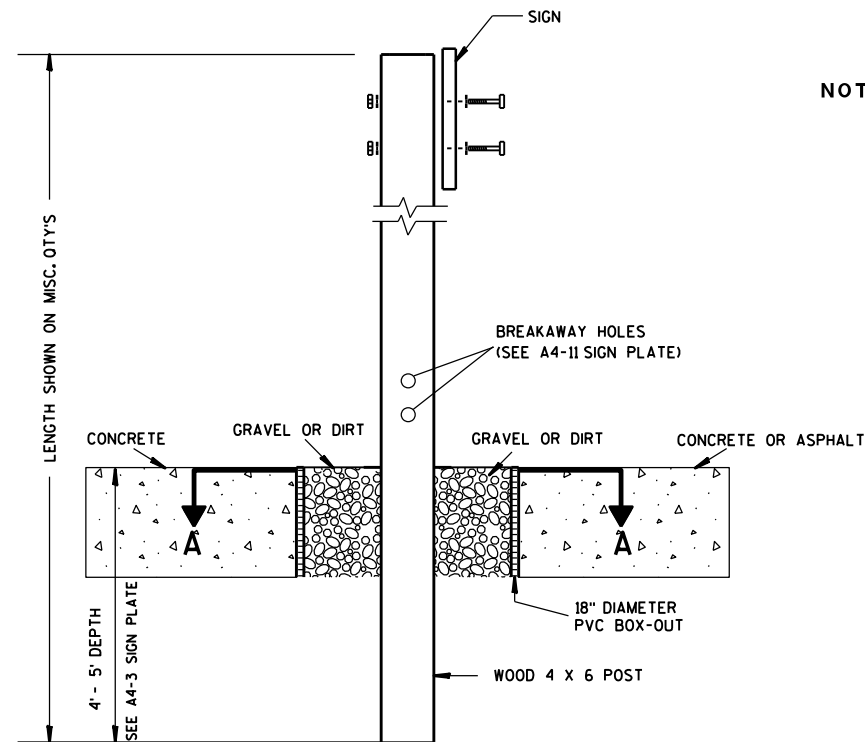
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

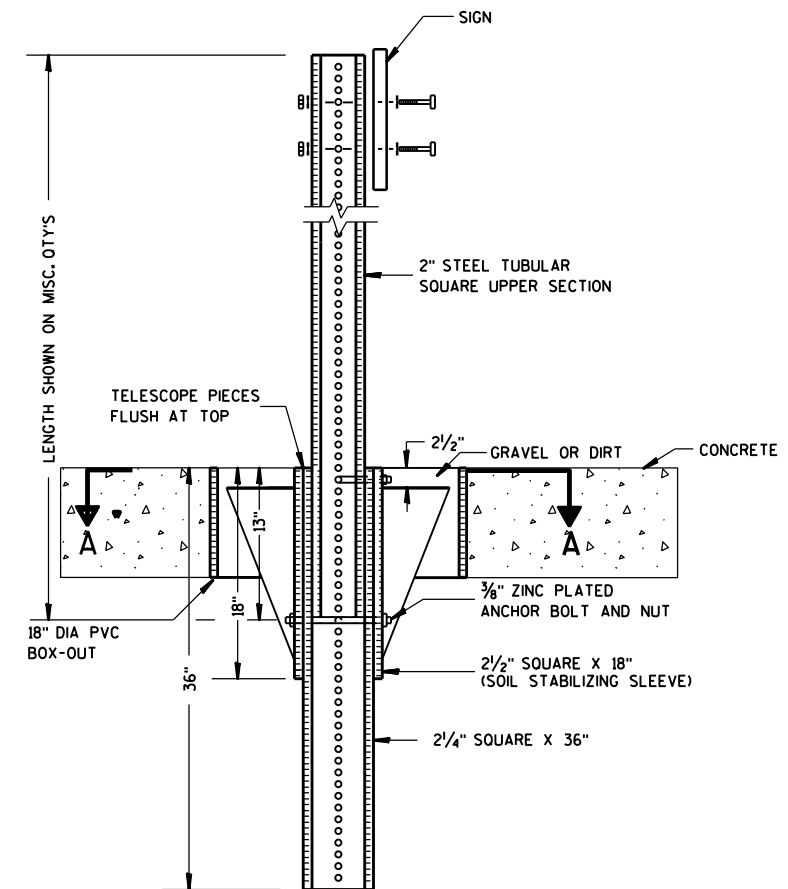
DATE 9/30/13 PLATE NO. A4-3.18



ELEVATION VIEW

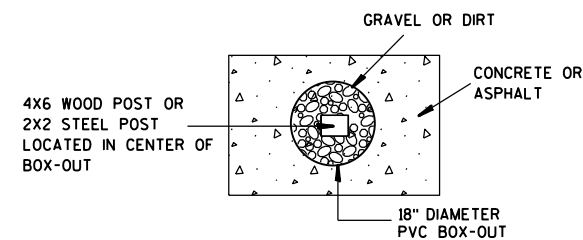
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

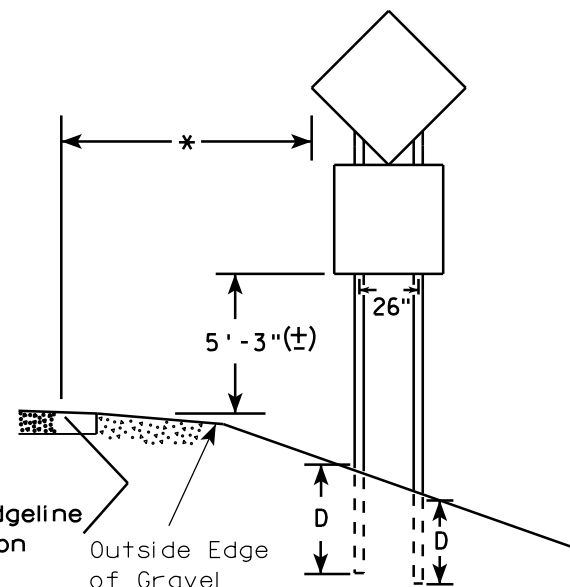
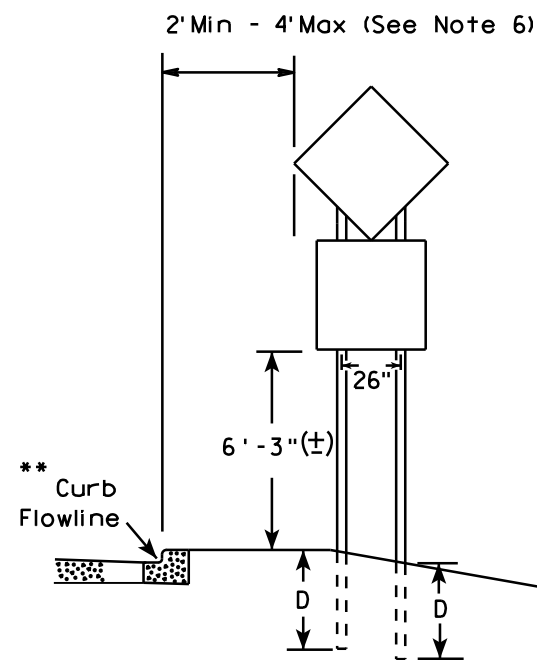
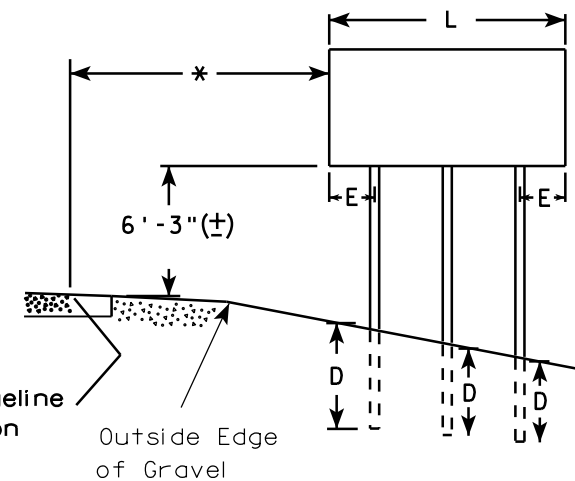
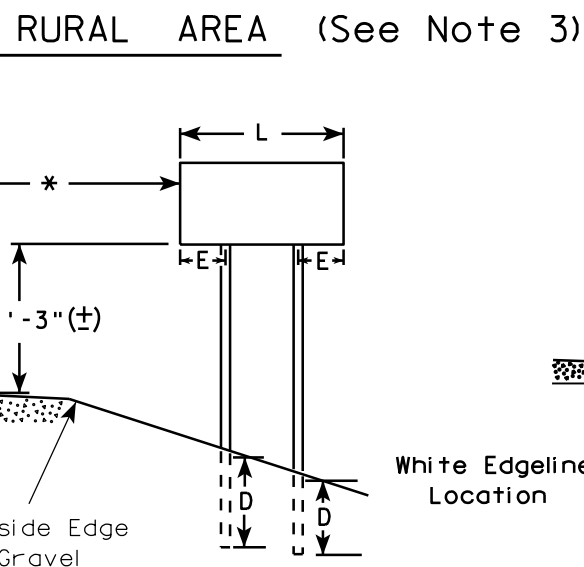
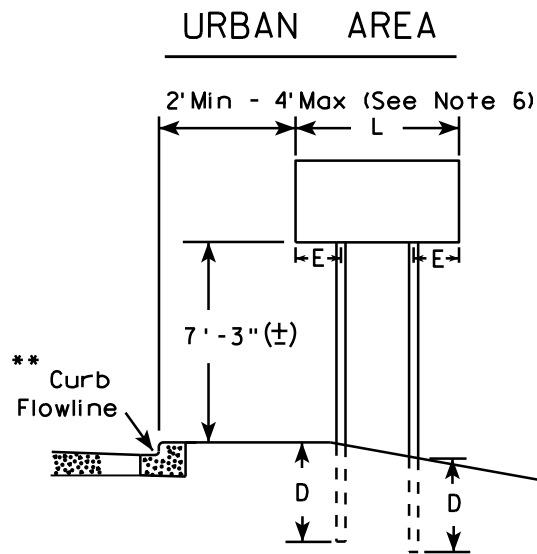
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



48" DIAMOND WARNING SIGN

48" DIAMOND WARNING SIGN

- GENERAL NOTES**
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
 2. See tables below for required number of posts.
 3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
 4. The (±) tolerance for mounting height is 3 inches.
 5. Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 7. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series) & End of Road Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width or 20 S.F. or less in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 120"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

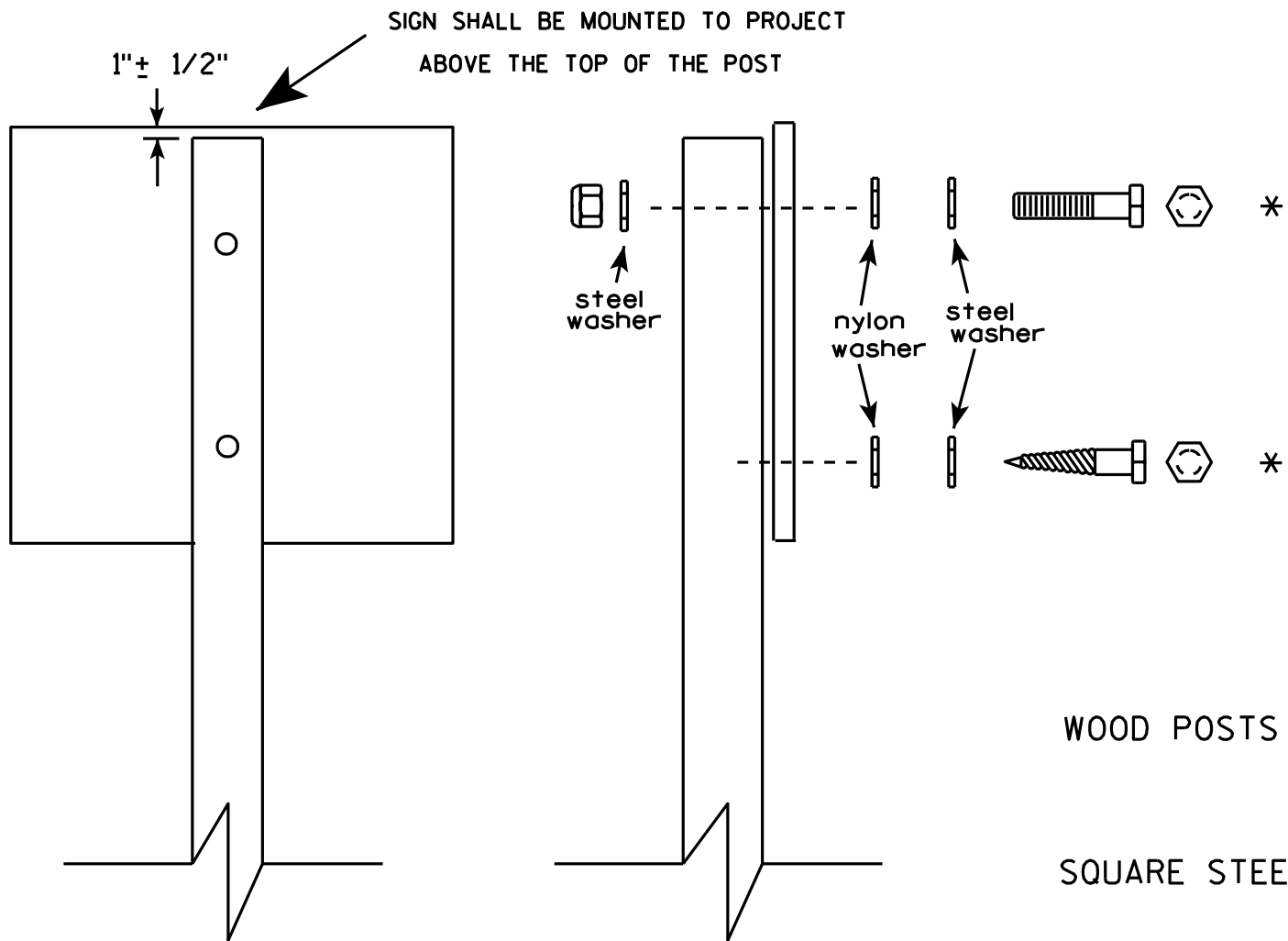
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF TYPE II SIGNS
ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

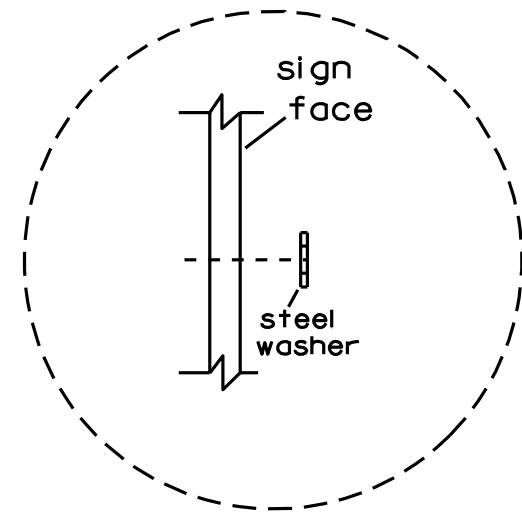
DATE 9/30/13 PLATE NO. A4-4.12



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.



Washer Placement when Sign Has Other Than Type H or Type F Face

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

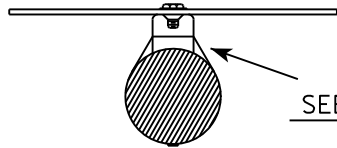
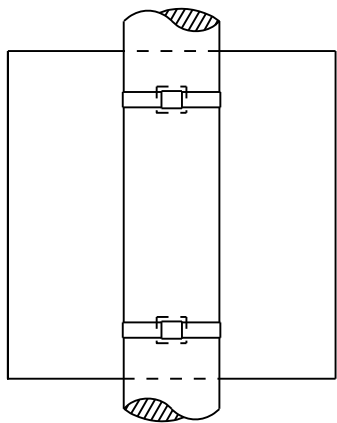
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/23/10 PLATE NO. A4-8.7

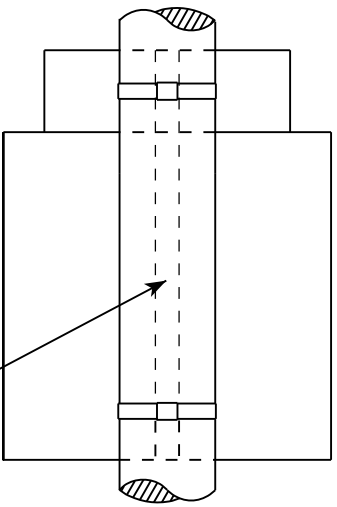
BANDING

SINGLE SIGN

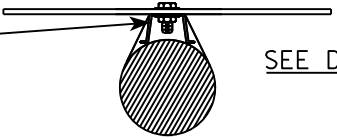


SEE DETAIL A

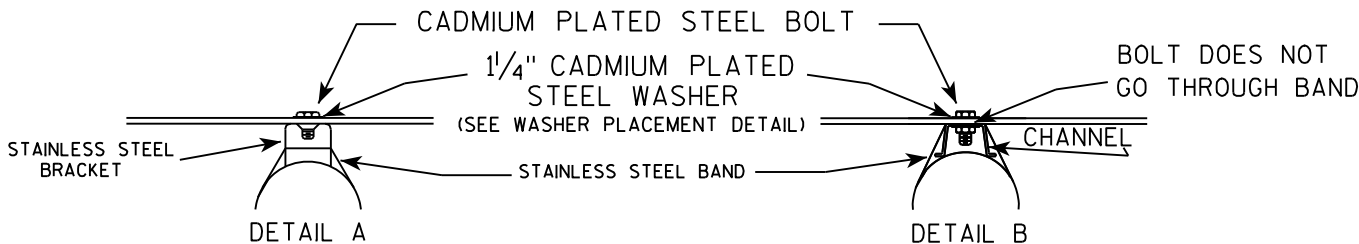
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



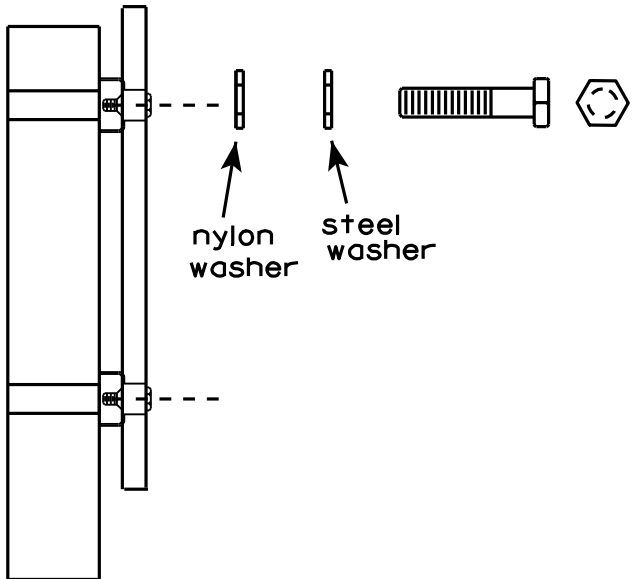
SEE DETAIL B



GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.

