

APR 2014
ORDER OF SHEETS

| | |
|--------------------------|------------------------------------|
| Section No. 1 | Title |
| Section No. 2 | Typical Sections and Details |
| Section No. 3 | Estimate of Quantities |
| Section No. 3 | Miscellaneous Quantities |
| Section No. 4 | Right of Way Plat |
| Section No. 5 | Plan and Profile |
| Section No. 6 | Standard Detail Drawings |
| Section No. 7 | Sign Plates |
| Section No. 8 | Structure Plans |
| Section No. 9 | Computer Earthwork Data |
| Section No. 9 | Cross Sections |

TOTAL SHEETS = 34



DESIGN DESIGNATION

A.A.D.T. VARIES = VARIES
A.A.D.T. VARIES = VARIES
D.H.V. = VARIES
D.D. = VARIES
T. = VARIES
DESIGN SPEED = VARIES
ESALS = VARIES

CONVENTIONAL SYMBOLS

| | |
|-----------------------------------|---------|
| PLAN | |
| CORPORATE LIMITS | //// |
| PROPERTY LINE | ---- |
| LOT LINE | ---- |
| LIMITED HIGHWAY EASEMENT | ---- |
| EXISTING RIGHT OF WAY | ---- |
| PROPOSED OR NEW R/W LINE | ---- |
| SLOPE INTERCEPT | - - - - |
| REFERENCE LINE | ----- |
| EXISTING CULVERT | - - - - |
| PROPOSED CULVERT (Box or Pipe) | ----- |
| COMBUSTIBLE FLUIDS | CAUTION |
| MARSH AREA | ▲▲▲ |
| WOODED OR SHRUB AREA | ~~~~~ |

| | |
|--|-------|
| PROFILE | |
| GRADE LINE | ----- |
| ORIGINAL GROUND | ----- |
| MARSH OR ROCK PROFILE (To be noted as such) | ROCK |
| SPECIAL DITCH | LABEL |
| GRADE ELEVATION | 95.36 |
| CULVERT (Profile View) | □ |
| UTILITIES | |
| ELECTRIC | E |
| FIBER OPTIC | FO |
| GAS | G |
| SANITARY SEWER | SAN |
| STORM SEWER | SS |
| TELEPHONE | T |
| WATER | W |
| UTILITY PEDESTAL | ⊕ |
| POWER POLE | ⊕ |
| TELEPHONE POLE | ⊕ |

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

NC REGION, EPOXY PAVEMENT MARKING
LOCATIONS ON STN PER ANNUAL PLAN
VARIOUS HIGHWAYS
NC WIDE

| |
|----------------------|
| STATE PROJECT NUMBER |
| 1009-41-68 |



LAYOUT
SCALE 0 (NOT TO SCALE)

TOTAL NET LENGTH OF CENTERLINE = 0

| STATE PROJECT | FEDERAL PROJECT | |
|---------------|-----------------|----------|
| | PROJECT | CONTRACT |
| 1009-41-68 | WISC 2014060 | 1 |
| | | |
| | | |
| | | |

| | |
|--|--------------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| PREPARED BY | |
| Surveyor | NA |
| Designer | REBECCA OLSEN |
| Project Manager | JEFFREY STEWART |
| Regional Examiner | REGIONAL EXAMINER |
| Regional Supervisor | MICHAEL KRETSCHMER |
| APPROVED FOR THE DEPARTMENT | |
| DATE: 01/14/2014 | (Signature) |

E

DNR LIAISON

JON SIMONSEN (715)365-8916
jonathan.simonsen@wisconsin.gov
COUNTIES: IRON, LINCOLN, ONEIDA, PRICE

MARC HERSHFIELD (715)421-7867
marc.hershfield@wisconsin.gov
COUNTIES: MARATHON, PORTAGE

BOBBI JO FISCHER (920)787-3015
bobbi.fischer@wisconsin.gov
COUNTIES: WAUPACA, WAUSHARA

GENERAL NOTES

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS.

DIGGERS



HOTLINE

Dial  or (800)242-8511

www.DiggersHotline.com

USH 2 WB
(SCENIC OVERLOOK)
PAVEMENT MARKING EPOXY 8-INCH

USH 2 EB AND WB
(JCT STH 169)
PAVEMENT MARKING EPOXY 8-INCH

USH 2 EB AND WB
(JCT STH 122)
PAVEMENT MARKING EPOXY 8-INCH

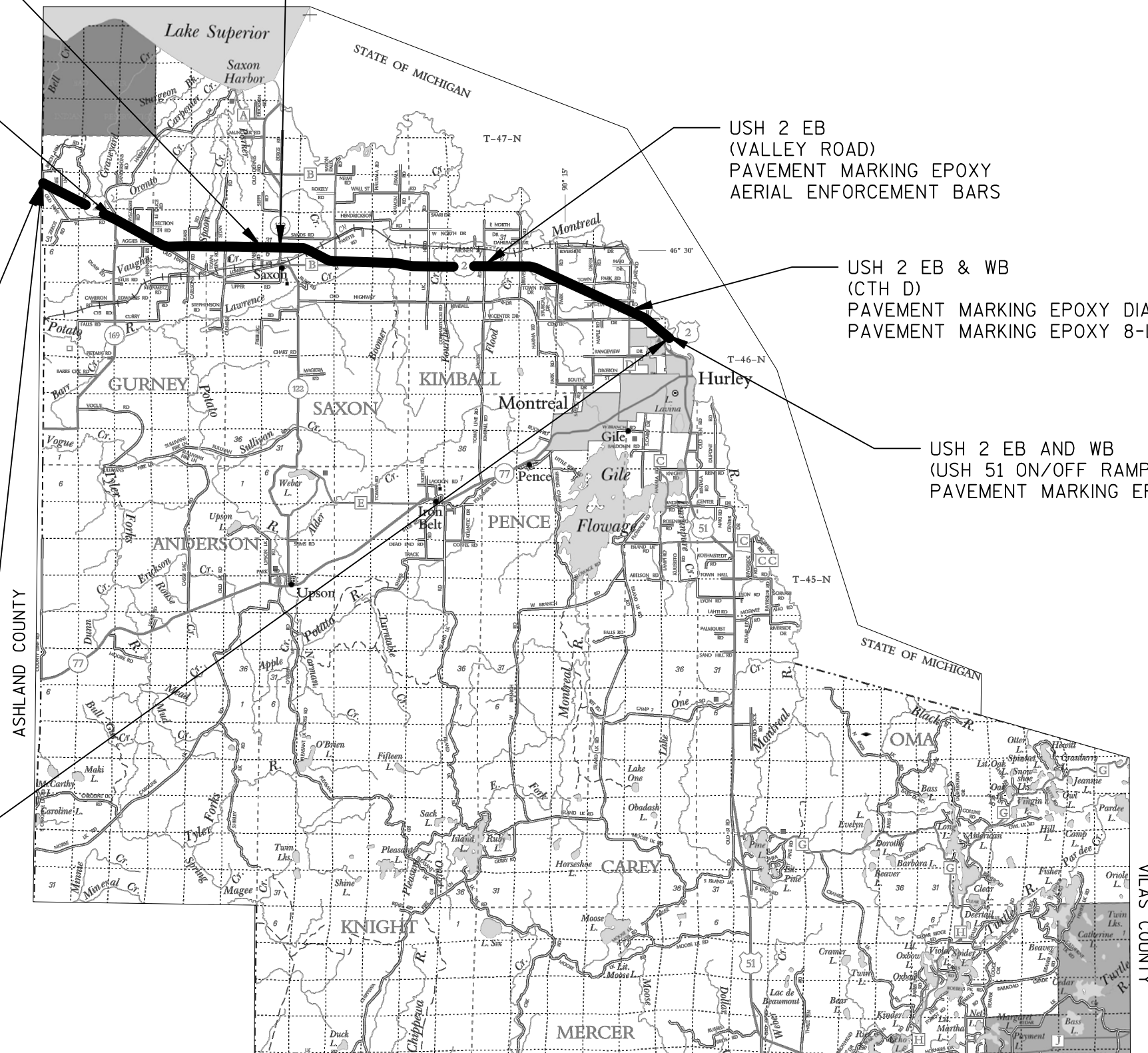
USH 2 EB
(VALLEY ROAD)
PAVEMENT MARKING EPOXY
AERIAL ENFORCEMENT BARS

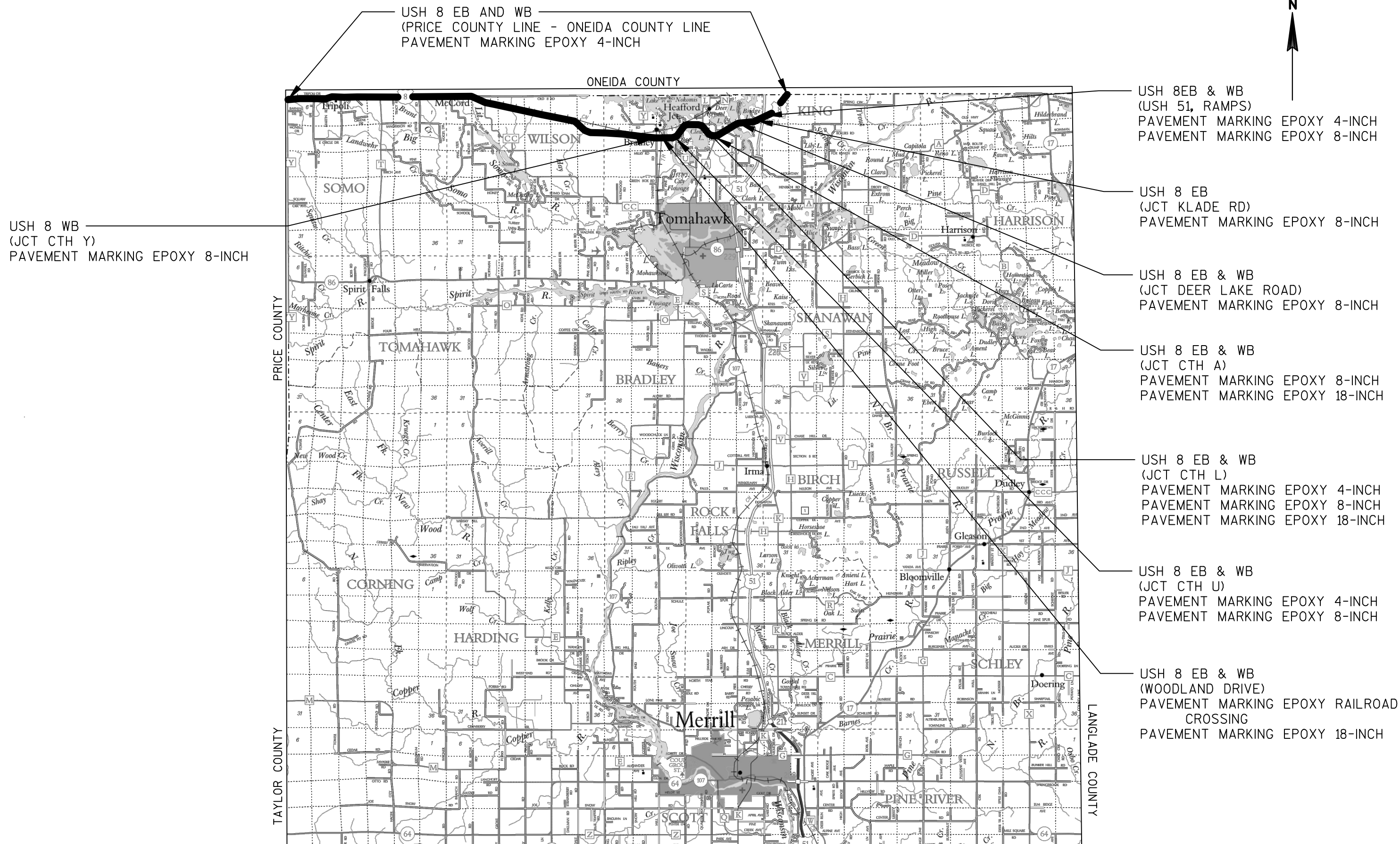
USH 2 EB & WB
(CTH D)
PAVEMENT MARKING EPOXY DIAGONAL 12-INCH
PAVEMENT MARKING EPOXY 8-INCH

