

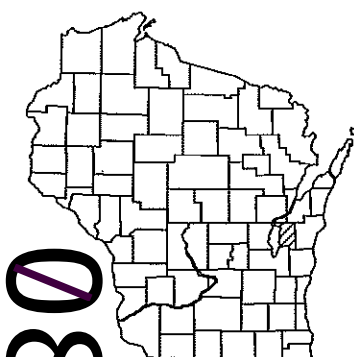
PROJECT ID: 4670-11-60

COUNTY: CALUMET

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right-of-Way Plat
Section No. 5	Plan and Profiles
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Grade Sections

TOTAL SHEETS = 106



DESIGN DESIGNATION

A.A.D.T. 2014	=	10,000
A.A.D.T. 2034	=	14,400
D.H.V.	=	1,019
D.D.	=	60/40
T.	=	9.6%
DESIGN SPEED	=	35 MPH/60 MPH
ESALS	=	3,452,900

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

**MENASHA - SHERWOOD**

USH 10 - SOUTH JUNCTION STH 55

**STH 114**

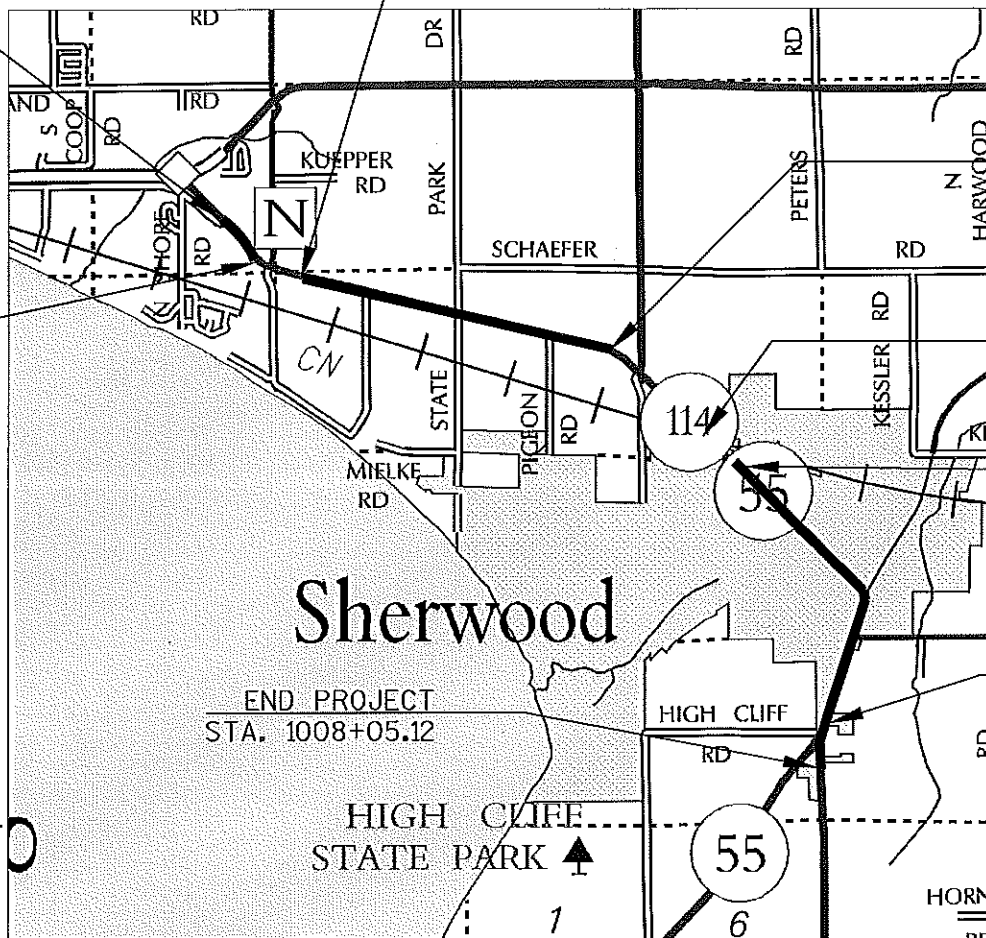
**CALUMET COUNTY**

STATE PROJECT NUMBER  
4670-11-60

BEGIN PROJECT  
STA. 203+75

Y= 541158.265 (+/-)  
X= 847817.638 (+/-)

START NET EXCEPTION TO  
CENTERLINE LENGTH  
EQUATION:  
STA 208+60.48 BK  
STA 10+00 AH



END PROJECT  
STA. 1008+05.12

END NET EXCEPTION TO  
CENTERLINE LENGTH EQUATION:  
STA 34+36.21 BK  
STA 232+68.87 AH

START NET EXCEPTION TO  
CENTERLINE LENGTH  
EQUATION  
STA 321+96.54 BK  
STA 9+55 AH

EQUATION  
STA 36+04.42 BK  
STA 348+36.17 AH

END NET EXCEPTION TO  
CENTERLINE LENGTH  
STA 367+75.00

EQUATION  
STA 464+07.20 BK  
STA 1000+00.00 AH

LAYOUT  
SCALE 0 1 MI.

TOTAL NET LENGTH OF CENTERLINE = 3.760 MI.

COORDINATES ARE SCALED FROM USGS TOPOGRAPHIC MAP  
SHERWOOD, WISCONSIN QUADRANGLE, FOR IDENTIFICATION ONLY.  
COORDINATES ON THIS PLAN ARE REFERENCED TO THE  
WISCONSIN COUNTY COORDINATE SYSTEM, CALUMET COUNTY.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
4670-11-60		

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	WISDOT
Designer	KIMBERLY KIMMES
Project Manager	PAUL VRANEY
Regional Examiner	REGIONAL EXAMINER
Regional Supervisor	CHAD DEGRAVE

APPROVED FOR THE DEPARTMENT  
DATE: 01/31/14 *Paul Vraney*  
(Signature)

GENERAL NOTES

THE LOCATIONS OF EXISTING MANHOLES AS SHOWN ON THE PLANS ARE APPROXIMATE.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

EXISTING PERMANENT SIGNS ARE TO REMAIN IN PLACE UNLESS SPECIFICALLY CALLED FOR REMOVAL ON MISCELLANEOUS QUANTITY TABLE.

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND ALL UTILITIES IN THE VICINITY OF THE PROJECT TO LOCATE THEIR FACILITIES AT LEAST THREE WORKING DAYS PRIOR TO BEGINNING WORK.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

ALL DISTURBED AREAS, NOT OTHERWISE SURFACED, ARE TO BE TOPSOILED, FERTILIZED, SEEDED AND COVERED WITH MULCH OR EROSION MAT, AS SHOWN ON THE PLANS.

THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY UTILITY WHICH IS NOT A MEMBER OF DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

THE EXACT LOCATIONS OF ALL EROSION CONTROL ITEMS SHALL BE DETERMINED BY THE ENGINEER.

IN ORDER TO MINIMIZE CONSTRUCTION/PERFORMANCE ISSUES WITH THE CENTERLINE RUMBLE STRIP, THE NOTCHED WEDGE LONGITUDINAL JOINT SHALL BE MILLED OUT IMMEDIATELY PRIOR TO PAVING.

UTILITIES

AMERICAN TRANSMISSION COMPANY, LLC - ELECTRICITY  
801 O'KEEFE RD.  
P.O. BOX 6113  
DE PERE, WI 54115-6113  
ATTN: KIM HACKELBERG  
PHONE: 920-338-6556  
E-MAIL: KHACKELBERG@ATCLLC.COM

AT&T WISCONSIN - COMMUNICATION LINE  
4TH FLOOR  
221 WEST WASHINGTON ST.  
APPLETON, WI 54911  
ATTN: GARY LAABS  
PHONE: 920-735-3063  
E-MAIL: GL1784@ATT.COM

TDS TELECOM - COMMUNICATION LINE  
SUITE 218A  
10 COLLEGE AVE  
APPLETON, WI 54911  
ATTN: STEVE JAKUBIEC  
PHONE: 920-882-4166  
E-MAIL: STEVE.JAKUBIEC@TDSTELECOM.COM

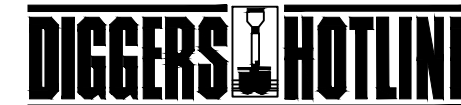
TIME WARNER ENTERTAINMENT CO, LP- COMMUNICATION LINE  
3520 DESTINATION DR.  
APPLETON, WI 54915  
ATTN: VINCE ALBIN  
PHONE: 920-831-9249  
E-MAIL: VINCE.ALBIN@TWCABLE.COM

VILLAGE OF SHERWOOD - SEWER AND WATER  
W482 CLIFTON RD  
SHERWOOD, WI 54169  
ATTN: BRUCE GENSKOW  
PHONE: 920-989-4096  
E-MAIL: SHERWOODUTILITY@TDS.NET

WISCONSIN ELECTRIC POWER COMPANY - GAS/PETROLEUM & ELECTRICITY  
800 S LYNNDAL DR.  
APPLETON, WI 54912  
ATTN: KEN VANOSS  
PHONE: 920-380-3318  
E-MAIL: KENNETH.VAN-OSS@WE-ENERGIES.COM

DNR AREA LIAISON

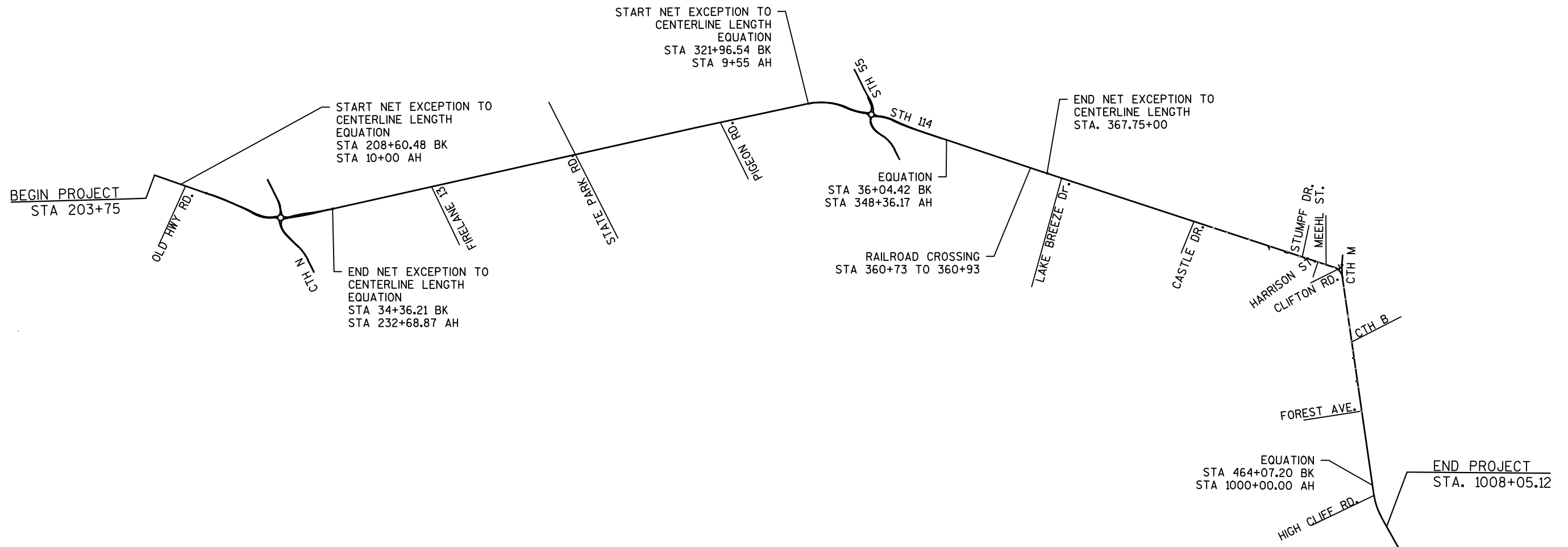
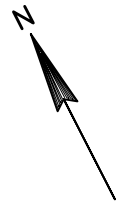
WISCONSIN DEPARTMENT OF NATURAL RESOURCES  
2984 SHAWANO AVENUE  
GREEN BAY, WI 54313-6727  
ATTN: MATTHEW D. SCHAEVE  
PHONE: 920-662-5472  
FAX: 920-662-5159  
E-MAIL: MATTHEW.SCHAEVE@WISCONSIN.GOV



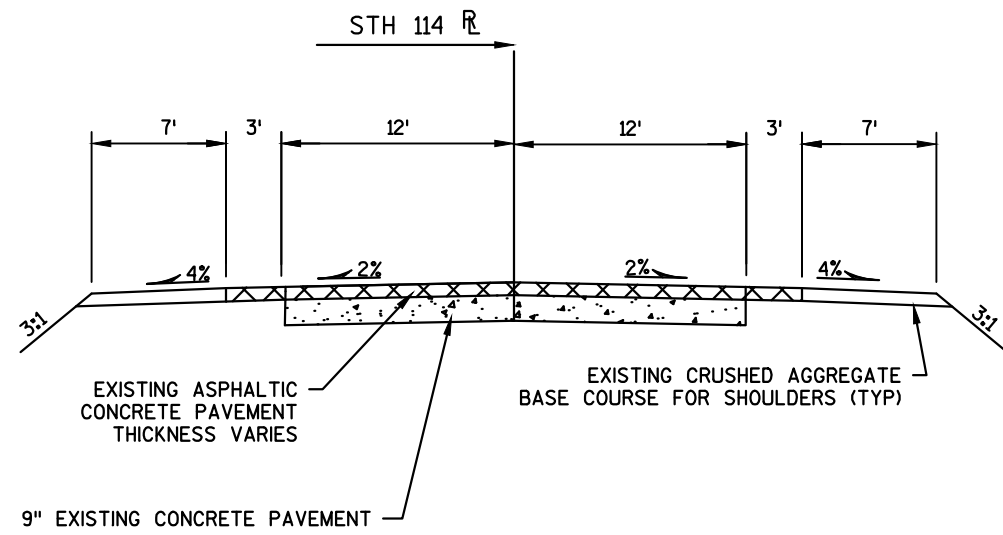
**Call 811 3 Work Days Before You Dig  
or Toll Free (800) 242-8511  
Hearing Impaired TDD (800) 542-2289  
www.DiggersHotline.com**

EMERGENCY CONTACT NUMBERS FOR WE ENERGIES

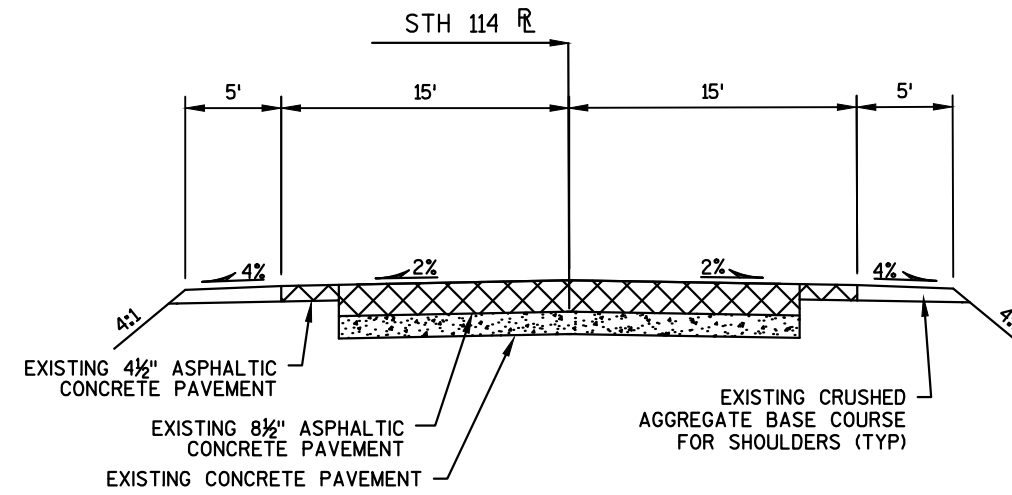
ELECTRIC 24 HOUR EMERGENCY SERVICE: 1-800-662-4797  
GAS 24 HOUR EMERGENCY SERVICE: 1-800-261-5325



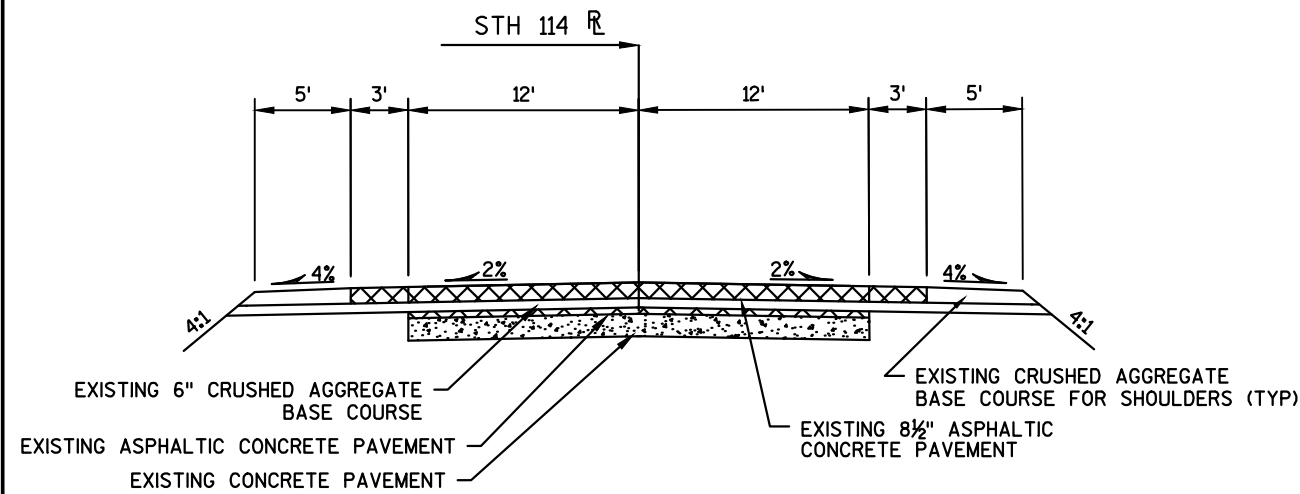
NOTE: IN AREAS WITHIN THE VILLAGE OF SHERWOOD'S NO PARKING LANE, CURB AND GUTTER WILL BE REPLACED AT REQUIRED LOCATIONS. SEE PLANS FOR LOCATIONS AND DETAILS.



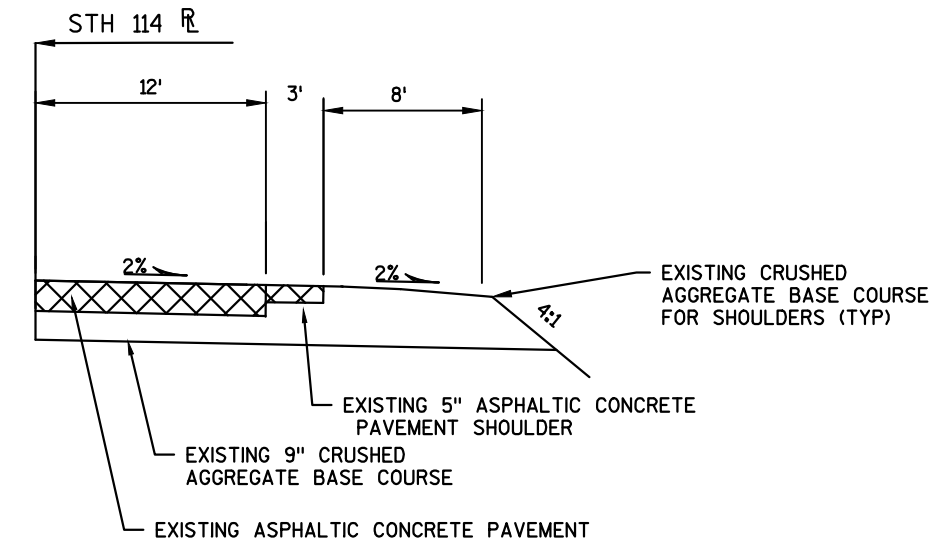
TYPICAL EXISTING CROSS SECTION FOR STH 114  
STA. 203+75 TO STA. 208+60.48



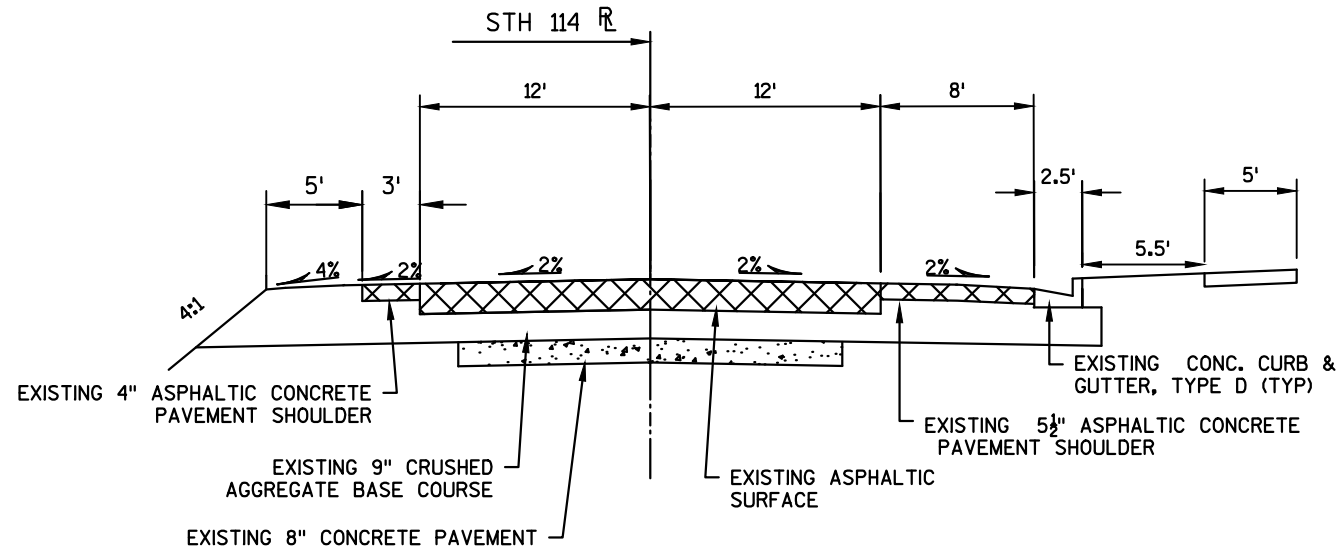
TYPICAL EXISTING CROSS SECTION FOR STH 114  
STA. 232+68.87 TO STA. 290+00  
STA. 310+00 TO STA. 321+96.54  
STA. 367+75 TO STA. 392+00



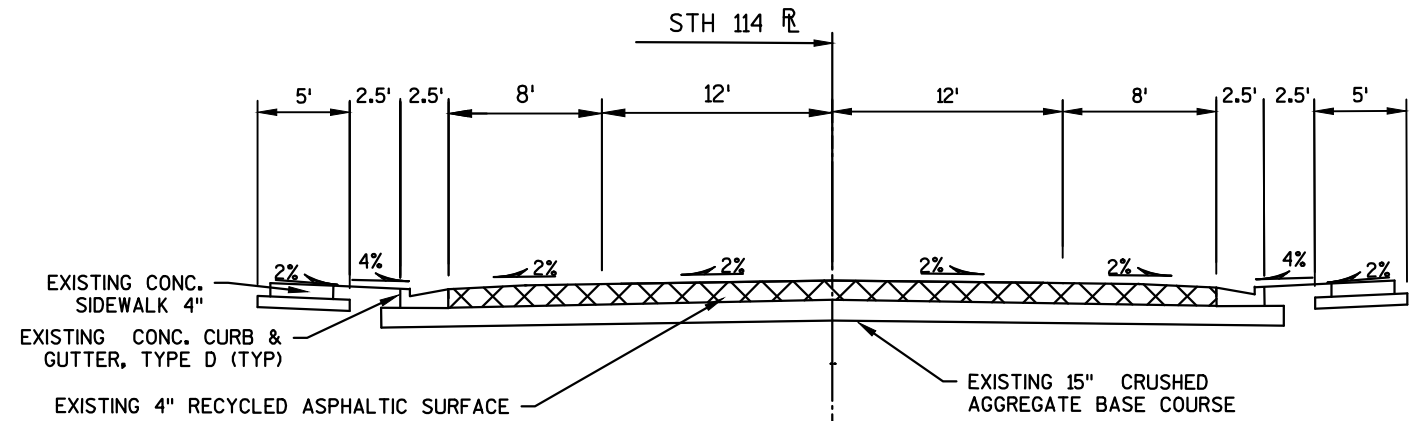
TYPICAL EXISTING CROSS SECTION FOR STH 114  
STA. 290+00 TO STA. 310+00



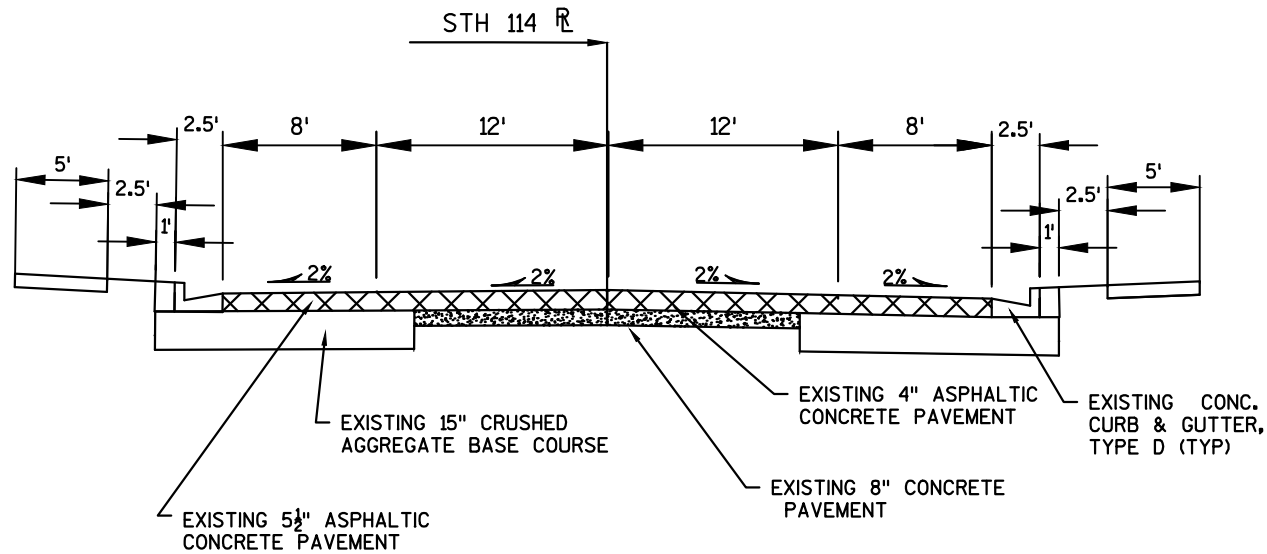
TYPICAL EXISTING HALF CROSS SECTION FOR STH 114  
STA. 392+00 TO STA. 399+00 LT.  
STA. 392+00 TO STA. 396+00 RT.  
STA. 450+00 TO STA. 462+00 RT.  
STA. 454+00 TO STA. 462+00 LT.



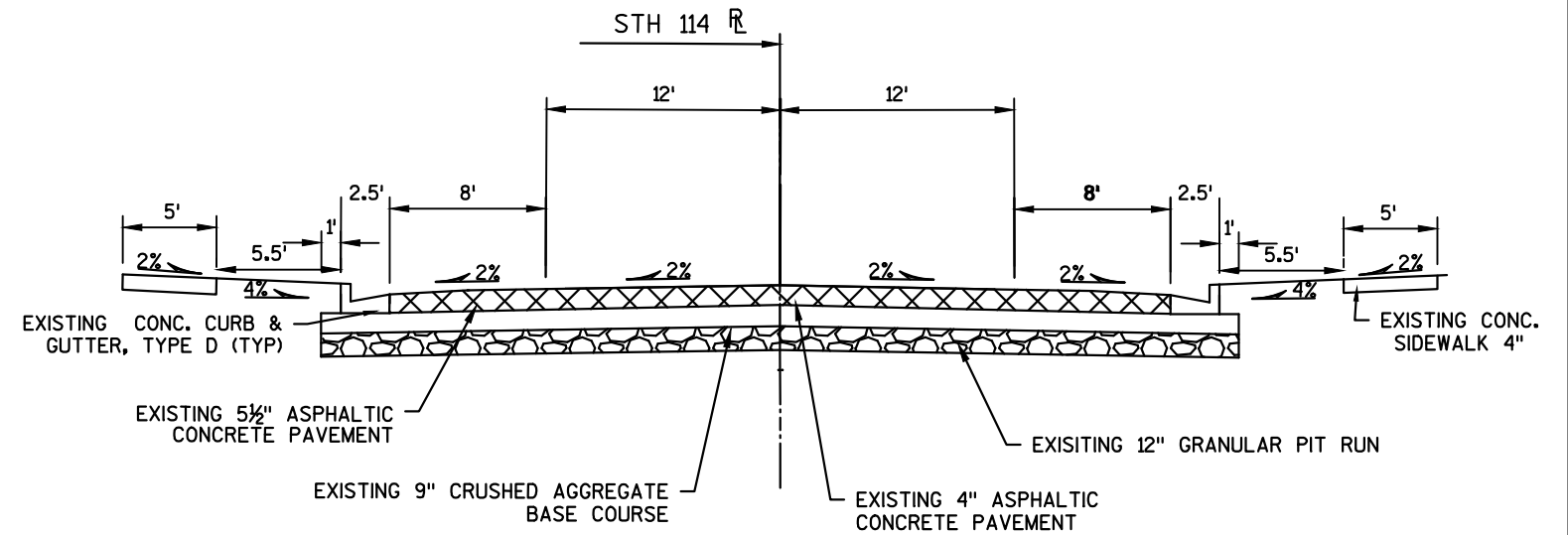
TYPICAL EXISTING CROSS SECTION FOR STH 114  
 399+00 TO 416+00 LT  
 396+00 TO 416+00 RT



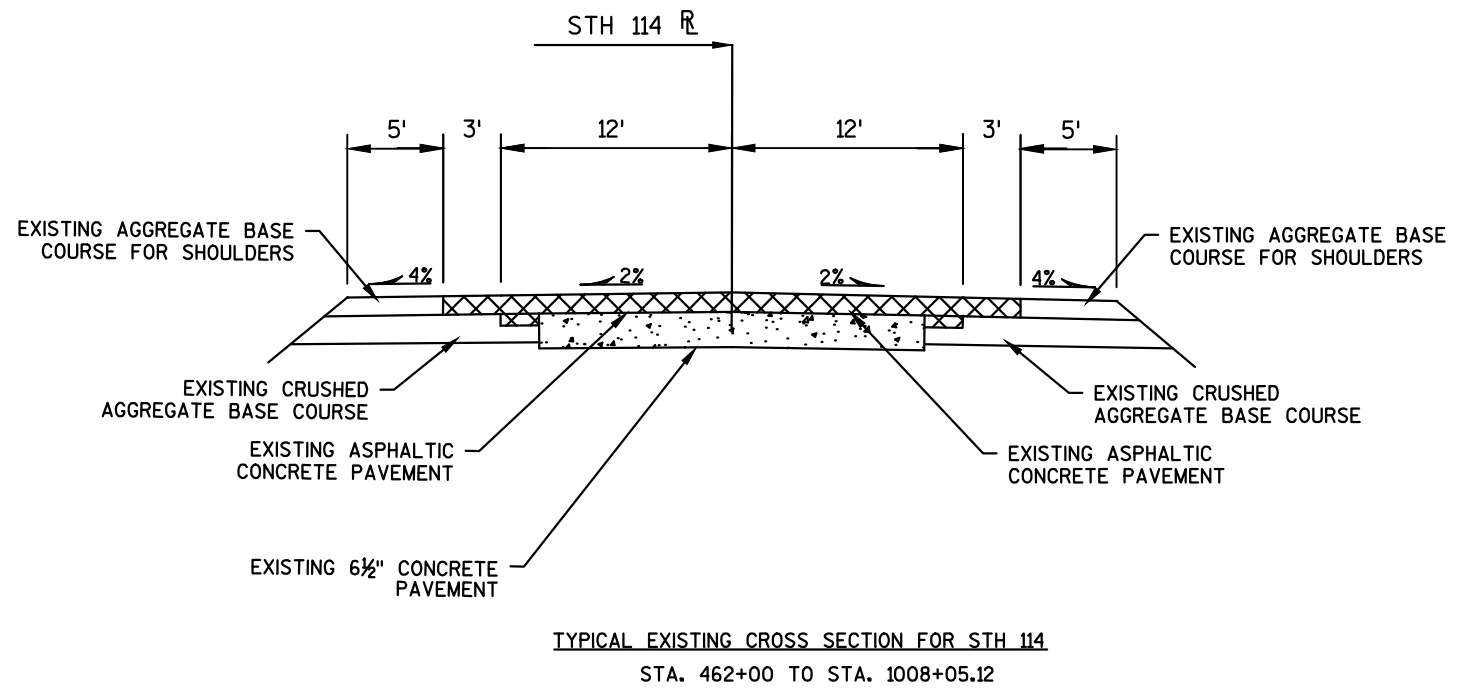
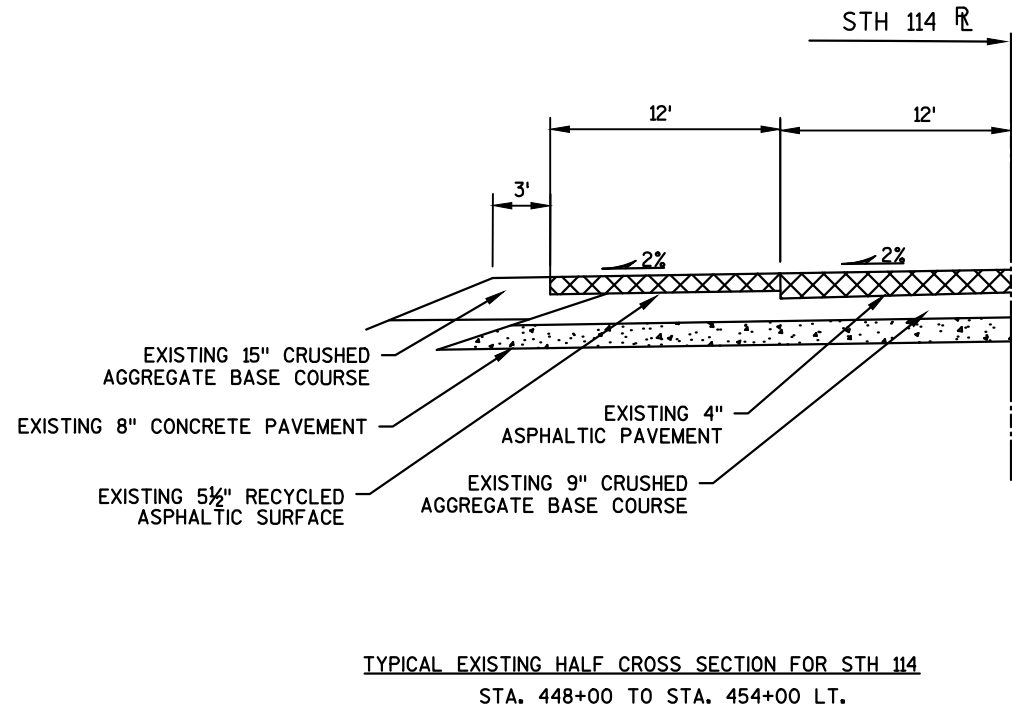
TYPICAL EXISTING CROSS SECTION FOR STH 114  
 STA. 416+00 TO STA. 436+75

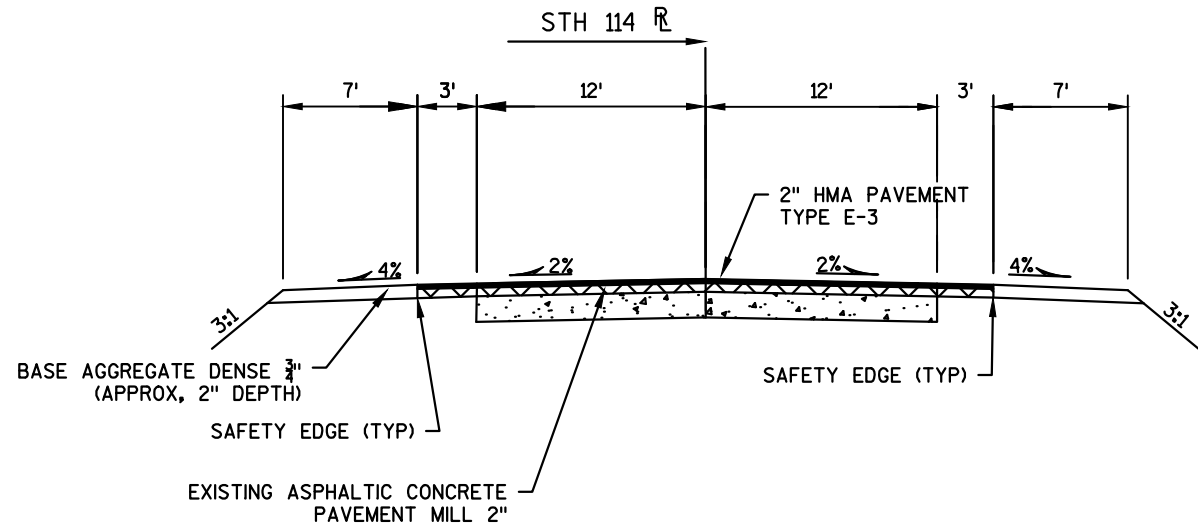


TYPICAL EXISTING CROSS SECTION FOR STH 114  
 STA. 436+75 TO STA. 440+00 LT.  
 STA. 436+75 TO STA. 439+00 RT.

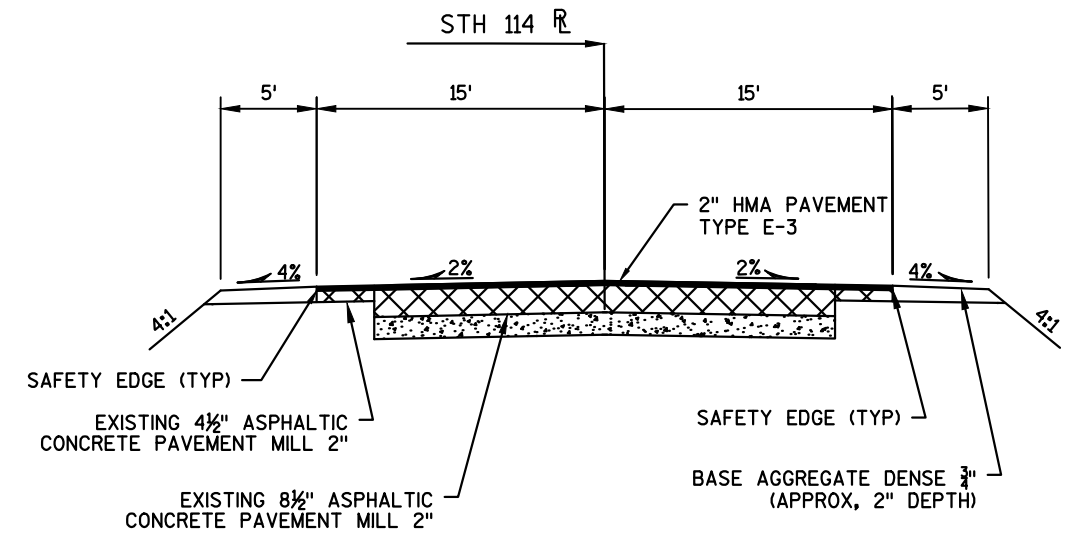


TYPICAL EXISTING CROSS SECTION FOR STH 114  
 STA. 439+00 TO STA. 450+00 RT.  
 STA. 440+00 TO STA. 448+00 LT.

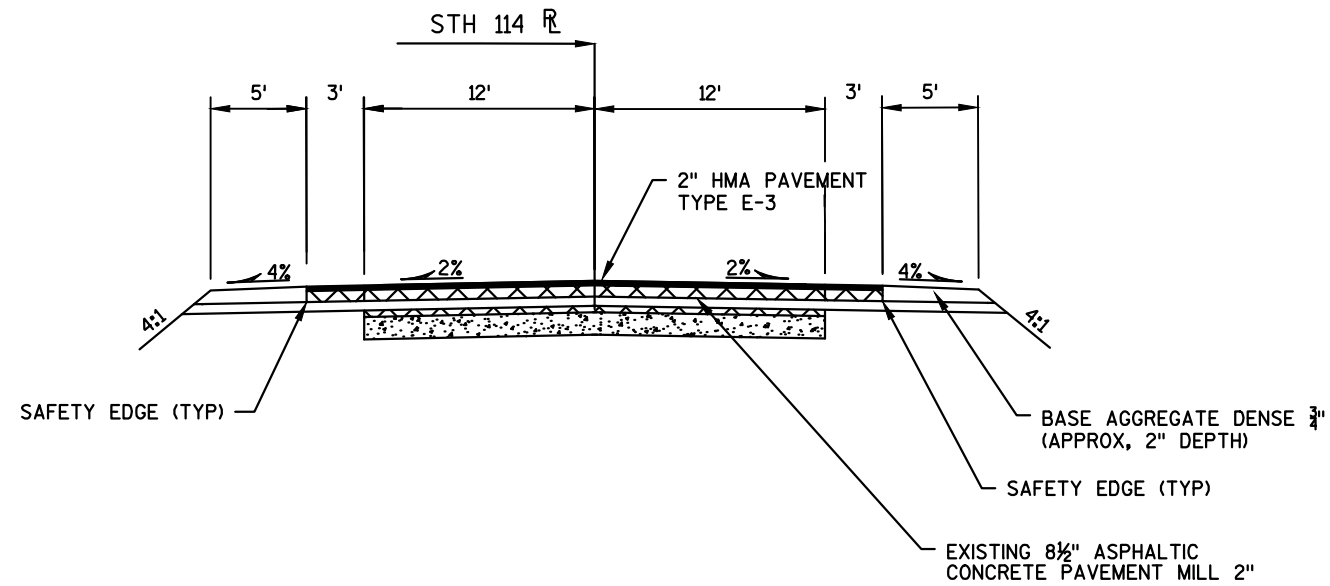




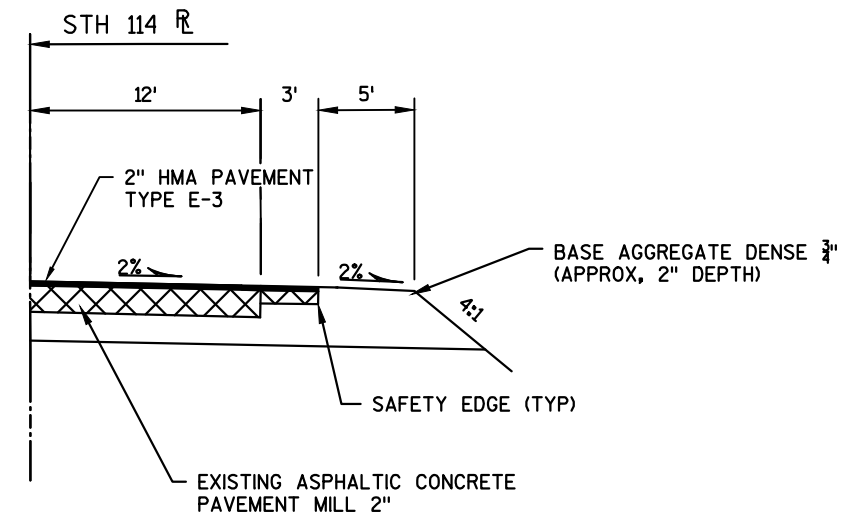
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STA. 203+75 TO STA. 208+60.48



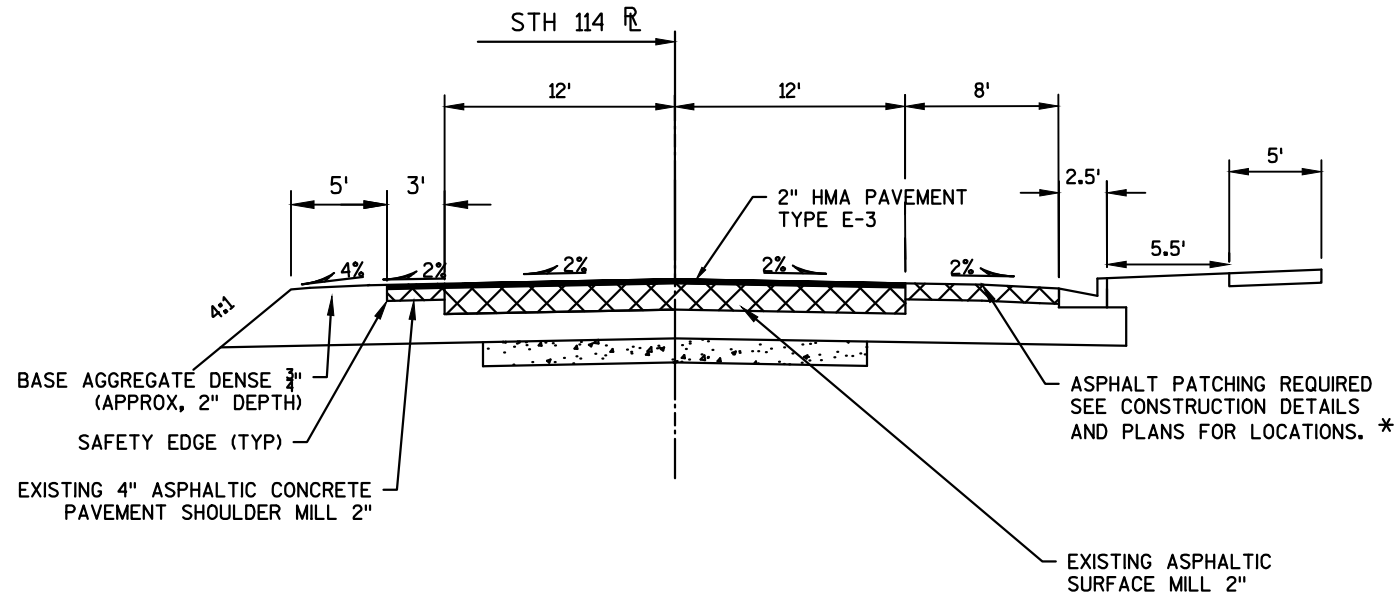
TYPICAL FINISHED CROSS SECTION FOR STH 114  
STA. 232+68.87 TO STA. 290+00  
STA. 310+00 TO STA. 321+96.54  
STA. 367+75 TO STA. 392+00



TYPICAL FINISHED CROSS SECTION FOR STH 114  
STA. 290+00 TO STA. 310+00



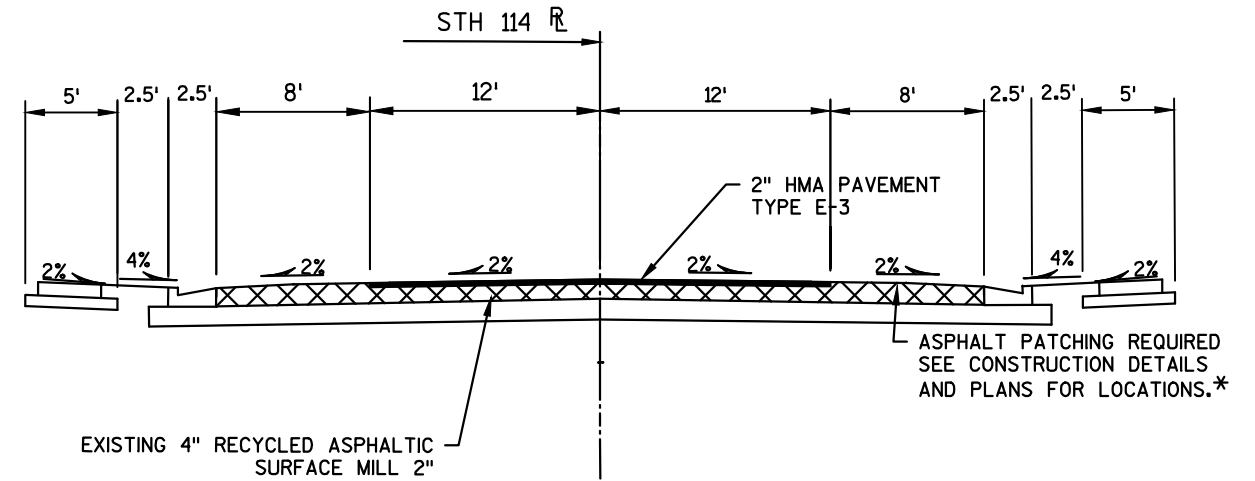
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STA. 392+00 TO STA. 396+00 RT.  
STA. 450+00 TO STA. 462+00 RT.  
STA. 454+00 TO STA. 462+00 LT.



TYPICAL FINISHED CROSS SECTION FOR STH 114

399+00 TO 416+00 LT  
396+00 TO 416+00 RT

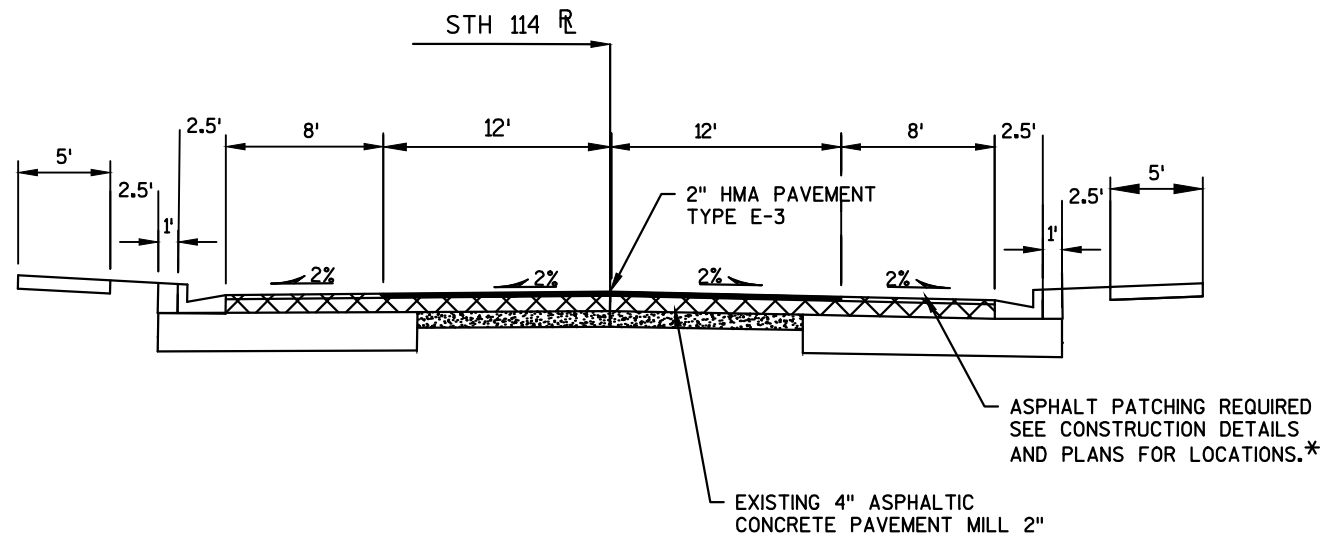
NOTE: \*- CURB AND GUTTER TYPE D REPLACEMENT REQUIRED AT SOME LOCATIONS. SEE PLANS FOR LOCATIONS AND DETAILS.



TYPICAL FINISHED CROSS SECTION FOR STH 114

STA. 416+00 TO STA. 436+75

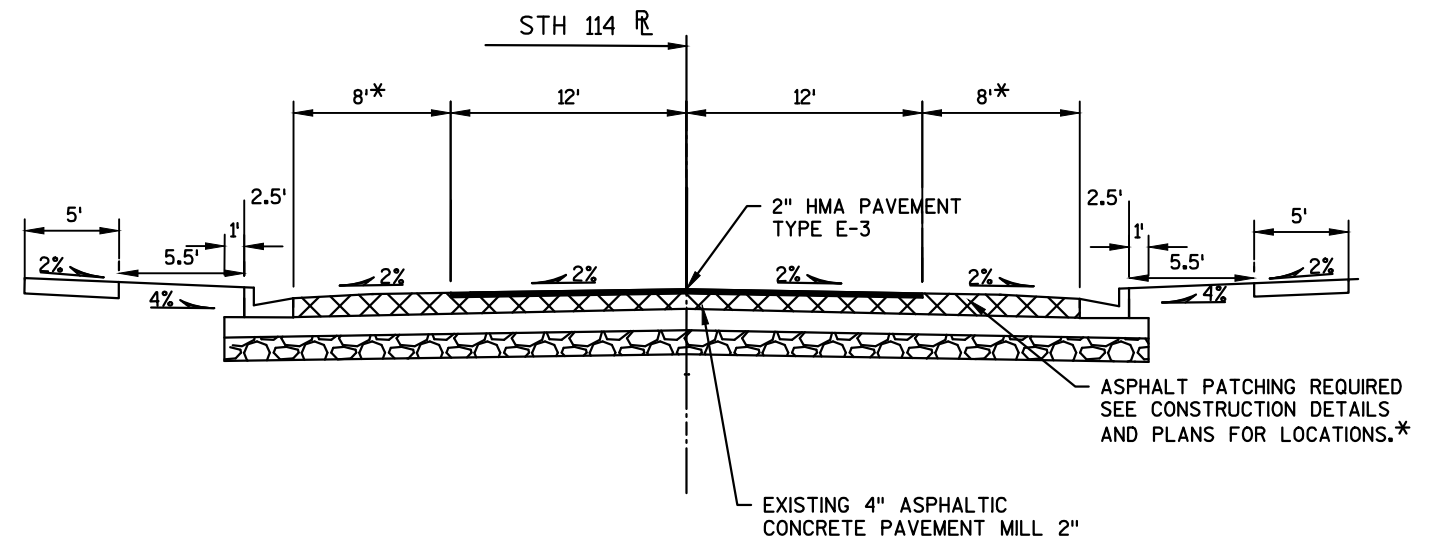
NOTE: \*- CURB AND GUTTER TYPE D REPLACEMENT REQUIRED AT SOME LOCATIONS. SEE PLANS FOR LOCATIONS AND DETAILS.



TYPICAL FINISHED CROSS SECTION FOR STH 114

STA. 436+75 TO STA. 440+00 LT.  
STA. 436+75 TO STA. 439+00 RT.

NOTE: \*- CURB AND GUTTER TYPE D REPLACEMENT REQUIRED AT SOME LOCATIONS. SEE PLANS FOR LOCATIONS AND DETAILS.

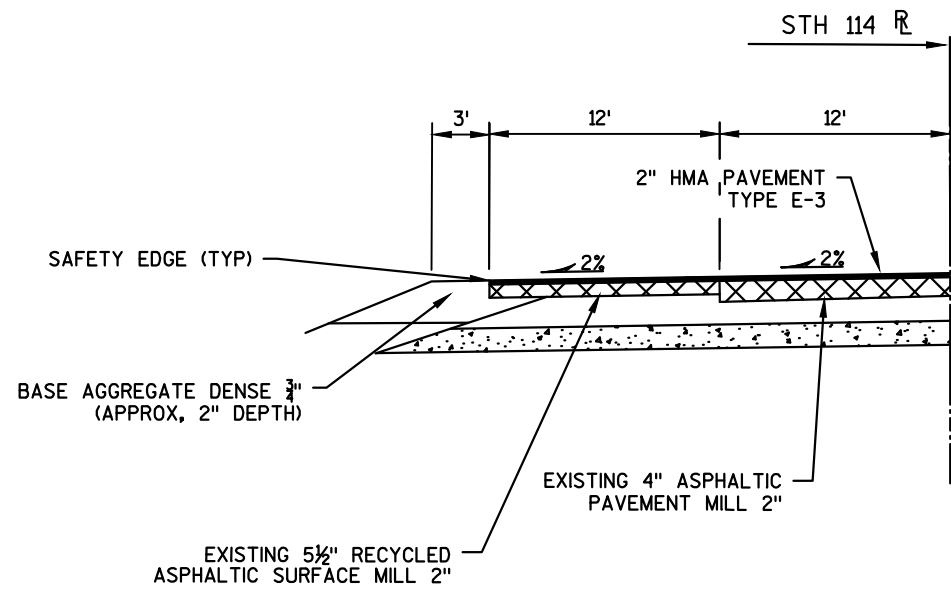


TYPICAL FINISHED CROSS SECTION FOR STH 114

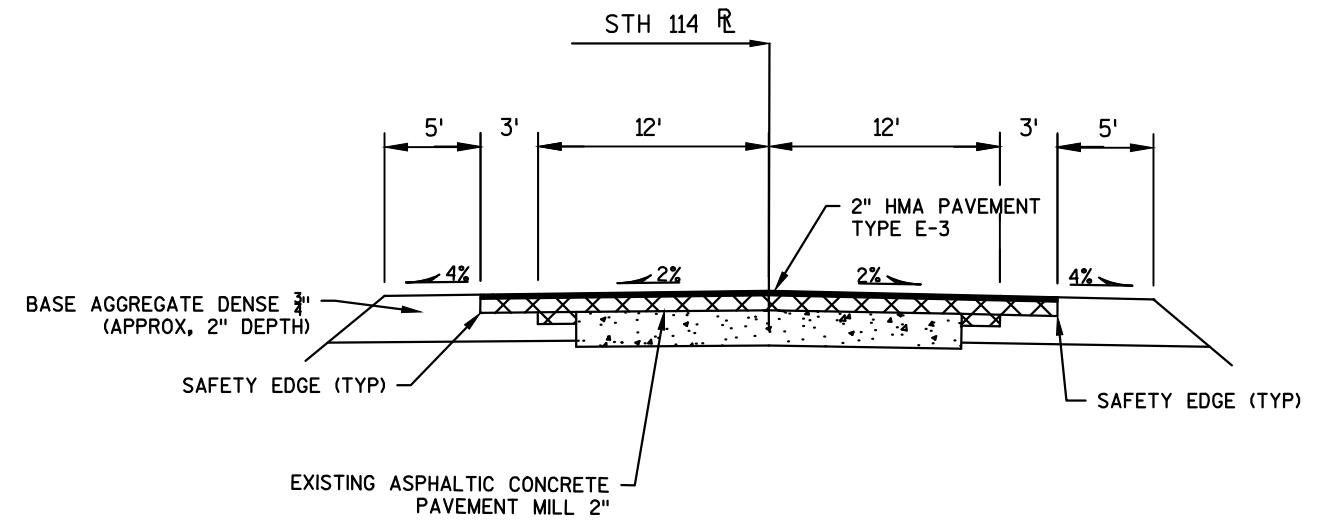
STA. 439+00 TO STA. 450+00 RT.  
STA. 440+00 TO STA. 448+00 LT.

NOTE: \*- CURB AND GUTTER TYPE D REPLACEMENT REQUIRED AT SOME LOCATIONS. SEE PLANS FOR LOCATIONS AND DETAILS.

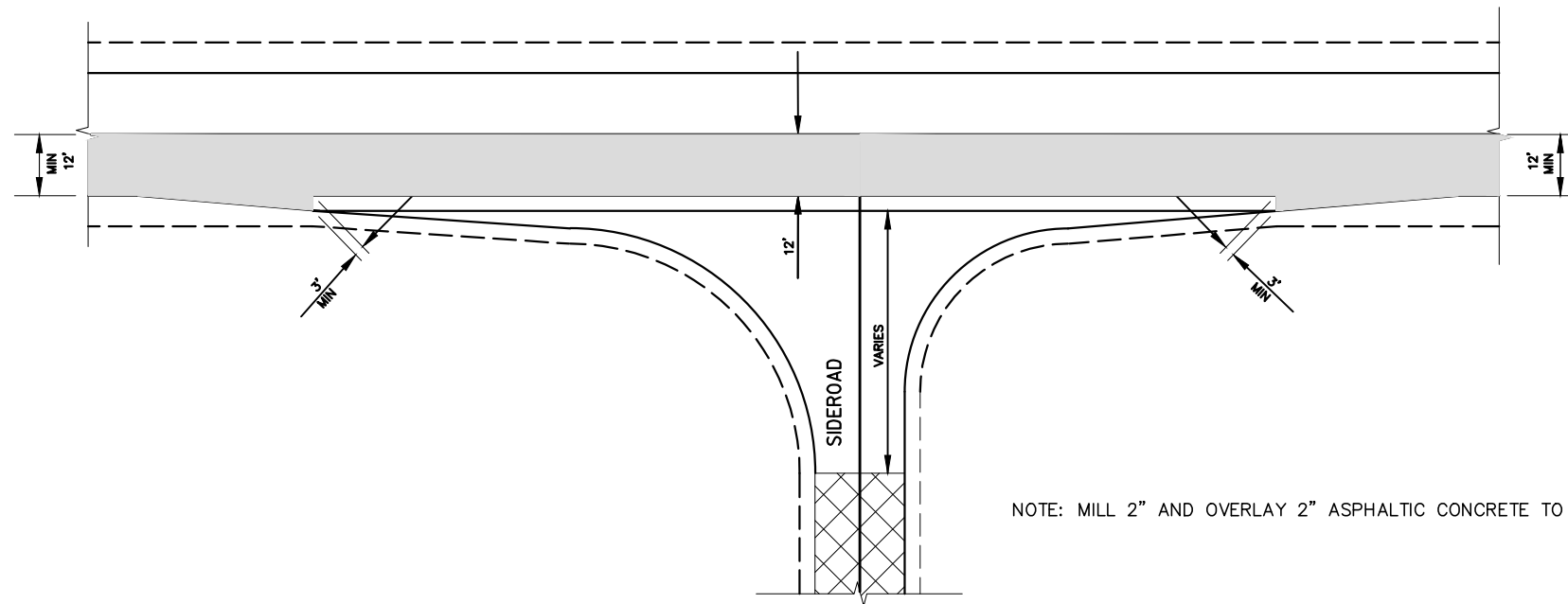




TYPICAL HALF FINISHED CROSS SECTION FOR STH 114  
STA. 448+00 TO STA. 454+00 LT.



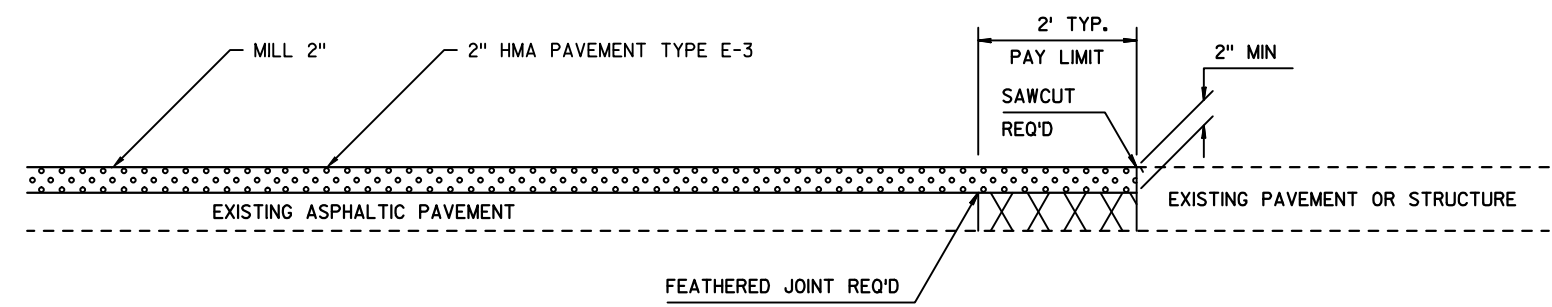
TYPICAL FINISHED CROSS SECTION FOR STH 114  
STA. 462+00 TO STA. 1008+05.12






NOTE: MILL 2" AND OVERLAY 2" ASPHALTIC CONCRETE TO RADIUS

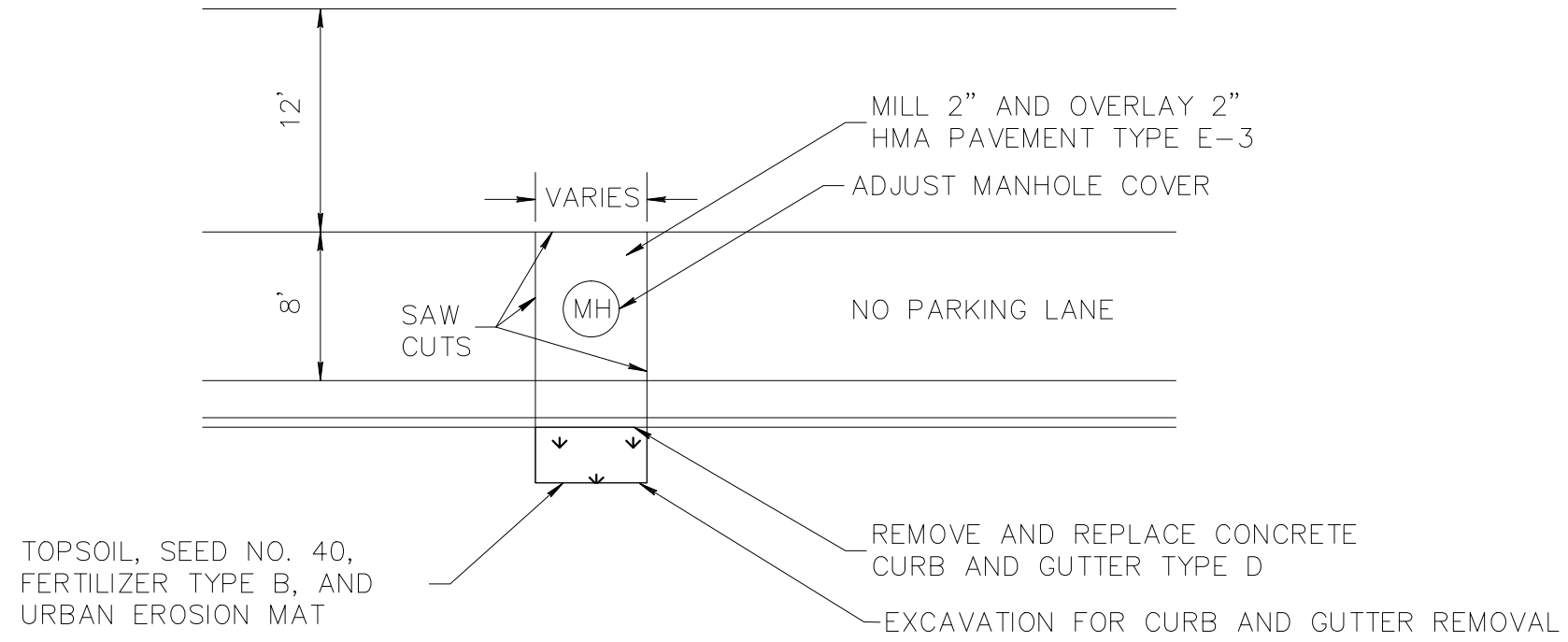
FINAL UPPER LAYER ASPHALTIC CONCRETE PAVING DETAIL AT INTERSECTIONS AND BYPASS LANES

- |                       |             |
|-----------------------|-------------|
| OLD HWY RD            | STUMPF DR   |
| FIRELANE 13           | HARRISON ST |
| STATE PARK ROAD RT/LT | MEEHL ST    |
| PIGEON RD             | CLIFTON RD  |
| LAKE BREEZE RD        | CTH B       |
| CASTLE DR             | FOREST AVE  |



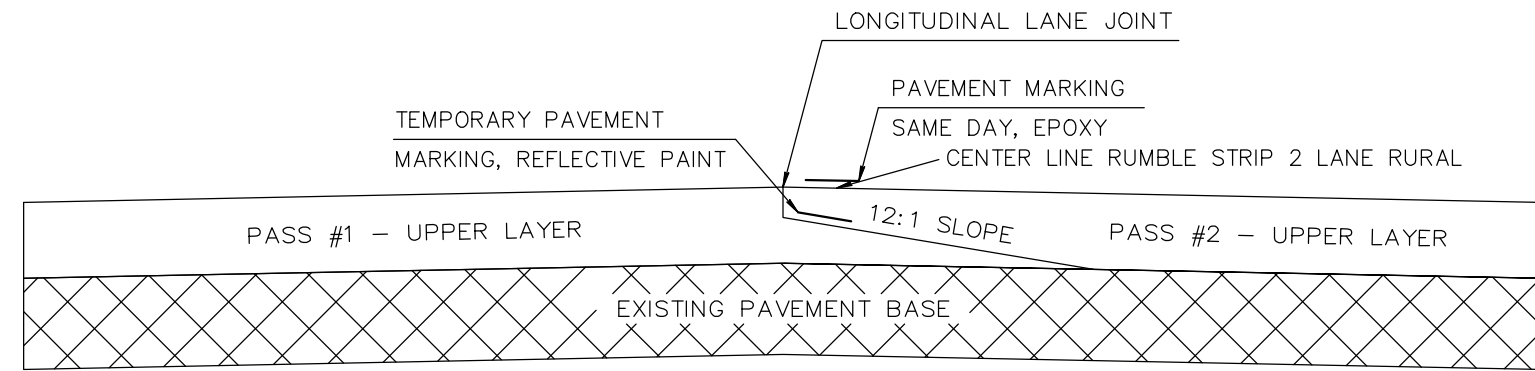
-  REMOVING ASPHALTIC SURFACE, MILLING
-  REMOVING ASPHALTIC SURFACE, BUTT JOINTS (FULL DEPTH REMOVAL OPTIONAL)
-  HMA ASPHALTIC PAVEMENT WEDGING

BUTT JOINT DETAIL FOR MILLED ASPHALTIC PAVEMENTS



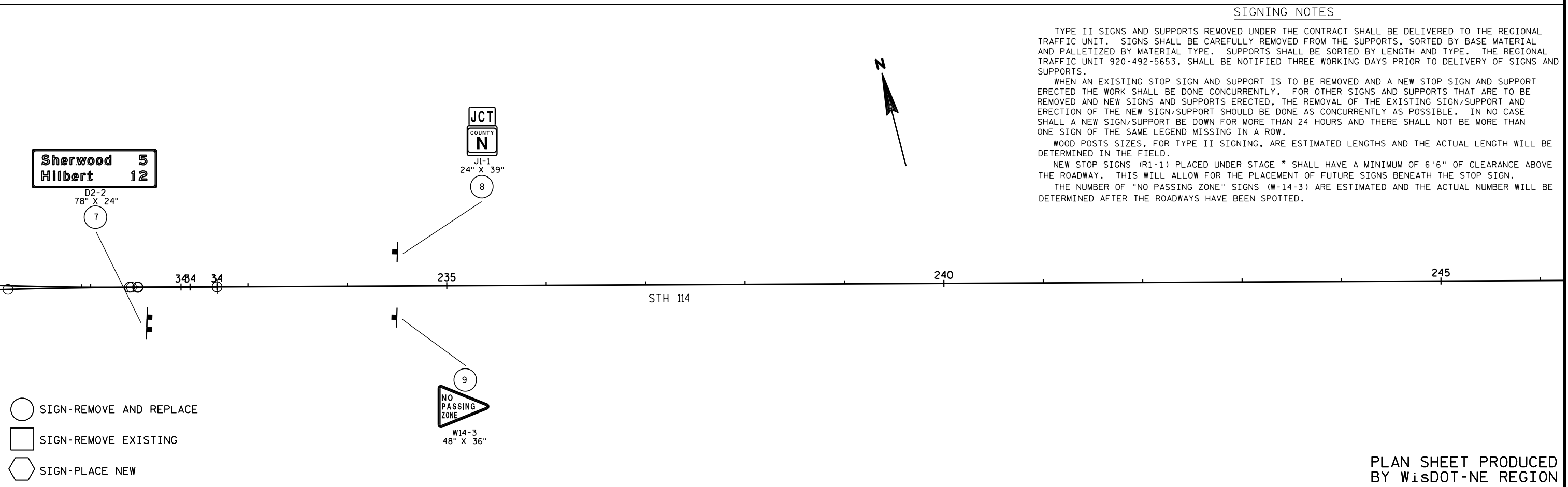
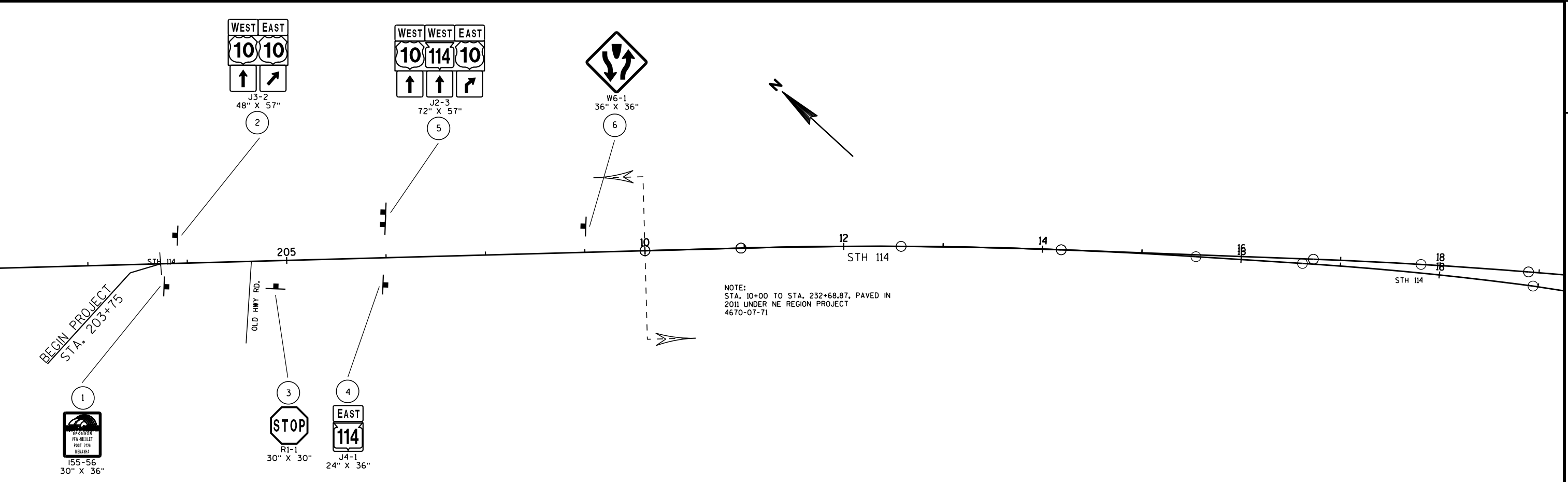
NOTES:  
SEE LOCATION AND DIMENSION IN PLAN SET.