WASHER PLACEMENT



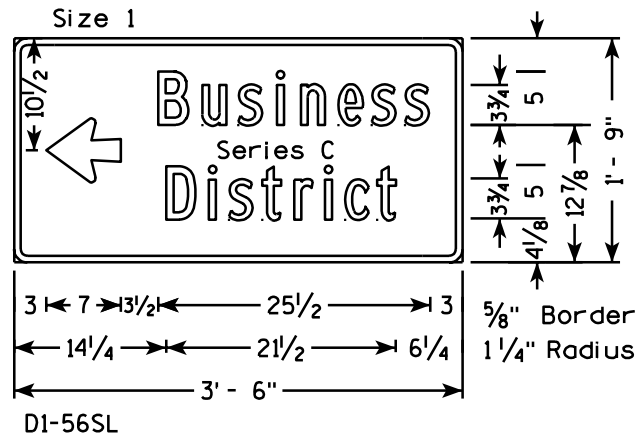
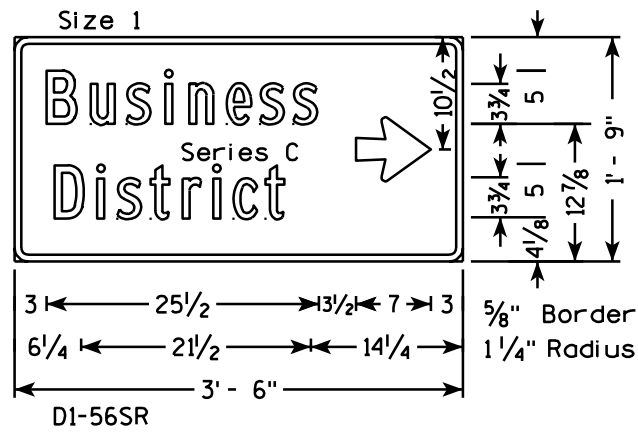
WASHERS (ALL POSTS) -
1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

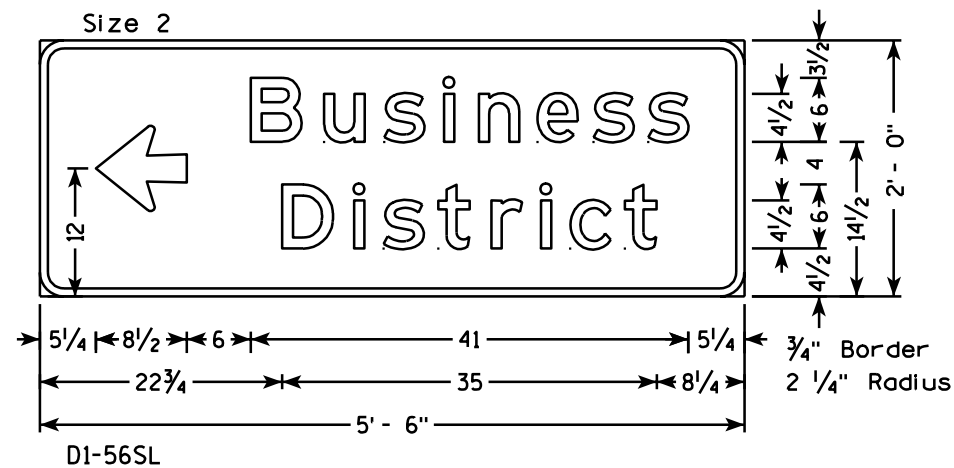
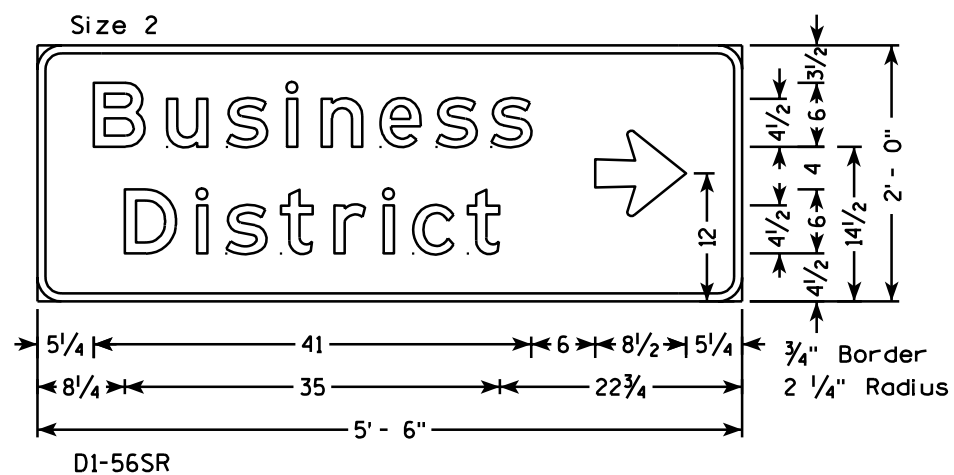
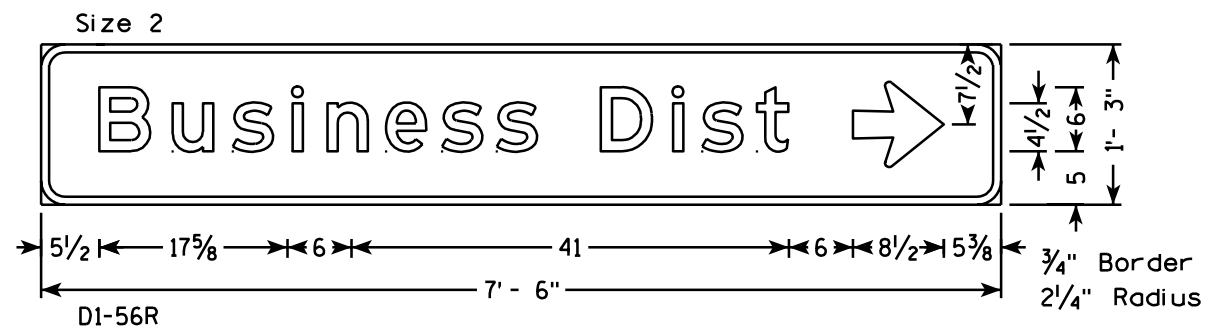
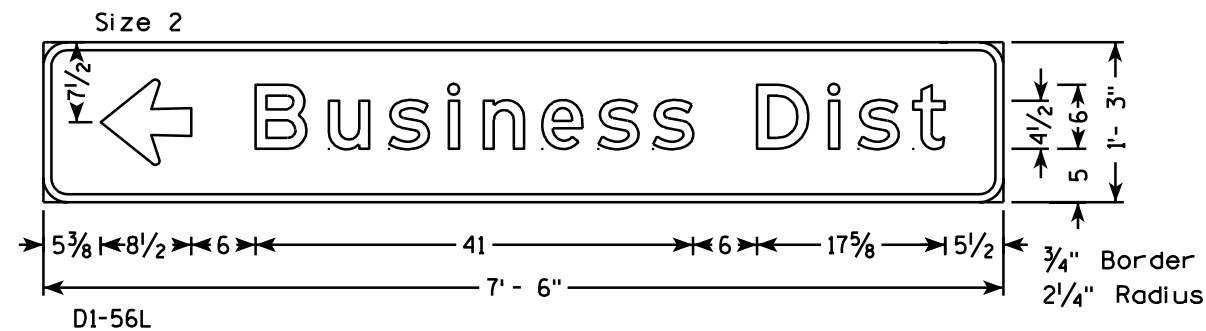
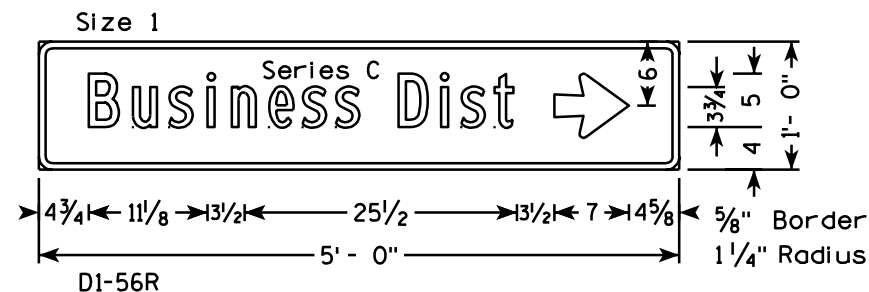
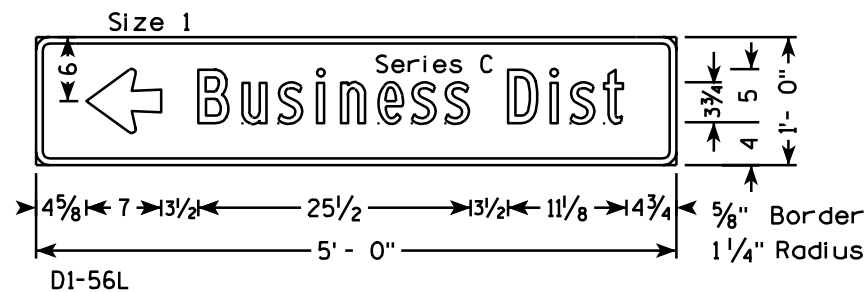
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/16/13 PLATE NO. A5-9.3



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Green
Message - White
3. Message Series - E except as noted
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



TYPICAL STANDARD
D1-56L & R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/19/08 PLATE NO. D1-56.1

PROJECT NO:

HWY:

COUNTY:

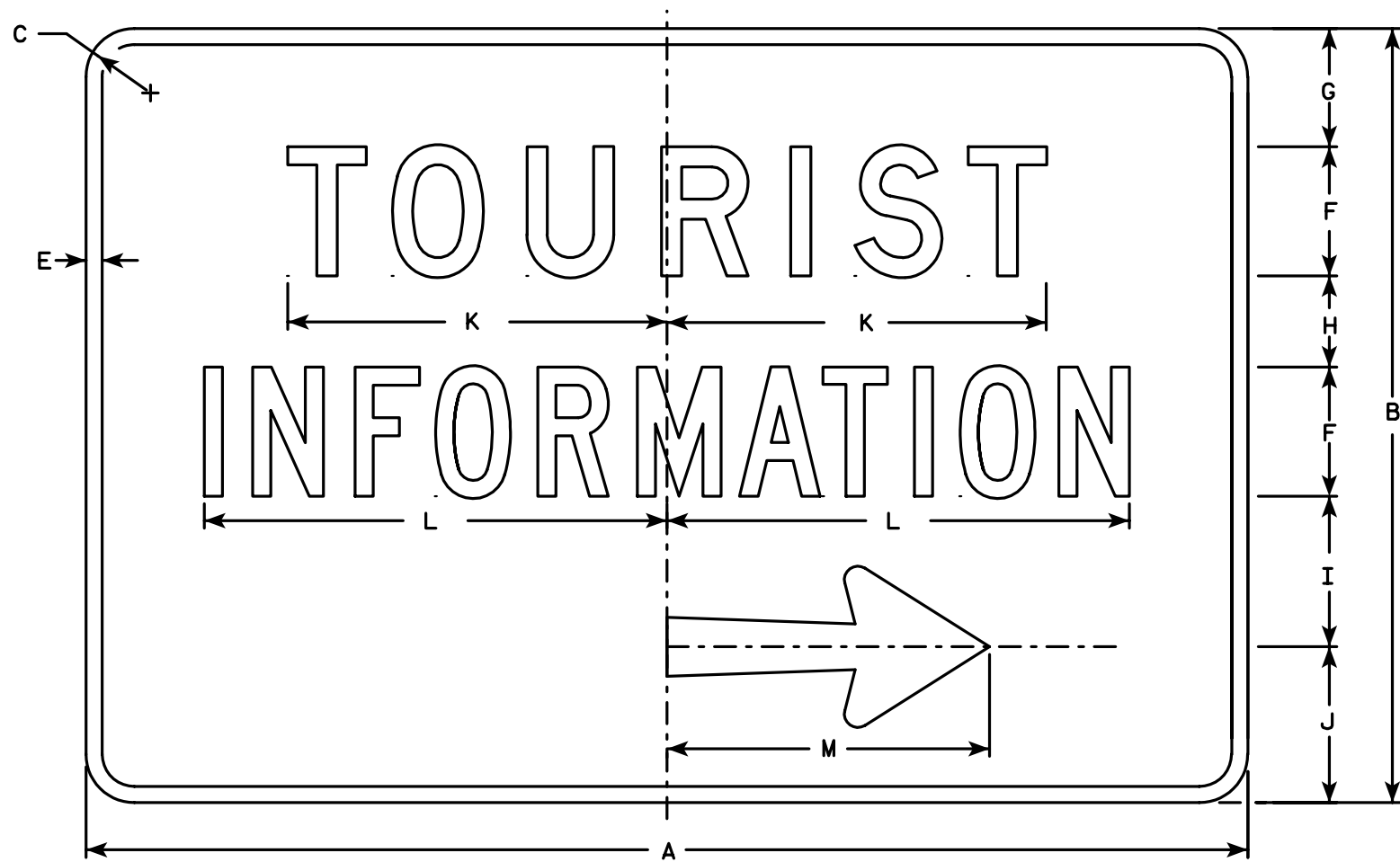
SHEET NO:

E

58, 59, 60, 61, 62, 63

7

LEVELS ON - 2, 3, 5, 6, 10.



D7-59R

Metric equivalent for this sign is:

SIZE	
1	
2	1350 mm X 900 mm
3	
4	
5	

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area sq. m.
1																												
2	54	36	2 1/4		3/4	6	5 1/2	4 1/4	7	7 1/4	17 5/8	21 1/2	15	2 3/4	1/8	2 1/8	6 1/8	5/8	3 1/8								13.5	1.22
3																												
4																												
5																												

STATE PROJECT NUMBER:

FILE NAME : C:\Users\Projects\tr_std\late\D759.DGN

PLOT DATE : 22-JAN-2002 08:12

ORG DATE : 5/21/97

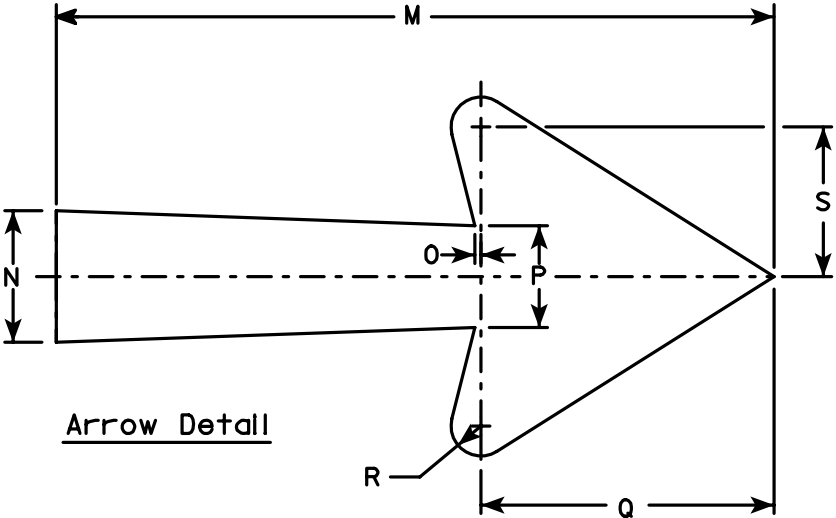
Originator : Don Kluever

SHEET NO:

E

NOTES

- Sign Is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - Blue
Message - White - Type H Reflective
- Message Series - See note 5
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- Line 1 are series D
Line 2 is series C
- D7-59L is same as D7-59R except the arrow is reversed.



STANDARD SIGN
D7-59

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Chita J. Spay*
State Traffic Engineer

DATE 1/11/02 PLATE NO. D7-59.6

7

Metric equivalent
for this sign is:

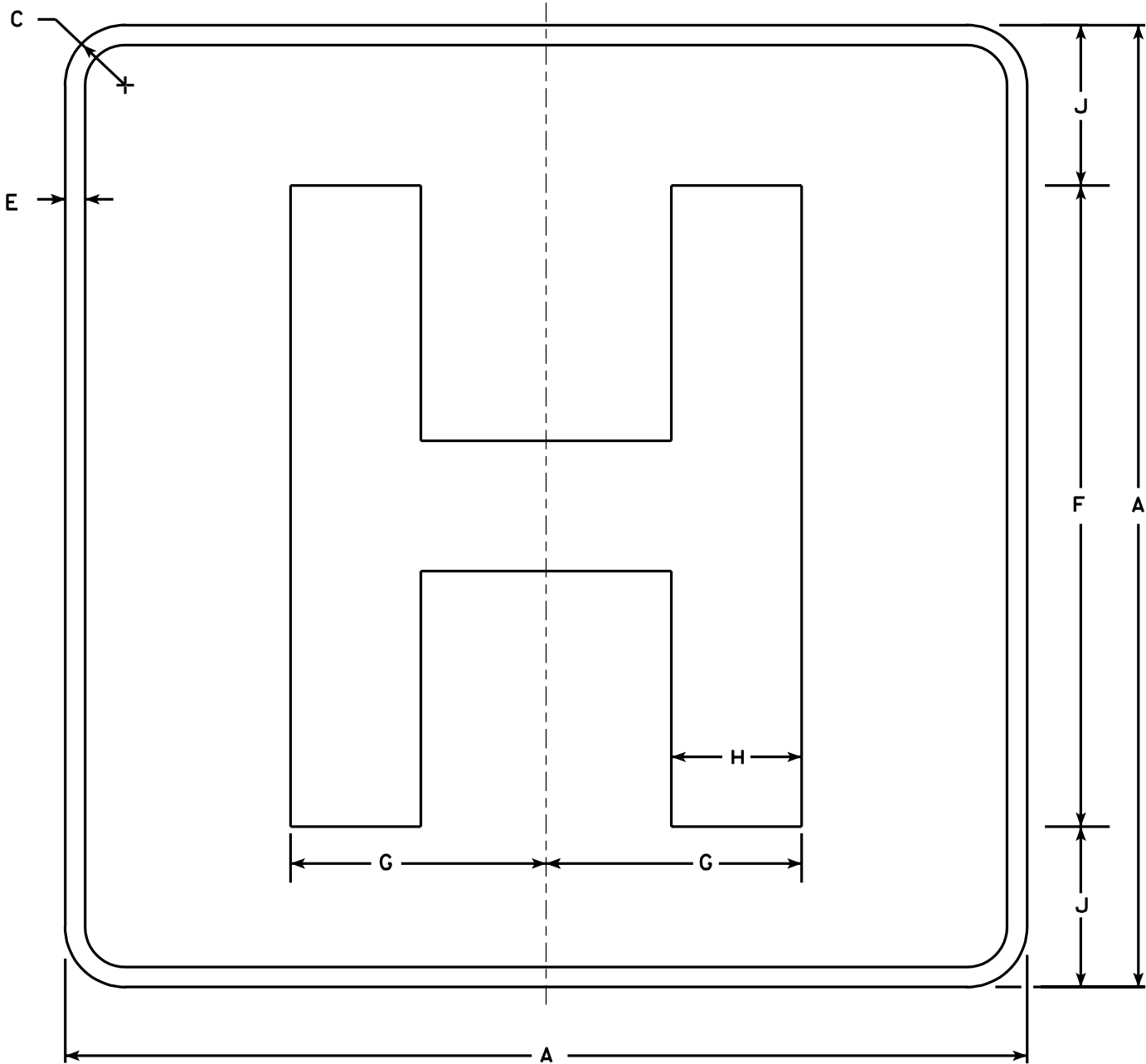
SIZE	
1	450 mmX 450 mm
2	600 mmX 600 mm
3	900 mmX 900 mm
4	X
5	X

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8		1/2	12	4 3/4	2 3/8		3																	4.0
2	24		1 1/2		1/2	16	6 3/8	3 1/4		4																	4.0
3	36		2 1/4		3/4	24	9 1/2	4 7/8		6																	9.0
4																											
5																											