USH 2 EB AND WB
(USH 51 ON/OFF RAMPS)
PAVEMENT MARKING EPOXY 8-INCH

USH 2 WB
(JCT BIRCH HILL RD)
PAVEMENT MARKING EPOXY 8-INCH

USH 2 EB AND WB
(ASHLAND COUNTY LINE TO MICHIGAN STATE LINE)
PAVEMENT MARKING EPOXY 4-INCH

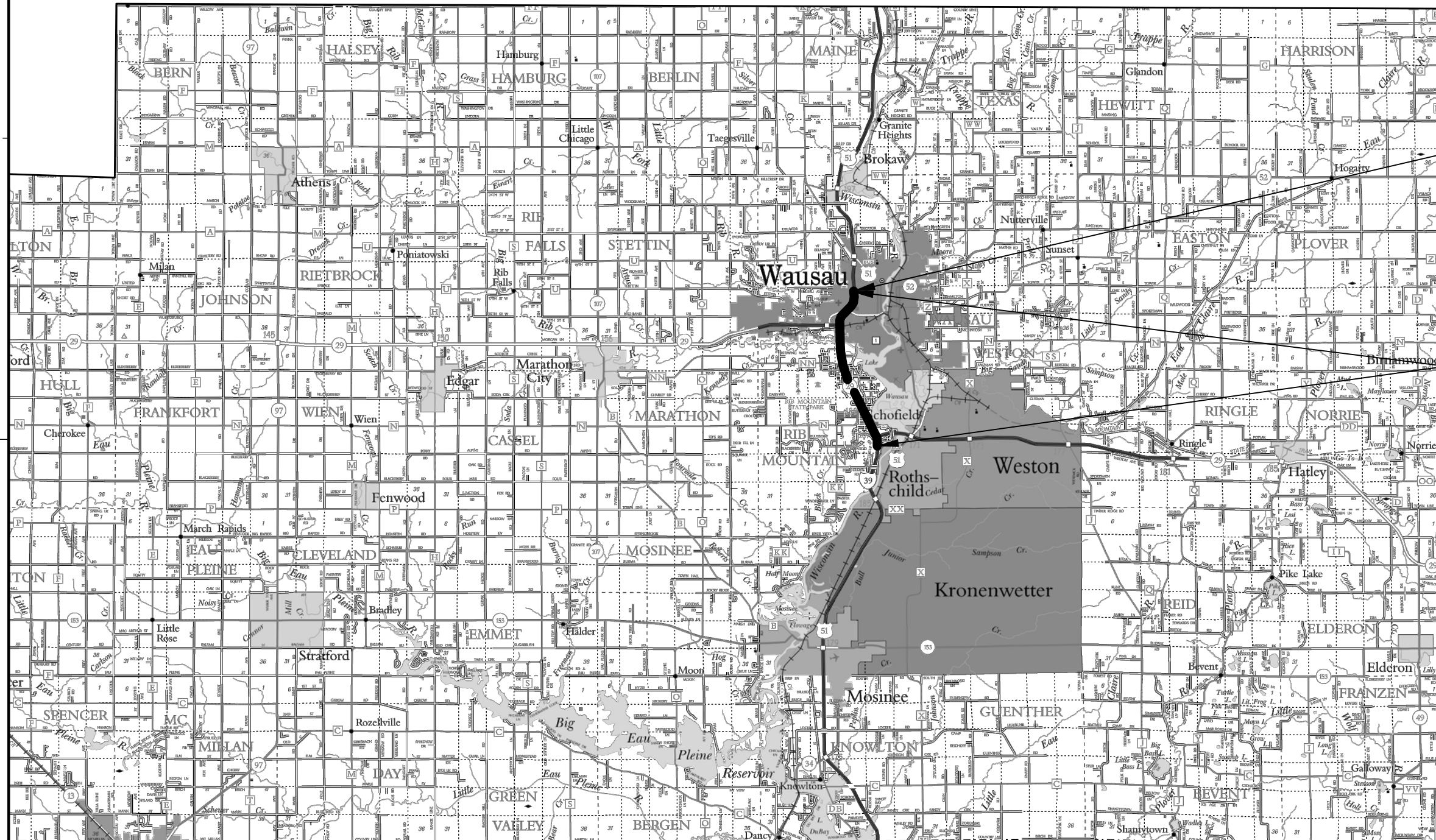




TAYLOR COUNTY

LINCOLN COUNTY

LANGLADE COUNTY



USH 51 NB
(BRIDGE STREET OFF RAMP)
PAVEMENT MARKING EPOXY 8-INCH

USH 51 NB AND SB
(CTH N - BRIDGE STREET)
PAVEMENT MARKING EPOXY 4-INCH

SHAWANO COUNTY

WOOD COUNTY

PORTAGE COUNTY

PROJECT NO: 1009-41-68

HWY: USH 51

COUNTY: MARATHON

PLAN SHEET

SHEET

E

FILE NAME : N:\PDS\C3D\10094167\10094167\SHEETSP\AN021202_MARATHON_PD.DWG

PLOT DATE : 10/3/2013 12:35 PM

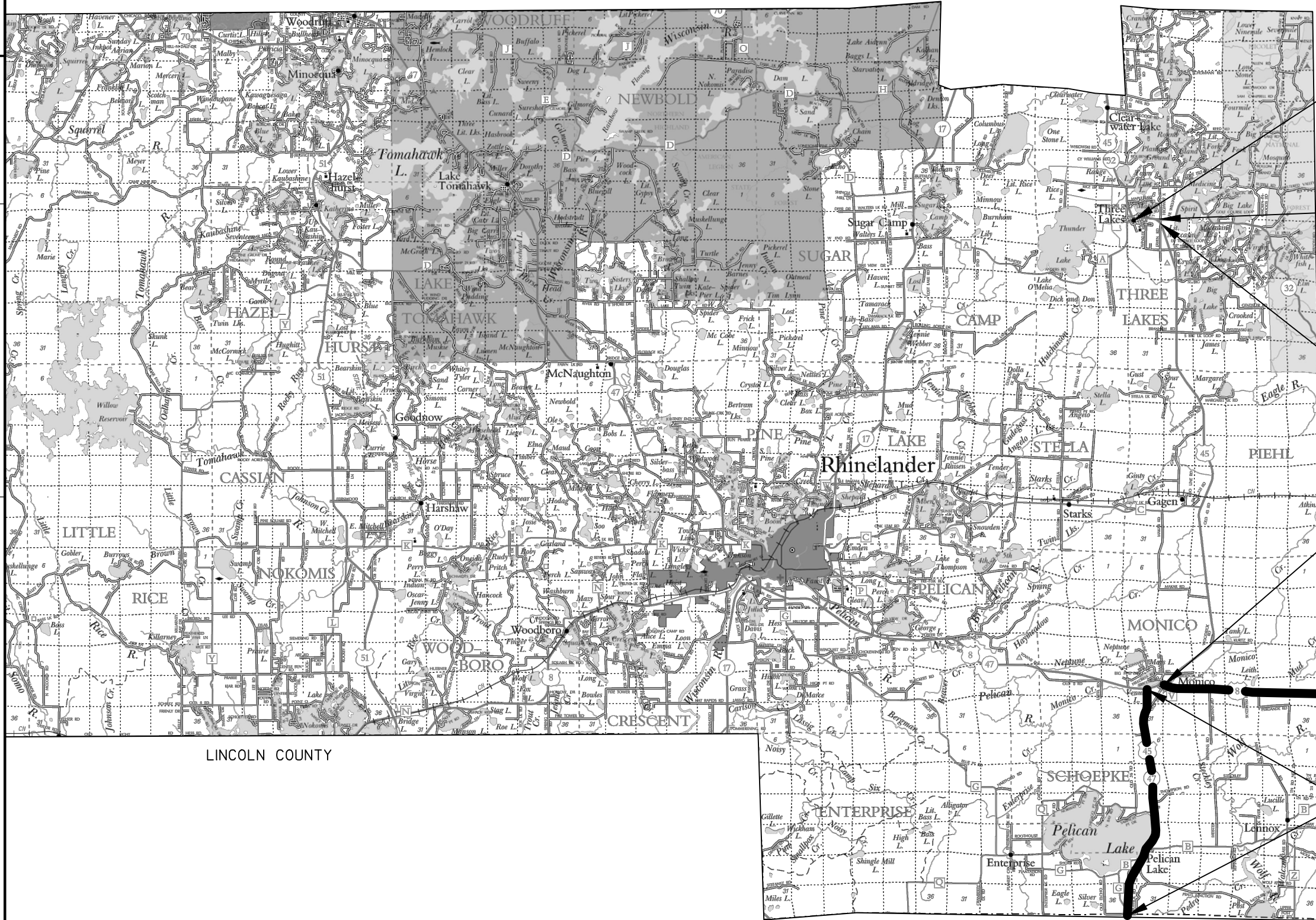
PLOT BY : OLSEN, REBECCA J

PLOT NAME :

PLOT SCALE : 1" = 1'

WISDOT/CADDs SHEET 42

VILAS COUNTY



USH 45
(JCT CTH A)
PAVEMENT MARKING EPOXY 18-INCH
PAVEMENT MARKING EPOXY 24-INCH

USH 45
(JCT STH 32)
PAVEMENT MARKING EPOXY ARROW
PAVEMENT MARKING EPOXY WORDS
PAVEMENT MARKING EPOXY 18-INCH

USH 45
(JCT JAVEN ROAD)
PAVEMENT MARKING EPOXY ARROWS
PAVEMENT MARKING EPOXY WORDS

USH 8
(JCT STH 45/47 TO FOREST COUNTY LINE)
PAVEMENT MARKING EPOXY 4-INCH
PAVEMENT MARKING EPOXY 8-INCH

USH 45
(LANGLADE COUNTY LINE TO USH 8)
PAVEMENT MARKING EPOXY 4-INCH
PAVEMENT MARKING EPOXY 8-INCH

FOREST COUNTY

LINCOLN COUNTY

LANGLADE COUNTY

MARATHON COUNTY



WAUPACA COUNTY

I 39 SB
(JCT CTH W)
PAVEMENT MARKING EPOXY 8-INCH
PAVEMENT MARKING EPOXY 18-INCH
PAVEMENT MARKING EPOXY 24-INCH

I 39 SB
(JCT CTH D)
PAVEMENT MARKING EPOXY 8-INCH
PAVEMENT MARKING EPOXY 18-INCH
PAVEMENT MARKING EPOXY 24-INCH

I 39 SB
(MILE MARKER 151 TO MILE MARKER 140)
PAVEMENT MARKING EPOXY 4-INCH
PAVEMENT MARKING EPOXY 8-INCH

ADAMS COUNTY

WAUSHARA COUNTY

STH 13 SB
(JCT LAKE SHORE DR)
PAVEMENT MARKING EPOXY 8-INCH

STH 13 SB
(JCT OLD HWY 13)
PAVEMENT MARKING EPOXY 8-INCH

STH 13 NB
(JCT 9TH STREET)
PAVEMENT MARKING EPOXY 8-INCH

STH 13 SB
(WAYSIDE)
PAVEMENT MARKING EPOXY 8-INCH

STH 13 SB
(JCT AIRPORT RD)
PAVEMENT MARKING EPOXY 8-INCH

STH 13 NB
(JCT CTH F)
PAVEMENT MARKING EPOXY 8-INCH
PAVEMENT MARKING EPOXY ARROWS
PAVEMENT MARKING EPOXY DIAGONAL 12-INCH
PAVEMENT MARKING EPOXY WORDS
PAVEMENT MARKING EPOXY STOPLINE 18-INCH

STH 13 SB
(JCT STH 111)
PAVEMENT MARKING EPOXY 8-INCH

USH 8 WB
(JCTH STH 111)
PAVEMENT MARKING EPOXY 8-INCH

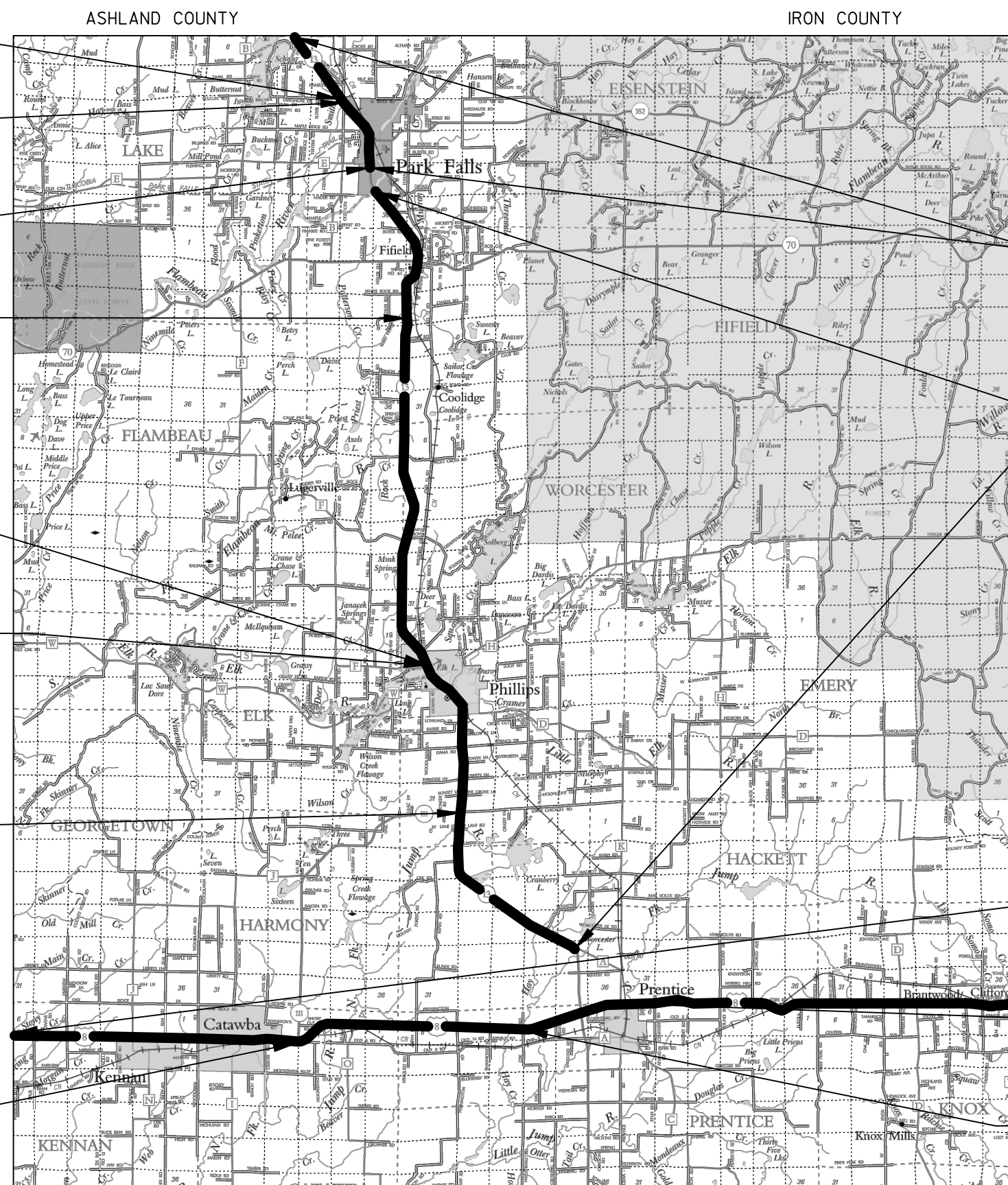
STH 13 NB AND SB
(9TH STREET - ASHLAND COUNTY LINE)
PAVEMENT MARKING EPOXY 4-INCH