ASPHALT PATCHING AND CURB AND GUTTER REMOVAL



NOTES:  
SEE SPV.0090.03 "MILLING AND TEMPORARY JOINT" FOR MILLING DETAIL  
CENTERLINE NO RUMBLE STRIPS WITHIN VILLAGE OF SHERWOOD.  
SEE MISCELLANEOUS QUANTITIES AND S.D.D.

PAVEMENT MARKING DETAIL FOR TAPERED OVERLAPPING JOINTS IN ASPHALTIC PAVEMENTS



PLAN SHEET PRODUCED BY WisDOT-NE REGION

SIGNING NOTES

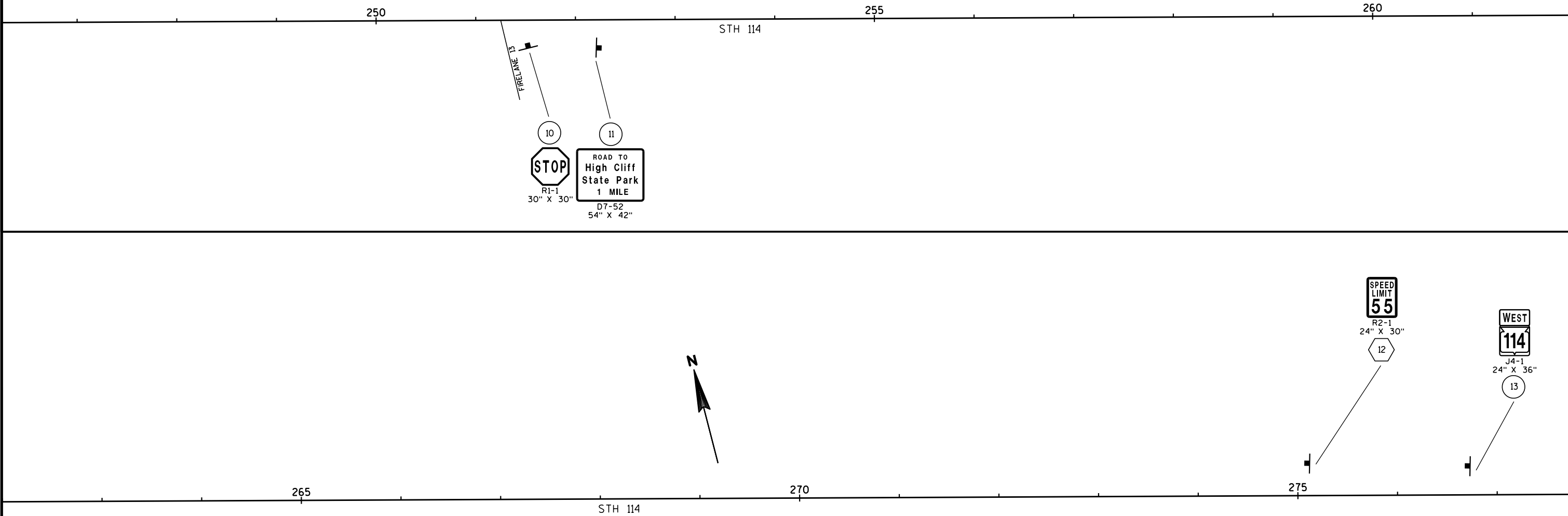
TYPE II SIGNS AND SUPPORTS REMOVED UNDER THE CONTRACT SHALL BE DELIVERED TO THE REGIONAL TRAFFIC UNIT. SIGNS SHALL BE CAREFULLY REMOVED FROM THE SUPPORTS, SORTED BY BASE MATERIAL AND PALLETIZED BY MATERIAL TYPE. SUPPORTS SHALL BE SORTED BY LENGTH AND TYPE. THE REGIONAL TRAFFIC UNIT 920-492-5653, SHALL BE NOTIFIED THREE WORKING DAYS PRIOR TO DELIVERY OF SIGNS AND SUPPORTS.

WHEN AN EXISTING STOP SIGN AND SUPPORT IS TO BE REMOVED AND A NEW STOP SIGN AND SUPPORT ERECTED THE WORK SHALL BE DONE CONCURRENTLY. FOR OTHER SIGNS AND SUPPORTS THAT ARE TO BE REMOVED AND NEW SIGNS AND SUPPORTS ERECTED, THE REMOVAL OF THE EXISTING SIGN/SUPPORT AND ERECTION OF THE NEW SIGN/SUPPORT SHOULD BE DONE AS CONCURRENTLY AS POSSIBLE. IN NO CASE SHALL A NEW SIGN/SUPPORT BE DOWN FOR MORE THAN 24 HOURS AND THERE SHALL NOT BE MORE THAN ONE SIGN OF THE SAME LEGEND MISSING IN A ROW.

WOOD POSTS SIZES, FOR TYPE II SIGNING, ARE ESTIMATED LENGTHS AND THE ACTUAL LENGTH WILL BE DETERMINED IN THE FIELD.

NEW STOP SIGNS (R1-1) PLACED UNDER STAGE \* SHALL HAVE A MINIMUM OF 6'6" OF CLEARANCE ABOVE THE ROADWAY. THIS WILL ALLOW FOR THE PLACEMENT OF FUTURE SIGNS BENEATH THE STOP SIGN.

THE NUMBER OF "NO PASSING ZONE" SIGNS (W-14-3) ARE ESTIMATED AND THE ACTUAL NUMBER WILL BE DETERMINED AFTER THE ROADWAYS HAVE BEEN SPOTTED.



- SIGN-REMOVE AND REPLACE
- SIGN-REMOVE EXISTING
- SIGN-PLACE NEW

PLAN SHEET PRODUCED BY WisDOT-NE REGION

SIGNING NOTES

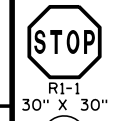
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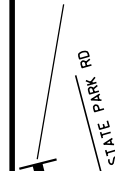
WOOD POSTS SIZES, FOR TYPE II SIGNING, ARE ESTIMATED LENGTHS AND THE ACTUAL LENGTH WILL BE DETERMINED IN THE FIELD.

NEW STOP SIGNS (R1-1) PLACED UNDER STAGE \* SHALL HAVE A MINIMUM OF 6'6" OF CLEARANCE ABOVE THE ROADWAY. THIS WILL ALLOW FOR THE PLACEMENT OF FUTURE SIGNS BENEATH THE STOP SIGN.

THE NUMBER OF "NO PASSING ZONE" SIGNS (W-14-3) ARE ESTIMATED AND THE ACTUAL NUMBER WILL BE DETERMINED AFTER THE ROADWAYS HAVE BEEN SPOTTED.



14



STATE PARK RD

R1-1  
30" X 30"



15



STATE PARK RD

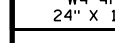
R1-1  
30" X 30"



16



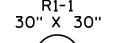
CROSS TRAFFIC DOES NOT STOP  
W4-4P  
24" X 12"



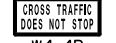
17



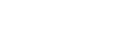
R1-1  
30" X 30"



18



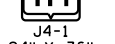
CROSS TRAFFIC DOES NOT STOP  
W4-4P  
24" X 12"



19



EAST  
114

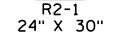


J4-1  
24" X 36"

20



SPEED LIMIT  
55



R2-1  
24" X 30"



280

285

STH 114

290

295

STH 114

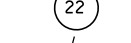
300



305



W1-7  
48" X 24"



22



High Cliff  
State Park

D7-53L  
54" X 36"



25



I55-56  
30" X 36"

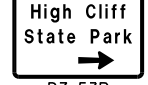


27



- SIGN-REMOVE AND REPLACE
- SIGN-REMOVE EXISTING
- SIGN-PLACE NEW

21



High Cliff  
State Park

D7-53R  
54" X 36"

23



R1-1  
30" X 30"

24



J13-1  
24" X 45"

26



I55-56

PICEON RD.

PLAN SHEET PRODUCED BY WisDOT-NE REGION

SIGNING NOTES

TYPE II SIGNS AND SUPPORTS REMOVED UNDER THE CONTRACT SHALL BE DELIVERED TO THE REGIONAL TRAFFIC UNIT. SIGNS SHALL BE CAREFULLY REMOVED FROM THE SUPPORTS, SORTED BY BASE MATERIAL AND PALLETIZED BY MATERIAL TYPE. SUPPORTS SHALL BE SORTED BY LENGTH AND TYPE. THE REGIONAL TRAFFIC UNIT 920-492-5653, SHALL BE NOTIFIED THREE WORKING DAYS PRIOR TO DELIVERY OF SIGNS AND SUPPORTS.

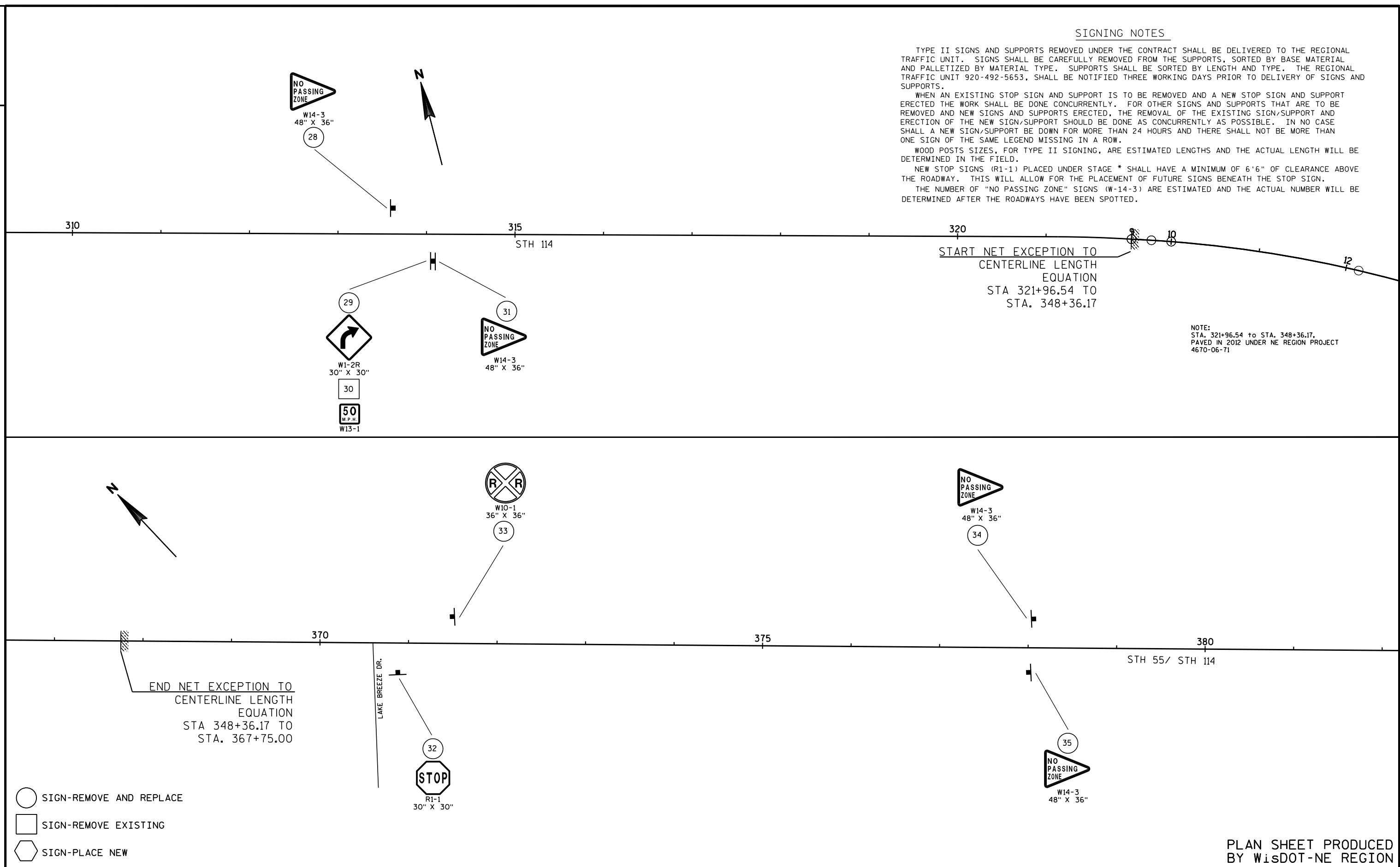
WHEN AN EXISTING STOP SIGN AND SUPPORT IS TO BE REMOVED AND A NEW STOP SIGN AND SUPPORT ERECTED THE WORK SHALL BE DONE CONCURRENTLY. FOR OTHER SIGNS AND SUPPORTS THAT ARE TO BE REMOVED AND NEW SIGNS AND SUPPORTS ERECTED, THE REMOVAL OF THE EXISTING SIGN/SUPPORT AND ERECTION OF THE NEW SIGN/SUPPORT SHOULD BE DONE AS CONCURRENTLY AS POSSIBLE. IN NO CASE SHALL A NEW SIGN/SUPPORT BE DOWN FOR MORE THAN 24 HOURS AND THERE SHALL NOT BE MORE THAN ONE SIGN OF THE SAME LEGEND MISSING IN A ROW.

WOOD POSTS SIZES, FOR TYPE II SIGNING, ARE ESTIMATED LENGTHS AND THE ACTUAL LENGTH WILL BE DETERMINED IN THE FIELD.

NEW STOP SIGNS (R1-1) PLACED UNDER STAGE \* SHALL HAVE A MINIMUM OF 6'6" OF CLEARANCE ABOVE THE ROADWAY. THIS WILL ALLOW FOR THE PLACEMENT OF FUTURE SIGNS BENEATH THE STOP SIGN.

THE NUMBER OF "NO PASSING ZONE" SIGNS (W-14-3) ARE ESTIMATED AND THE ACTUAL NUMBER WILL BE DETERMINED AFTER THE ROADWAYS HAVE BEEN SPOTTED.

NOTE:  
STA. 321+96.54 TO STA. 348+36.17,  
PAVED IN 2012 UNDER NE REGION PROJECT  
4670-06-71



START NET EXCEPTION TO  
CENTERLINE LENGTH  
EQUATION  
STA 321+96.54 TO  
STA. 348+36.17

END NET EXCEPTION TO  
CENTERLINE LENGTH  
EQUATION  
STA 348+36.17 TO  
STA. 367+75.00

- SIGN-REMOVE AND REPLACE
- SIGN-REMOVE EXISTING
- ⬡ SIGN-PLACE NEW

PLAN SHEET PRODUCED  
BY WisDOT-NE REGION

PROJECT NO: 4670-11-60	HWY: STH 114	COUNTY: CALUMET	PERMANENT SIGNING	SHEET	E
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W3-5  
36" X 36"

36



R2-1  
24" X 30"

37



R7-1L  
24" X 30"

40

385

390

395

STH 55/ STH 114

SIGNING NOTES

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38



R2-1  
24" X 30"

CASTLE DR.

39



R1-1  
30" X 30"



R2-1  
24" X 30"

42



R7-1D  
24" X 30"

41

REINSTALL VILLAGE PARKING BELOW SIGN



R7-1D  
18" X 24"

43



R7-1D  
24" X 30"

45



W3-5  
36" X 36"

47



I55-56

48



W8-8

49



R7-1D  
24" X 30"

50



R2-1  
24" X 30"

51



R7-1D  
18" X 24"

52



W14-3  
48" X 36"

53



R2-1  
24" X 30"

54

400

405

410

STH 55/ STH 114

○ SIGN-REMOVE AND REPLACE

□ SIGN-REMOVE EXISTING

⬡ SIGN-PLACE NEW



R2-1  
24" X 30"

44

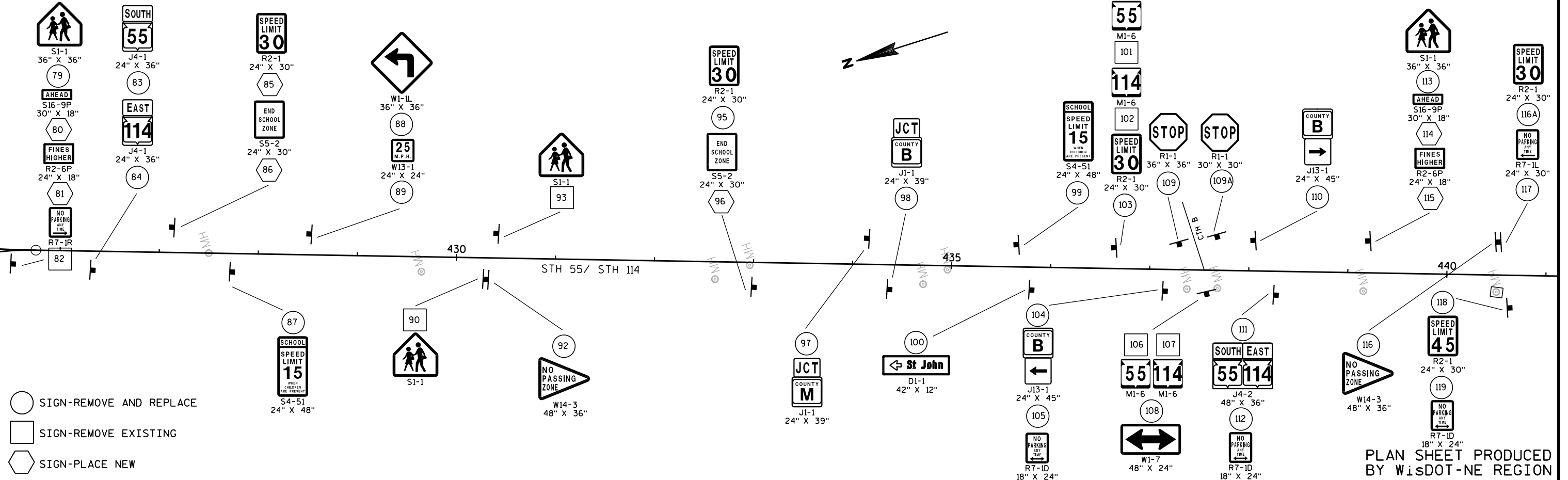
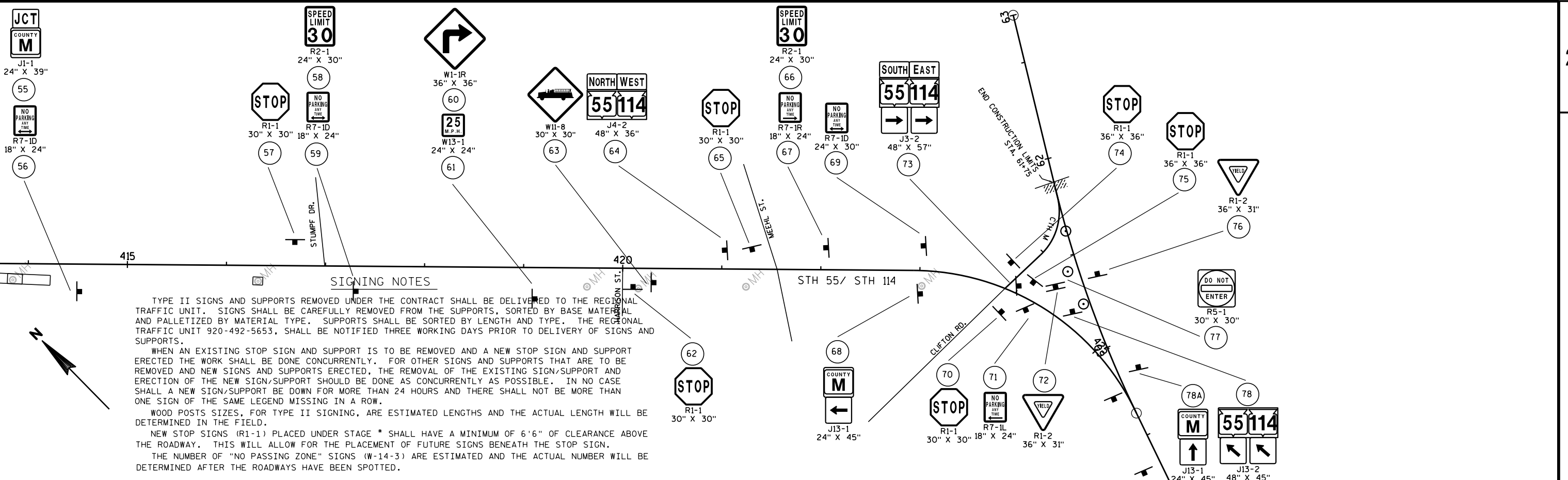


W14-3  
48" X 36"

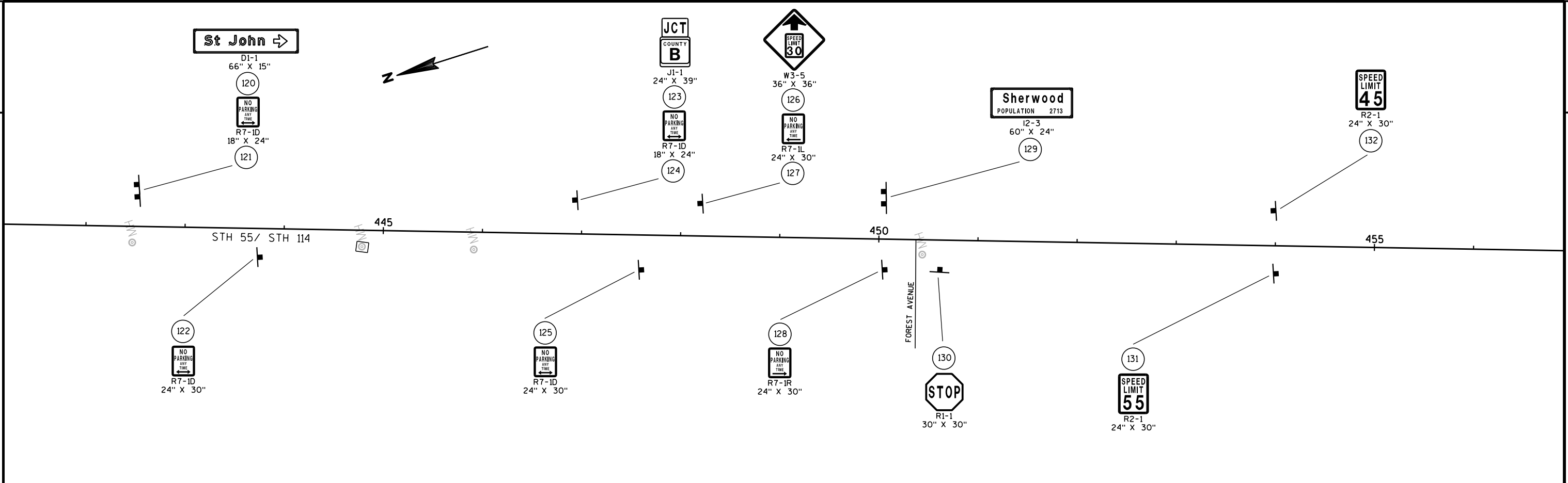
46

PLAN SHEET PRODUCED BY WisDOT-NE REGION





PLAN SHEET PRODUCED BY WisDOT-NE REGION



SIGNING NOTES

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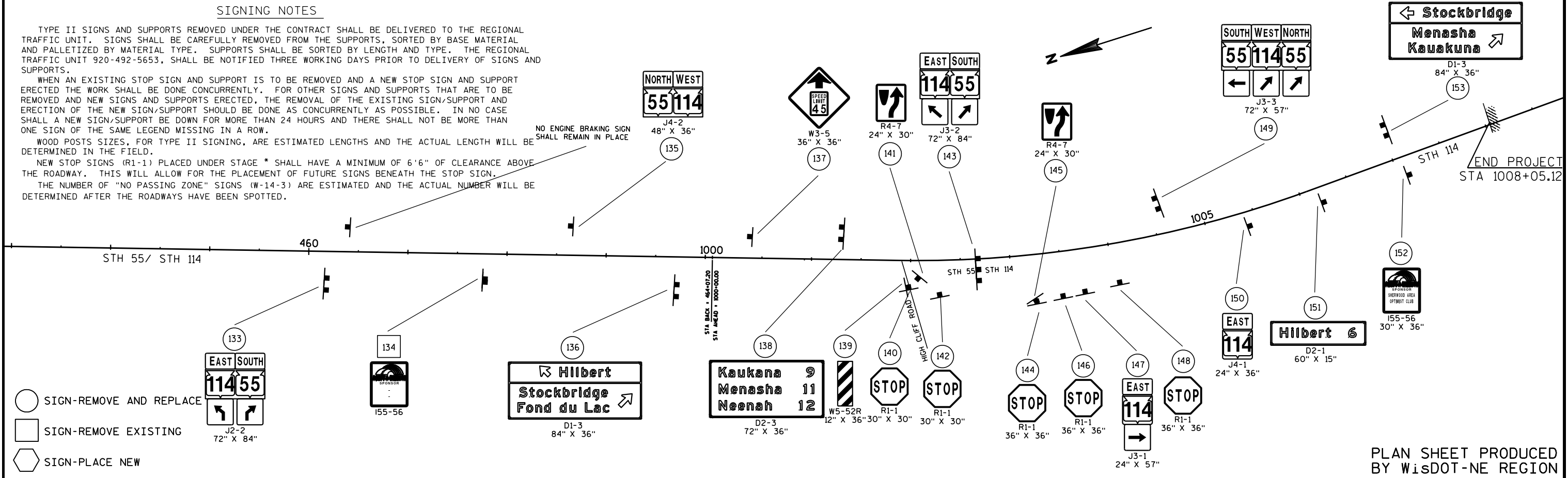
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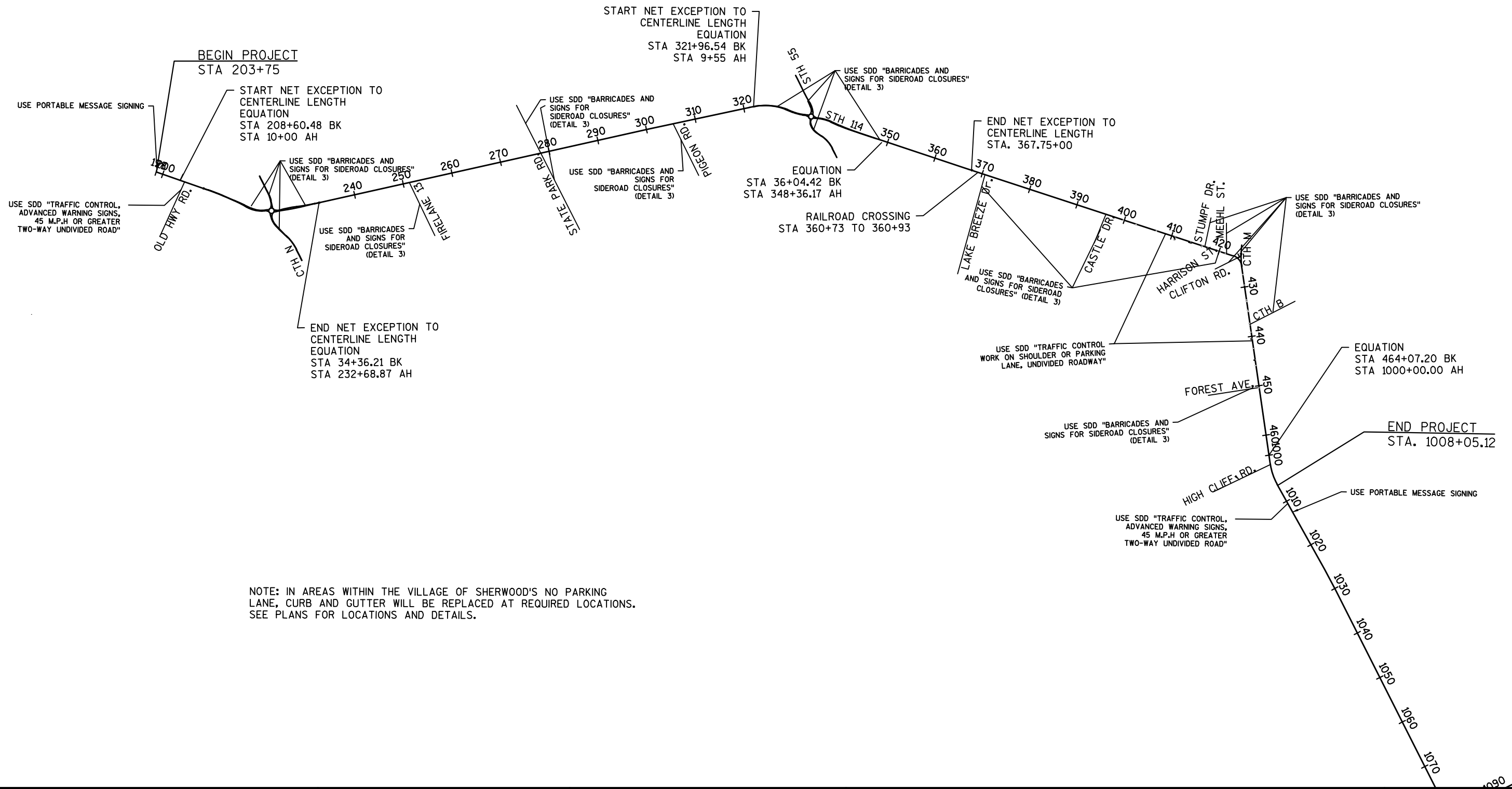
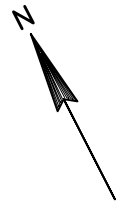
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NO ENGINE BRAKING SIGN SHALL REMAIN IN PLACE



- SIGN-REMOVE AND REPLACE
- SIGN-REMOVE EXISTING
- SIGN-PLACE NEW



NOTE: IN AREAS WITHIN THE VILLAGE OF SHERWOOD'S NO PARKING LANE, CURB AND GUTTER WILL BE REPLACED AT REQUIRED LOCATIONS. SEE PLANS FOR LOCATIONS AND DETAILS.

DATE 03APR14

## ESTIMATE OF QUANTITIES

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	4670-11-60 QUANTITY
0010	204.0115	REMOVING ASPHALTIC SURFACE BUTT JOINTS	SY	115.000	115.000
0020	204.0120	REMOVING ASPHALTIC SURFACE MILLING	SY	76,960.000	76,960.000
0030	204.0150	REMOVING CURB & GUTTER	LF	55.000	55.000
0040	211.0100	PREPARE FOUNDATION FOR ASPHALTIC PAVING (PROJECT) 01. 4670-11-60	LS	1.000	1.000
0050	213.0100	FINISHING ROADWAY (PROJECT) 01. 4670-11-60	EACH	1.000	1.000
0060	305.0110	BASE AGGREGATE DENSE 3/4-INCH	TON	2,450.000	2,450.000
0070	305.0500	SHAPING SHOULDERS	STA	312.000	312.000
0080	440.4410.S	INCENTIVE IRI RIDE	DOL	7,400.000	7,400.000
0090	455.0105	ASPHALTIC MATERIAL PG58-28	TON	490.000	490.000
0100	455.0605	TACK COAT	GAL	1,930.000	1,930.000
0110	460.1103	HMA PAVEMENT TYPE E-3	TON	8,850.000	8,850.000
0120	460.2000	INCENTIVE DENSITY HMA PAVEMENT	DOL	5,380.000	5,380.000
0130	460.4110.S	REHEATING HMA PAVEMENT LONGITUDINAL JOINTS	LF	20,230.000	20,230.000
0140	465.0110	ASPHALTIC SURFACE PATCHING	TON	110.000	110.000
0150	465.0475	ASPHALT CENTER LINE RUMBLE STRIP 2-LANE RURAL	LF	13,980.000	13,980.000
0160	601.0411	CONCRETE CURB & GUTTER 30-INCH TYPE D	LF	55.000	55.000
0170	611.8110	ADJUSTING MANHOLE COVERS	EACH	14.000	14.000
0180	618.0100	MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) 01. 4670-11-60	EACH	1.000	1.000
0190	619.1000	MOBILIZATION	EACH	1.000	1.000
0200	624.0100	WATER	MGAL	2.000	2.000
0210	625.0100	TOPSOIL	SY	41.000	41.000
0220	627.0200	MULCHING	SY	41.000	41.000
0230	628.1504	SILT FENCE	LF	360.000	360.000
0240	628.1520	SILT FENCE MAINTENANCE	LF	360.000	360.000
0250	628.1905	MOBILIZATIONS EROSION CONTROL	EACH	1.000	1.000
0260	628.1910	MOBILIZATIONS EMERGENCY EROSION CONTROL	EACH	1.000	1.000
0270	628.7010	INLET PROTECTION TYPE B	EACH	8.000	8.000
0280	629.0210	FERTILIZER TYPE B	CWT	1.000	1.000
0290	630.0140	SEEDING MIXTURE NO. 40	LB	1.000	1.000
0300	634.0612	POSTS WOOD 4X6-INCH X 12-FT	EACH	3.000	3.000
0310	634.0614	POSTS WOOD 4X6-INCH X 14-FT	EACH	87.000	87.000
0320	634.0616	POSTS WOOD 4X6-INCH X 16-FT	EACH	34.000	34.000
0330	637.2210	SIGNS TYPE II REFLECTIVE H	SF	846.030	846.030
0340	637.2230	SIGNS TYPE II REFLECTIVE F	SF	204.570	204.570
0350	638.2602	REMOVING SIGNS TYPE II	EACH	205.000	205.000
0360	638.3000	REMOVING SMALL SIGN SUPPORTS	EACH	129.000	129.000
0370	642.5201	FIELD OFFICE TYPE C	EACH	1.000	1.000
0380	643.0100	TRAFFIC CONTROL (PROJECT) 01. 4670-11-60	EACH	1.000	1.000
0390	643.0300	TRAFFIC CONTROL DRUMS	DAY	360.000	360.000
0400	643.0705	TRAFFIC CONTROL WARNING LIGHTS TYPE A	DAY	360.000	360.000
0410	643.0900	TRAFFIC CONTROL SIGNS	DAY	1,080.000	1,080.000
0420	643.1050	TRAFFIC CONTROL SIGNS PCMS	DAY	56.000	56.000
0430	646.0406	PAVEMENT MARKING SAME DAY EPOXY 4-INCH	LF	46,670.000	46,670.000
0440	647.0566	PAVEMENT MARKING STOP LINE EPOXY 18-INCH	LF	105.000	105.000
0450	647.0766	PAVEMENT MARKING CROSSWALK EPOXY 6-INCH	LF	1,350.000	1,350.000
0460	648.0100	LOCATING NO-PASSING ZONES	MI	7.520	7.520
0470	649.0200	TEMPORARY PAVEMENT MARKING REFLECTIVE PAINT 4-INCH	LF	20,620.000	20,620.000
0480	650.8000	CONSTRUCTION STAKING RESURFACING REFERENCE	LF	20,230.000	20,230.000

DATE 03APR14

E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	4670-11-60 QUANTITY
0490	650.9910	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) 01. 4670-11-60	LS	1.000	1.000
0500	690.0150	SAWING ASPHALT	LF	805.000	805.000
0510	690.0250	SAWING CONCRETE	LF	25.000	25.000
0520	SPV.0060	SPECIAL 01. ADJUSTING WATER VALVE BOXES	EACH	1.000	1.000
0530	SPV.0090	SPECIAL 01. PAVEMENT MARKING GROOVED WET REFLECTIVE EPOXY 4-INCH	LF	44,465.000	44,465.000
0540	SPV.0090	SPECIAL 02. PAVEMENT MARKING GROOVED WET REFLECTIVE EPOXY 8-INCH	LF	880.000	880.000
0550	SPV.0090	SPECIAL 03. MILLING AND REMOVING TEMPORARY JOINT	LF	20,230.000	20,230.000

3

3

**REMOVING ASPHALTIC SURFACE BUTT JOINTS**

STATION	TO	STATION	LOCATION	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	REMARKS
CATEGORY 0010					
203+75	-	204+25	STH 114	7	BEGINNING OF PROJECT
204+74.01	-	204.74.01	OLD HWY ROAD	5	SOUTH OF STH 114
208+60.48	-	209+10.48	STH 114	7	WEST OF STH 114 AND CTH N RAB
232+68.87	-	233+18.87	STH 114	7	EAST OF STH 114 and CTH N RAB
251+25	-	251+25	FIRELANE 13	5	SOUTH OF STH 114
278+30.95	-	278+30.95	STATE PARK ROAD	11	NORTH AND SOUTH OF STH 114
305+49.09	-	305+49.09	PIGEON ROAD	5	SOUTH OF STH 114
321+96.54	-	322+46.54	STH 114	7	WEST OF STH 55/114 RAB
367+75	-	368+25	STH 114	7	EAST OF STH 55/114 RAB
370+59.76	-	370+59.76	LAKE BREEZE DR.	5	SOUTHWEST OF STH 114
396+15.45	-	396+15.45	CASTLE DR.	5	WEST OF STH 114
416+99.19	-	416+99.19	STUMPF DR.	5	NORTH OF STH 114
420+00	-	420+00	HARRISON ST.	5	WEST OF STH 114
421+55.89	-	421+55.89	MEEHL ST.	5	NORTH OF STH 114
423+87.13	-	423+87.13	CLIFTON ROAD	5	EAST OF STH 114
437+55.09	-	437+55.09	CTH B	5	EAST OF STH 114 and CTH N RAB
450+37.43	-	450+37.43	FOREST AVENUE	5	WEST OF STH 114
1001+91.11	-	1001+91.11	HIGH CLIFF ROAD	5	N. SIDE RAB
1008+05.12	-	1008+05.12	STH 114	7	END OF PROJECT

**TOTAL 115**

**PREPARE FOUNDATION FOR ASPHALTIC PAVING (PROJECT)**

STATION	TO	STATION	LOCATION	211.0100 ASPHALTIC PAVING PROJECT LS
CATEGORY 0010				
203+75	-	208+60.48	STH 114	1
232+68.87	-	321+96.54	STH 114	
367+75	-	416+99.19	STH 114	
416+99.19	-	447+68.6	STH 114	
59+27.37	-	61+75	COLLECTOR	
60+46.80	-	61+75	CTH M	
447+68.6	-	1008+05.12	STH 114	

**TOTAL 1**

**CONCRETE CURB & GUTTER SUMMARY**

STATION	TO	STATION	LOCATION	204.0150 REMOVING CURB & GUTTER LF	601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D LF
CATEGORY 0010					
406+20.36	-	406+32.36	STH 114	12	12
410+22.50	-	410+29.5	STH 114	7	7
417+37.65	-	417+51.65	STH 114	14	14
440+30.83	-	440+42.83	STH 114	12	12
443+97.44	-	444+07.44	STH 114	10	10

**TOTAL 55 55**

**FINISHING ROADWAY (PROJECT)**

STATION	TO	STATION	LOCATION	213.0100 FINISHING ROADWAY PROJECT EACH
CATEGORY 0010				
PROJECT			STH 114	1

**TOTAL 1**

**MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT)**

STATION	TO	STATION	LOCATION	618.0100 MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) EACH
CATEGORY 0010				
PROJECT			STH 114	1

**TOTAL 1**

**REMOVING ASPHALTIC SURFACE MILLING**

STATION	TO	STATION	LOCATION	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY	REMARKS
CATEGORY 0010					
203+75	-	208+60.48	STH 114	1,618	BEGINNING OF PROJECT TO CTHN/STH114 RAB
201+74.01	-	204+74.01	OLD HWY RD RT	206	BACK TO RADIUS OF INTERSECTION
232+68.87	-	321+96.54	STH 114	29,759	EAST OF CTH N/STH 114 RAB TO STH 55/114 RAB
251+25	-	251+25	FIRELANE 13 RT	310	BACK TO RADIUS OF INTERSECTION
278+30.95	-	278+30.95	STATE PARK RD RT/LT	668	BACK TO RADIUS OF INTERSECTION
275+30.95	-	278+30.95	STH 114 RT	506	STATE PARK RD TURN LANE
278+30.95	-	281+30.95	STH 114 LT	506	STATE PARK RD TURN LANE
305+49.09	-	305+49.09	PIGEON RD. RT	371	BACK TO RADIUS OF INTERSECTION
302+49.09	-	305+49.09	STH 114 RT	506	PIGEON RD. TURN LANE
302+49.09	-	309+49.09	STH 114	785	PIGEON RD BYPASS LANE
367+75	-	396+15.45	STH 114	9,468	STH 55/114 RAB TO CASTLE DRIVE
370+59.76	-	370+59.76	LAKE BREEZE DR. RT	314	BACK TO RADIUS OF INTERSECTION
367+59.76	-	370+59.76	STH 114 RT	545	LAKE BREEZE ROAD TURN LANE
367+59.76	-	374+59.76	STH 114	785	LAKE BREEZE ROAD BYPASS LANE
396+15.45	-	396+15.45	CASTLE DR. RT	407	BACK TO RADIUS OF INTERSECTION
393+15.45	-	396+15.45	STH 114	785	CASTLE DRIVE BYPASS LANE
396+15.45	-	416+99.19	STH 114	6,714	CASTLE DRIVE TO STUMPF DRIVE
416+99.19	-	416+99.19	STUMPF DR. LT	274	BACK TO RADIUS OF INTERSECTION
416+99.19	-	447+68.6	STH 114	9,549	URBAN AREA
420+00	-	420+00	HARRISON ST RT	346	BACK TO RADIUS OF INTERSECTION
421+55.89	-	421+55.89	MEEHL ST. LT	168	BACK TO RADIUS OF INTERSECTION
423+87.13	-	423+87.13	CLIFTON RD. RT	290	BACK TO RADIUS OF INTERSECTION
59+27.37	-	61+75	COLLECTOR	770	COLLECTOR ROAD
61+75	-	60+46.80	CTH M	400	CTH M
437+55.09	-	437+55.09	CTH B LT	245	BACK TO RADIUS OF INTERSECTION
450+37.43	-	450+37.43	FOREST AVE RT	211	BACK TO RADIUS OF INTERSECTION
447+37.43	-	450+37.43	STH 114	785	FOREST AVENUE BYPASS LANE
447+68.6	-	450+85.40	STH 114	1,021	URBAN AREA TO END OF CURB AND GUTTER ON WEST SIDE
450+85.40	-	1008+05.12	STH 114	7,090	CURB AND GUTTER ON WEST TO END OF PROJECT
1001+91.11	-	1001+91.11	HIGH CLIFF ROAD RT	575	BACK TO RADIUS OF INTERSECTION
1000+05.12	-	1008+05.12	STH 114	984	STH 55 BYPASS LANE

**TOTAL      76,960**

**SHOULDER WORK**

STATION	TO	STATION	DIR	LOCATION	305. 0110 BASE AGGREGATE DENSE 3/4- INCH TON	305. 0500 SHAPING SHOULDERS STA
CATEGORY 0010						
203+75	-	208+60. 48	LT & RT	STH 114	75	10
232+68. 87	-	321+96. 54	LT & RT	STH 114	1, 390	178
367+75	-	396+15. 45	LT & RT	STH 114	440	57
396+15. 45	-	416+99. 19	LT	STH 114	170	21
447+68. 6	-	450+85. 40	LT	STH 114	25	3
450+85. 40	-	1008+05. 12	LT & RT	STH 114	350	43

**TOTAL      2, 450      312**

**WATER**

STATION	TO	STATION	LOCATION	624. 0100 WATER MGAL
CATEGORY 0010				
UNDISTRIBUTED			STH 114	2

**TOTAL      2**

**ADJUSTING WATER VALVE BOXES**

STATION	TO	STATION	SPV. 0060 EACH
CATEGORY 0020			
UNDISTRIBUTED			1

**TOTAL      1**

**FIELD OFFICE TYPE C**

STATION	TO	STATION	LOCATION	642. 5201 EACH
CATEGORY 0010				
UNDISTRIBUTED			STH 114	1

**TOTAL      1**

**ADJUSTING MANHOLES COVERS**

STATION	LOCATION	611. 8110 ADJUSTING MANHOLE COVERS EACH	REMARKS
CATEGORY 0010			
399+58. 26	STH 114	1	MANHOLE LOCATED IN NO PARKING LANE
406+20. 41	STH 114	1	MANHOLE LOCATED IN NO PARKING LANE
410+21. 15	STH 114	1	MANHOLE LOCATED IN NO PARKING LANE
410+75. 17	STH 114	1	MANHOLE LOCATED IN NO PARKING LANE
415+81. 30	STH 114	1	MANHOLE LOCATED IN NO PARKING LANE
417+37. 65	STH 114	1	MANHOLE LOCATED IN NO PARKING LANE
424+68. 32	STH 114	1	MAINLINE MANHOLE- STORM
427+68. 63	STH 114	1	MAINLINE MANHOLE - STORM
432+56. 84	STH 114	1	MAINLINE MANHOLE - STORM
434+48. 74	STH 114	1	MAINLINE MANHOLE - STORM
440+30. 83	STH 114	1	MANHOLE LOCATED IN NO PARKING LANE
443+98. 05	STH 114	1	MANHOLE LOCATED IN NO PARKING LANE

**TOTAL      12**

STATION	LOCATION	611. 8110 ADJUSTING MANHOLE COVERS EACH	REMARKS
CATEGORY 0020			
60+12. 80	COLLECTOR RD	1	MAINLINE MANHOLE- SANITARY
61+31. 50	CTH M	1	MAINLINE MANHOLE- SANITARY

**TOTAL      2**

**LOCATING NO-PASSING ZONES**

STATION	TO	STATION	LOCATION	648. 0100 LOCATING NO-PASSING ZONES MI
CATEGORY 0010				
203+75. 00	-	208+60. 48	STH 114	0. 18
232+68. 87	-	321+96. 54	STH 114	3. 38
367+75. 00	-	1008+05. 12	STH 114	3. 95

**TOTAL      7. 52**



**ASPHALTIC MATERIALS**

STATION	TO	STATION	LOCATION	455. 0105 ASPHALTIC MATERIAL PG58-28 AT 5.5% TON	455. 0605 TACK COAT GAL	460. 1103 HMA PAVEMENT TYPE E-3 TON	460. 4110. S* REHEATING HMA PAVEMENT LONGITUDINAL JOINTS LF	465. 0110 ASPHALTIC SURFACE PATCHING TON	465. 0475** ASPHALT CENTER LINE RUMBLE STRIP 2-LANE RURAL LF	REMARKS
CATEGORY 0010										
203+75	-	208+60.48	STH 114	10	40	186	486	--	85	BEGINNING OF PROJECT TO CTHN/STH114 RAB
204+74.01	-	204+74.01	RT	1	5	24	--	--	--	INTERSECTION (OLD HWY RD)
232+68.87	-	321+96.54	STH 114	190	744	3,422	8,928	--	7,728	EAST OF CTH N/STH 114 RAB TO STH 55/114 RAB
251+25	-	251+25	RT	2	8	36	--	--	--	INTERSECTION (FIRELANE 13)
278+30.95	-	278+30.95	RT & LT	4	17	77	--	--	--	INTERSECTION (STATE PARK ROAD RT/LT)
278+30.95	-	278+30.95	RT & LT	6	25	116	--	--	--	STATE PARK LANE TURN LANES
305+49.09	-	305+49.09	RT	2	9	43	--	--	--	INTERSECTION (PIGEON RD)
305+49.09	-	305+49.09	RT	3	13	58	--	--	--	PIGEON RD. TURN LANE
305+49.09	-	305+49.09	LT	5	20	90	--	--	--	PIGEON RD. PASSING LANE
367+75	-	396+15.45	STH 114	60	238	1,088	2,840	--	2,040	STH 55/114 RAB TO CASTLE DRIVE
370+59.76	-	370+59.76	RT	2	8	36	--	--	--	INTERSECTION (LAKE BREEZE RD)
370+59.76	-	370+59.76	RT	3	14	63	--	--	--	LAKE BREEZE ROAD TURN LANE
370+59.76	-	370+59.76	LT	5	20	90	--	--	--	LAKE BREEZE ROAD PASSING LANE
396+15.45	-	396+15.45	RT	3	10	47	--	--	--	INTERSECTION (CASTLE DR)
396+15.45	-	416+99.19	STH 114	42	168	772	2,084	--	2,084	CASTLE DRIVE TO STUMPF DRIVE
396+15.45	-	396+15.45	LT	5	20	90	--	--	--	CASTLE DRIVE PASSING LANE
399+49.12	-	399+67.12	NO PARKING LANE	--	--	--	--	2	--	NO PARKING LANE
406+20.36	-	406+31.36	NO PARKING LANE	--	--	--	--	1	--	NO PARKING LANE
410+22.50	-	410+29	NO PARKING LANE	--	--	--	--	1	--	NO PARKING LANE
410+73.34	-	410+82.34	NO PARKING LANE	--	--	--	--	1	--	NO PARKING LANE
415+78.41	-	415+89.41	NO PARKING LANE	--	--	--	--	1	--	NO PARKING LANE
416+99.19	-	416+99.19	LT	2	7	32	--	--	--	INTERSECTION (STUMPF DR)
417+37.65	-	417+51.65	NO PARKING LANE	--	--	--	--	1	--	NO PARKING LANE
416+99.19	-	447+68.6	STH 114	60	240	1,098	3,070	--	--	URBAN AREA
420+00	-	420+00	RT	2	9	40	--	--	--	INTERSECTION (HARRISON ST)
421+55.89	-	421+55.89	LT	2	4	19	--	--	--	INTERSECTION (MEEHL ST)
423+87.13	-	423+87.13	RT	2	10	33	--	--	--	INTERSECTION (CLIFTON RD)
59+27.37	-	61+75	COLLECTOR	5	20	89	248	--	--	COLLECTOR ROAD
61+75	-	60+46.80	CTH M	3	10	46	128	--	--	CTH M
437+55.09	-	437+55.09	LT	2	6	28	--	--	--	INTERSECTION (CTH B)
440+30.83	-	440+42.83	NO PARKING LANE	--	--	--	--	1	--	NO PARKING LANE
443+97.44	-	444+07.44	NO PARKING LANE	--	--	--	--	1	--	NO PARKING LANE
447+68.6	-	450+85.40	STH 114	6	26	117	318	--	316	URBAN AREA TO END OF CURB AND GUTTER ON WEST SIDE
450+37.43	-	450+37.43	RT	2	5	24	--	--	--	INTERSECTION (FOREST AVE)
450+37.43	-	450+37.43	LT	5	20	90	--	--	--	FOREST AVE. PASSING LANE
450+85.40	-	1008+05.12	STH 114	45	177	815	2,128	--	1,727	CURB AND GUTTER ON WEST TO END OF PROJECT
1000+05.12	-	1008+05.12	STH 114	6	25	113	--	--	--	STH 114 BYPASS LANE FOR STH 55
1001+91.11	-	1001+91.11	RT	4	14	66	--	--	--	Intersection
UNDISTRIBUTED				STH 114	--	--	--	100	--	STH 114

**TOTAL**

**490**

**1,930**

**8,850**

**20,230**

**110**

**13,980**

NOTES:

\* PAYMENT FOR REHEATING HMA PAVEMENT LONGITUDINAL JOINTS IS FOR THE CENTERLINE JOINT ONLY

\*\* CALCULATION OF QUANTITY TAKES INTO ACCOUNT THE GAPS AT INTERSECTIONS PER THE STANDARD DETAILS

PROJECT NO: 4670-11-60

HWY: STH 114

COUNTY: CALUMET

MISCELLANEOUS QUANTITIES

SHEET:

**E**

FILE NAME :

PLOT DATE :

PLOT BY : DUMS, ALEXANDER T

PLOT NAME :

PLOT SCALE : 1:1

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**LANDSCAPING SUMMARY**

STATION	TO	STATION	LOCATION	625. 0100 TOPSOIL SY	627. 0200 MULCHING SY	629. 0210 FERTILIZER TYPE B CWT	630. 0140 SEEDING MIXTURE NO. 40 LB	REMARKS
CATEGORY 0010								
399+49. 12	-	399+67. 12	STH 114	8	8	0. 16	0. 16	REPAIR FOR CURB AND GUTTER
406+20. 36	-	406+31. 36	STH 114	5	5	0. 12	0. 12	REPAIR FOR CURB AND GUTTER
410+22. 50	-	410+29. 5	STH 114	4	4	0. 08	0. 08	REPAIR FOR CURB AND GUTTER
410+73. 34	-	410+82. 34	STH 114	4	4	0. 12	0. 12	REPAIR FOR CURB AND GUTTER
415+78. 41	-	415+89. 41	STH 114	5	5	0. 14	0. 14	REPAIR FOR CURB AND GUTTER
417+37. 65	-	417+51. 65	STH 114	6	6	0. 12	0. 12	REPAIR FOR CURB AND GUTTER
440+30. 83	-	440+42. 83	STH 114	5	5	0. 14	0. 14	REPAIR FOR CURB AND GUTTER
443+97. 44	-	444+07. 44	STH 114	4	4	0. 12	0. 12	REPAIR FOR CURB AND GUTTER

**TOTAL            41                    41                    1                    1**

**EROSION CONTROL**

STATION	TO	STATION	LOCATION	628. 1504 SILT FENCE LF	628. 1520 SILT FENCE MAINTENANCE LF	628. 1905 MOBILIZATIONS EROSION CONTROL EACH	628. 1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	628. 7010 INLET PROTECTION TYPE B EACH
CATEGORY 0010								
399+39. 12	-	399+77. 12	STH 114	40	40	--	--	1
406+00. 36	-	406+41. 36	STH 114	40	40	--	--	1
410+12. 50	-	410+39	STH 114	30	30	--	--	1
410+53. 34	-	410+92. 34	STH 114	40	40	--	--	1
415+58. 41	-	415+99. 41	STH 114	40	40	--	--	1
417+17. 65	-	417+71. 65	STH 114	60	60	--	--	1
440+00. 83	-	440+62. 83	STH 114	60	60	--	--	1
443+77. 44	-	444+27. 44	STH 114	50	50	1	1	1

**TOTAL            360                    360                    1                    1                    8**

**TRAFFIC CONTROL**

LOCATION/STAGE	643. 0300	643. 0705	APPROX.	643. 0900		APPROX.	643. 1050		REMARKS
	TRAFFIC CONTROL DRUMS DAYS	TRAFFIC CONTROL WARNING LIGHTS TYPE A DAYS	SERVICE PERIOD DAYS	NO	SIGN DAYS	SERVICE PERIOD DAYS	NO	PCMS DAYS	
<b>CATEGORY 0010</b>									
<b>ADVANCE WARNING</b>									
PROJECT LIMITS, NB	--	--	30	5	150	28	1	28	ADVANCE WARNING
PROJECT LIMITS, SB	--	--	30	5	150	28	1	28	ADVANCE WARNING
<b>SIDE ROADS</b>									
CTH N	--	--	30	4	120	--	--	--	ADVANCE WARNING
STH 55	--	--	30	4	120	--	--	--	ADVANCE WARNING
CTH M	--	--	30	2	60	--	--	--	ADVANCE WARNING
CTH B	--	--	30	2	60	--	--	--	ADVANCE WARNING
<b>SIDE ROADS</b>									
OLD HWY ROAD	--	--	30	1	30	--	--	--	ADVANCE WARNING
FIRELANE 13	--	--	30	1	30	--	--	--	ADVANCE WARNING
STATE PARK ROAD	--	--	30	2	60	--	--	--	ADVANCE WARNING
PIGEON RAOD	--	--	30	1	30	--	--	--	ADVANCE WARNING
LAKE BREEZE DRIVE	--	--	30	1	30	--	--	--	ADVANCE WARNING
CASTLE DRIVE	--	--	30	1	30	--	--	--	ADVANCE WARNING
STUMPF DRIVE	--	--	30	1	30	--	--	--	ADVANCE WARNING
HARRISON STREET	--	--	30	1	30	--	--	--	ADVANCE WARNING
MEEHL STREET	--	--	30	1	30	--	--	--	ADVANCE WARNING
CLIFTON RAOD	--	--	30	1	30	--	--	--	ADVANCE WARNING
Forest AVENUE	--	--	30	1	30	--	--	--	ADVANCE WARNING
HIGH CLIFF ROAD	--	--	30	2	60	--	--	--	ADVANCE WARNING
<b>Asphaltic Repairs (No Parking Lane)</b>									
399+49. 12	45	45	3	15	--	--	--	--	ADVANCE WARNING
406+20. 36	45	45	3	15	--	--	--	--	ADVANCE WARNING
410+22. 50	45	45	3	15	--	--	--	--	ADVANCE WARNING
410+73. 34	45	45	3	15	--	--	--	--	ADVANCE WARNING
415+78. 41	45	45	3	15	--	--	--	--	ADVANCE WARNING
417+37. 65	45	45	3	15	--	--	--	--	ADVANCE WARNING
440+30. 83	45	45	3	15	--	--	--	--	ADVANCE WARNING
443+97. 44	45	45	3	15	--	--	--	--	ADVANCE WARNING
<b>TOTALS</b>	<b>360</b>	<b>360</b>			<b>1, 080</b>			<b>56</b>	

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**PAVEMENT MARKING**

STATION	TO	STATION	LOCATION	646. 0406** PAVEMENT MARKING SAME DAY EPOXY 4-INCH (YELLOW) LF	649. 0200 TEMPORARY PAVEMENT MARKING REFLECTIVE PAINT 4-INCH (YELLOW) LF	SPV. 0090. 01 PAVEMENT MARKING GROOVED WET REFLECTIVE EPOXY 4-INCH (WHITE) LF	SPV. 0090. 02 PAVEMENT MARKING GROOVED WET REFLECTIVE EPOXY 8-INCH (WHITE) LF	REMARKS
CATEGORY 0010								
203+75	-	208+60. 48	STH 114	1, 942	975	971	--	BEGINNING OF PROJECT TO RAB CTH N/STH 114
232+68. 87	-	321+96. 54	STH 114	8, 380	2, 702	17, 855	--	RAB CTH N/STH 114 TO RAB STH 55/114
276+20. 95	-	277+80. 95	STH 114	--	--	--	160	STATE PARK RD RT TURN LANE EAST BOUND (CHANNELIZING LANE LINE )
278+80. 95	-	280+40. 95	STH 114	--	--	--	160	STATE PARK RD RT TURN LANE WEST BOUND (CHANNELIZING LANE LINE)
302+99. 09	-	304+99. 09	STH 114	--	--	--	200	PIGEON RD. RT TURN LANE (CHANNELIZING LANE LINE )
304+49. 09	-	306+99. 09	STH 114	--	--	63	--	PIGEON RD. BYPASS LANE
367+75	-	416+99. 19	STH 114	9, 882	4, 120	9, 648	--	RAB STH 55/114 TO URBAN AREA
368+09. 76	-	370+09. 76	STH 114	--	--	--	200	LAKE BREEZE DR TURN LANE (CHANNELIZING LANE LINE)
369+59. 76	-	372+09. 76	STH 114	--	--	63	--	LAKE BREEZE DR BYPASS LANE
394+05. 45	-	395+65. 45	STH 114	--	--	--	160	CASTLE DR. RT TURN LANE (CHANNELIZING LANE LINE)
416+99. 19	-	447+68. 6	STH 114	6, 750	2, 995	6, 140	--	URBAN AREA
59+27. 37	-	61+75	COLLECTOR	995	495	496	--	URBAN AREA
61+75	-	60+46. 80	CTH M	9, 140	4, 568	4, 570	--	URBAN AREA
1003+38. 12	-	1005+88. 12	STH 114	--	--	65	--	STH 114 BYPASS LANE
447+68. 6	-	1008+05. 12	STH 114	9, 582	4, 765	4, 594	--	END OF URBAN TO END OF PROJECT

**TOTAL                    46, 670                    20, 620                    44, 465                    880**

\*\*NOTE: SAME DAY EPOXY 4-INCH YELLOW QUANTITY IS FOR 2 LAYERS OF EPOXY. SEE SPECIALS FOR APPLICATION RATES.