PROJECT NO:

SHEET NO:

E



D9-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Blue
Message - White - Type H Reflective
3. Message Series - E Modified
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

STANDARD SIGN
D9-2

WISCONSIN DEPT OF TRANSPORTATION

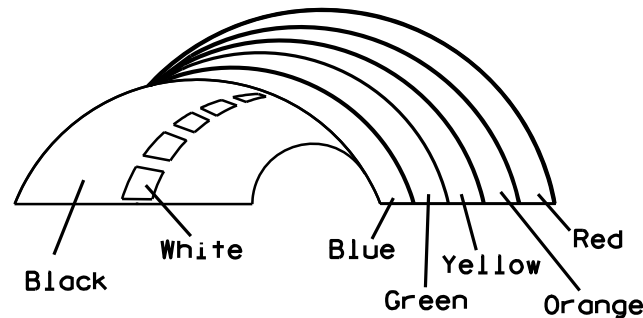
APPROVED
Chester J. Spang
for State Traffic Engineer

DATE 1/15/02 PLATE NO. D9-2.4



* VARIES

Background Colors of Symbol*



*1/4" Black Border between each color of rainbow and border of rainbow

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - (See Note 5)
3. Message Series - (See Note 6)
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Border - Blue
Line 1 - Red
Line 2 - Black
Line 3-5 - Blue
6. Line 1 - Dutch 8011L
Line 2 - Series E
Line 3-5 - Series C
7. Contractor shall provide and install a new post bracket in accordance with the I55-56B sign detail.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	36	1 1/2	1/2	5/8	3	2	3 1/2	2 7/8	1	8	2 1/8	11 1/4	11 1/8	9 3/8	1 1/4		3/4	12 5/8	7 1/2							7.5
3																											
4																											
5																											

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

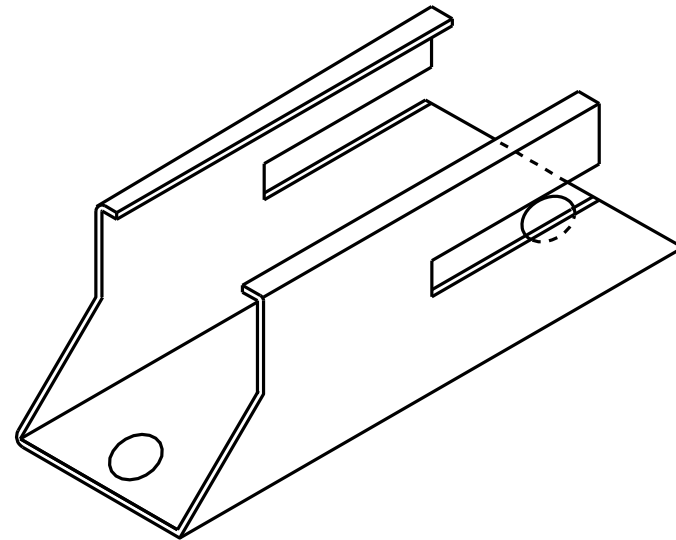
STANDARD SIGN
I55-56

WISCONSIN DEPT OF TRANSPORTATION

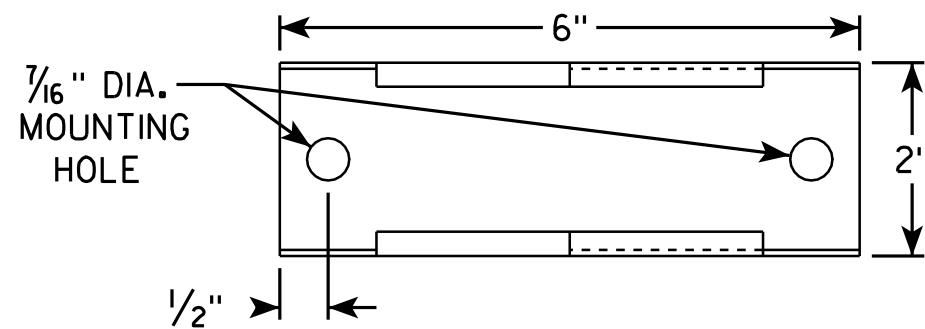
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 4/27/11 PLATE NO. I55-56.3

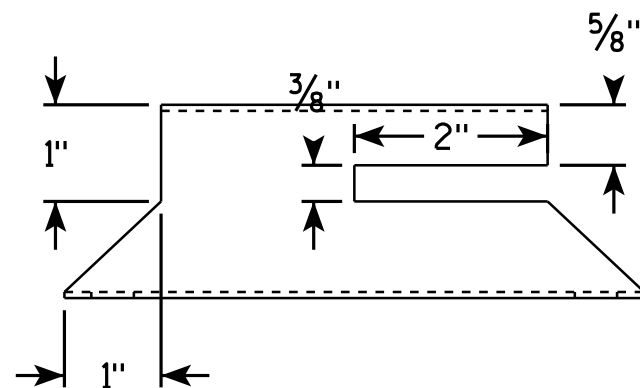
ISOMETRIC VIEW



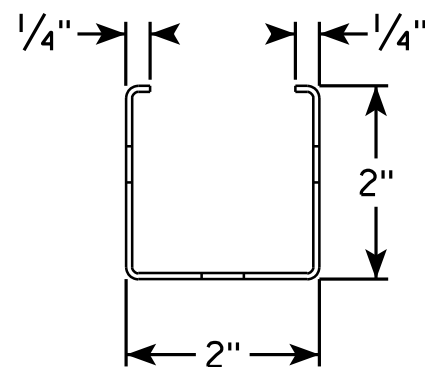
TOP VIEW



SIDE VIEW



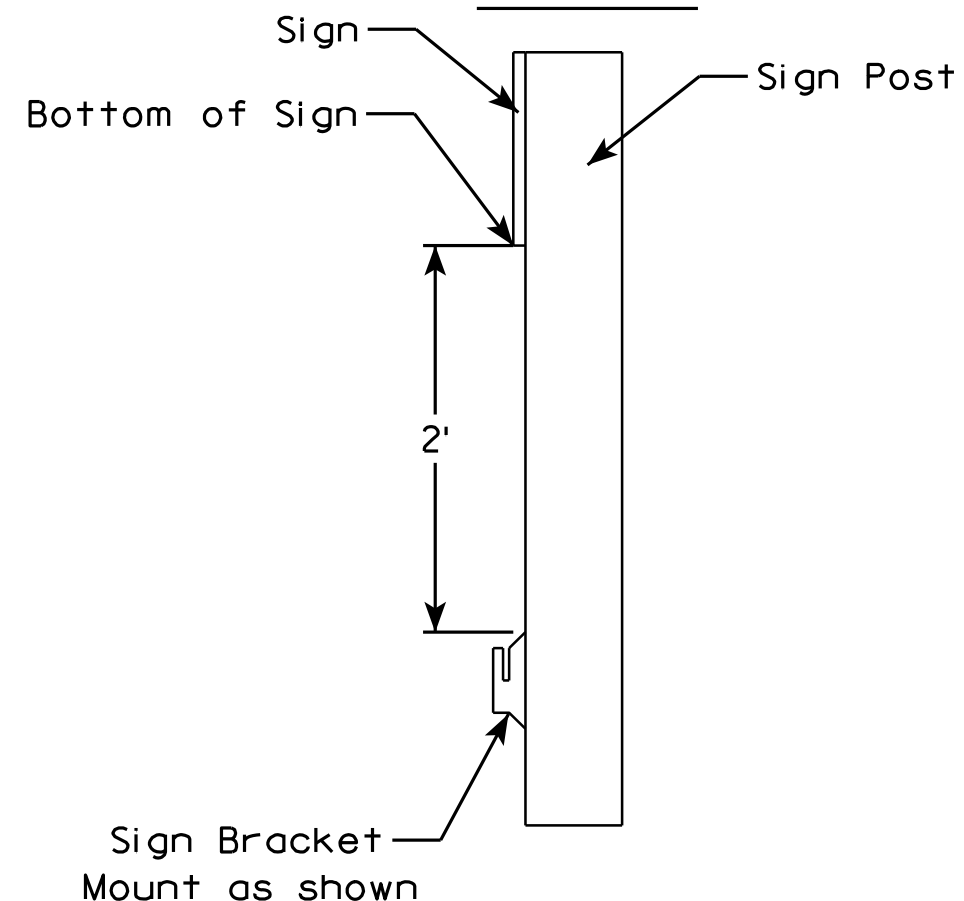
END VIEW



NOTES

1. Must be capable of permanent attachment to a wood or steel channel sign post utilizing the fastening hardware specified on the A4-8 sign plate.
2. Shall be entirely primed and painted with two coats of a black powder coated enamel paint.
3. Shall be made with 12 gauge steel, and incorporate no welds, no hinged components, no threaded lock-type components, and no parts which are loose or can be separated from the main body.
4. Shall have rounded edges with at least 1/8" radii.
5. Shall not have unrounded and uncoated metal edges which can contact the back surface of the roll-up sign.
6. Top of bracket shall be mounted 2' below the bottom of the I55-56 sign.
7. Cost of bracket and fastening hardware shall be incidental to the I55-56 sign.

SIDE VIEW



ROLLUP SIGN BRACKET
I55-56B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/5/10 PLATE NO. I55-56B.1

PROJECT NO:

HWY:

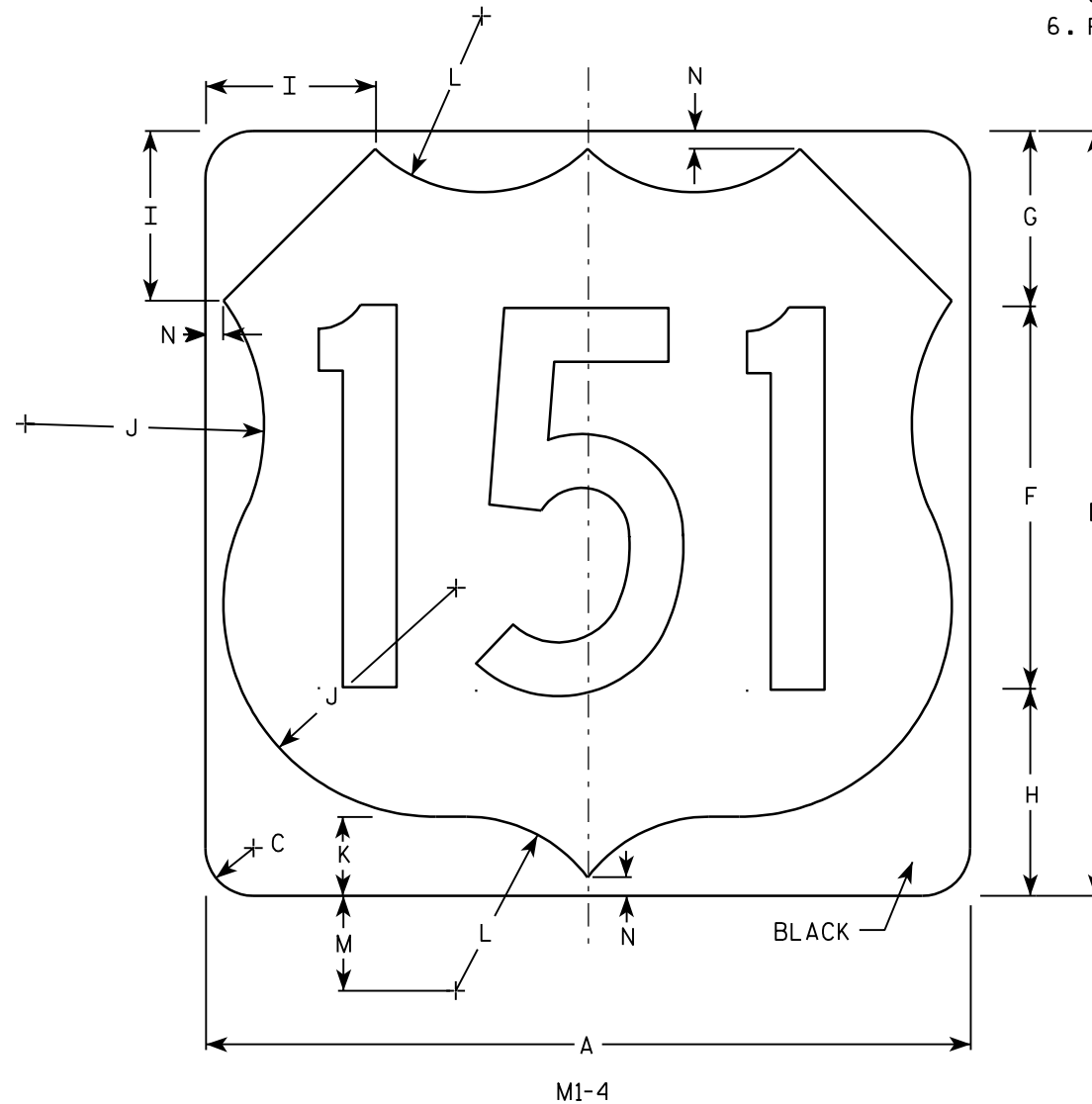
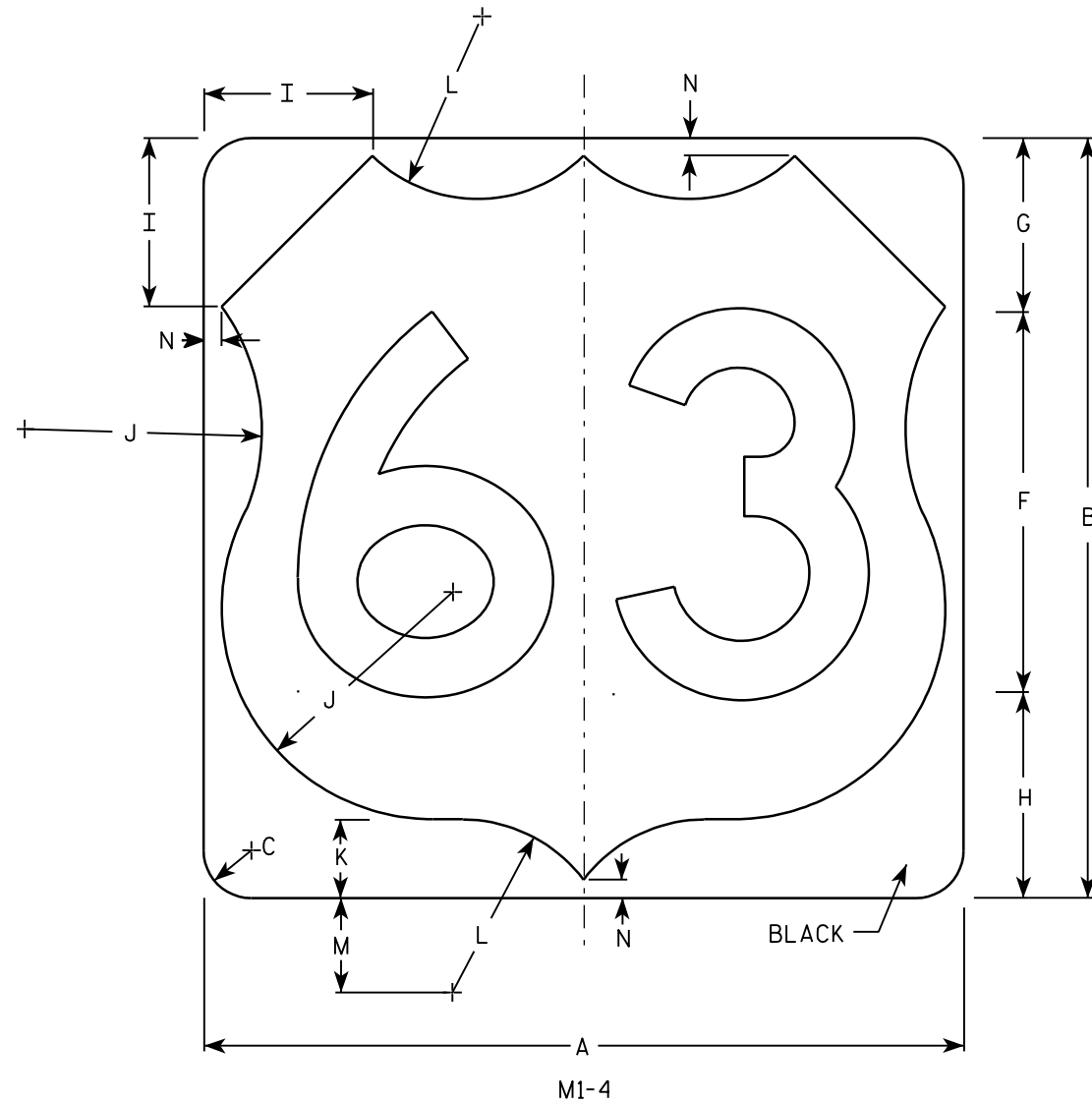
COUNTY:

SHEET NO:

E

NOTES

1. Sign is Type II - See Note 6 - reference
WIS DOT Standard Specification for HIGHWAY
and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 6
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base
material is plywood but borders shall be rounded
as shown. When base material is metal, the
corners and borders shall be rounded.
5. Substitute appropriate numerals and adjust
spacing as per Plate A10-1.
6. Permanent Signs
Background - Type H Reflective
Detour or other temporary signs
Background - Reflective



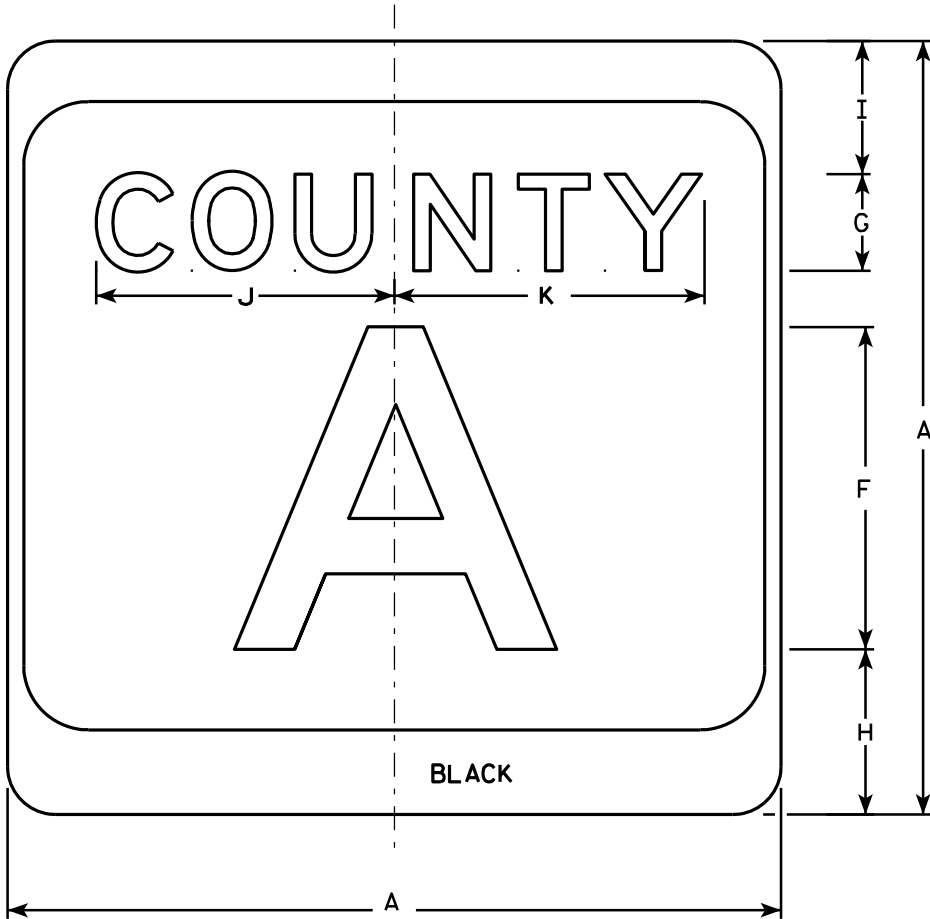
Metric equivalent
for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