STH 13 NB AND SB
(CTH A - FLAMBEAU BRIDGE)
PAVEMENT MARKING EPOXY 4-INCH

USH 8 EB AND WB
(RUSK COUNTY LINE TO LINCOLN COUNTY LINE)
PAVEMENT MARKING EPOXY 4-INCH

USH 8 EB & WB
(AT JCT CTH A)
PAVEMENT MARKING EPOXY 8-INCH
PAVEMENT MARKING EPOXY STOP LINE 18-INCH

USH 8
(AT JCT HAY CREEK RD)
PAVEMENT MARKING AERIAL ENFORCEMENT BARS



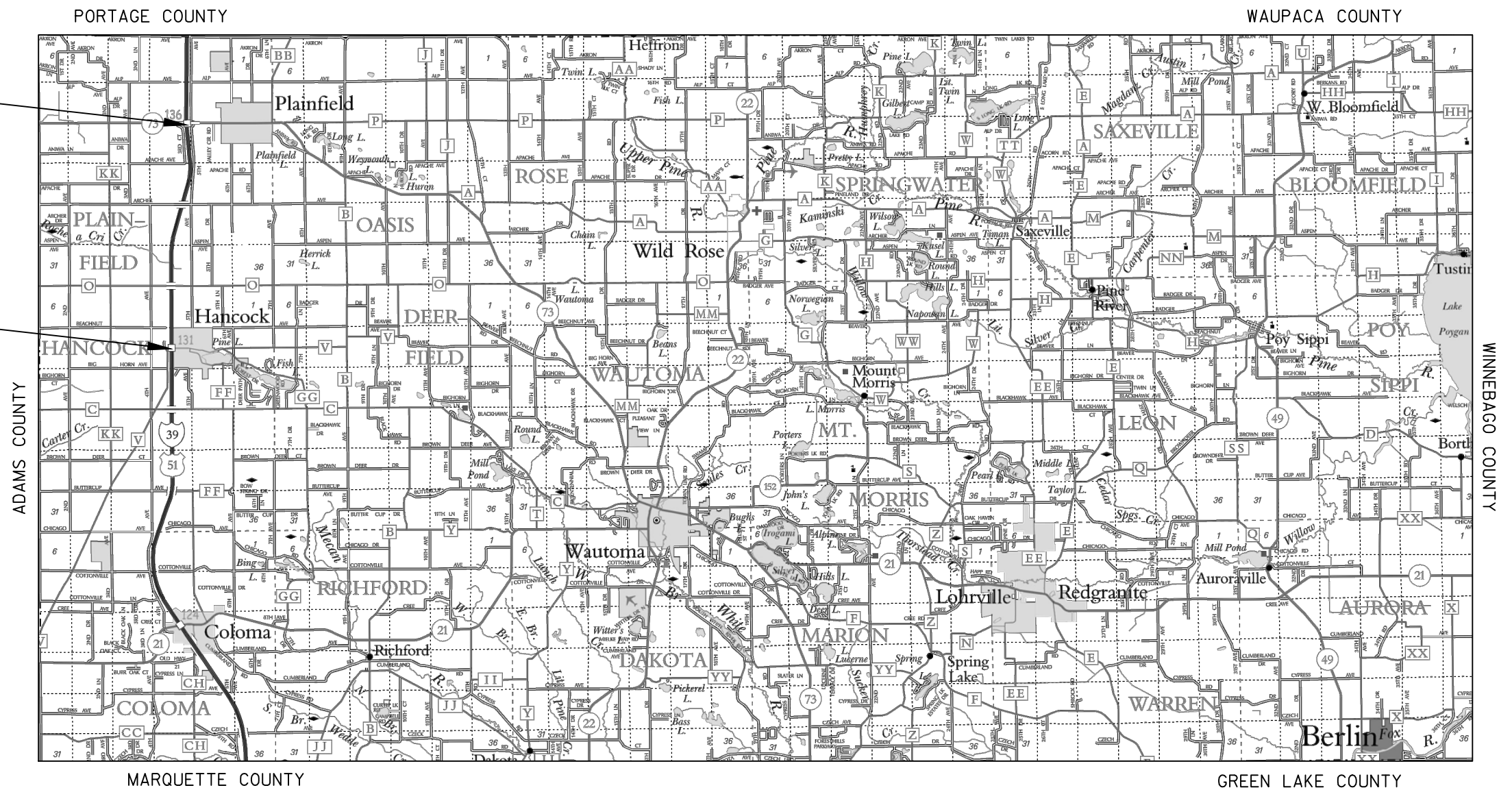


11



I 39 SB
(JCT STH 73)
EPOXY PAVEMENT MARKING 18-INCH
EPOXY PAVEMENT MARKING 24-INCH

I 39 SB
(JCT CTH V)
EPOXY PAVEMENT MARKING 18-INCH
EPOXY PAVEMENT MARKING 24-INCH



| | | | | | |
|--------------|----------|--|---|---------------|---------------|
| DATE 04FEB14 | | | E S T I M A T E O F Q U A N T I T I E S | | |
| LINE | | | | | 1009-41-68 |
| NUMBER | ITEM | ITEM DESCRIPTION | UNIT | TOTAL | QUANTITY |
| 0010 | 619.1000 | MOBILIZATION | EACH | 1.000 | 1.000 |
| 0020 | 643.0100 | TRAFFIC CONTROL (PROJECT) 01. | EACH | 1.000 | 1.000 |
| 0030 | 646.0106 | PAVEMENT MARKING EPOXY 4-INCH | LF | 2,089,615.000 | 2,089,615.000 |
| 0040 | 646.0126 | PAVEMENT MARKING EPOXY 8-INCH | LF | 14,911.000 | 14,911.000 |
| 0050 | 647.0110 | PAVEMENT MARKING RAILROAD CROSSINGS EPOXY | EACH | 4.000 | 4.000 |
| | | | | | |
| 0060 | 647.0166 | PAVEMENT MARKING ARROWS EPOXY TYPE 2 | EACH | 5.000 | 5.000 |
| 0070 | 647.0176 | PAVEMENT MARKING ARROWS EPOXY TYPE 3 | EACH | 1.000 | 1.000 |
| 0080 | 647.0356 | PAVEMENT MARKING WORDS EPOXY | EACH | 6.000 | 6.000 |
| 0090 | 647.0566 | PAVEMENT MARKING STOP LINE EPOXY 18-INCH | LF | 355.000 | 355.000 |
| 0100 | 647.0726 | PAVEMENT MARKING DIAGONAL EPOXY 12-INCH | LF | 599.000 | 599.000 |
| | | | | | |
| 0110 | 647.0746 | PAVEMENT MARKING DIAGONAL EPOXY 24-INCH | LF | 733.000 | 733.000 |
| 0120 | 647.0803 | PAVEMENT MARKING AERIAL ENFORCEMENT BARS EPOXY 24-INCH | LF | 120.000 | 120.000 |
| 0130 | ASP.1T0A | ON-THE-JOB TRAINING APPRENTICE AT \$5.00/HR | HRS | 1,200.000 | 1,200.000 |
| 0140 | ASP.1T0G | ON-THE-JOB TRAINING GRADUATE AT \$5.00/HR | HRS | 600.000 | 600.000 |
| 0150 | SPV.0045 | SPECIAL 01. TRUCK OR TRAILER MOUNTED ATTENUATOR | DAY | 56.000 | 56.000 |

| LOCATION | | PAVEMENT MARKING | | | | | | | | | | | | | | REMARKS |
|----------------|---|-----------------------------|-------------|-----------------------------|-------------|-----------------------------|------------------------------|------------------------------|----------------------------|--------------------------------|-------------------------------|-------------------------------|--|------------------------|---|---------|
| | | 646.0106 EPOXY 4 INCH | | 646.0126 EPOXY 8-INCH | | 647.0110 R R CROSSING | 647.0166 ARROWS TYPE 2 | 647.0176 ARROWS TYPE 3 | 647.0356 WORDS EPOXY | 647.0566 STOP LINE EPOXY | 647.0726 DIAGONAL EPOXY | 647.0746 DIAGONAL EPOXY | 647.0803 AERIAL ENFORCE BARS EPOXY | | | |
| | | YELLOW LF | WHITE LF | YELLOW LF | WHITE LF | EPOXY WHITE | EPOXY WHITE EACH | EPOXY WHITE EACH | EPOXY WHITE EACH | 18-INCH WHITE LF | 12-INCH YELLOW LF | 12-INCH WHITE LF | 24-INCH YELLOW LF | 24-INCH WHITE LF | | |
| IRON COUNTY | USH 2 EB (ASHLAND COUNTY LINE TO MICHIGAN STATE LINE) | 6099 | 1228 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH, LANELINES | |
| IRON COUNTY | USH 2 EB (ASHLAND COUNTY LINE TO MICHIGAN STATE LINE) | 31152 | 95248 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH-SOLID, EDGELINES | |
| IRON COUNTY | USH 2 EB (ASHLAND COUNTY LINE TO MICHIGAN STATE LINE) | 2534 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID-SOLID | |
| IRON COUNTY | USH 2 EB (ASHLAND COUNTY LINE TO MICHIGAN STATE LINE) | 4699 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES LEFT | |
| IRON COUNTY | USH 2 EB (ASHLAND COUNTY LINE TO MICHIGAN STATE LINE) | 31086 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID-DASH | |
| IRON COUNTY | USH 2 EB (ASHLAND COUNTY LINE TO MICHIGAN STATE LINE) | 31047 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID | |
| IRON COUNTY | USH 2 EB (ON/OFF RAMPS TO USH 51) | --- | --- | --- | 550 | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGE LINE AND GORE | |
| IRON COUNTY | USH 2 EB (AT JCT STH 169) | --- | --- | --- | 200 | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE | |
| IRON COUNTY | USH 2 EB (AT JCT STH 122) | --- | --- | --- | 250 | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE | |
| IRON COUNTY | USH 2 EB (APPROXIMATELY 400 FT EAST OF CTH D) | --- | --- | --- | --- | --- | --- | --- | --- | --- | 153 | --- | --- | --- | LANELINES | |
| IRON COUNTY | USH 2 EB (VALLEY RD) | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | 60 | | |
| IRON COUNTY | USH 2 WB (MICHIGAN STATE LINE TO ASHLAND COUNTY LINE) | --- | 1108 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | | |
| IRON COUNTY | USH 2 WB (MICHIGAN STATE LINE TO ASHLAND COUNTY LINE) | --- | 91235 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES | |
| IRON COUNTY | USH 2 WB (ON/OFF RAMPS TO USH 51) | --- | --- | --- | 500 | --- | --- | --- | --- | --- | --- | --- | --- | --- | GORES | |
| IRON COUNTY | USH 2 WB (AT JCT CTH D) | --- | --- | --- | 200 | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE | |
| IRON COUNTY | USH 2 WB (AT JCT STH 122) | --- | --- | --- | 200 | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE | |
| IRON COUNTY | USH 2 WB (AT SCENIC OVERLOOK) | --- | --- | --- | 250 | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE | |
| IRON COUNTY | USH 2 WB (AT JCT STH 169) | --- | --- | --- | 250 | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE | |
| IRON COUNTY | USH 2 WB (AT JCT BIRCH HILL RD) | --- | --- | --- | 200 | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE | |
| LINCOLN COUNTY | USH 8 EB (PRICE COUNTY LINE - ONEIDA COUNTY LINE) | 8015 | 99686 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH, EDGELINES | |
| LINCOLN COUNTY | USH 8 EB (PRICE COUNTY LINE - ONEIDA COUNTY LINE) | 25674 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH-SOLID | |
| LINCOLN COUNTY | USH 8 EB (PRICE COUNTY LINE - ONEIDA COUNTY LINE) | 2059 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES | |
| LINCOLN COUNTY | USH 8 EB (PRICE COUNTY LINE - ONEIDA COUNTY LINE) | 19602 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID-DASH | |
| LINCOLN COUNTY | USH 8 EB (PRICE COUNTY LINE - ONEIDA COUNTY LINE) | 62412 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID | |
| LINCOLN COUNTY | USH 8 EB & WB (AT CTH L) | --- | 79 | --- | 350 | --- | --- | --- | --- | 23 | --- | --- | --- | --- | BYPASS LANE, RIGHT TURN LANE, STOP LINE | |
| LINCOLN COUNTY | USH 8 EB & WB (AT DEER LAKE ROAD) | --- | 66 | --- | 250 | --- | --- | --- | --- | --- | --- | --- | --- | --- | BYPASS LANE, RIGHT TURN LANE | |
| LINCOLN COUNTY | USH 8 EB (AT USH 51) | --- | 436 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | LANELINES | |
| LINCOLN COUNTY | USH 8 EB & WB (AT JCT CTH U) | --- | 66 | --- | 220 | --- | --- | --- | --- | --- | --- | --- | --- | --- | BYPASS LANE, RIGHT TURN LANE | |
| LINCOLN COUNTY | USH 8 EB (AT JCT CTH A) | --- | --- | --- | 270 | --- | --- | --- | --- | 32 | --- | --- | --- | --- | RIGHT TURN LANE, STOP LINE | |
| LINCOLN COUNTY | USH 8 EB (AT HWY 51 S AND HWY 51 N) | --- | --- | --- | 300 | --- | --- | --- | --- | --- | --- | --- | --- | --- | OFF RAMPS | |
| LINCOLN COUNTY | USH 8 EB (AT KLADE ROAD) | --- | --- | 1003 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID | |
| LINCOLN COUNTY | USH 8 EB AND WB (AT WOODLAND DRIVE) | --- | --- | --- | --- | 4 | --- | --- | --- | 24 | --- | --- | --- | --- | LANELINES, OFF RAMPS | |
| LINCOLN COUNTY | USH 8 WB (AT USH 51) | --- | 449 | --- | 700 | --- | --- | --- | --- | --- | --- | --- | --- | --- | | |

continued onto next page...