**PAVEMENT MARKING (CONTINUED)**

STATION	LOCATION	647. 0566 PAVEMENT MARKING STOP LINE EPOXY 18-INCH LF	647. 0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF
<b>CATEGORY 0010</b>			
420+00	HARRISON STREET	18	--
421+55. 89	MEEHL STREET	24	--
84+59. 54	CLIFTON ROAD	16	--
60+46. 80	CTH M	20	--
437+55. 09	CTH B	27	--
416+99. 19	STH 114	--	90
	STUMPF AVENUE	--	105
417+23	STH 114	--	90
	HARRISON STREET	--	114
421+12	STH 114	--	88
	MEEHL STREET	--	105
423+05	STH 114	--	90
	CLIFTON ROAD	--	126
60+74	CTHM	--	106
424+33	STH 114	--	81
2+53	COLLECTOR	--	40
437+79	CTH B	--	125
437+85	STH 114	--	90
450+37. 43	FOREST AVENUE	--	100

**TOTAL            105            1, 350**

**MILLING AND REMOVING TEMPORARY JOINT**

STATION	TO	STATION	SPV. 0090. 03 MILLING AND REMOVING TEMPORARY JOINT LF	REMARKS
<b>CATEGORY 0010</b>				
203+75	-	208+60. 48	485	STH 114
232+68. 87	-	321+96. 54	8, 928	STH 114
367+75	-	416+99. 19	4, 925	STH 114
416+99. 19	-	447+68. 6	3, 070	STH 114
59+27. 37	-	61+75	248	COLLECTOR
60+46. 80	-	61+75	130	CTH M
447+68. 6	-	1008+05. 12	2, 444	STH 114

**TOTAL            20, 230**

**CONSTRUCTION STAKING**

STATION	TO	STATION	LOCATION	650. 8000 RESURFACING REFERENCE LF	650. 9910 SUPPLEMENTAL CONTROL LS
CATEGORY 0010					
203+75	-	208+60. 48	STH 114	485	--
232+68. 87	-	321+96. 54	STH 114	8928	--
367+75	-	396+15. 45	STH 114	2840	--
396+15. 45	-	416+99. 19	STH 114	2084	--
416+99. 19	-	447+68. 6	STH 114	3069	--
59+27. 37	-	61+75	COLLECTOR	248	--
61+75	-	60+46. 80	CTH M	130	--
447+68. 6	-	450+85. 40	STH 114	318	--
450+85. 40	-	1008+05. 12	STH 114	2128	--
Project			STH 114	--	1

**TOTAL      20, 230      1**

**SAWING CONCRETE**

STATION	LOCATION	690. 0250 LF	REMARKS
CATEGORY 0010			
406+20.36	STH 114	5	CURB AND GUTTER REPLACEMENT
410+22.50	STH 114	5	CURB AND GUTTER REPLACEMENT
417+37.65	STH 114	5	CURB AND GUTTER REPLACEMENT
440+30.83	STH 114	5	CURB AND GUTTER REPLACEMENT
443+97.44	STH 114	5	CURB AND GUTTER REPLACEMENT

**TOTAL      25**

**SAWING ASPHALT**

STATION	LOCATION	690. 0150 SAWING ASPHALT LF	REMARKS
CATEGORY 0010			
203+75	STH 114	30	BEGINNING OF PROJECT
204+74. 01	OLD HWY ROAD	24	MATCH TO EXISTING JOINT
208+60. 48	STH 114	30	MATCH TO EXISTING JOINT
232+68. 87	STH 114	30	MATCH TO EXISTING JOINT
251+25	FIRELANE 13	24	MATCH TO EXISTING JOINT
278+30. 95	STATE PARK ROAD	24	MATCH TO EXISTING JOINT
305+49. 09	PIGEON ROAD	24	MATCH TO EXISTING JOINT
321+96. 54	STH 114	30	MATCH TO EXISTING JOINT
367+75	STH 114	30	MATCH TO EXISTING JOINT
370+59. 76	LAKE BREEZE DR.	24	MATCH TO EXISTING JOINT
396+15. 45	CASTLE DR.	24	MATCH TO EXISTING JOINT
399+49. 12	NO PARKING LANE	52	REPAIR OF MANHOLE
406+20. 36	NO PARKING LANE	40	REPAIR OF MANHOLE
410+22. 50	NO PARKING LANE	29	REPAIR OF MANHOLE
410+73. 34	NO PARKING LANE	34	REPAIR OF MANHOLE
415+78. 41	NO PARKING LANE	38	REPAIR OF MANHOLE
416+99. 19	STUMPF DR.	24	MATCH TO EXISTING JOINT
417+37. 65	NO PARKING LANE	44	REPAIR OF MANHOLE
420+00	HARRISON ST.	24	MATCH TO EXISTING JOINT
421+55. 89	MEEHL ST.	24	MATCH TO EXISTING JOINT
423+87. 13	CLIFTON ROAD	24	MATCH TO EXISTING JOINT
437+55. 09	CTH B	24	MATCH TO EXISTING JOINT
440+30. 83	NO PARKING LANE	40	REPAIR OF MANHOLE
443+97. 44	NO PARKING LANE	36	REPAIR OF MANHOLE
450+37. 43	FOREST AVENUE	24	MATCH TO EXISTING JOINT
1001+91. 11	HIGH CLIFF ROAD	24	MATCH TO EXISTING JOINT
1008+05. 12	STH 114	30	END OF PROJECT

**TOTAL      805**

ERECTION & REMOVAL OF PERMANENT SIGNING, TYPE II

SIGN NO.	LOCATION	SIGN CODE	W X H	637. 2210 SIGNS TYPE II REFLECTIVE TYPE H S. F.	637. 2230 SIGNS TYPE II REFLECTIVE TYPE F S. F.	634. 0612 POSTS WOOD 4x6x12 EACH	634. 0614 POSTS WOOD 4x6x14 EACH	634. 0616 POSTS WOOD 4x6x16 EACH	638. 2602 REMOVI NG SIGNS TYPE II EACH	638. 3000 REMOVI NG SMALL SIGN SUPPORTS EACH	REMARKS
1	STH 114, E. OF USH 10	I55-56	30" X 36"	7.50			1		1	1	VFW-NICOLET POST 2126 MENASHA
2	"	J3-2	48" X 57"	19.00				1	6	1	WEST USH 10, EAST USH 10, SEE PLAN SHEET
3	OLD HWY RD	R1-1	30" X 30"	5.18			1		1	1	
4	STH 114, E. OF OLD HWY RD	J4-1	24" X 36"	6.00			1		2	1	EAST STH 114
5	"	J2-3	72" X 57"	28.50				2	9	1	WEST USH 10, WEST STH 114, EAST USH 10, SEE PLAN SHEET
6	"	W6-1	36" X 36"		9.00		1		1	1	
7	STH 114, E. OF CTH N	D2-2	78" X 24"	13.00		2			1	2	SEE SIGN DETAIL SHEET
8	"	J1-1	24" X 39"	6.50			1		2	1	JCT CTH N
9	"	W14-3	48" X 36"		6.00		1		1	1	
10	FIRELANE 13	R1-1	30" X 30"	5.18			1		1	1	
11	STH 114, E. OF FIRELANE 13	D7-52	54" X 42"	15.75			1		1	1	SEE SIGN DETAIL SHEET
12	STH 114, W. OF STATE PARK RD	R2-1	24" X 30"	5.00			1		1	1	55 MPH
13	"	J4-1	24" X 36"	6.00			1		1	1	WEST STH 114
14	STATE PARK RD	R1-1	30" X 30"	5.18			1		1	1	
15	"	R1-1	30" X 30"	5.18			1		1	1	
16	"	W4-4P	24" X 12"		2.00				1		MOUNT BELOW SIGN #15
17	"	R1-1	30" X 30"	5.18			1		1	1	
18	"	W4-4P	24" X 12"		2.00				1		MOUNT BELOW SIGN #17
19	STH 114, E. OF STATE PARK RD	J4-1	24" X 36"	6.00			1		2	1	EAST STH 114
20	"	R2-1	24" X 30"	5.00			1		1	1	55 MPH
21	STH 114, W. OF PIGEON RD	D7-53R	54" X 36"	13.50			1		1	1	HIGH CLIFF STATE PARK
22	STH 114 AT PIGEON RD	W1-7	48" X 24"		8.00	1			1	1	
23	PIGEON RD	R1-1	30" X 30"	5.18			1		1	1	
24	STH 114, E. OF PIGEON RD	J13-1	24" X 45"	7.50			1		1	1	SEE PLAN SHEET
25	"	D7-53L	54" X 36"	13.50			1		1	1	HIGH CLIFF STATE PARK
26	"	I55-56							1	1	
27	"	I55-56	30" X 36"	7.50			1		1	1	VFW-NICOLET POST 2126 MENASHA
28	"	W14-3	48" X 36"		6.00		1		1	1	
29	"	W1-2R	30" X 30"		6.25		1		1	1	
30	"	W13-1							1		
31	"	W14-3	48" X 36"		6.00				1		MOUNT ON BACK OF SIGN #29
32	LAKE BREEZE DR	R1-1	30" X 30"	5.18			1		1	1	
33	STH 114/STH 55, E. OF LAKE BREEZE DR	W10-1	36" X 36"		7.07		1		1	1	
34	"	W14-3	48" X 36"		6.00		1		1	1	
35	"	W14-3	48" X 36"		6.00		1		1	1	
36	"	W3-5	36" X 36"		9.00		1		1	1	45 MPH
37	"	R2-1	24" X 30"	5.00			1		1	1	45 MPH
38	"	R2-1	24" X 30"	5.00			1		1	1	55 MPH

PAGE SUBTOTALS

206.51 73.32 3 29 3 54 35

PLAN SHEET PRODUCED  
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ERECTION & REMOVAL OF PERMANENT SIGNING, TYPE II

SIGN NO.	LOCATION	SIGN CODE	W X H	637. 2210 SIGNS TYPE II REFLECTIVE TYPE H S. F.	637. 2230 SIGNS TYPE II REFLECTIVE TYPE F S. F.	634. 0612 POSTS WOOD 4x6x12 EACH	634. 0614 POSTS WOOD 4x6x14 EACH	634. 0616 POSTS WOOD 4x6x16 EACH	638. 2602 REMOVI NG SIGNS TYPE II EACH	638. 3000 REMOVI NG SMALL SIGN SUPPORTS EACH	REMARKS
39	CASTLE RD	R1-1	30" X 30"	5.18			1		1	1	
40	STH 114/STH 55, E. OF CASTLE RD	R7-1L	24" X 30"	5.00			1		1	1	
41	"	R7-1D	24" X 30"	5.00			1		2	1	REINSTALL VILLAGE PARKING SIGN
42	"	R2-1	24" X 30"	5.00				1	1	1	45 MPH
43	"	R7-1D	18" X 24"	3.00					1		MOUNT BELOW SIGN #42
44	"	R2-1	24" X 30"	5.00			1		1	1	45 MPH
45	"	R7-1D	24" X 30"	5.00			1		1	1	
46	"	W14-3	48" X 36"		6.00		1		1	1	
47	"	W3-5	36" X 36"		9.00		1		1	1	30 MPH
48	"	I55-56							1	1	
49	"	W8-8							1		
50	"	R7-1D	24" X 30"	5.00			1		1	1	
51	"	R2-1	24" X 30"	5.00				1	1	1	30 MPH
52	"	R7-1D	18" X 24"	3.00					1		MOUNT BELOW SIGN #51
53	"	W14-3	48" X 36"		6.00		1		1	1	
54	"	R2-1	24" X 30"	5.00			1		1	1	45 MPH
55	"	J1-1	24" X 39"	6.50				1	2	1	JCT CTH M
56	"	R7-1D	18" X 24"	3.00					1		MOUNT BELOW SIGN #55
57	STUMPF RD	R1-1	30" X 30"	5.18			1		1	1	
58	STH 114/STH 55, E. OF STUMPF RD	R2-1	24" X 30"	5.00				1	1	1	30 MPH
59	"	R7-1D	18" X 24"	3.00					1		MOUNT BELOW SIGN #58
60	"	W1-1R	36" X 36"		9.00			1	1	1	
61	"	W13-1	24" X 24"		4.00				1		25 MPH, MOUNT BELOW SIGN #60
62	HARRISON ST	R1-1	30" X 30"	5.18			1		1	1	
63	STH 114/STH 55, E. OF HARRISON ST	W11-8	30" X 30"		6.25		1		1	1	
64	"	J4-2	48" X 36"	12.00			1		2	1	NORTH STH 55, WEST STH 114
65	MEEHL ST	R1-1	30" X 30"	5.18			1		1	1	
66	STH 114/STH 55, E. OF MEEHL ST	R2-1	24" X 30"	5.00				1	1	1	30 MPH
67	"	R7-1R	18" X 24"	3.00					1		MOUNT BELOW SIGN #66
68	"	J13-1	24" X 45"	7.50				1	2	1	CTH M, SEE PLAN SHEET
69	"	R7-1D	24" X 30"	5.00			1		1	1	
70	CLIFTON RD	R1-1	30" X 30"	5.18			1		1	1	
71	STH 114/STH 55, E. OF CTH M	R7-1L	18" X 24"	3.00			1		1	1	
72	CTH M SPLITTER ISLAND	R1-2	36" X 31"	3.88			1		1	1	
73	"	J3-2	48" X 57"	19.00				1	6	1	SOUTH STH 55, EAST STH 114
74	CTH M	R1-1	36" X 36"	7.46			1		1	1	
75	CTH M SPLITTER ISLAND	R1-1	36" X 36"	7.46			1		1	1	
76	CTH M	R1-2	36" X 31"	3.88			1		1	1	
77	CTH M SPLITTER ISLAND	R5-1	30" X 30"	6.25					1		MOUNT ON BACKSIDE OF SIGN #72

PAGE SUBTOTALS

172.83 40.25 0 22 8 48 31

PLAN SHEET PRODUCED  
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**ERECTION & REMOVAL OF PERMANENT SIGNING, TYPE II**

SIGN NO.	LOCATION	SIGN CODE	W X H	637. 2210 SIGNS TYPE II REFLECTIVE TYPE H S. F.	637. 2230 SIGNS TYPE II REFLECTIVE TYPE F S. F.	634. 0612 POSTS WOOD 4x6x12 EACH	634. 0614 POSTS WOOD 4x6x14 EACH	634. 0616 POSTS WOOD 4x6x16 EACH	638. 2602 REMOVING SIGNS TYPE II EACH	638. 3000 REMOVING SMALL SIGN SUPPORTS EACH	REMARKS	
78	CTH M SPLITTER ISLAND	J13-2	48" X 45"	15.00				1	4	1	STH 55, STH 114, SEE PLAN SHEET	
78A	STH 114/STH 55, AT CTH M	J13-1	24" X 45"	7.50				1	2	1	CTH M, SEE PLAN SHEET	
79	STH 114/STH 55, E. OF CTH M	S1-1	36" X 36"		6.75			1	1	1		
80	"	S16-9P	30" X 18"		3.75						MOUNT BELOW SIGN #79	
81	"	R2-6P	24" X 18"	3.00							MOUNT BELOW SIGN #80	
82	"	R7-1R								1		
83	"	J4-1	24" X 36"	6.00				1	1	1	SOUTH STH 55	
84	"	J4-1	24" X 36"	6.00					1		EAST STH 114, MOUNT BELOW SIGN #83	
85	"	R2-1	24" X 30"	5.00				1			30 MPH	
86	"	S5-2	24" X 30"	5.00							MOUNT BELOW SIGN #85	
87	"	S4-51	24" X 48"		8.00			1	1	1	15 MPH	
88	"	W1-1L	36" X 36"		9.00			1	1	1		
89	"	W13-1	24" X 24"		4.00				1		25 MPH, MOUNT BELOW SIGN #88	
90	"	S1-1	---						1	1		
91	VACANT		---									
92	STH 114/STH 55, E. OF CTH M	W14-3	48" X 36"		6.00				1	1	MOUNT ON BACK OF SIGN #90	
93	"	S1-1	---						1	1		
94	VACANT	---	---									
95	STH 114/STH 55, W. OF CTH B	R2-1	24" X 30"	5.00			1		1	1	30 MPH	
96	"	S5-2	24" X 30"	5.00							MOUNT BELOW SIGN #95	
97	"	J1-1	24" X 39"	6.50			1		2	1	JCT CTH M	
98	"	J1-1	24" X 39"	6.50			1		2	1	JCT CTH B	
99	"	S4-51	24" X 48"		8.00			1	1	1	15 MPH	
100	"	D1-1	42" X 12"	3.50			1		1	1	SEE SIGN DETAIL SHEET	
101	"	M1-6							1			
102	"	M1-6							1			
103	"	R2-1	24" X 30"	5.00			1		1	1	30 MPH	
104	"	J13-1	24" X 45"	7.50					1	2	1	CTH B, SEE PLAN SHEET
105	"	R7-1D	18" X 24"	3.00					1		MOUNT BELOW SIGN #104	
106	STH 114/STH 55, AT CTH B	M1-6							1			
107	"	M1-6							1			
108	"	W1-7	48" X 24"		8.00		1		1	1		
109	CTH B	R1-1	36" X 36"	7.46			1		1	1		
109A	"	R1-1	30" X 30"	5.18			1		1	1		
110	STH 114/STH 55, E. OF CTH B	J13-1	24" X 45"	7.50				1	2	1	CTH B, SEE PLAN SHEET	
111	"	J4-2	48" X 36"	12.00				1	2	1	SOUTH STH 55, EAST STH 114	
112	"	R7-1D	18" X 24"	3.00					1		MOUNT BELOW SIGN #111	
113	"	S1-1	36" X 36"		6.75			1	1	1		
114	"	S16-9P	30" X 18"		3.75						MOUNT BELOW SIGN #113	
115	"	R2-6P	24" X 18"	3.00							MOUNT BELOW SIGN #114	

PAGE SUBTOTALS

127.64      64.00      0      8      12      39      23

PLAN SHEET PRODUCED  
BY WisDOT - NE REGION

**ERECTION & REMOVAL OF PERMANENT SIGNING, TYPE II**

SIGN NO.	LOCATION	SIGN CODE	W X H	637. 2210 SIGNS TYPE II REFLECTIVE TYPE H S. F.	637. 2230 SIGNS TYPE II REFLECTIVE TYPE F S. F.	634. 0612 POSTS WOOD 4x6x12 EACH	634. 0614 POSTS WOOD 4x6x14 EACH	634. 0616 POSTS WOOD 4x6x16 EACH	638. 2602 REMOVI NG SIGNS TYPE II EACH	638. 3000 REMOVI NG SMALL SIGN SUPPORTS EACH	REMARKS
116	STH 114/STH 55, E. OF CTH B	W14-3	48" X 36"		6.00		1		1	1	
116A	"	R2-1	24" X 30"	5.00				1	1	1	30 MPH
117	"	R7-1L	24" X 30"	5.00					1		
118	"	R2-1	24" X 30"	5.00				1	1	1	45 MPH
119	"	R7-1D	18" X 24"	3.00					1		MOUNT BELOW SIGN #118
120	"	D1-1	66" X 15"	6.88		2			1	2	SEE SIGN DETAIL SHEET
121	"	R7-1D	18" X 24"	3.00					1		MOUNT BELOW SIGN #120 ON LEFT POST
122	"	R7-1D	24" X 30"	5.00		1			1	1	
123	STH 114/STH 55, W. OF FOREST AVE	J1-1	24" X 39"	6.50				1	2	1	JCT CTH B
124	"	R7-1D	18" X 24"	3.00					1		MOUNT BELOW SIGN #123
125	"	R7-1D	24" X 30"	5.00			1		1	1	
126	"	W3-5	36" X 36"		9.00			1	1	1	30 MPH
127	"	R7-1L	24" X 30"	5.00					1		MOUNT BELOW SIGN #126
128	"	R7-1R	24" X 30"	5.00			1		1	1	
129	"	I2-3	60" X 24"	10.00			2		1	2	SHERWOOD, SEE SIGN DETAIL SHEET
130	FOREST AVE	R1-1	30" X 30"	5.18			1		1	1	
131	STH 114/STH 55, E. OF FOREST AVE	R2-1	24" X 30"	5.00			1		1	1	55 MPH
132	"	R2-1	24" X 30"	5.00			1		1	1	45 MPH
133	STH 114/STH 55, W. OF HIGH CLIFF RD	J2-2	72" X 84"	42.00				2	6	2	EAST STH 114, SOUTH STH 55, SEE PLAN SHEET
134	"	I55-56							1	1	
135	"	J4-2	48" X 36"	12.00			1		4	1	NORTH STH 55, WEST STH 114
136	"	D1-3	84" X 36"	21.00			2		1	2	SEE SIGN DETAIL SHEET
137	"	W3-5	36" X 36"		9.00		1		1	1	45 MPH
138	"	D2-3	72" X 36"	18.00			2		1	2	SEE SIGN DETAIL SHEET
139	HIGH CLIFF RD	W5-52R	12" X 36"		3.00				1		MOUNT TO SIDE OF SIGN #140 FACING STH 114 EB TRAFFIC
140	"	R1-1	30" X 30"	5.18			1		1	1	
141	"	R4-7	24" X 30"	5.00			1		1	1	
142	"	R1-1	30" X 30"	5.18			1		1	1	
143	STH 114/STH 55 SPLIT	J3-2	72" X 84"	42.00				2	6	2	EAST STH 114, SOUTH STH 55, SEE PLAN SHEET
144	"	R1-1	36" X 36"	7.46			1		1	1	
145	"	R4-7	24" X 30"	5.00					1		MOUNT ON BACK OF SIGN #144
146	"	R1-1	36" X 36"	7.46			1		1	1	
147	"	J3-1	24" X 57"	9.50				1	3	1	EAST STH 114, SEE PLAN SHEET
148	"	R1-1	36" X 36"	7.46			1		1	1	
149	STH 114, E. OF STH 55	J3-3	72" X 57"	28.50				2	9	2	SOUTH STH 55, WEST STH 114, NORTH STH 55, SEE PLAN SHEET
150	"	J4-1	24" X 36"	6.00			1		2	1	EAST STH 114
151	"	D2-1	60" X 15"	6.25			1		1	1	SEE SIGN DETAIL SHEET
152	"	I55-56	30" X 36"	7.50			1		1	1	SHERWOOD AREA OPTIMIST CLUB
153	"	D1-3	84" X 36"	21.00			2		1	2	SEE SIGN DETAIL SHEET

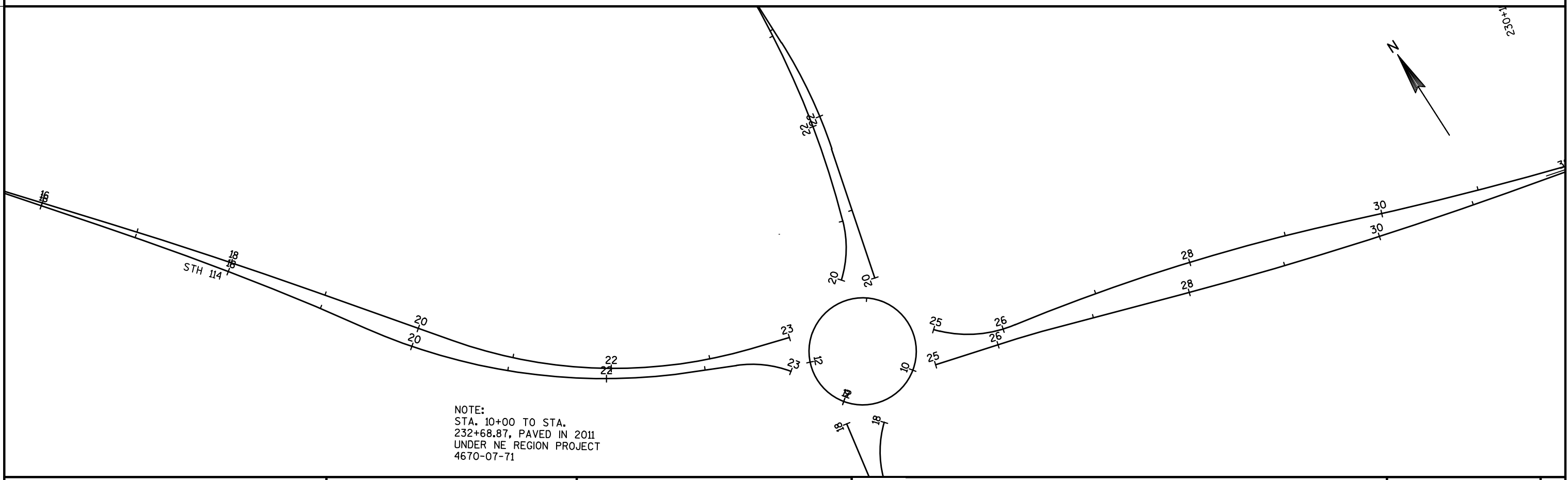
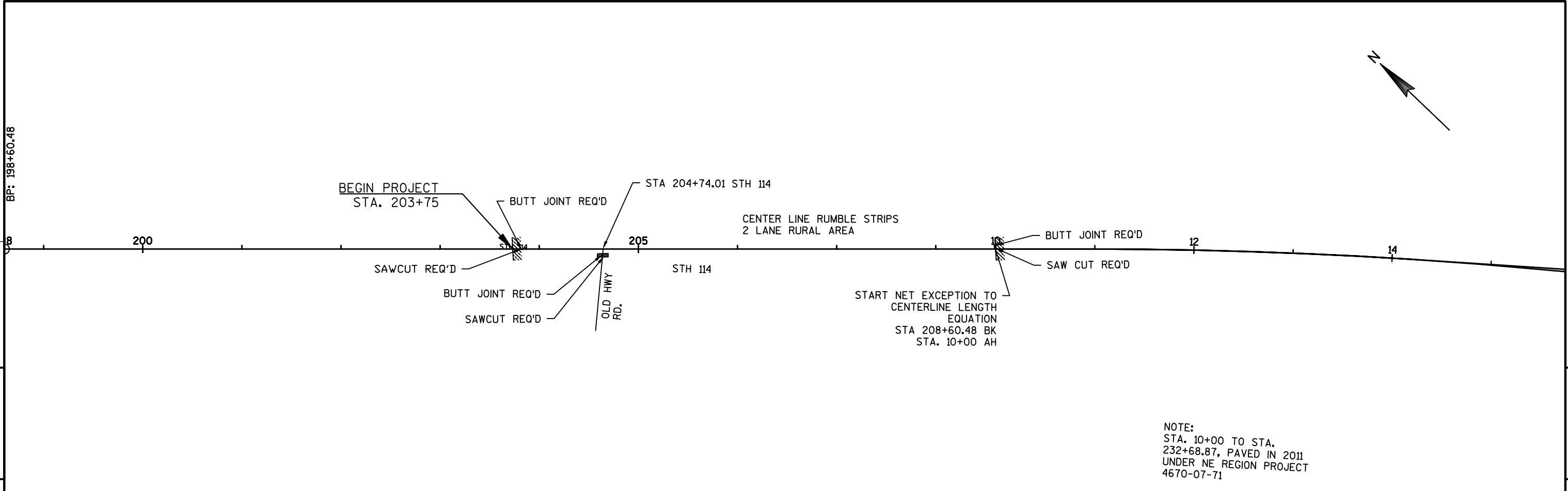
PAGE SUBTOTALS

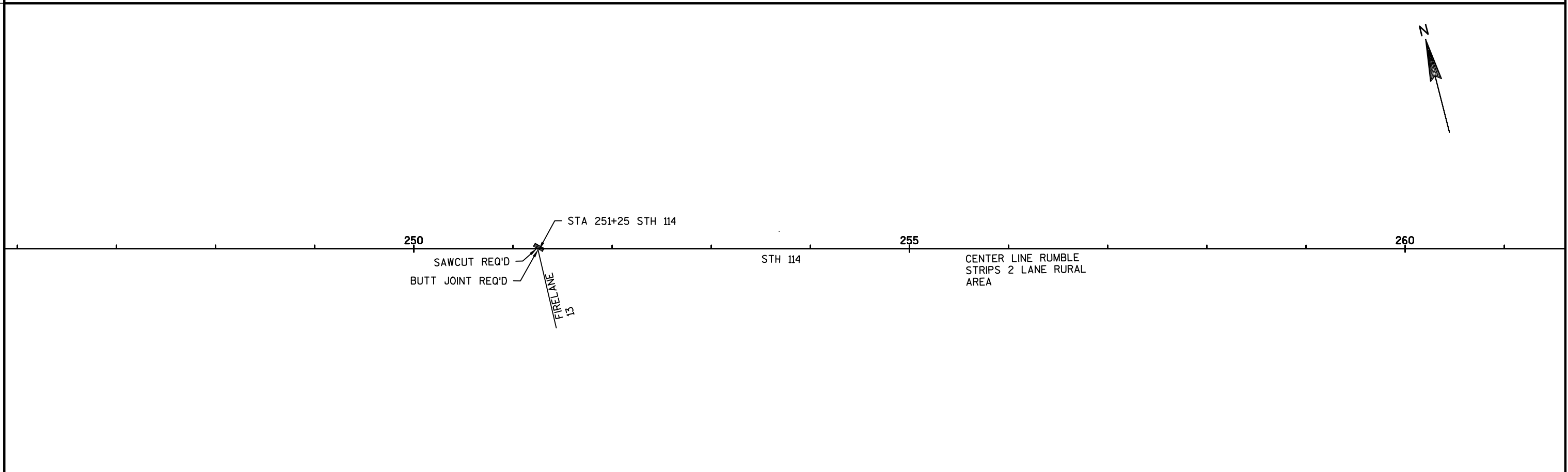
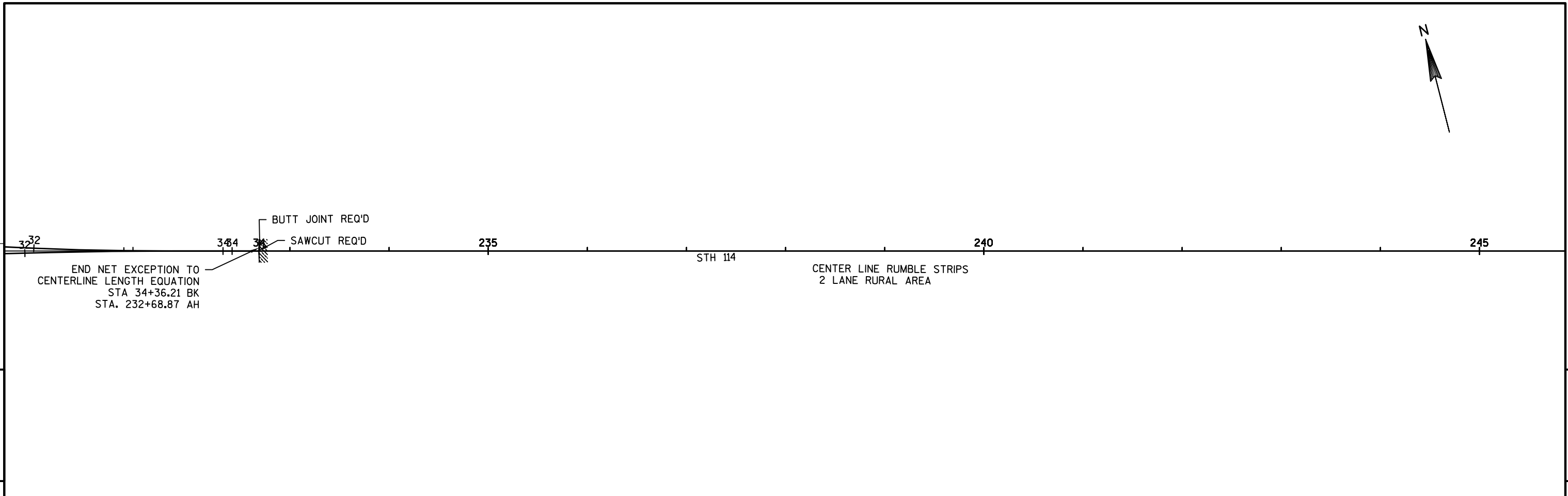
339.05    27.00    0    28    11    64    40

PROJECT TOTALS

846.03    204.57    3    87    34    205    129

PLAN SHEET PRODUCED  
BY WisDOT - NE REGION





PROJECT NO: 4670-11-60	HWY: STH 114	COUNTY: CALUMET	PLAN: ALIGNMENT DIAGRAM	SHEET	E
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265

270

275

STH 114

CENTER LINE RUMBLE STRIPS 2  
LANE RURAL AREA

5

5



STATE PARK  
RD

STA. 278+30.95 STH 114

280

285

290

STH 114

CENTER LINE RUMBLE STRIPS 2  
LANE RURAL AREA

STATE PARK  
RD

PROJECT NO: 4670-11-60

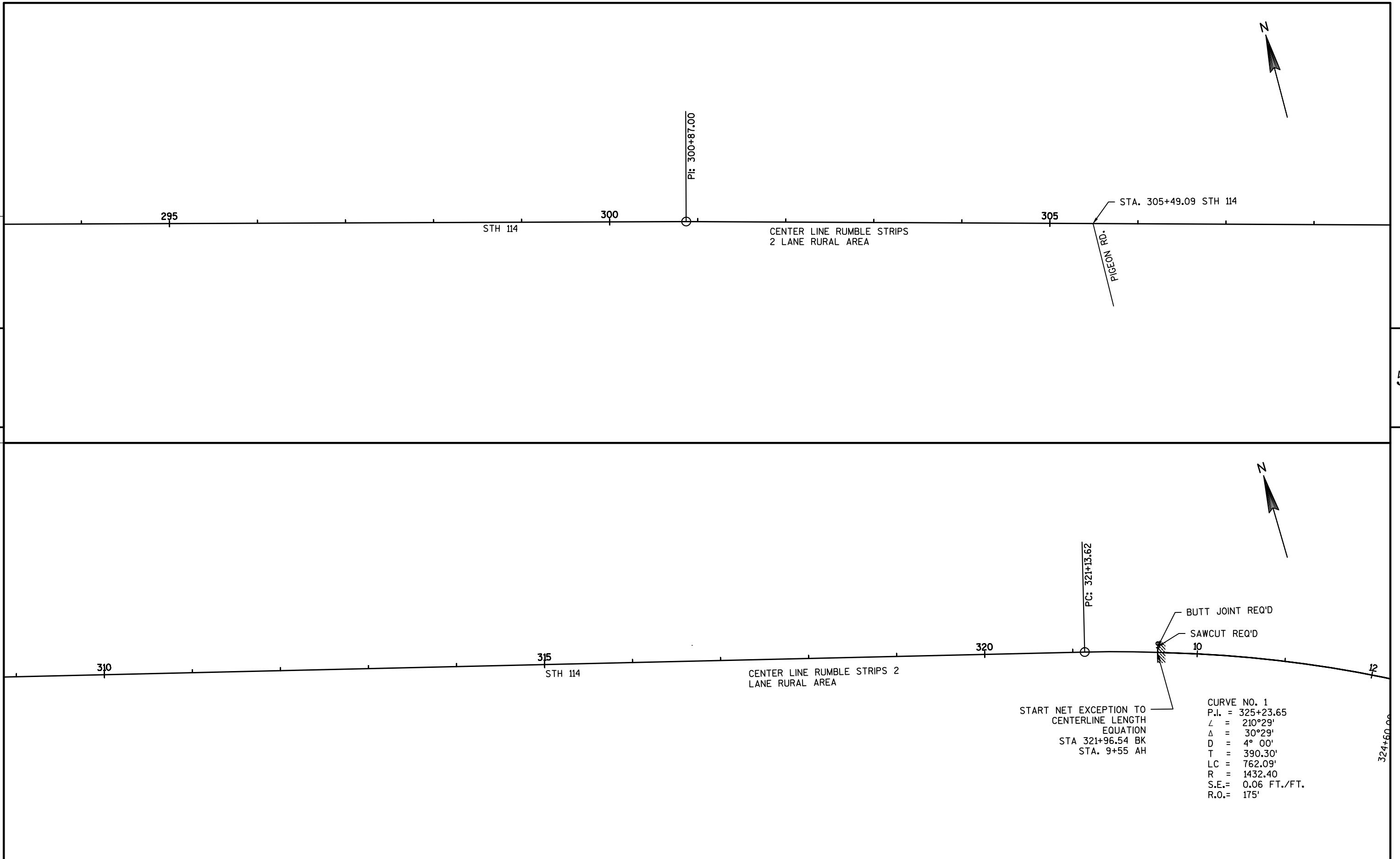
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COUNTY: CALUMET

PLAN: ALIGNMENT DIAGRAM

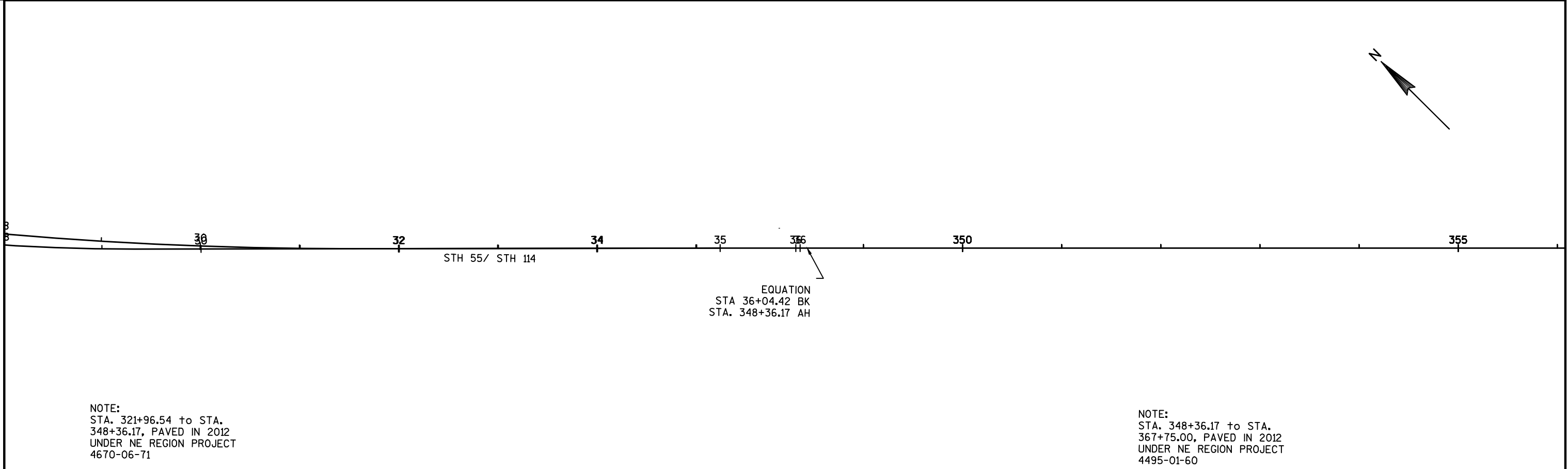
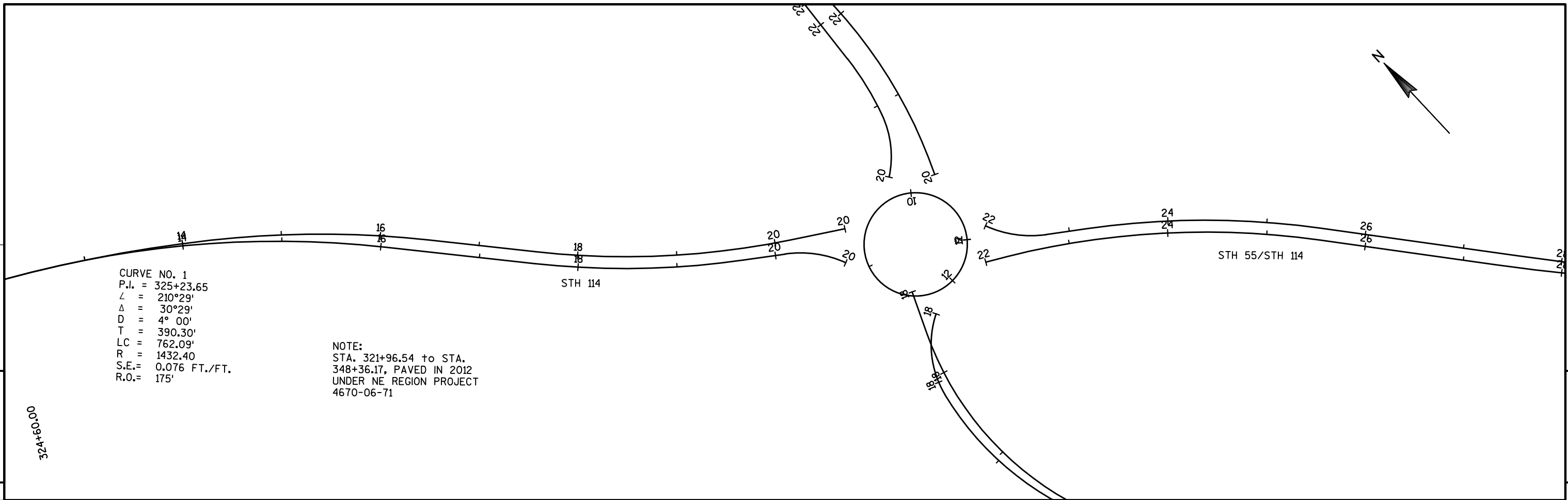
SHEET

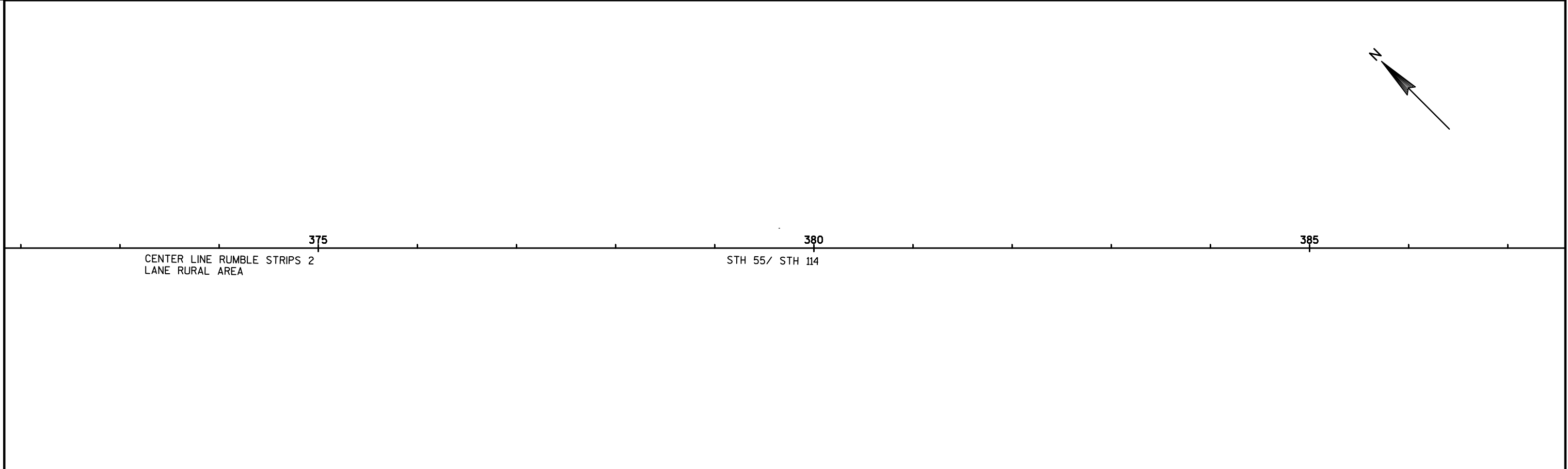
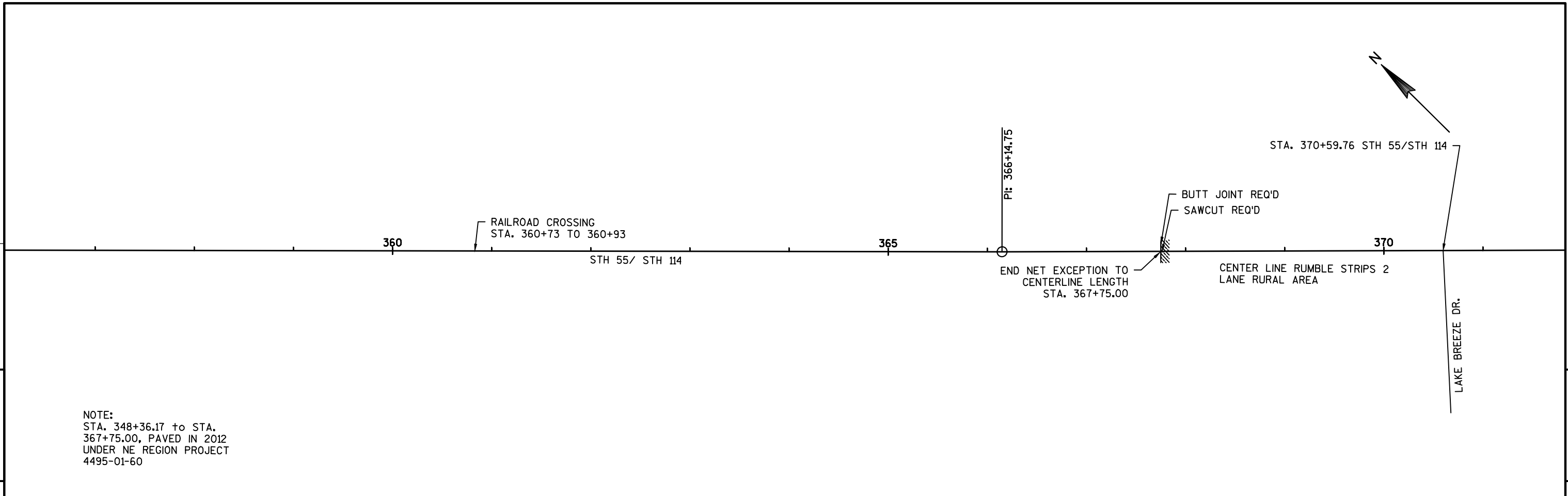
E



5

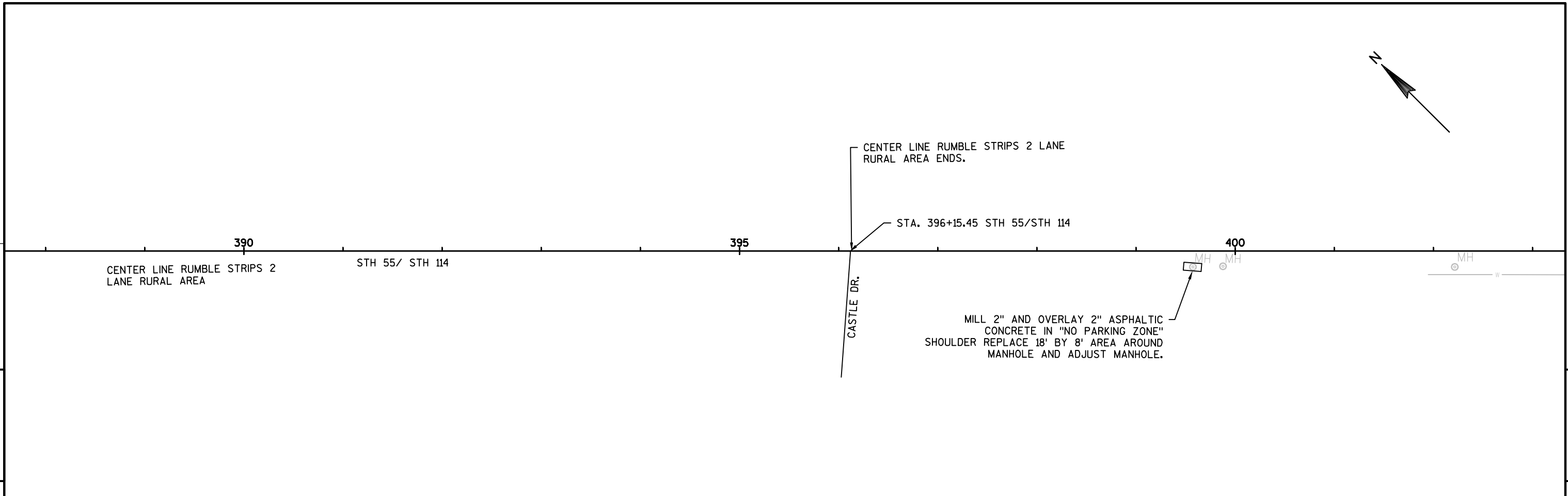
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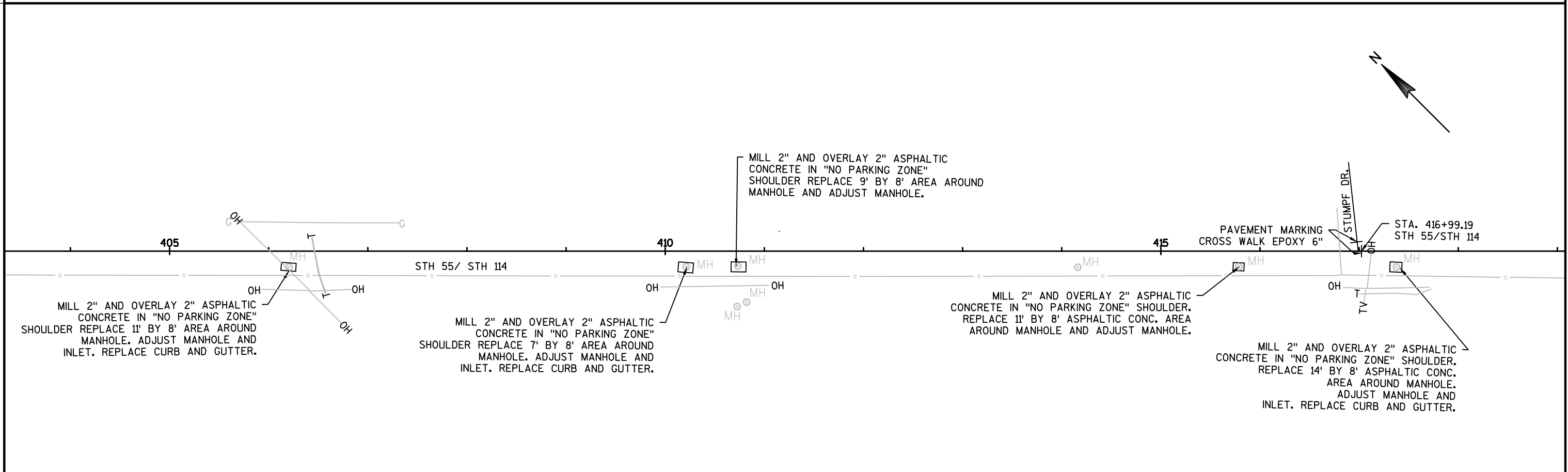
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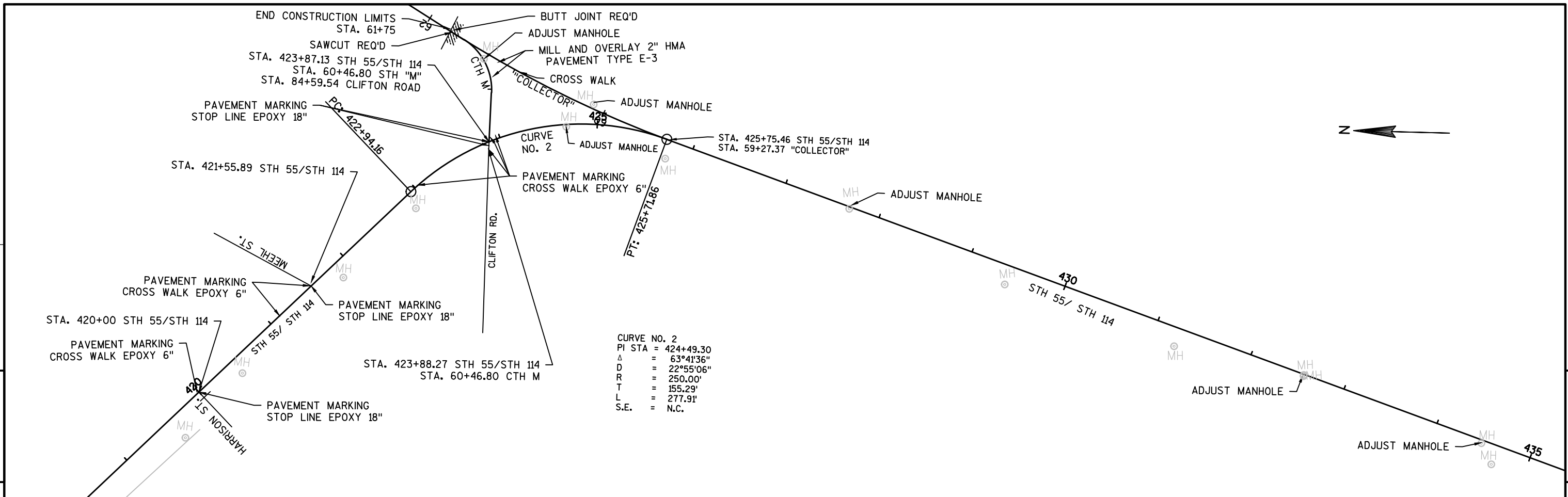


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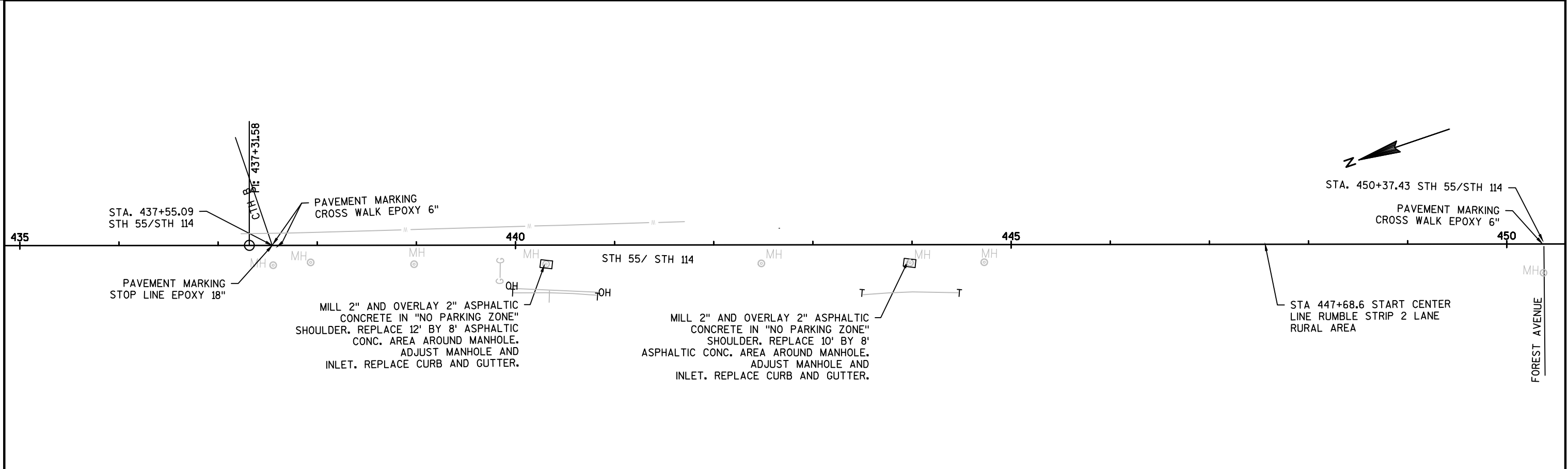
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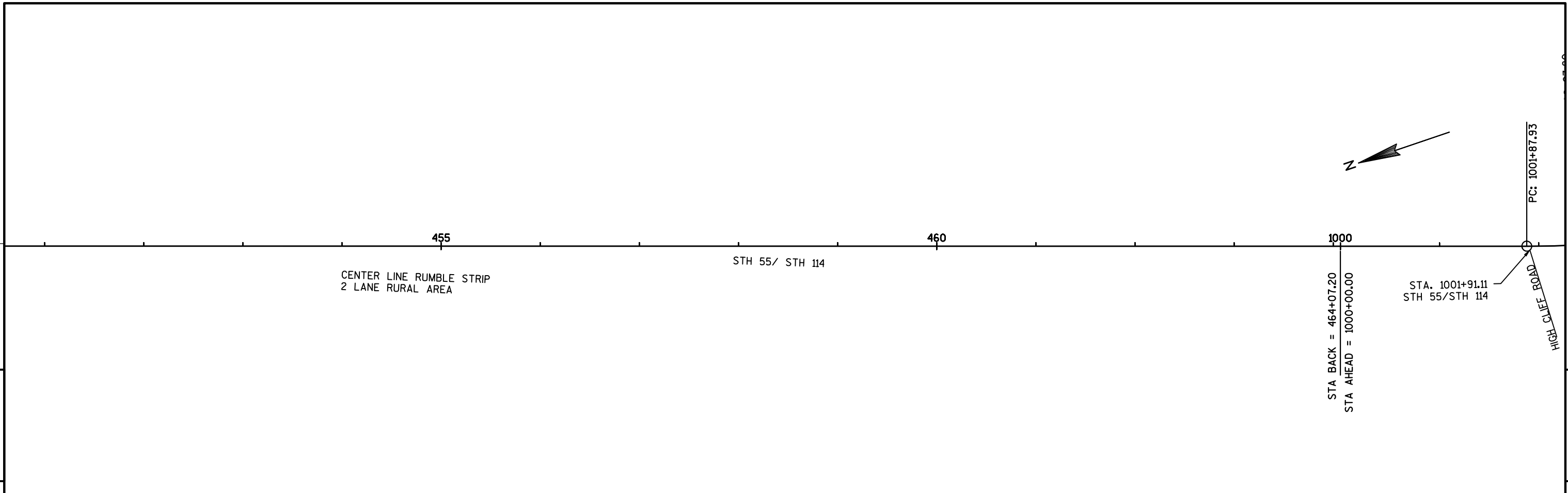


PROJECT NO: 4670-11-60	HWY: STH 114	COUNTY: CALUMET	PLAN: ALIGNMENT DIAGRAM	SHEET	E
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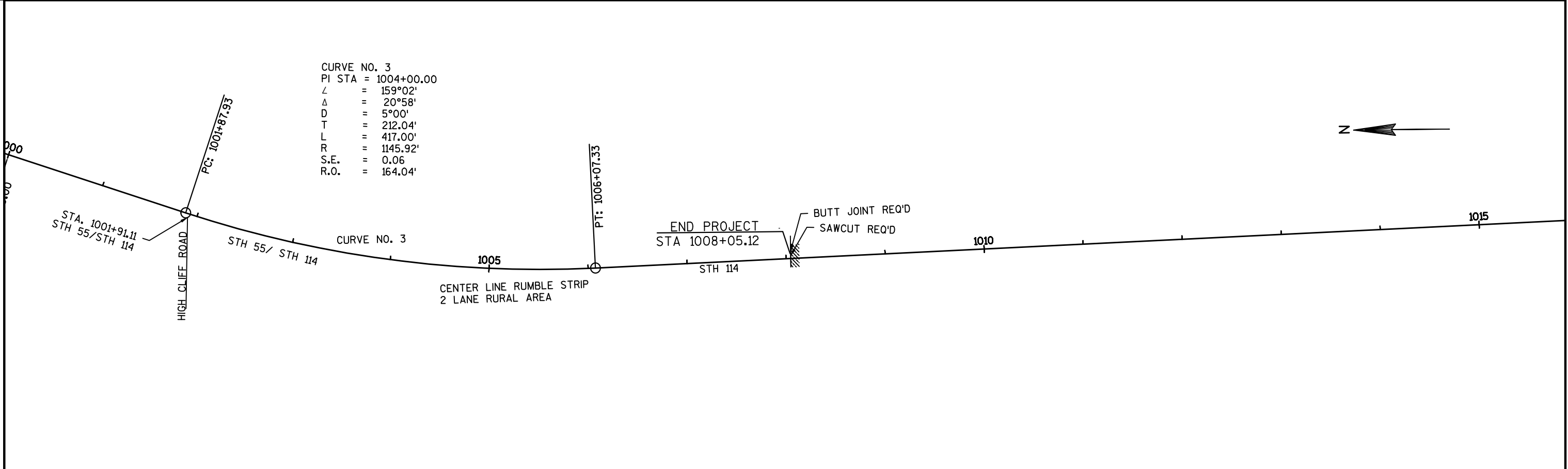
CURVE NO. 2  
 PI STA = 424+49.30  
 $\Delta$  = 63°41'36"  
 D = 22°55'06"  
 R = 250.00'  
 T = 155.29'  
 L = 277.91'  
 S.E. = N.C.





5

5

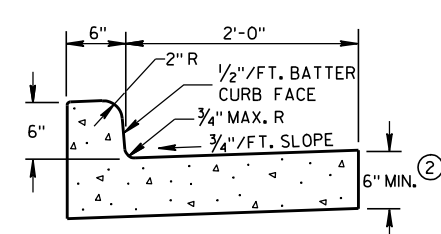


CURVE NO. 3  
 PI STA = 1004+00.00  
 L = 159°02'  
 Δ = 20°58'  
 D = 5°00'  
 T = 212.04'  
 L = 417.00'  
 R = 1145.92'  
 S.E. = 0.06  
 R.O. = 164.04'

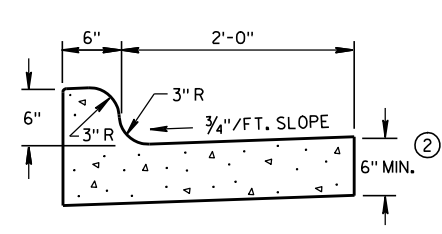
PROJECT NO: 4670-11-60	HWY: STH 114	COUNTY: CALUMET	PLAN: ALIGNMENT DIAGRAM	SHEET	E
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## Standard Detail Drawing List

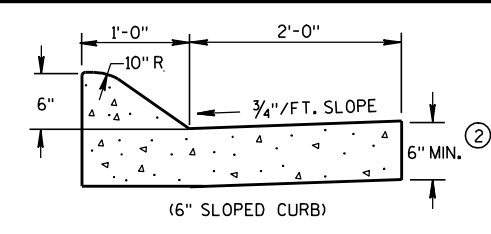
08D01-17	CONCRETE CURB, CONCRETE CURB AND GUTTER AND TIES
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09A01-13A	AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND D AND TEE INTERSECTION BYPASS LANE
13A11-02A	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13A11-02B	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
14B29-01	SAFETY EDGE
15C03-02	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-02	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-02	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C08-16B	PAVEMENT MARKING (INTERSECTIONS)
15C08-16C	PAVEMENT MARKING (CLIMBING LANE & PASSING LANE)
15C08-16D	PAVEMENT MARKING (CLIMBING LANE & PASSING LANE)
15C08-16F	PAVEMENT MARKING (ISLANDS)
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C19-02A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-01	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D28-02	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY



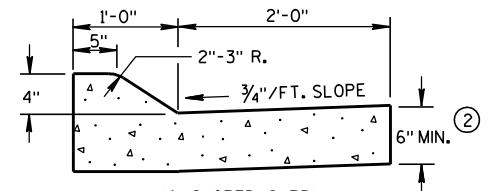
TYPES A & D ①



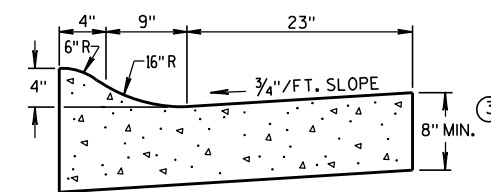
TYPES K & L ①



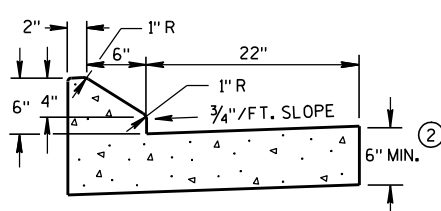
(6" SLOPED CURB)



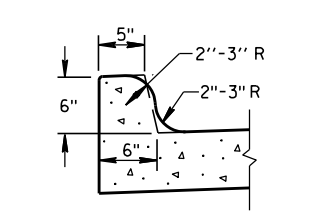
(4" SLOPED CURB)



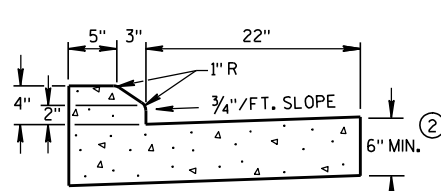
4" SLOPED CURB TYPES R & T ① ④



6" SLOPED CURB TYPES G & J ①

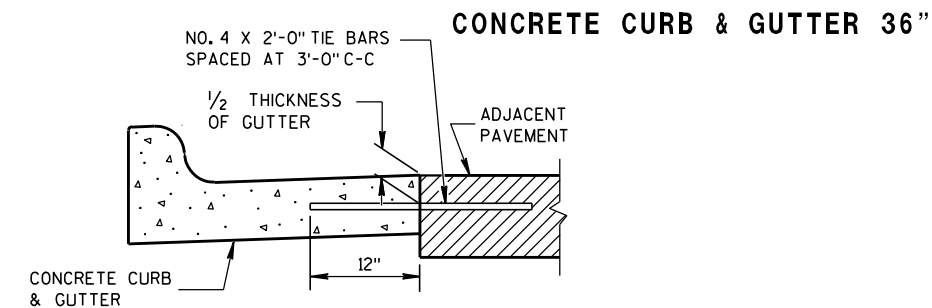


OPTIONAL CURB SHAPE FOR TYPES K & L ①

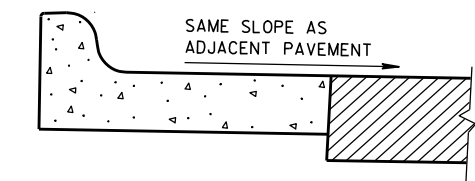


4" SLOPED CURB TYPES G & J ①

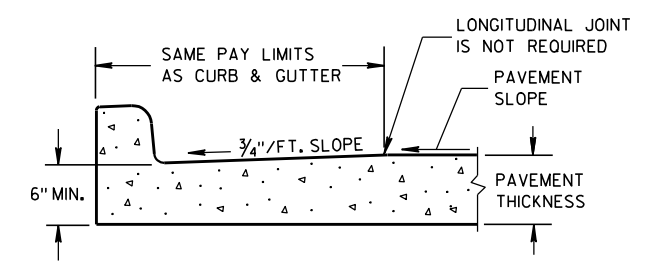
CONCRETE CURB & GUTTER 30"



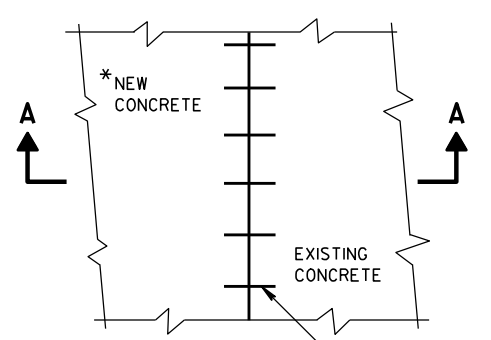
TYPICAL TIE BAR LOCATION ①



REVERSE SLOPE GUTTER ⑤  
(TYPICAL FOR ALL CURB & GUTTER TYPES)



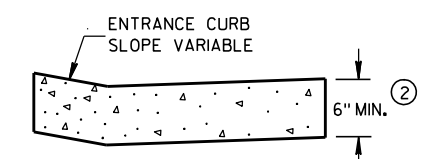
PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB & GUTTER



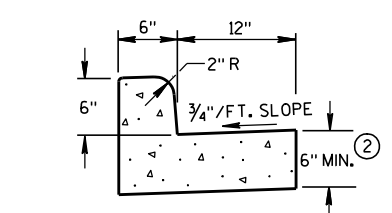
PLAN VIEW

\* NEW CURB & GUTTER, SURFACE DRAINS, CONCRETE PAVEMENT OR OTHER NEW CONCRETE.

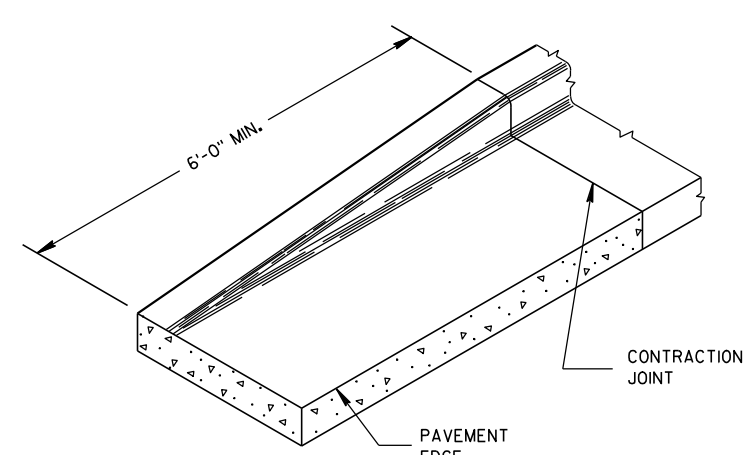
NO. 6 TIE BARS SPACED 2'-6" C-C, INSTALLED PERPENDICULAR TO THE LONGITUDINAL JOINT.



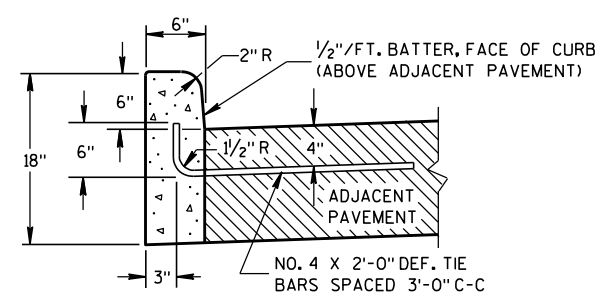
DRIVEWAY ENTRANCE CURB (WHEN DIRECTED BY THE ENGINEER)



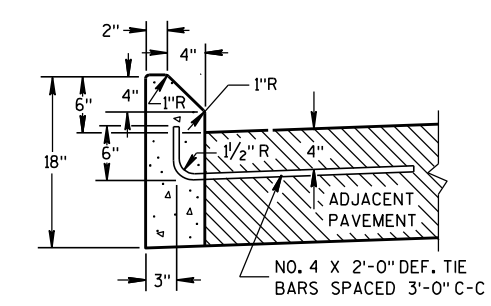
TYPES A & D CONCRETE CURB & GUTTER 18"



END SECTION CURB & GUTTER



TYPES A & D CONCRETE CURB



TYPES G & J CONCRETE CURB

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.  
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

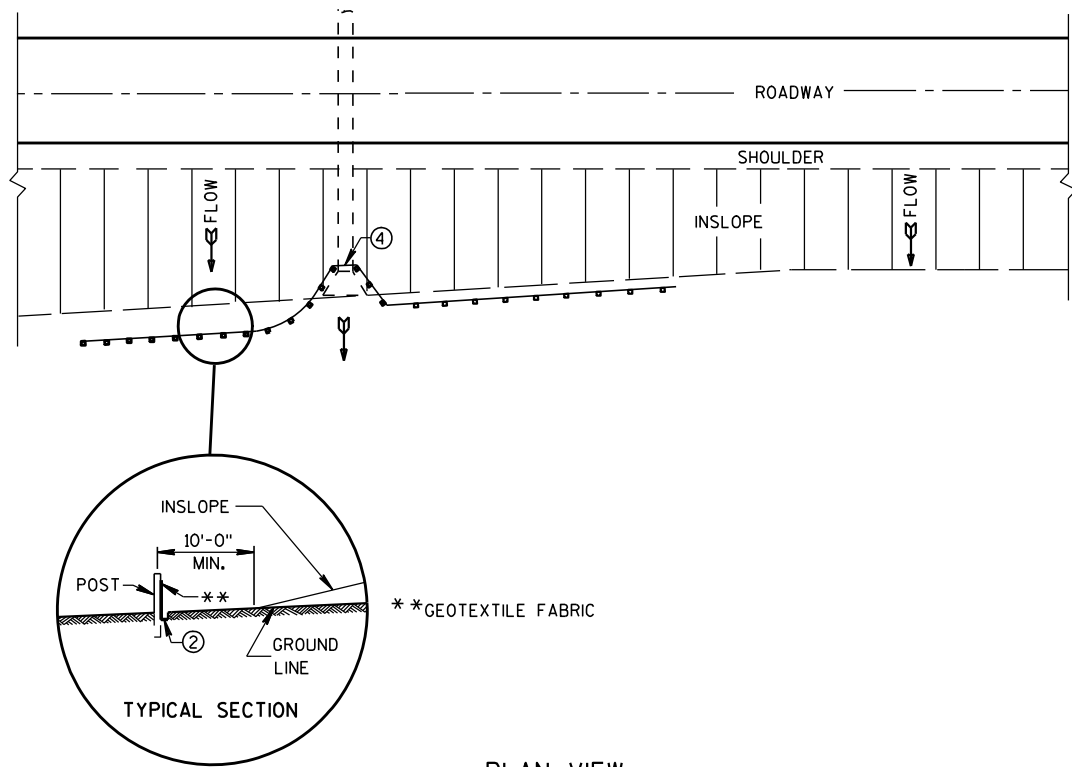
INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE. A LONGITUDINAL CONSTRUCTION JOINT IS NOT REQUIRED WITH INTEGRAL CURB AND GUTTER.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

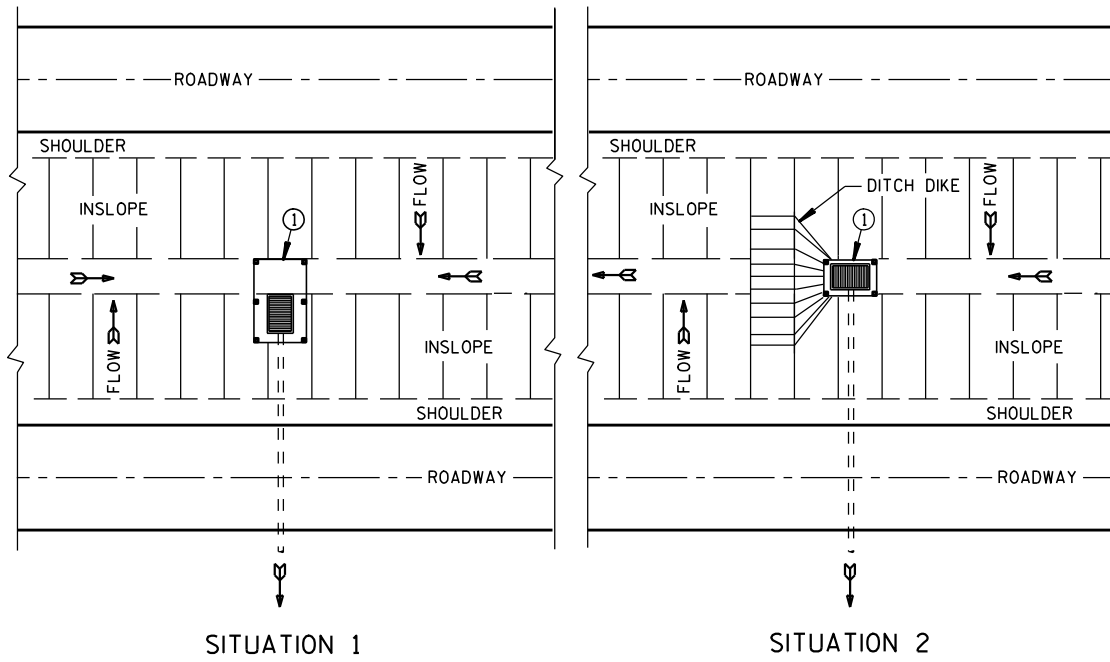
UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K AND R.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ④ THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑤ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.