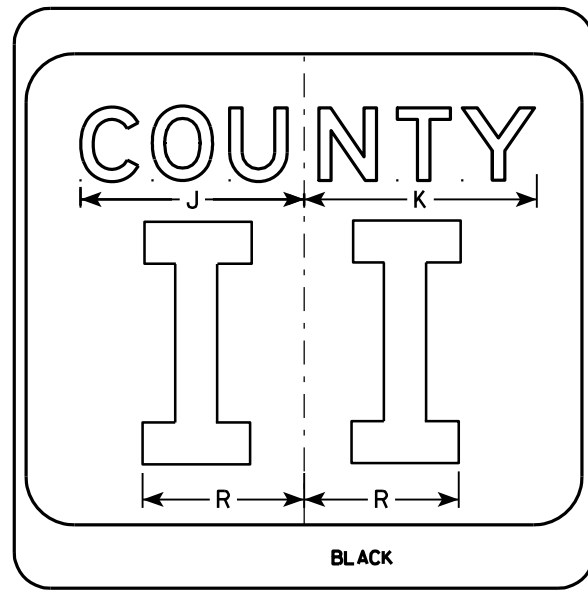
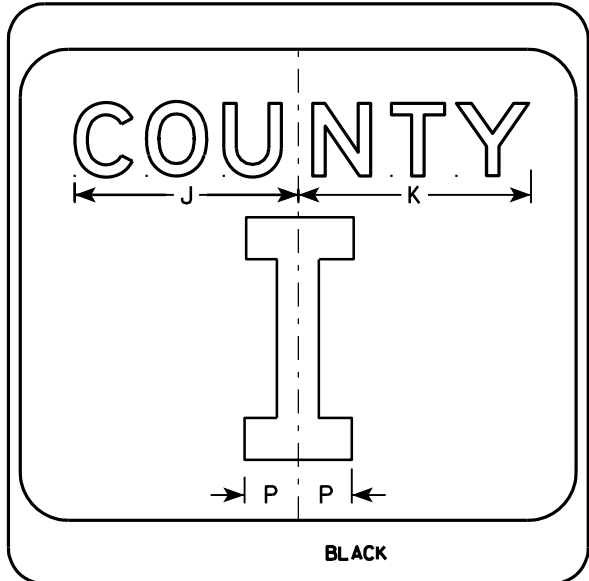
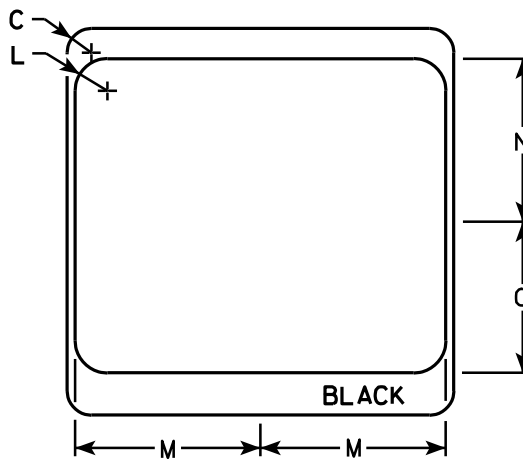
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Areq sq. ft.	Areq m ²
1																												
2	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0	.36
3	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81
4	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81
5	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81

PROJECT NO: HWY: COUNTY: SHEET NO: E

7



M1-5A



NOTES

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 7
Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.
Message Series D for 2 letters unless message is too big then Series C.
Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

CTH MARKER
M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 9/27/11 PLATE NO. M1-5A.8

PROJECT NO:

HWY:

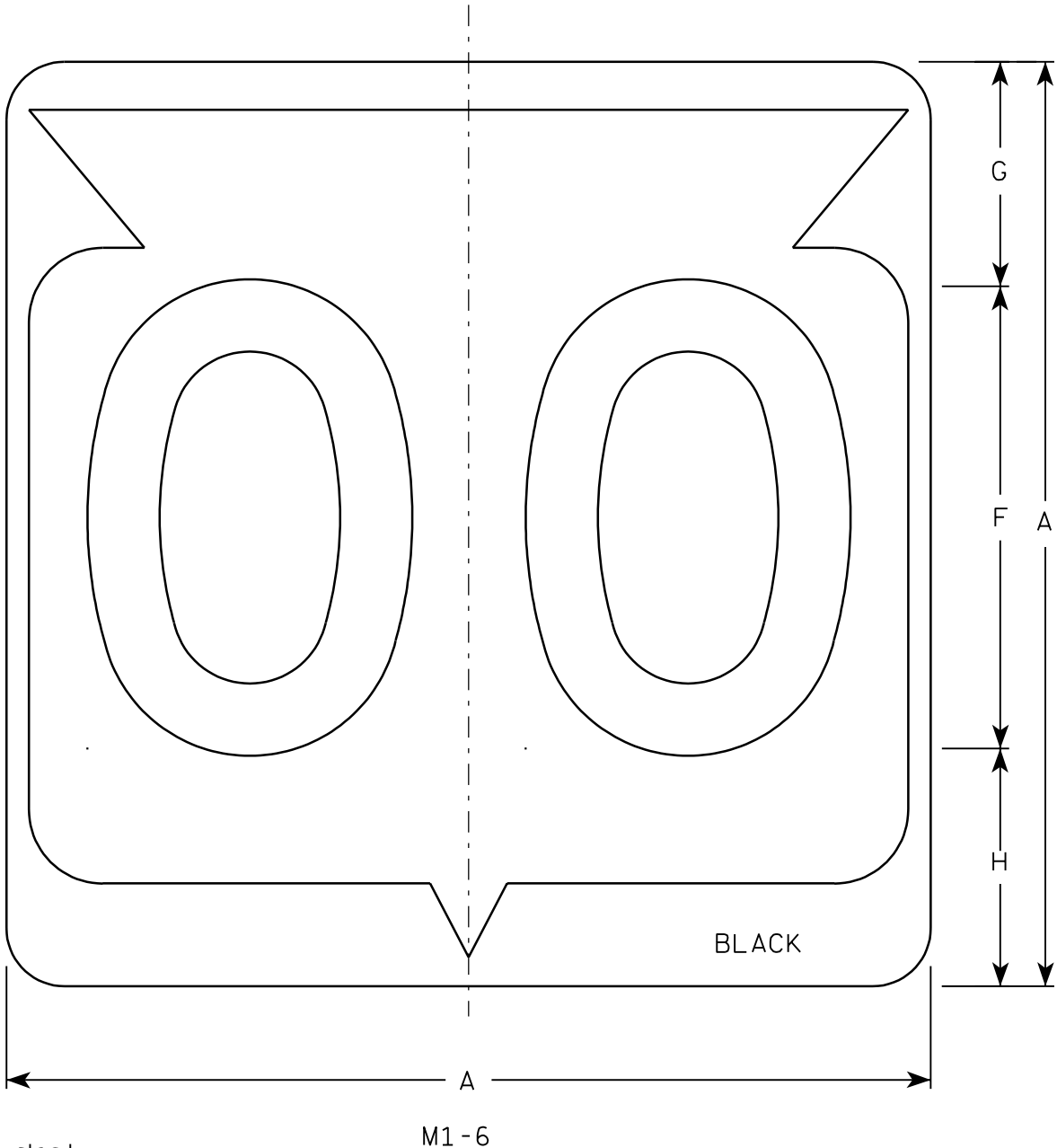
COUNTY:

SHEET NO:

E

7

7



Metric equivalent
for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0	.36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

FILE NAME : C:\Users\Projects\tr_stdp\late\M16.DGN

PLOT DATE : 13-OCT-2005 14:55

PLOT BY : DITJPH

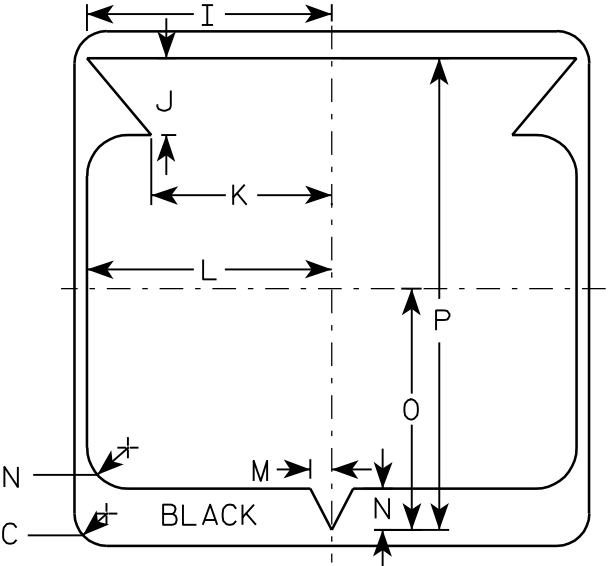
PLOT NAME :

PLOT SCALE : 6.715871:1.000000

WISDOT/CADDS SHEET 42

NOTES

1. Sign is Type II - See Note 6 - reference
WIS DOT Standard Specification for HIGHWAY
and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 6
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base
material is plywood but borders shall be rounded
as shown. When base material is metal, the
corners and borders shall be rounded.
5. Substitute appropriate Series numerals and
adjust spacing as per plate A10-1.
6. Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective



STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

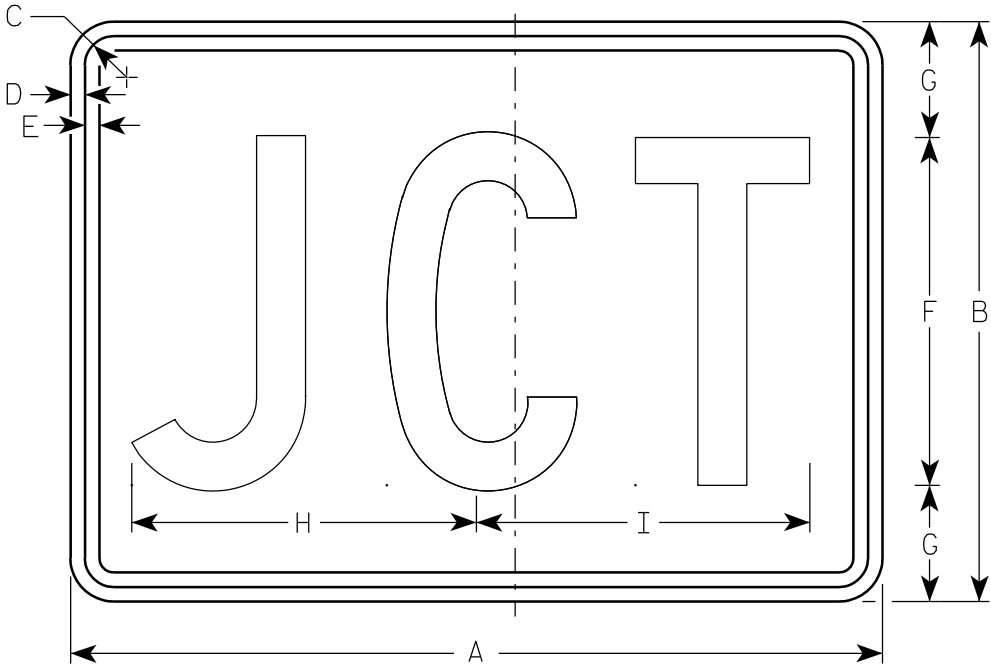
Chester J. Spang
for State Traffic Engineer

DATE 3/20/02

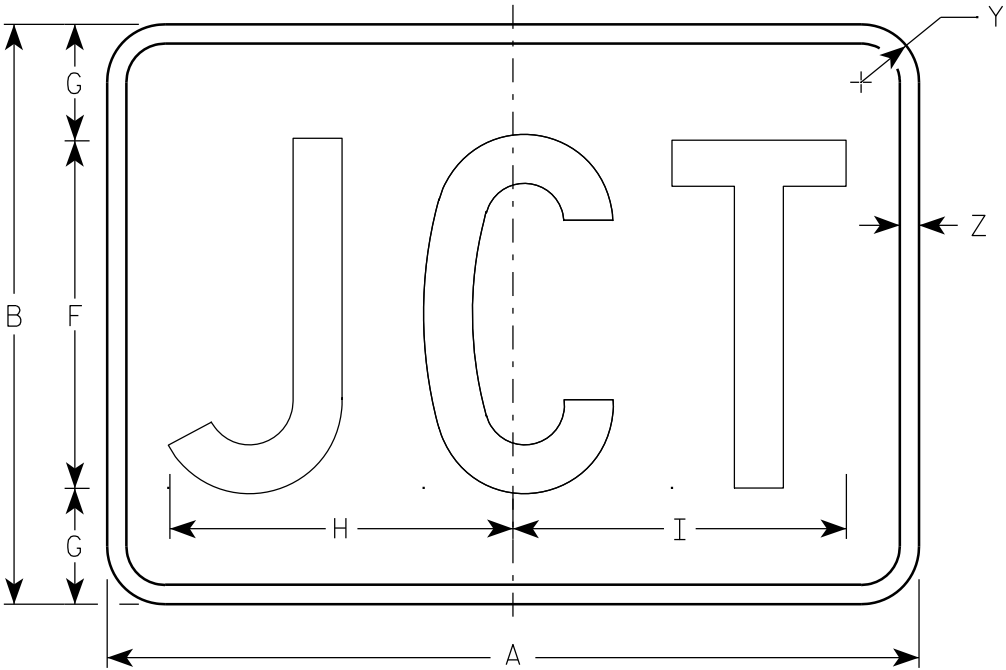
PLATE NO. M1-6.9

NOTES

1. Sign is Type II - See Note 5 - reference
WIS DOT Standard Specification for HIGHWAY
and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base
material is plywood but borders shall be rounded
as shown. When base material is metal, the
corners and borders shall be rounded.
5. M2-1 Background - White - Type H Reflective
(Detour or temporary Signs - Reflective)
Message - Black
MB2-1 Background - Blue
Message - White - Type H Reflective
(Detour or temporary Signs - Reflective)
MG2-1 Background - Green
Message - White - Type H Reflective
MK2-1 Background - Green
Message - White - Type H Reflective
MM2-1 Background - White - Type H Reflective
Message - Green
MN2-1 Background - Brown
Message - White - Type H Reflective
MR2-1 Background - Brown
Message - Yellow - Type H Reflective



M2-1
MK2-1
MM2-1
MR2-1



MB2-1
MG2-1
MN2-1

Metric equivalent
for this sign is:

SIZE	
1	
2	525 mm X 375 mm
3	750 mm X 525 mm
4	750 mm X 525 mm
5	750 mm X 525 mm

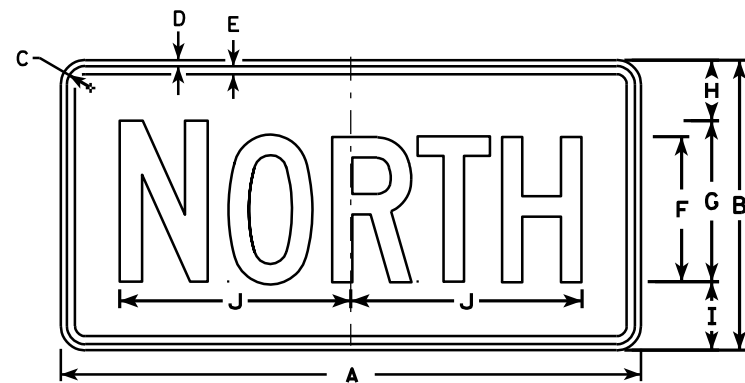
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20	0.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20

STANDARD SIGN
M2 - 1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

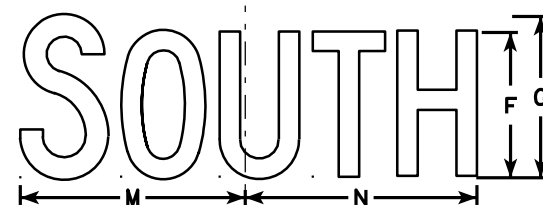
DATE 3/16/10 PLATE NO. M2-1.10



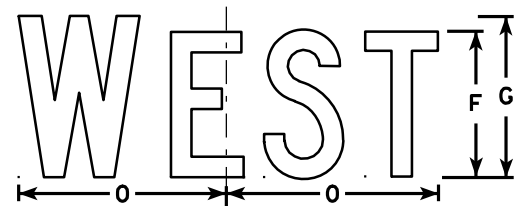
M3-1
MK3-1
M03-1



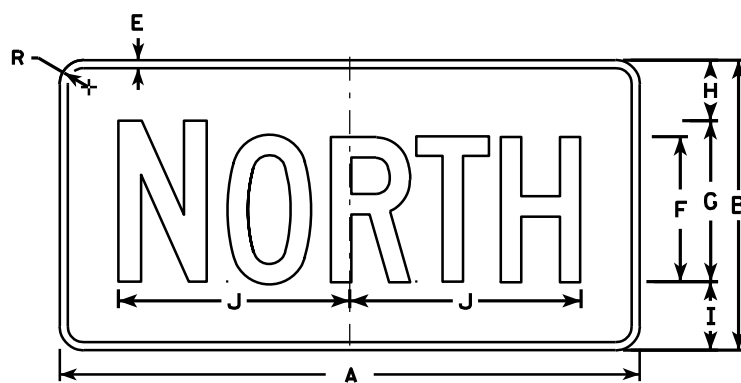
M3-2
MK3-2
M03-2



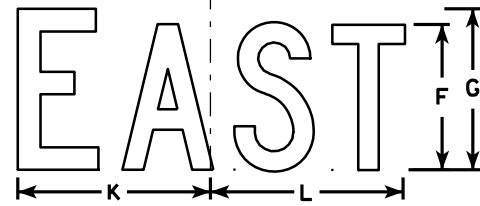
M3-3
MK3-3
M03-3



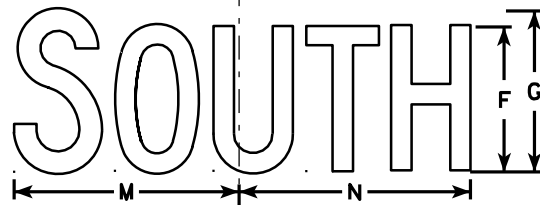
M3-4
MK3-4
M03-4



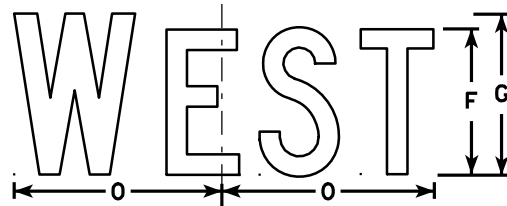
MB3-1
MG3-1
MM3-1
MN3-1



MB3-2
MG3-2
MM3-2
MN3-2



MB3-3
MG3-3
MM3-3
MN3-3



MB3-4
MG3-4
MM3-4
MN3-4

NOTES

1. All Signs Type II - See Note 5 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White - Type H Reflective (Detour or temporary signs - Reflective)
Message - Black
MB3-1 thru MB3-4 Background - Blue
Message - White - Type H Reflective (Detour or temporary signs - Reflective)
MG3-1 thru MG3-4 Background - Green
Message - White - Type H Reflective
MK3-1 thru MK3-4 Background - Green
Message - White - Type H Reflective
MM3-1 thru MM3-4 Background - White - Type H Reflective
Message - Green
MN3-1 thru MN3-4 Background - Brown
Message - White - Type H Reflective
M03-1 thru M03-4 Background - Orange - Reflective
Message - Black
6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