| | | PAVEMENT MARKING | | | | | | | | | | | | | | REMARKS |
|-----------------|---|-----------------------------|-------------|-----------------------------|-------------|---|--|--|---|--|--|-------------|--|-------------|--|--------------------------------------|
| | | 646.0106 EPOXY 4 INCH | | 646.0126 EPOXY 8-INCH | | 647.0110 R R CROSSING EPOXY WHITE | 647.0166 ARROWS TYPE 2 EPOXY WHITE EACH | 647.0176 ARROWS TYPE 3 EPOXY WHITE EACH | 647.0356 WORDS EPOXY WHITE EACH | 647.0566 STOP LINE EPOXY 18-INCH WHITE LF | 647.0726 DIAGONAL EPOXY 12-INCH | | 647.0746 DIAGONAL EPOXY 24-INCH | | 647.0803 AERIAL ENFORCE BARS EPOXY 24-INCH WHITE LF | |
| LOCATION | | YELLOW LF | WHITE LF | YELLOW LF | WHITE LF | | | | | | YELLOW LF | WHITE LF | YELLOW LF | WHITE LF | | |
| LINCOLN COUNTY | USH 8 WB (ONEIDA COUNTY LINE - PRICE COUNTY LINE) | 92 | 104332 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH, EDGELINES |
| LINCOLN COUNTY | USH 8 WB (ONEIDA COUNTY LINE - PRICE COUNTY LINE) | 1518 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH-SOLID |
| LINCOLN COUNTY | USH 8 WB (ONEIDA COUNTY LINE - PRICE COUNTY LINE) | 2164 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES |
| LINCOLN COUNTY | USH 8 WB (AT CTH Y) | --- | --- | --- | 250 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE |
| MARATHON COUNTY | USH 51 NB (FROM MILE MARKER 186.8 TO MILE MARKER 193.5) | 39380 | 34005 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES (MAINLINE AND RAMPS) |
| MARATHON COUNTY | USH 51 NB (BRIDGE STREET OFF RAMP) | --- | --- | --- | 290 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | GORE |
| MARATHON COUNTY | USH 51 SB (FROM MILE MARKER 193.5 TO MILE MARKER 186.8) | 33023 | 31324 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES (MAINLINE AND RAMPS) |
| ONEIDA COUNTY | USH 8 EB (JCT USH45/STH47 TO FOREST COUNTY LINE) | --- | 31467 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES |
| ONEIDA COUNTY | USH 8 EB (JCT USH45/STH47 TO FOREST COUNTY LINE) | 6811 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH |
| ONEIDA COUNTY | USH 8 EB (JCT USH45/STH47 TO FOREST COUNTY LINE) | 1452 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH-SOLID |
| ONEIDA COUNTY | USH 8 EB (JCT USH45/STH47 TO FOREST COUNTY LINE) | 2772 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID-DASH |
| ONEIDA COUNTY | USH 8 EB (JCT USH45/STH47 TO FOREST COUNTY LINE) | 10665 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID |
| ONEIDA COUNTY | USH 8 EB (AT JCT USH45/STH47) | --- | --- | --- | 712 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | GORES AND EDGELINES |
| ONEIDA COUNTY | USH 8 WB (FOREST COUNTY LINE TO JCT USH45/STH 47) | --- | 27667 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES |
| ONEIDA COUNTY | USH 8 WB (AT JCT USH45/STH47) | --- | --- | --- | 360 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE |
| ONEIDA COUNTY | USH 45 NB (LANGLADE COUNTY LINE TO USH 8) | 3772 | 40747 | --- | 633 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH, EDGELINES, GORES |
| ONEIDA COUNTY | USH 45 NB (LANGLADE COUNTY LINE TO USH 8) | 11890 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH-SOLID |
| ONEIDA COUNTY | USH 45 NB (LANGLADE COUNTY LINE TO USH 8) | 8123 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID-DASH |
| ONEIDA COUNTY | USH 45 NB (LANGLADE COUNTY LINE TO USH 8) | 18798 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID-SOLID |
| ONEIDA COUNTY | USH 45 SB (USH 8 TO LANGLADE COUNTY LINE) | --- | 40445 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES |
| ONEIDA COUNTY | USH 45 SB (AT JCT CTH Q) | --- | --- | --- | 200 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE |
| ONEIDA COUNTY | USH 45 (JCT JAVEN RD) | --- | --- | --- | --- | --- | 1 | --- | 1 | --- | --- | --- | --- | --- | --- | |
| ONEIDA COUNTY | USH 45 (JCT STH 32) | --- | --- | --- | --- | --- | 2 | --- | 2 | 20 | --- | --- | --- | --- | --- | |
| ONEIDA COUNTY | USH 45 (JCT CTH A) | --- | --- | --- | --- | --- | --- | --- | --- | 47 | --- | --- | --- | 41 | --- | |
| PORTAGE COUNTY | I39 SB (FROM MILE MARKER 151 TO MILE MARKER 140) | 65318 | 63285 | --- | 950 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES (MAINLINE AND RAMPS), GORE |
| PORTAGE COUNTY | I39 SB (AT JCT CTH W) | --- | --- | --- | 2323 | --- | --- | --- | --- | 51 | --- | --- | --- | 220 | --- | GORES, STOPLINES |
| PORTAGE COUNTY | I39 SB (AT JCT CTH D) | --- | --- | --- | 950 | --- | --- | --- | --- | 51 | --- | --- | --- | 198 | --- | GORES, STOPLINES |
| PRICE COUNTY | STH 13 NB (FROM NORTH JCT CTH A TO FLAMBEAU BRIDGE) | 10059 | 131376 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH, EDGELINES |
| PRICE COUNTY | STH 13 NB (FROM NORTH JCT CTH A TO FLAMBEAU BRIDGE) | 34369 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH-SOLID |
| PRICE COUNTY | STH 13 NB (FROM NORTH JCT CTH A TO FLAMBEAU BRIDGE) | 34362 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID-DASH |
| PRICE COUNTY | STH 13 NB (FROM NORTH JCT CTH A TO FLAMBEAU BRIDGE) | 89995 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID |
| PRICE COUNTY | STH 13 NB (AT JCT CTH F) | --- | --- | --- | 400 | --- | 5 | 1 | 6 | 22 | 387 | 59 | --- | --- | --- | LT AND RT TURNLANES |
| PRICE COUNTY | STH 13 NB (AT JCT 9TH STREET) | --- | --- | --- | 100 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE |
| PRICE COUNTY | STH 13 NB (FROM 9TH STREET TO ASHLAND COUNTY LINE) | --- | 15381 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES |
| PRICE COUNTY | STH 13 NB (FROM 9TH STREET TO ASHLAND COUNTY LINE) | 2893 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | MEDIAN ISLAND - DASH |
| PRICE COUNTY | STH 13 NB (FROM 9TH STREET TO ASHLAND COUNTY LINE) | 3446 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | MEDIAN ISLAND - DASH-SOLID |
| PRICE COUNTY | STH 13 NB (FROM 9TH STREET TO ASHLAND COUNTY LINE) | 3886 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | MEDIAN ISLAND - SOLID |
| PRICE COUNTY | STH 13 SB (FROM ASHLAND COUNTY LINE TO 9TH STREET) | --- | 18395 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES |
| PRICE COUNTY | STH 13 SB (AT JCT LAKE SHORE DRIVE) | --- | --- | --- | 200 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE |

continued onto next page...

| PAVEMENT MARKING | | | | | | | | | | | | | | | | | |
|------------------|---|-----------------------------|-------------|-----------------------------|-------------|-----------------------------|------------------------------|------------------------------|------------------------|------------------------|-------------------------------|-------------|-------------------------------|-------------|--|-----|--|
| LOCATION | | 646.0106 EPOXY 4 INCH | | 646.0126 EPOXY 8-INCH | | 647.0110 R R CROSSING | 647.0166 ARROWS TYPE 2 | 647.0176 ARROWS TYPE 3 | 647.0356 WORDS | 647.0566 STOP LINE | 647.0726 DIAGONAL EPOXY | | 647.0746 DIAGONAL EPOXY | | 647.0803 AERIAL ENFORCE BARS EPOXY | | REMARKS |
| | | YELLOW LF | WHITE LF | YELLOW LF | WHITE LF | EPOXY WHITE | EPOXY WHITE EACH | EPOXY WHITE EACH | EPOXY WHITE EACH | 18-INCH WHITE LF | YELLOW LF | WHITE LF | YELLOW LF | WHITE LF | 24-INCH WHITE LF | | |
| PRICE COUNTY | STH 13 SB (AT JCT OLD 13 RD) | --- | --- | --- | 200 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE |
| PRICE COUNTY | STH 13 SB (FROM FLAMBEAU BRIDGE TO NORTH JCT CTH A) | --- | 126791 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES |
| PRICE COUNTY | STH 13 SB (WAYSIDE AT SOUTH FORK FLAMBEAU RIVER) | --- | --- | --- | 250 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE |
| PRICE COUNTY | STH 13 SB (AT JCT AIRPORT RD) | --- | --- | --- | 250 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | LEFT TURN LANE |
| PRICE COUNTY | STH 13 SB (AT JCT STH 111) | --- | --- | --- | 250 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE |
| PRICE COUNTY | USH 8 EB (AT STH 13) | --- | 528 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | LANELINES |
| PRICE COUNTY | USH 8 EB (RUSK COUNTY LINE TO LINCOLN COUNTY LINE) | 17674 | 162726 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE DASH, EDGELINES CENTERLINE DASH-SOLID EDGELINES |
| PRICE COUNTY | USH 8 EB (RUSK COUNTY LINE TO LINCOLN COUNTY LINE) | 56786 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| PRICE COUNTY | USH 8 EB (RUSK COUNTY LINE TO LINCOLN COUNTY LINE) | 3062 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| PRICE COUNTY | USH 8 EB (RUSK COUNTY LINE TO LINCOLN COUNTY LINE) | 45738 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID-DASH CENTERLINE SOLID RIGHT TURN LANE |
| PRICE COUNTY | USH 8 EB (RUSK COUNTY LINE TO LINCOLN COUNTY LINE) | 51535 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| PRICE COUNTY | USH 8 EB (AT JCT CTH A) | --- | --- | --- | 400 | --- | --- | --- | --- | 66 | --- | --- | --- | --- | --- | --- | |
| PRICE COUNTY | USH 8 EB (AT HAY CREEK ROAD) | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | 60 | EDGELINES LANELINES |
| PRICE COUNTY | USH 8 WB (LINCOLN COUNTY LINE TO RUSK COUNTY LINE) | --- | 153857 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| PRICE COUNTY | USH 8 WB (AT JCT STH 13) | --- | 528 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| PRICE COUNTY | USH 8 WB (LINCOLN COUNTY LINE TO RUSK COUNTY LINE) | 3221 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGELINES CENTERLINE SOLID RIGHT TURN LANE |
| PRICE COUNTY | USH 8 WB (LINCOLN COUNTY LINE TO RUSK COUNTY LINE) | 634 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| PRICE COUNTY | USH 8 WB (AT JCT CTH A) | --- | --- | --- | 250 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| PRICE COUNTY | USH 8 WB (AT JCT STH 111) | --- | --- | --- | 250 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | RIGHT TURN LANE RIGHT TURN LANE EDGE LINE |
| WAUPACA COUNTY | USH 45 NB (KANAMAN ROAD TO REINKE ROAD) | --- | 119 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| WAUPACA COUNTY | USH 45 NB (KANAMAN ROAD TO REINKE ROAD) | --- | 9873 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| WAUPACA COUNTY | USH 45 NB (KANAMAN ROAD TO REINKE ROAD) | --- | 26 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGE LINE DASH CENTERLINE DASH CENTERLINE DASH-SOLID |
| WAUPACA COUNTY | USH 45 NB (KANAMAN ROAD TO REINKE ROAD) | 726 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| WAUPACA COUNTY | USH 45 NB (KANAMAN ROAD TO REINKE ROAD) | 2442 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| WAUPACA COUNTY | USH 45 NB (KANAMAN ROAD TO REINKE ROAD) | 2970 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | CENTERLINE SOLID-DASH CENTERLINE SOLID BYPASS LANE |
| WAUPACA COUNTY | USH 45 NB (KANAMAN ROAD TO REINKE ROAD) | 4224 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| WAUPACA COUNTY | USH 45 SB (REINKE ROAD TO KANAMAN ROAD) | --- | 40 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | |
| WAUPACA COUNTY | USH 45 SB (REINKE ROAD TO KANAMAN ROAD) | --- | 8923 | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | --- | EDGE LINE |
| WAUSHARA COUNTY | I39 SB (AT JCT STH 73) | --- | --- | --- | --- | --- | --- | --- | --- | 41 | --- | --- | --- | 223 | --- | --- | |
| WAUSHARA COUNTY | I39 SB (AT JCT CTH V) | --- | --- | --- | --- | --- | --- | --- | --- | 45 | --- | --- | --- | 92 | --- | --- | |
| SUBTOTALS: | | 798179 | 1291436 | 1003 | 13908 | | | | | | 540 | 59 | 0 | 733 | | | |
| PROJECT TOTALS: | | 2,089,615 | | 14,911 | | 4 | 5 | 1 | 6 | 355 | 599 | | 733 | | 120 | | |

TRUCK OR TRAILER MOUNTED ATTENUATOR

| | SPV.0045.01 DAY | REMARKS |
|--------------------|--------------------|---------|
| PROJECT 1009-41-68 | 56 | |
| TOTAL | 56 | |

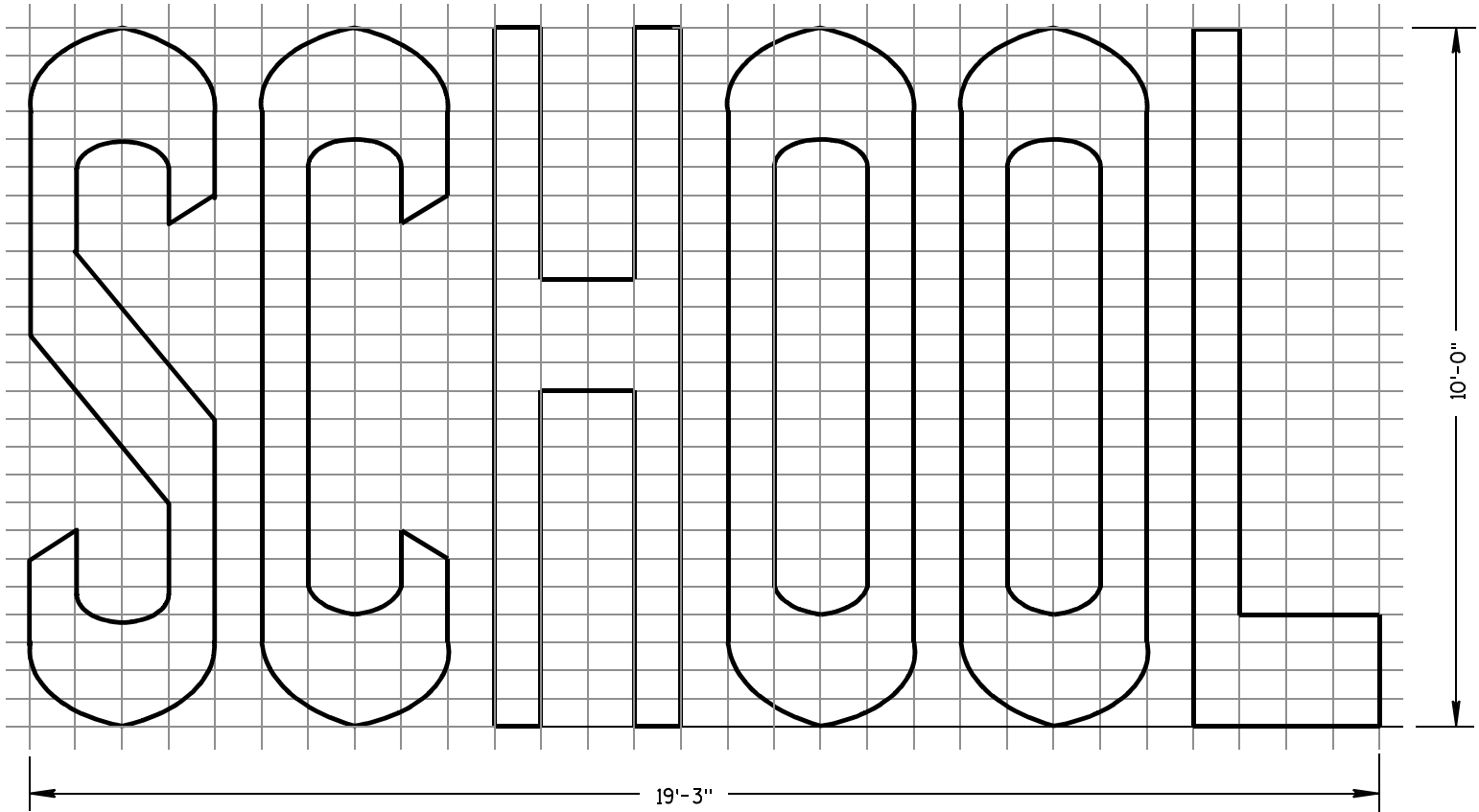
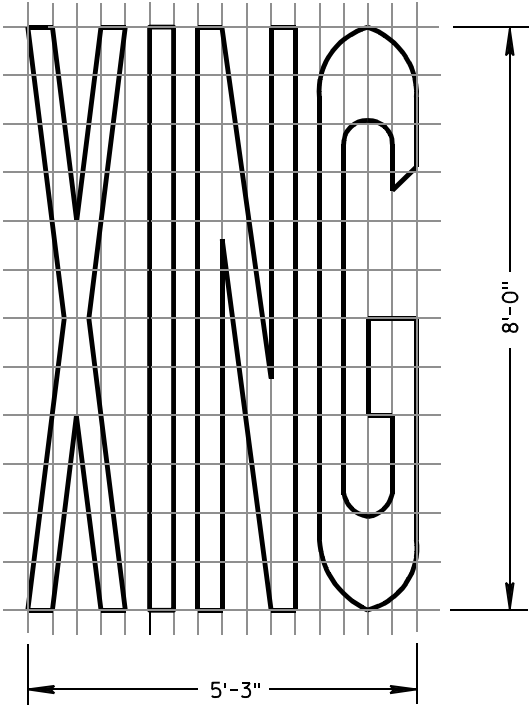
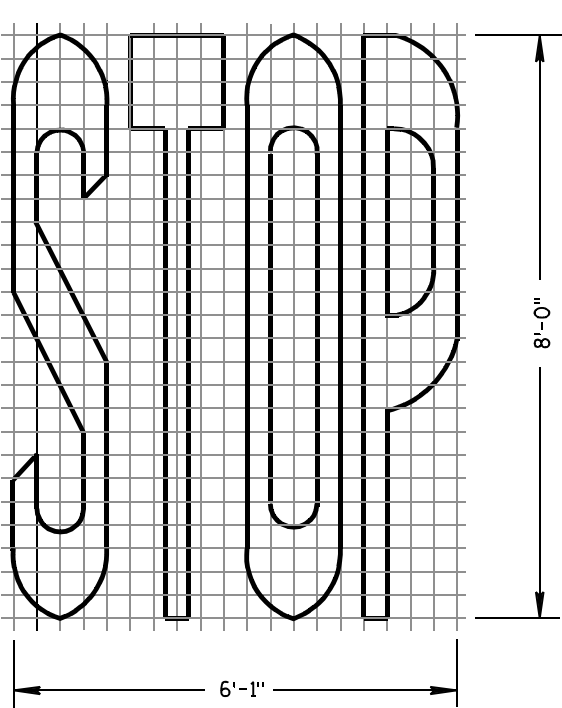
Standard Detail Drawing List