<b>CONCRETE CURB, CONCRETE CURB &amp; GUTTER AND TIES</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 9/4/08 DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



PLAN VIEW  
TYPICAL APPLICATION OF SILT FENCE

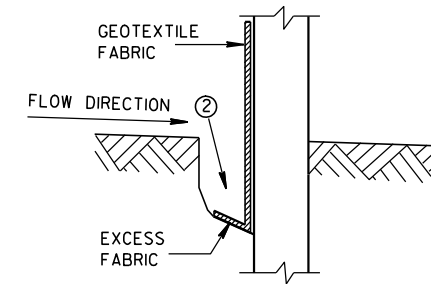


PLAN VIEW  
SILT FENCE AT MEDIAN SURFACE DRAINS

**GENERAL NOTES**

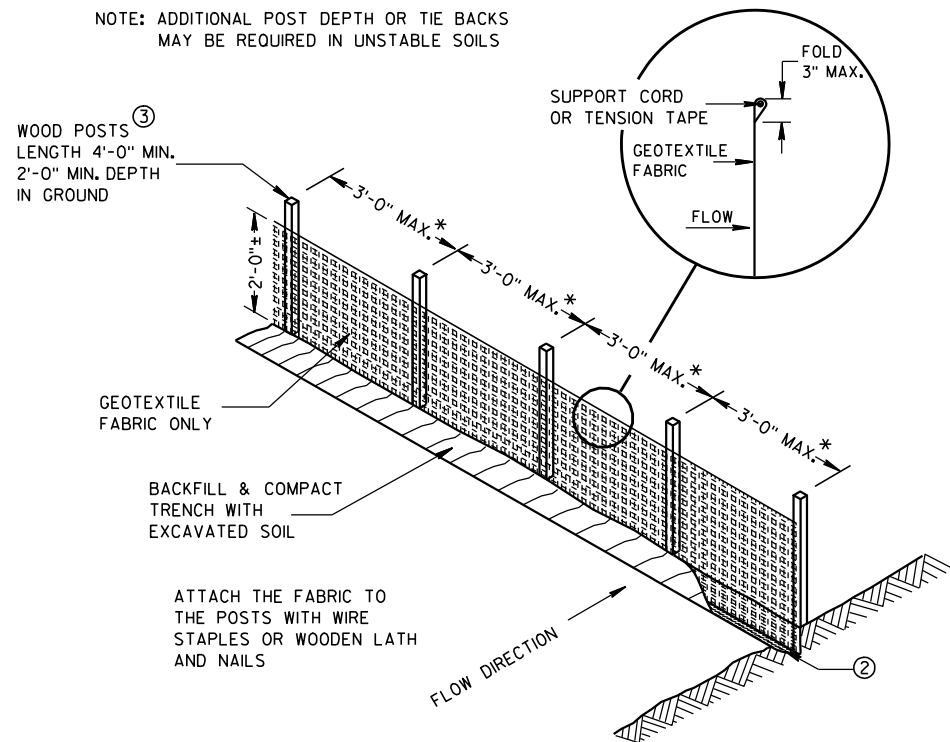
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



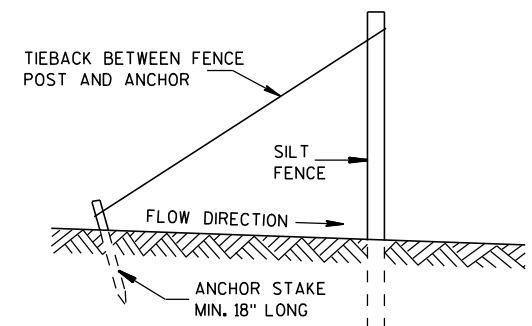
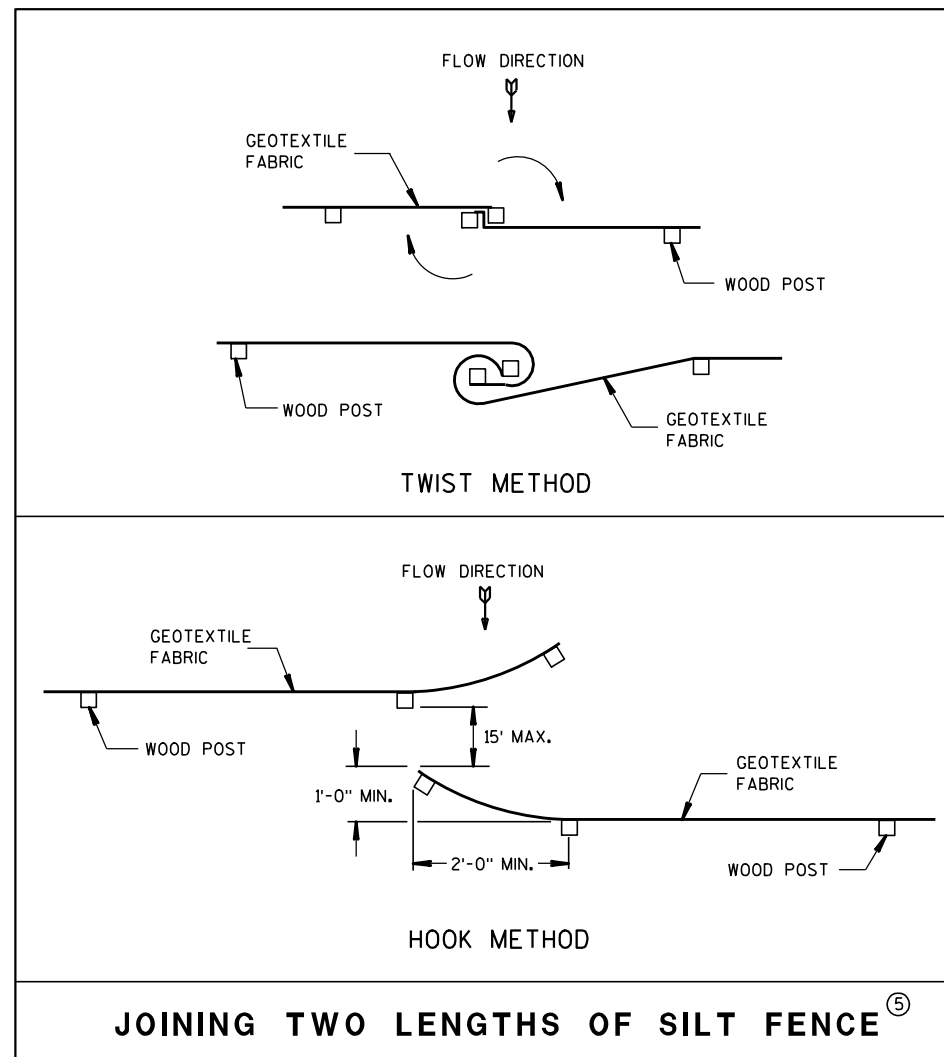
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS



SILT FENCE

\* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

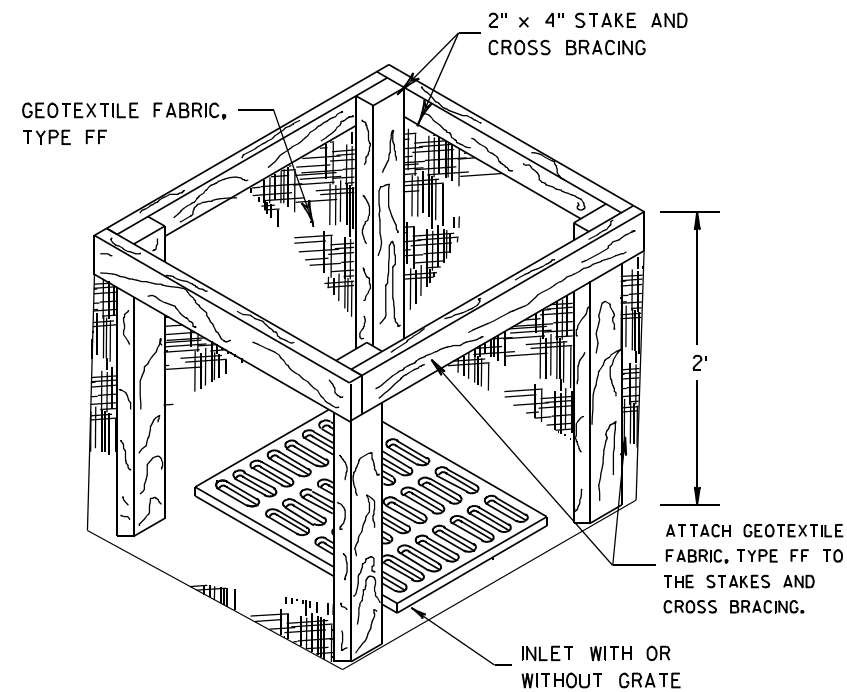
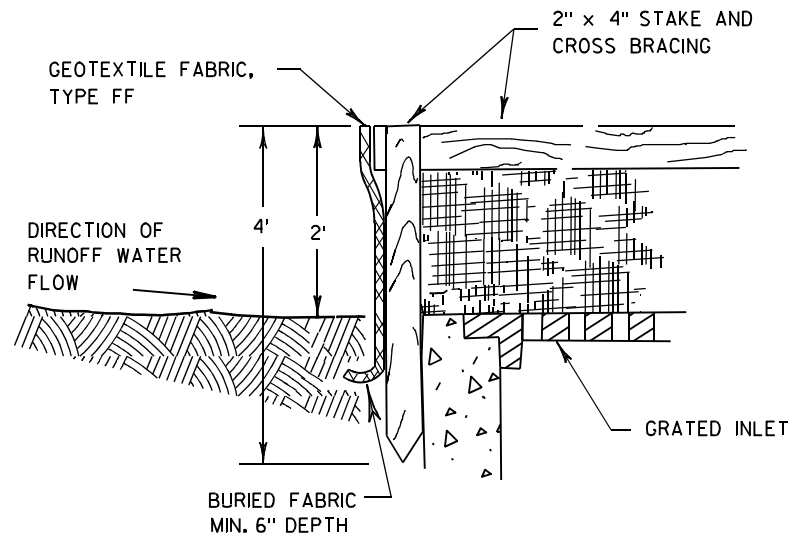


SILT FENCE TIE BACK  
(WHEN REQUIRED BY THE ENGINEER)

**SILT FENCE**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
4-29-05 /S/ Beth Cannestra  
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER  
FHWA



**INLET PROTECTION, TYPE A**

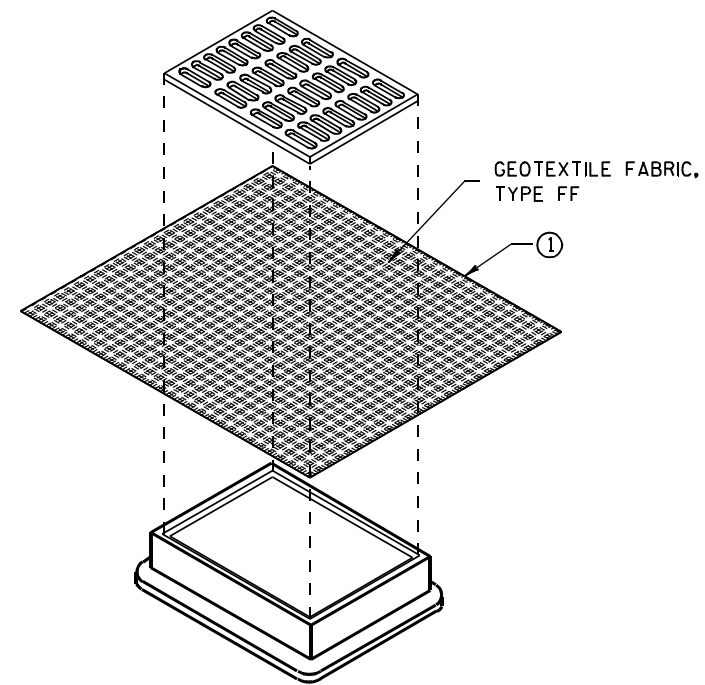
**GENERAL NOTES**

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

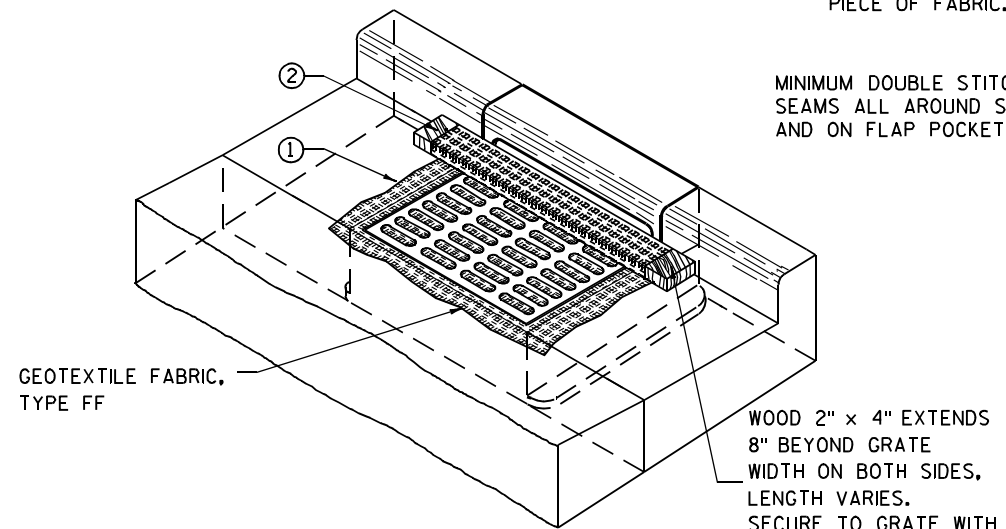
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B  
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



**INLET PROTECTION, TYPE C (WITH CURB BOX)**

**INSTALLATION NOTES**

**TYPE B & C**

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

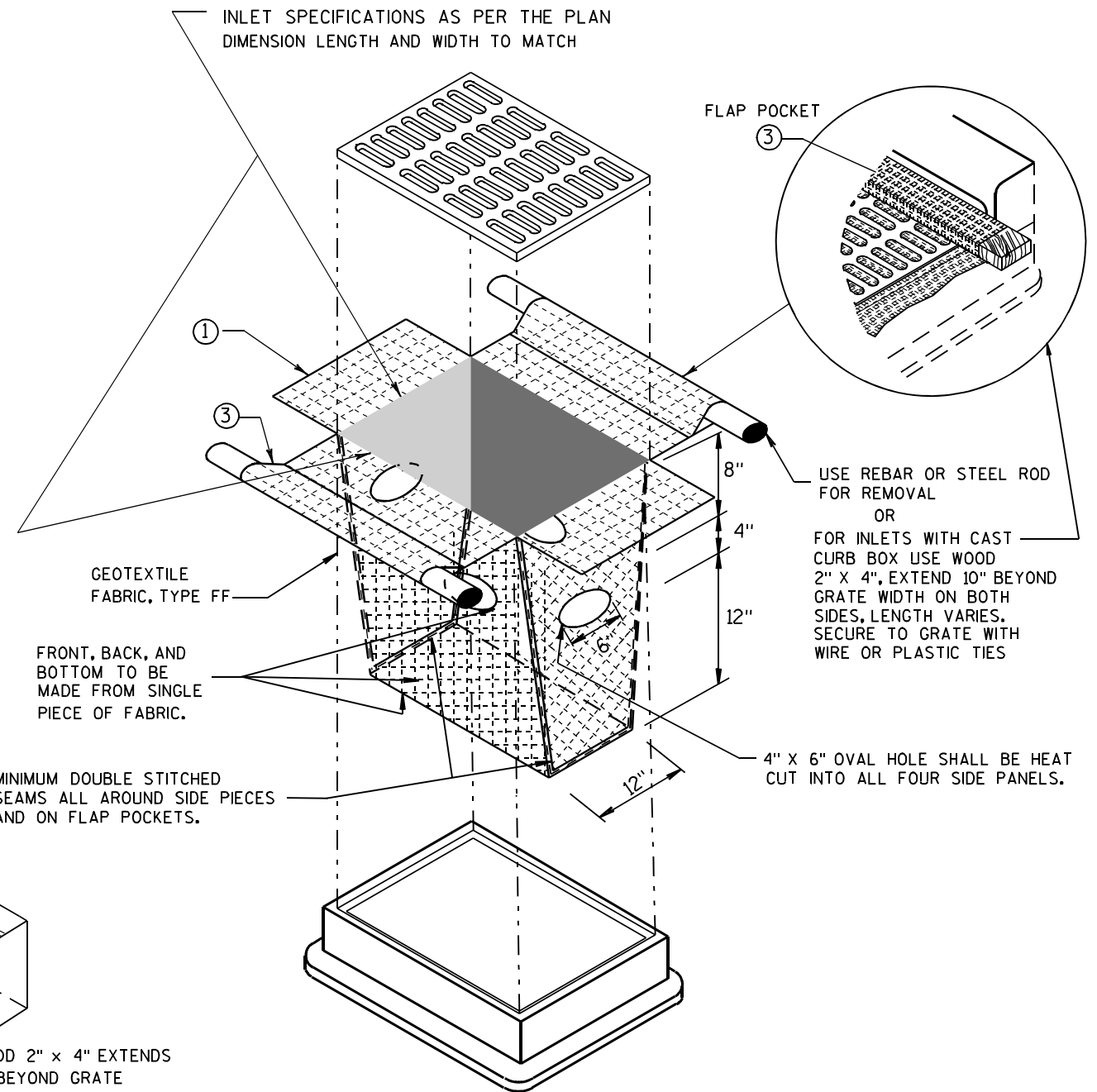
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

**TYPE D**

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

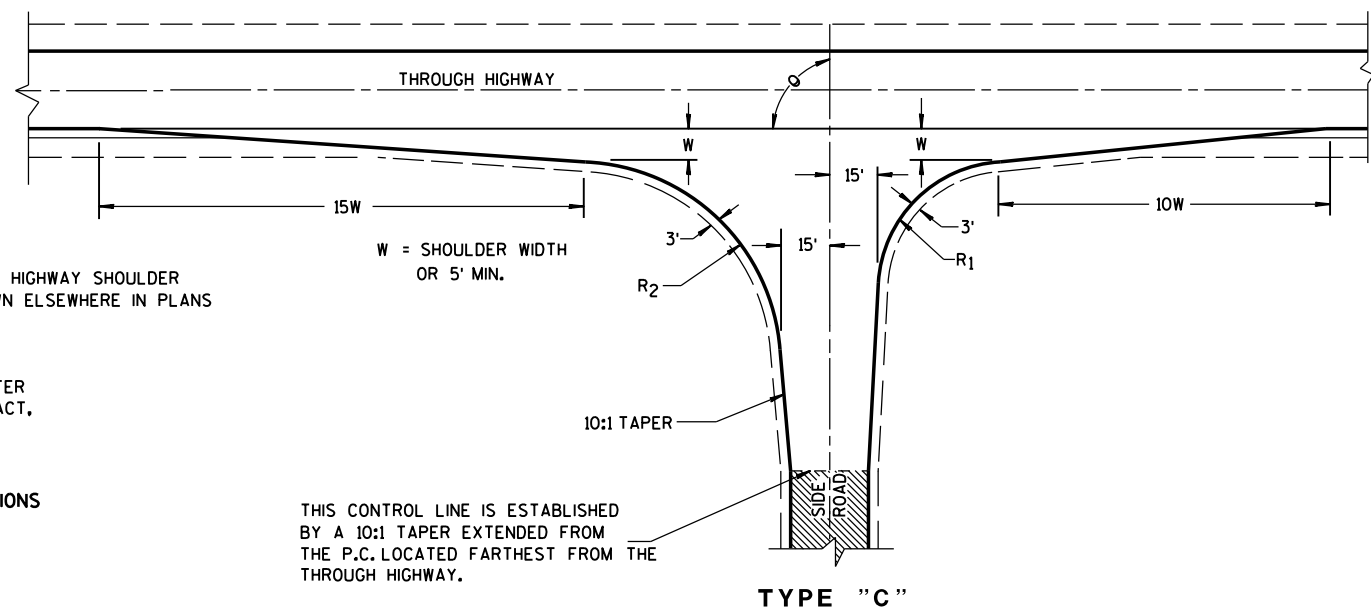
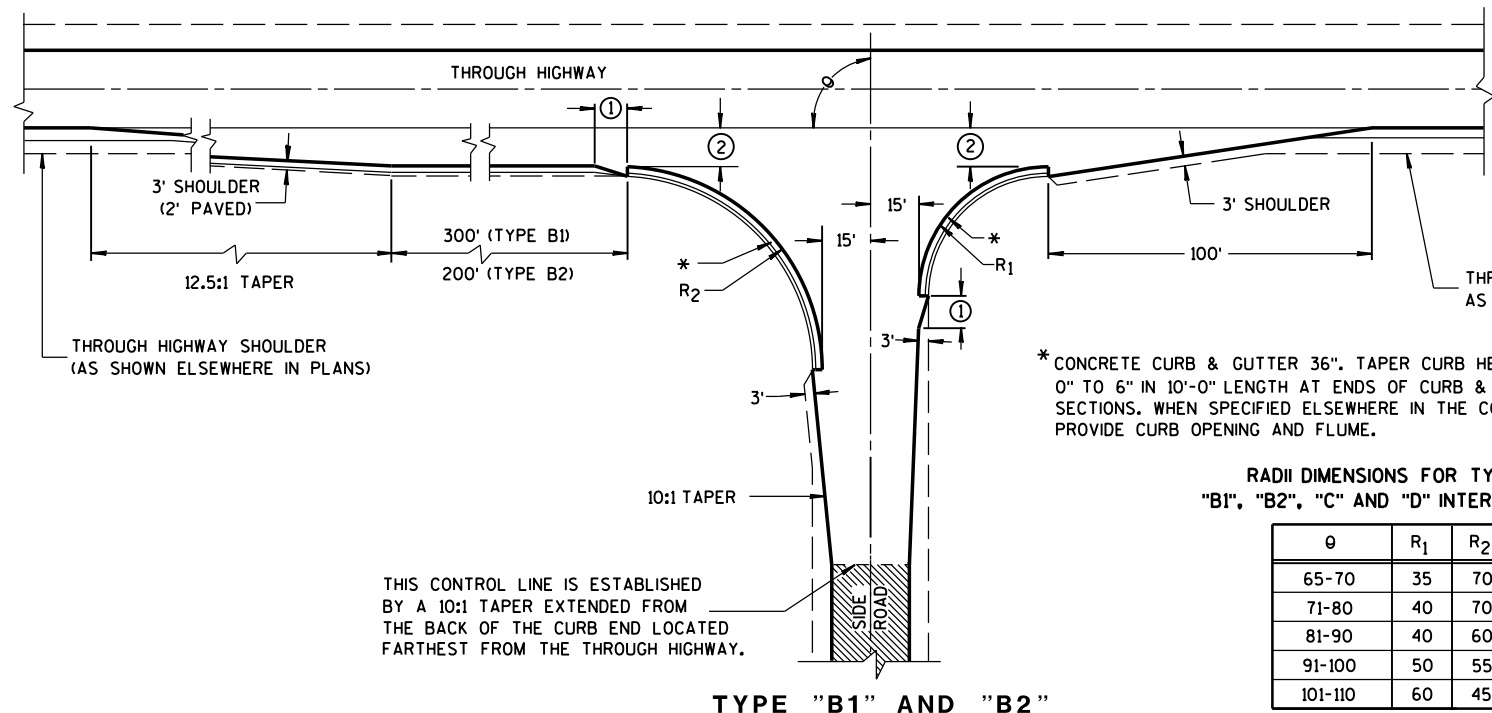
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



**INLET PROTECTION, TYPE D**

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

<b>INLET PROTECTION TYPE A, B, C, AND D</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



RADII DIMENSIONS FOR TYPES "B1", "B2", "C" AND "D" INTERSECTIONS

θ	R <sub>1</sub>	R <sub>2</sub>
65-70	35	70
71-80	40	70
81-90	40	60
91-100	50	55
101-110	60	45

**GENERAL NOTES**

DESIGNS MAY BE USED INTERCHANGEABLY IN COMBINATION OR SEPARATELY FOR ANY ONE COMPLETE INTERSECTION DEPENDING UPON INTERSECTION ANGLE AND SURFACING OF EACH APPROACH ROADWAY.

**SIDE ROAD SURFACING NOTE**

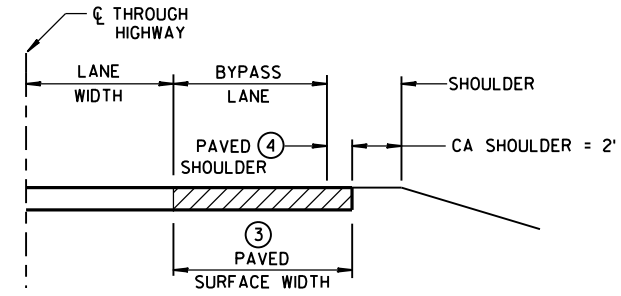
WHEN THE SIDE ROAD IS NOT PRESENTLY PAVED, PAVEMENT SHALL BE PLACED TO THE LIMITS SHOWN UNLESS OTHERWISE PROVIDED IN THE CONTRACT. WHERE THE CONSTRUCTION LIMITS ARE BEYOND THE PAVING LIMITS, CRUSHED AGGREGATE SURFACING SHALL BE PLACED BETWEEN THE PAVING LIMITS AND CONSTRUCTION LIMITS.

WHEN THE SIDE ROAD IS PRESENTLY PAVED, NEW PAVEMENT SHALL BE PLACED TO THE LIMITS OF DESIGN AS SHOWN AND BEYOND, IF NECESSARY, TO MEET EXISTING PAVEMENT.

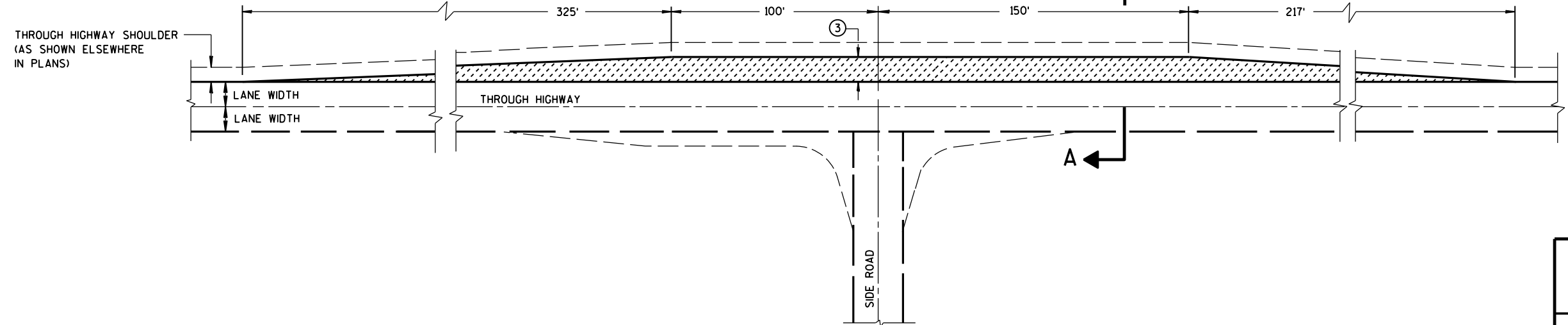
WHEN THE SIDE ROAD IS THE CONSTRUCTION PROJECT, THE INTERSECTION SURFACING SHALL BE THE SAME AS FOR THE PROJECT.

- EXISTING PAVED SURFACE
- BYPASS LANE

- ① 10-FT TYPICAL.
- ② 12-FT\*\* PLUS ADDITIONAL WIDTH FOR BIKE LANE IF SHOWN ELSEWHERE IN THE PLAN.
- \*\*10-FT MAY BE USED ON TYPE B2 ON RESURFACING PROJECTS IF SPECIFIED IN THE CONTRACT.
- ③ BYPASS LANE PAVED SURFACE WIDTH OUTSIDE OF TRAVEL LANE  
-ASPHALT = 12-FT PLUS PAVED SHOULDER WIDTH.  
-PC CPNCRETE = 13-FT PLUS PAVED SHOULDER WIDTH.
- ④ BYPASS LANE PAVED SHOULDER WIDTH = THE GREATER OF 1-FT OR THE PAVED SHOULDER WIDTH OF THE THROUGH HIGHWAY.



SECTION A-A (SHOWING BYPASS LANE AND SHOULDER)



TEE INTERSECTION BYPASS LANE DETAIL

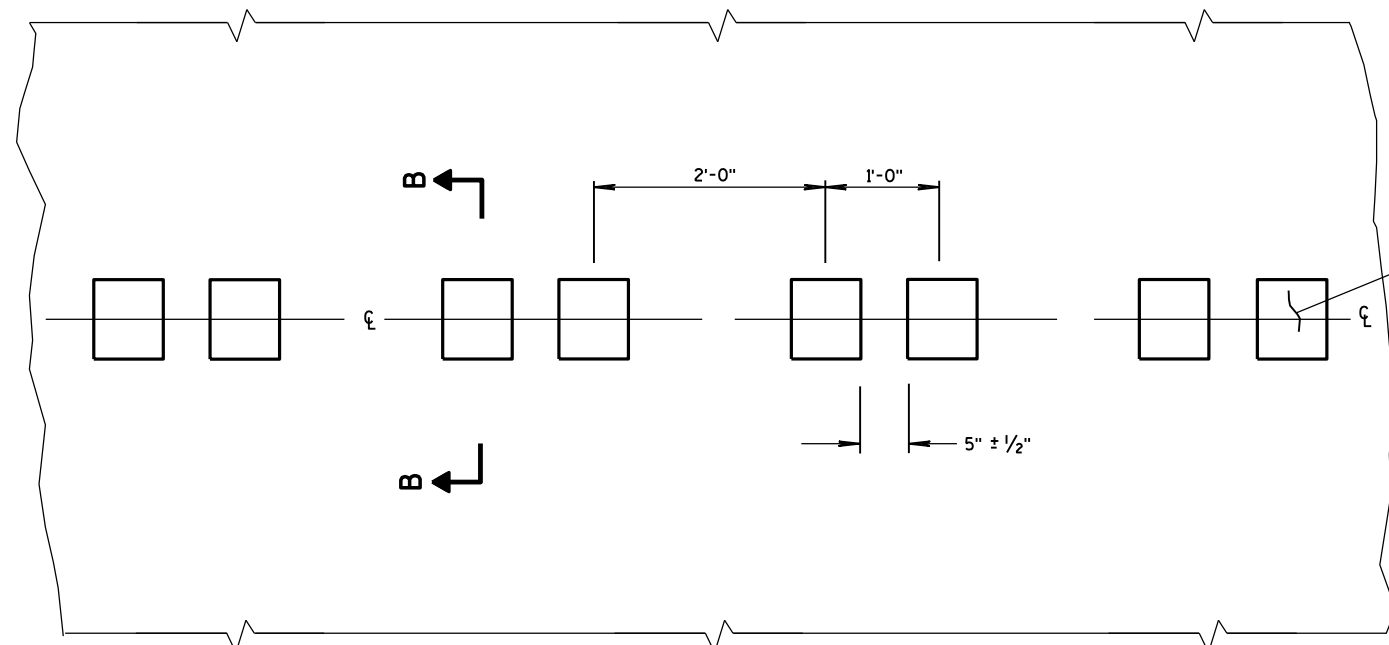
AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND "D" AND TEE INTERSECTION BYPASS LANE  
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



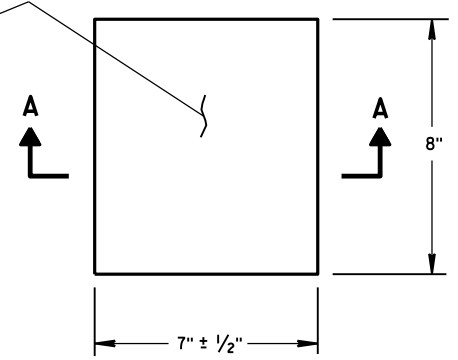
**GENERAL NOTES**

DETAILS OF CONSTRUCTION SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.  
 DO NOT MILL CENTER LINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.  
 INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.  
 SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

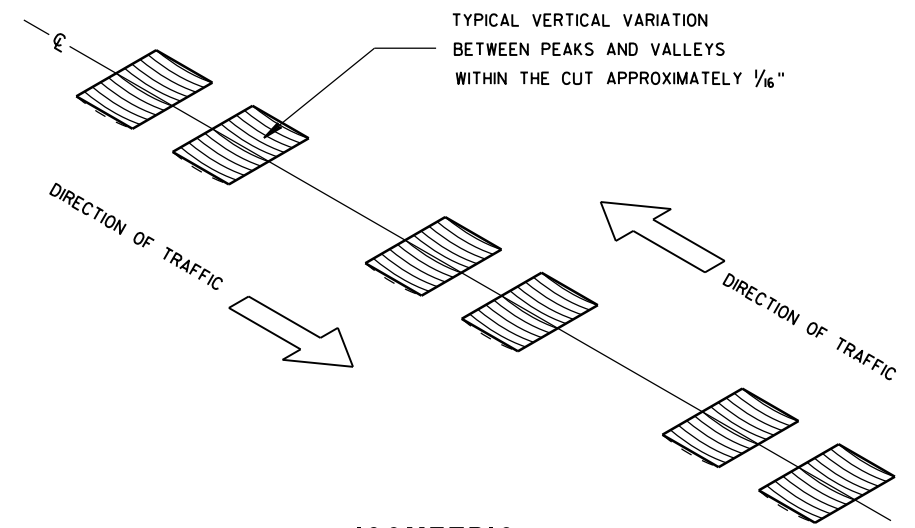
① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.



PLAN VIEW  
CENTER LINE WITH GROOVES

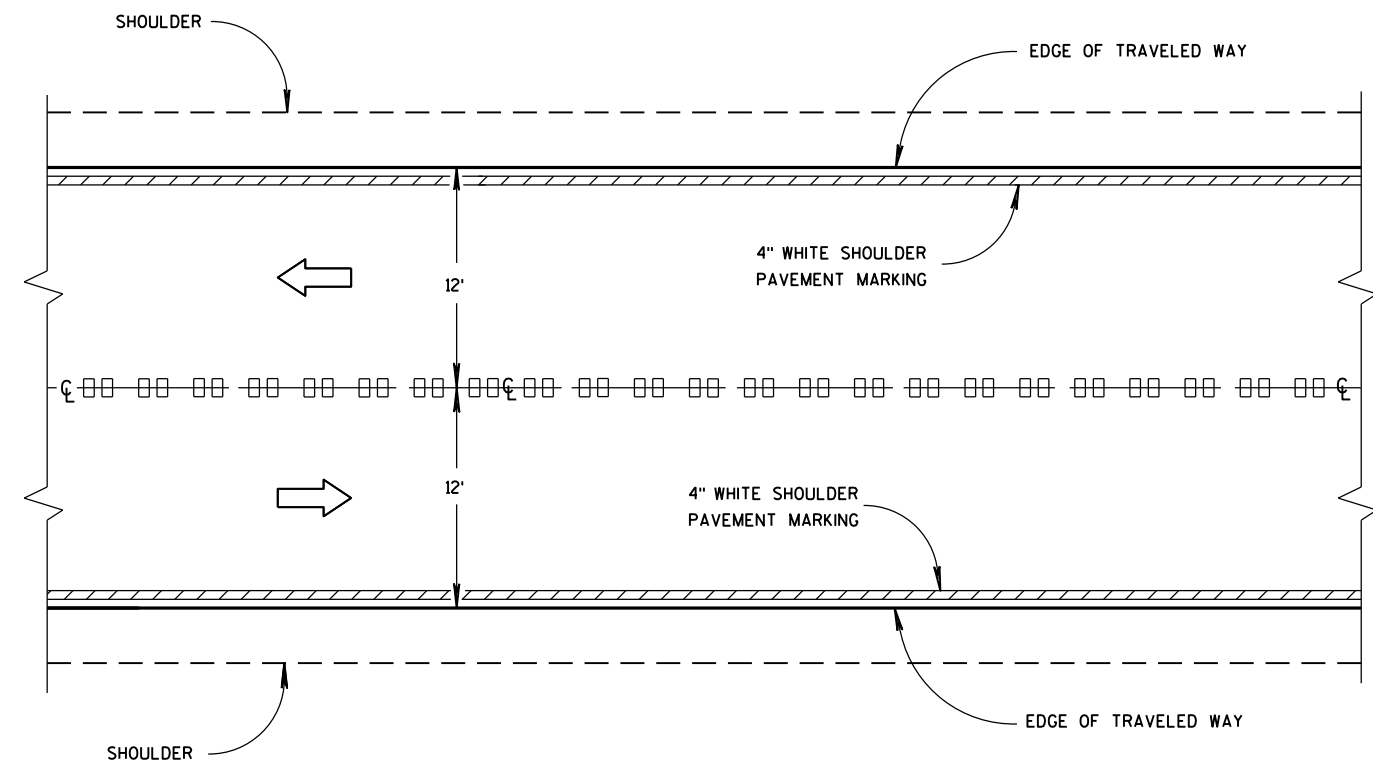


PLAN VIEW  
(SINGLE GROOVE)

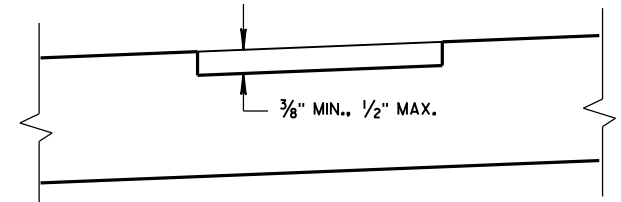


ISOMETRIC

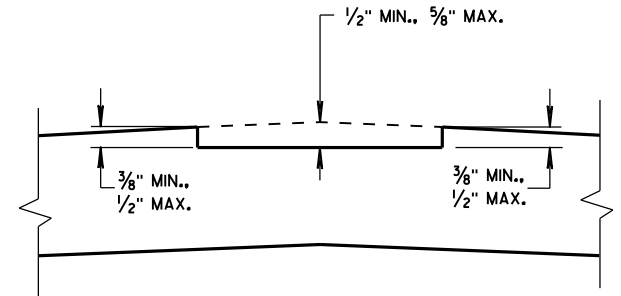
**PLACEMENT DETAIL FOR MILLED RUMBLE STRIP**



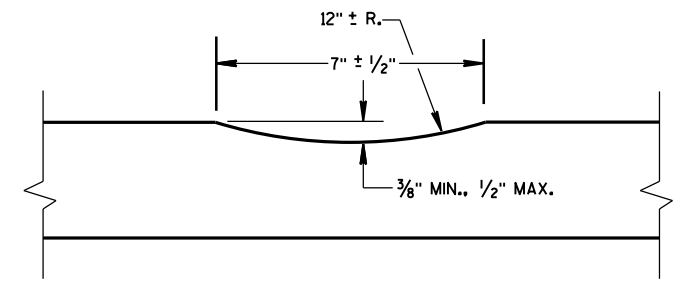
CENTER LINE GROOVES ON TWO-WAY ROADWAYS



SECTION B-B  
SUPERELEVATED ROADWAY



SECTION B-B  
CROWNED ROADWAY

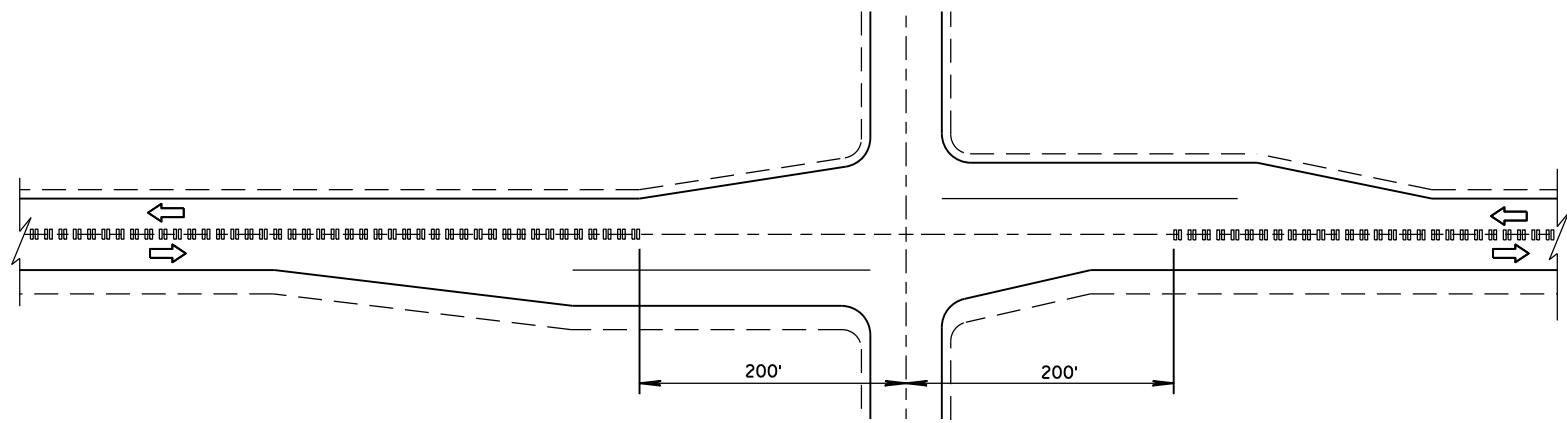


SECTION A-A

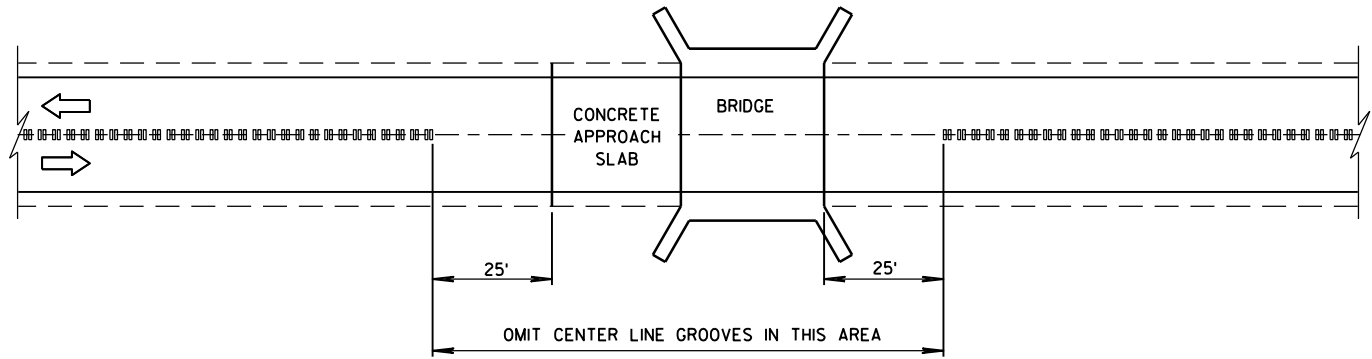
**2-LANE RURAL  
CENTER LINE RUMBLE STRIP,  
MILLING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

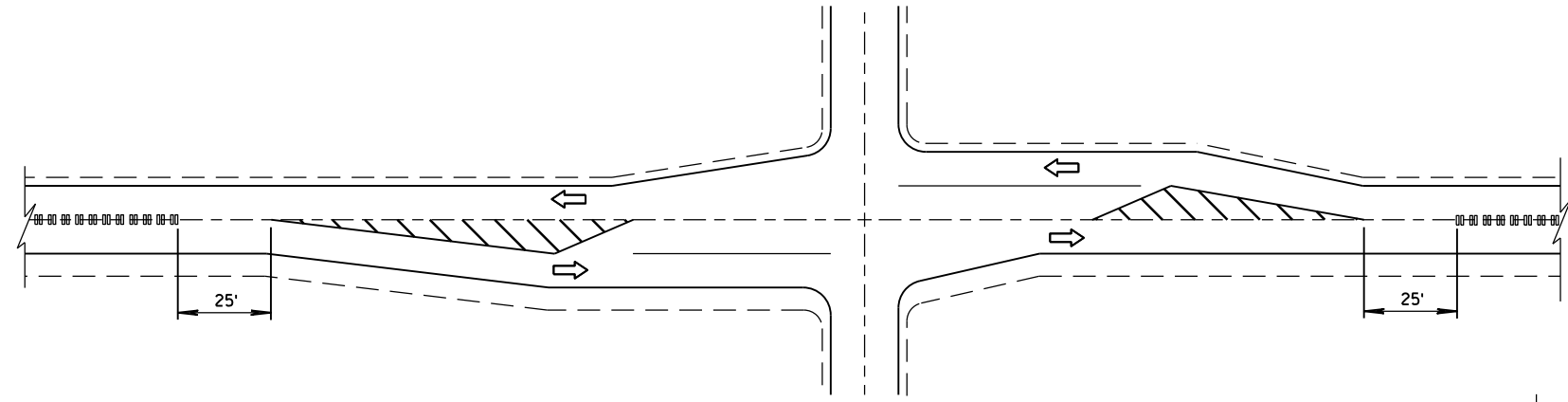
① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.



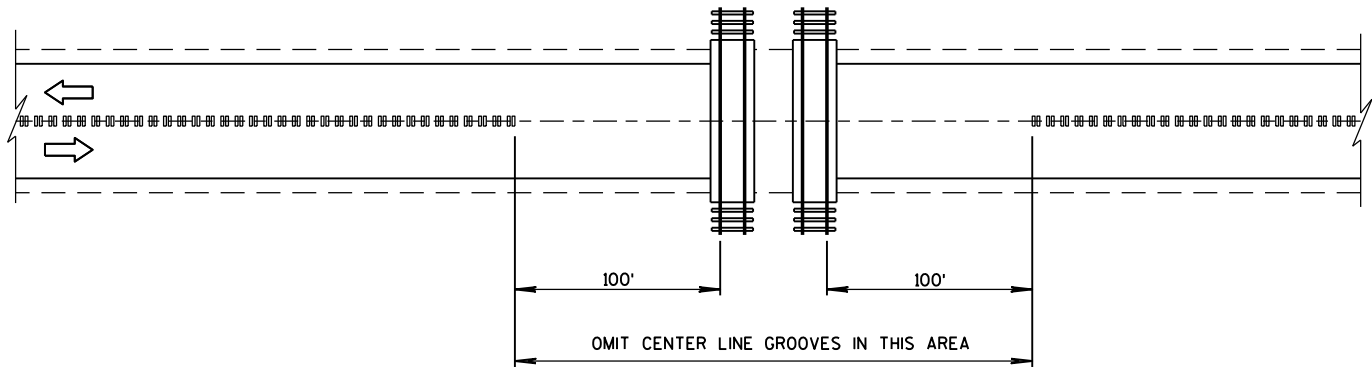
CENTER LINE GROOVES AT INTERSECTIONS



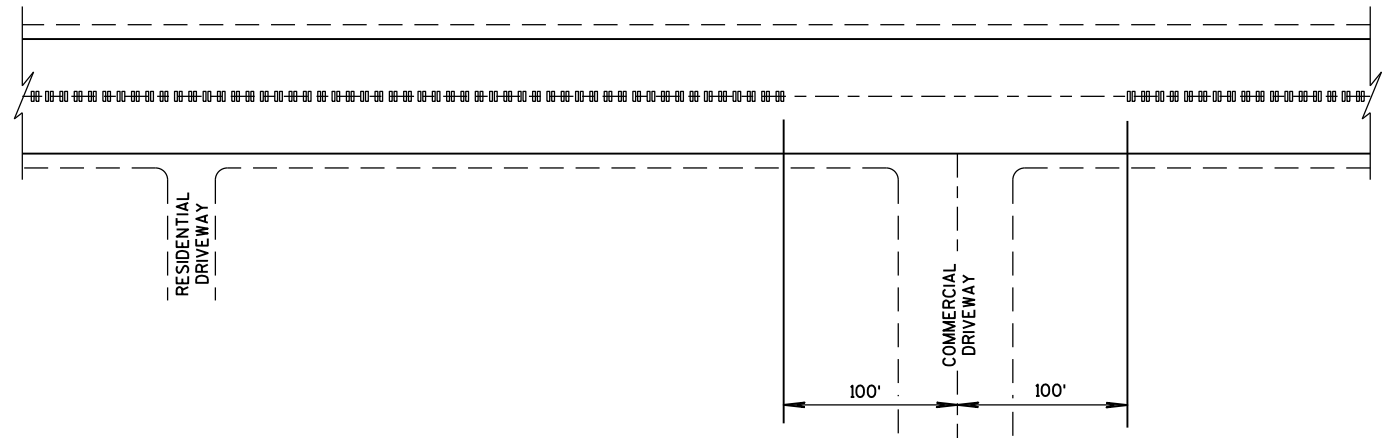
CENTER LINE GROOVES AT BRIDGES



CENTER LINE GROOVES AT INTERSECTIONS  
(WITH LEFT TURN LANES)



CENTER LINE GROOVES AT RAILROADS



CENTER LINE GROOVES AT DRIVEWAYS ①

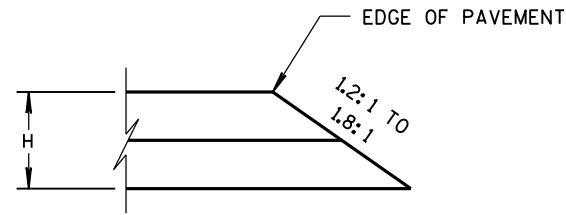
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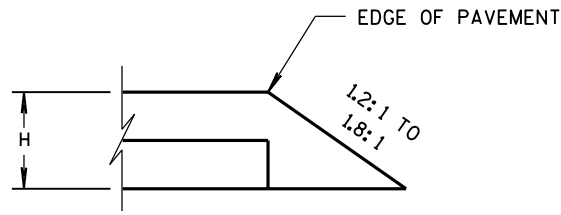
S.D.D. 13 A 11-2b

S.D.D. 13 A 11-2b

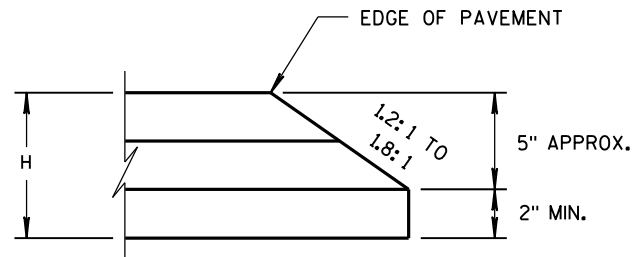
<b>2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 5/15/2013 DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



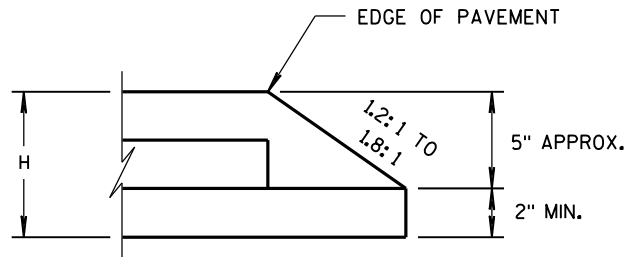
CONSTRUCTED WITH FINAL TWO LAYERS  
FOR H 5" OR LESS



CONSTRUCTED WITH FINAL LAYER  
FOR H 5" OR LESS

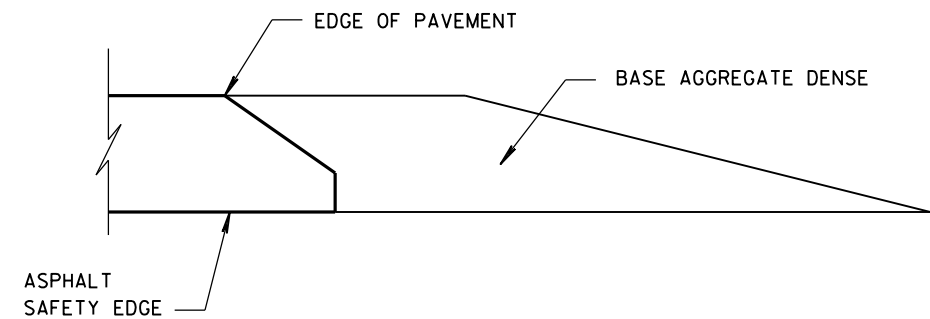


CONSTRUCTED WITH FINAL TWO LAYERS  
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER  
FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

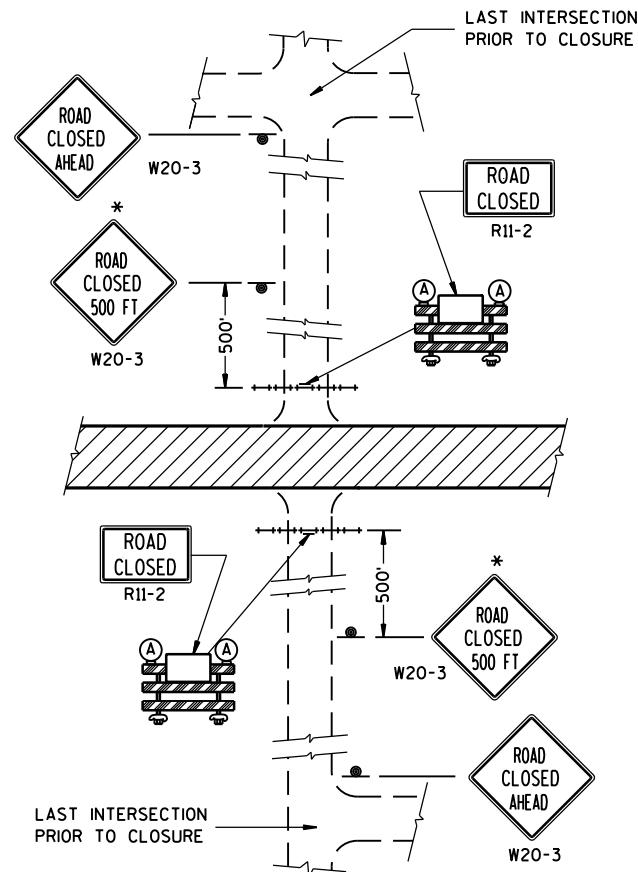
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6

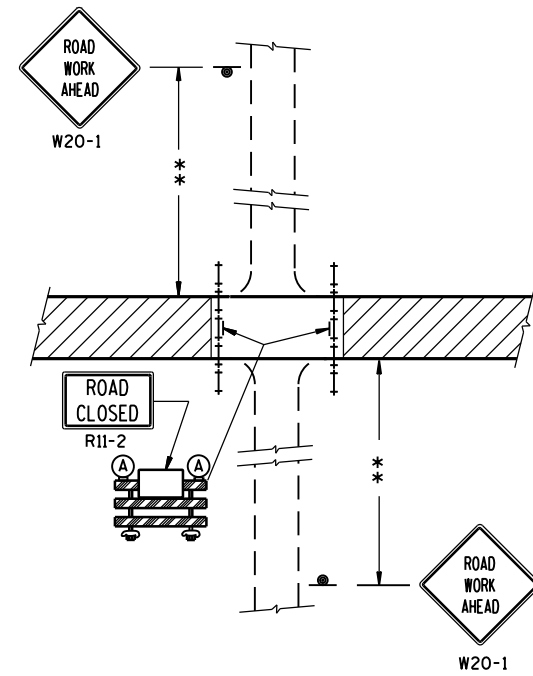
S.D.D. 14 B 29-1

S.D.D. 14 B 29-1

SAFETY EDGE <sub>SM</sub>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**DETAIL 1**  
(NO ACCESS TO PROJECT)



**DETAIL 2**  
(PUBLIC CROSS-TRAFFIC MAINTAINED.  
NO ACCESS TO PROJECT).

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

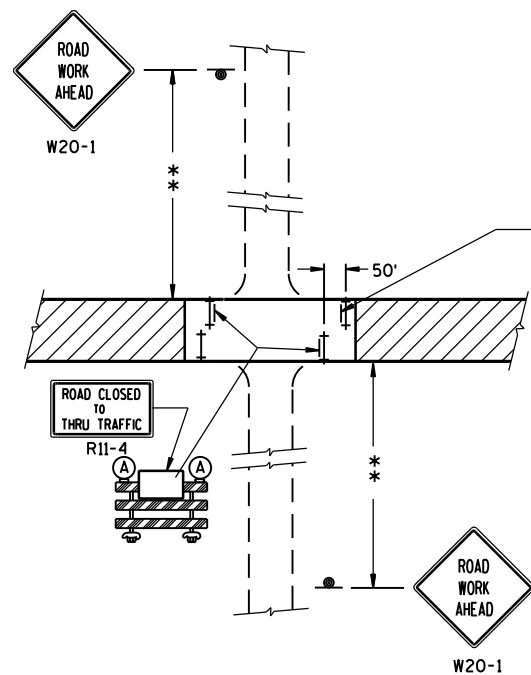
TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

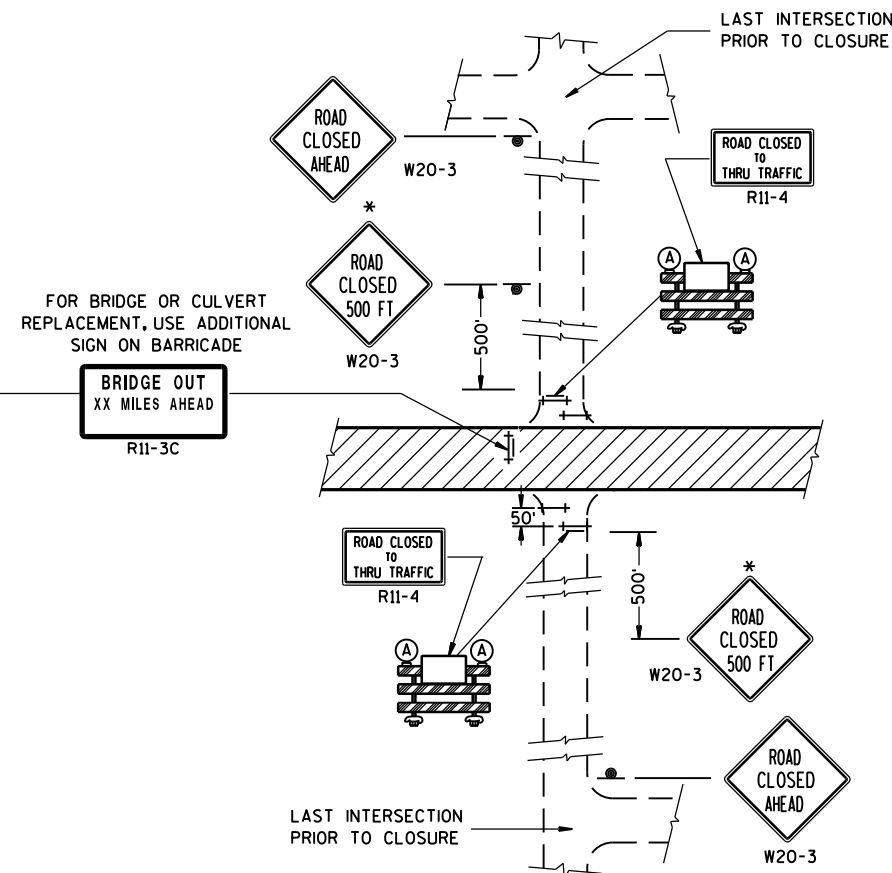
ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

R11-4 AND R11-3 SHALL BE 60" X 30".