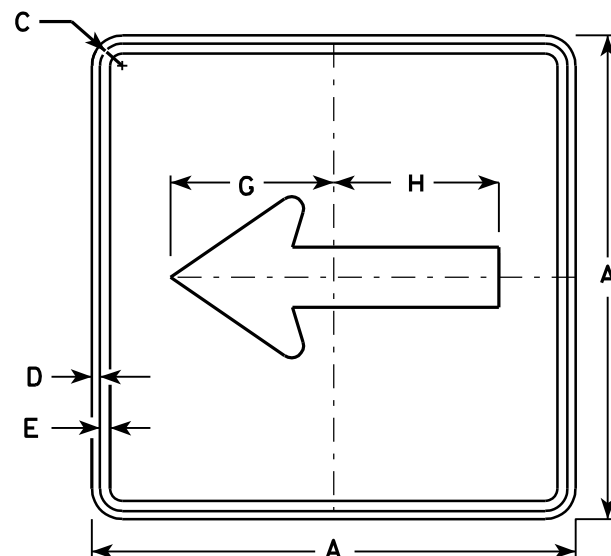
PROJECT NO: HWY: COUNTY: SHEET NO: E

STANDARD SIGNS M3-1 thru M3-4 SERIES

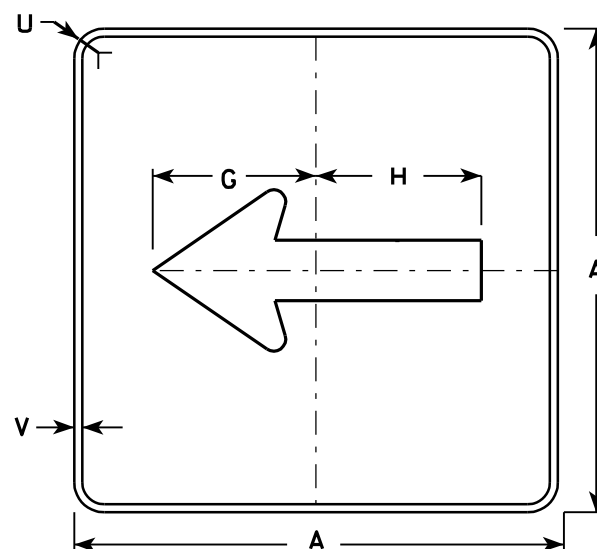
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

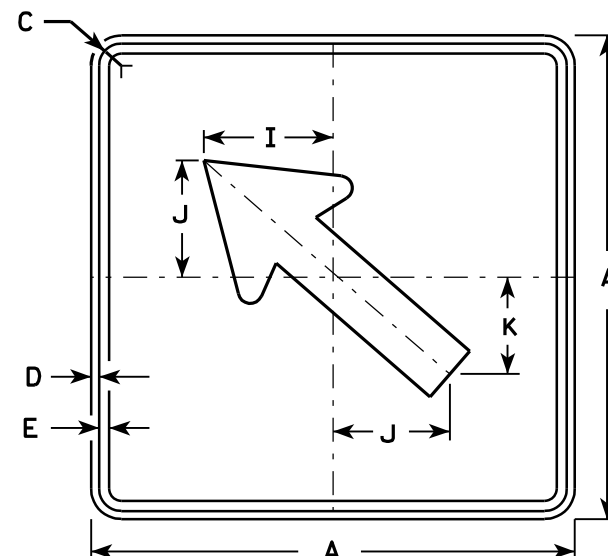
DATE 11/10/10 PLATE NO. M3-1.12



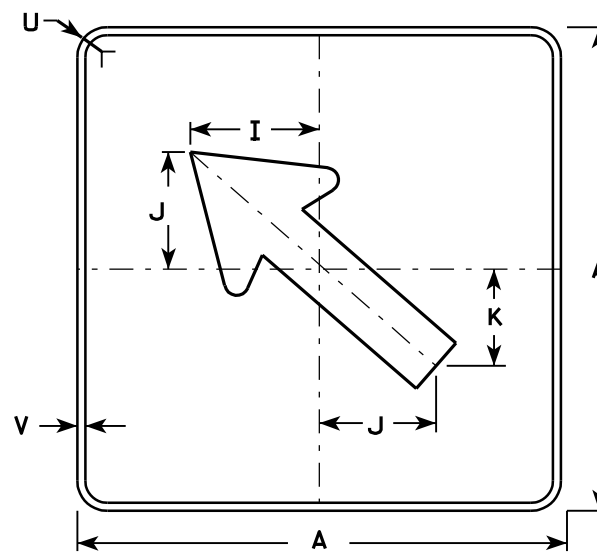
M6-1
MK6-1
MM6-1
MO6-1
MP6-1
MR6-1



MB6-1
MG6-1
MN6-1



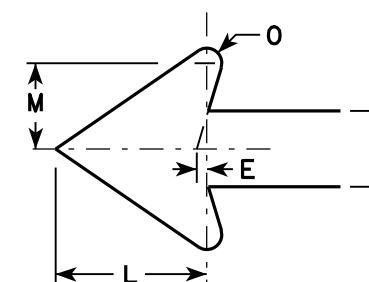
M6-2
MK6-2
MM6-2
MO6-2
MP6-2
MR6-2



MB6-2
MG6-2
MN6-2

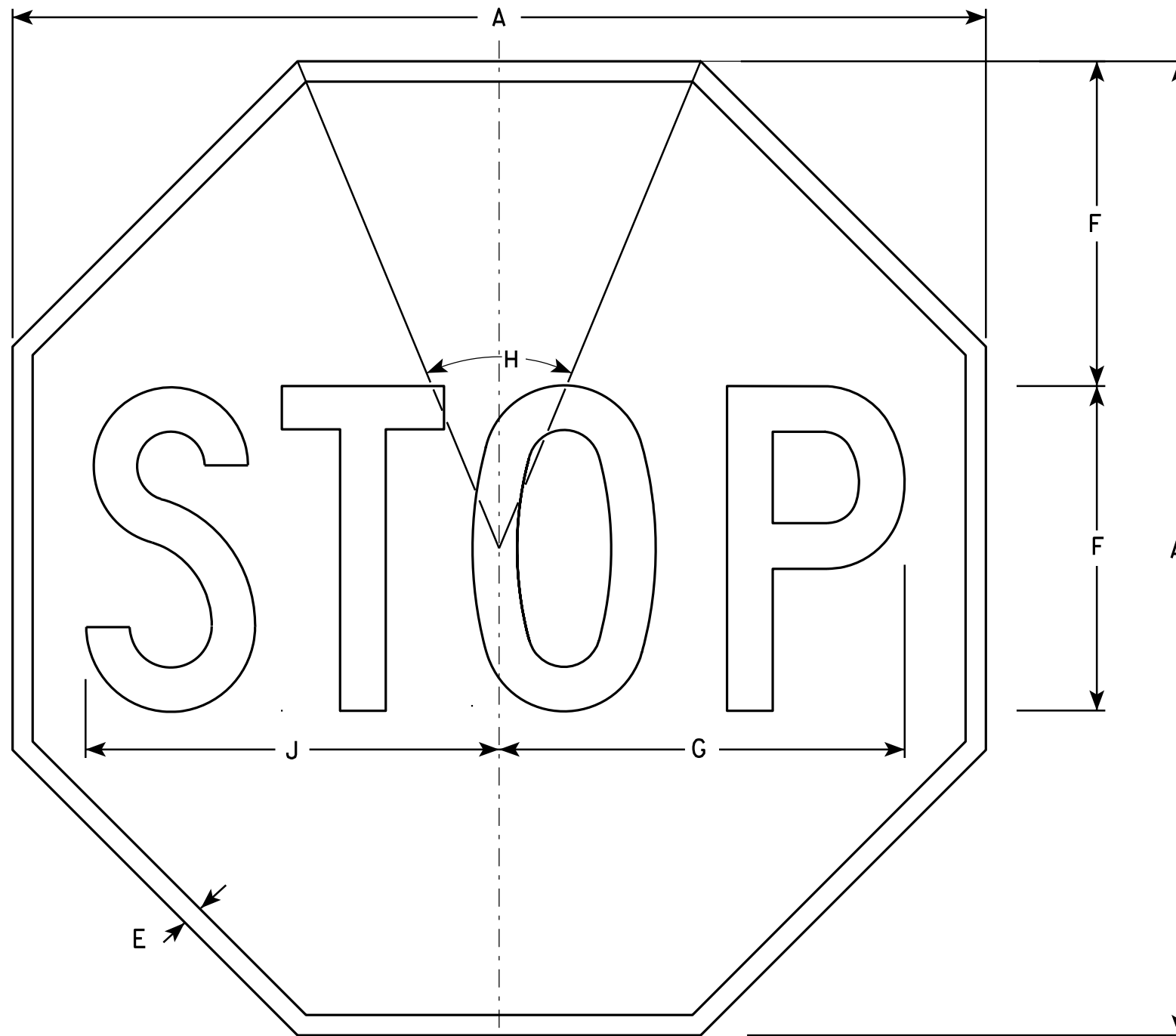
NOTES

- Signs are Type II - See Note 4 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White - Type H Reflective
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White - Type H Reflective
MG6-1 and MG6-2 Background - Green
Message - White - Type H Reflective
MK6-1 and MK6-2 Background - Green
Message - White - Type H Reflective
MM6-1 and MM6-2 Background - White - Type H Reflective
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White - Type H Reflective
MO6-1 and MO6-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White - Type H Reflective
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow - Type H Reflective



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

PROJECT NO: HWY: COUNTY: SHEET NO: E



NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Red
Message - White
- 3. Message Series - C

R1-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24				3/8	8	10	45°		10 1/4																	3.31
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN
R1 - 1

WISCONSIN DEPT OF TRANSPORTATION

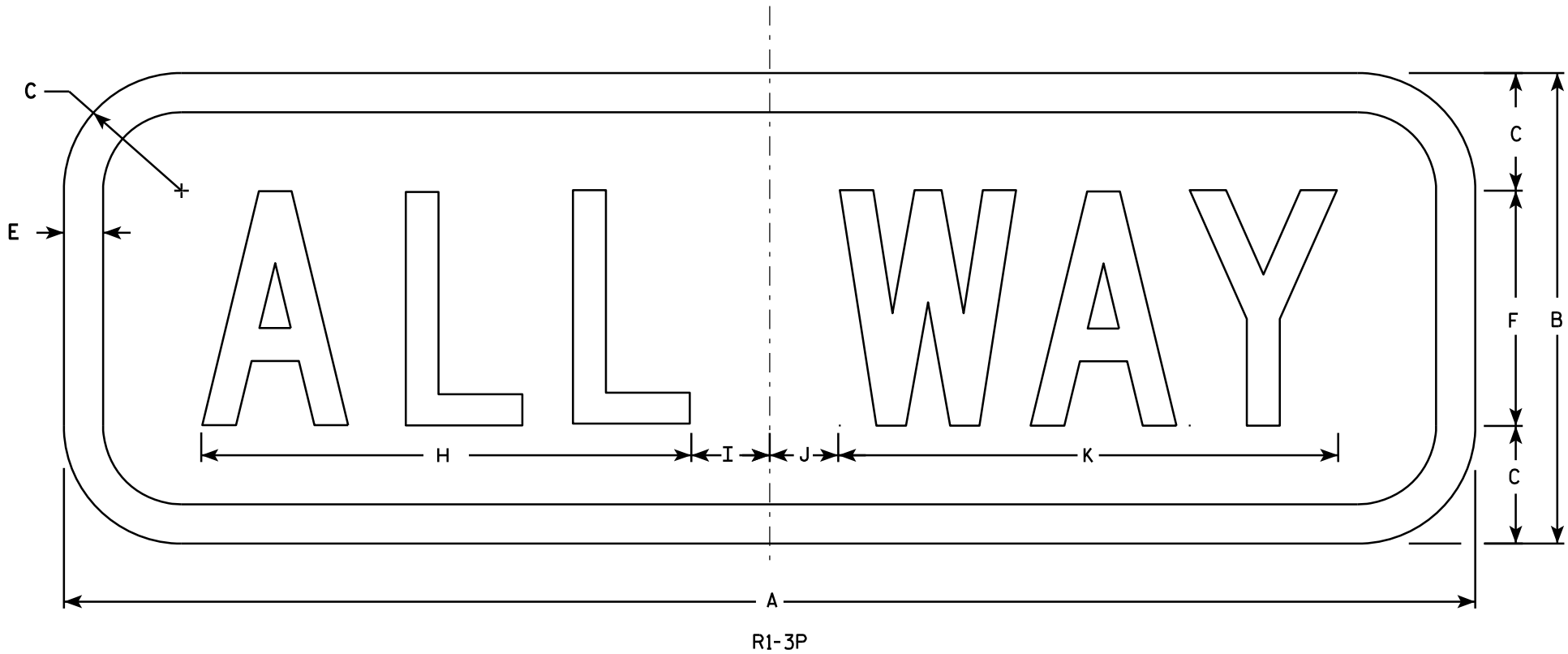
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1.12

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																0.75
2S	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																1.5
2M	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
3	30	12	2 1/4		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5
4																											
5																											

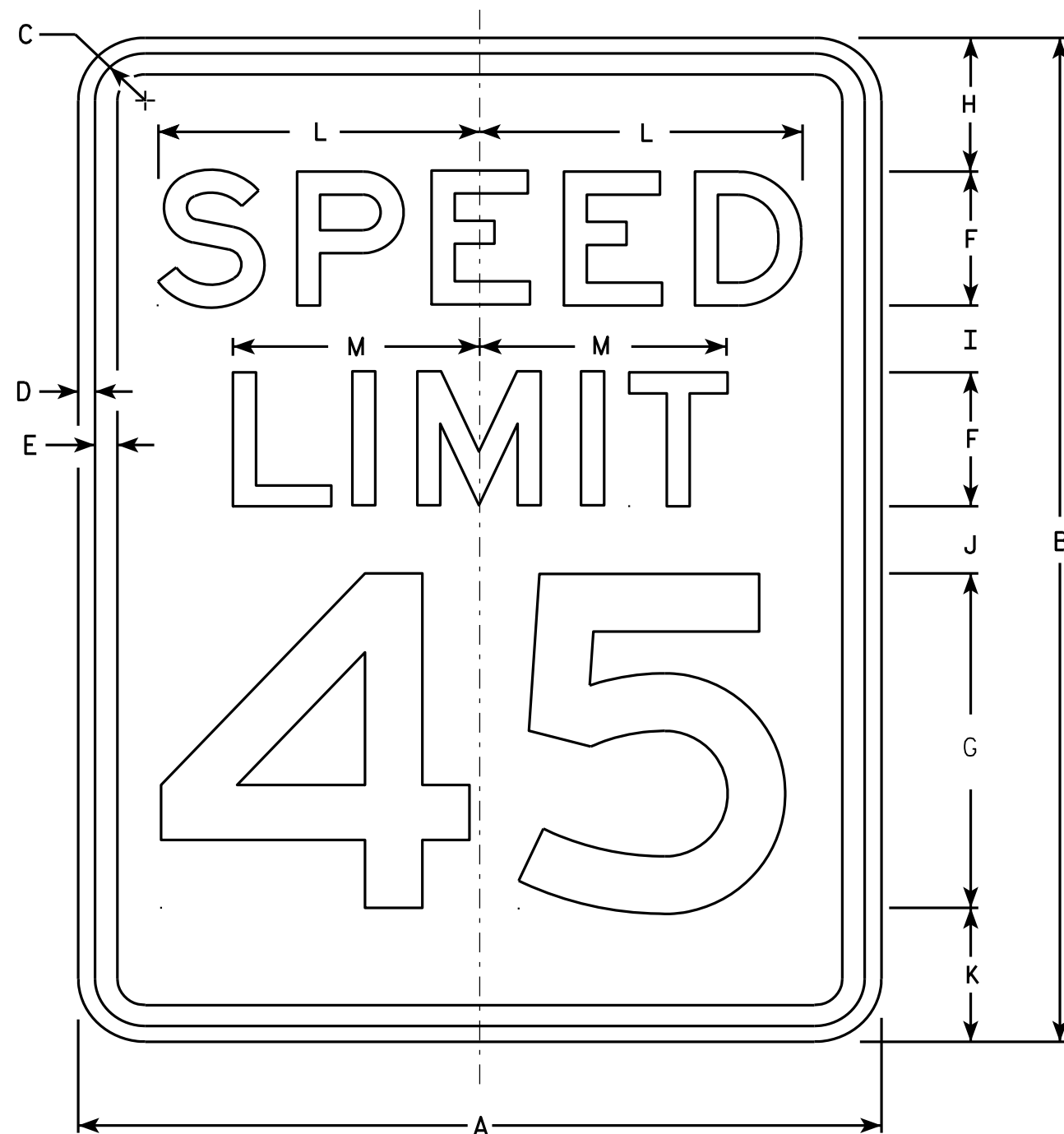
STANDARD SIGN
R1 - 3P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/02/10 PLATE NO. R1-3P.1

PROJECT NO: HWY: COUNTY: SHEET NO: E



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

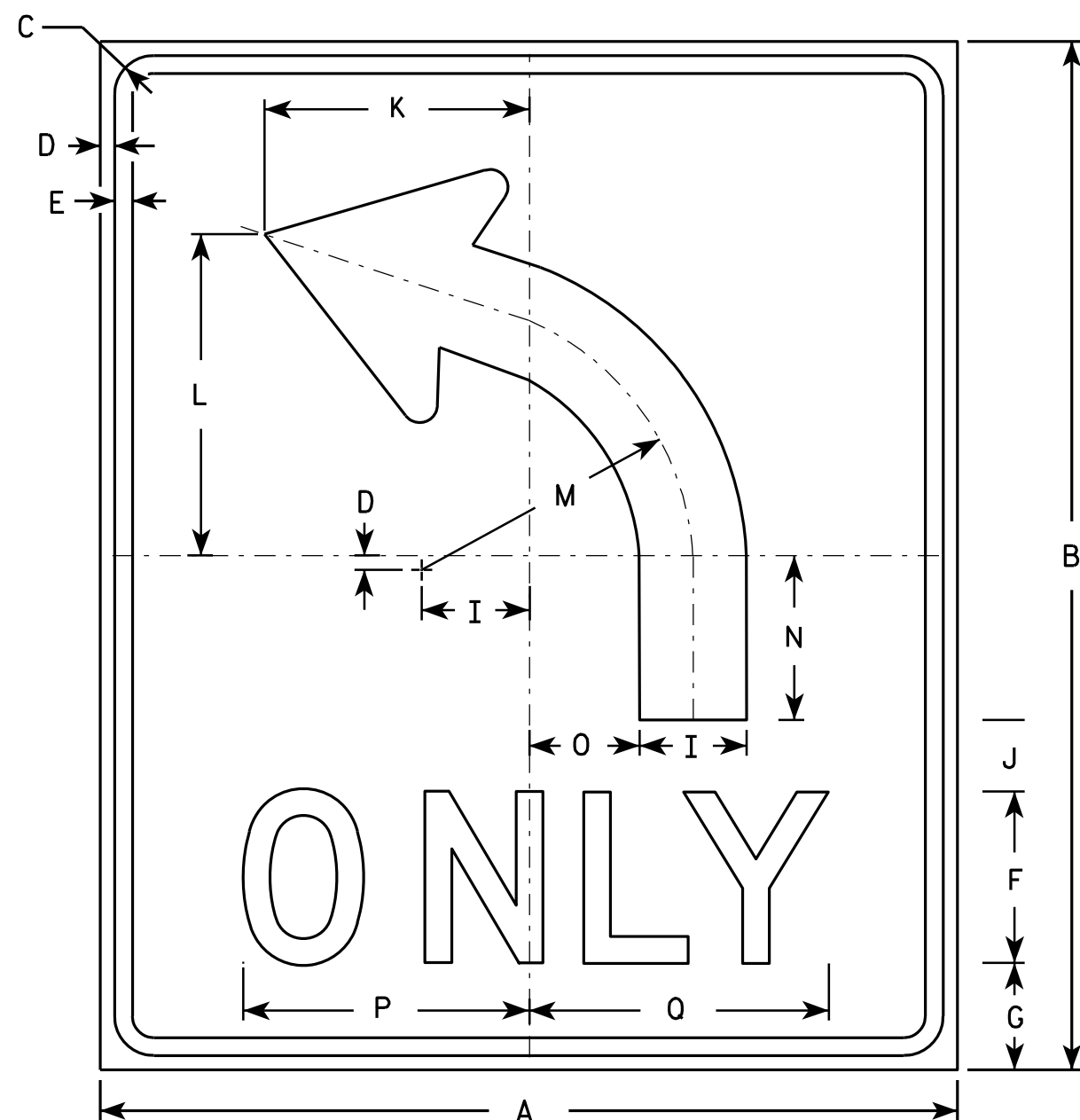
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN R2-1

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
For State Traffic Engineer
DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: HWY: COUNTY: SHEET NO: E

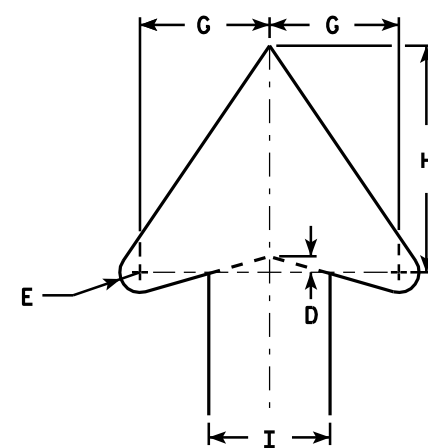
7



R3-50L

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. R3-50R is the same as R3-50L except curved portion of arrow points right.