| | |
|-----------|---|
| 15C07-12B | PAVEMENT MARKING WORDS |
| 15C07-12C | PAVEMENT MARKING ARROWS |
| 15C08-16A | PAVEMENT MARKING (MAINLINE) |
| 15C08-16B | PAVEMENT MARKING (INTERSECTIONS) |
| 15C08-16C | PAVEMENT MARKING (CLIMBING LANE & PASSING LANE) |
| 15C08-16D | PAVEMENT MARKING (CLIMBING LANE & PASSING LANE) |
| 15C08-16E | PAVEMENT MARKING (LEFT TURN LANE) |
| 15C09-09A | SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD-HIGHWAY GRADE CROSSINGS |
| 15C12-04 | TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS) |
| 15C14-01 | AERIAL ENFORCEMENT BARS PAVEMENT MARKING DETAILS |
| 15C18-03 | MEDIAN ISLAND MARKING |
| 15C19-02A | MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY |
| 15C19-02B | MOVING PAVEMENT MARKING OPERATION MULTI-LANE UNDIVIDED ROADWAY |
| 15C19-02C | MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY |
| 15C21-05 | SIGNING AND MARKING FOR TWO LANE TO FOUR LANE DIVIDED TRANSITIONS |
| 15C31-01A | PAVEMENT MARKING (RAMPS AND GORES) |
| 15C31-01D | PAVEMENT MARKING FOR PARALLEL ON-RAMP AND PARALLEL OFF-RAMP |
| 15C33-01 | STOP LINE AND CROSSWALK PAVEMENT MARKING |

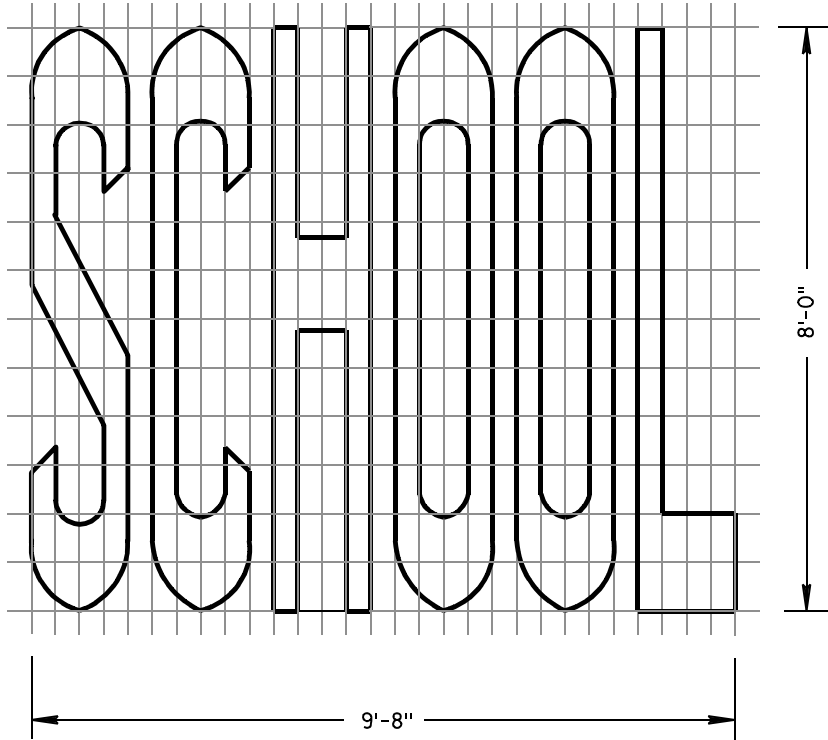
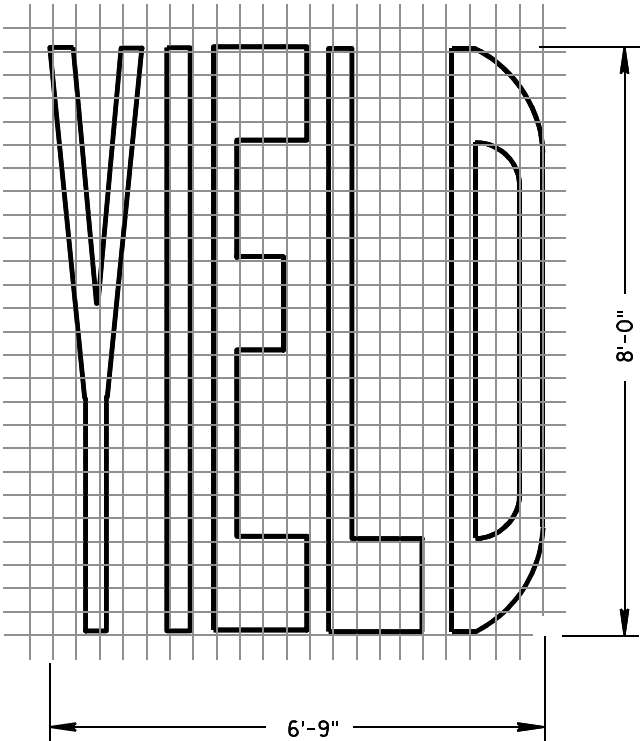
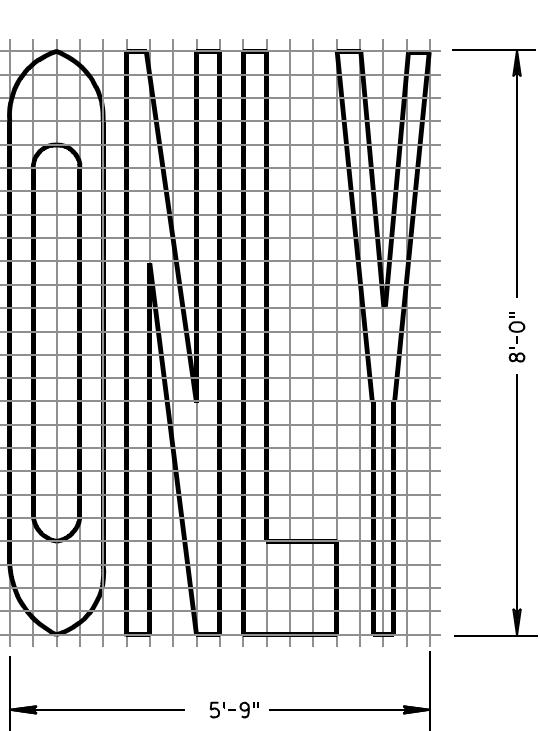
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

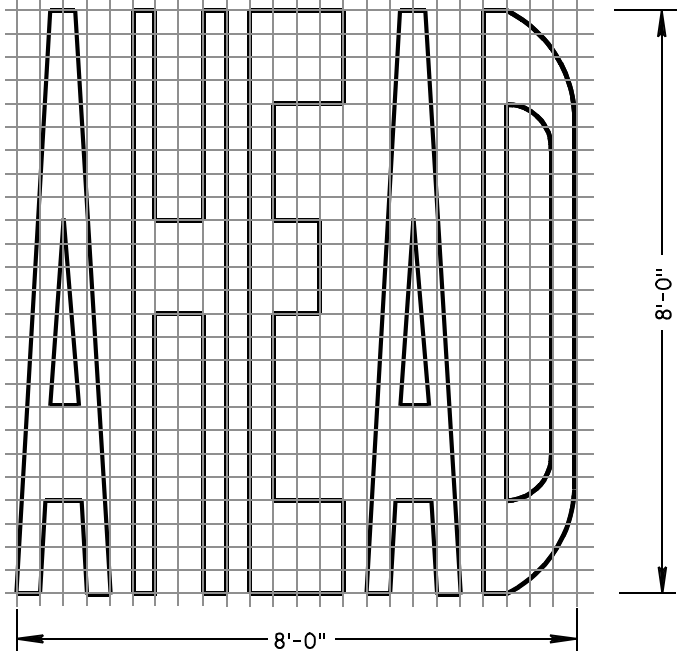
ALL LETTERS, ARROWS AND SYMBOLS SHALL BE IN CONFORMANCE WITH REQUIREMENTS INCLUDED IN "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKING" BOOK BY THE FEDERAL HIGHWAY ADMINISTRATION. ALL LETTERS, ARROWS AND SYMBOLS SHALL BE WHITE AND REFLECTORIZED. SMALL DIFFERENCES IN DIMENSIONS WITHIN THE TOLERANCES OF THAT BOOK ARE ACCEPTABLE.



TWO-LANE



SINGLE-LANE



PAVEMENT MARKING WORDS

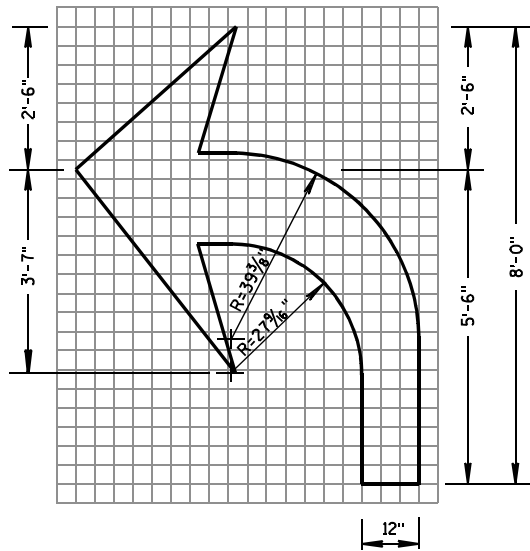
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