**DETAIL 3**  
(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR,  
LOCAL BUSINESS AND RESIDENT ACCESS).



**DETAIL 4**  
(CONTRACTOR, LOCAL BUSINESS AND  
RESIDENT ACCESS TO PROJECT)

\*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

\*\*500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

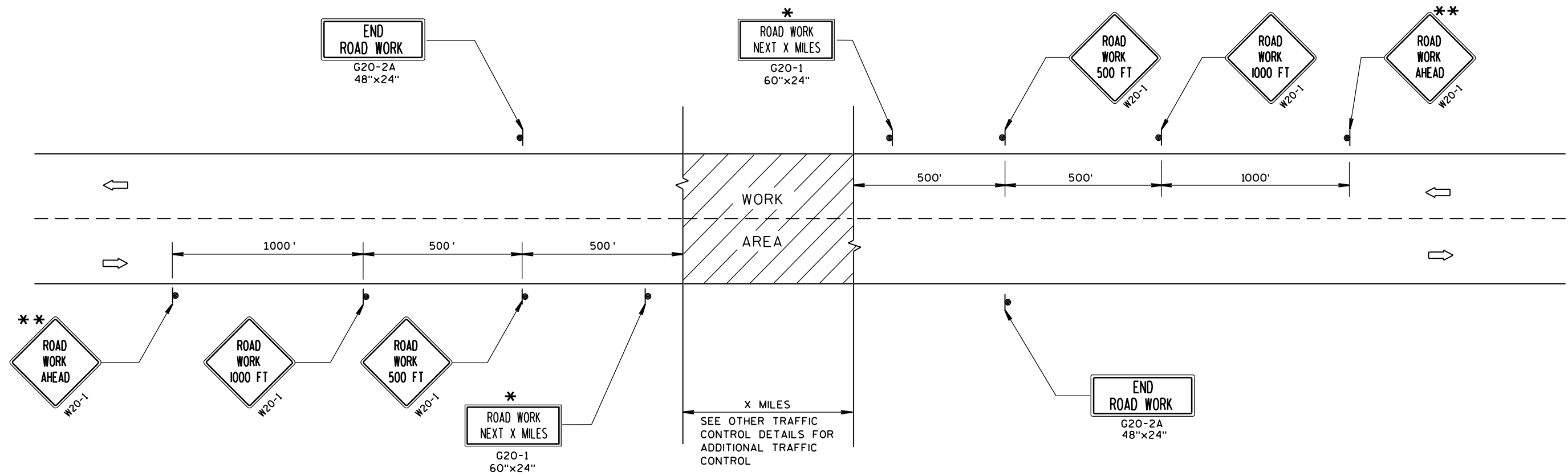
**LEGEND**

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- Ⓐ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS  
FOR  
SIDEROAD CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
8/2013 /S/ Travis Feltes  
DATE STATE TRAFFIC ENGINEER OF DESIGN  
FHWA



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

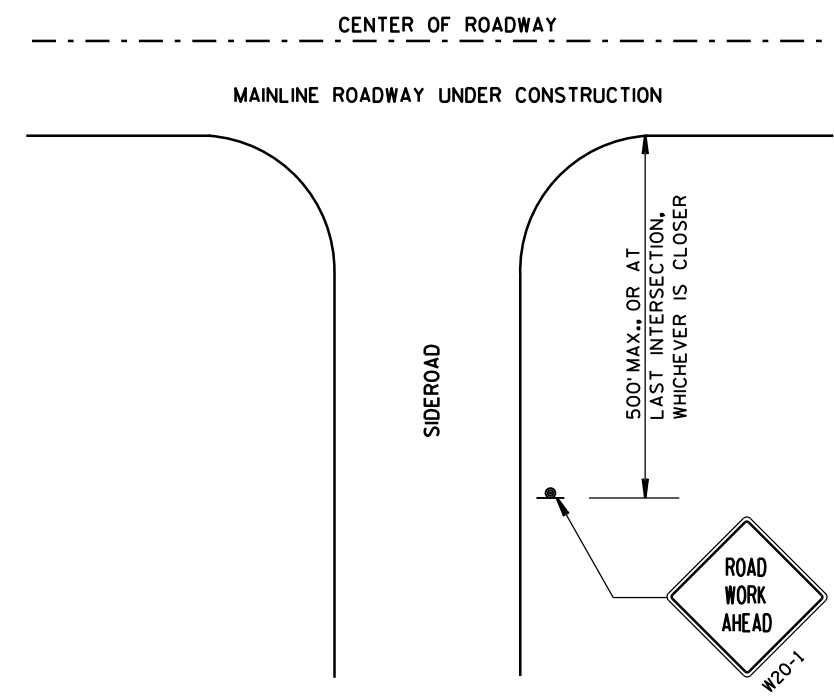
ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

\* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

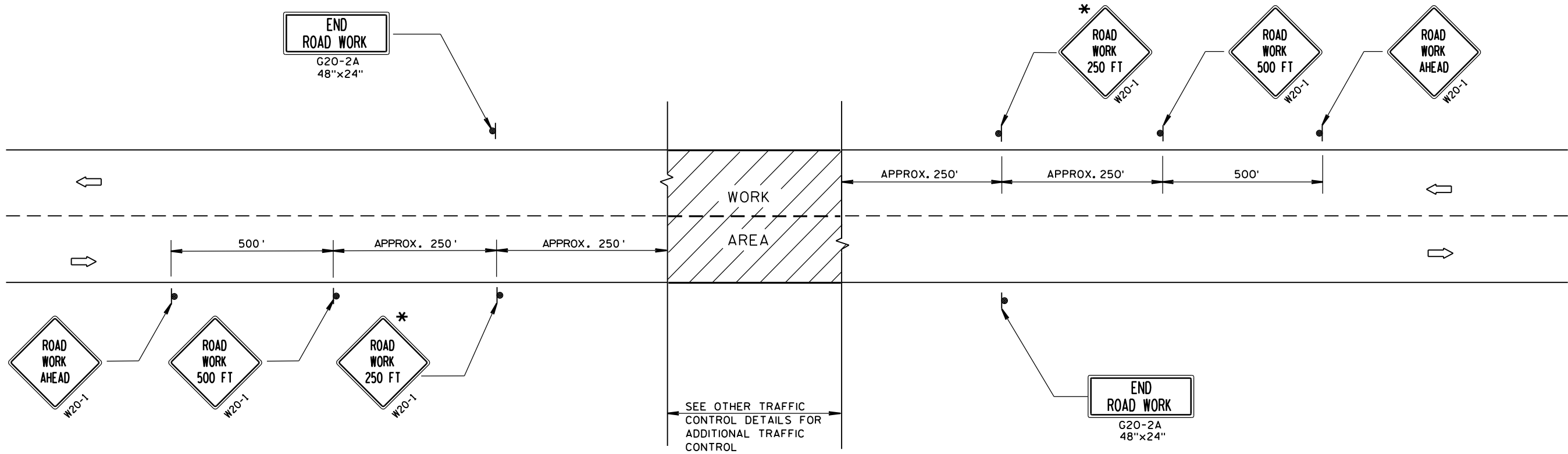
\*\* PLACE ADDITIONAL W20-1 "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

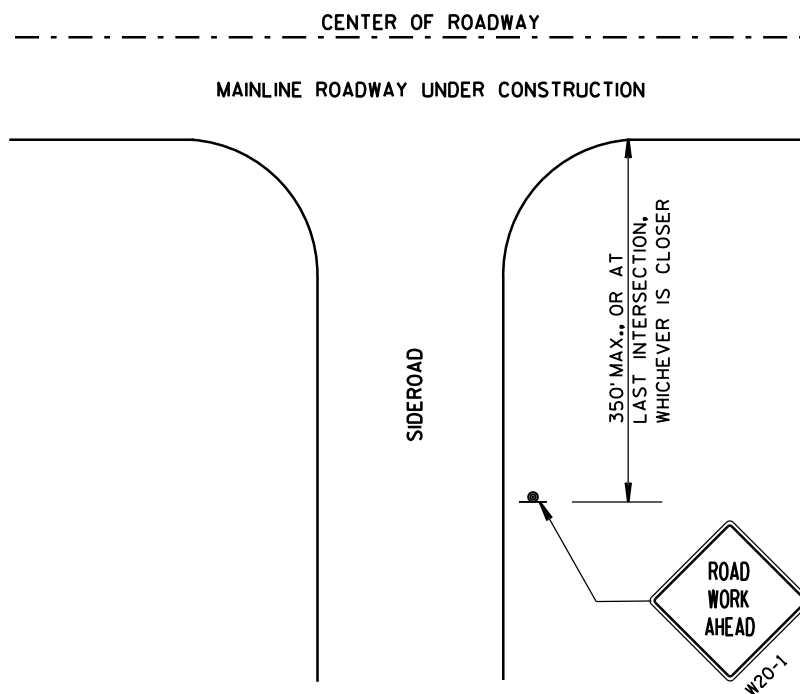
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

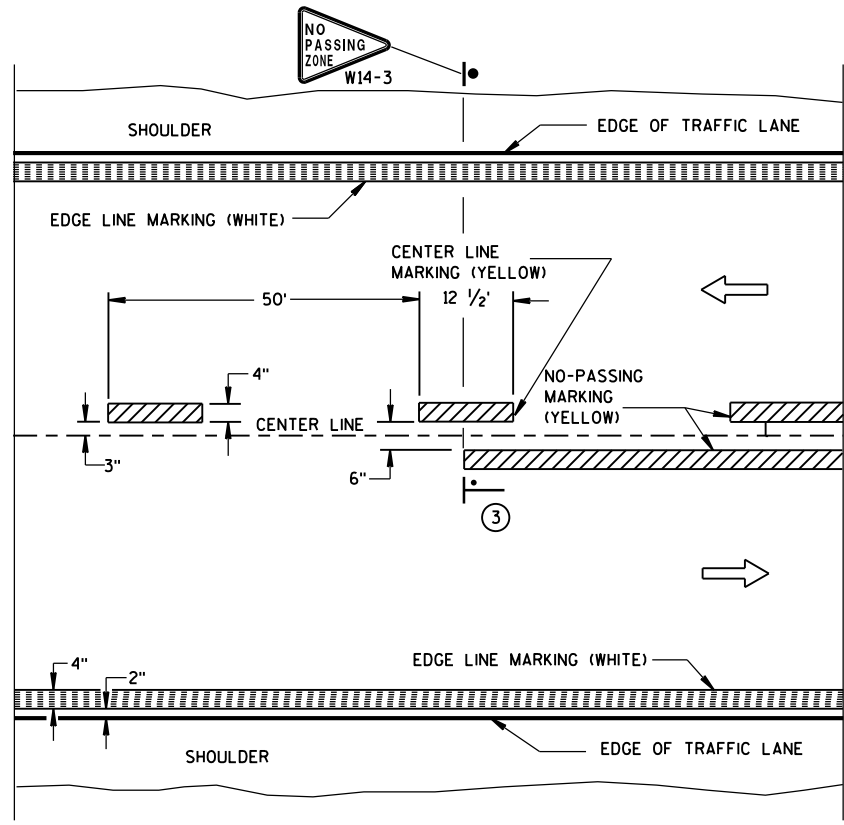
\* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



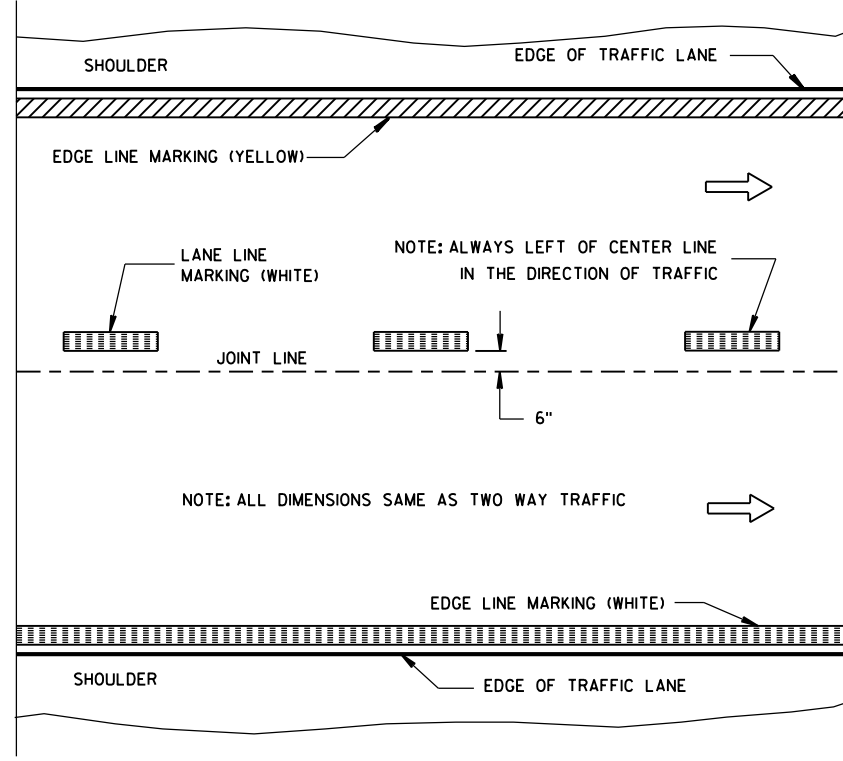
**LEGEND**

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

<b>TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	

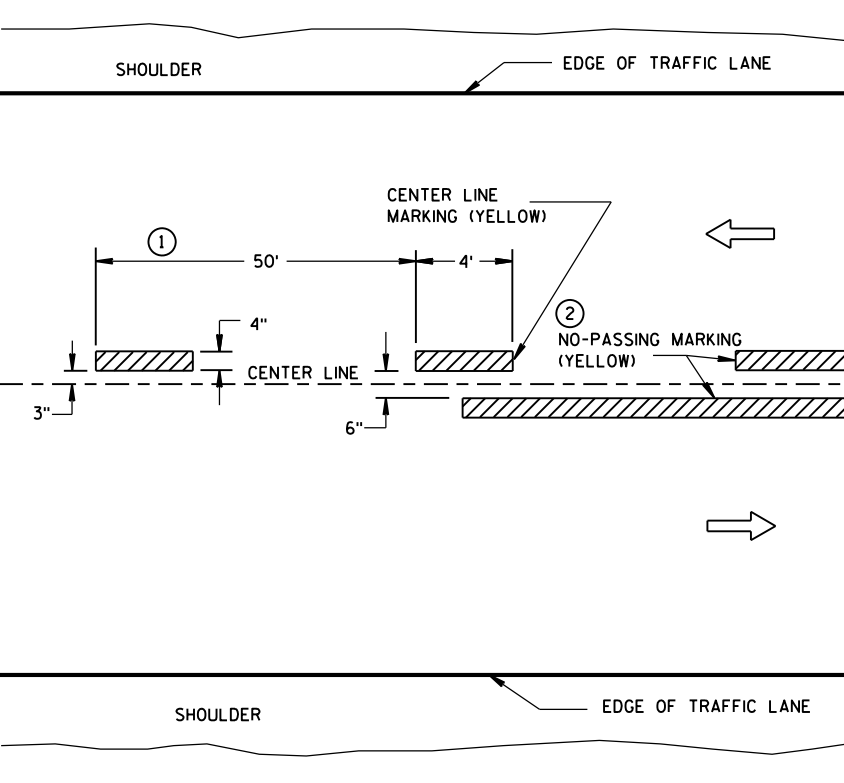


TWO WAY TRAFFIC

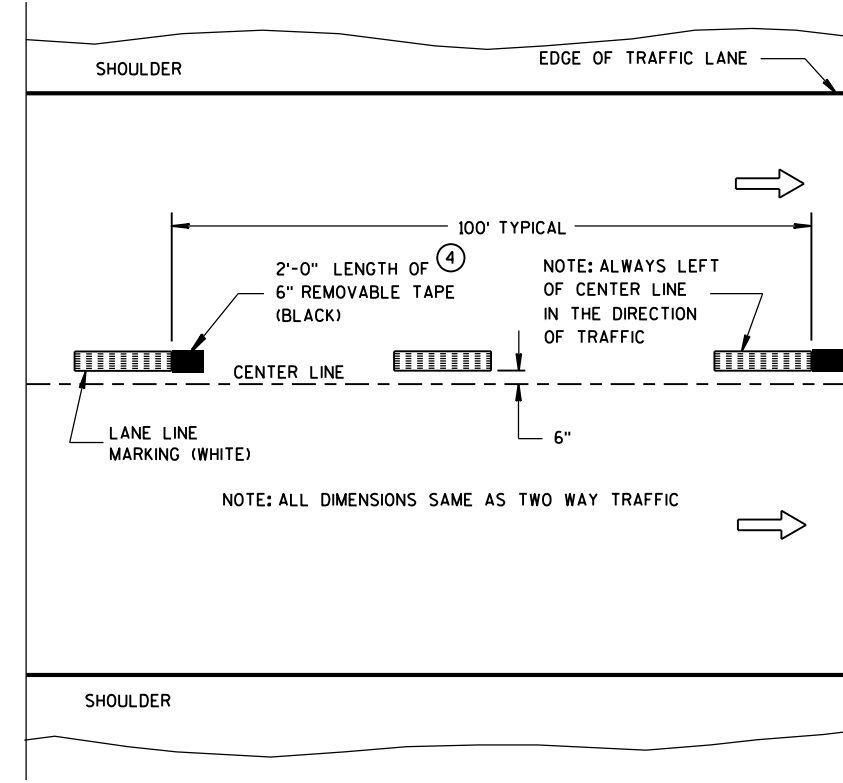


ONE WAY TRAFFIC

**PERMANENT PAVEMENT MARKING**



TWO WAY TRAFFIC



ONE WAY TRAFFIC

**TEMPORARY (INTERMEDIATE) PAVEMENT MARKING**  
(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

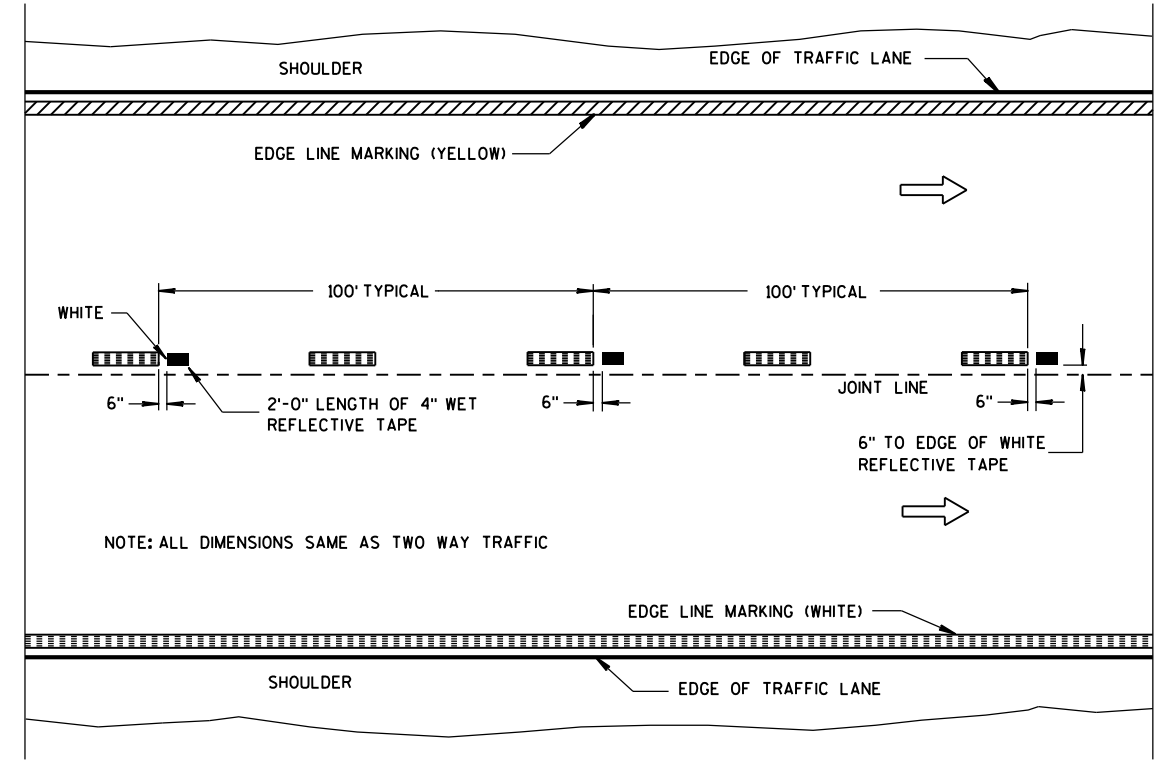
**GENERAL NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- ② NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- ③ NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
- ④ CONCRETE ONLY.

**NOTE**

ARROW SYMBOL ( → ) SHOWS DIRECTION OF TRAVEL



**WET REFLECTIVE TAPE SUPPLEMENT TO  
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE**

**LEGEND**

● "T" MARKING

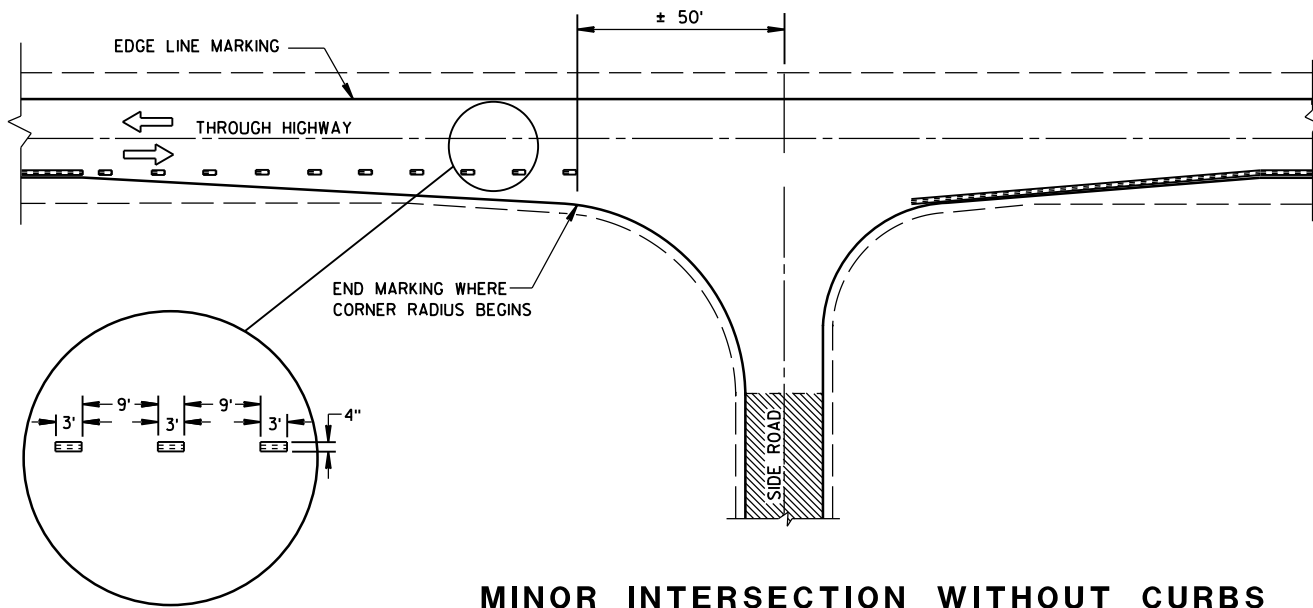
● POST MOUNTED SIGN

**PAVEMENT MARKING  
(MAINLINE)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
5-13-2013  
DATE  
FHWA

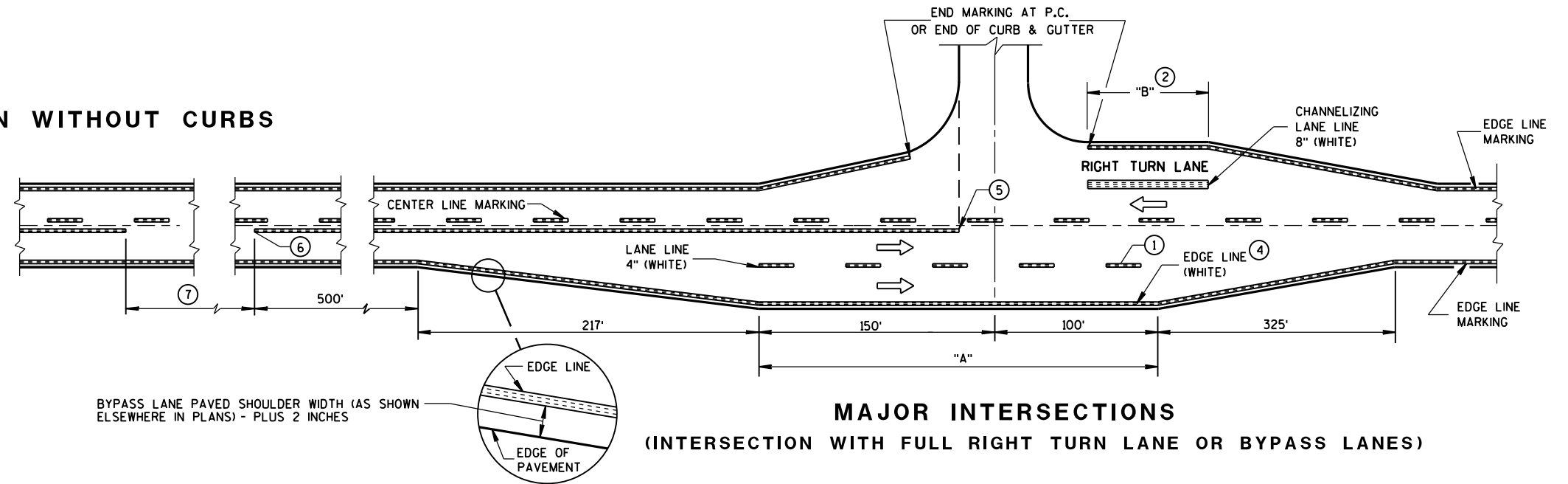
/S/ Travis Feltes  
STATE TRAFFIC ENGINEER



**MINOR INTERSECTION WITHOUT CURBS**

⑦

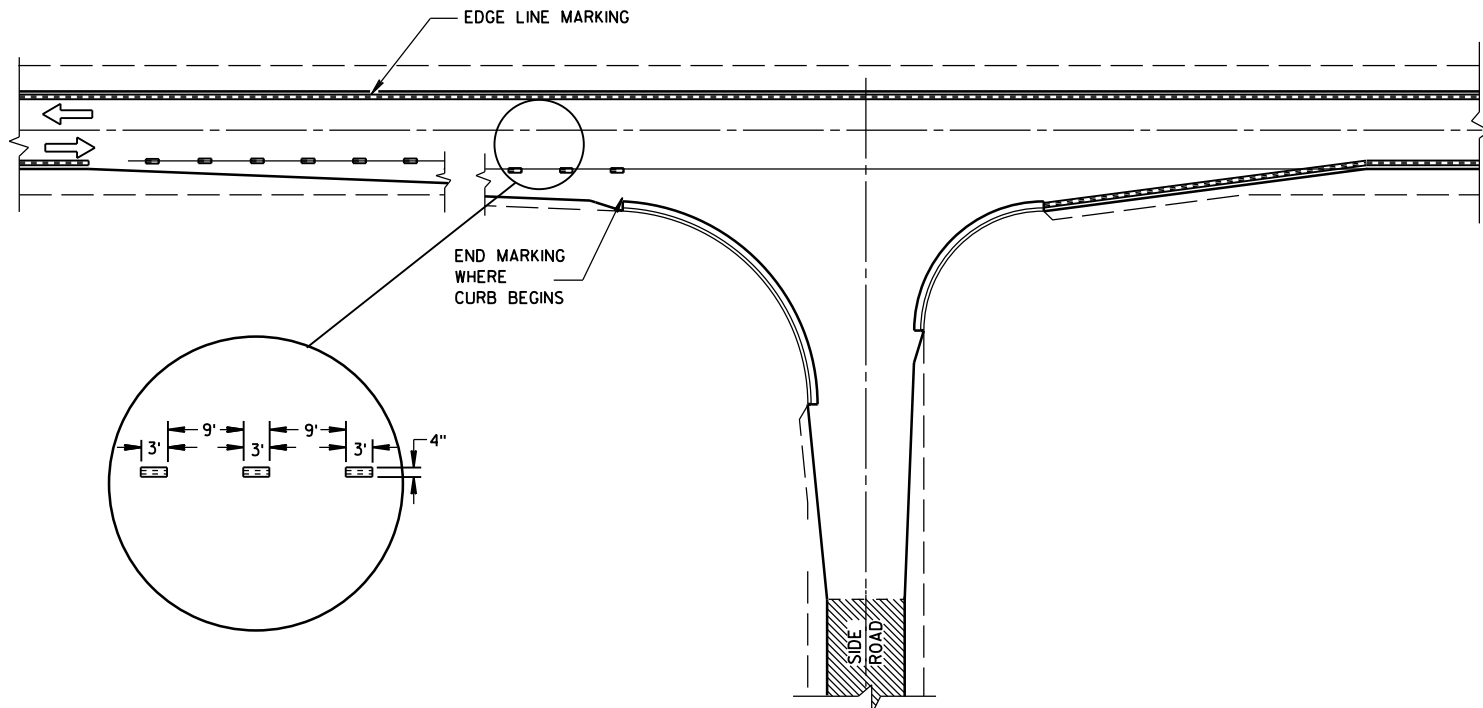
POSTED SPEED (MPH)	MINIMUM DISTANCE BETWEEN ZONES (FEET)
25 - 30	528
35 - 40	528
45 - 50	686
55	792



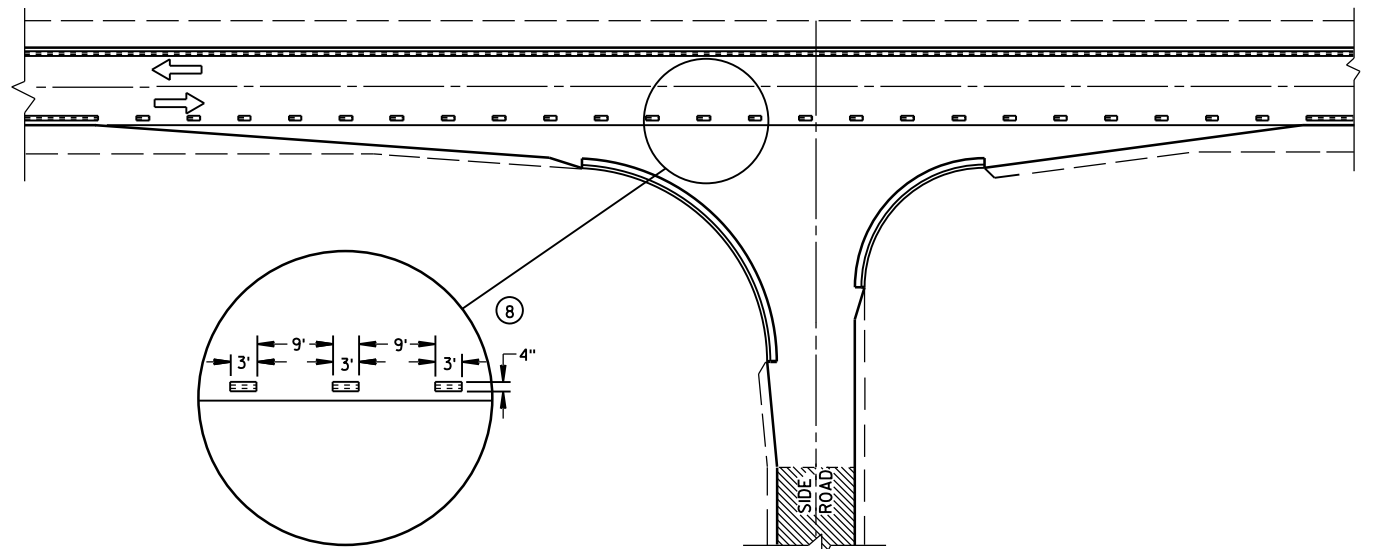
**MAJOR INTERSECTIONS  
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)**

**GENERAL NOTES**

- EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
  - ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
  - ③ ALTERNATIVE MARKING SHALL BE PROVIDED WHEN SPECIFIED IN THE CONTRACT. TYPICAL SITUATIONS WHERE THIS MARKING MAY BE REQUIRED ARE WHERE THE INTERSECTION IS ON A SHARP HORIZONTAL CURVE OR CREST VERTICAL CURVE IN AN UNLIGHTED AREA SUCH THAT THE EDGE LINE MAY BE MISLEADING TO THE MOTORIST OR DISAPPEAR FROM SIGHT.
  - ④ THE EDGE LINE IN THE TAPER AREAS OF THE BYPASS LANE AND THE BYPASS LANE SHALL BE LOCATED 1-FOOT FROM EDGE OF PAVEMENT TO THE OUTSIDE EDGE OF EDGE LINE.
  - ⑤ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.
  - ⑥ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
  - ⑦ IF THE DISTANCE BETWEEN 2 SUCCESSIVE NO-PASSING ZONES IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES, CONNECT THE 2 ZONES.
  - ⑧ 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ARROW SYMBOL ( → ) SHOWS DIRECTION OF TRAVEL



**MINOR INTERSECTION WITH CURBS  
(TYPICAL MARKING)**

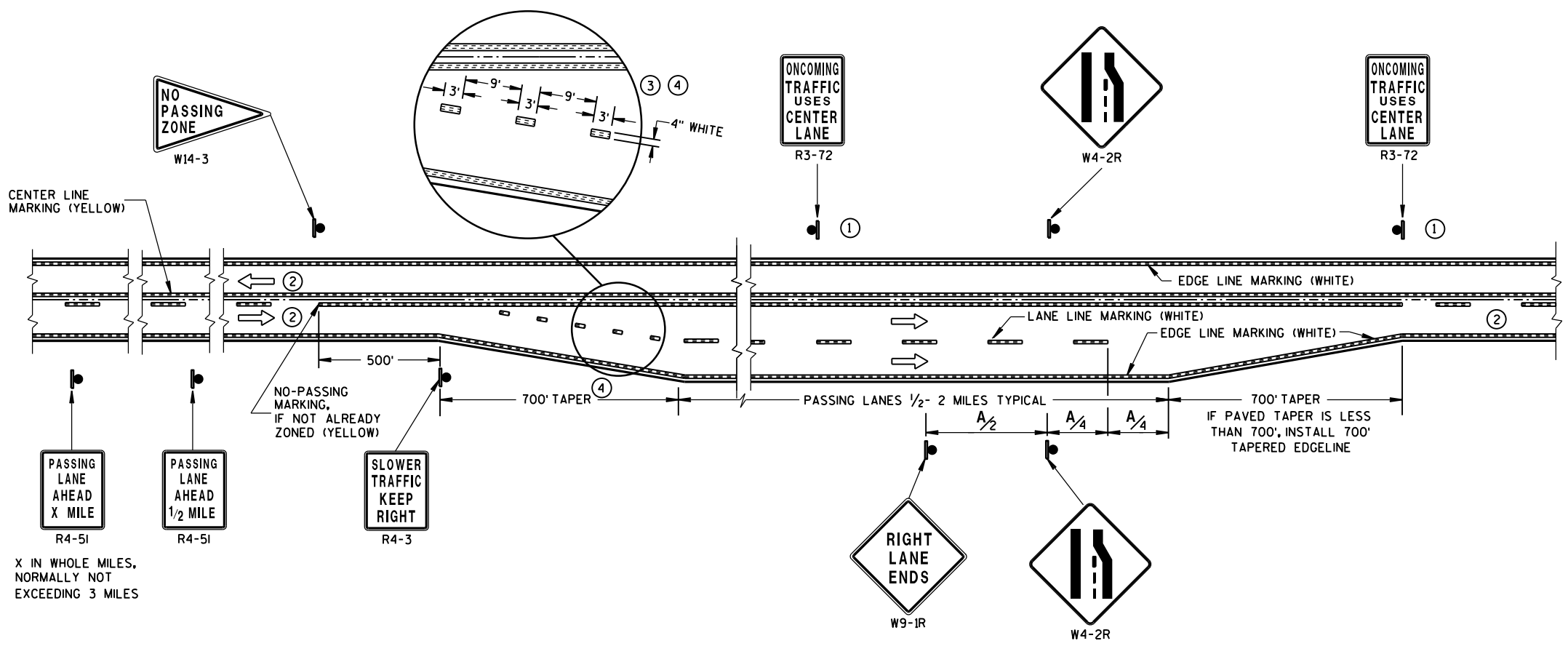


**MINOR INTERSECTION WITH CURBS  
③ (FOR SPECIAL CONDITIONS AS SPECIFIED)**

**PAVEMENT MARKING  
(INTERSECTIONS)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION





**SOLID DOUBLE-YELLOW LINE  
(THROUGHOUT ENTIRE PASSING/CLIMBING LANE)**

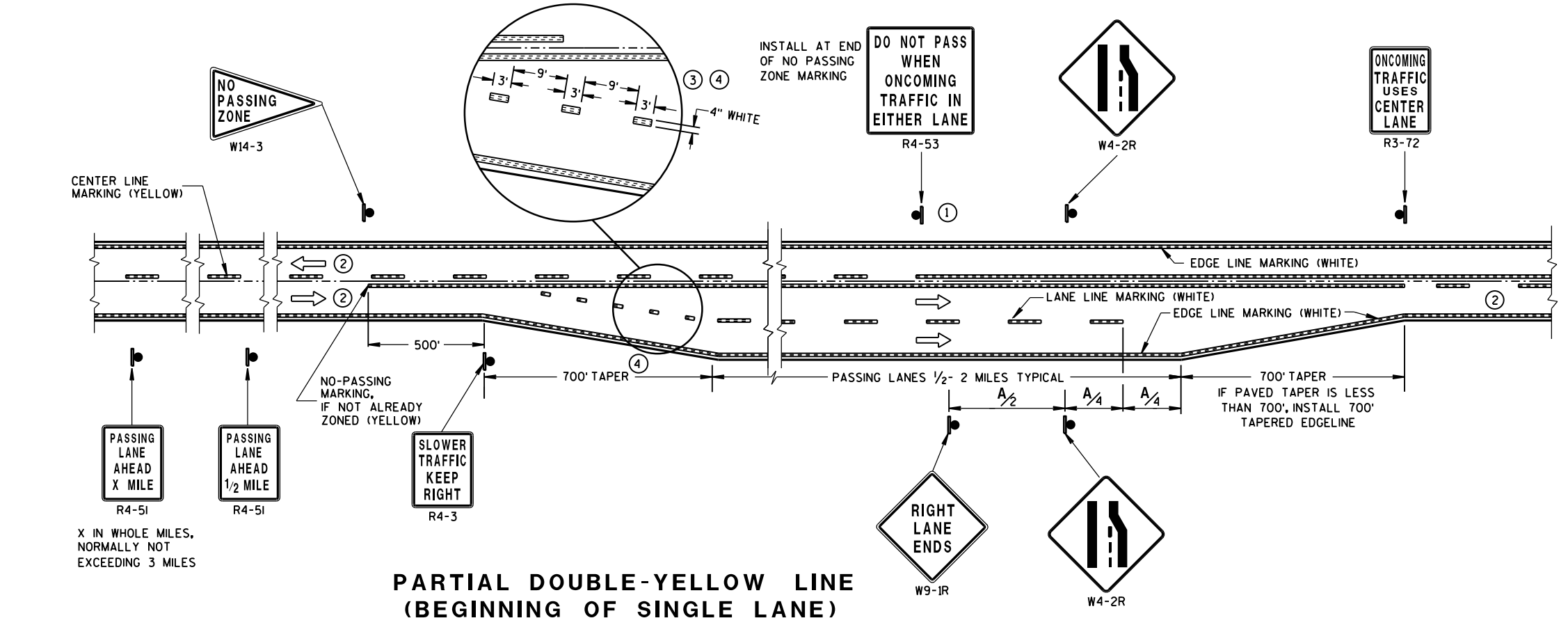
**GENERAL NOTES**

- ① SIGN SHALL BE REPEATED AT 1 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- ② THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- ③ THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ④ WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL (  $\Rightarrow$  ) SHOWS DIRECTION OF TRAVEL

**DISTANCE TABLE**

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	750
50	850
55	950



**PARTIAL DOUBLE-YELLOW LINE  
(BEGINNING OF SINGLE LANE)**

**PAVEMENT MARKING & SIGNING  
(CLIMBING LANE & PASSING LANE)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

NO PASSING ZONE  
W14-3

DO NOT PASS  
R4-1



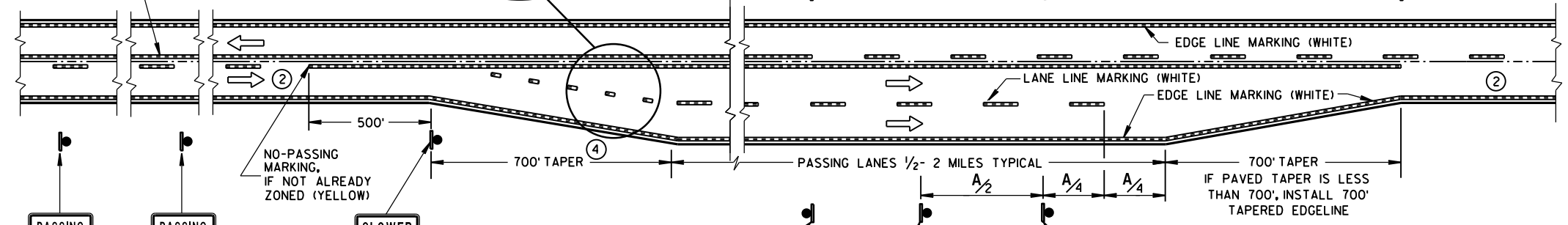
DO NOT PASS WHEN ONCOMING TRAFFIC IN EITHER LANE  
R4-53

**GENERAL NOTES**

- ① SIGN SHALL BE REPEATED AT 1/2 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- ② THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- ③ THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3" LINE 9" GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ④ WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL ( → ) SHOWS DIRECTION OF TRAVEL

CENTER LINE MARKING (YELLOW)



PASSING LANE AHEAD X MILE  
R4-51

PASSING LANE AHEAD 1/2 MILE  
R4-51

SLOWER TRAFFIC KEEP RIGHT  
R4-3

RIGHT LANE ENDS  
W9-1R



X IN WHOLE MILES, NORMALLY NOT EXCEEDING 3 MILES

**SOLID DOUBLE-YELLOW LINE (END OF SINGLE LANE)**

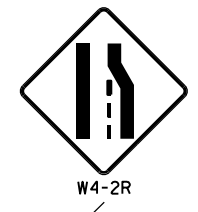
**DISTANCE TABLE**

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	750
50	850
55	950

6

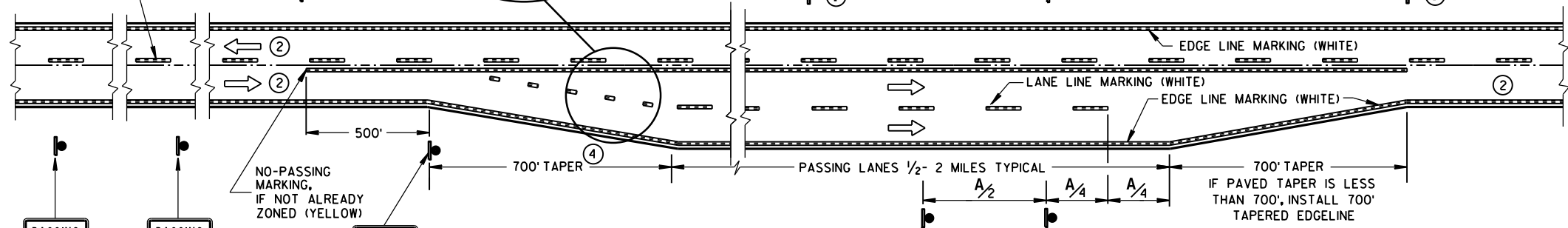
NO PASSING ZONE  
W14-3

DO NOT PASS WHEN ONCOMING TRAFFIC IN EITHER LANE  
R4-53



DO NOT PASS WHEN ONCOMING TRAFFIC IN EITHER LANE  
R4-53

CENTER LINE MARKING (YELLOW)



PASSING LANE AHEAD X MILE  
R4-51

PASSING LANE AHEAD 1/2 MILE  
R4-51

SLOWER TRAFFIC KEEP RIGHT  
R4-3

RIGHT LANE ENDS  
W9-1R



X IN WHOLE MILES, NORMALLY NOT EXCEEDING 3 MILES

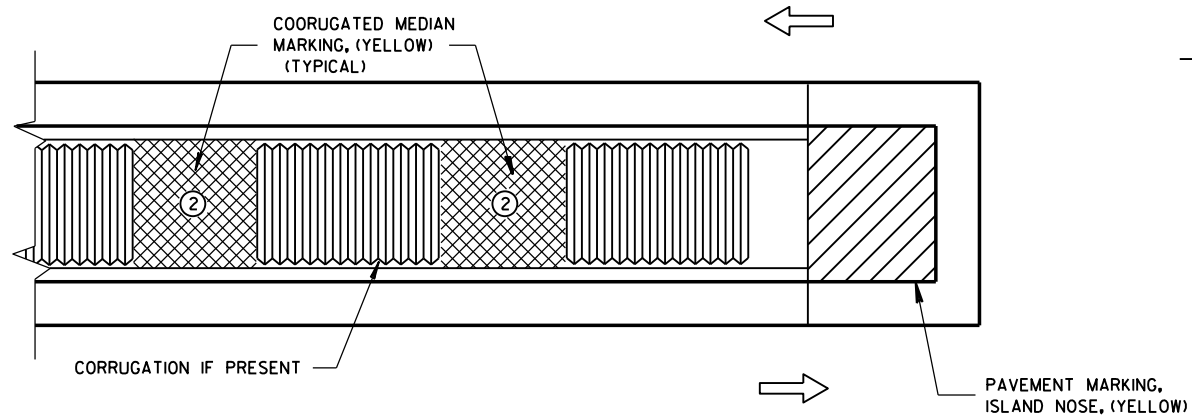
**DASHED YELLOW CENTERLINE (THROUGHOUT SINGLE LANE)**

**PAVEMENT MARKING & SIGNING (CLIMBING LANE & PASSING LANE)**

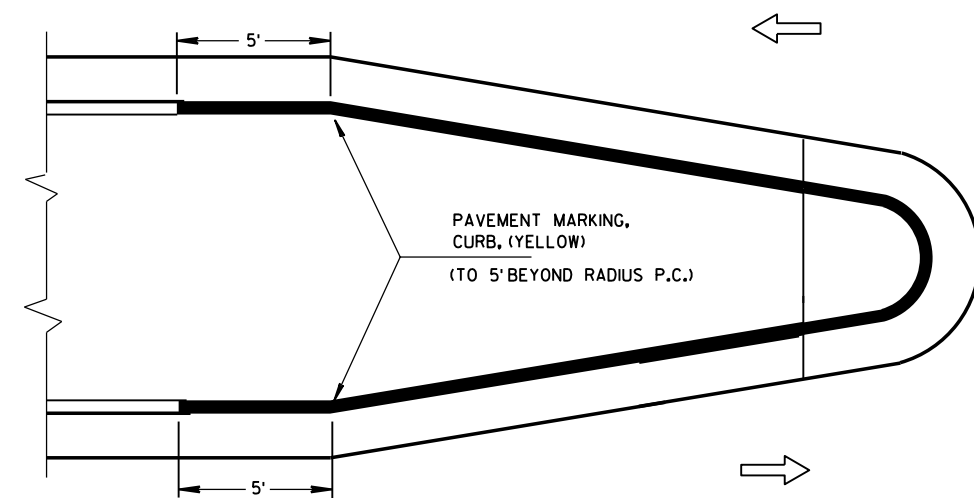
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

S.D.D. 15 C 8-16d

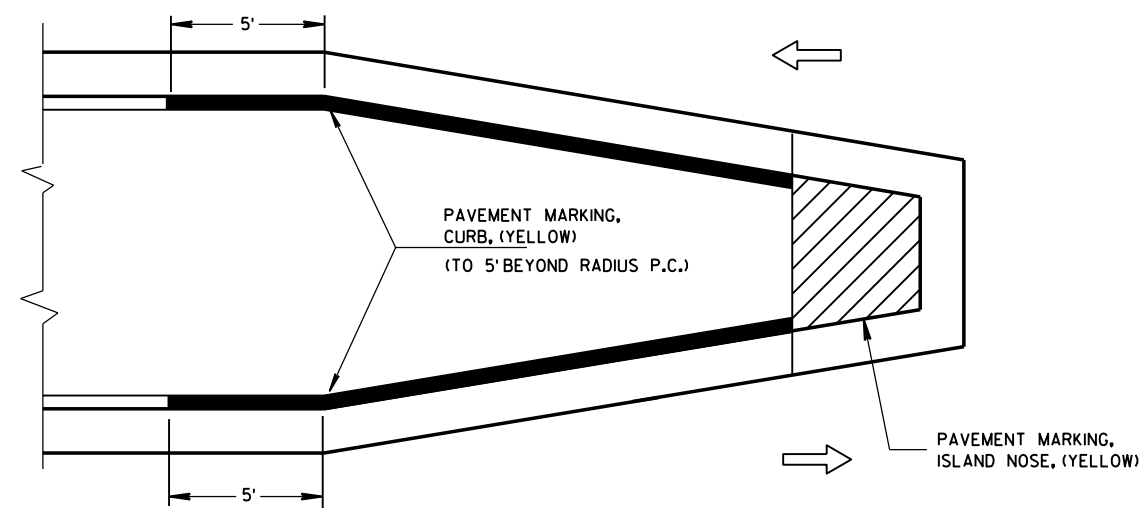
S.D.D. 15 C 8-16d



**MEDIAN ISLAND WITH SQUARE BLUNT NOSE**

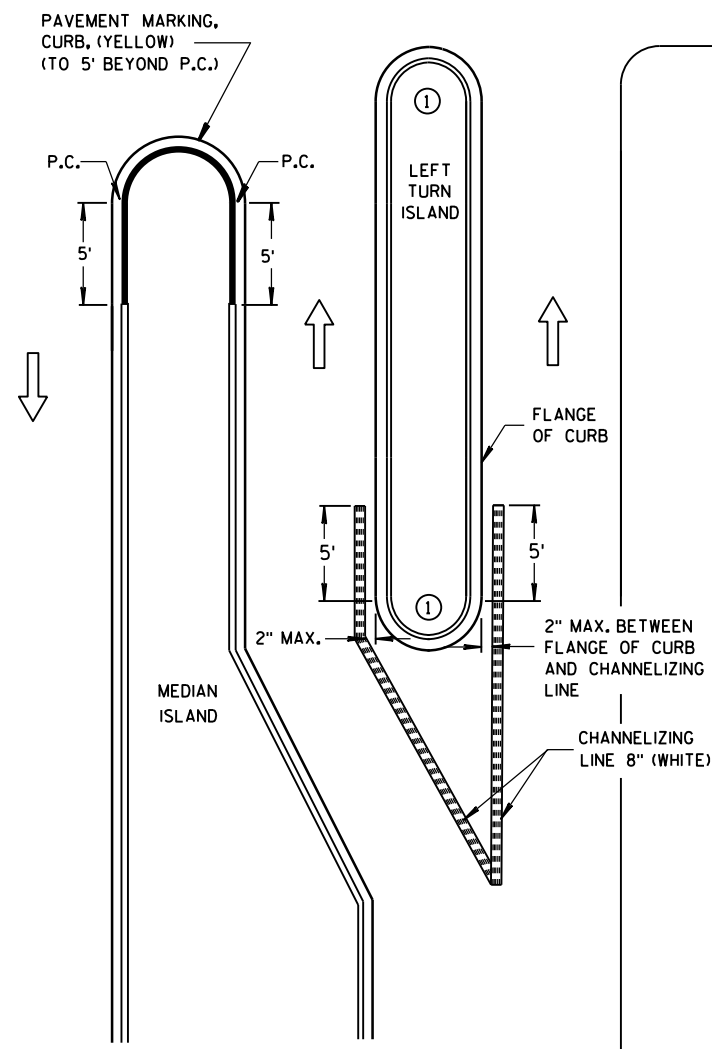


**MEDIAN ISLAND WITH ROUND BLUNT NOSE**



**MEDIAN ISLAND WITH SLOPED NOSE**

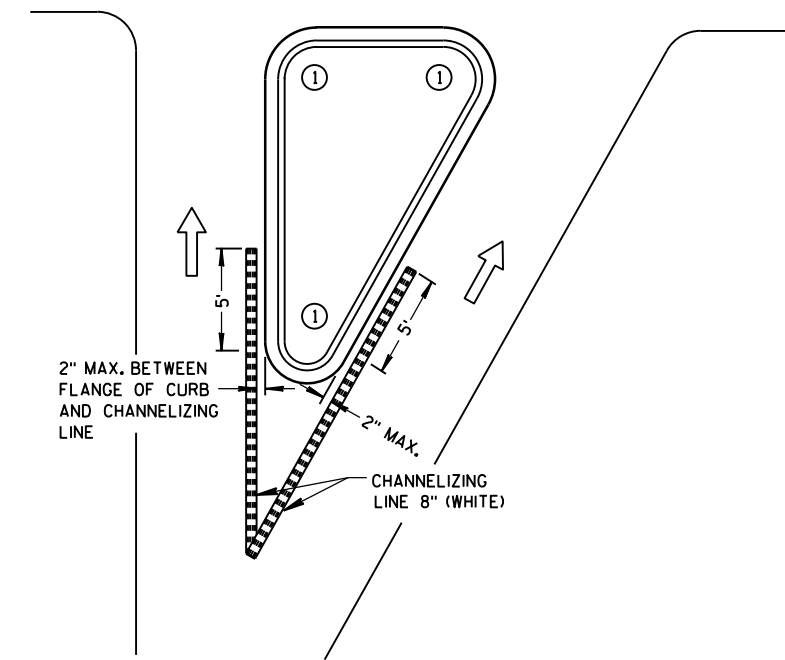
**TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS**



**LEFT TURN & MEDIAN ISLAND**





**GENERAL NOTES**

- ① DO NOT MARK CURB NOSES THAT SEPARATE LANES OF TRAFFIC TRAVELING IN THE SAME DIRECTION.
- ② WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.



**RIGHT TURN ISLAND**





**LEGEND**

-  ISLAND NOSE MARKING
-  CURB MARKING
-  CORRUGATED MEDIAN MARKING
-  DIRECTION OF TRAVEL

**PAVEMENT MARKING (ISLANDS)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

**LEGEND**

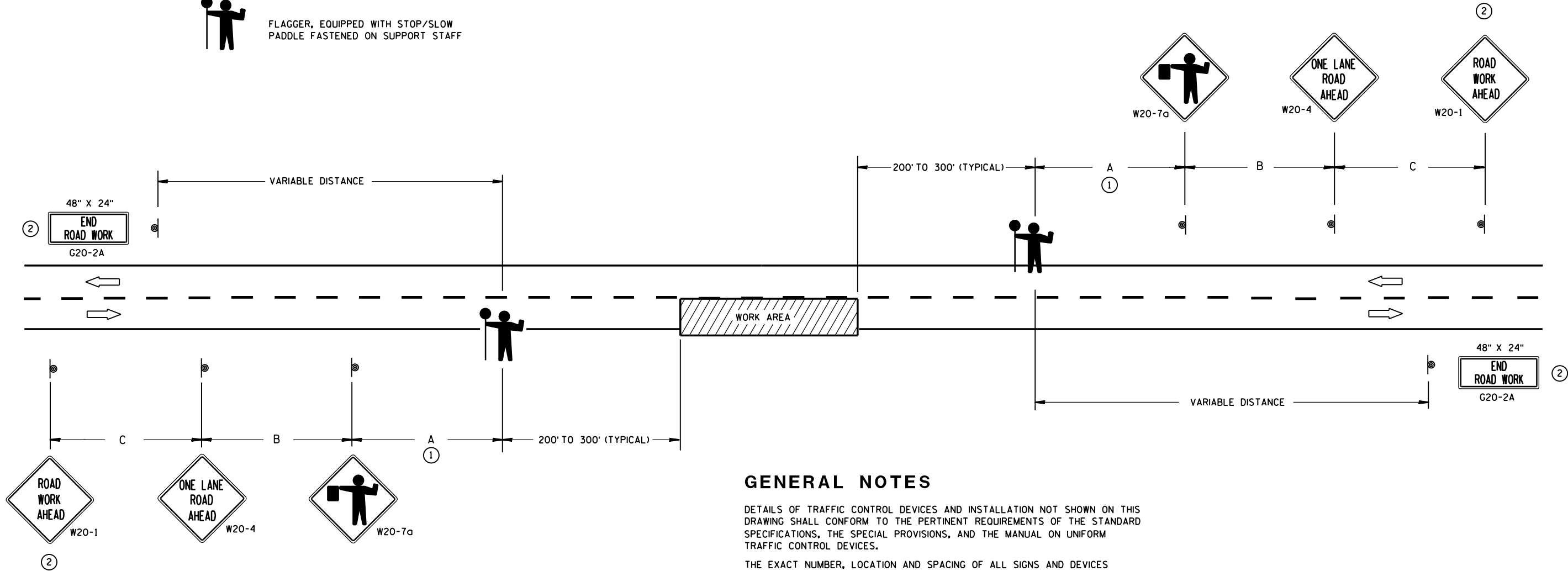
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

**SIGN SPACING TABLE**

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



**GENERAL NOTES**

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

- ① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

**TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
8/2013 /S/ Travis Feltes  
DATE STATE TRAFFIC ENGINEER OF DESIGN  
FHWA

### GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

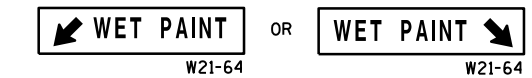
THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.



③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.

④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

### LEGEND

V1 LEAD VEHICLE

V2 SHADOW VEHICLE

V3 TRAIL VEHICLE WITH TMA

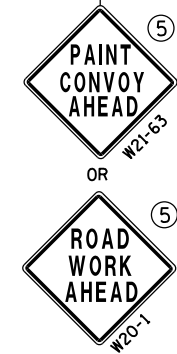
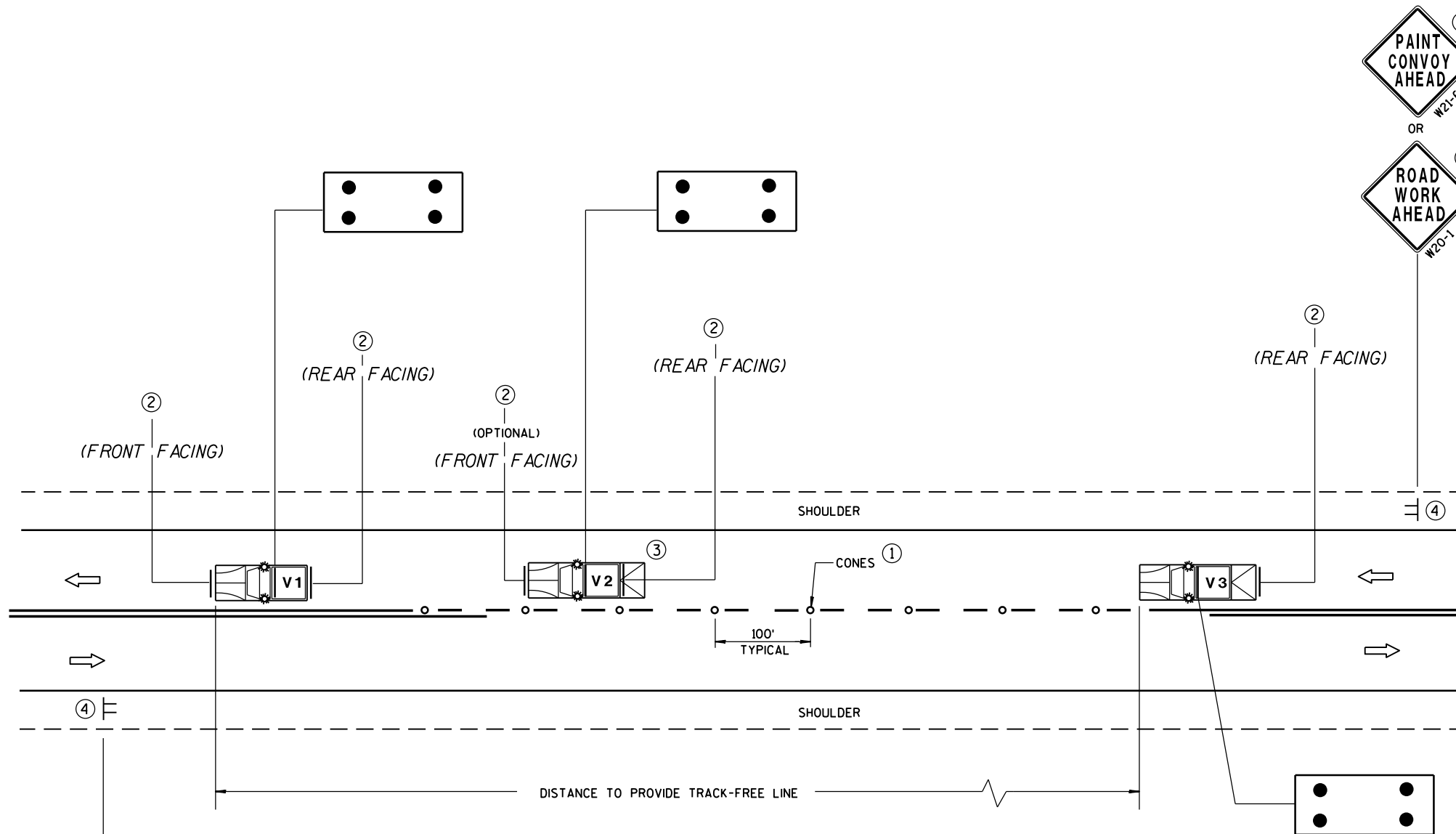
TMA TRUCK-MOUNTED ATTENUATOR

Sign on temporary support

DIRECTION OF TRAFFIC

CONES

FLASHING ARROW PANEL (CAUTION)

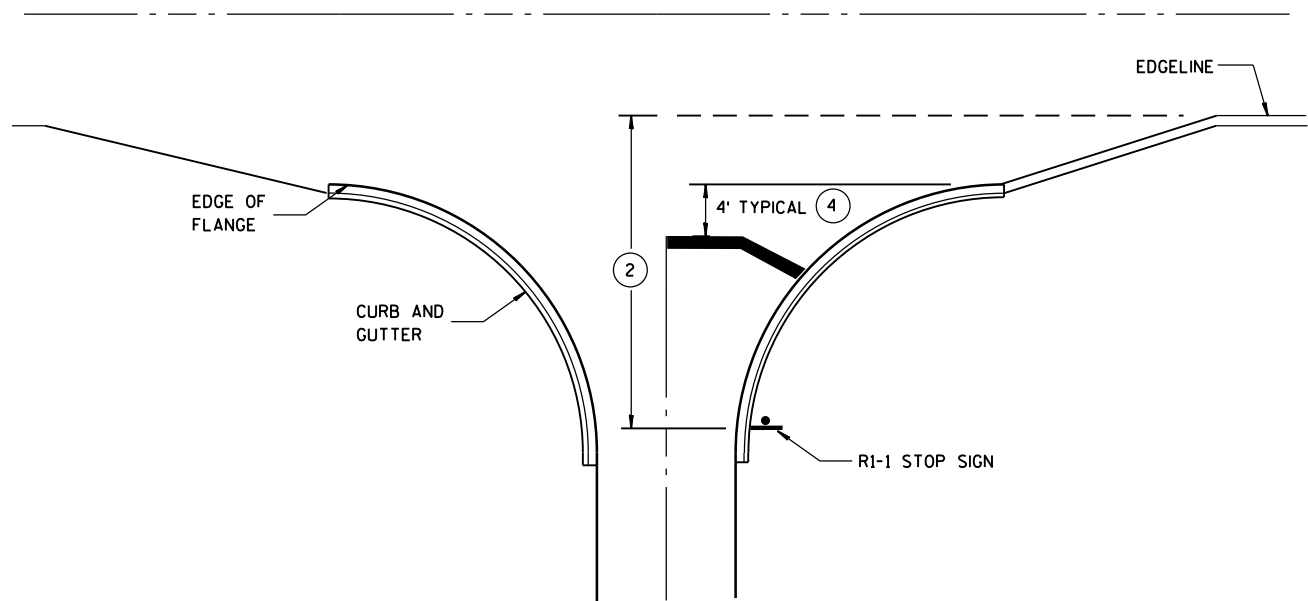


## MOVING PAVEMENT MARKING OPERATIONS TWO-LANE TWO-WAY ROADWAY

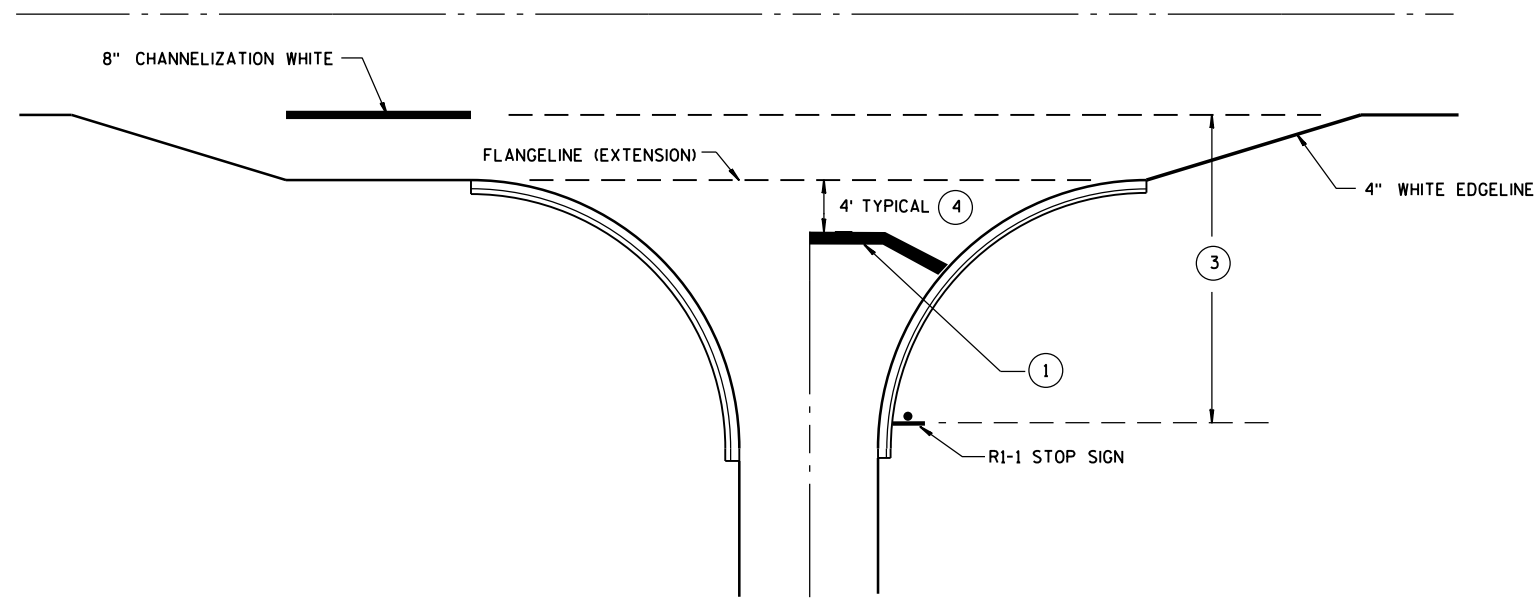
MOVING PAVEMENT MARKING  
OPERATION  
TWO-LANE TWO-WAY ROADWAY

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

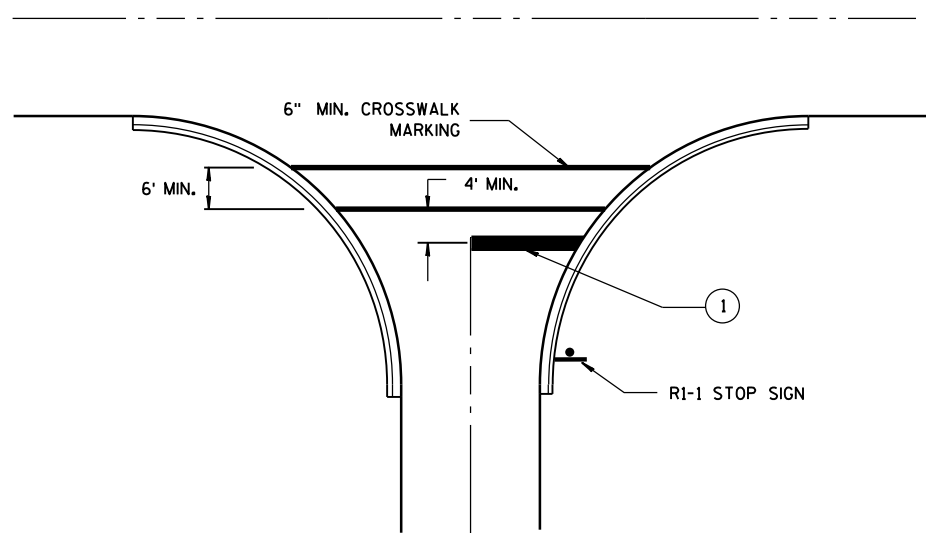
APPROVED  
5/3/2013 /S/ Travis Feltes  
DATE STATE TRAFFIC ENGINEER  
FHWA



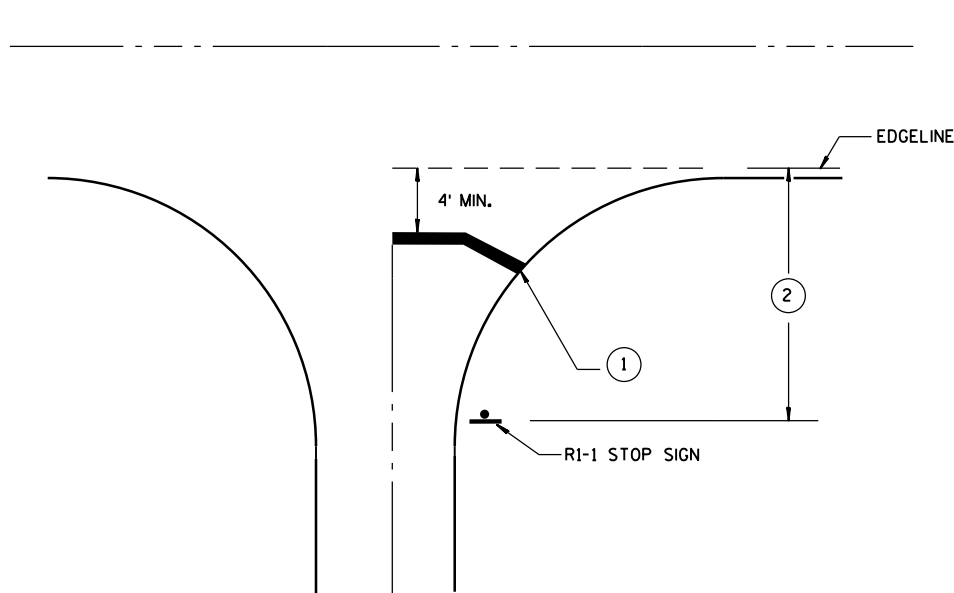
**TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER**



**TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE**



**TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING**



**TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER**

**GENERAL NOTES**

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
- ③ IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- ④ MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES.