ARROW DETAIL

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	36	1 3⁄8	1⁄2	5⁄8	6	4	7	3 3⁄4	2 1⁄2	9 1⁄4	11 1⁄4	9 1⁄2	5 3⁄4	3 7⁄8	10	10 1⁄2										7.5
2M	30	36	1 3⁄8	1⁄2	5⁄8	6	4	7	3 3⁄4	2 1⁄2	9 1⁄4	11 1⁄4	9 1⁄2	5 3⁄4	3 7⁄8	10	10 1⁄2										7.5
3																											
4																											
5																											

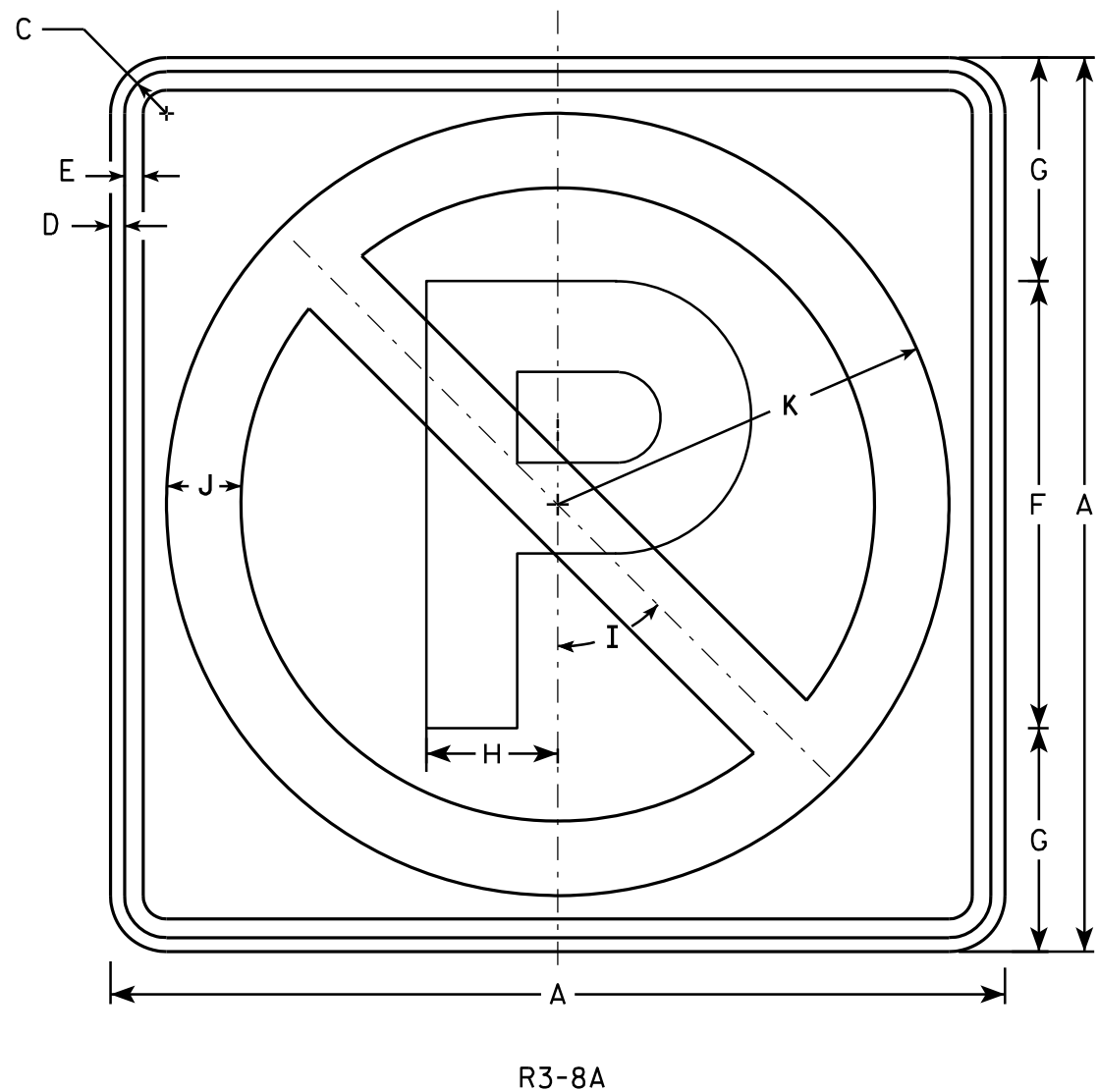
PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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STANDARD SIGN
R3-50

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/24/2011 PLATE NO. R3-50.2



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Border & Letter P are non reflective black, the circle with diagonal bar is reflective red.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24		1 1/8	3/8	1/2	12	6	3 1/2	45°	2	10 1/2																4.0
2M	24		1 1/8	3/8	1/2	12	6	3 1/2	45°	2	10 1/2																4.0
3																											
4																											
5																											

STANDARD SIGN
R3-8A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/01/12 PLATE NO. R3-8A.1

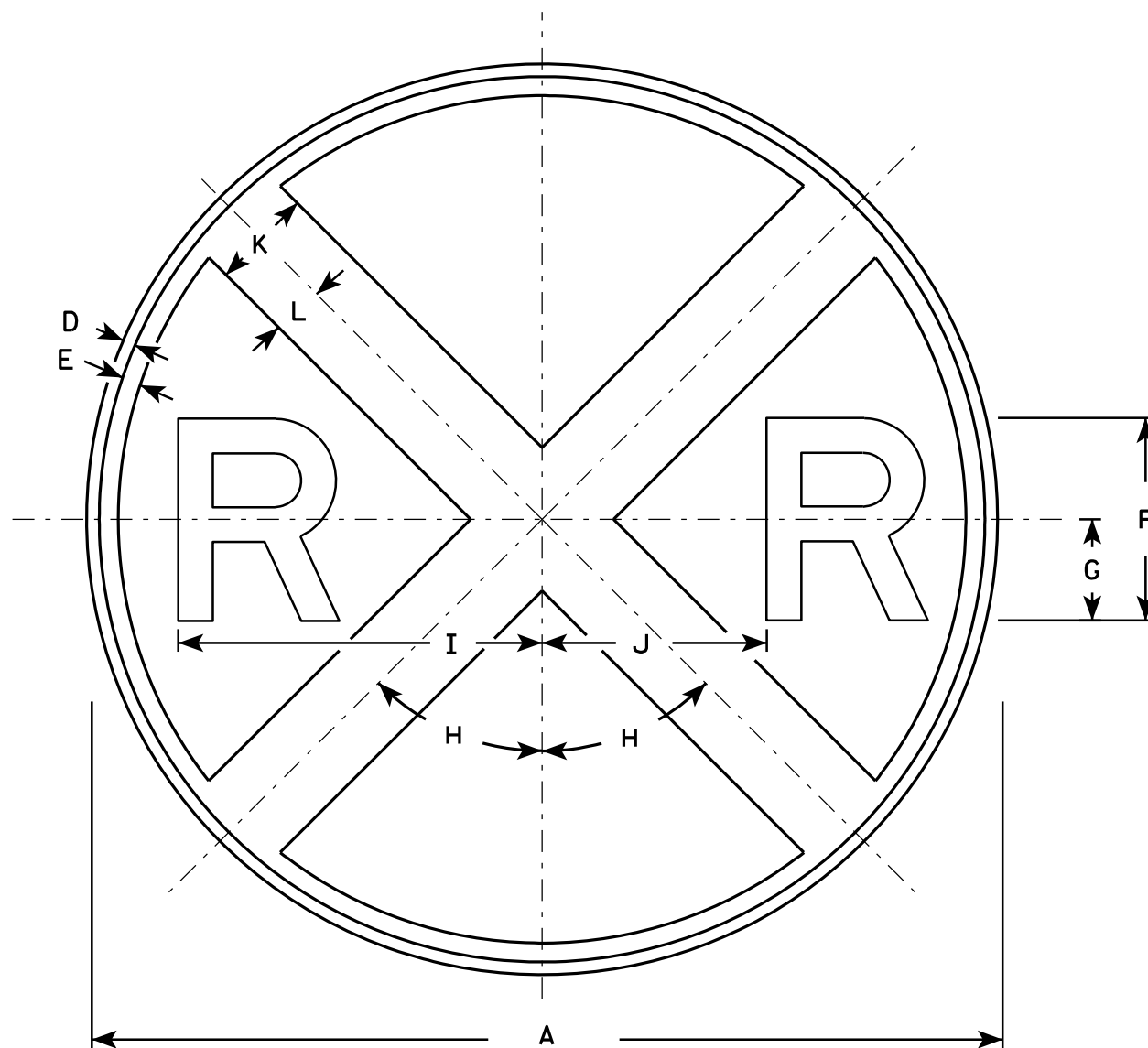
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



W10-1

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - E

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30			3⁄8	5⁄8	7	3 1⁄2	45°	12 3⁄8	7 1⁄8	3	1 1⁄2															4.91
2S	36			5⁄8	3⁄4	8	4	45°	14 3⁄8	8 5⁄8	4	2															7.07
2M	36			5⁄8	3⁄4	8	4	45°	14 3⁄8	8 5⁄8	4	2															7.07
3																											
4	48			3⁄4	1 1⁄4	10	5	45°	18 3⁄8	11 5⁄8	5	2 1⁄2															12.5
5																											

STANDARD SIGN
W10-1

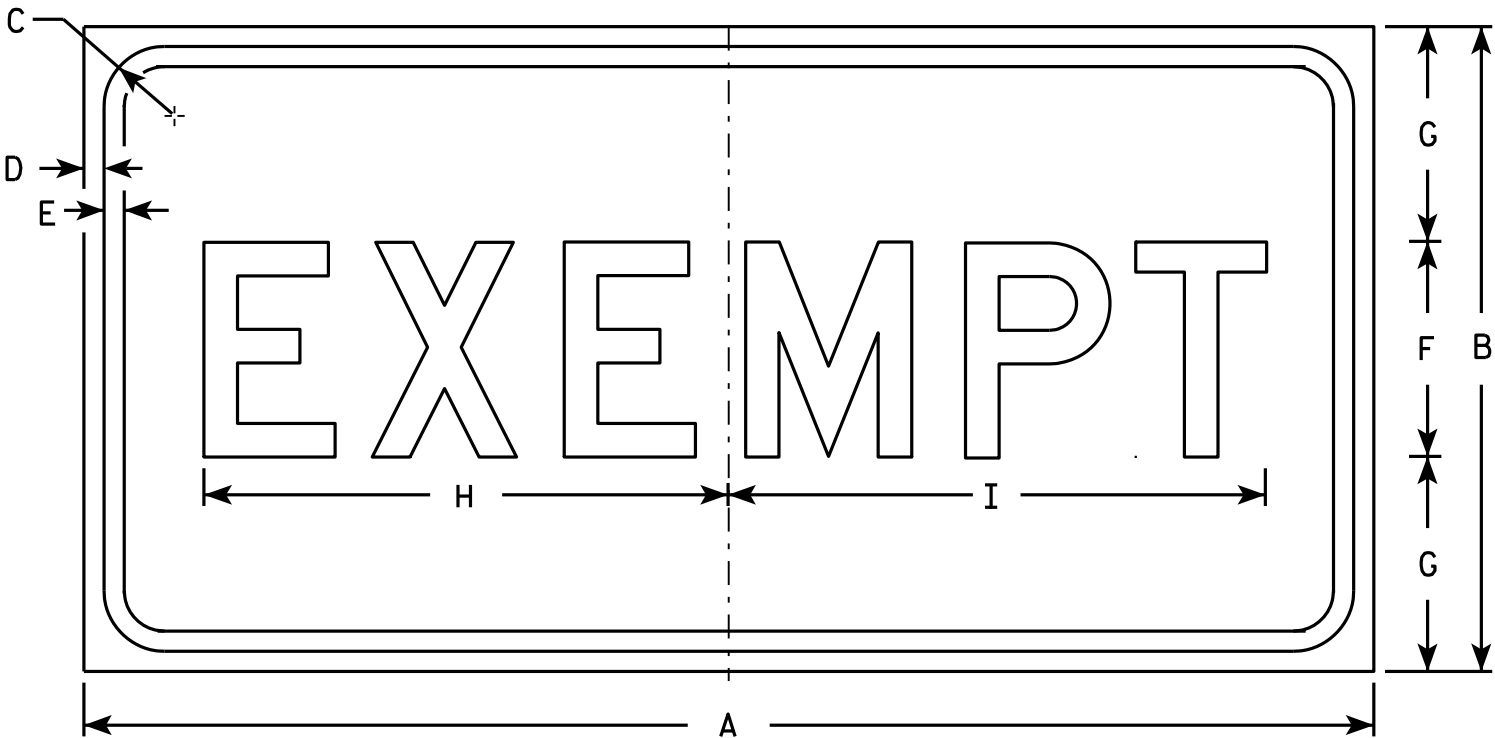
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 3/13/13 PLATE NO. W10-1.8

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - See Note 5
Message - Black
- 3. Message Series - D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Background - R15-3 is White Type H Reflective
W10-1A is Yellow.



R15-3
and
W10-1A

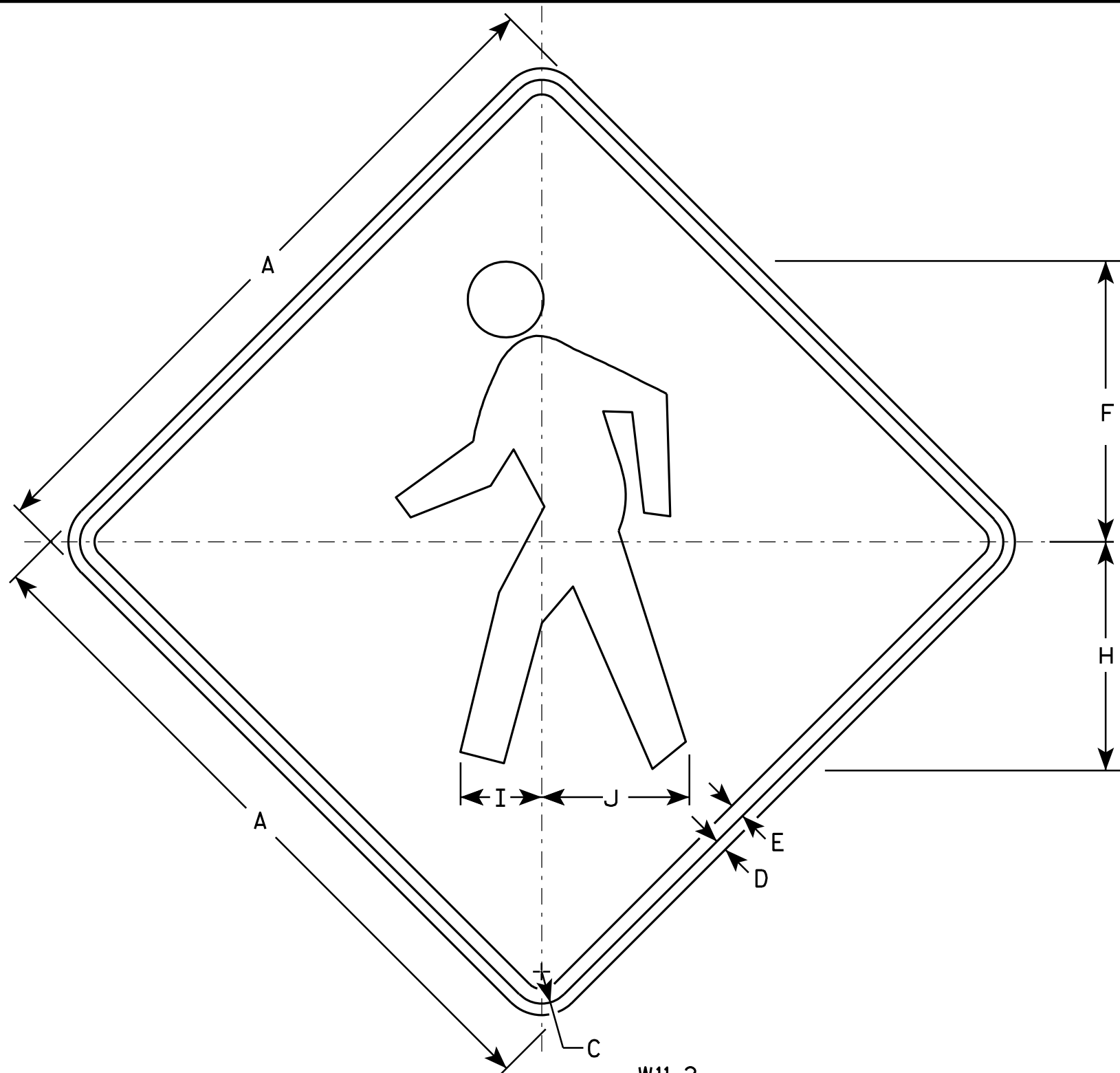
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	4	4	9 3/4	10																		2
2M	24	12	1 1/8	3/8	3/8	4	4	9 3/4	10																		2
3																											
4																											
5																											

STANDARD SIGN
R15-3 & W10-1A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 3/13/13 PLATE NO. R15-3.7