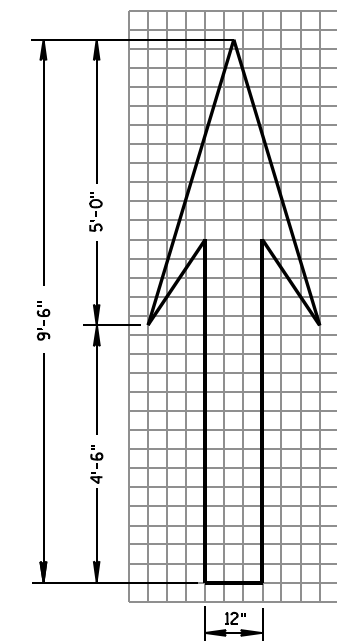
7-1-11
DATE

/S/ Thomas N. Notbohm
STATE TRAFFIC ENGINEER OF DESIGN

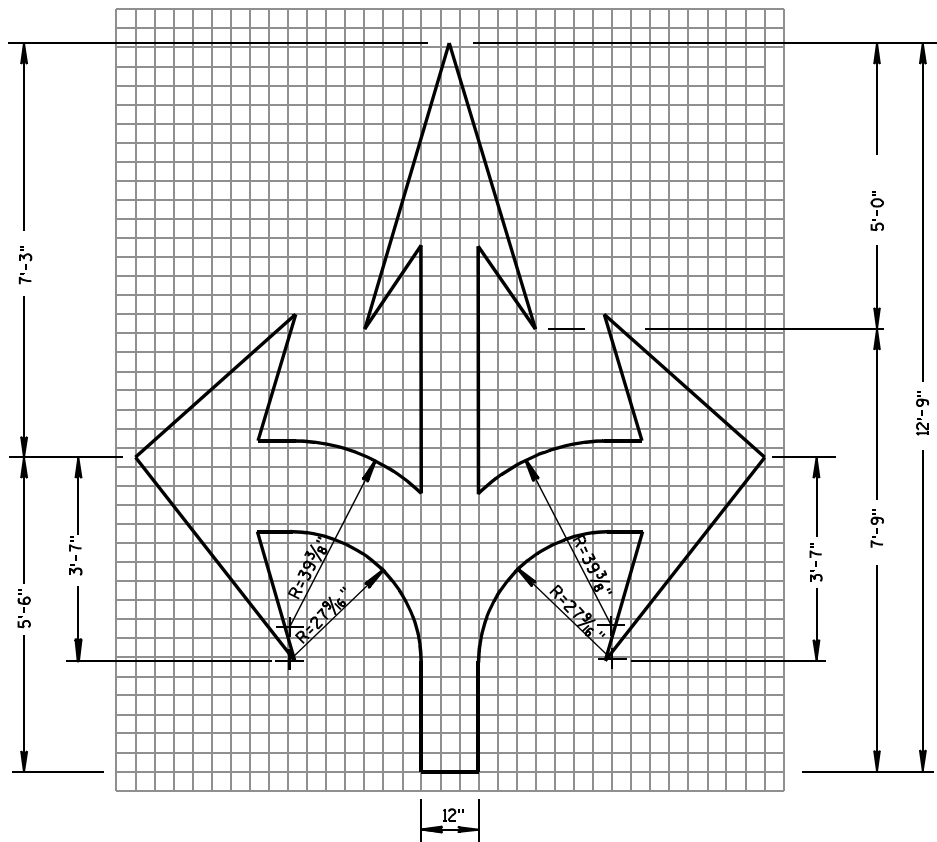
FHWA



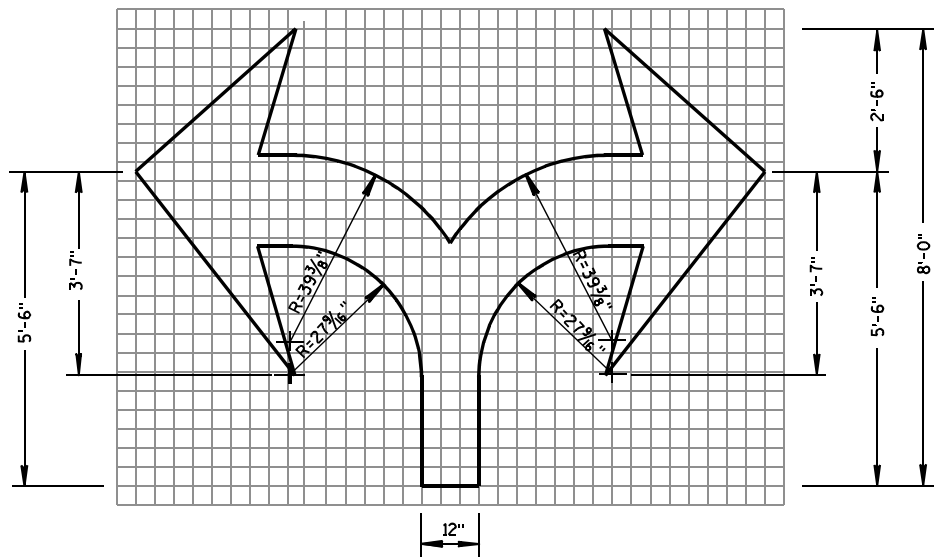
TYPE 2



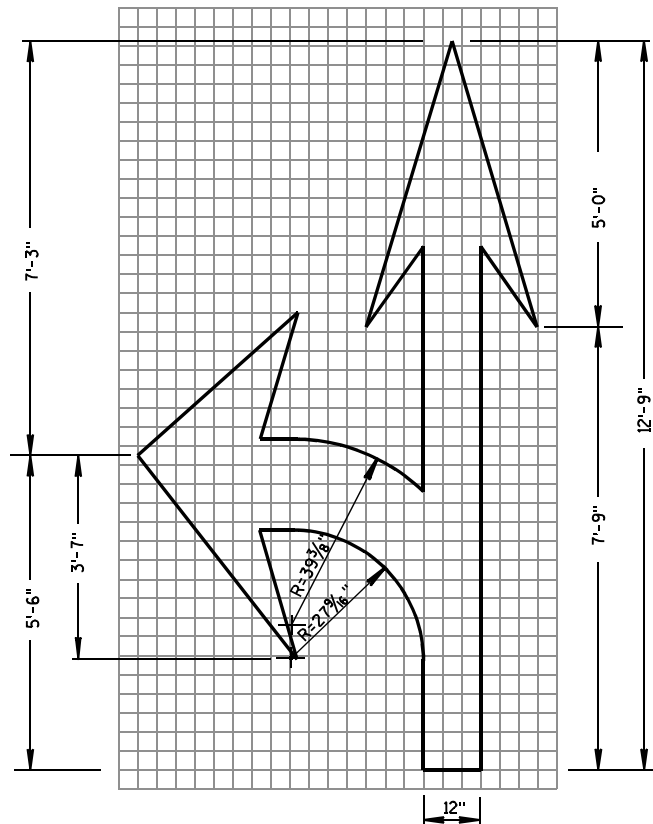
TYPE 1



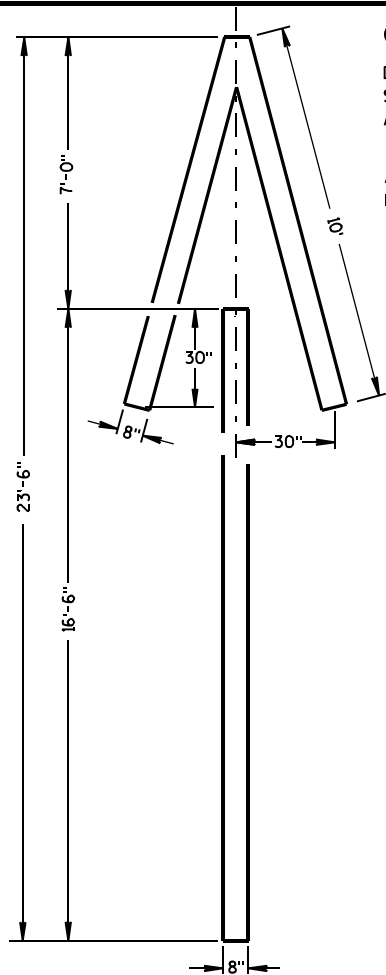
TYPE 6



TYPE 7



TYPE 3

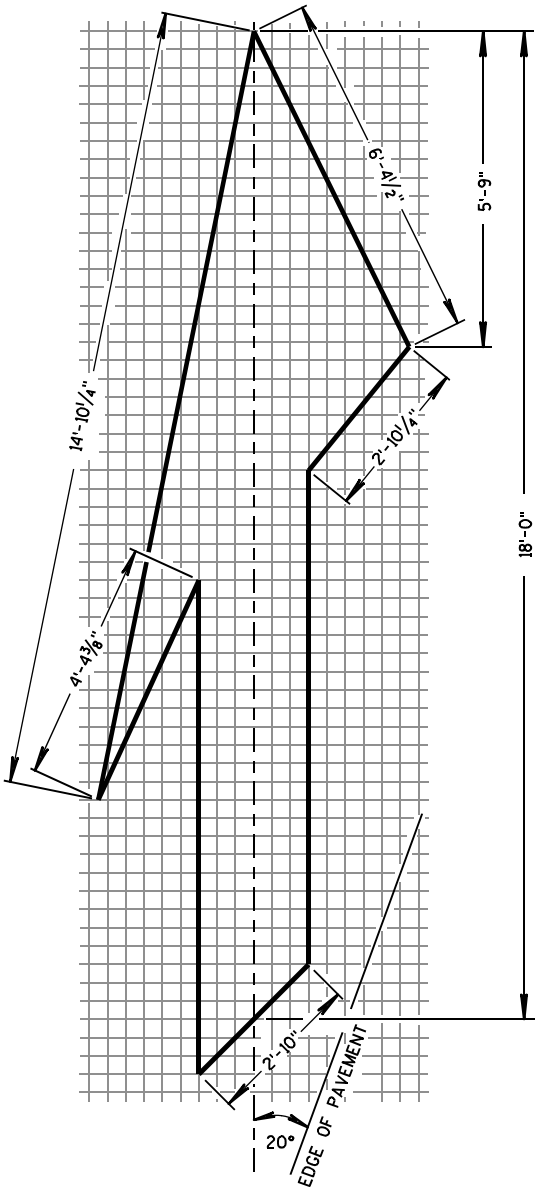


TYPE 4

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

ALL LETTERS, ARROWS AND SYMBOLS SHALL BE IN CONFORMANCE WITH REQUIREMENTS INCLUDED IN "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKING" BOOK BY THE FEDERAL HIGHWAY ADMINISTRATION. ALL LETTERS, ARROWS AND SYMBOLS SHALL BE WHITE AND REFLECTORIZED. SMALL DIFFERENCES IN DIMENSIONS WITHIN THE TOLERANCES OF THAT BOOK ARE ACCEPTABLE.



TYPE 5 LANE DROP ARROW

PAVEMENT MARKING ARROWS

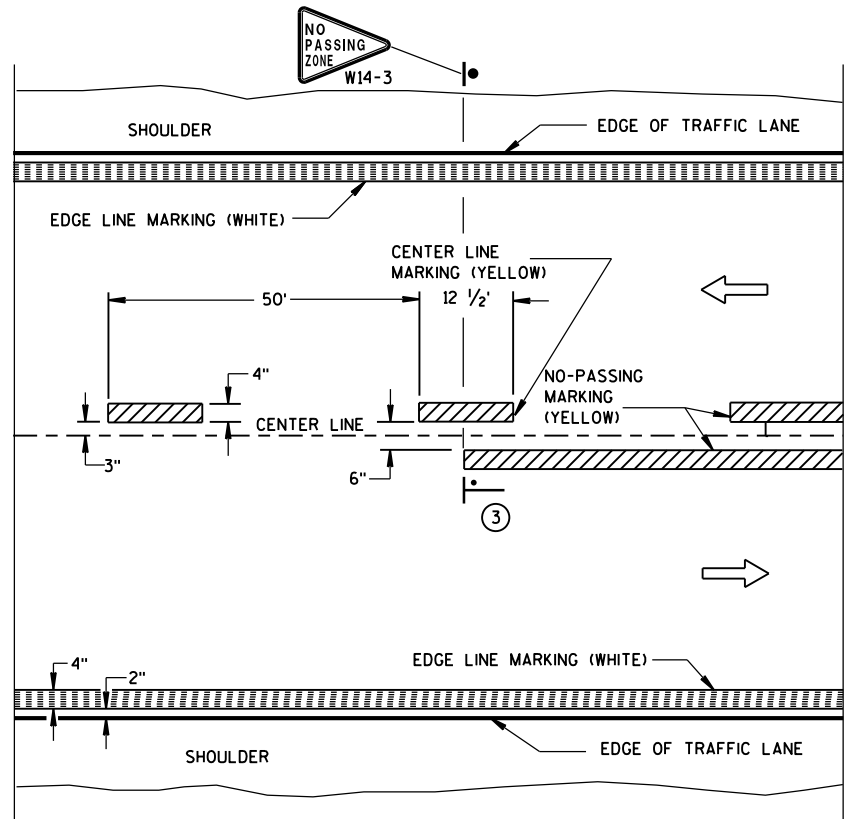
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

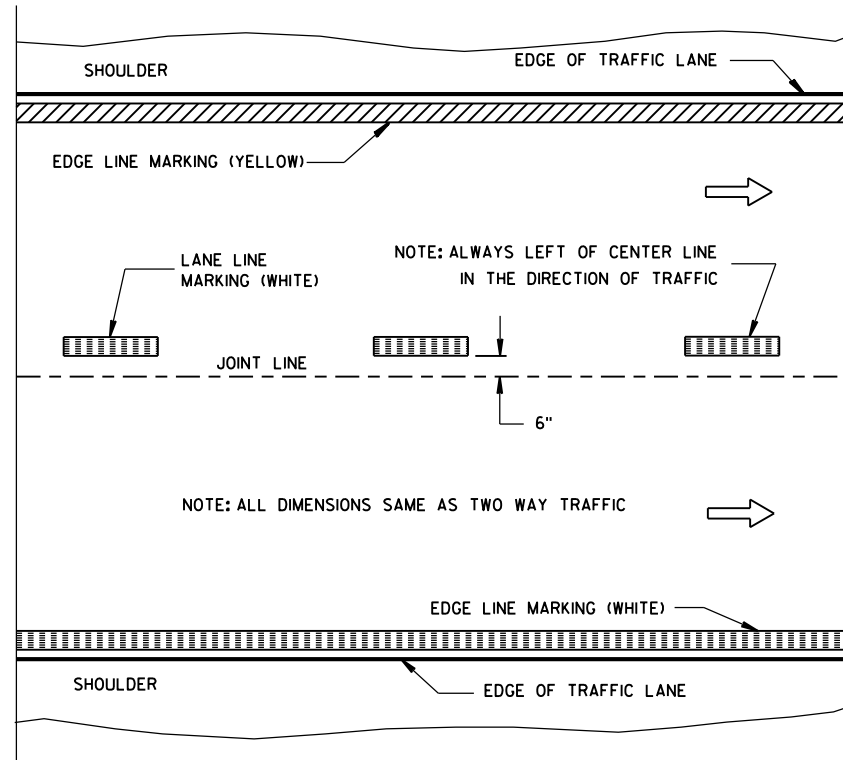
7/1/11
DATE

/S/ Thomas N. Notbohm
STATE TRAFFIC ENGINEER OF DESIGN

FHWA

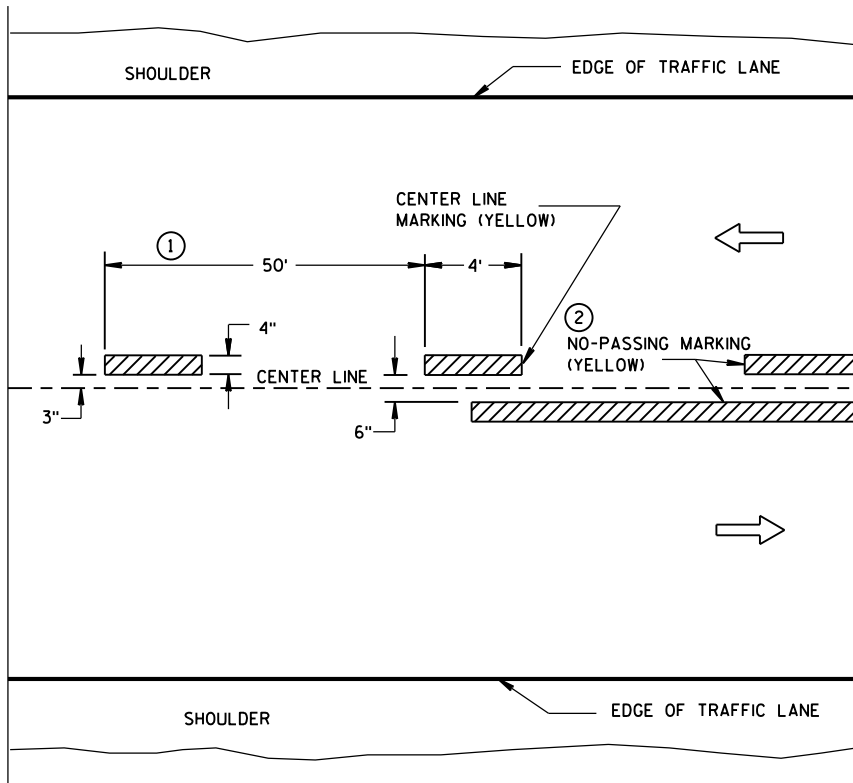


TWO WAY TRAFFIC

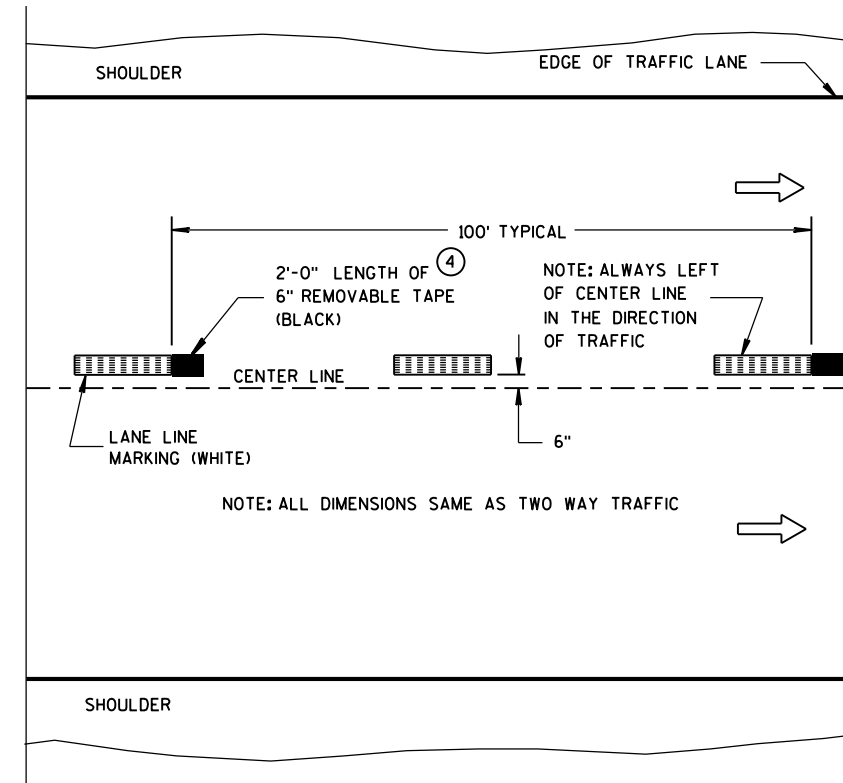


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING
(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