**STOP LINE AND CROSSWALK PAVEMENT MARKING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE 4/30/2013 /S/ Travis Feltz  
STATE TRAFFIC ENGINEER  
FHWA

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

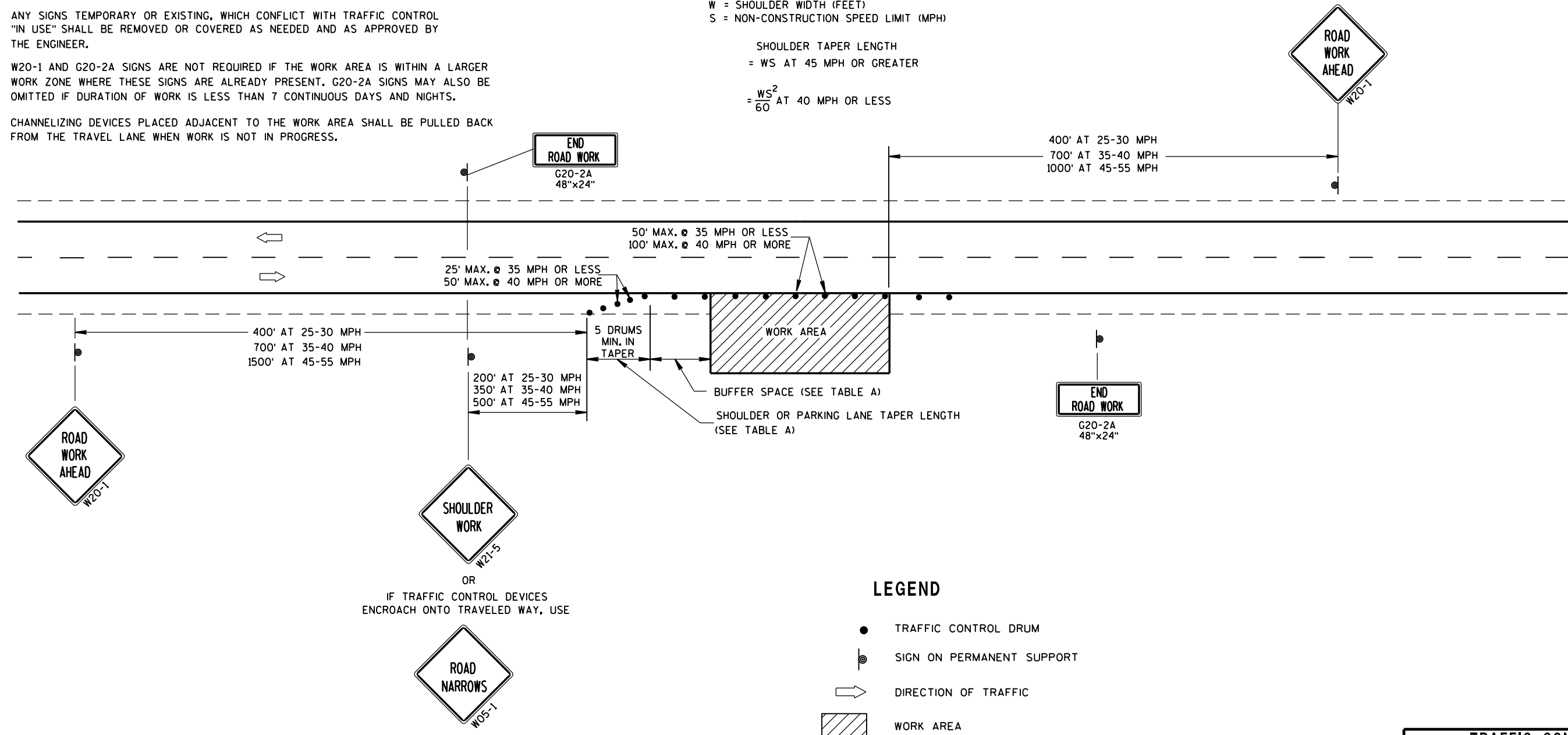
CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

**TABLE A**

S	SHOULDER TAPER LENGTH (FEET)				BUFFER SPACE (FEET)
	4	6	8	10	
30	20	30	40	50	85
35	30	45	55	70	120
40	40	55	75	90	170
45	60	90	120	150	220
50	70	100	135	170	280
55	75	110	150	185	335

W = SHOULDER WIDTH (FEET)  
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

SHOULDER TAPER LENGTH  
= WS AT 45 MPH OR GREATER  
=  $\frac{WS^2}{60}$  AT 40 MPH OR LESS



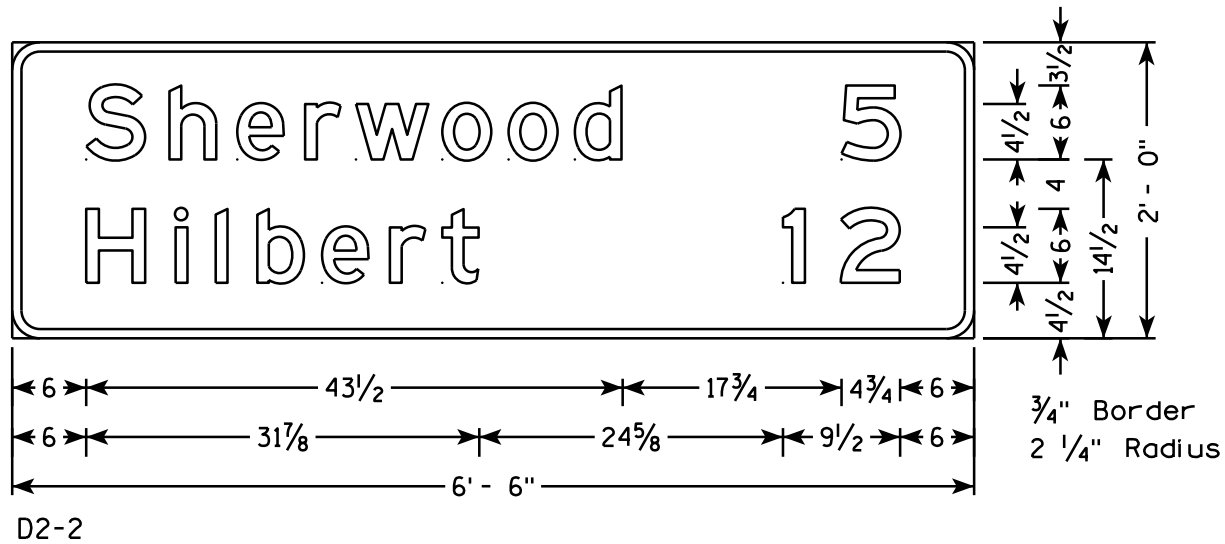
**LEGEND**

- TRAFFIC CONTROL DRUM
- ⊙ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA

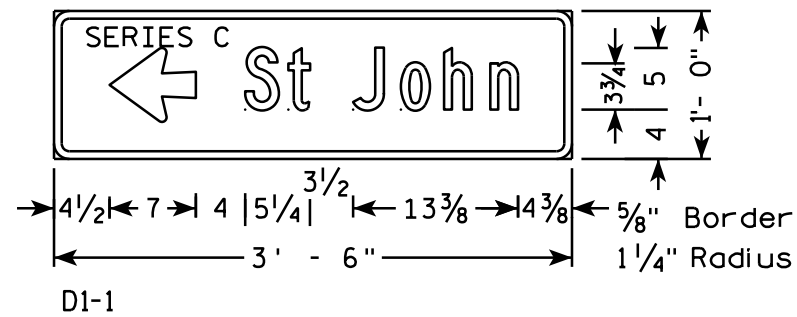
<b>TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	

6

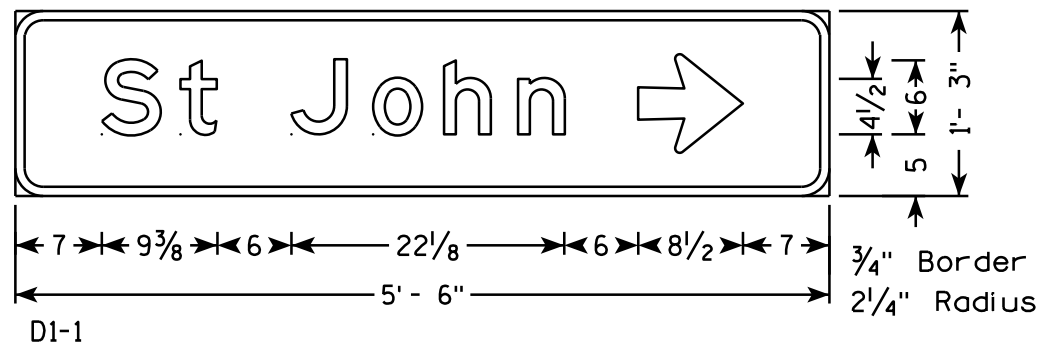
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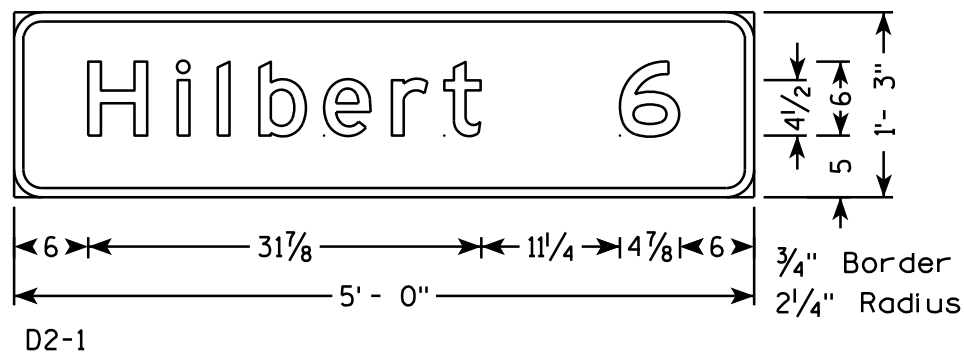
D2-2



D1-1

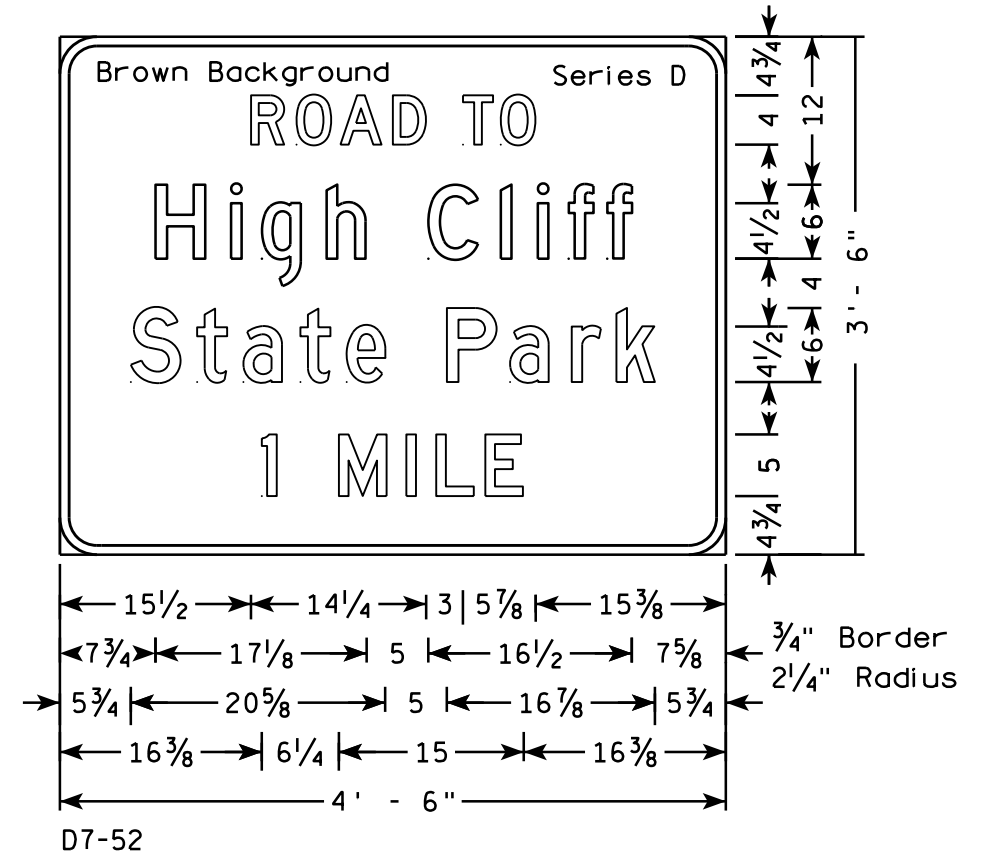


D1-1

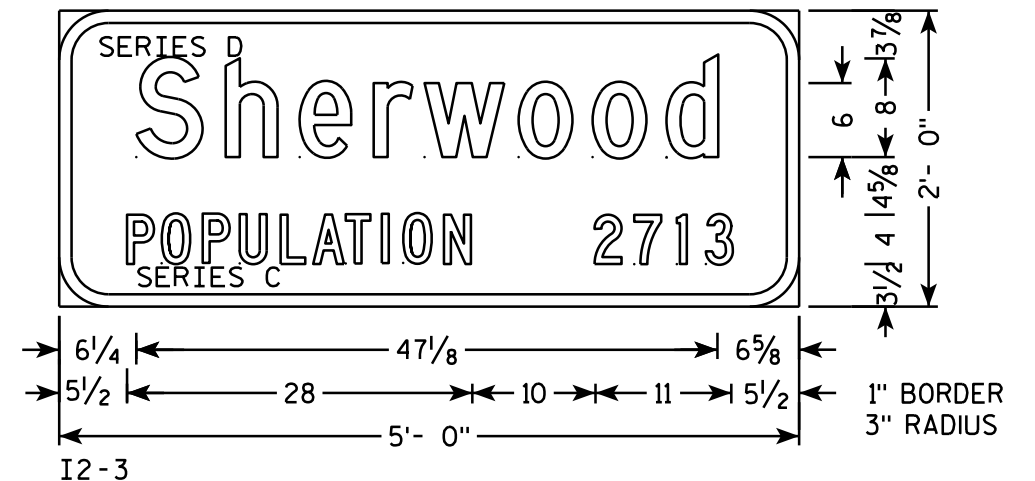


D2-1

- NOTES**
1. All Signs Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
  2. Color:  
Background - GREEN except as Shown  
Message - WHITE
  3. Message Series - E except as Shown

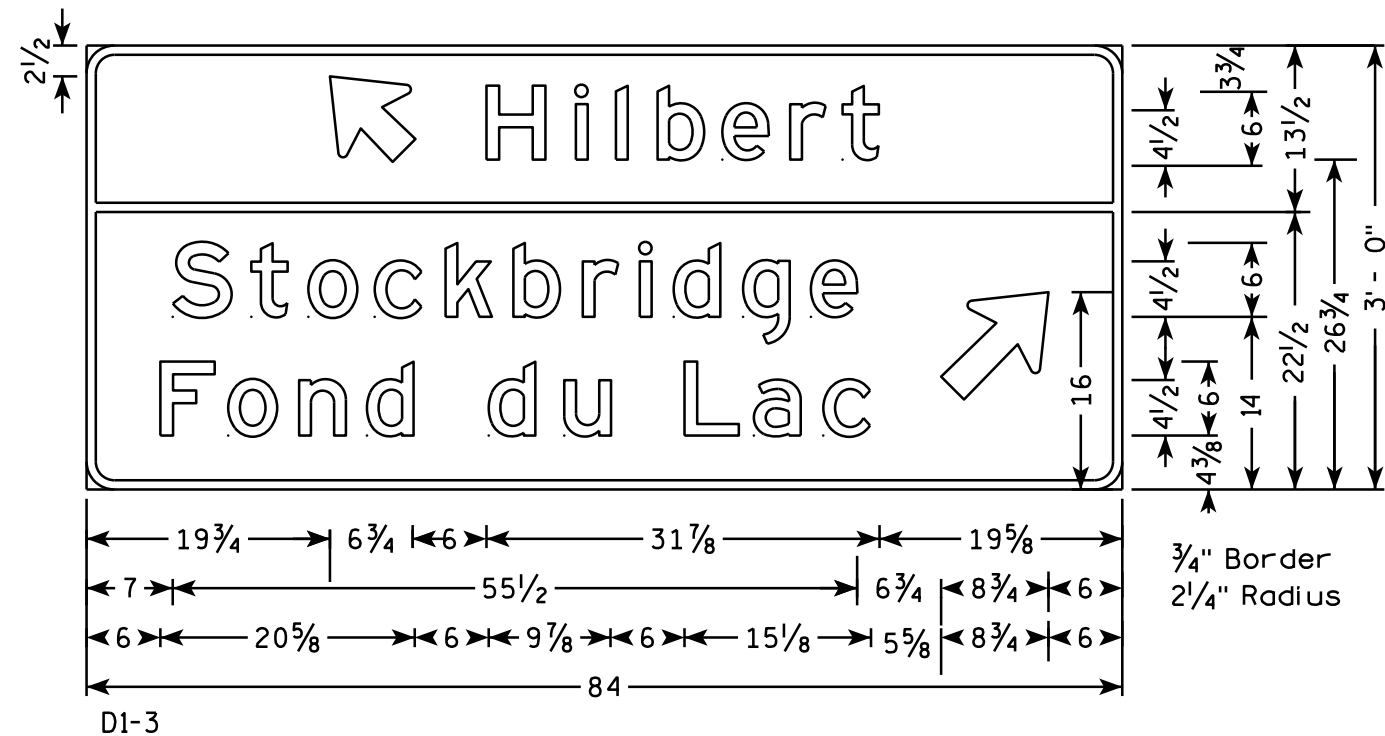


D7-52



I2-3

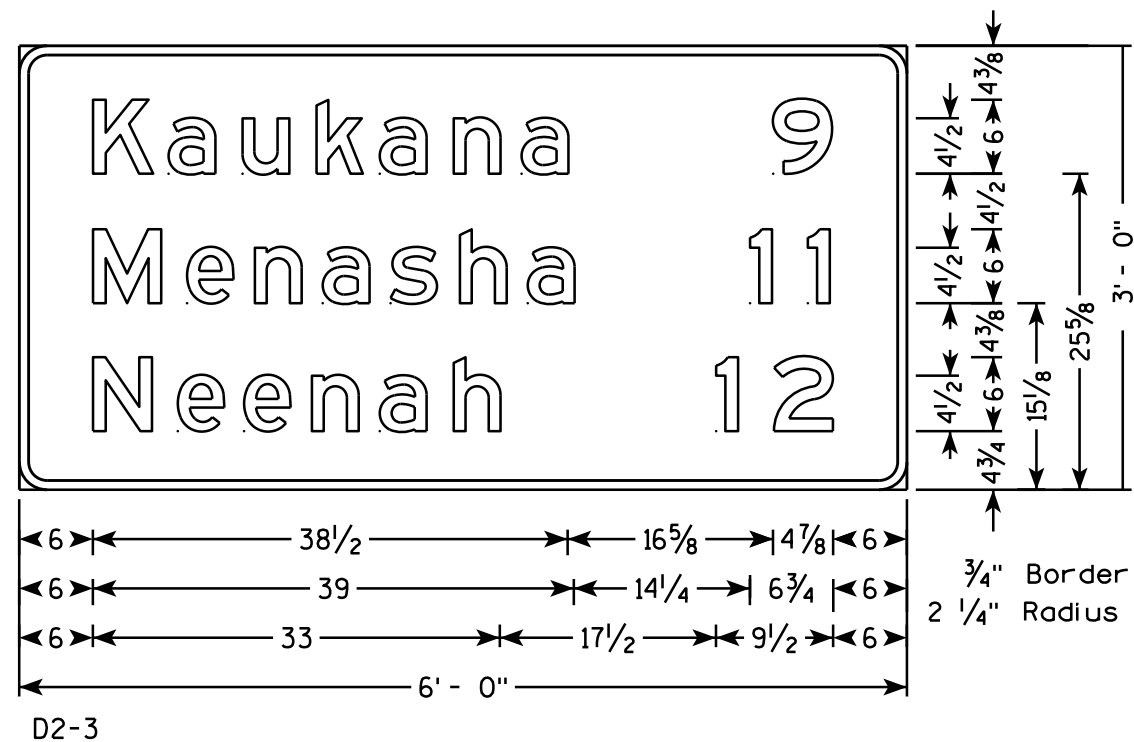
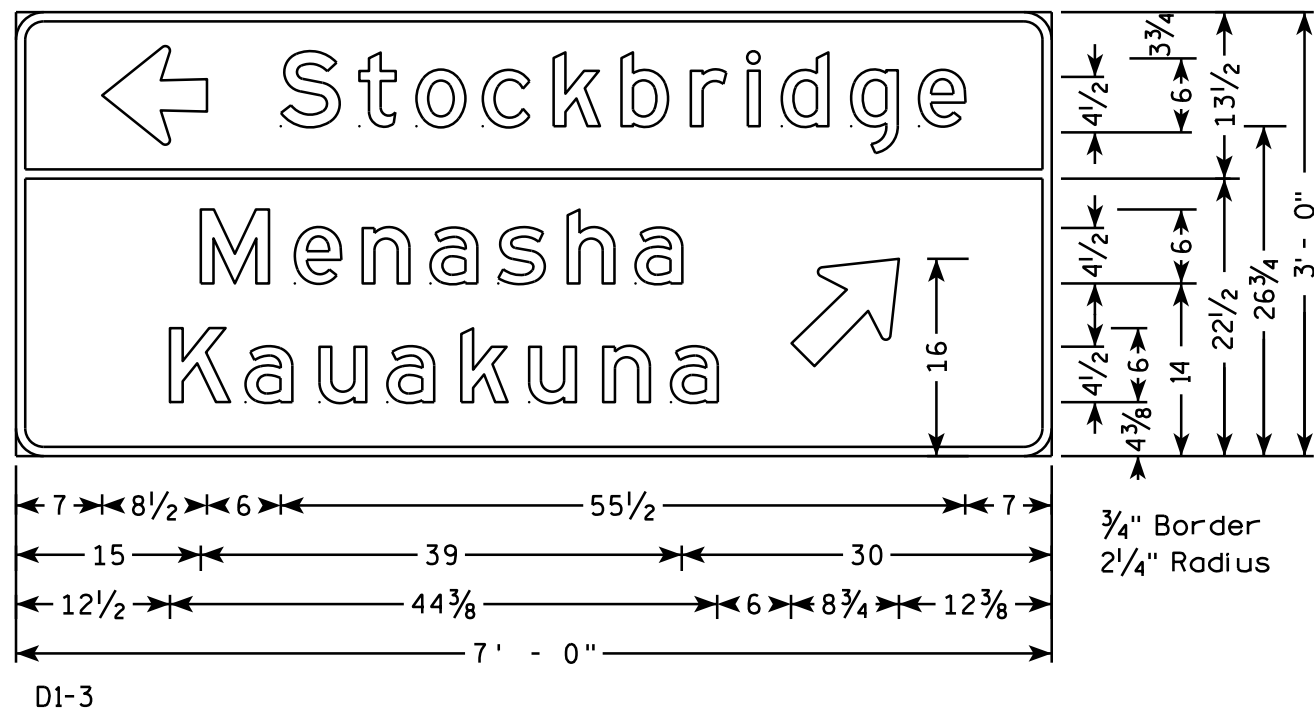




**NOTES**

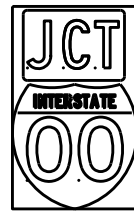
1. All Signs Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - GREEN  
Message - WHITE
3. Message Series - E

7

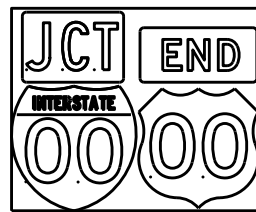


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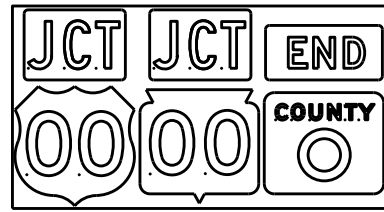
**TYPICAL ASSEMBLIES**



JI-1



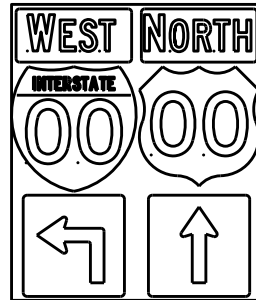
JI-2



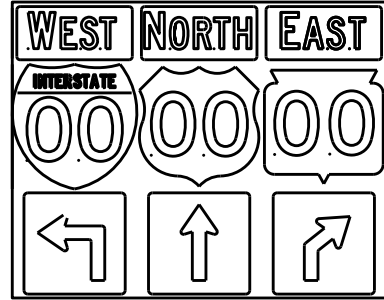
JI-3



J2-1



J2-2

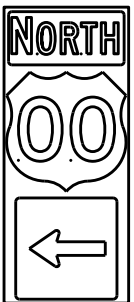


J2-3

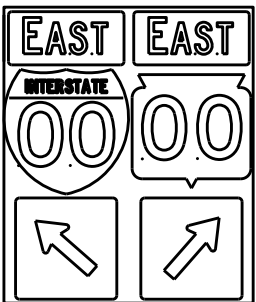


JV

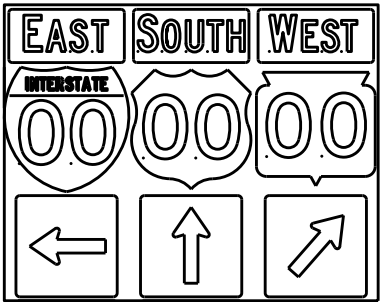
(Typical Vertical J-Assembly  
See Note 10 and 11)



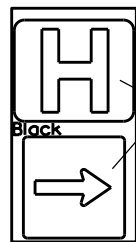
J3-1



J3-2

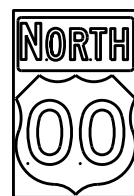


J3-3

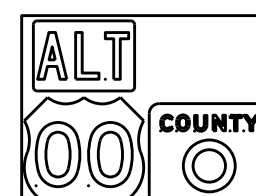


JH-1

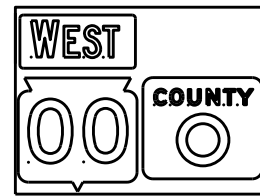
Blue Background



J4-1

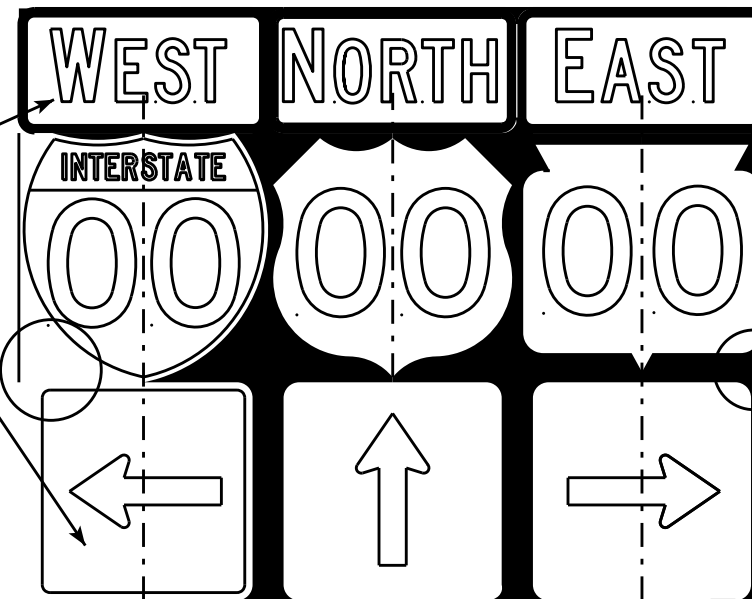
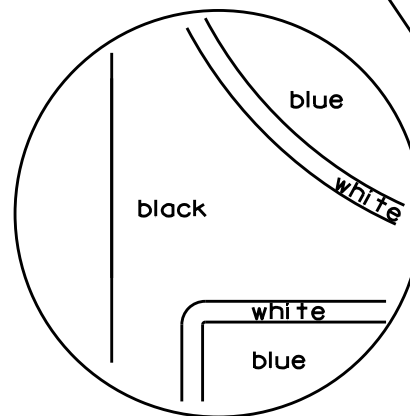


J4-2



J4-2

[blue background with interstate]



black

white

[black background]

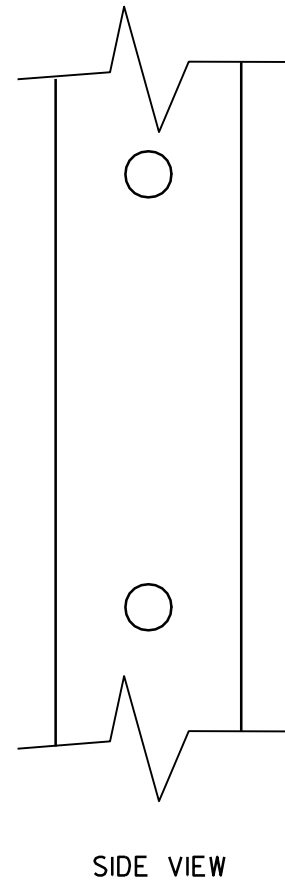
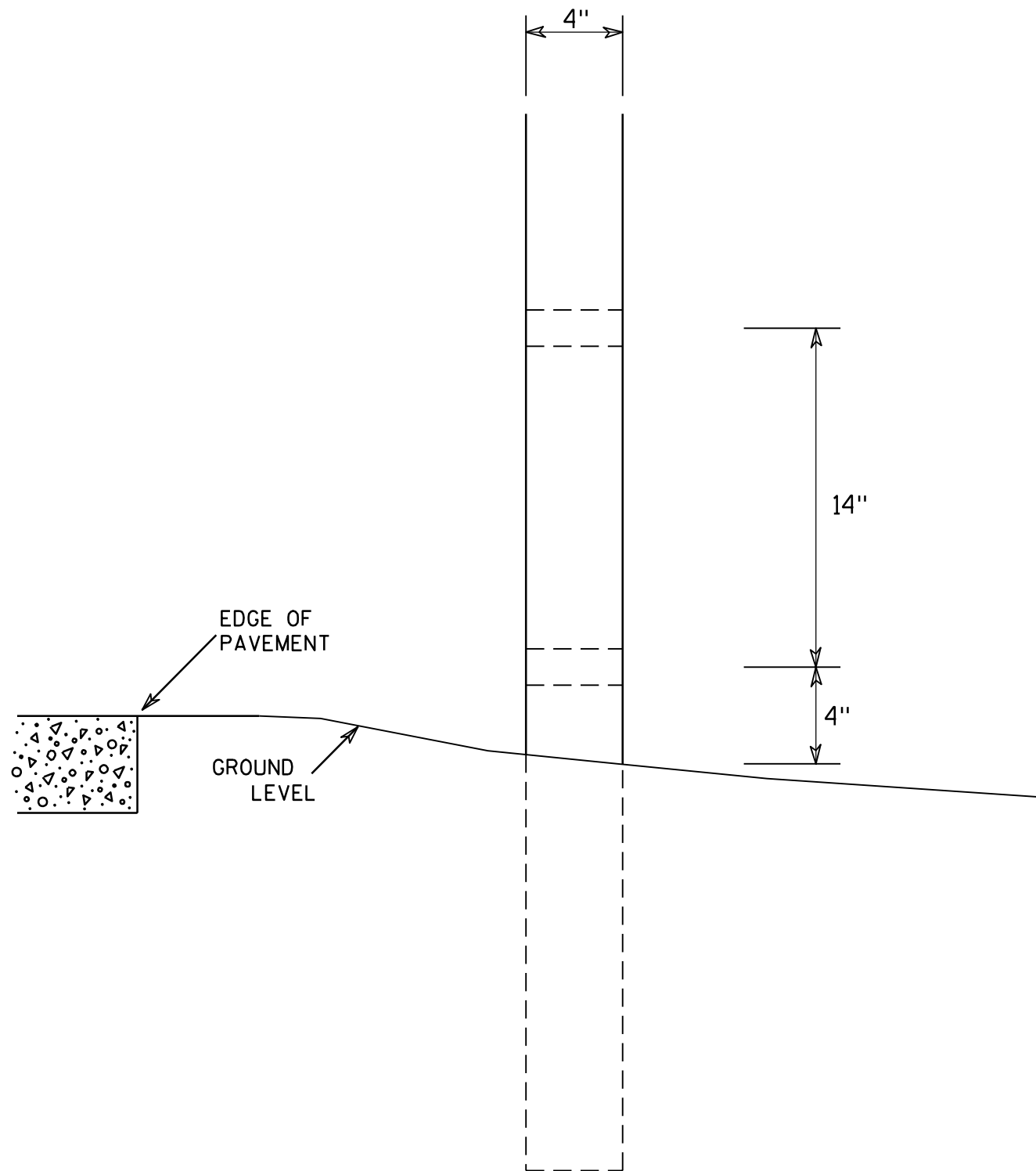
ROUTE MARKERS & COMPONENTS IN TYPICAL ASSEMBLIES	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 2/06/14	PLATE NO. A2-1S.8

**NOTES**

- Signs are Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:  
Background - Black Non-reflective  
Message - see Note 5
- Message Series - See Note 5
- Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
- The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
- Certain marker heads require the component pieces to be the same color. As an example, all the components used with an MI-1 Interstate marker shall be blue.
- Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
- Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- All Vertical J Assemblies are given a Sign Code of JV
- For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

7

7



GENERAL NOTES

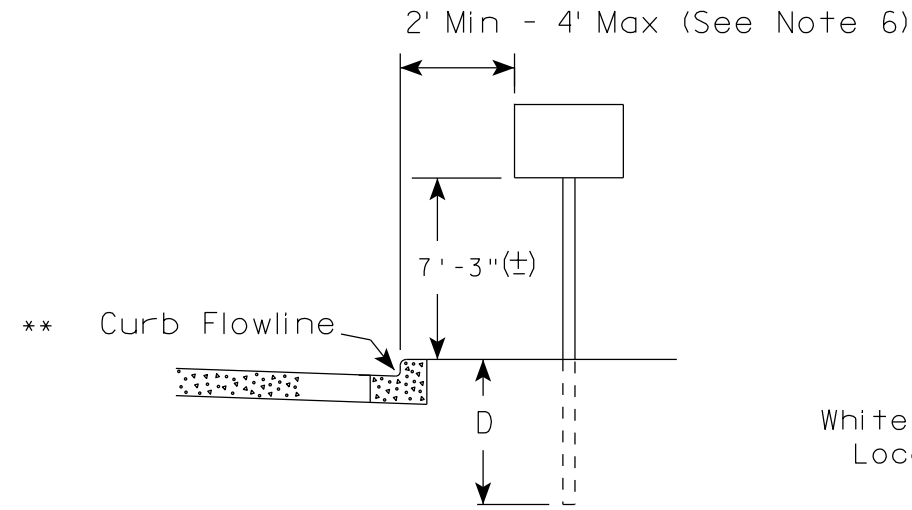
1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

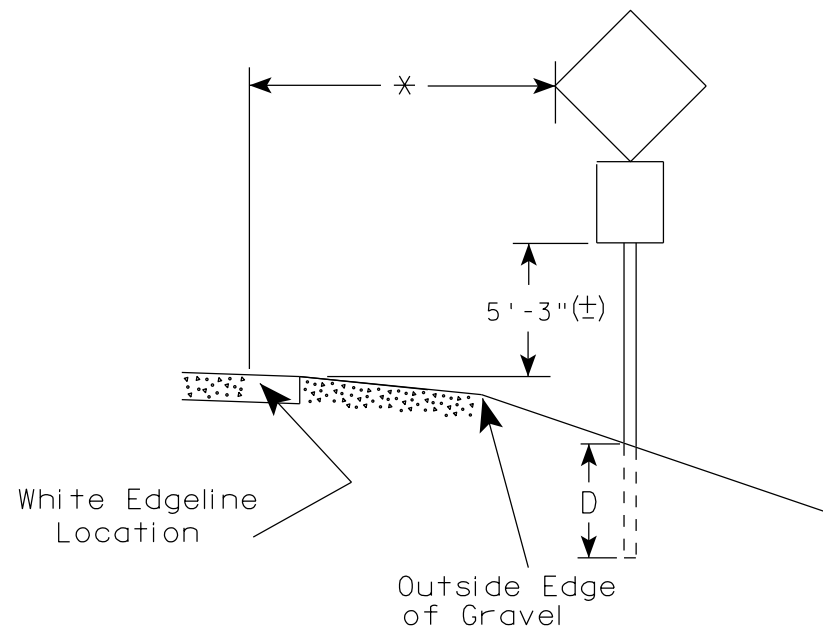
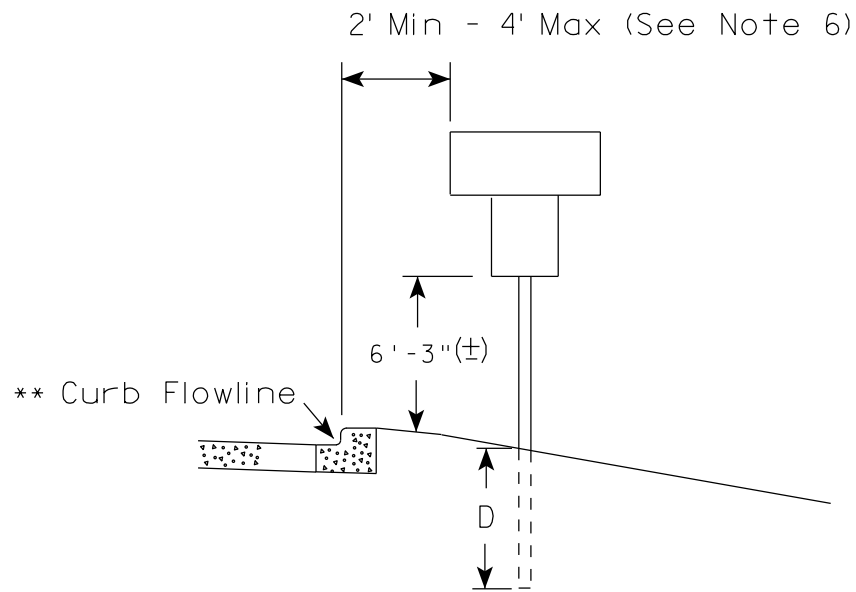
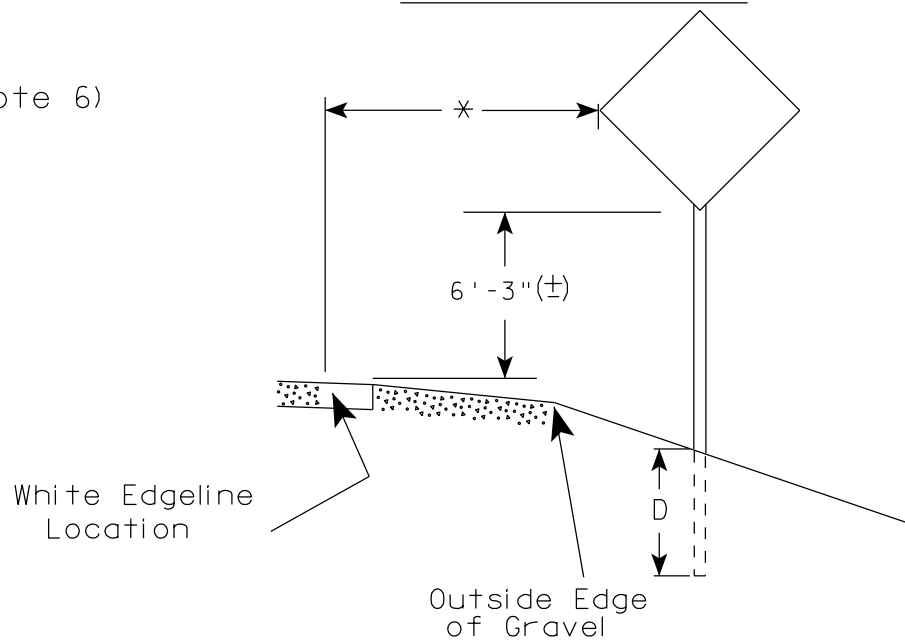
7

<b>4 X 6 WOOD POST MODIFICATIONS</b>	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet, 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'-3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series) & End of Rod Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (+).

POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq.Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

✖✖ The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 9/30/13 PLATE NO. A4-3.18



**ELEVATION VIEW**

**DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT**

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
  2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
  3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



**ELEVATION VIEW**

**DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT**



**PLAN VIEW**

**FOR NEW CONCRETE/ ASPHALT INSTALLATIONS**

<b>SIGN POST BOX-OUTS A4-3B</b>	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

**GENERAL NOTES**

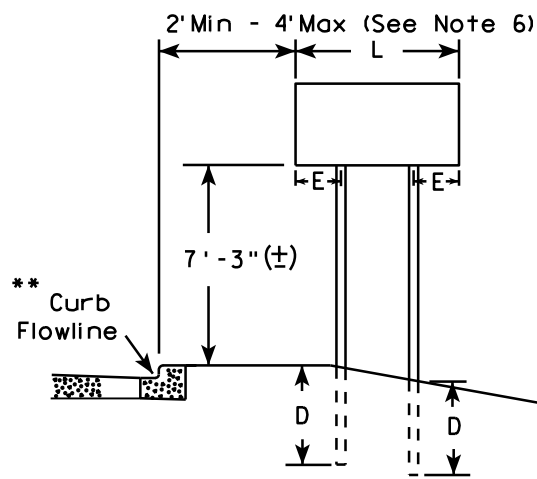
- For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- See tables below for required number of posts.
- For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
- The (±) tolerance for mounting height is 3 inches.
- Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
- Offset distance shall be consistent with existing signs or consistent throughout length of project.
- Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
- The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series) & End of Road Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (±).

\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

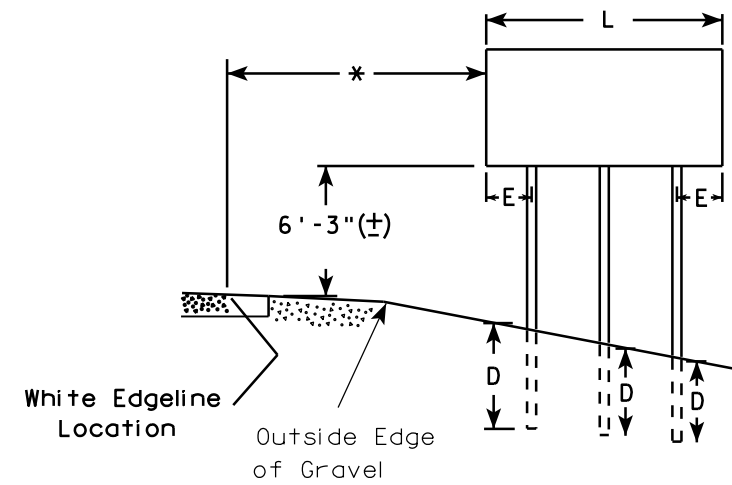
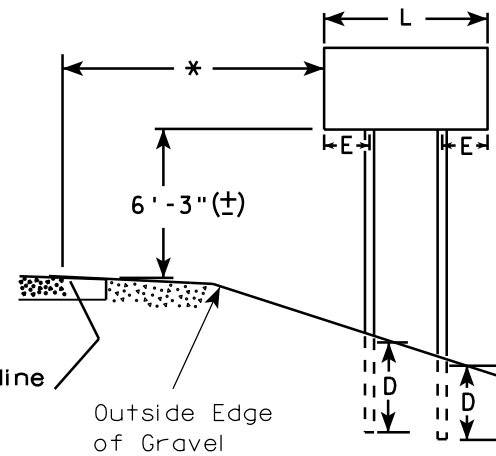
\*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

\*\*\* See A4-3 sign plate for signs 4' or less in width or 20 S.F. or less in area.

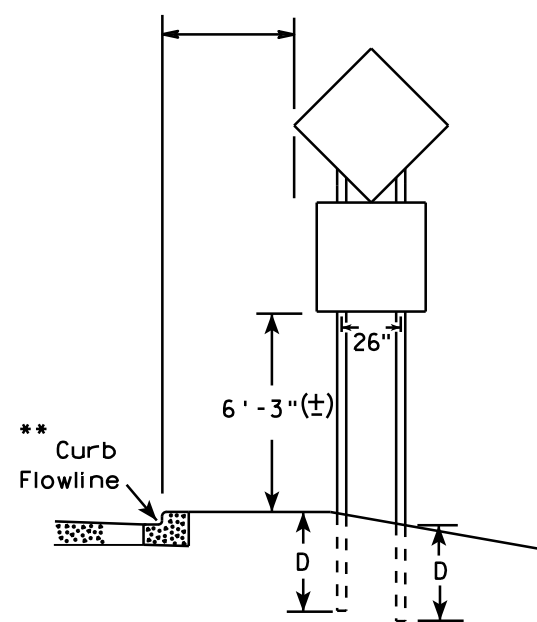
**URBAN AREA**



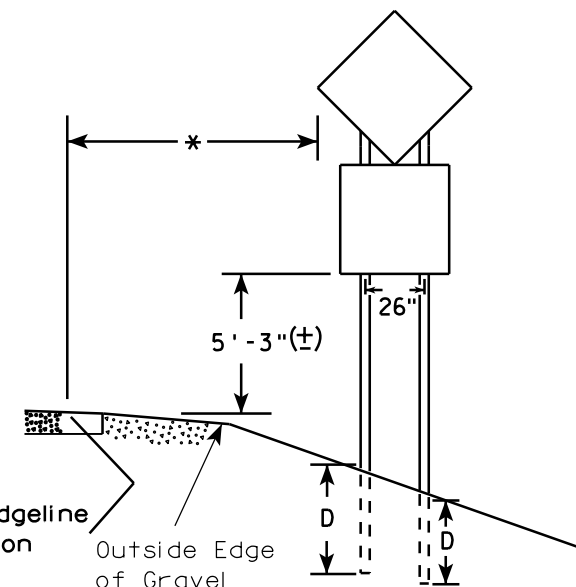
**RURAL AREA (See Note 3)**



2' Min - 4' Max (See Note 6)



**48" DIAMOND WARNING SIGN**



**48" DIAMOND WARNING SIGN**

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 120"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)	
L	E
168" and greater	12"

**POST EMBEDMENT DEPTH**

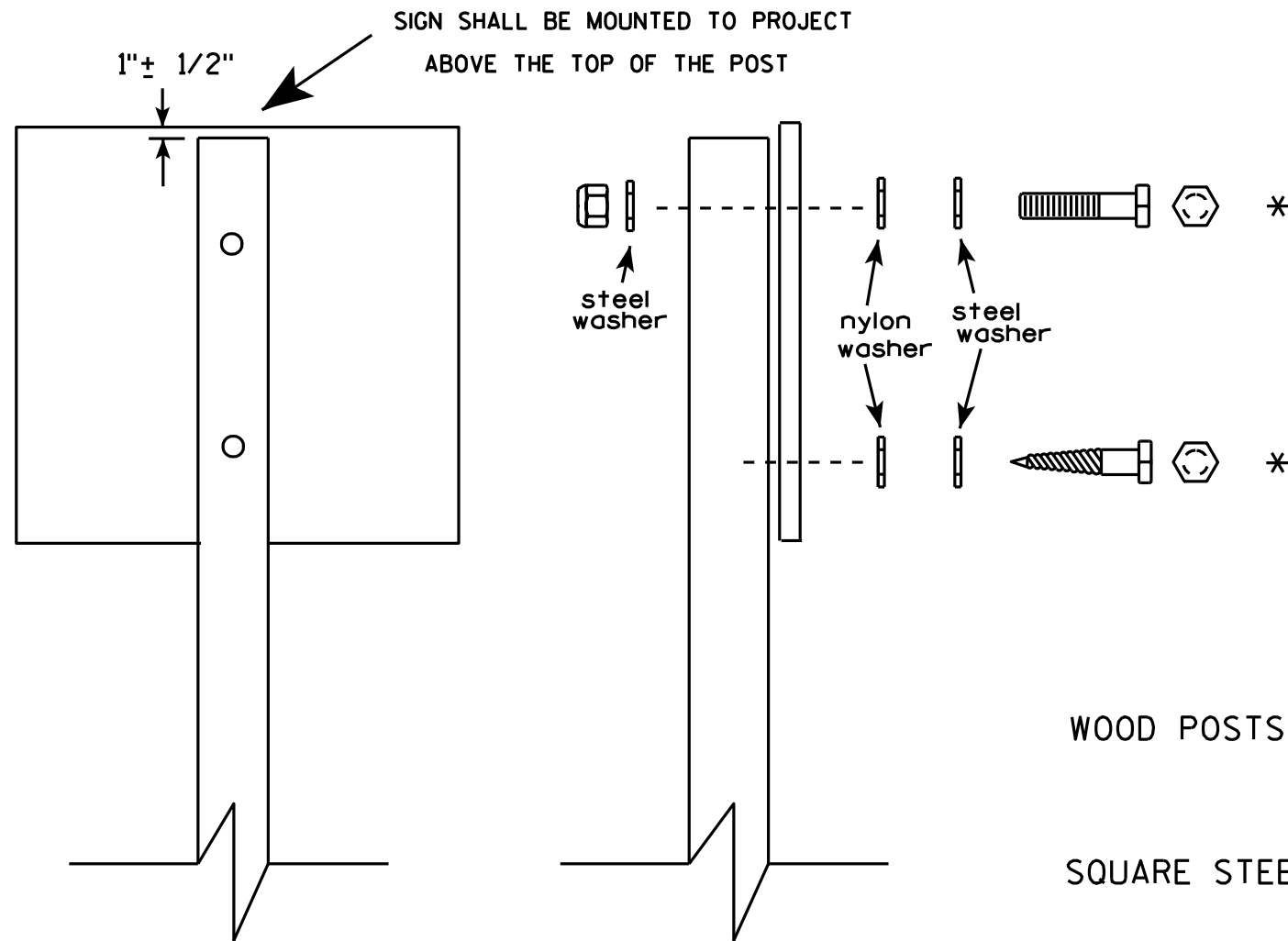
Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 9/30/13 PLATE NO. A4-4.12



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" X 3"

MACHINE BOLTS - 5/16" X 6-1/2" or 7" Length w/ nuts

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts

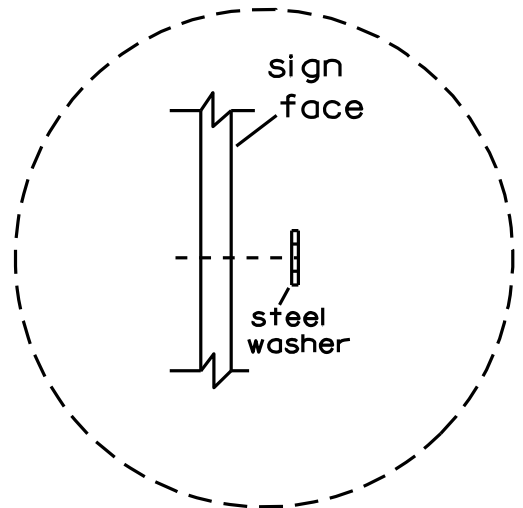
RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL

O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON for all Type H signs.



Washer Placement when Sign Has Other Than Type H or Type F Face

\* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

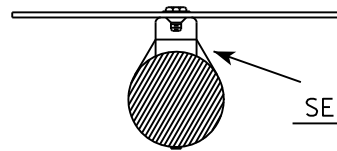
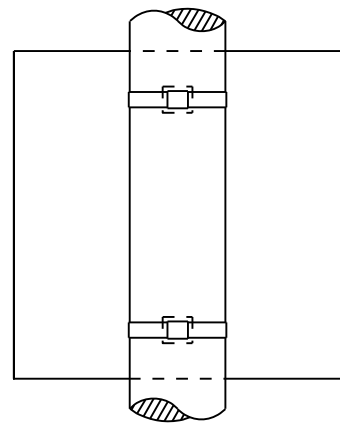
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*  
For State Traffic Engineer

DATE 3/23/10 PLATE NO. A4-8.7

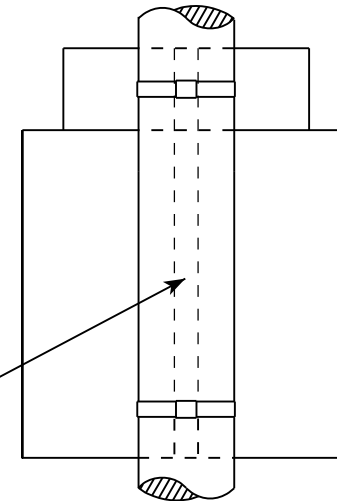
# BANDING

SINGLE SIGN

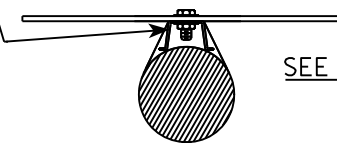


SEE DETAIL A

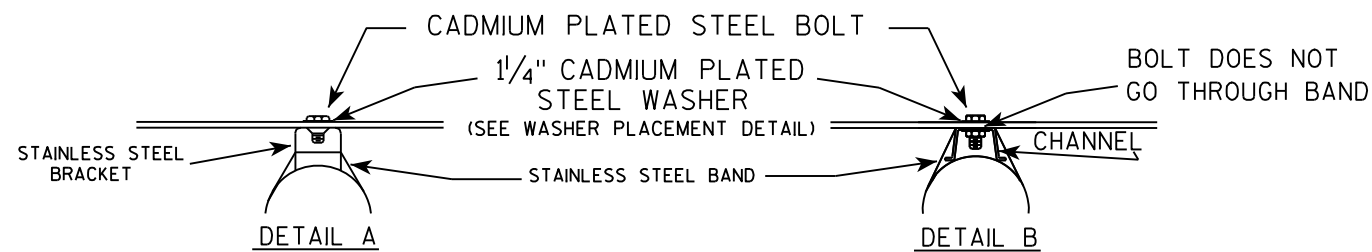
"J" ASSEMBLY



CHANNEL  
SEE TYPICAL PANEL  
INSTALLATION SHEET



SEE DETAIL B

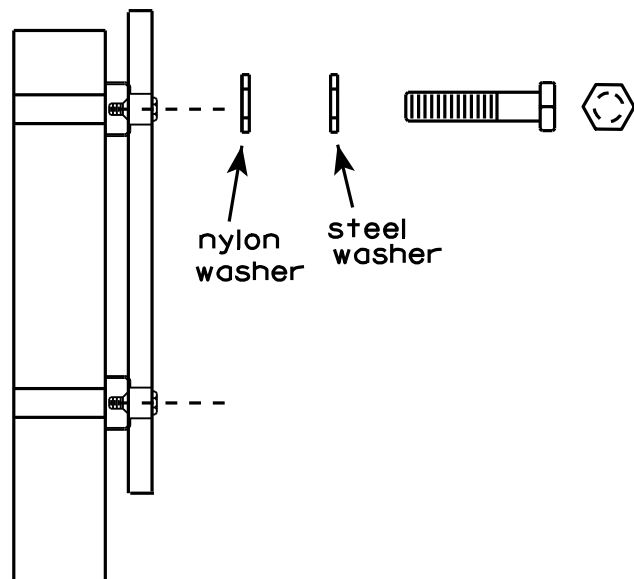


BOLT DOES NOT  
GO THROUGH BAND

GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.

WASHER PLACEMENT



WASHERS (ALL POSTS) -  
 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL  
 1-1/4" O.D. X 3/8" I.D. X .080 NYLON  
 FOR ALL TYPE H SIGNS

STANDARD SIGN  
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 8/16/13 PLATE NO. A5-9.3

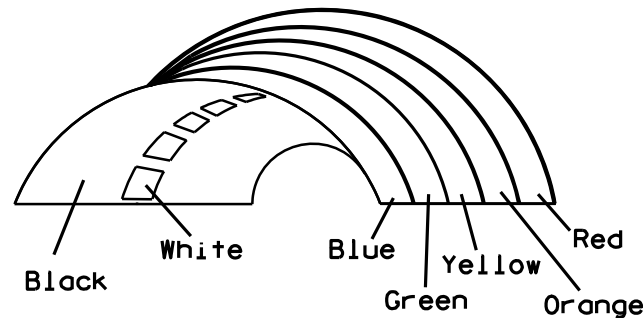


**NOTES**

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - (See Note 5)
3. Message Series - (See Note 6)
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Border - Blue  
Line 1 - Red  
Line 2 - Black  
Line 3-5 - Blue
6. Line 1 - Dutch 8011L  
Line 2 - Series E  
Line 3-5 - Series C
7. Contractor shall provide and install a new post bracket in accordance with the I55-56B sign detail.



Background Colors of Symbol\*



\*1/4" Black Border between each color of rainbow and border of rainbow

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	36	1 1/2	1/2	5/8	3	2	3 1/2	2 7/8	1	8	2 1/8	11 1/4	11 1/8	9 3/8	1 1/4		3/4	12 5/8	7 1/2							7.5
3																											
4																											
5																											

**STANDARD SIGN**  
**I55-56**

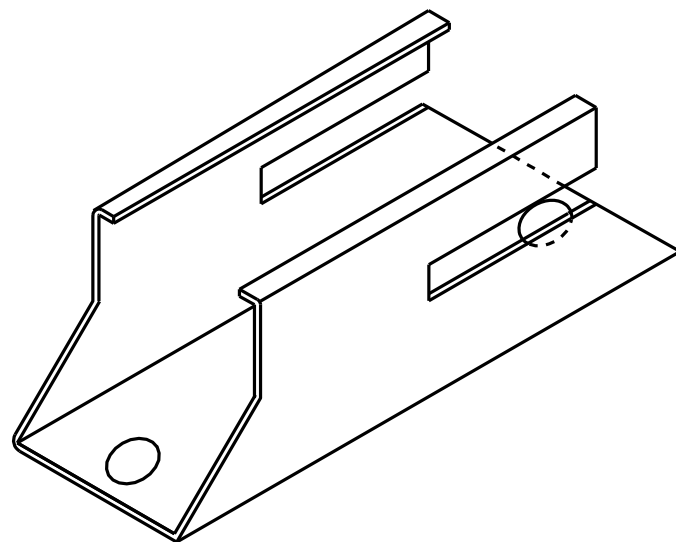
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

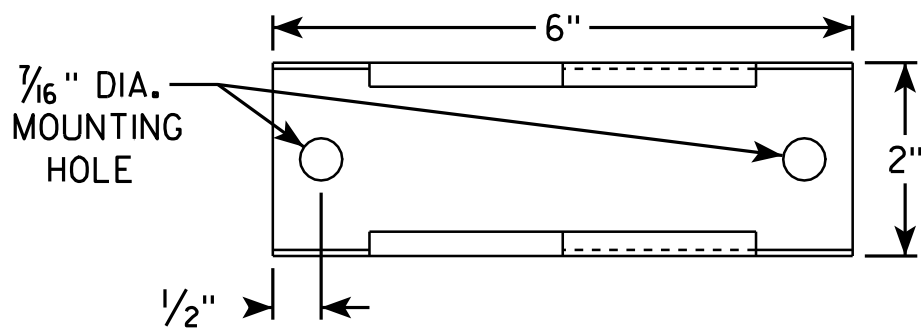
DATE 4/27/11 PLATE NO. I55-56.3

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_

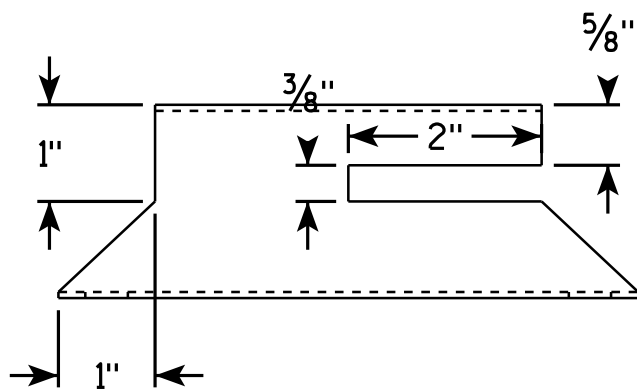
ISOMETRIC VIEW



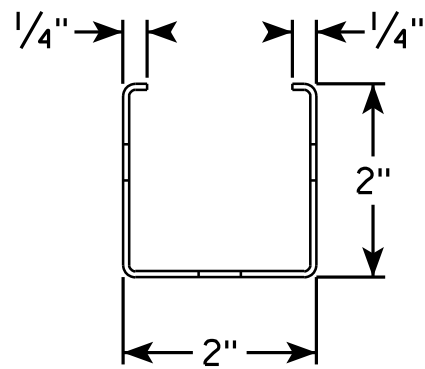
TOP VIEW



SIDE VIEW



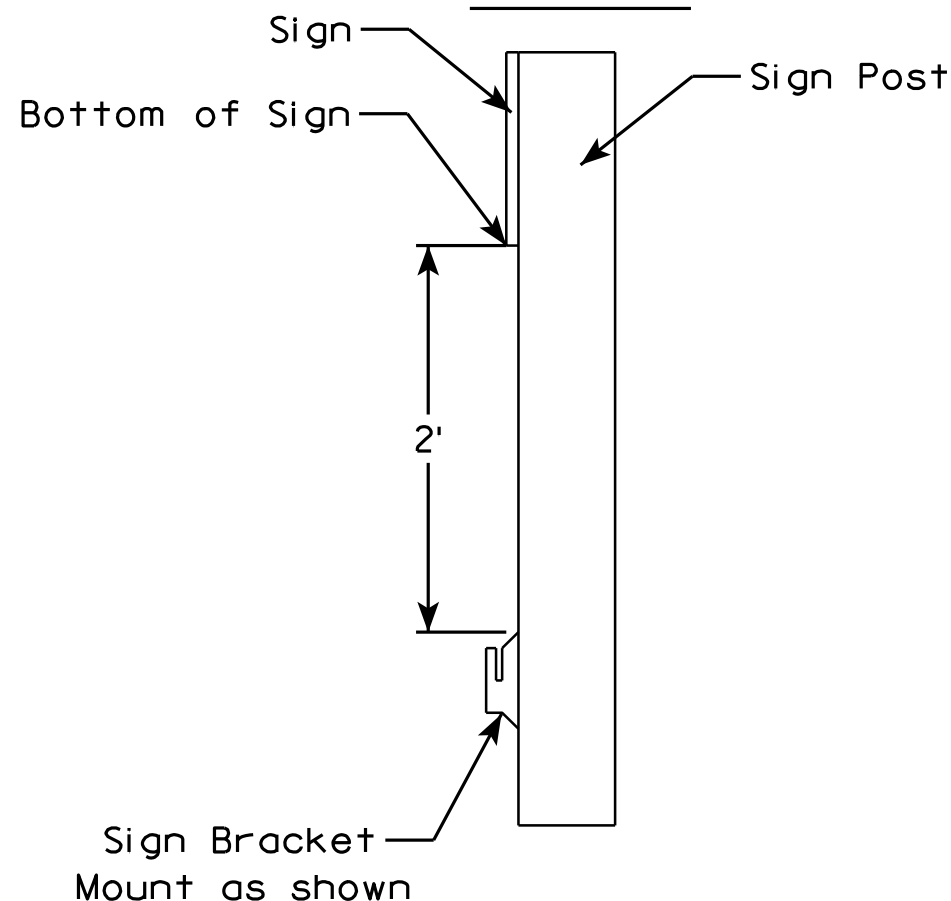
END VIEW



NOTES

1. Must be capable of permanent attachment to a wood or steel channel sign post utilizing the fastening hardware specified on the A4-8 sign plate.
2. Shall be entirely primed and painted with two coats of a black powder coated enamel paint.
3. Shall be made with 12 gauge steel, and incorporate no welds, no hinged components, no threaded lock-type components, and no parts which are loose or can be separated from the main body.
4. Shall have rounded edges with at least 1/8" radii.
5. Shall not have unrounded and uncoated metal edges which can contact the back surface of the roll-up sign.
6. Top of bracket shall be mounted 2' below the bottom of the I55-56 sign.
7. Cost of bracket and fastening hardware shall be incidental to the I55-56 sign.

SIDE VIEW



ROLLUP SIGN BRACKET  
I55-56B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 2/5/10 PLATE NO. I55-56B.1

PROJECT NO:

HWY:

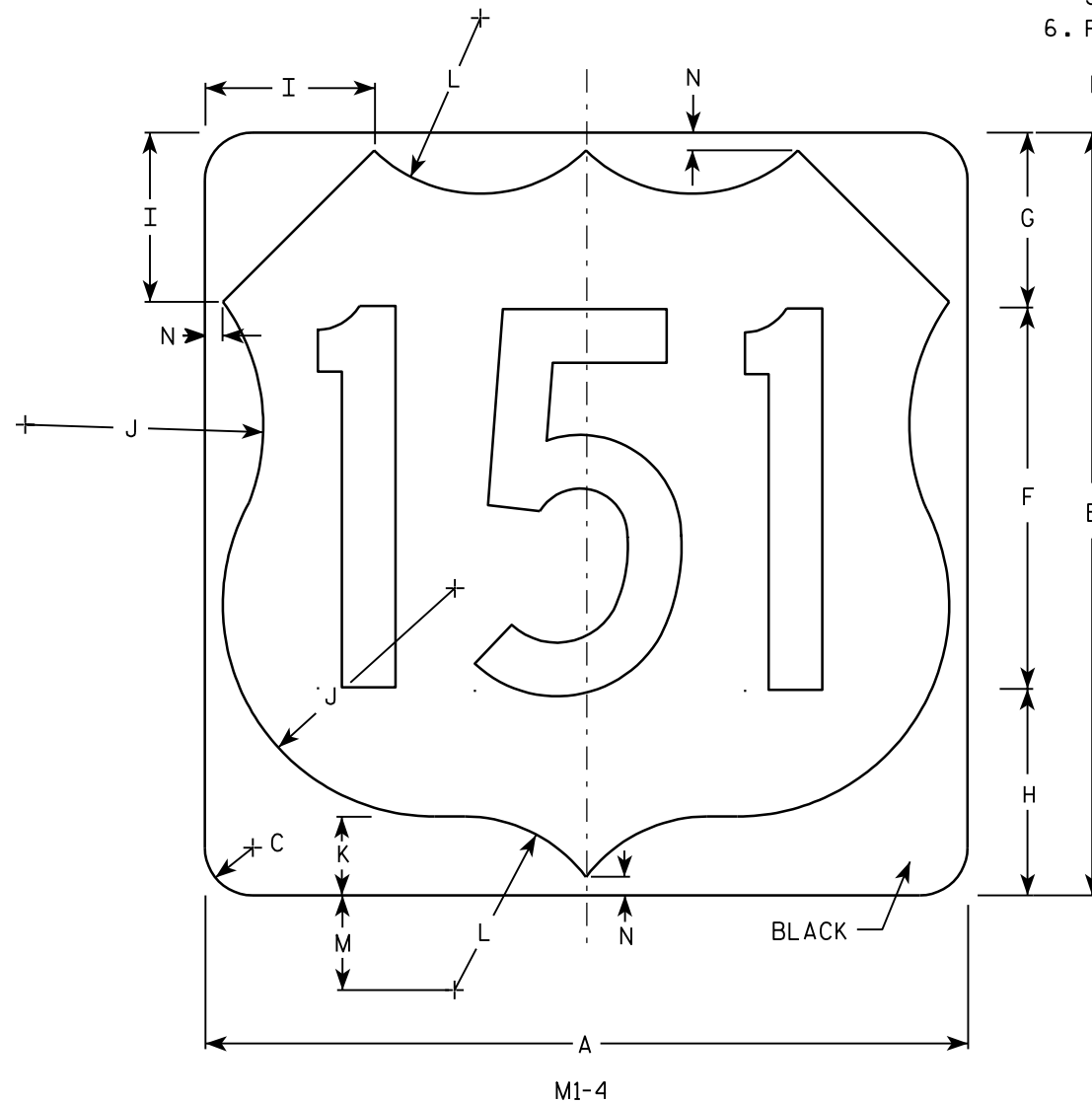
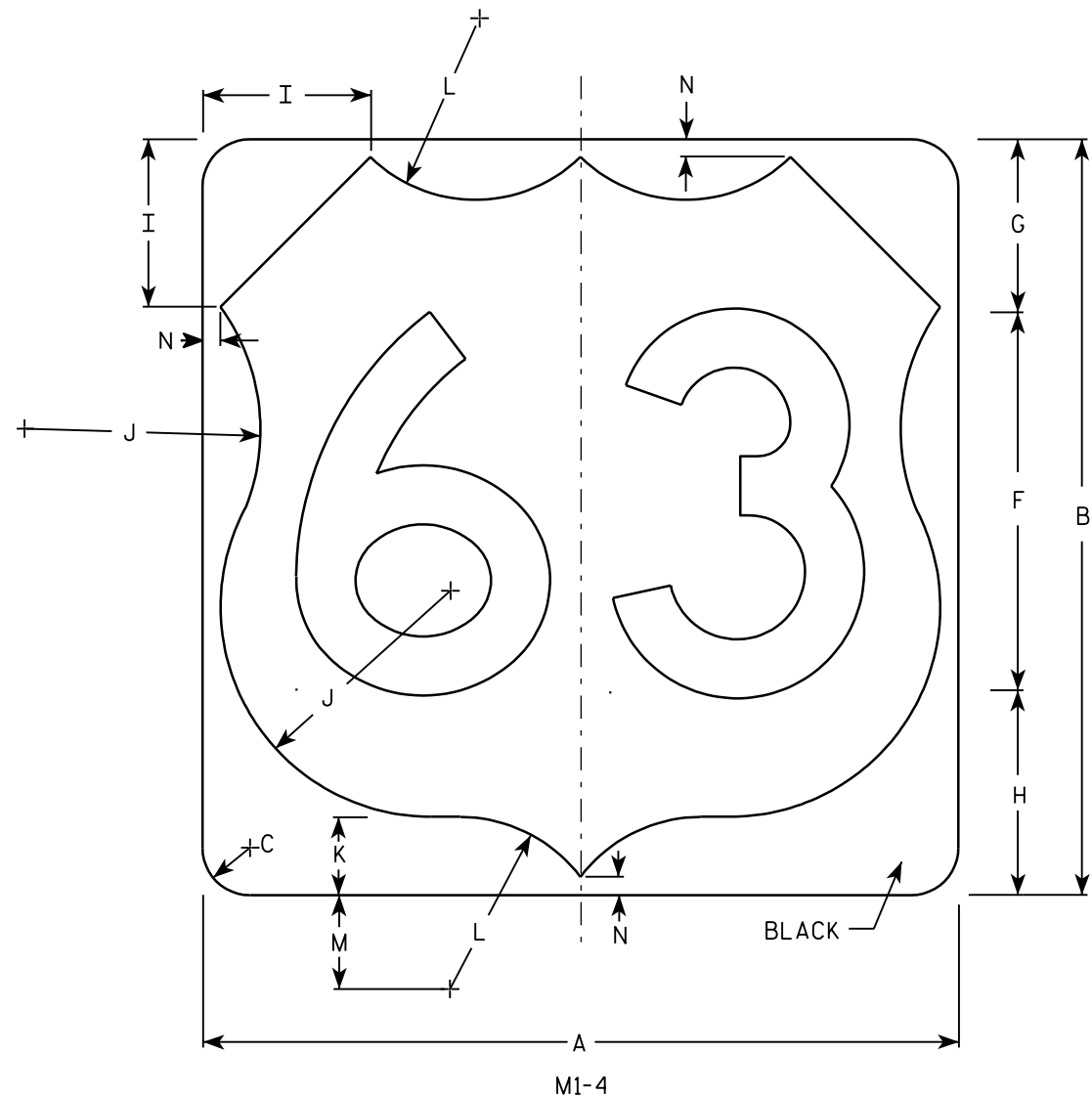
COUNTY:

SHEET NO:

E

**NOTES**

1. Sign is Type II - See Note 6 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White & Black - See Note 6  
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and adjust spacing as per Plate A10-1.
6. Permanent Signs  
Background - Type H Reflective  
Detour or other temporary signs  
Background - Reflective



Metric equivalent for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m <sup>2</sup>
1																												
2	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0	.36
3	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81
4	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81
5	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81

USH MARKER  
M1-4 FOR ASSEMBLIES

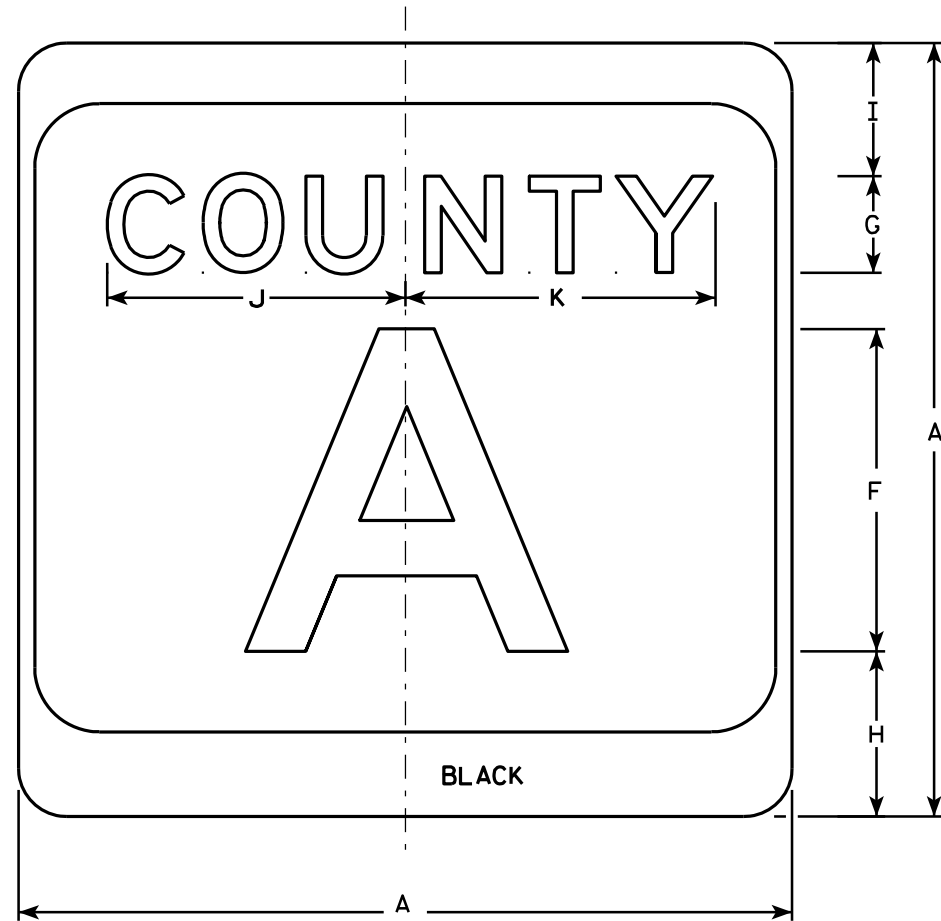
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

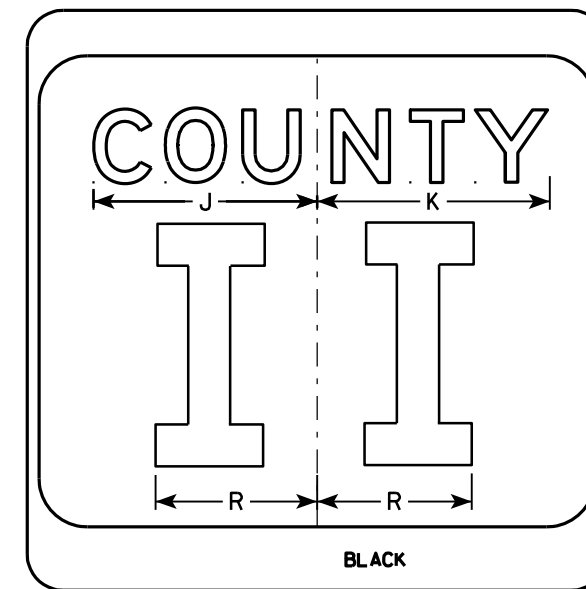
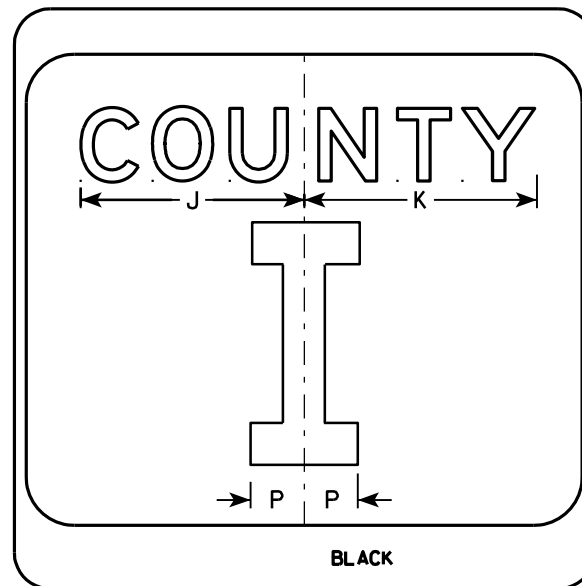
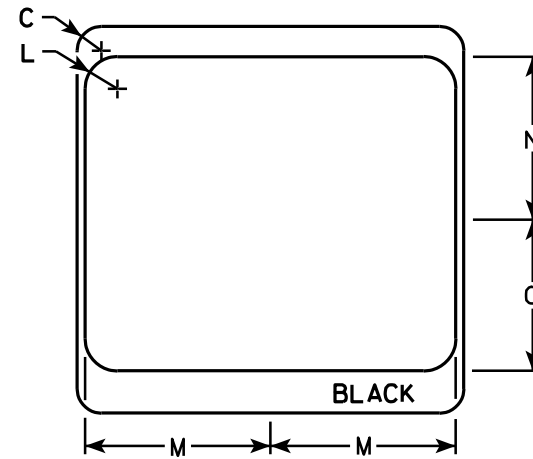
DATE 08/25/05 PLATE NO. M1-4.9