W11-2

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	9 3/4		7 7/8	2 7/8	5 1/8																	4.0
2S	30		1 3/8	1/2	5/8	12 1/8		9 7/8	3 1/2	6 3/8																	6.25
2M	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
3	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
4	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

STANDARD SIGN W11-2

WISCONSIN DEPT OF TRANSPORTATION

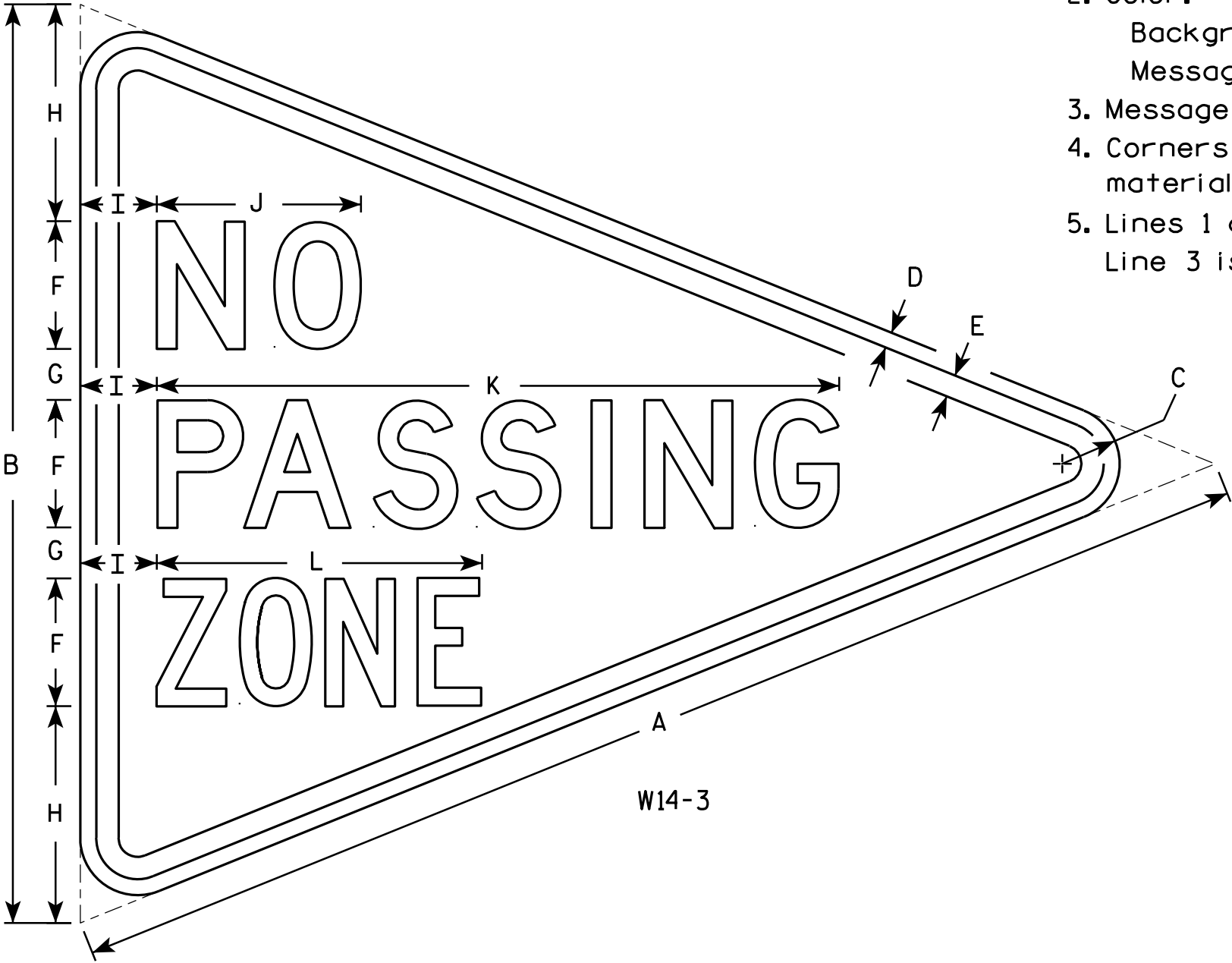
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W11-2.7

PROJECT NO: HWY: COUNTY: SHEET NO: E

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Message Series - See note 5
- 4. Corners and borders shall be rounded on all base materials for this sign.
- 5. Lines 1 and 2 are Series D.
Line 3 is series C.



W14-3

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
2M	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
3	64	48	3	3/4	1 1/4	6	3	12	4	10 3/4	33 5/8	16 1/2															10.7
4																											
5																											

STANDARD SIGN
W14-3

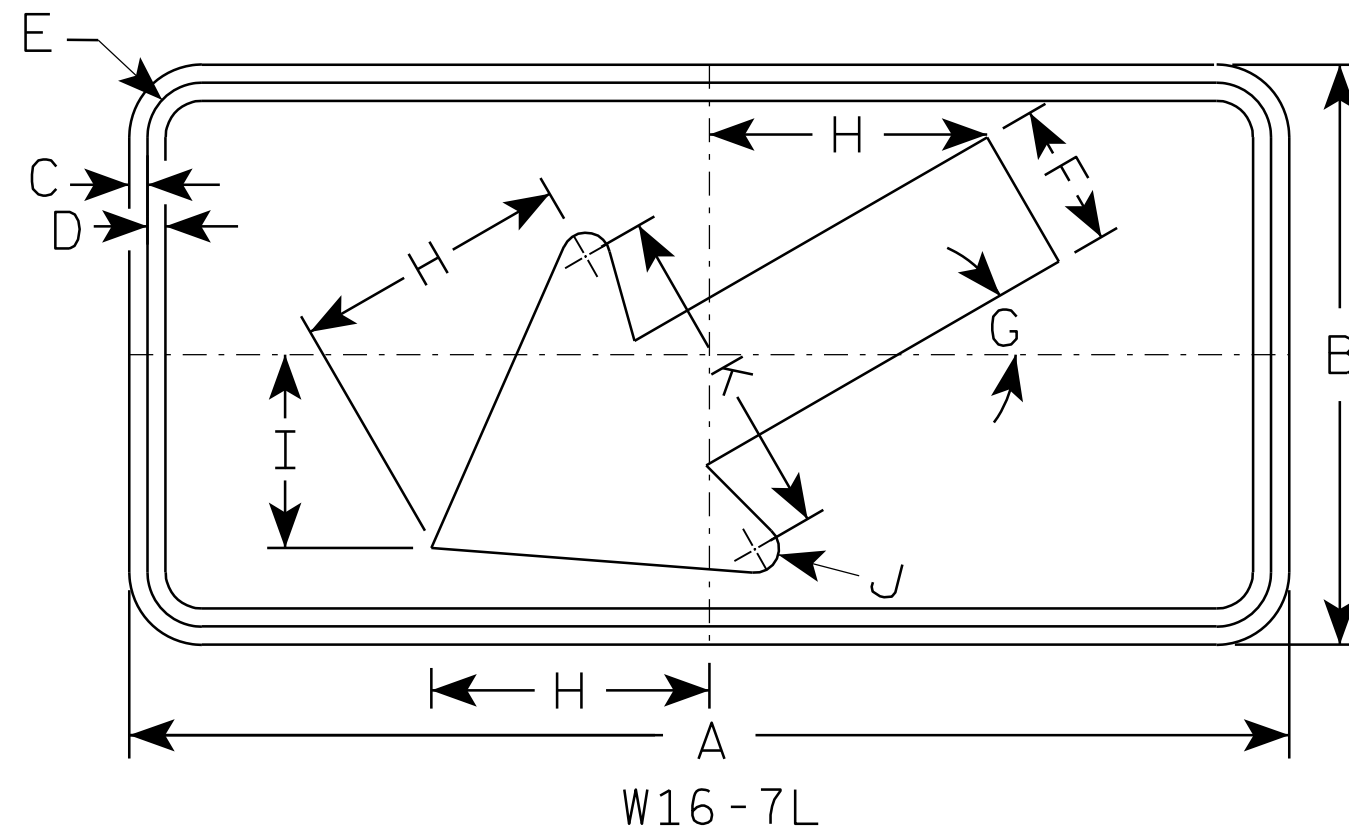
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 6/7/10 PLATE NO. W14-3.9

NOTES

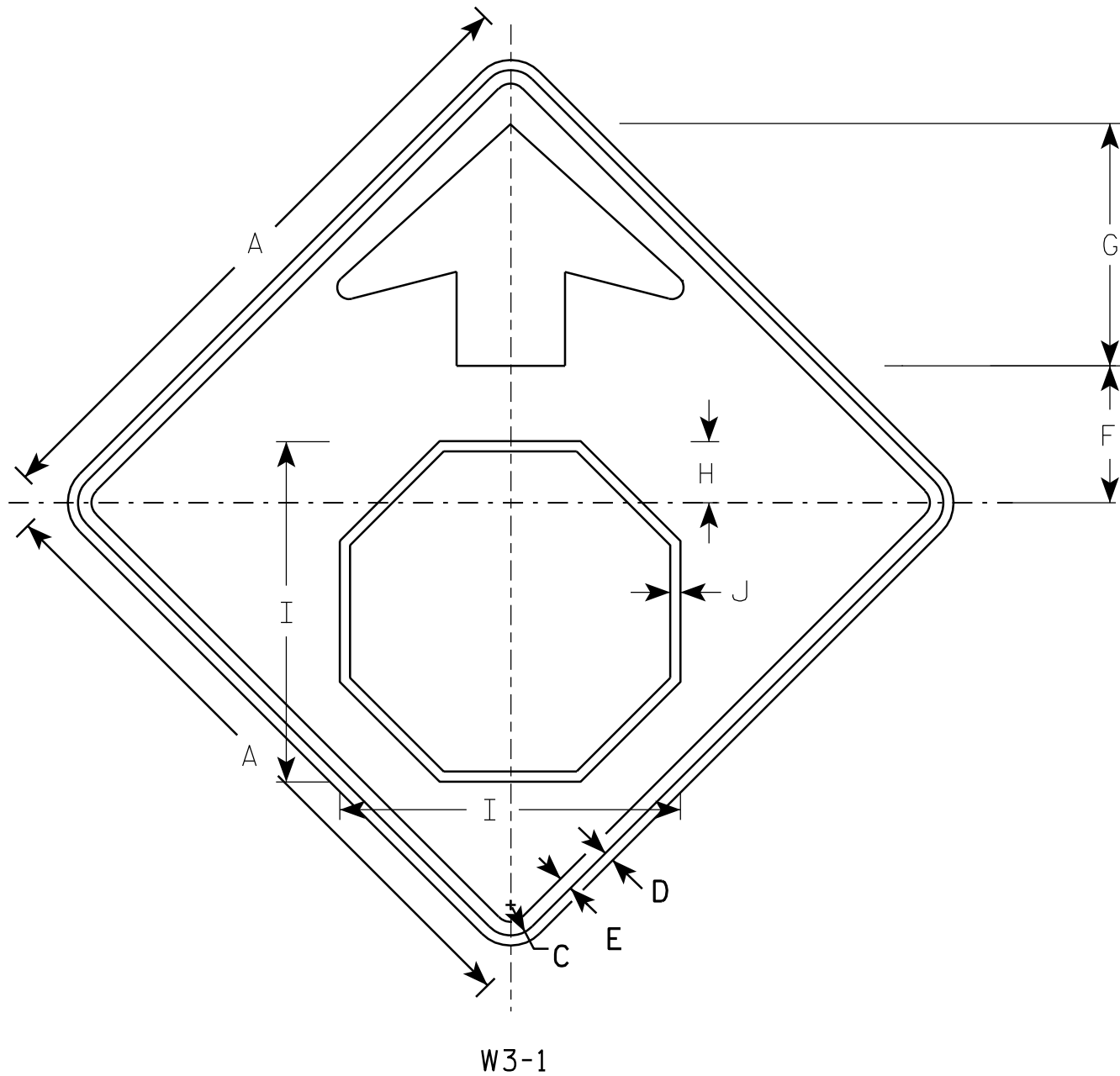
1. Sign is Type II - Type F Reflective -
reference WIS DOT Standard Specification for
HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base
material is plywood but borders shall be rounded
as shown. When base material is metal, the
corners and borders shall be rounded.
4. W16-7R is the same as W16-L
except the arrow is reversed along
the vertical centerline.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	3/8	3/8	1 1/8	3	30°	5 3/4	4	1/2	7																2.0
2M	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
3	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
4																											8
5																											8

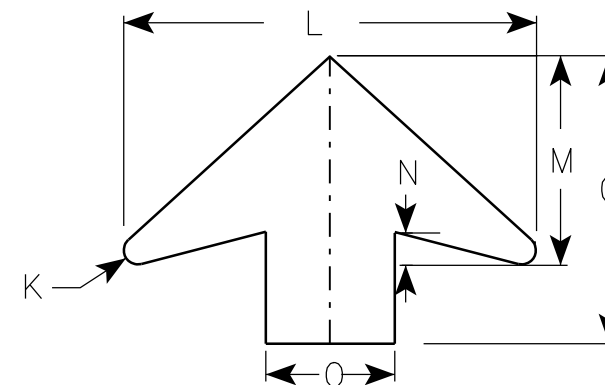
STANDARD SIGN	
W16-7	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 11/02/10	PLATE NO. W16-7.5

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - YELLOW
Arrow & Border - BLACK
Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	2 7/8	15 3/4	1/2	1/2	16	8	1 1/4	5												6.25
2S	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
2M	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
3	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
4	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
5	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0

PROJECT NO:

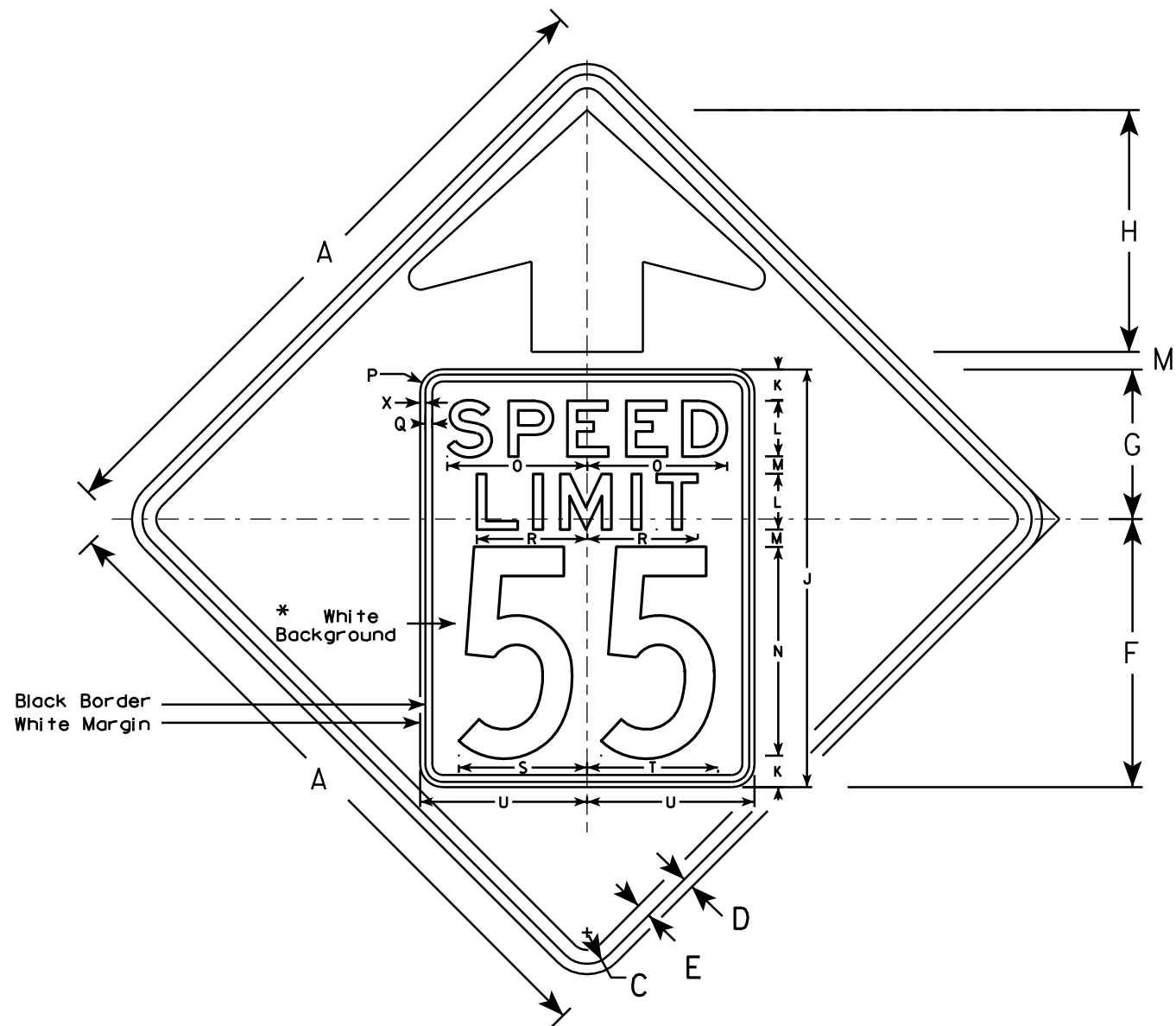
STANDARD SIGN
W3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/7/10 PLATE NO. W3-1.12

SHEET NO:

E

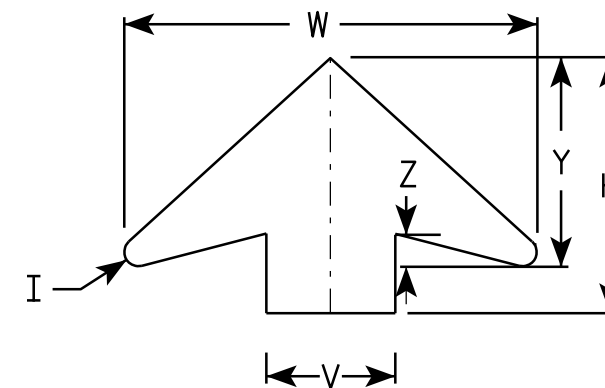


W3-5

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color: *
Background - YELLOW*
Message - BLACK
3. Message Series - C for numbers Series E for wording
4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

*Speed Limit Sign shall have a White Background



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36		1 5⁄8	5⁄8	3⁄4	14 1⁄2	9 1⁄2	11 1⁄2	5⁄8	24	2	3	1	12	7 1⁄8	1 1⁄2	3⁄8	5 3⁄4	7 1⁄4	7 1⁄8	9	6	19 1⁄4	3⁄8	9 3⁄4	1 5⁄8	9.0
2M	36		1 5⁄8	5⁄8	3⁄4	14 1⁄2	9 1⁄2	11 1⁄2	5⁄8	24	2	3	1	12	7 1⁄8	1 1⁄2	3⁄8	5 3⁄4	7 1⁄4	7 1⁄8	9	6	19 1⁄4	3⁄8	9 3⁄4	1 5⁄8	9.0
3	36		1 5⁄8	5⁄8	3⁄4	14 1⁄2	9 1⁄2	11 1⁄2	5⁄8	24	2	3	1	12	7 1⁄8	1 1⁄2	3⁄8	5 3⁄4	7 1⁄4	7 1⁄8	9	6	19 1⁄4	3⁄8	9 3⁄4	1 5⁄8	9.0
4	48		2 1⁄4	3⁄4	1	19 1⁄4	10 3⁄4	17 3⁄8	7⁄8	30	2 1⁄4	4	1 1⁄4	15	10	1 5⁄8	1⁄2	8	9 1⁄4	9 3⁄8	12	8	25 5⁄8	3⁄8	13	2	16.0
5	48		2 1⁄4	3⁄4	1	19 1⁄4	10 3⁄4	17 3⁄8	7⁄8	30	2 1⁄4	4	1 1⁄4	15	10	1 5⁄8	1⁄2	8	9 1⁄4	9 3⁄8	12	8	25 5⁄8	3⁄8	13	2	16.0