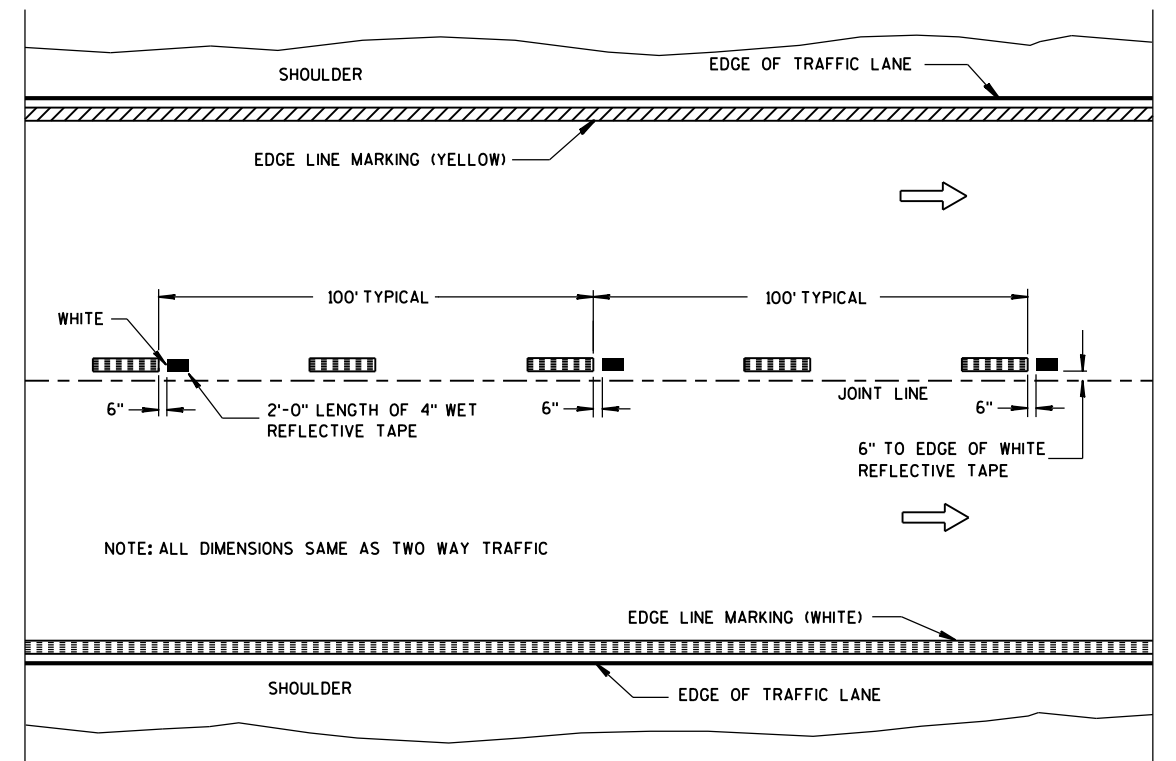
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- 1 HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- 2 NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- 3 NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
- 4 CONCRETE ONLY.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL



WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE

LEGEND

- "T" MARKING
- POST MOUNTED SIGN

PAVEMENT MARKING
(MAINLINE)

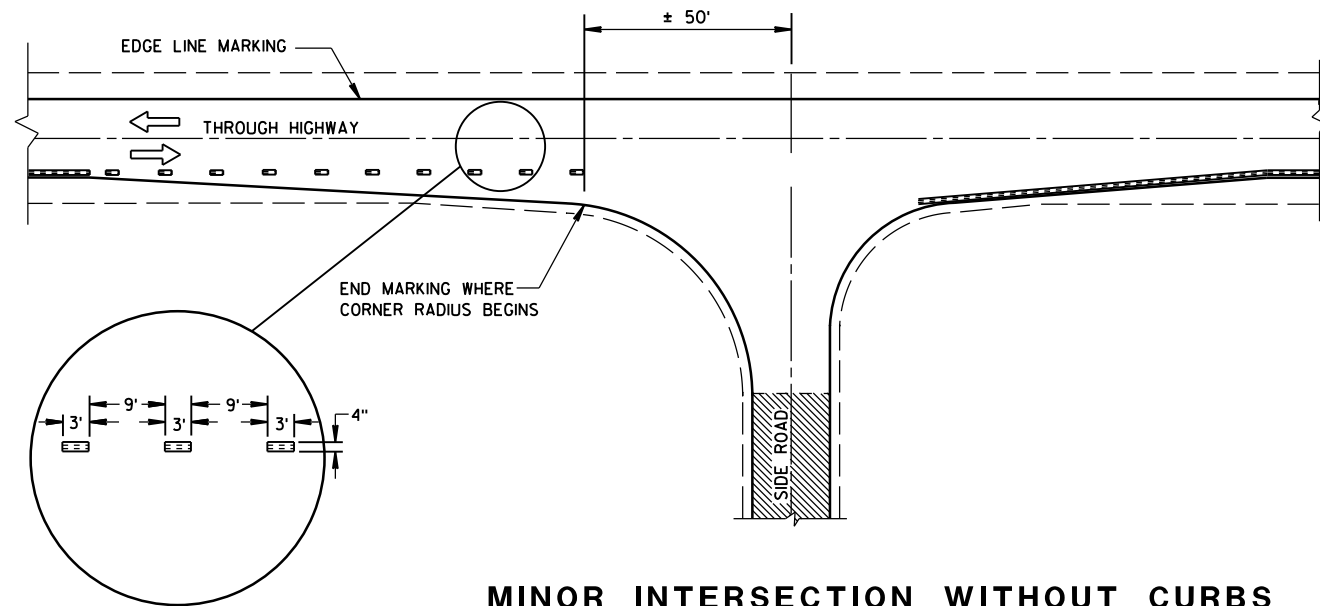
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

5-13-2013
DATE

FHWA

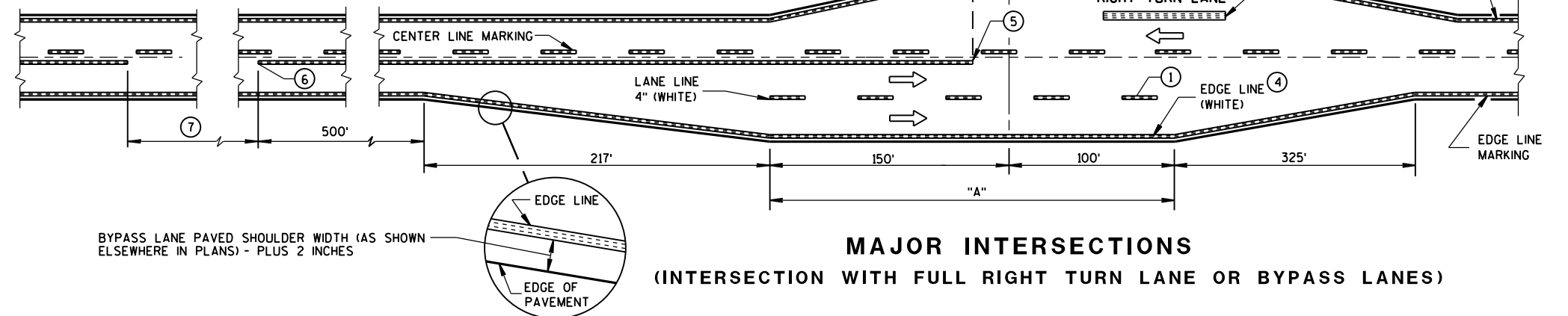
/S/ Travis Feltes
STATE TRAFFIC ENGINEER



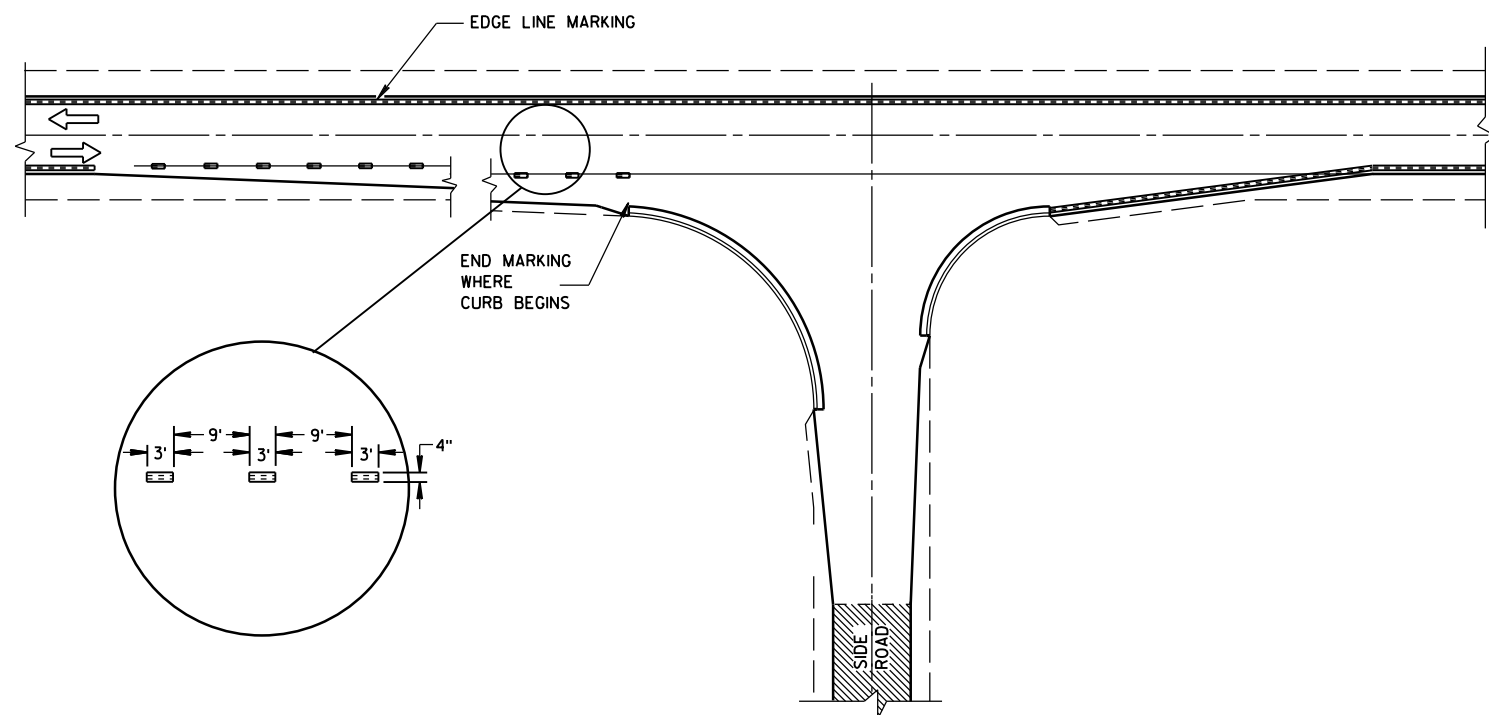
MINOR INTERSECTION WITHOUT CURBS

⑦

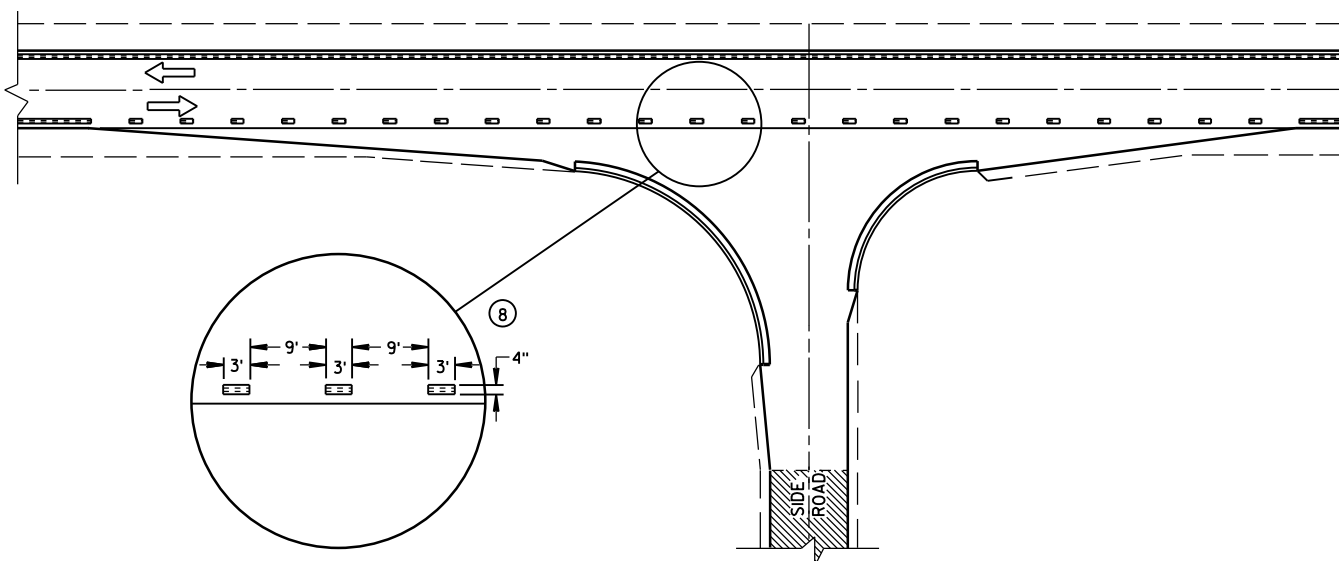
| POSTED SPEED (MPH) | MINIMUM DISTANCE BETWEEN ZONES (FEET) |
|--------------------|---------------------------------------|
| 25 - 30 | 528 |
| 35 - 40 | 528 |
| 45 - 50 | 686 |
| 55 | 792 |



MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)



MINOR INTERSECTION WITH CURBS
(TYPICAL MARKING)