**NOTES**

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White & Black - See Note 7  
Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.  
Message Series D for 2 letters unless message is too big then Series C.  
Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs  
Background - Type H Reflective  
Detour or temporary Signs  
Background - Reflective



M1-5A



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

**CTH MARKER**  
**M1-5A FOR ASSEMBLIES**

WISCONSIN DEPT OF TRANSPORTATION

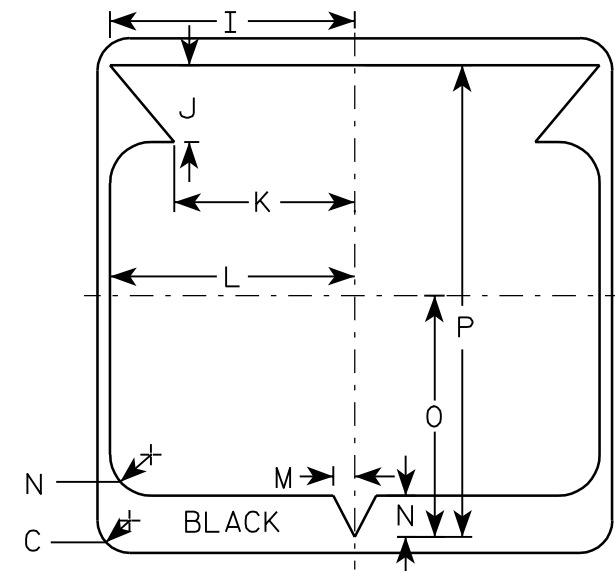
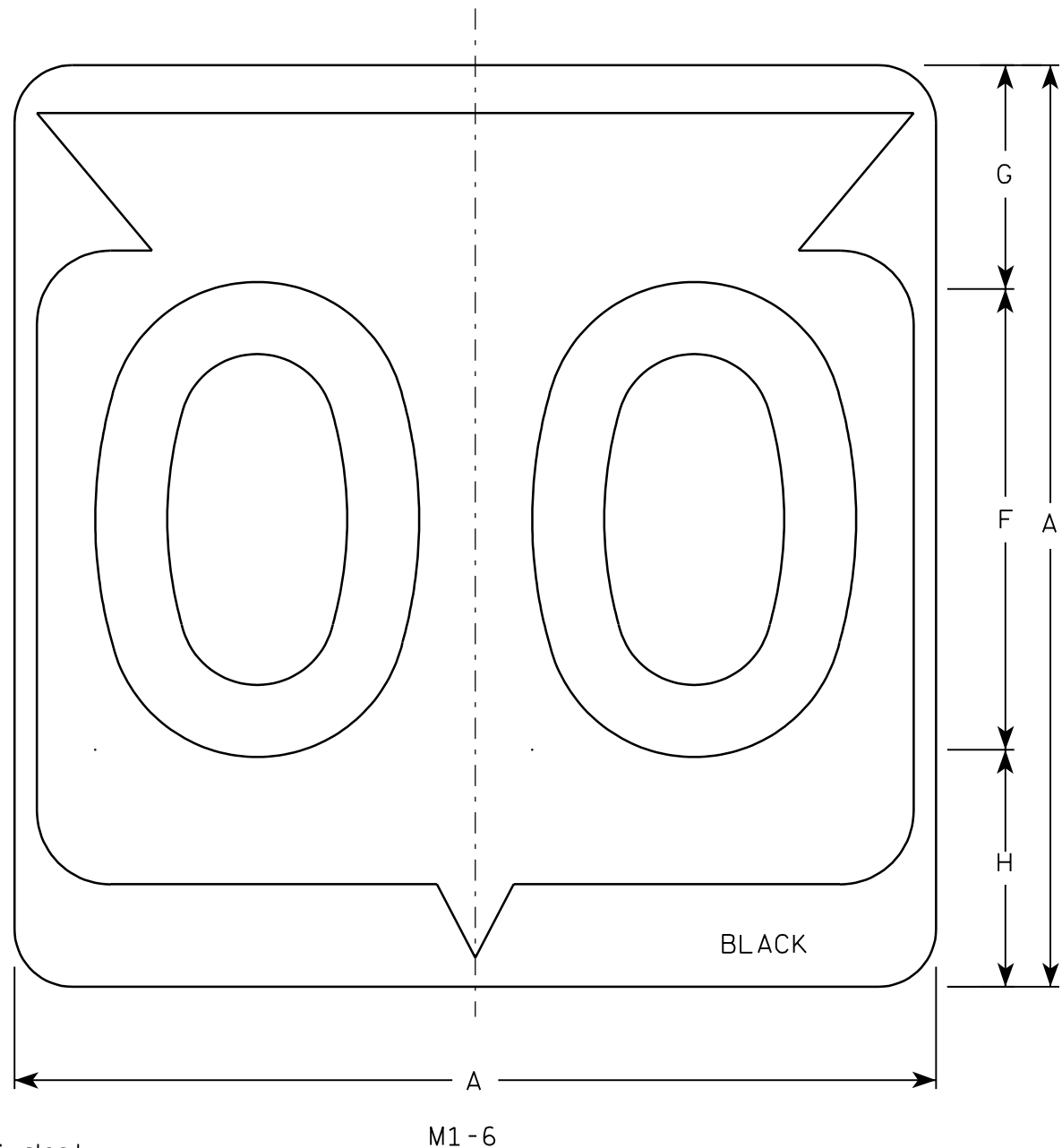
APPROVED *Matthew R. Raub*  
For State Traffic Engineer

DATE 9/27/11 PLATE NO. MI-5A.8

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**

NOTES

1. Sign is Type II - See Note 6 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White & Black - See Note 6  
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate Series numerals and adjust spacing as per plate A10-1.
6. Permanent Signs  
Background - Type H Reflective  
Detour or temporary Signs  
Background - Reflective



7

7

Metric equivalent for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m <sup>2</sup>
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0	.36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81

STATE ROUTE MARKER  
M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

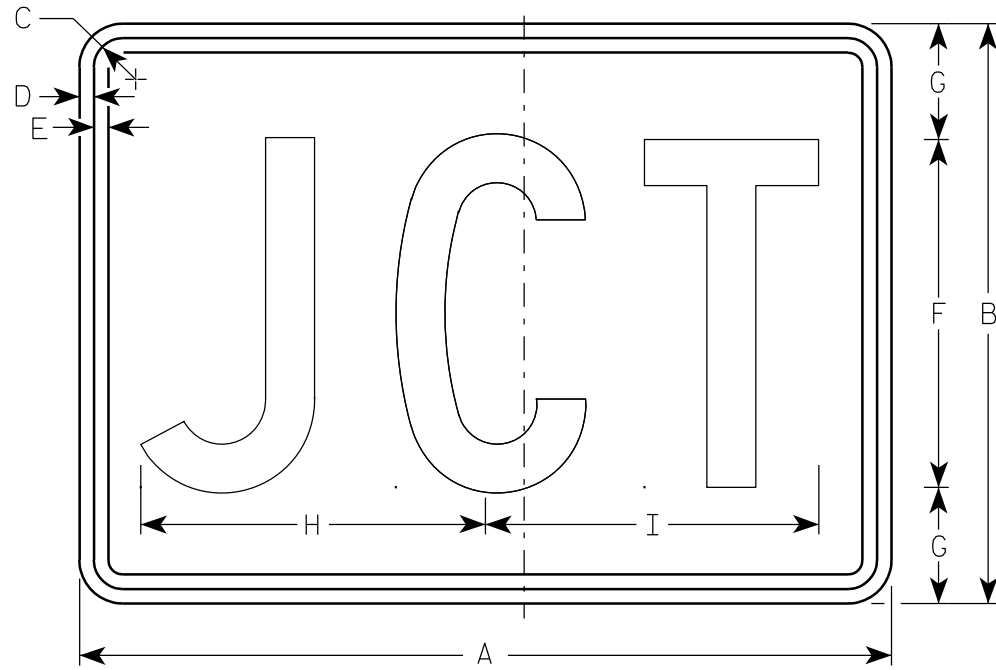
APPROVED *Chester J. Spang*  
for State Traffic Engineer

DATE 3/20/02 PLATE NO. M1-6.9

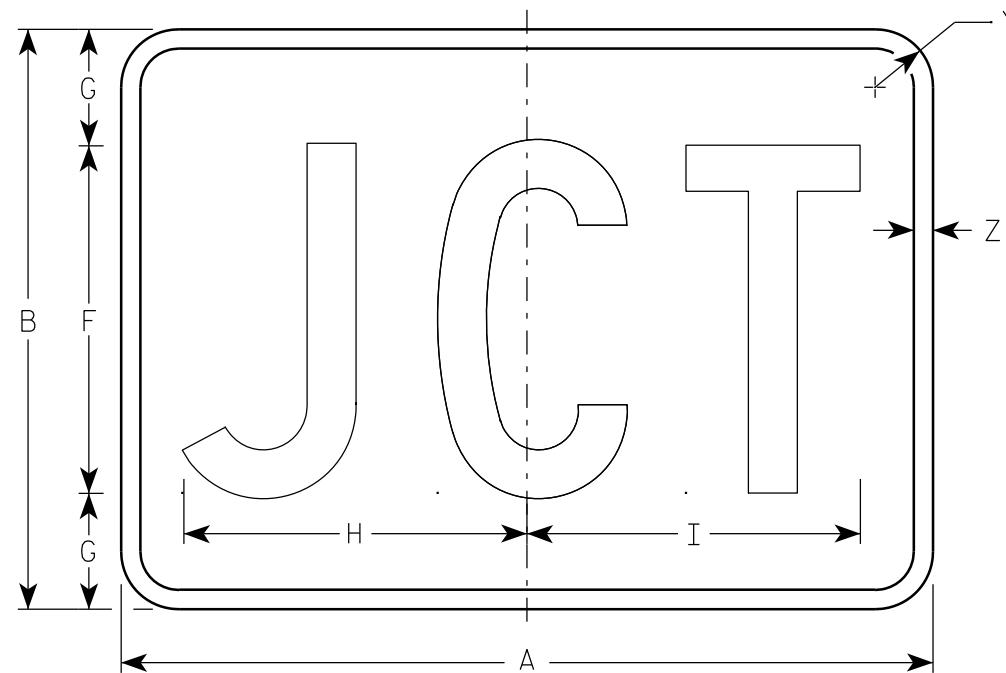
PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**

NOTES

1. Sign is Type II - See Note 5 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - See note 5  
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M2-1 Background - White - Type H Reflective  
(Detour or temporary Signs - Reflective)  
Message - Black  
MB2-1 Background - Blue  
Message - White - Type H Reflective  
(Detour or temporary Signs - Reflective)  
MG2-1 Background - Green  
Message - White - Type H Reflective  
MK2-1 Background - Green  
Message - White - Type H Reflective  
MM2-1 Background - White - Type H Reflective  
Message - Green  
MN2-1 Background - Brown  
Message - White - Type H Reflective  
MR2-1 Background - Brown  
Message - Yellow - Type H Reflective



M2-1  
MK2-1  
MM2-1  
MR2-1



MB2-1  
MG2-1  
MN2-1

7

Metric equivalent for this sign is:

SIZE	
1	
2	525 mm X 375 mm
3	750 mm X 525 mm
4	750 mm X 525 mm
5	750 mm X 525 mm

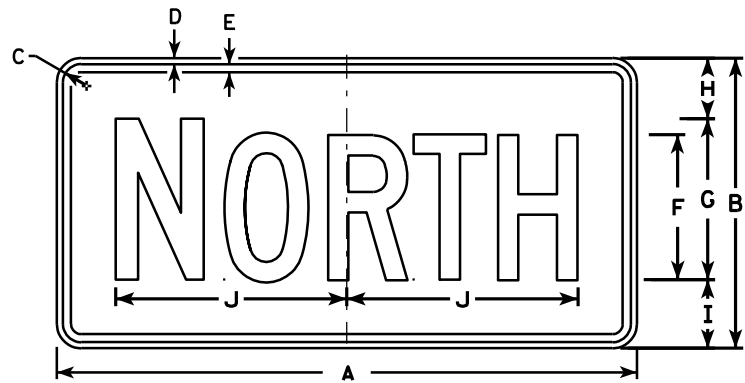
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m <sup>2</sup>
1																												
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20	0.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20

STANDARD SIGN  
M2-1

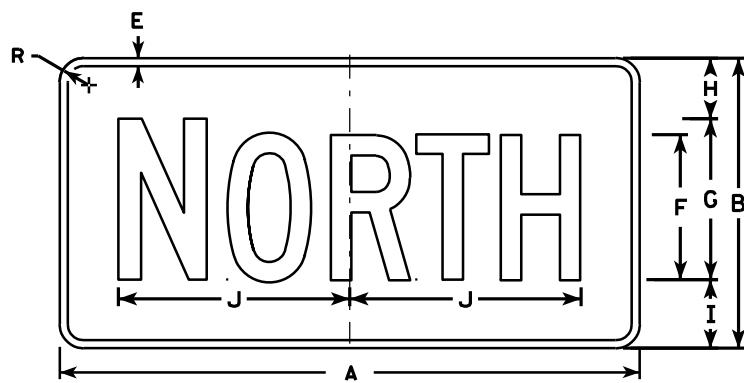
WISCONSIN DEPT OF TRANSPORTATION  
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/16/10 PLATE NO. M2-1.10

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



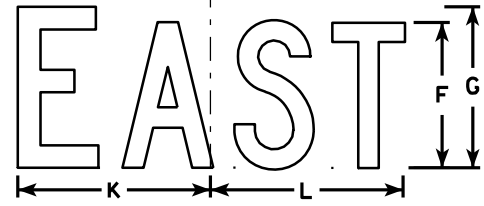
M3-1  
MK3-1  
M03-1



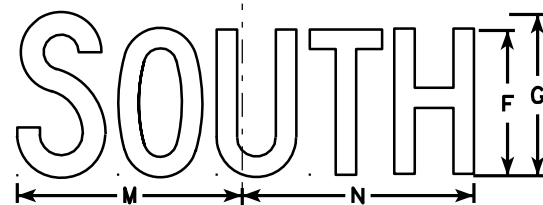
MB3-1  
MG3-1  
MM3-1  
MN3-1



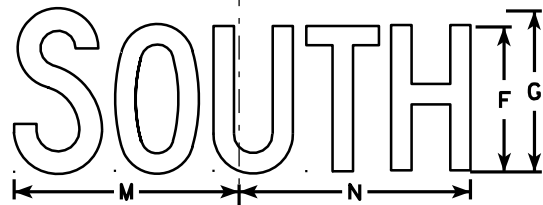
M3-2  
MK3-2  
M03-2



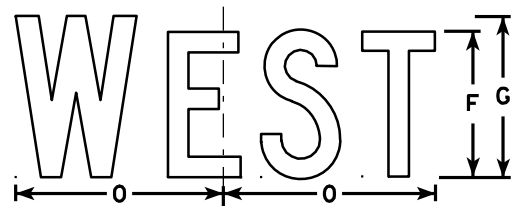
MB3-2  
MG3-2  
MM3-2  
MN3-2



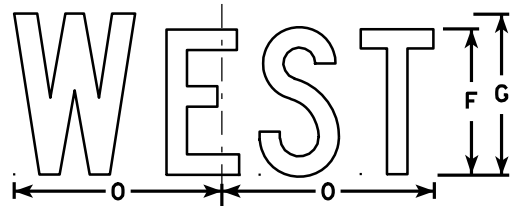
M3-3  
MK3-3  
M03-3



MB3-3  
MG3-3  
MM3-3  
MN3-3



M3-4  
MK3-4  
M03-4



MB3-4  
MG3-4  
MM3-4  
MN3-4

**NOTES**

- All Signs Type II - See Note 5 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:  
Background - See note 5  
Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White - Type H Reflective (Detour or temporary signs - Reflective)  
Message - Black  
MB3-1 thru MB3-4 Background - Blue  
Message - White - Type H Reflective (Detour or temporary signs - Reflective)  
MG3-1 thru MG3-4 Background - Green  
Message - White - Type H Reflective  
MK3-1 thru MK3-4 Background - Green  
Message - White - Type H Reflective  
MM3-1 thru MM3-4 Background - White - Type H Reflective  
Message - Green  
MN3-1 thru MN3-4 Background - Brown  
Message - White - Type H Reflective  
M03-1 thru M03-4 Background - Orange - Reflective  
Message - Black
- Note the first letter of each direction is larger than the remainder of the message.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

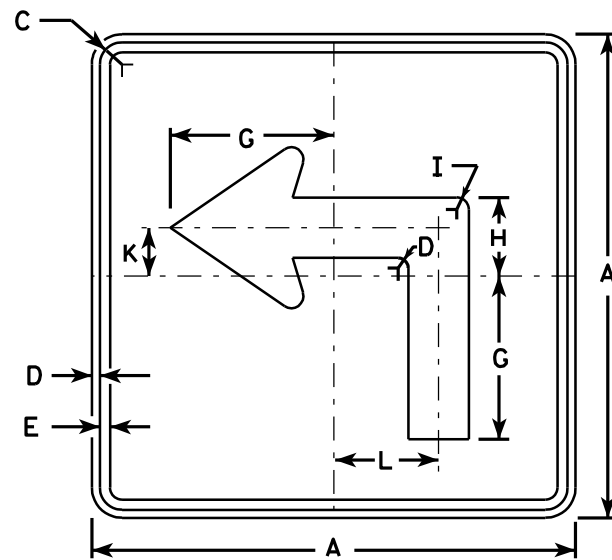
**STANDARD SIGNS**  
M3-1 thru M3-4  
SERIES

WISCONSIN DEPT OF TRANSPORTATION

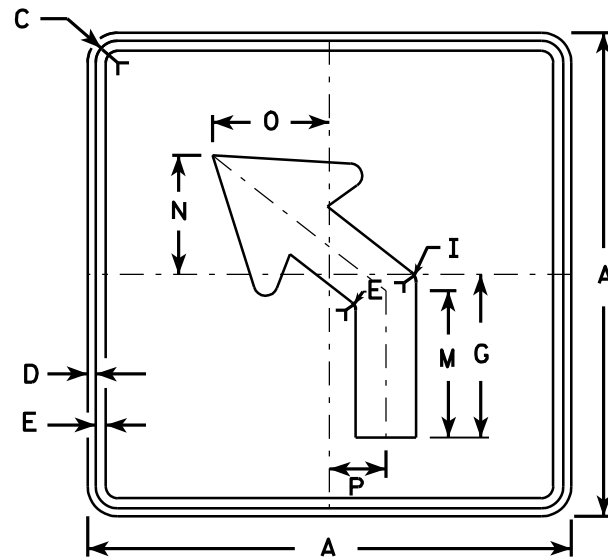
APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 11/10/10 PLATE NO. M3-1.12

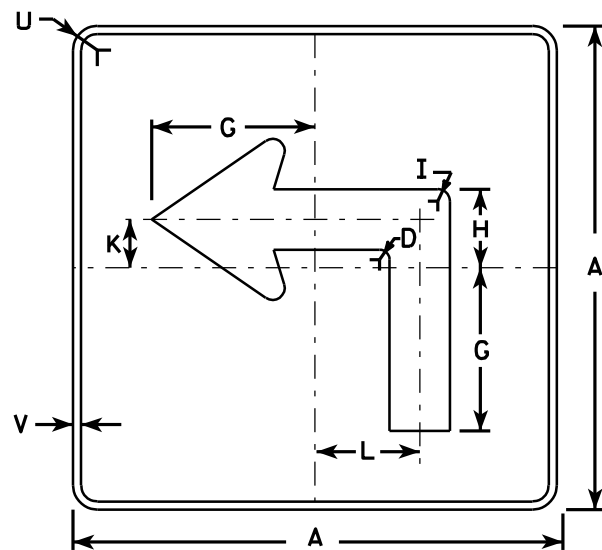
PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



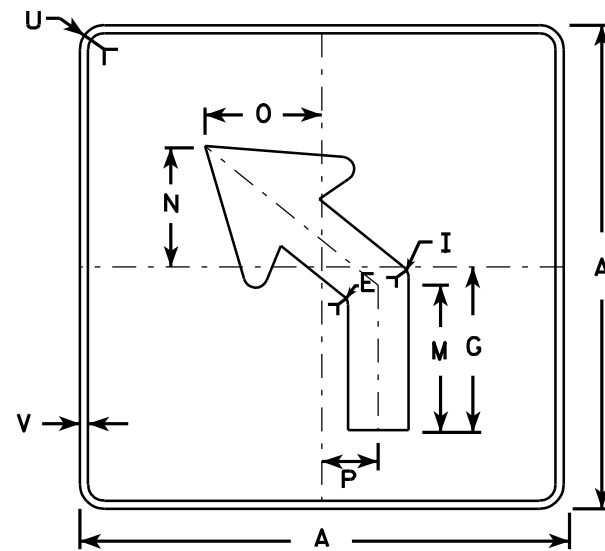
M5-1L  
MK5-1L  
MM5-1L  
M05-1L  
MP5-1L  
MR5-1L



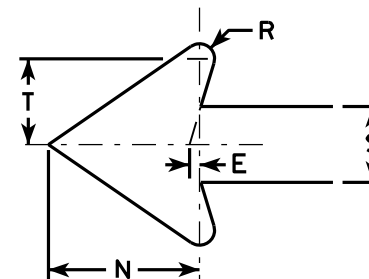
M5-2L  
MK5-2L  
MM5-2L  
M05-2L  
MP5-2L  
MR5-2L



MB5-1L  
MG5-1L  
MN5-1L



MB5-2L  
MG5-2L  
MN5-2L



**NOTES**

- Signs are Type II - See Note 4 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:  
Background - See note 4  
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- |                 |   |
|-----------------|---|
| M5-1 and M5-2   | Background - White - Type H Reflective  |
|                 | Message - Black                         |
| MB5-1 and MB5-2 | Background - Blue                       |
|                 | Message - White - Type H Reflective     |
| MG5-1 and MG5-2 | Background - Green                      |
|                 | Message - White - Type H Reflective     |
| MK5-1 and MK5-2 | Background - Green                      |
|                 | Message - White Type H Reflective       |
| MM5-1 and MM5-2 | Background - White - Type H Reflective  |
|                 | Message - Green                         |
| MN5-1 and MN5-2 | Background - Brown                      |
|                 | Message - White - Type H Reflective     |
| M05-1 and M05-2 | Background - Orange - Type F Reflective |
|                 | Message - Black                         |
| MP5-1 and MP5-2 | Background - White - Type H Reflective  |
|                 | Message - Blue                          |
| MR5-1 and MR5-2 | Background - Brown                      |
|                 | Message - Yellow - Type H Reflective    |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

**STANDARD SIGN**  
**M5-1 & M5-2**

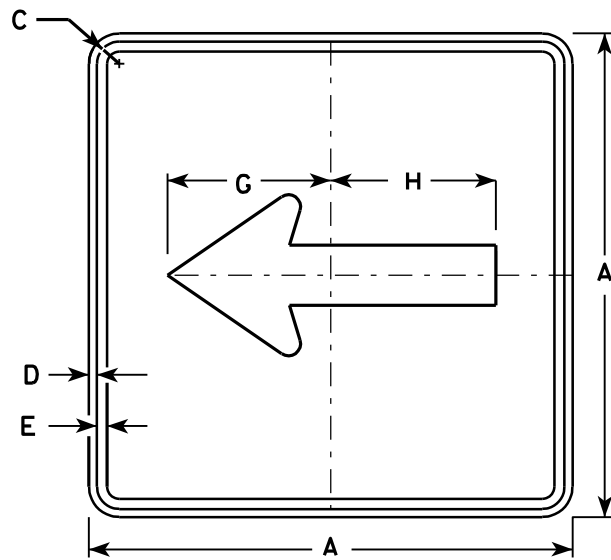
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

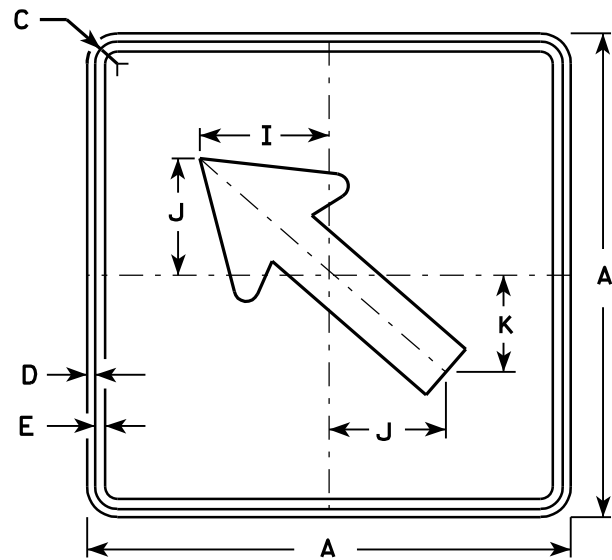
DATE 7/29/13 PLATE NO. M5-1.12

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**

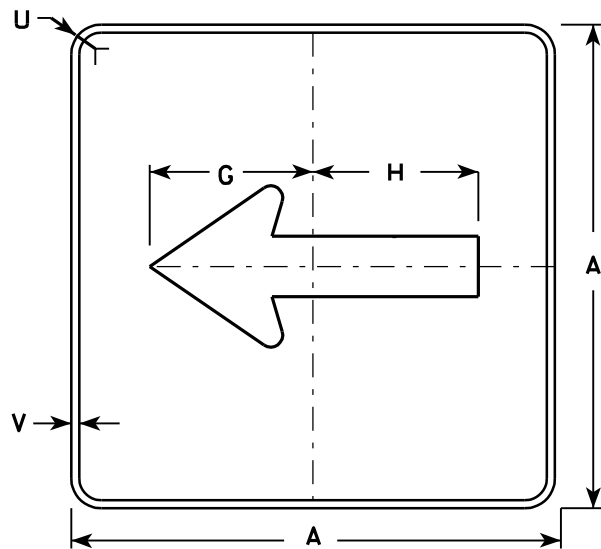




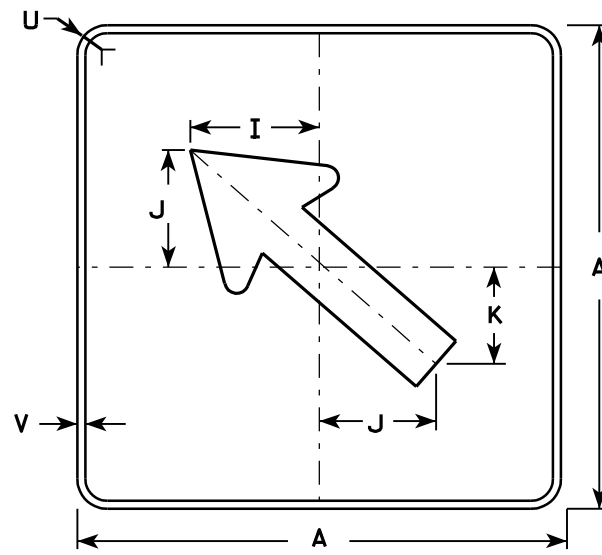
M6-1  
MK6-1  
MM6-1  
MO6-1  
MP6-1  
MR6-1



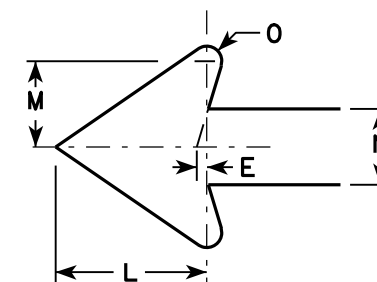
M6-2  
MK6-2  
MM6-2  
MO6-2  
MP6-2  
MR6-2



MB6-1  
MG6-1  
MN6-1



MB6-2  
MG6-2  
MN6-2



**NOTES**

- Signs are Type II - See Note 4 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:  
Background - See note 4  
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White - Type H Reflective  
Message - Black  
MB6-1 and MB6-2 Background - Blue  
Message - White - Type H Reflective  
MG6-1 and MG6-2 Background - Green  
Message - White - Type H Reflective  
MK6-1 and MK6-2 Background - Green  
Message - White - Type H Reflective  
MM6-1 and MM6-2 Background - White - Type H Reflective  
Message - Green  
MN6-1 and MN6-2 Background - Brown  
Message - White - Type H Reflective  
MO6-1 and MO6-2 Background - Orange - Type F Reflective  
Message - Black  
MP6-1 and MP6-2 Background - White - Type H Reflective  
Message - Blue  
MR6-1 and MR6-2 Background - Brown  
Message - Yellow - Type H Reflective

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

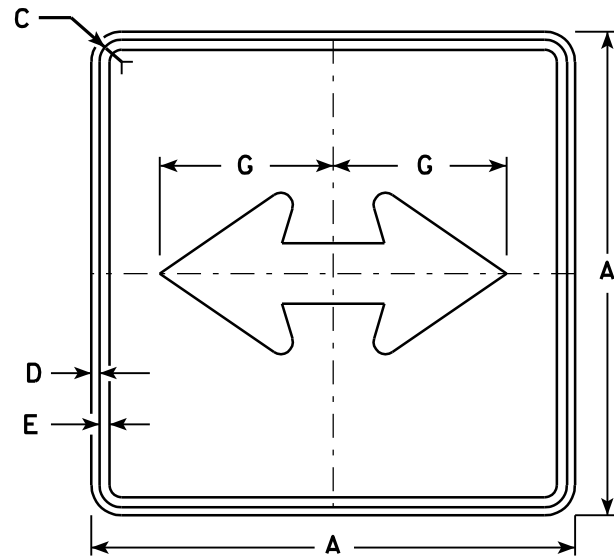
**STANDARD SIGN  
M6-1 & M6-2  
SERIES**

WISCONSIN DEPT OF TRANSPORTATION

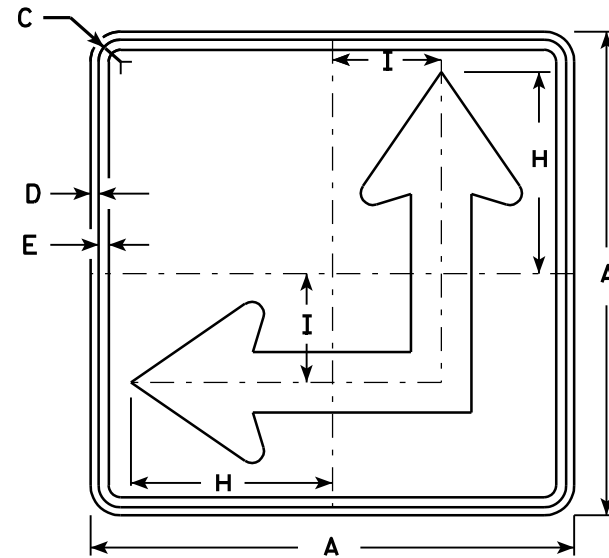
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 7/29/13 PLATE NO. M6-1.13

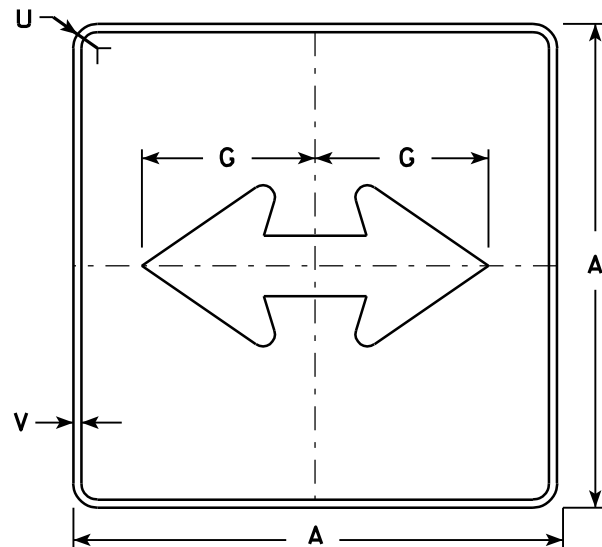
PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



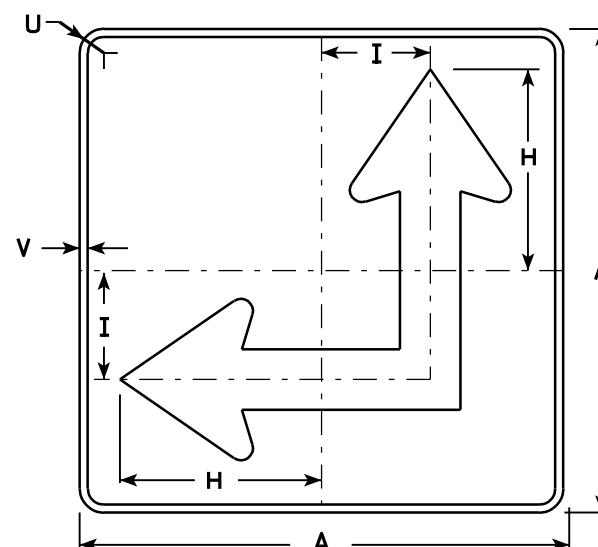
M6-4  
MK6-4  
MM6-4  
MO6-4  
MP6-4  
MR6-4



M6-6  
MK6-6  
MM6-6  
MO6-6  
MP6-6  
MR6-6



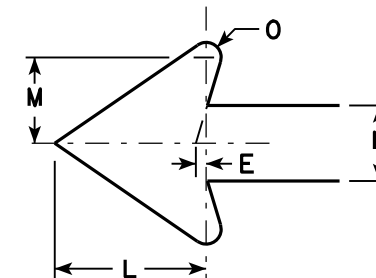
MB6-4  
MG6-4  
MN6-4



MB6-6  
MG6-6  
MN6-6

**NOTES**

- Signs are Type II - See Note 4 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:  
Background - See Note 4  
Message - See Note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-4 and M6-6 Background - White - Type H Reflective  
Message - Black  
MB6-4 and MB6-6 Background - Blue  
Message - White - Type H Reflective  
MG6-4 and MG6-6 Background - Green  
Message - White - Type H Reflective  
MK6-4 and MK6-6 Background - Green  
Message - White - Type H Reflective  
MM6-4 and MM6-6 Background - White - Type H Reflective  
Message - Green  
MN6-4 and MN6-6 Background - Brown  
Message - White - Type H Reflective  
MO6-4 and MO6-6 Background - Orange - Type F Reflective  
Message - Black  
MP6-4 and MP6-6 Background - White - Type H Reflective  
Message - Blue  
MR6-4 and MR6-6 Background - Brown  
Message - Yellow - Type H Reflective
- M6-6R same as M6-6L except arrow points ahead and right.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	8 3/4	4 1/4			5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

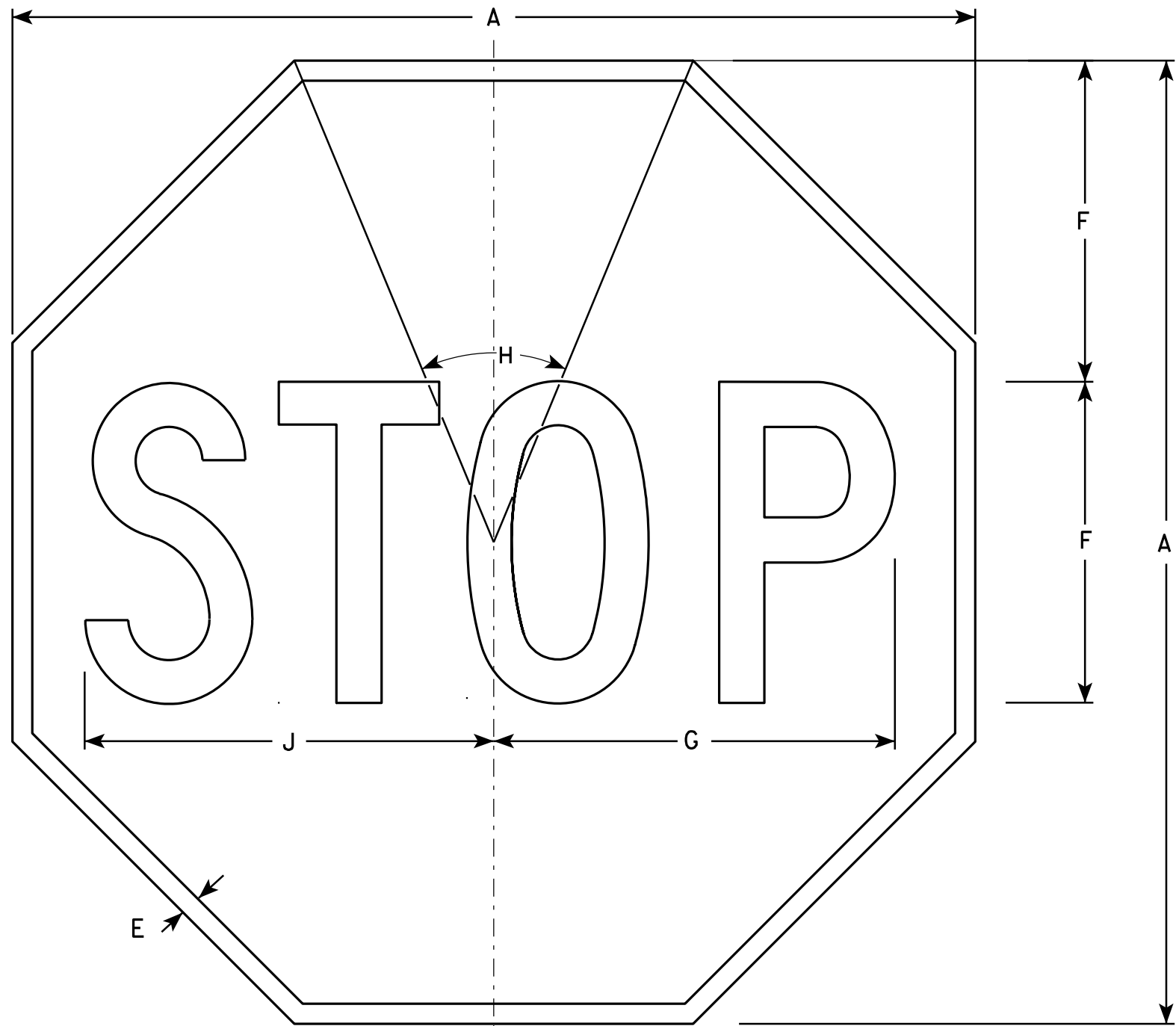
**STANDARD SIGN  
M6-4 & M6-6  
SERIES**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 7/29/13 PLATE NO. M6-4.8

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



**NOTES**

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Red  
Message - White
3. Message Series - C

7

7

R1-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. Ft.
1	24				3/8	8	10	45°		10 1/4																	3.31
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

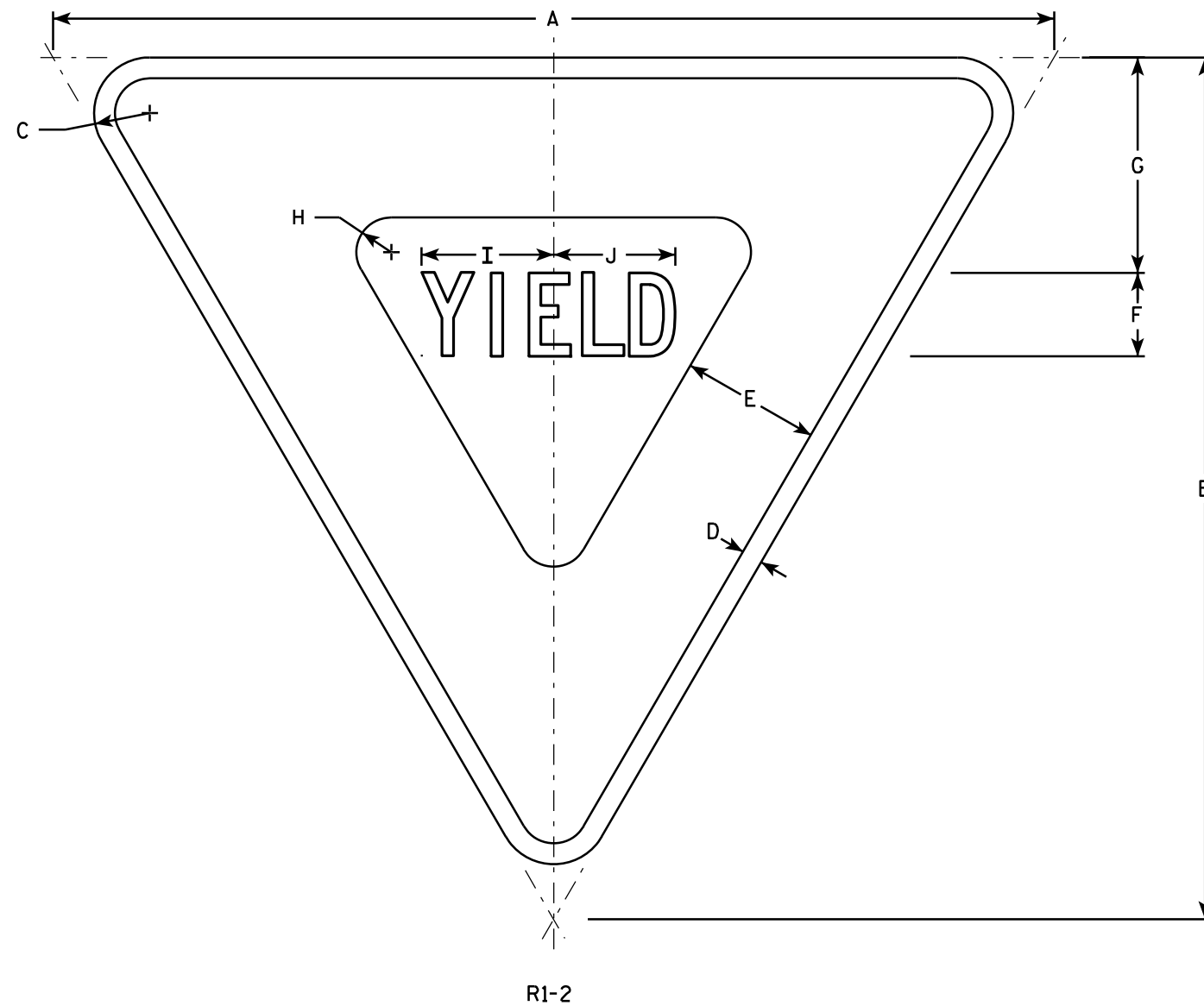
**STANDARD SIGN**  
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1.12

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



**NOTES**

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. The border strip and word message are reflectorized red.

R1-2

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	26	1 1/2	5/8	4	2 1/2	6 3/8	7/8	4	3 5/8																	2.71
2S	36	31	2	3/4	5	3	7 3/4	1 1/4	4 3/4	4 3/8																	3.88
2M	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
3	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
4	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
5	60	52	3	1 1/2	8	5	13	2 1/2	7 7/8	7 1/4																	10.83
6	24	21	1 1/2	3/8	3	2	4 3/4	7/8	3 1/4	3																	1.75
7	18	15 1/2	1	3/8	2 1/2	1 1/2	3 7/8	5/8	2 3/8	2 1/4																	0.97

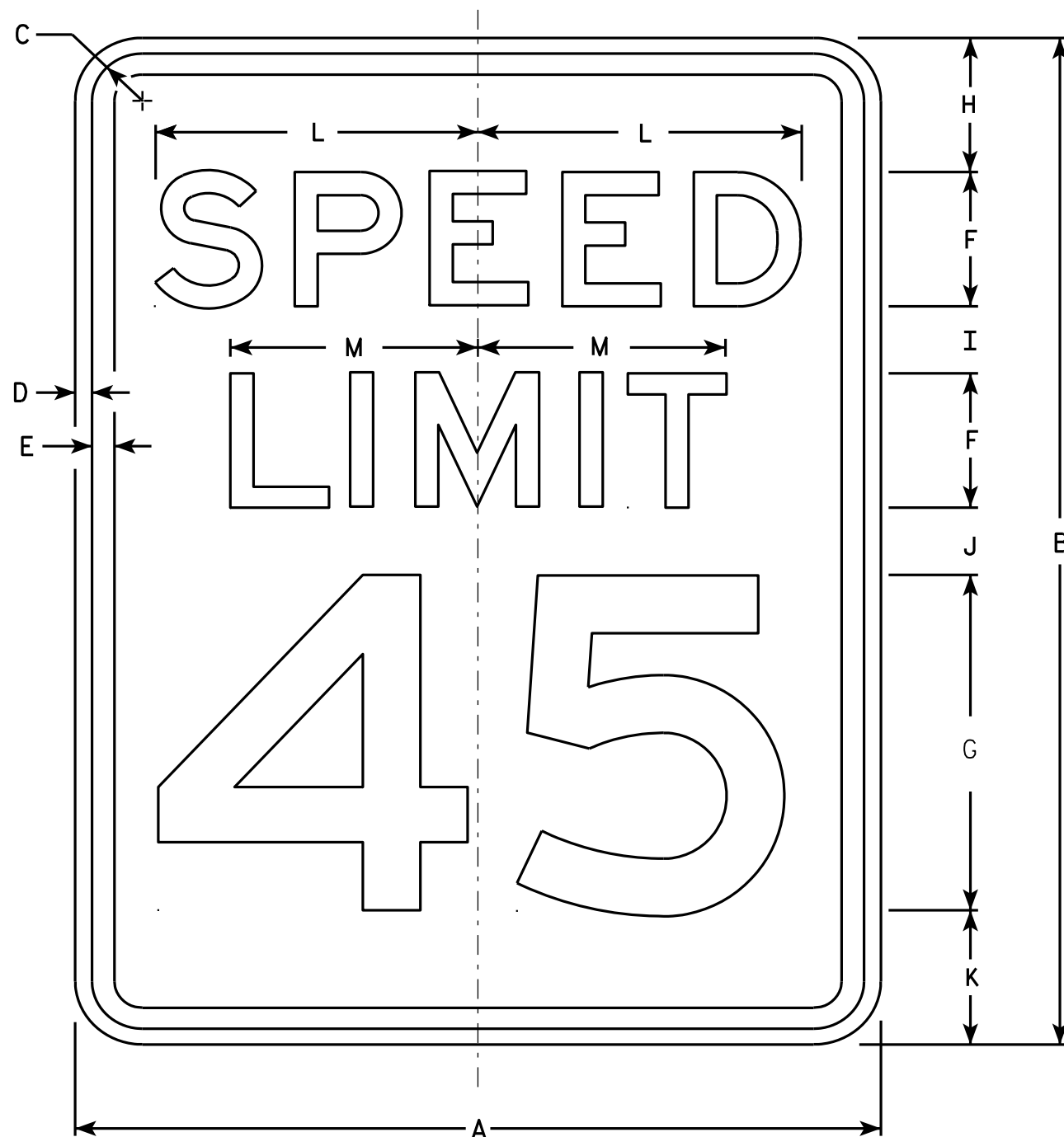
**STANDARD SIGN**  
**R1-2**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 11/02/10 PLATE NO. R1-2.11

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN  
R2-1

WISCONSIN DEPT OF TRANSPORTATION

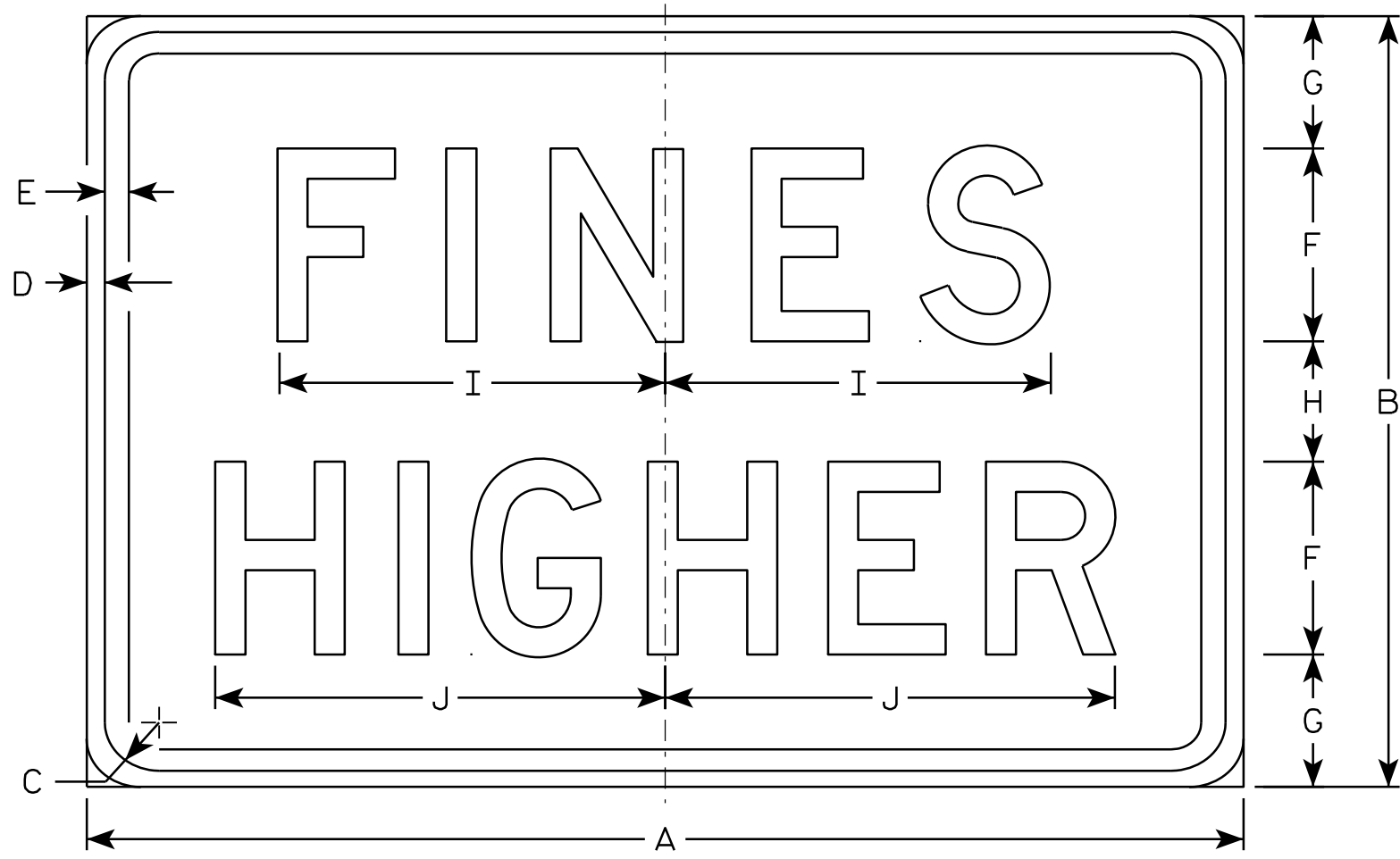
APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R2-6P

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	18	1 1/8	3/8	3/8	4	3 1/2	3	7 5/8	9 1/4																	3.0
2M	24	18	1 1/8	3/8	3/8	4	3 1/2	3	7 5/8	9 1/4																	3.0
3	36	24	1 1/8	3/8	1/2	6	4 1/8	3 3/4	12	14																	6.0
4	36	24	1 1/8	3/8	1/2	6	4 1/8	3 3/4	12	14																	6.0
5	48	36	1 3/8	1/2	5/8	8	7	6	15 1/8	19																	12.0

STANDARD SIGN  
R2-6P

WISCONSIN DEPT OF TRANSPORTATION

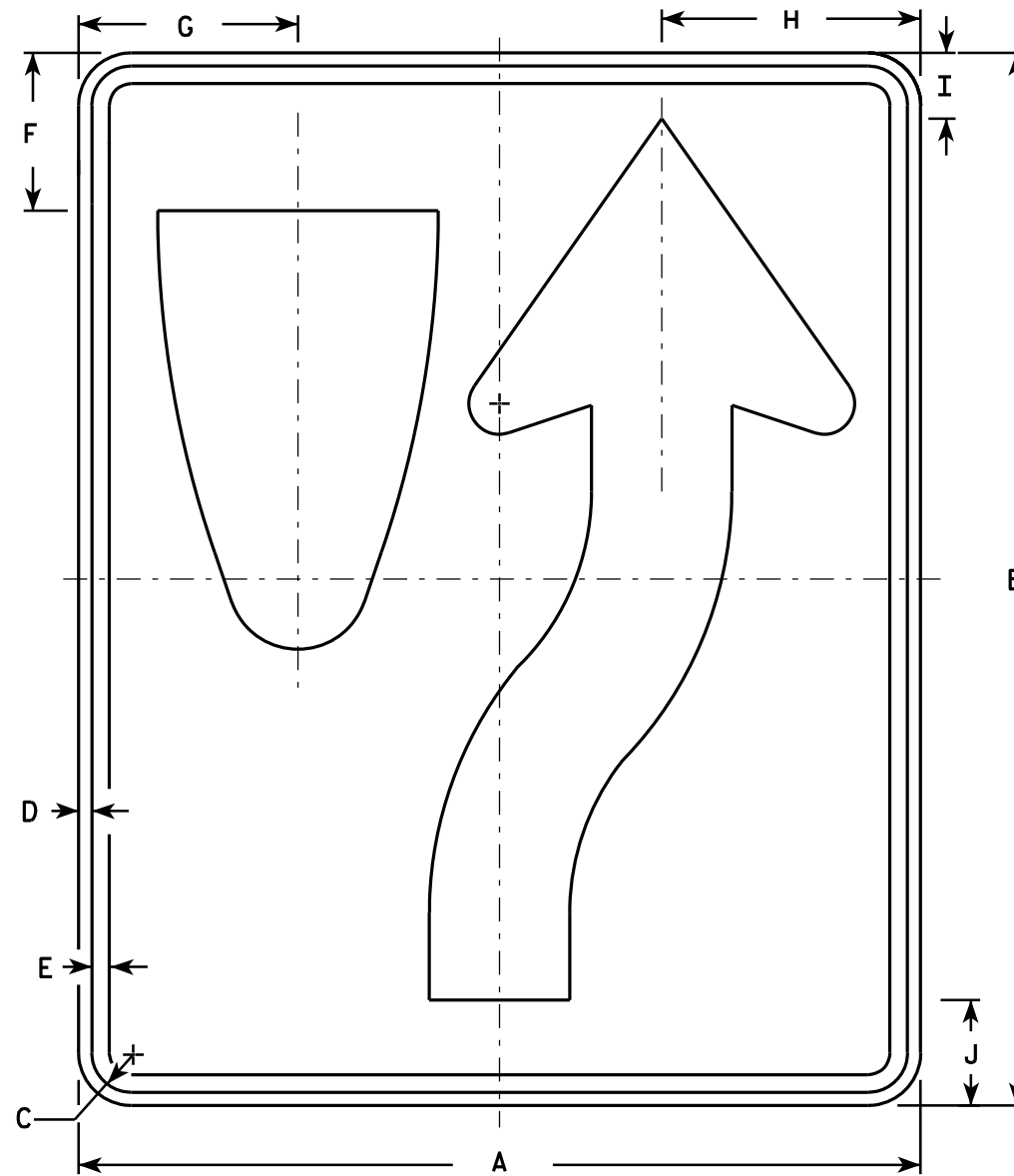
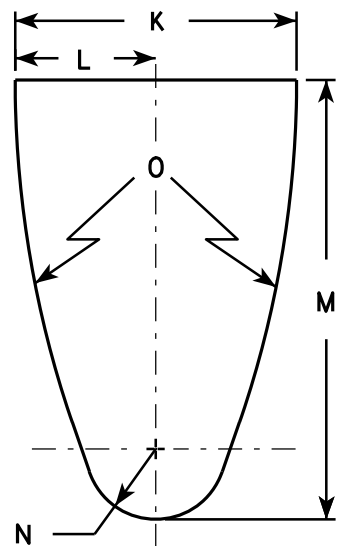
APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 12/20/10 PLATE NO. R2-6P.2

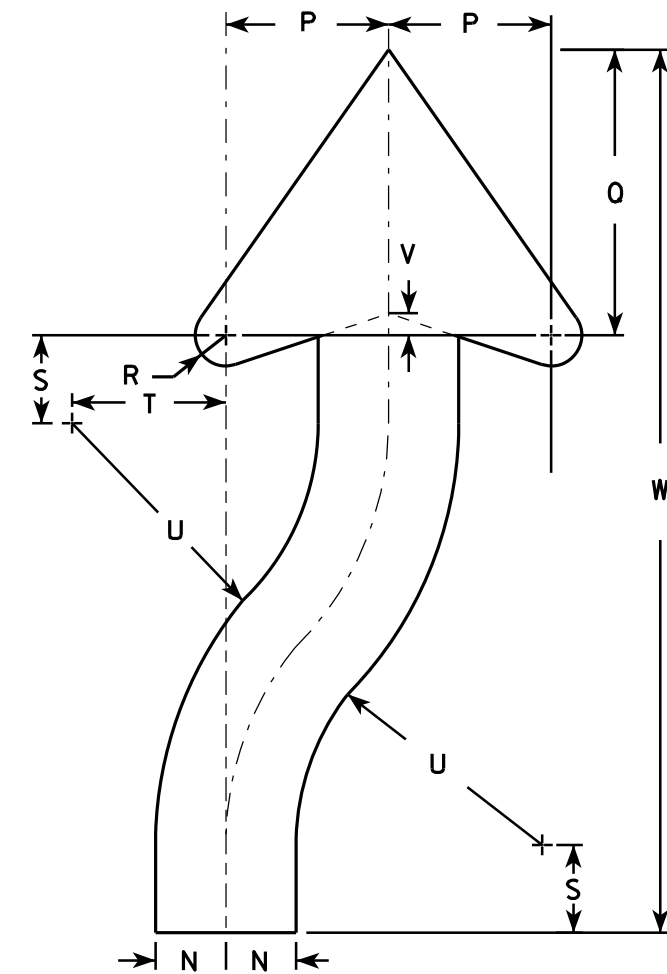
PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E

**NOTES**

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. material is plywood but borders shall be rounded
2. Color:  
Background - White  
Message - Black
3. Corners may be square or rounded when base as shown. When base material is metal, the corners and borders shall be rounded.
4. R4-8 is the same as R4-7 except Legend is reversed.



R4-7



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3 3/8	4 3/4	5 1/2	1 3/8	2 1/4	6	3	9 3/8	1 1/2	22 1/2	3 1/2	6 1/8	5/8	1 7/8	3 1/4	6 3/4	1/2	20 3/8				3.0
2S	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
2M	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
3	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
4	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
5	48	60	2 1/4	3/4	1	9	12 1/2	14 3/4	3 3/4	6	16	8	25	4	60	9 1/4	16 1/4	1 5/8	5	8 3/4	18	1 1/4	50 1/4				20.0

**STANDARD SIGN**  
R4-7 & R4-8

WISCONSIN DEPT OF TRANSPORTATION

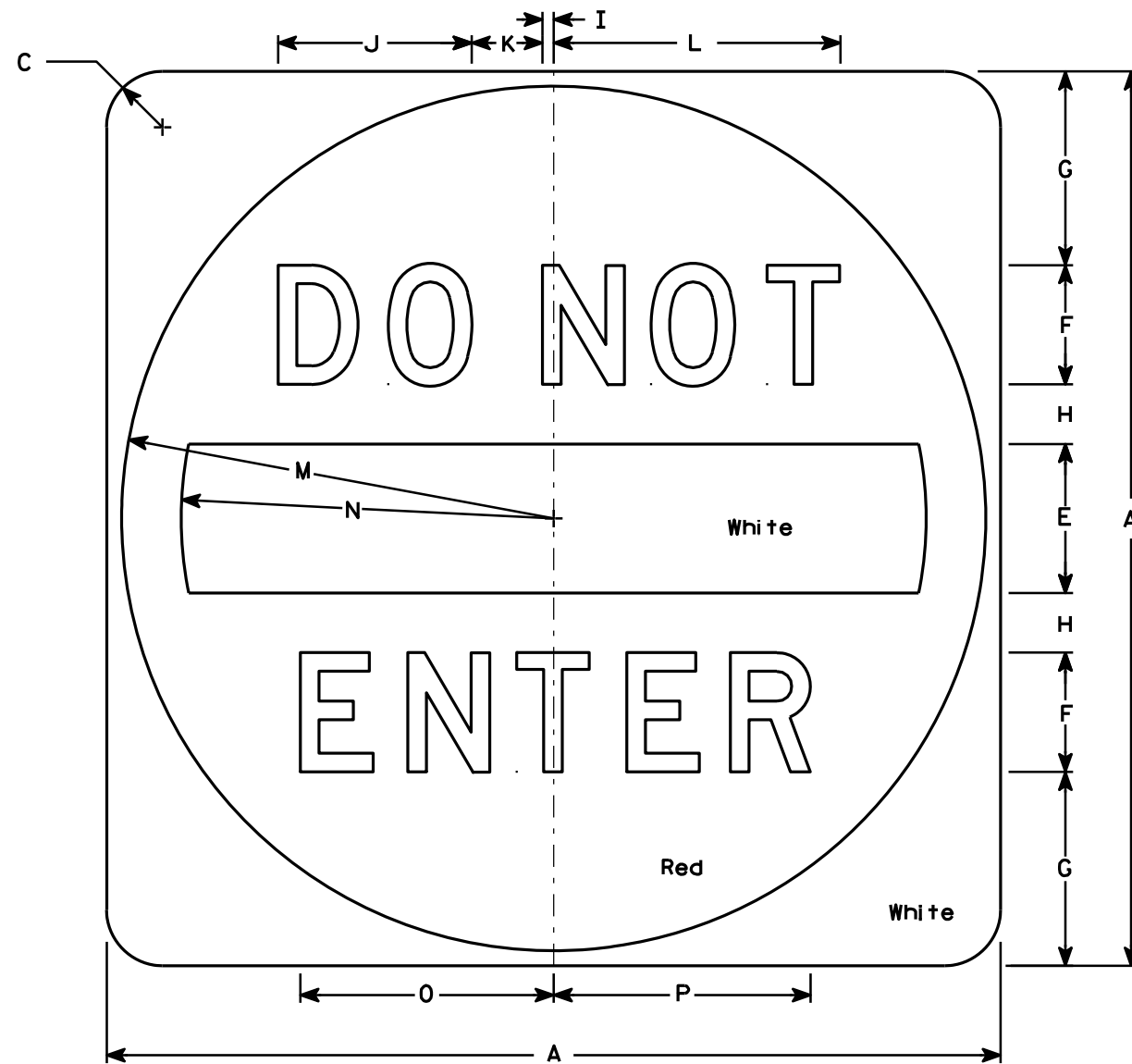
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-7.8

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - See detail  
Message - White - Type H Reflective
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but when base material is metal, the corners shall be rounded.