STANDARD SIGN

W3-5

WISCONSIN DEPT OF TRANSPORTATION

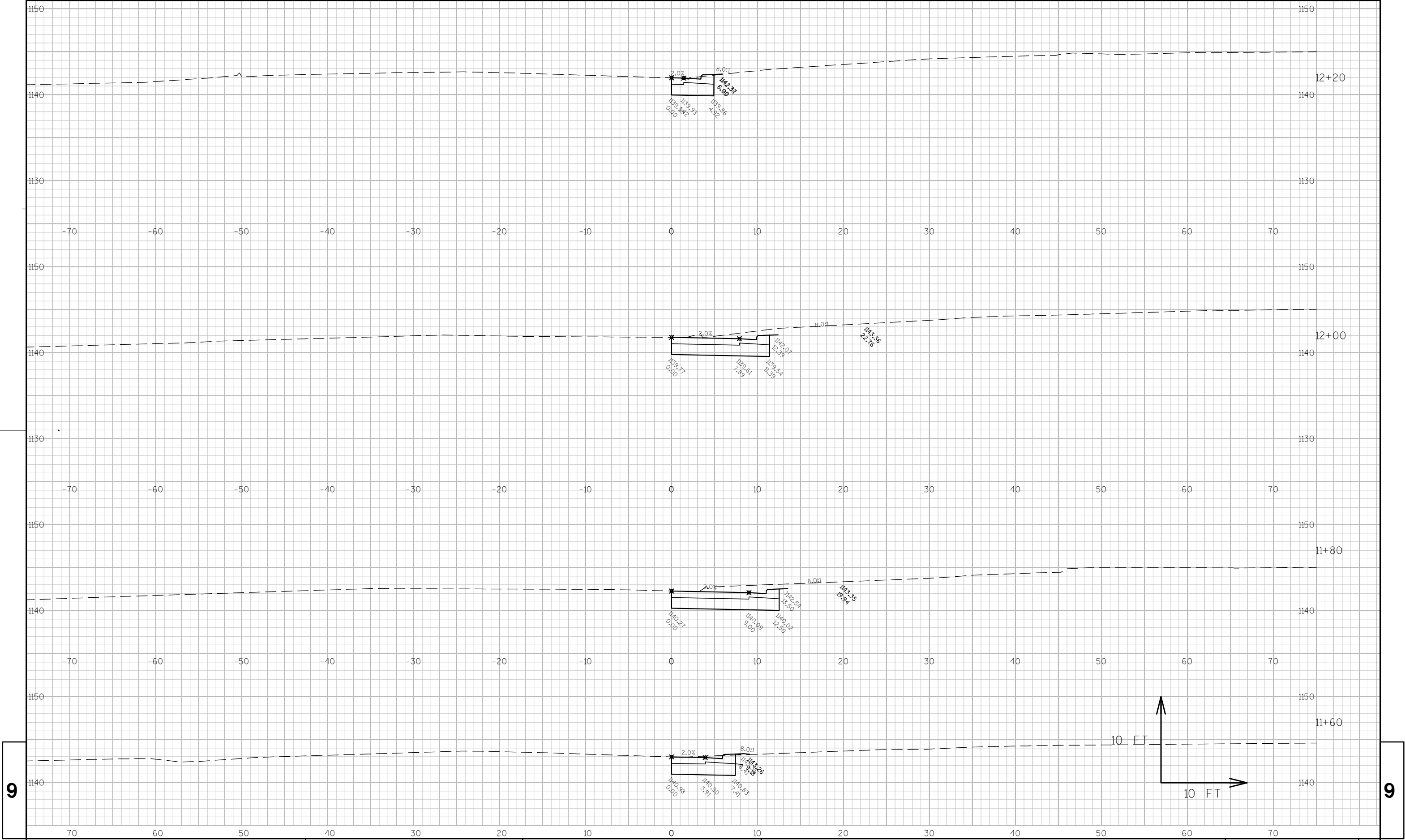
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W3-5.5

PROJECT NO:

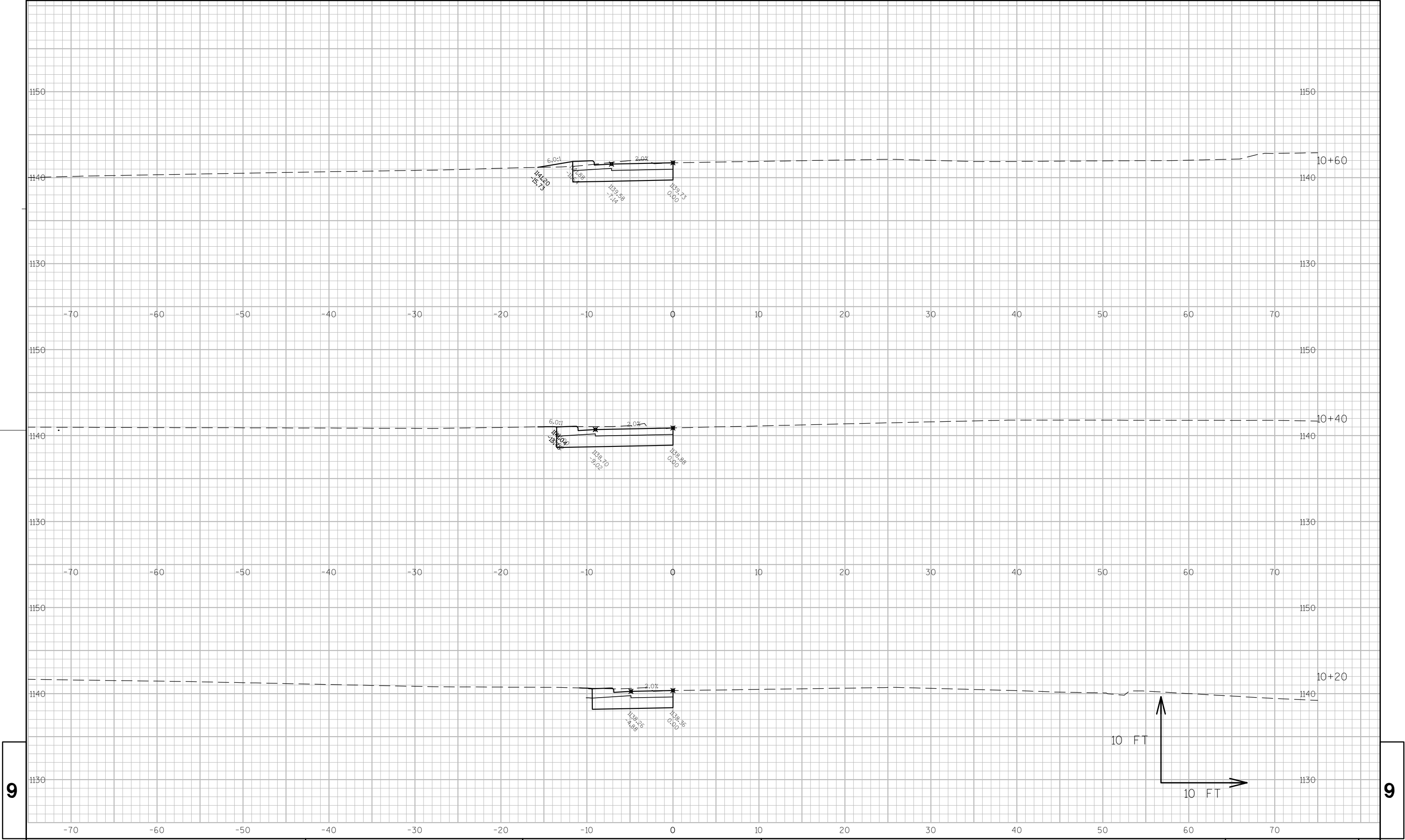
SHEET NO:

E



9

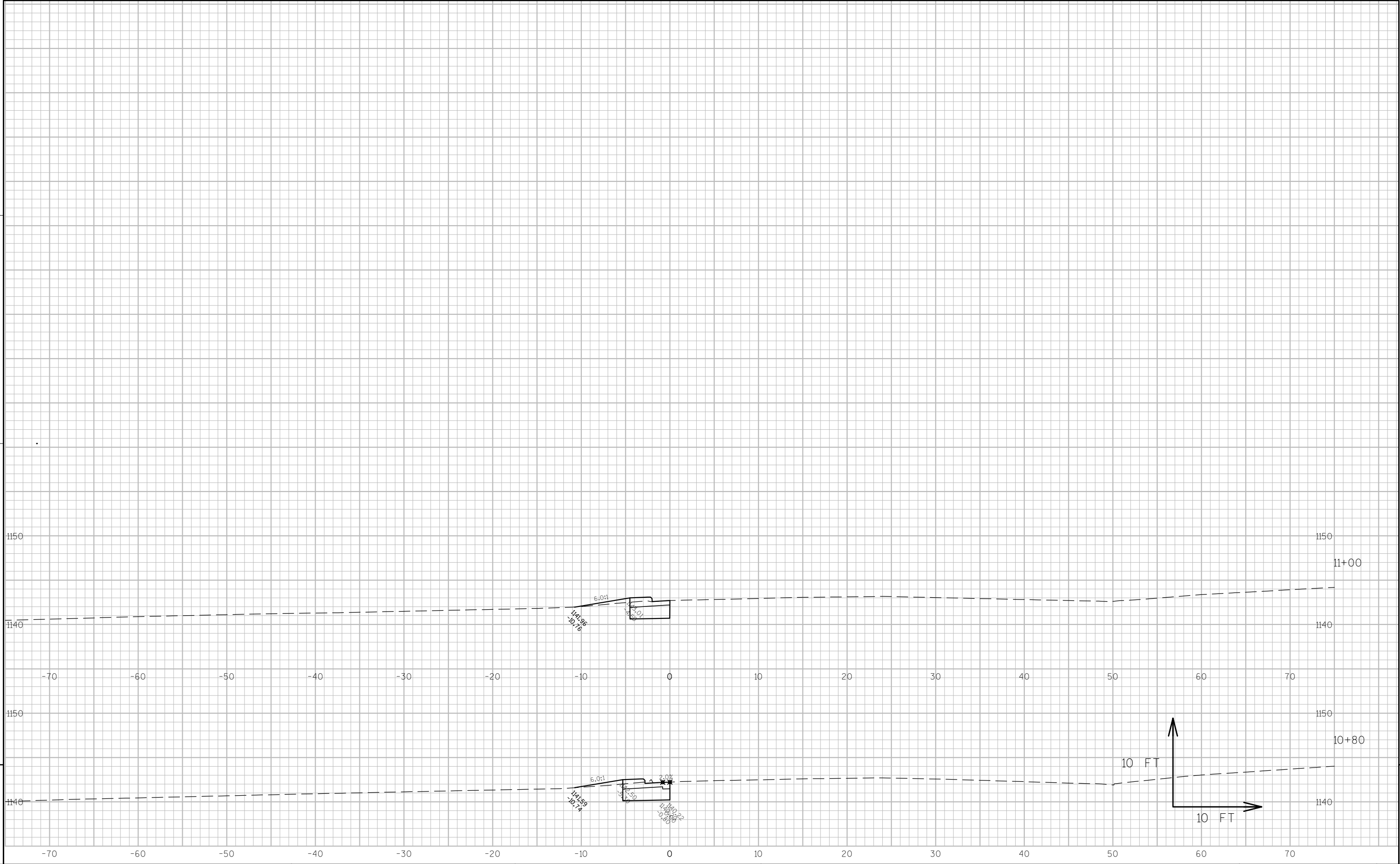
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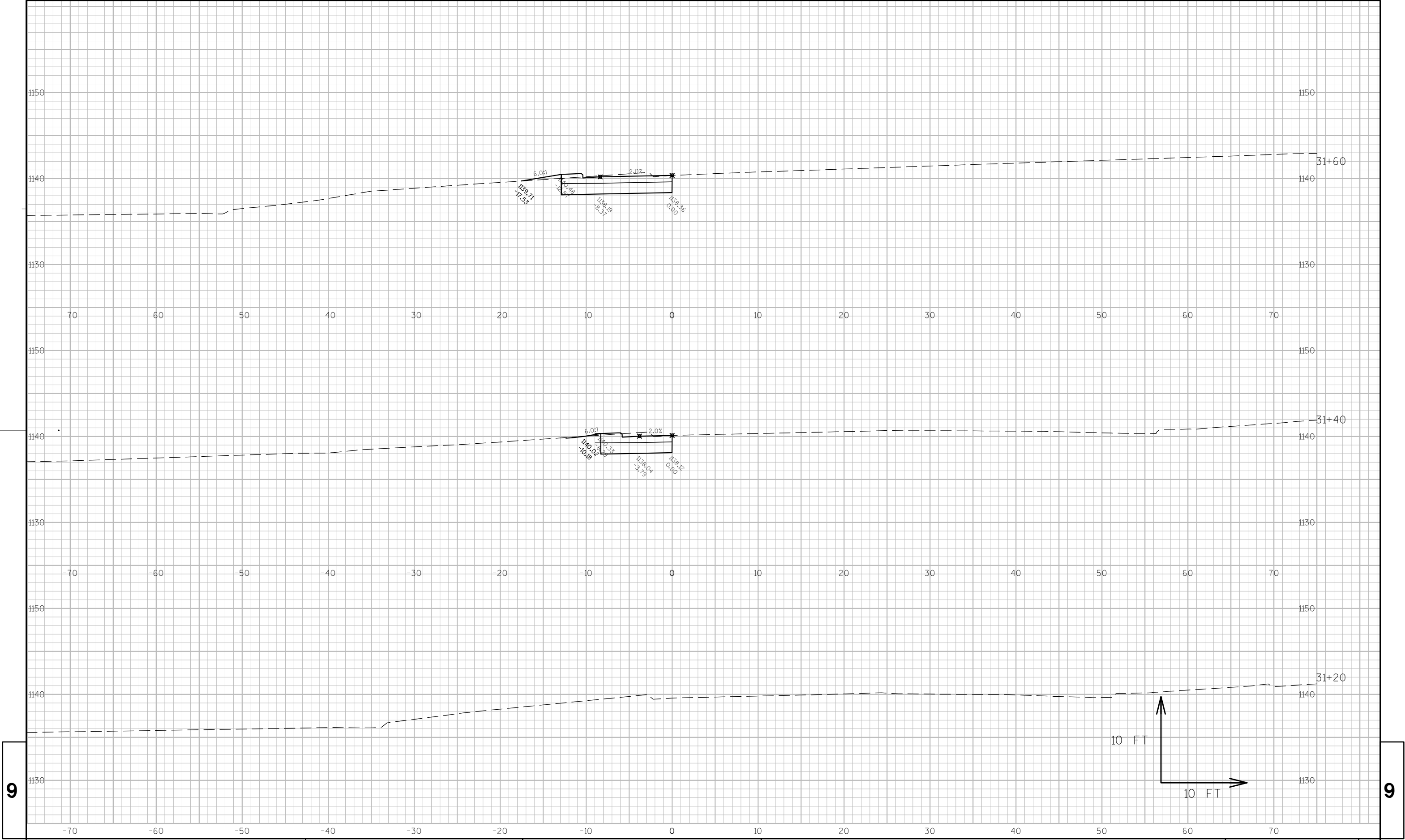
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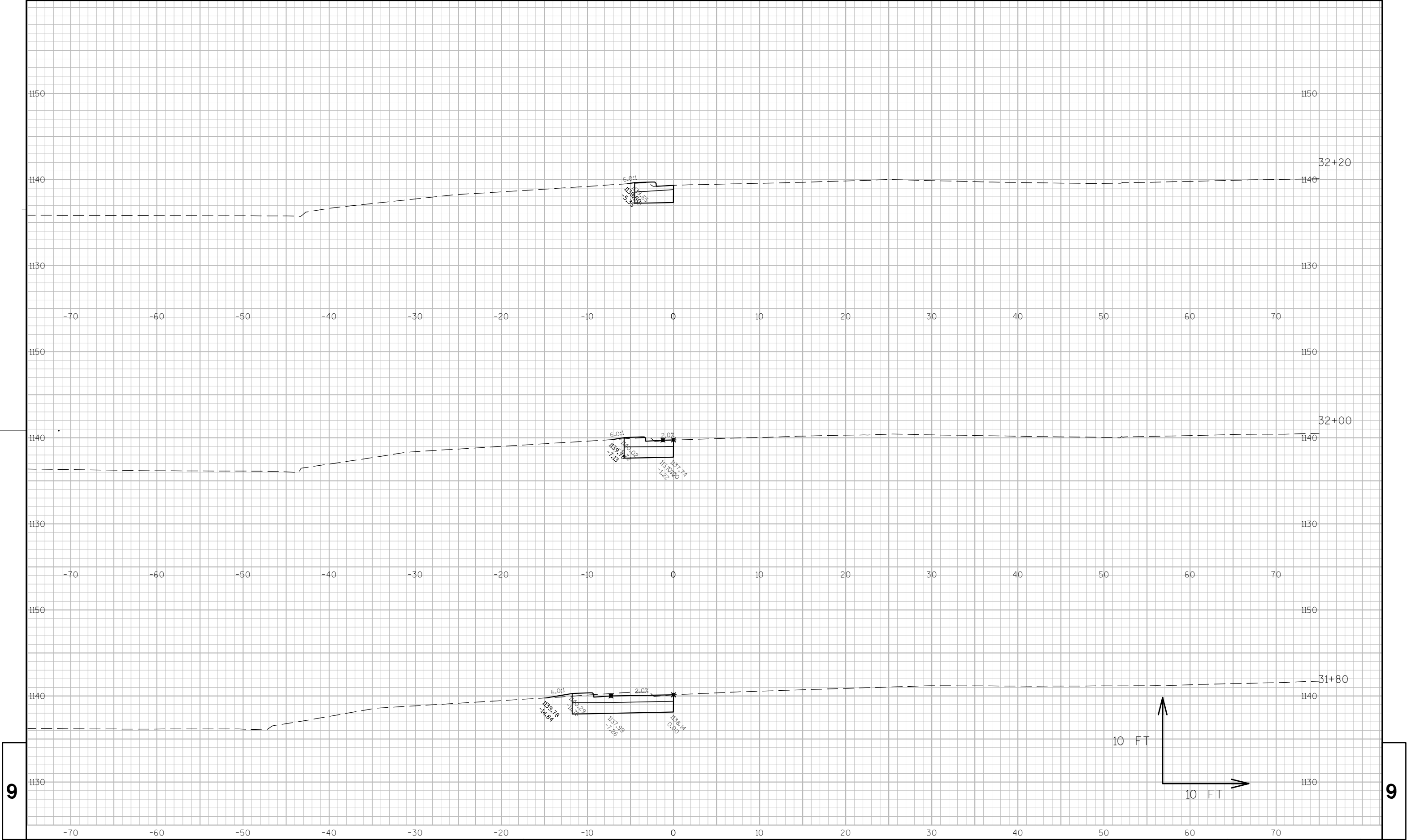
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PROJECT NO:1580-12-72	HWY: STH 27	COUNTY: RUSK	CROSS SECTIONS: NW	SHEET -----	E
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Notes



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