R5-1

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30		1 7/8		5	4	6 1/2	2	3/8	6 1/2	2 3/8	9 5/8	14 1/2	12 1/2	8 1/2	8 5/8											6.26
2M	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
3	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
4	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
5	48		3		8	6	11	3	5/8	9 3/4	3 5/8	14 1/2	23 1/2	20	12 3/4	12 7/8											16.0

**STANDARD SIGN**  
**R5-1**

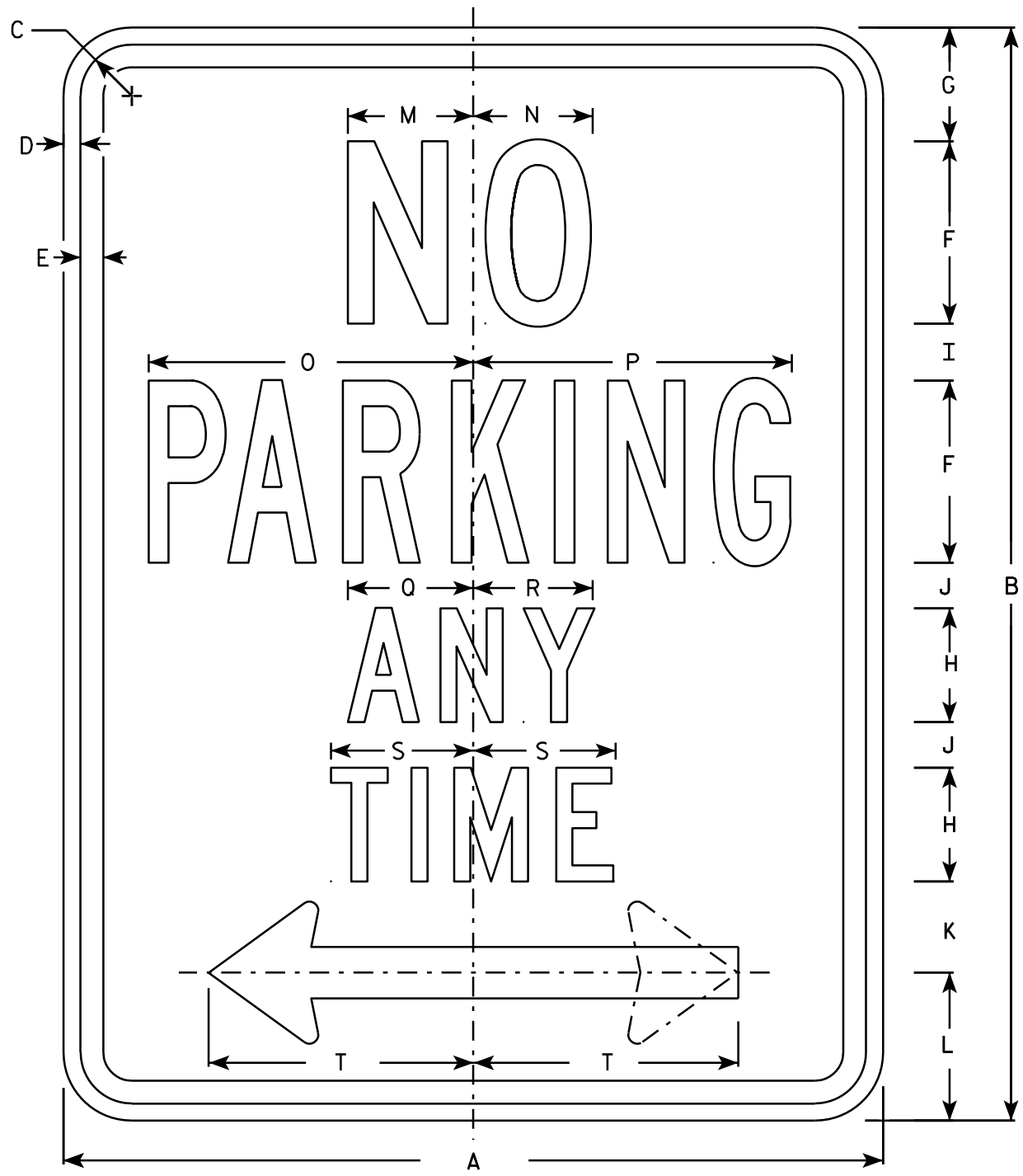
*WISCONSIN DEPT OF TRANSPORTATION*

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 12/17/10 PLATE NO. R5-1.15

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E

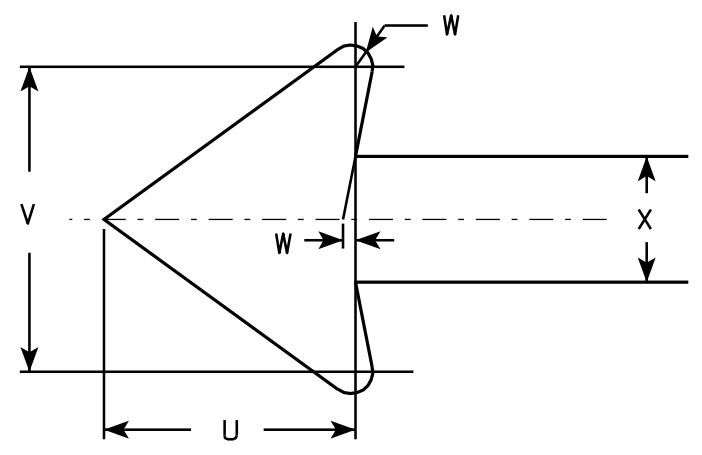




R7-1

**NOTES**

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - Red
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 3 and 4 are series C, line 2 is series B.
6. R7-1D (double arrow)  
R7-1L (left arrow)  
R7-1R (right arrow)



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 1/4	2 1/8	2 1/2	3 7/8	1 1/2	1 3/4	1/8	3/4			1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	2 3/4	2 5/8	3 1/8	5 7/8	2 1/4	2 5/8	1/4	1 1/8			3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
4																											
5																											

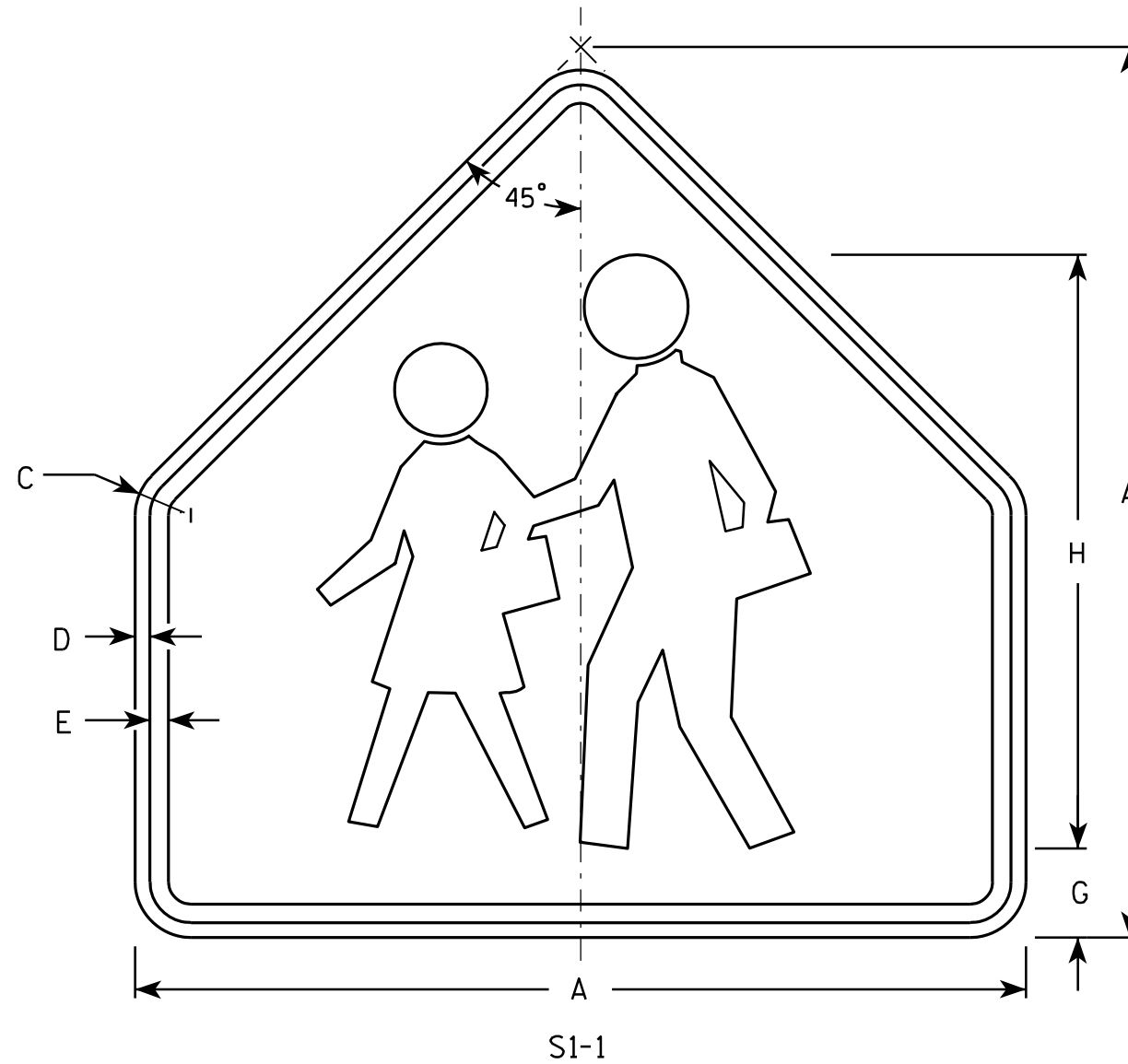
**STANDARD SIGN**  
R7-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/31/2011 PLATE NO. R7-1.9

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow-Green  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8		3	20																			4.69
2	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
3	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
4	48		2 1/4	3/4	1		4 3/4	32																			12
5																											

**STANDARD SIGN**  
S1-1

WISCONSIN DEPT OF TRANSPORTATION

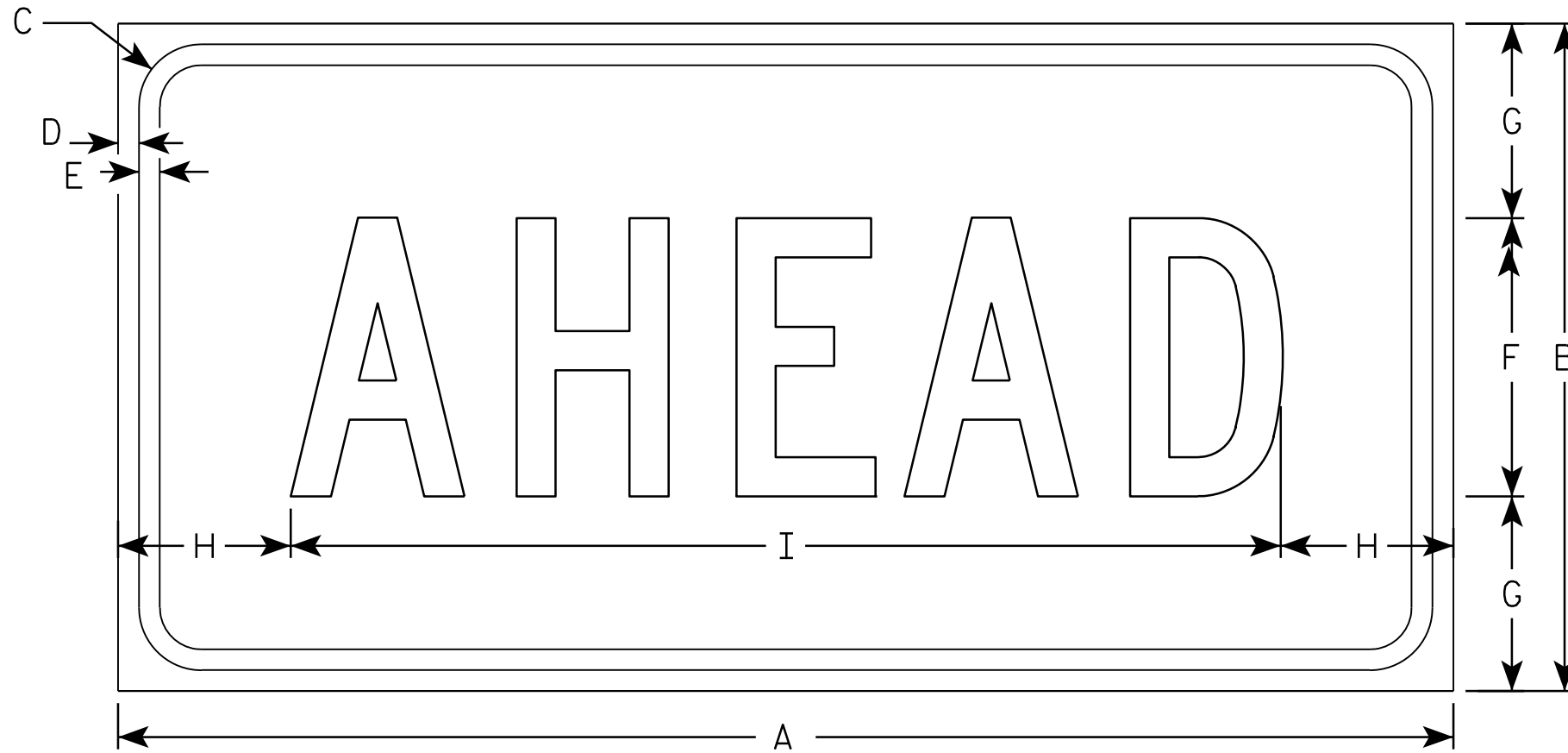
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 6/30/05 PLATE NO. S1-1.8

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E

**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow-Green  
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



S16-9P

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24	12	1 1/8	3/8	3/8	5	3 1/2	3 1/8	17 3/4																		2.0
2S	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
2M	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
3	30	18	1 1/8	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
4	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5																											

STANDARD SIGN

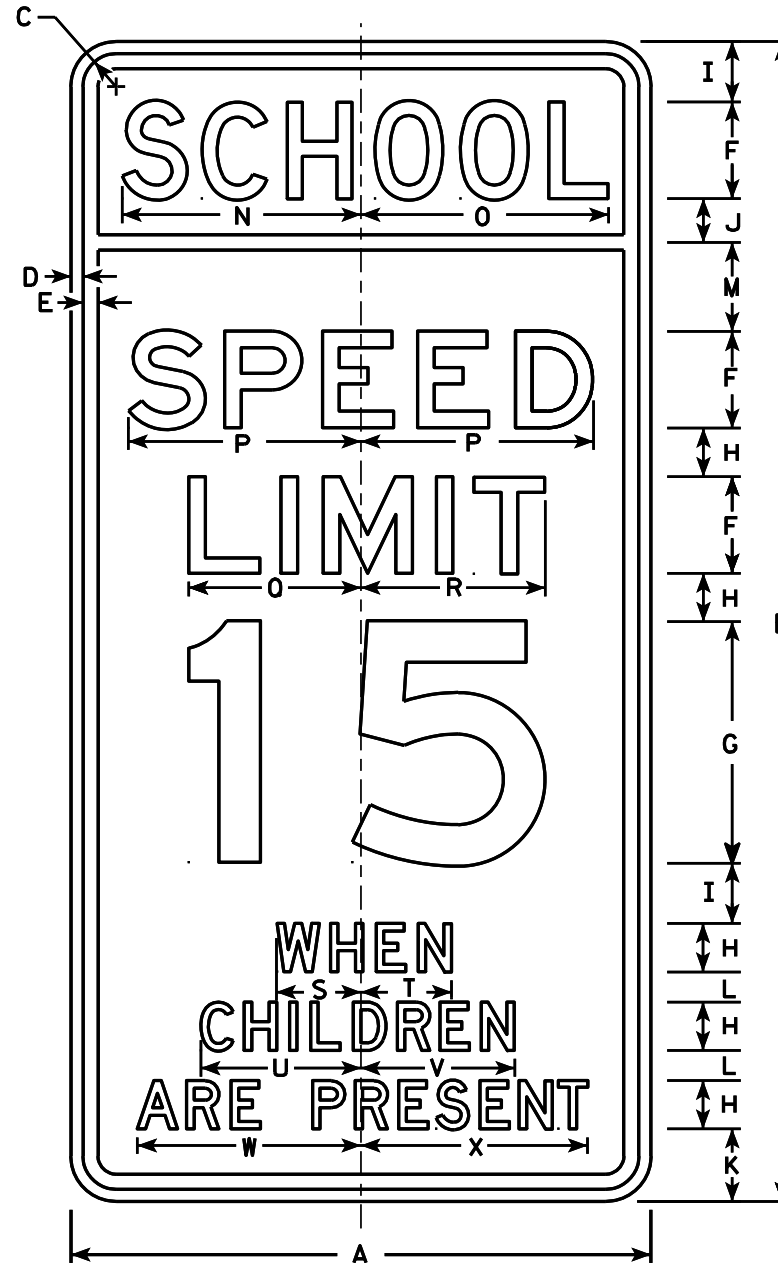
S16-9P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 7/22/13 PLATE NO. S16-9P.1

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



S4-51

**NOTES**

1. Sign is Type II - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. (See note 5).
2. Color:  
Background - See note 5  
Message - Black
3. Message Series - See note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Top panel (SCHOOL) background - Yellow Green -Type F Reflective. Lower panel background - White -Type H Reflective.
6. From top to bottom:  
Lines 1, 5, 6 & 7 are series D  
Lines 2, 3 & 4 are series E
7. Line 4 substitute appropriate numerals and adjust spacing to achieve proper balance.

7

7

Metric equivalent for this sign is:

SIZE	
1	
2	600 mm X 1200 mm
3	900 mm X 1800 mm
4	
5	

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m <sup>2</sup>
1																												
2	24	48	1 3/8	1/2	5/8	4	10	2	2 1/2	1 3/4	3	1 1/4	3 3/4	9 7/8	10 1/4	9 5/8	7 1/8	7 5/8	3 1/2	3 3/8	6 5/8	6 3/8	9 1/4	9 3/8		8.00	0.72	
3	36	72	2 1/4	3/4	1	6	15	3	3 3/4	2 3/4	4 1/2	1 7/8	5 1/2	15	15 1/4	14 1/2	11 1/4	11 1/2	5 1/2	5 3/4	10	9 3/4	14	14 1/8		18.00	1.62	
4																												
5																												

**STANDARD SIGN**  
**S4-51**

WISCONSIN DEPT OF TRANSPORTATION

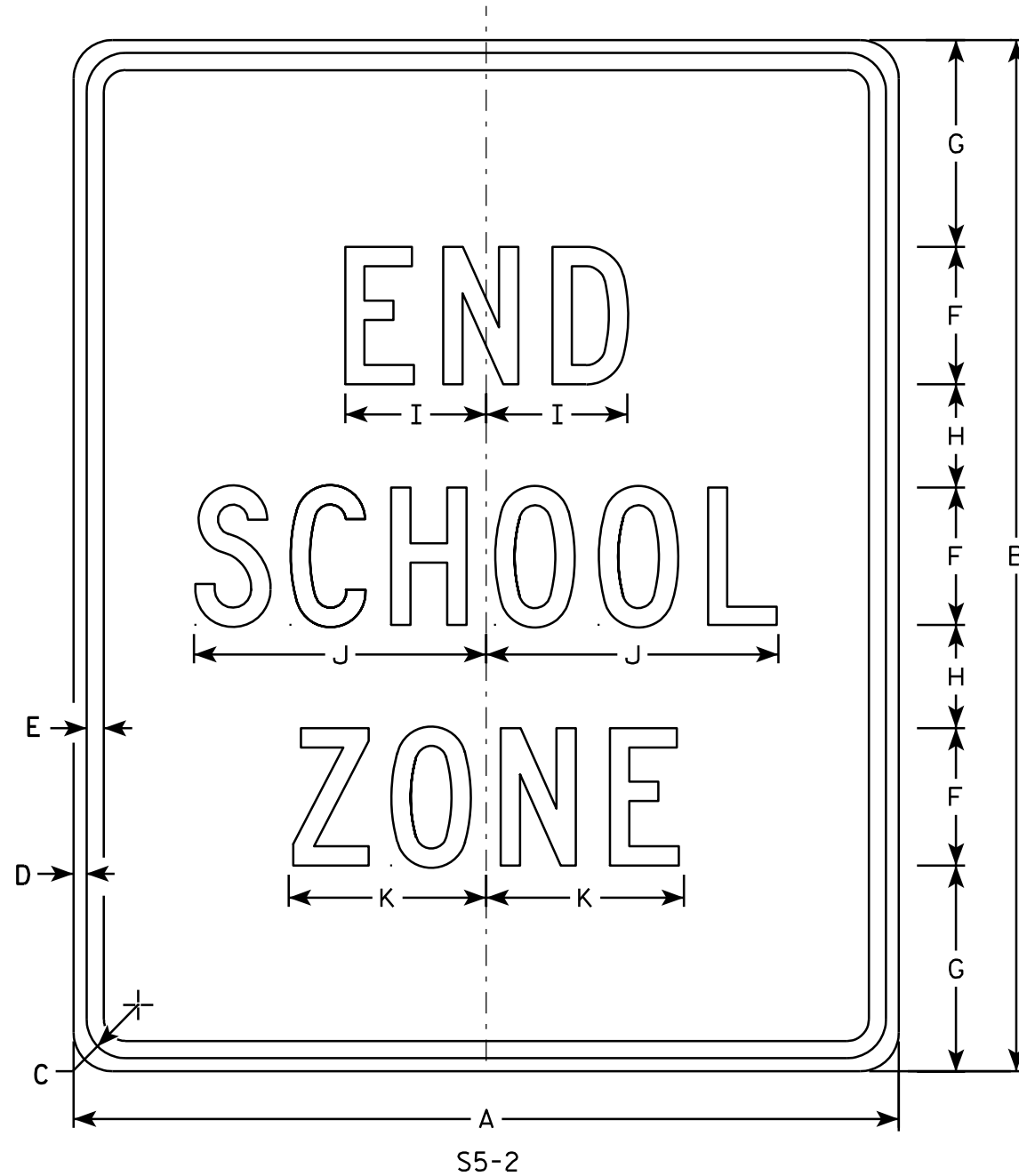
APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 4/26/10 PLATE NO. S4-51.9

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	30	1 1/8	3/8	1/2	4	6	3	4 1/8	8 1/2	5 3/4																5.0
3	36	42	1 5/8	5/8	3/4	6	7	5	6 1/8	12 5/8	8 5/8																10.5
4																											
5																											

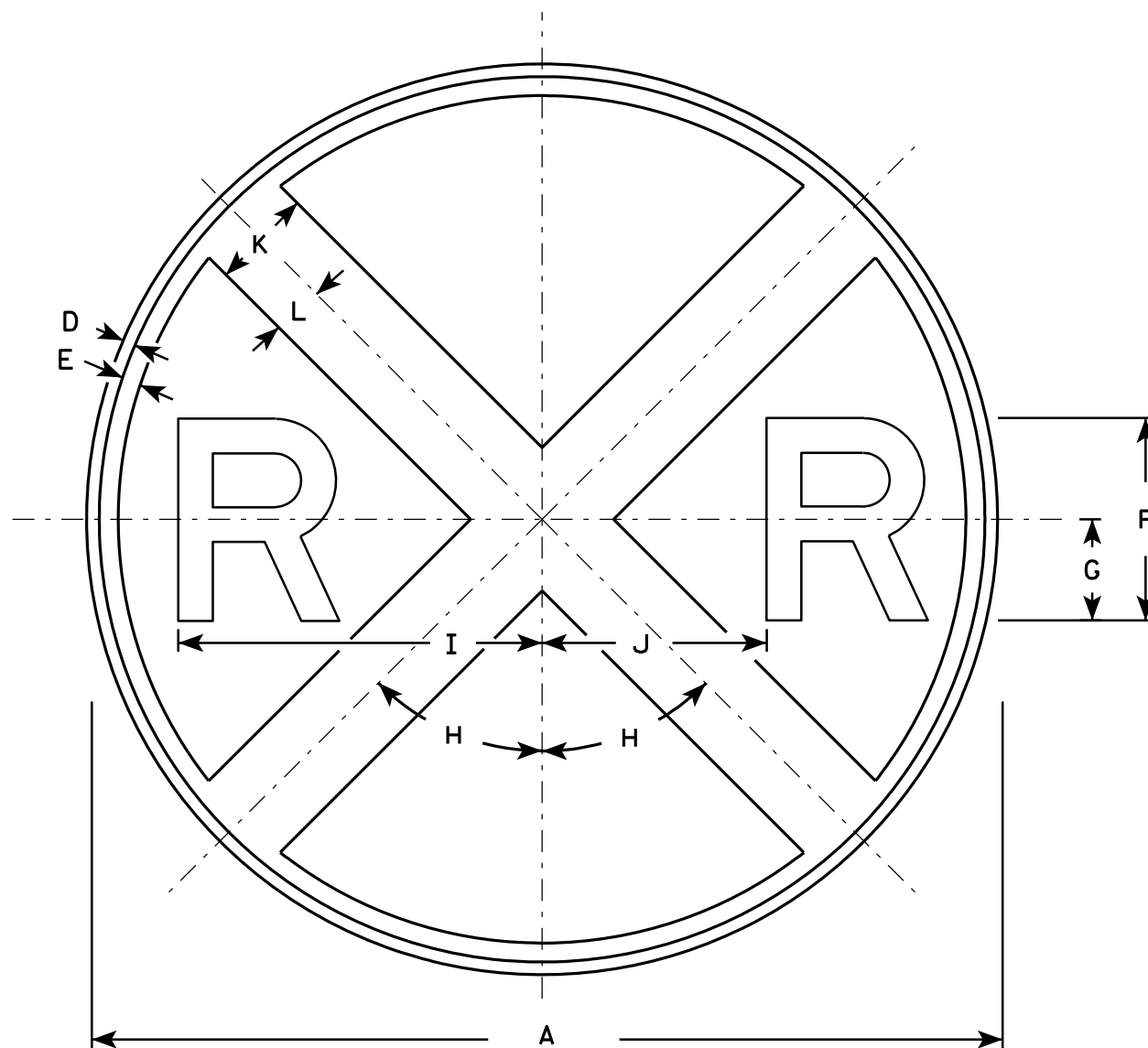
**STANDARD SIGN**  
S5-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 3/8/11 PLATE NO. S5-2.2

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



W10-1

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - E

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30			3/8	5/8	7	3 1/2	45°	12 3/8	7 1/8	3	1 1/2															4.91
2S	36			5/8	3/4	8	4	45°	14 3/8	8 5/8	4	2															7.07
2M	36			5/8	3/4	8	4	45°	14 3/8	8 5/8	4	2															7.07
3																											
4	48			3/4	1 1/4	10	5	45°	18 3/8	11 5/8	5	2 1/2															12.57
5																											

**STANDARD SIGN**  
W10-1

WISCONSIN DEPT OF TRANSPORTATION

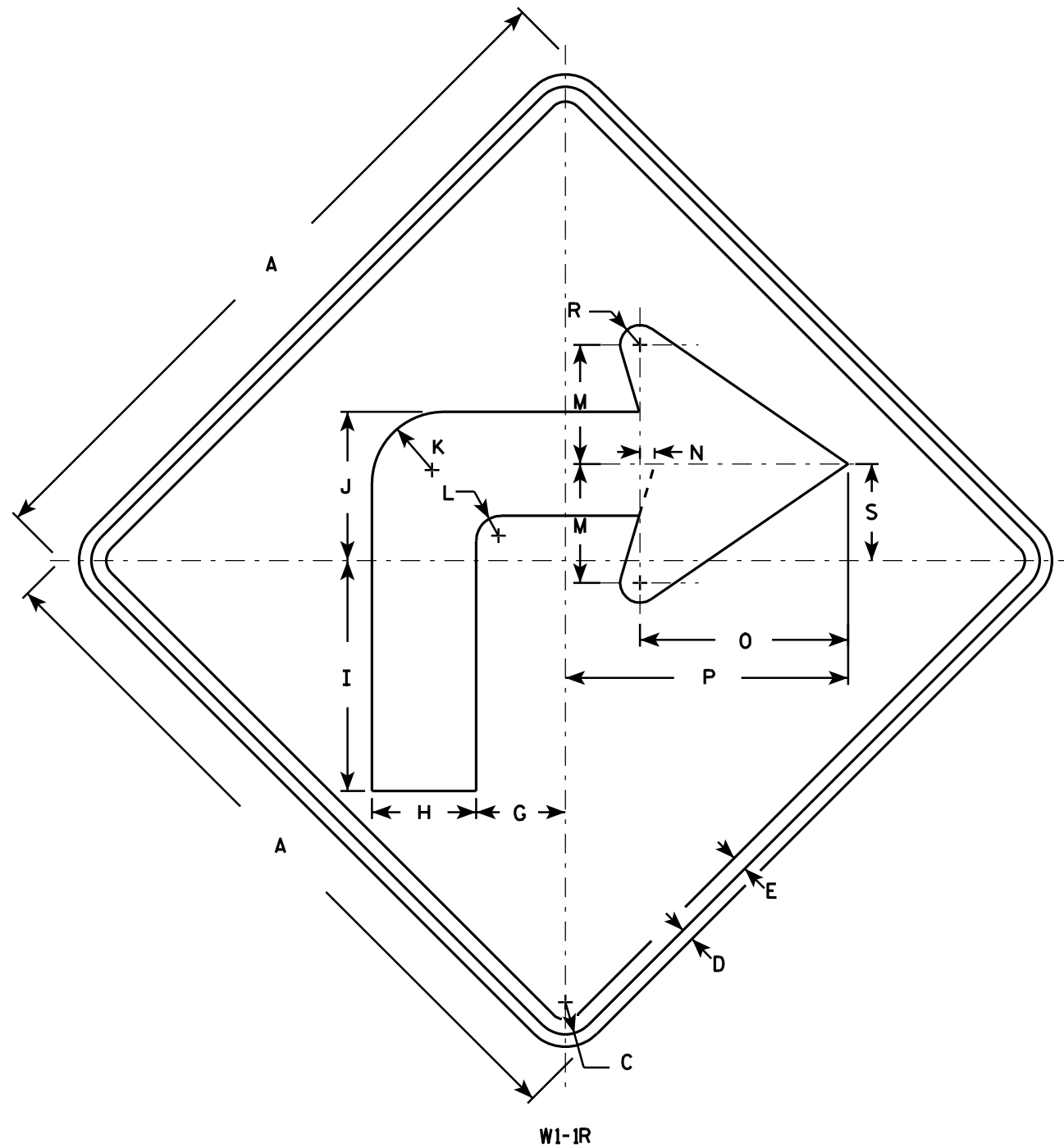
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/13/13 PLATE NO. W10-1.8

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-1L is the same as W1-1R except the arrow is reversed along the vertical centerline.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		3	3 1/2	7 3/4	5	2 1/2	7/8	4	1/2	7	9 1/2		5/8	3 1/4								4.0
2S	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
2M	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
3	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
4	48		2 1/4	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0
5	48		2 1/4	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0

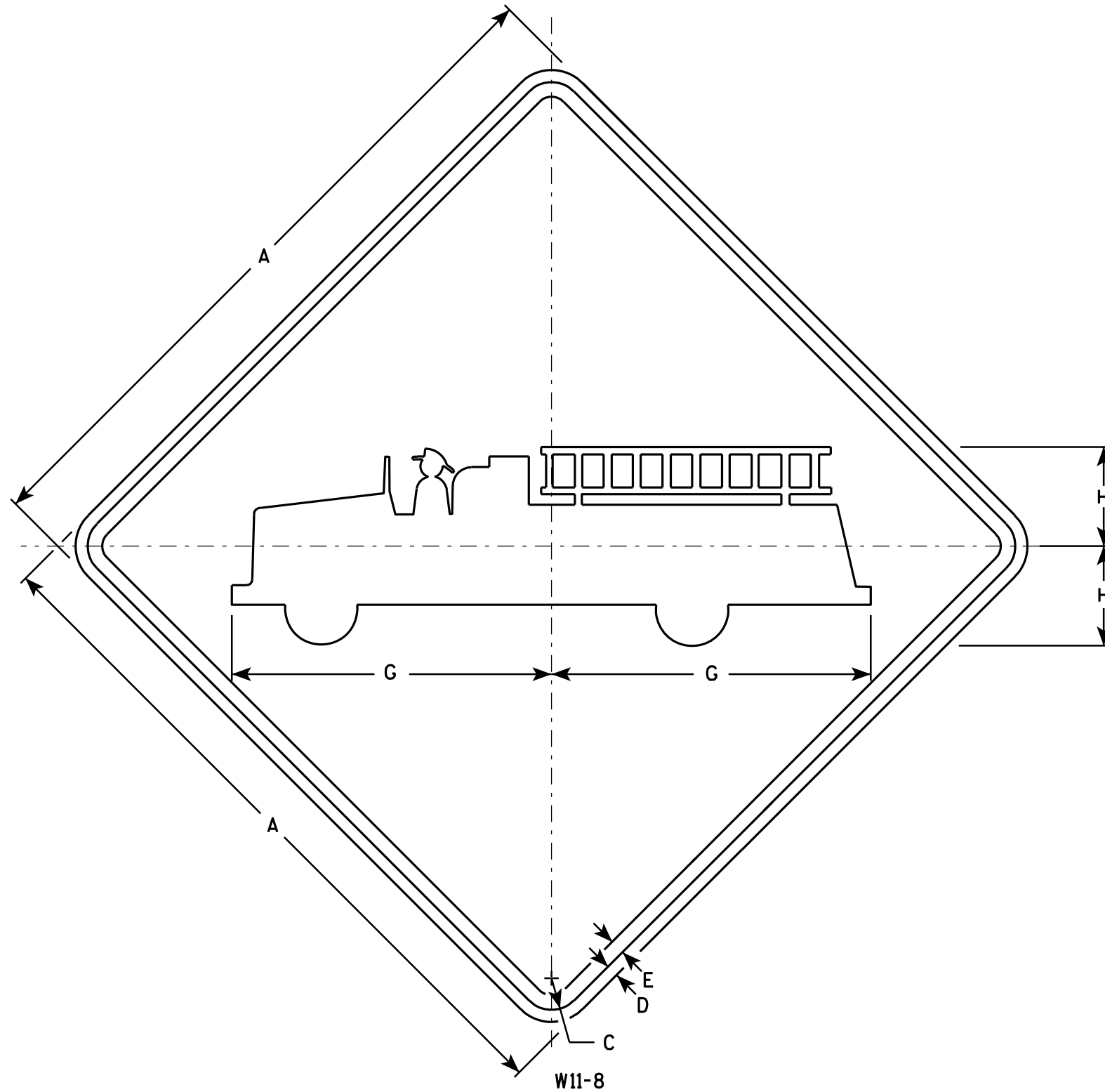
**STANDARD SIGN**  
**W1-1**

*WISCONSIN DEPT OF TRANSPORTATION*

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/15/12 PLATE NO. W1-1.11

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ **E**



**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		11	3 3/8																			4.0
2S	30		1 3/8	1/2	5/8		13 3/4	4 3/8																			6.25
2M	30		1 3/8	1/2	5/8		13 3/4	4 3/8																			6.25
3	36		1 5/8	5/8	3/4		16 1/2	5 1/4																			9.0
4	48		2 1/4	3/4	1		22	7																			16.0
5																											

**STANDARD SIGN**  
W11-8

WISCONSIN DEPT OF TRANSPORTATION

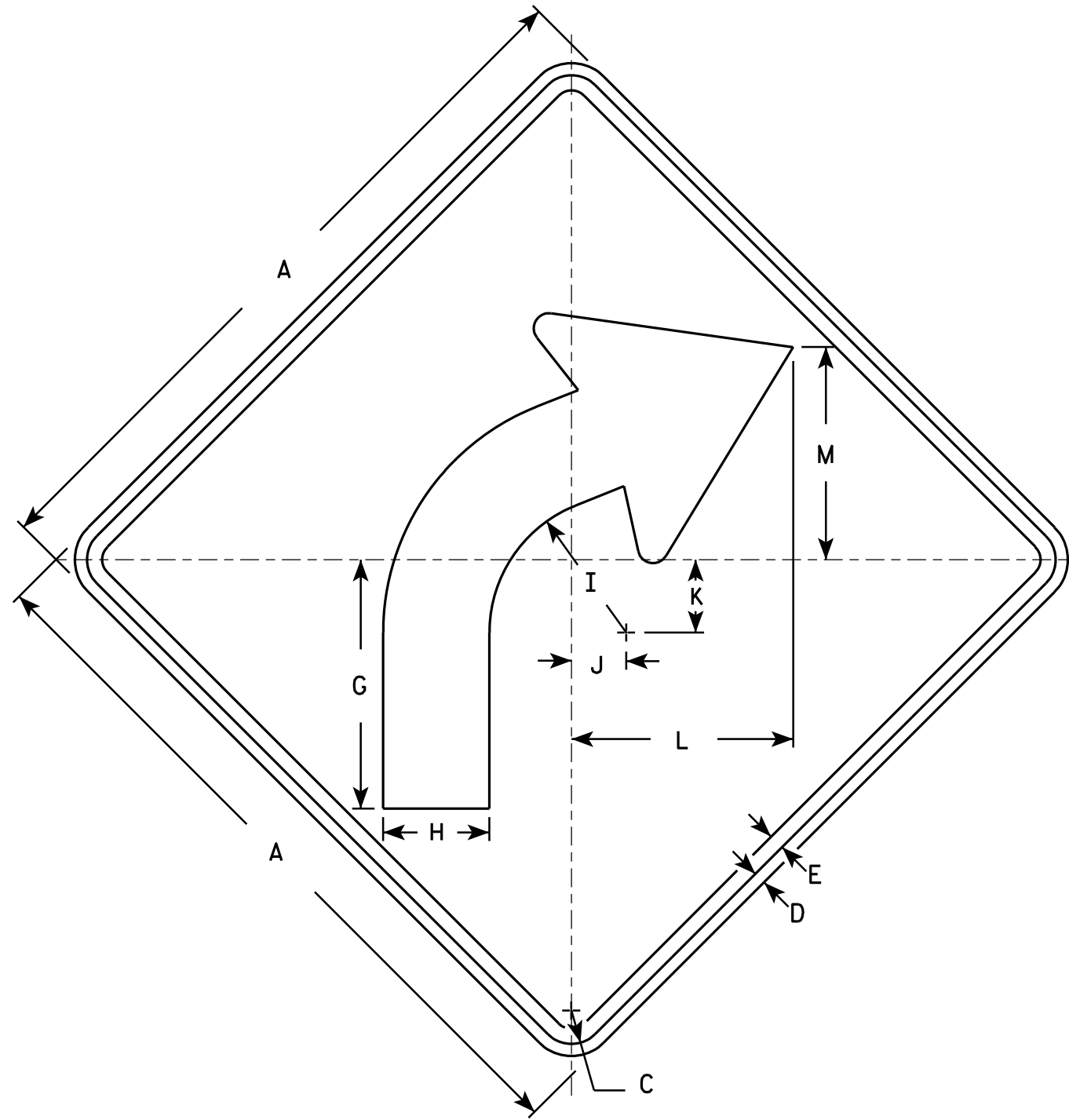
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/13/13 PLATE NO. W11-8.7

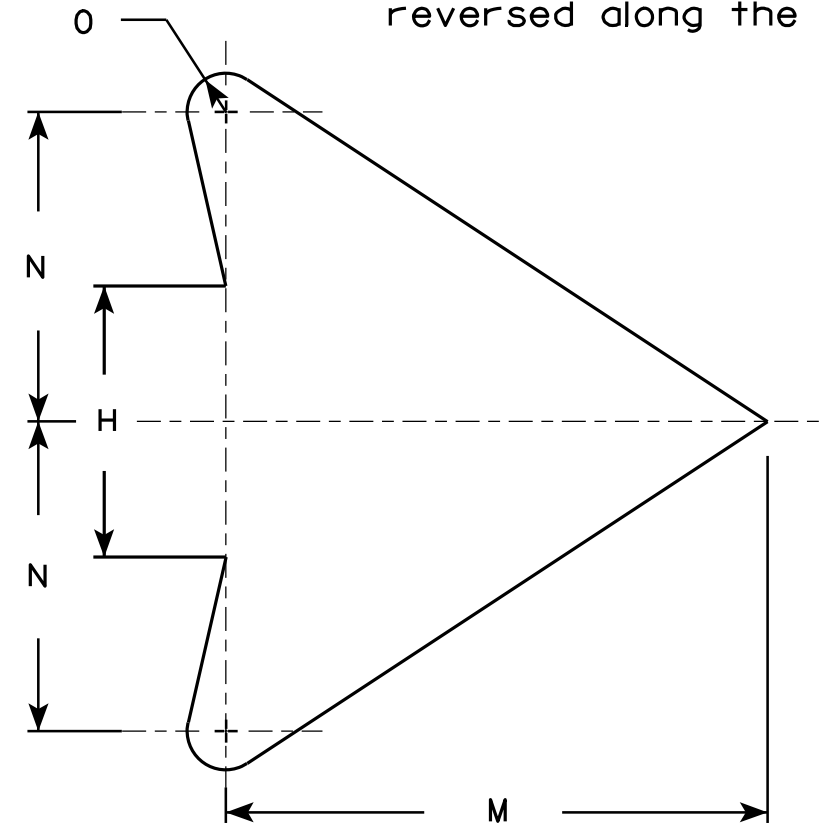


**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-2L is the same as W1-2R except the arrow is reversed along the vertical centerline.



W1-2R



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2												4.0
2S	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8												6.25
2M	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1												16.0

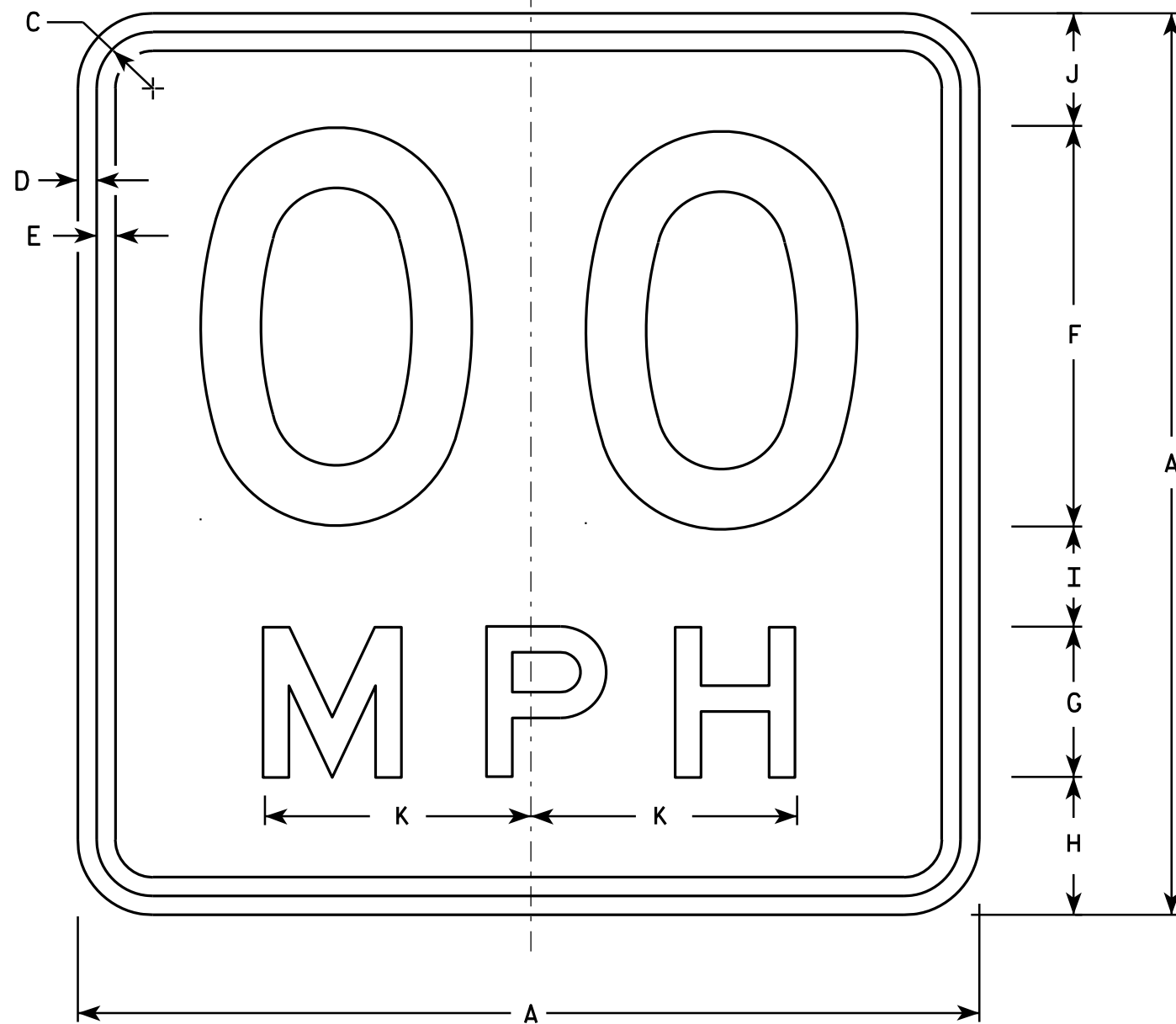
STANDARD SIGN  
W1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 5/15/12 PLATE NO. W1-2.10

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D  
Line 2 is Series E

W13-1

\* For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.  
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

STANDARD SIGN

W13-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

PROJECT NO:

HWY:

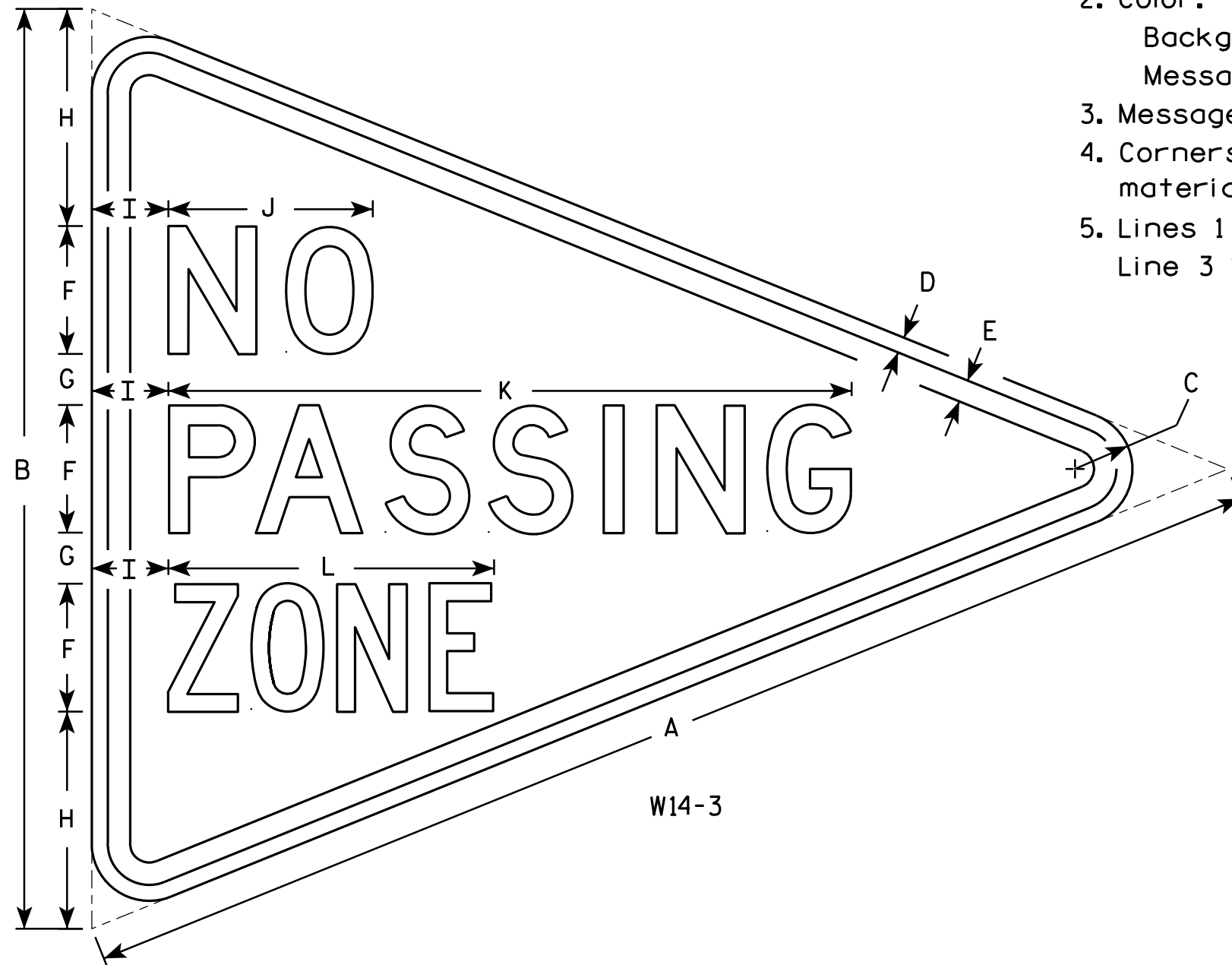
COUNTY:

SHEET NO:

E

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - See note 5
4. Corners and borders shall be rounded on all base materials for this sign.
5. Lines 1 and 2 are Series D.  
Line 3 is series C.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
2M	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
3	64	48	3	3/4	1 1/4	6	3	12	4	10 3/4	33 5/8	16 1/2															10.7
4																											
5																											

**STANDARD SIGN**  
**W14-3**

WISCONSIN DEPT OF TRANSPORTATION

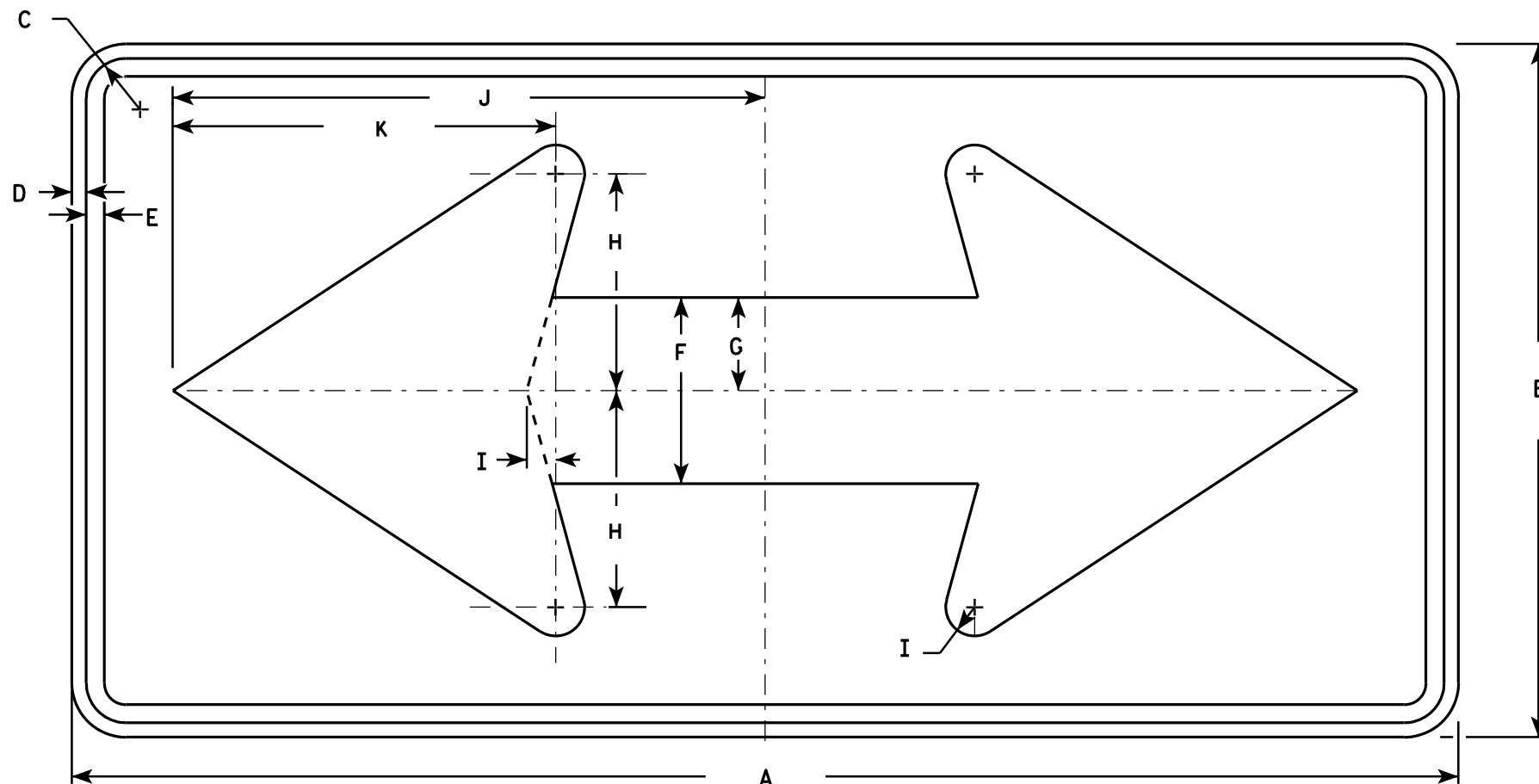
APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 6/7/10 PLATE NO. W14-3.9

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**

**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W1-7

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	1/2	5	2 1/2	5 3/4	3/4	15 5/8	10 1/8																4.5
2S	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
2M	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
3	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
4	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
5	96	48	2 1/4	3/4	1	13	6 1/2	15	2	41	26 1/2																32.0

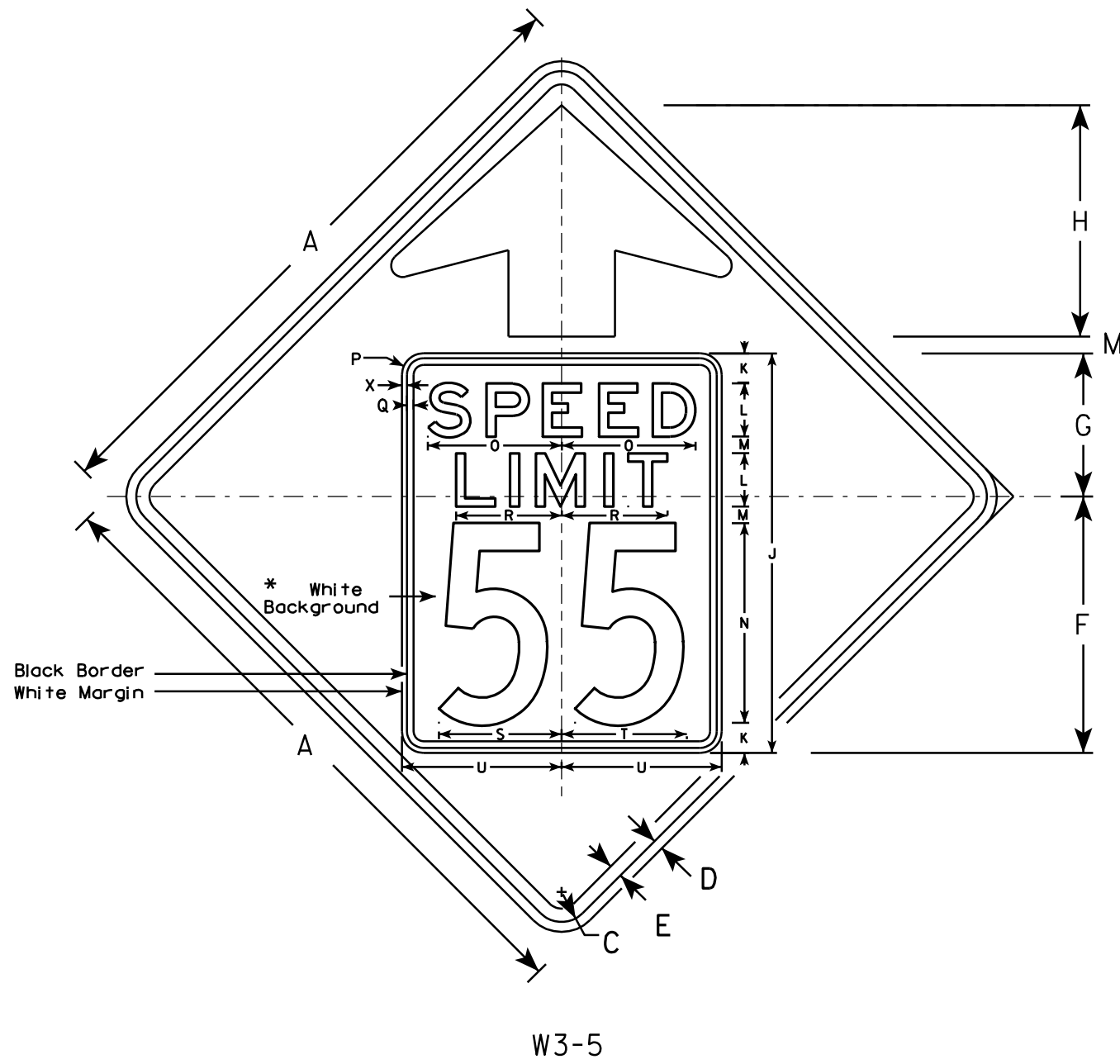
**STANDARD SIGN**  
W1-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-7.7

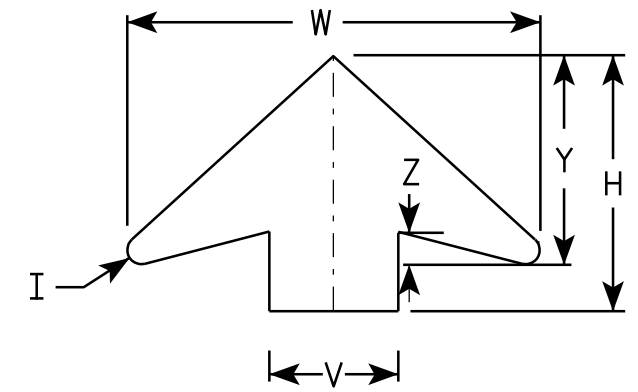
PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color: \*  
Background - YELLOW\*  
Message - BLACK
3. Message Series - C for numbers Series E for wording
4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

\*Speed Limit Sign shall have a White Background



ARROW DETAIL

7

7

W3-5

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36		1 5/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 5/8	9.0
2M	36		1 5/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 5/8	9.0
3	36		1 5/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 5/8	9.0
4	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0
5	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0

**STANDARD SIGN**  
**W3-5**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W3-5.5

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W4-4P

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	3	2 1/4	1 1/2	8	1 1/2	3/8	10	6 1/4	2 1/4	2 1/4	6 1/4											2.0
2M	24	12	1 1/8	3/8	3/8	3	2 1/4	1 1/2	8	1 1/2	3/8	10	6 1/4	2 1/4	2 1/4	6 1/4											2.0
3	36	15	1 1/8	3/8	1/2	4	2 5/8	1 3/4	10 3/4	2 3/8	3/8	13 1/2	8 3/8	3	3 1/8	8 3/8											3.75
4	42	18	1 1/8	3/8	1/2	5	3	2	13 3/8	3 1/8	3/8	16 7/8	10 1/2	3 5/8	3 7/8	10 3/8											5.25
5																											

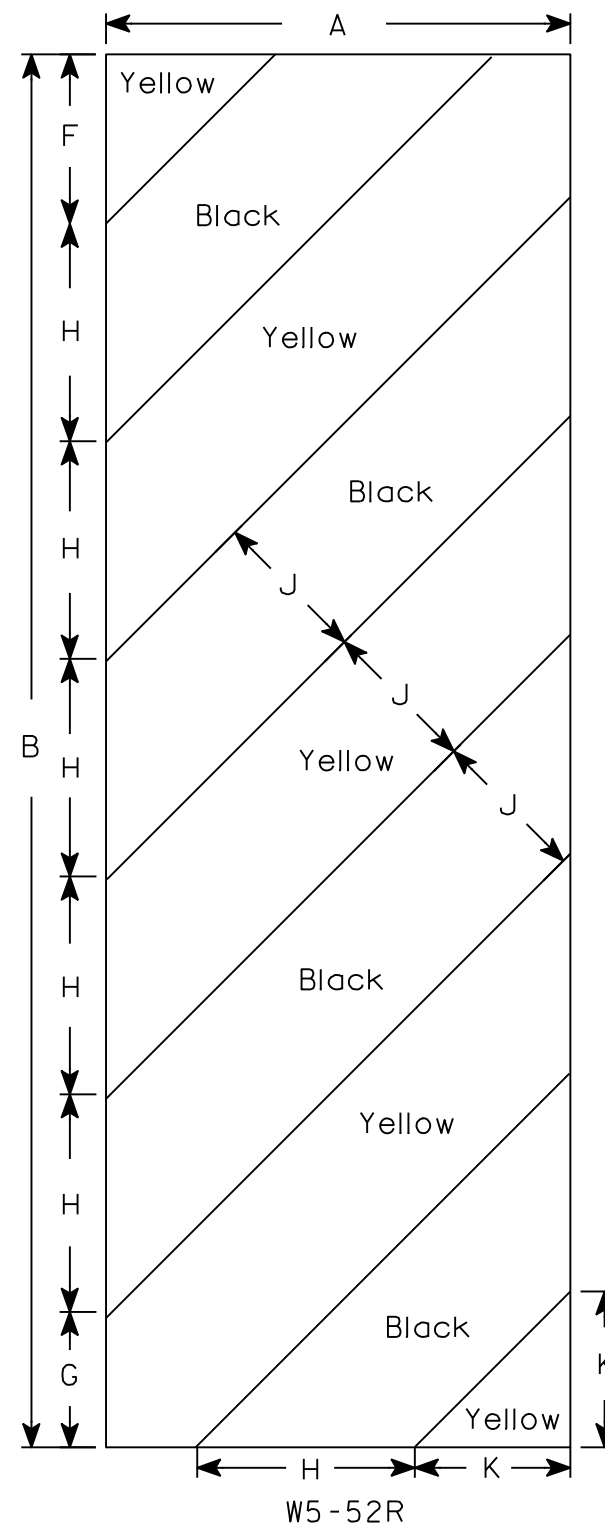
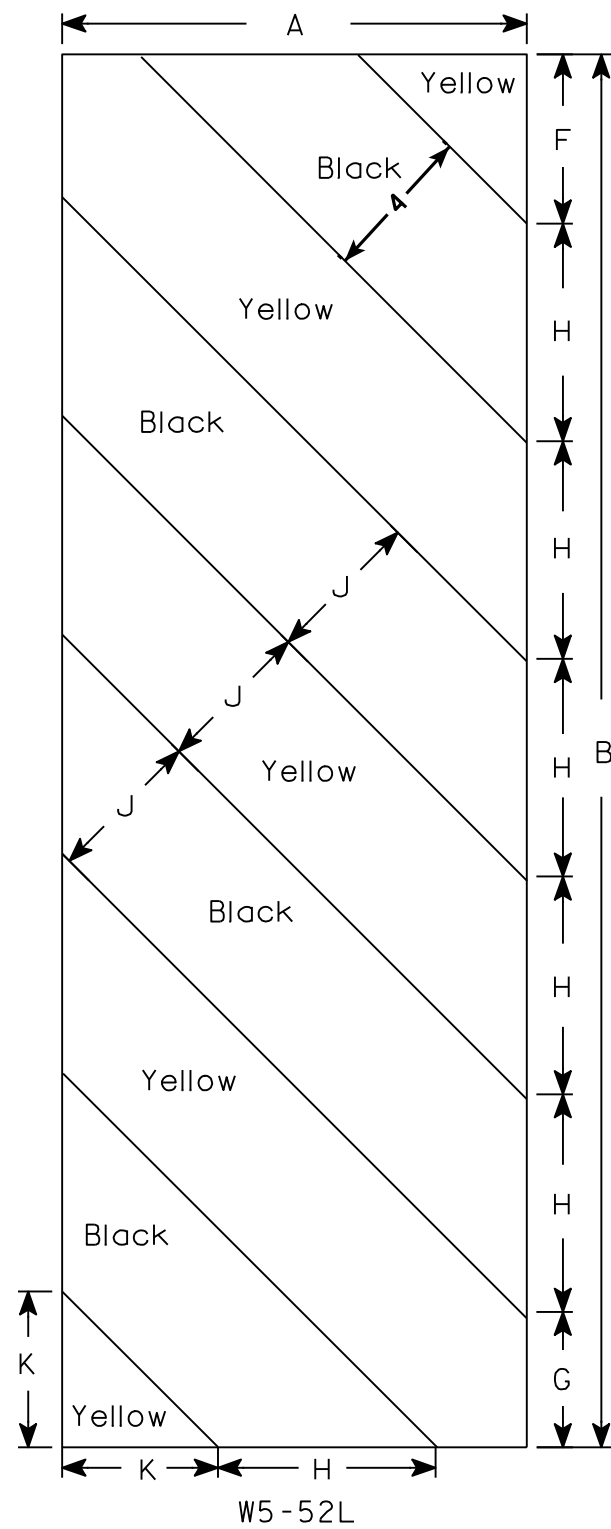
**STANDARD SIGN**  
W4-4P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 03/12/13 PLATE NO. W4-4P.2

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Alternate colors of stripes as shown.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54				6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

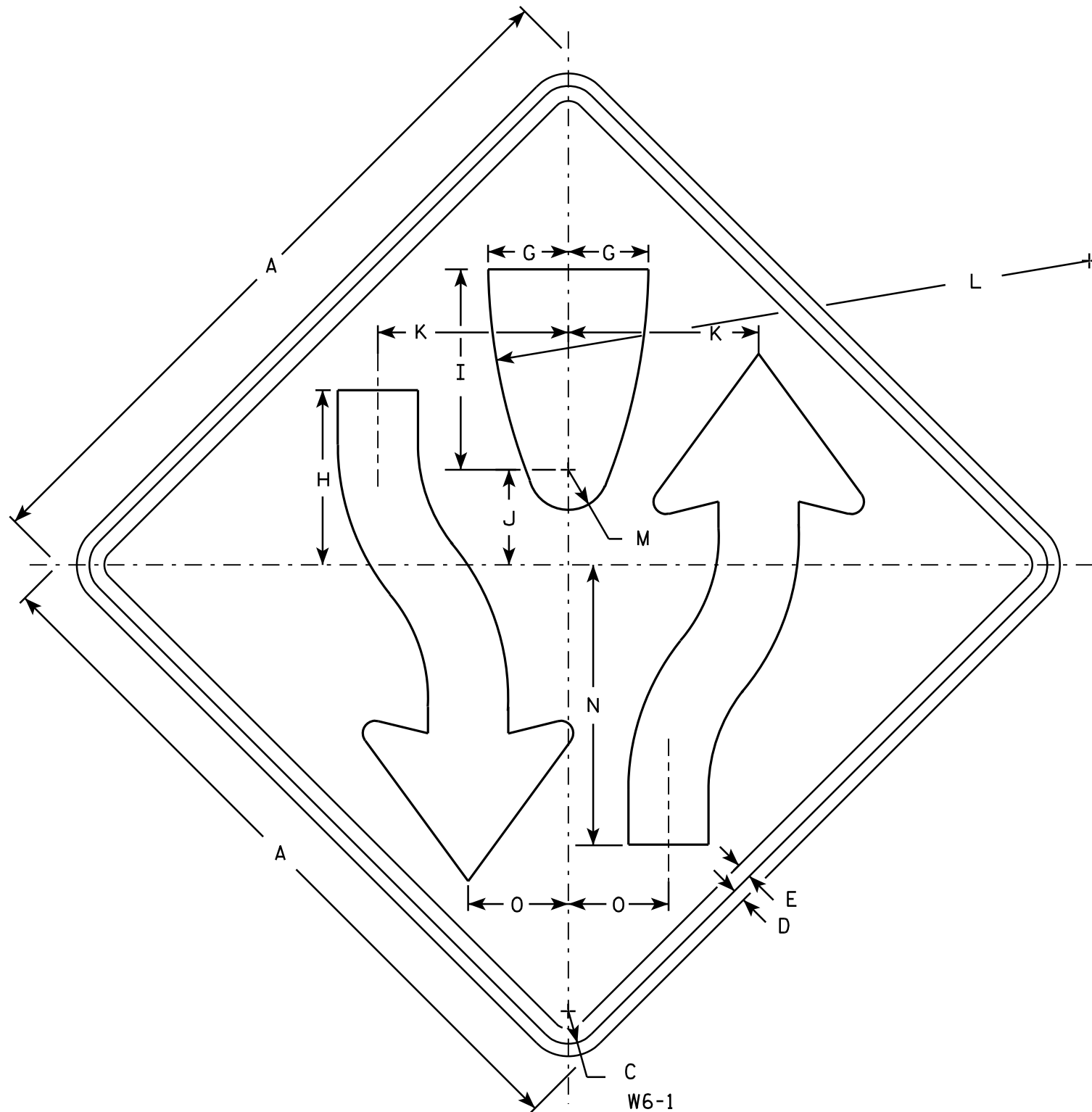
STANDARD SIGN  
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

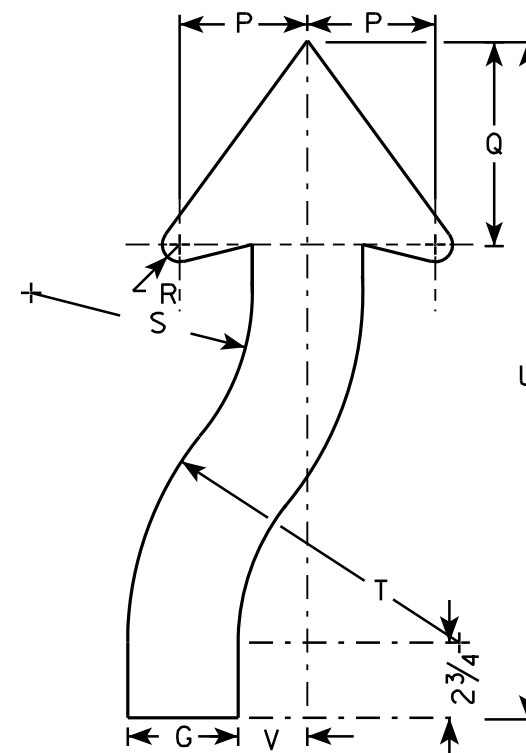
DATE 5/29/12 PLATE NO. W5-52.9

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W6-2 same as W6-1 but is rotated 180° when mounted.



**ARROW DETAIL**

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8		3 1/4	8	8 1/4	4 1/8	7 7/8	25	1 3/4	11 5/8	4 1/8	3 7/8	6 3/4	5/8	6 5/8	9 7/8	21 5/8	2				6.25	
2S	36		1 5/8	5/8	3/4		4	8 3/4	10	4 3/4	9 1/2	30	2	14	5	4 5/8	7 3/8	7/8	8	12	24 1/2	2 1/2				9.0	
2M	36		1 5/8	5/8	3/4		4	8 3/4	10	4 3/4	9 1/2	30	2	14	5	4 5/8	7 3/8	7/8	8	12	24 1/2	2 1/2				9.0	
3																											
4	48		2 1/4	3/4	1		5 3/8	11 5/8	13 3/8	6 3/8	12 5/8	40	2 5/8	18 5/8	6 5/8	6 1/4	9 7/8	1 1/4	10 5/8	16	32 5/8	3 3/8				16.0	
5	48		2 1/4	3/4	1		5 3/8	11 5/8	13 3/8	6 3/8	12 5/8	40	2 5/8	18 5/8	6 5/8	6 1/4	9 7/8	1 1/4	10 5/8	16	32 5/8	3 3/8				16.0	

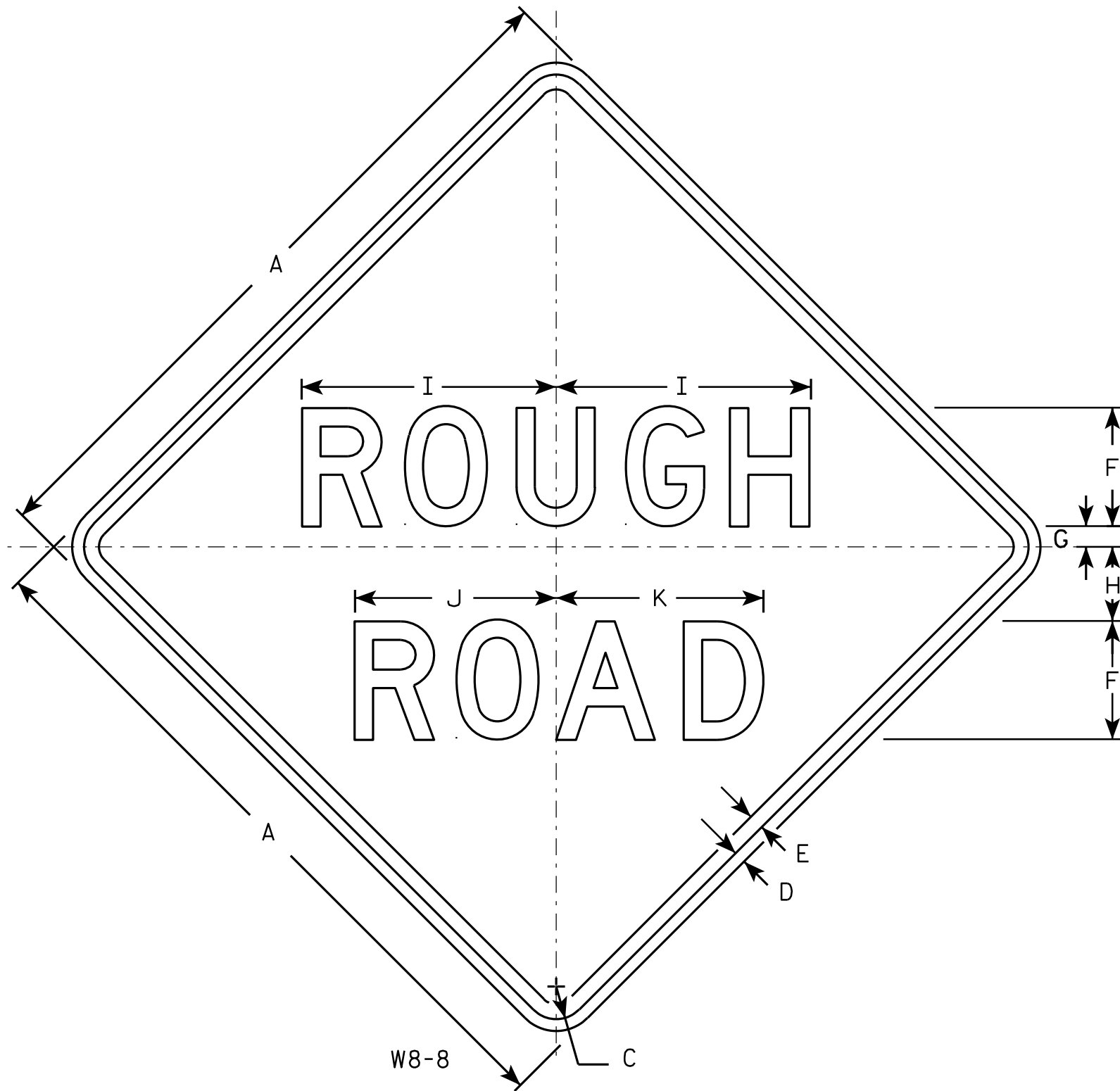
**STANDARD SIGN**  
**W6-1 & W6-2**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 03/12/13 PLATE NO. W6-1.14





NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	4	3/4	3	8 5/8	6 3/4	7																4.0
2S	36		1 5/8	5/8	3/4	6	1	5	12 7/8	10 1/8	10 1/2																9.0
2M	36		1 5/8	5/8	3/4	6	1	5	12 7/8	10 1/8	10 1/2																9.0
3	36		1 5/8	5/8	3/4	6	1	5	12 7/8	10 1/8	10 1/2																9.0
4	36		1 5/8	5/8	3/4	6	1	5	12 7/8	10 1/8	10 1/2																9.0
5	48		2 1/4	3/4	1	8	1 1/2	6	17 1/8	13 1/2	14																16.0

**STANDARD SIGN**  
**W8-8**

*WISCONSIN DEPT OF TRANSPORTATION*

APPROVED *Matthew R. Rauch*  
State Traffic Engineer  
for

DATE 03/14/13 PLATE NO. W8-8.9

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



## *Wisconsin Department of Transportation*

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