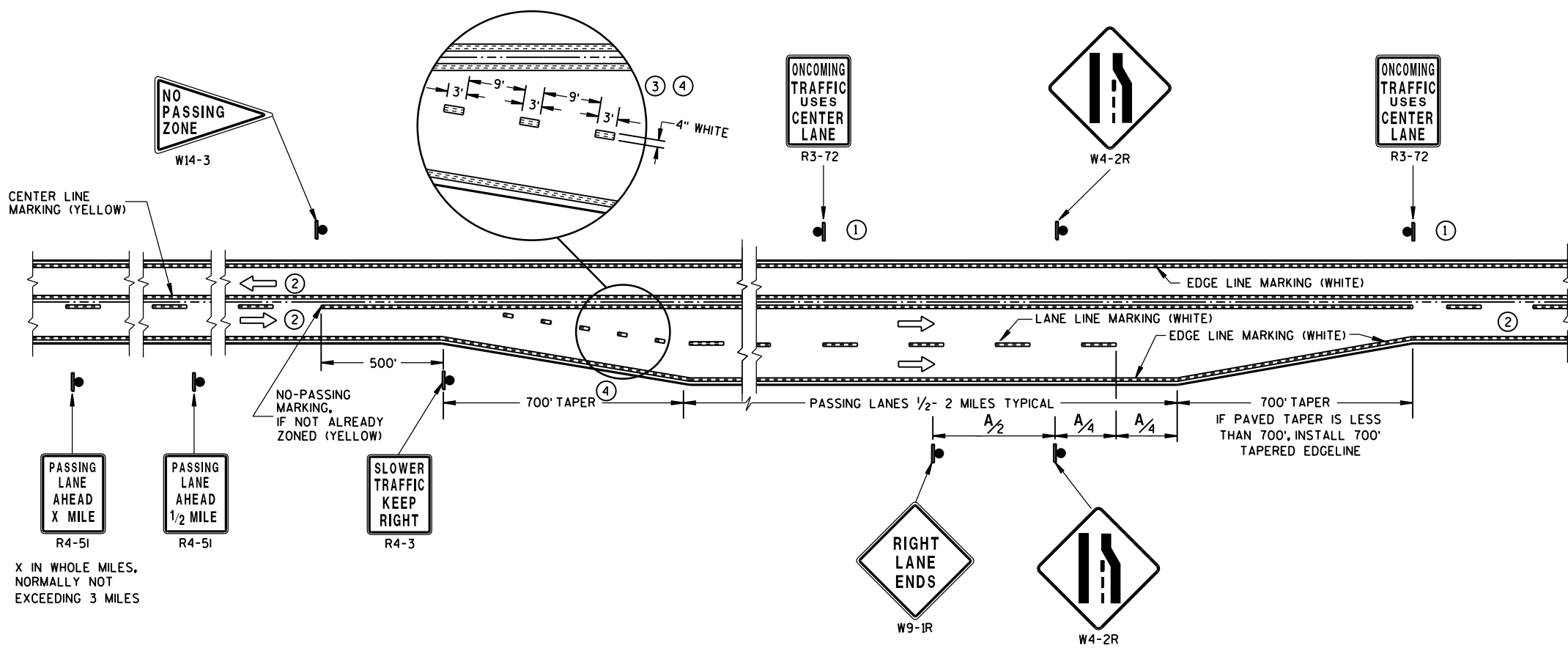
MINOR INTERSECTION WITH CURBS
③ (FOR SPECIAL CONDITIONS AS SPECIFIED)

GENERAL NOTES

- EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
 - ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
 - ③ ALTERNATIVE MARKING SHALL BE PROVIDED WHEN SPECIFIED IN THE CONTRACT. TYPICAL SITUATIONS WHERE THIS MARKING MAY BE REQUIRED ARE WHERE THE INTERSECTION IS ON A SHARP HORIZONTAL CURVE OR CREST VERTICAL CURVE IN AN UNLIGHTED AREA SUCH THAT THE EDGE LINE MAY BE MISLEADING TO THE MOTORIST OR DISAPPEAR FROM SIGHT.
 - ④ THE EDGE LINE IN THE TAPER AREAS OF THE BYPASS LANE AND THE BYPASS LANE SHALL BE LOCATED 1-FOOT FROM EDGE OF PAVEMENT TO THE OUTSIDE EDGE OF EDGE LINE.
 - ⑤ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.
 - ⑥ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
 - ⑦ IF THE DISTANCE BETWEEN 2 SUCCESSIVE NO-PASSING ZONES IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES, CONNECT THE 2 ZONES.
 - ⑧ 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

PAVEMENT MARKING
(INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**SOLID DOUBLE-YELLOW LINE
(THROUGHOUT ENTIRE PASSING/CLIMBING LANE)**

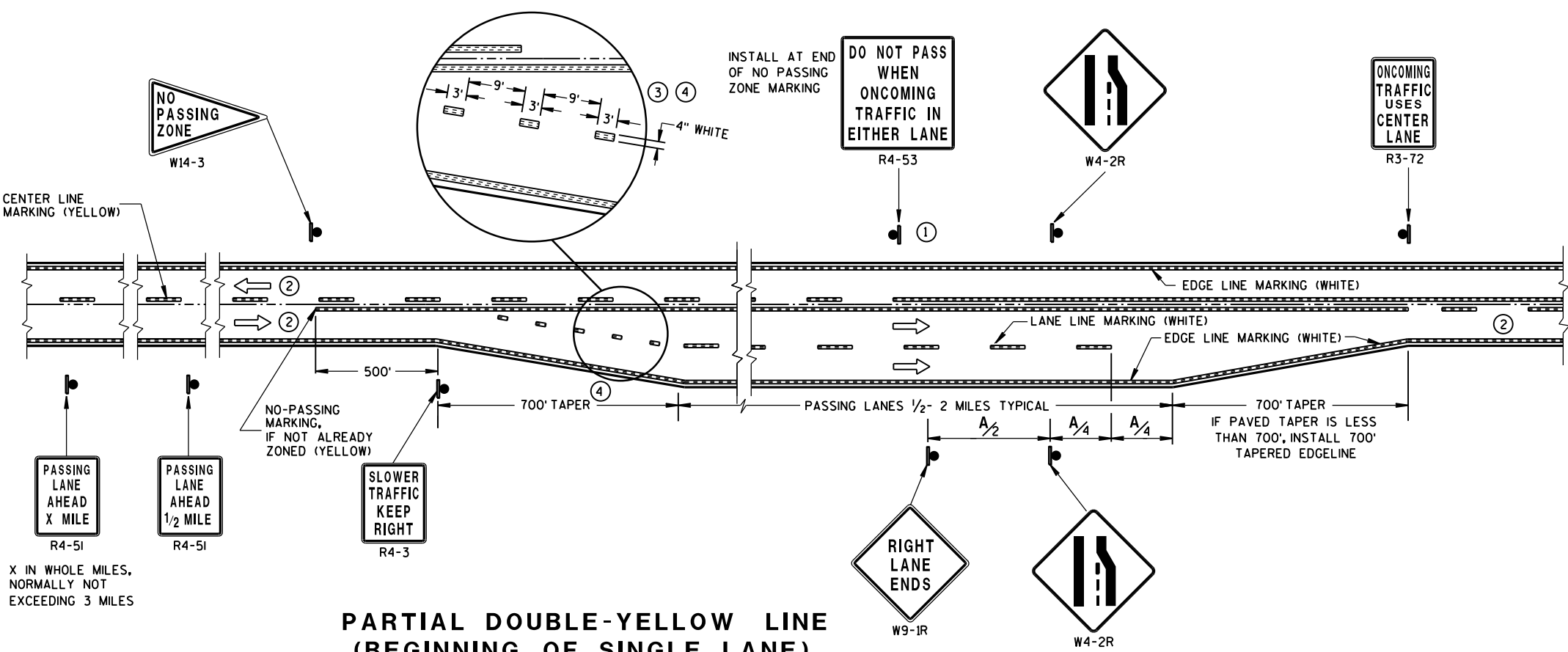
GENERAL NOTES

- ① SIGN SHALL BE REPEATED AT 1 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- ② THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- ③ THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ④ WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL (➡) SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

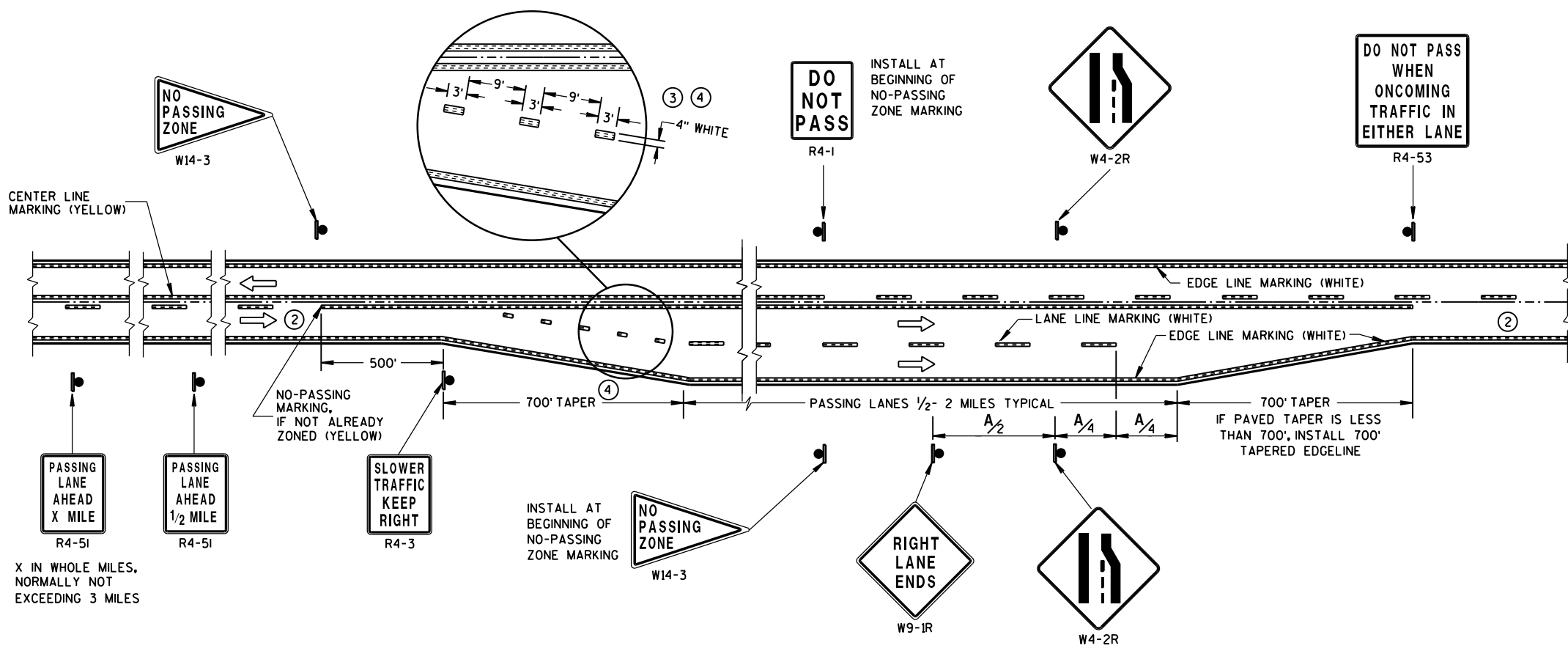
| POSTED OR 85th PERCENTILE SPEED | DISTANCE "A" |
|---------------------------------|--------------|
| 45 | 750 |
| 50 | 850 |
| 55 | 950 |



**PARTIAL DOUBLE-YELLOW LINE
(BEGINNING OF SINGLE LANE)**

**PAVEMENT MARKING & SIGNING
(CLIMBING LANE & PASSING LANE)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**SOLID DOUBLE-YELLOW LINE
(END OF SINGLE LANE)**

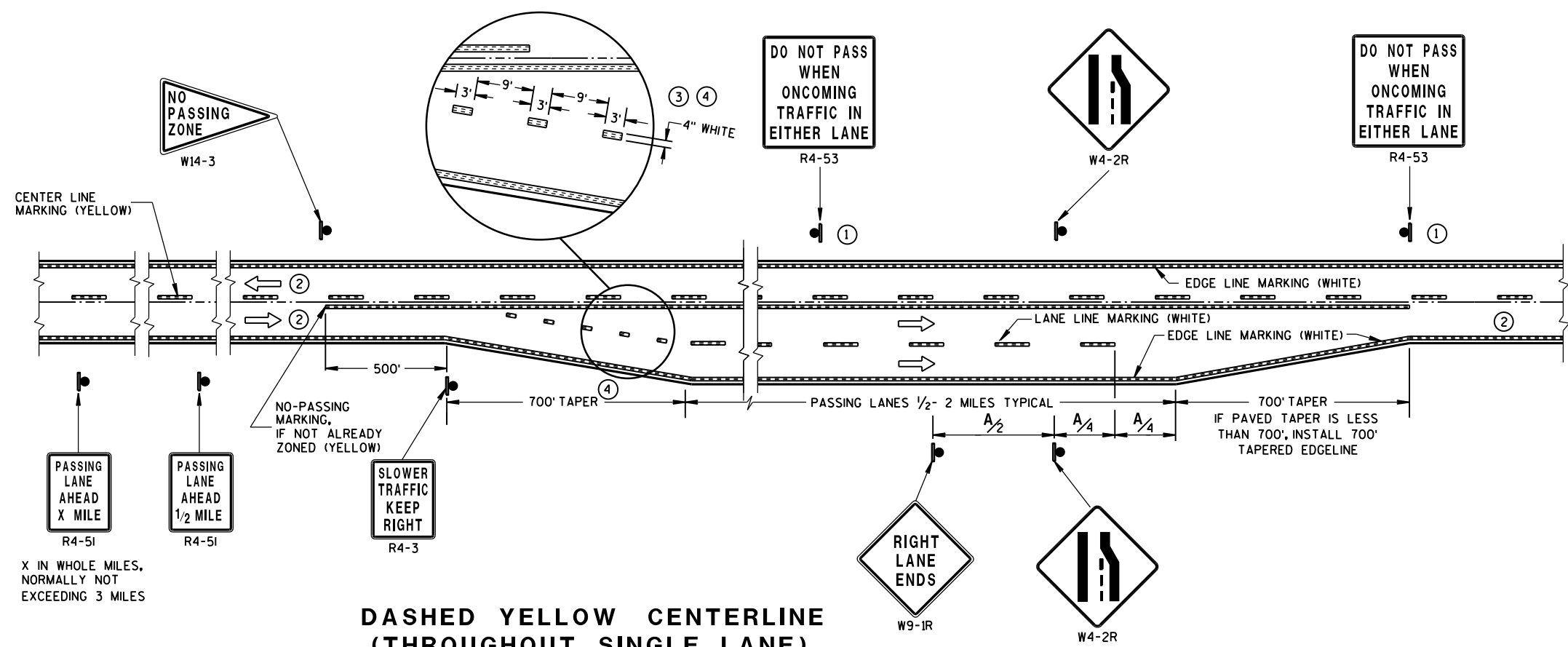
GENERAL NOTES

- ① SIGN SHALL BE REPEATED AT 1/2 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- ② THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- ③ THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ④ WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

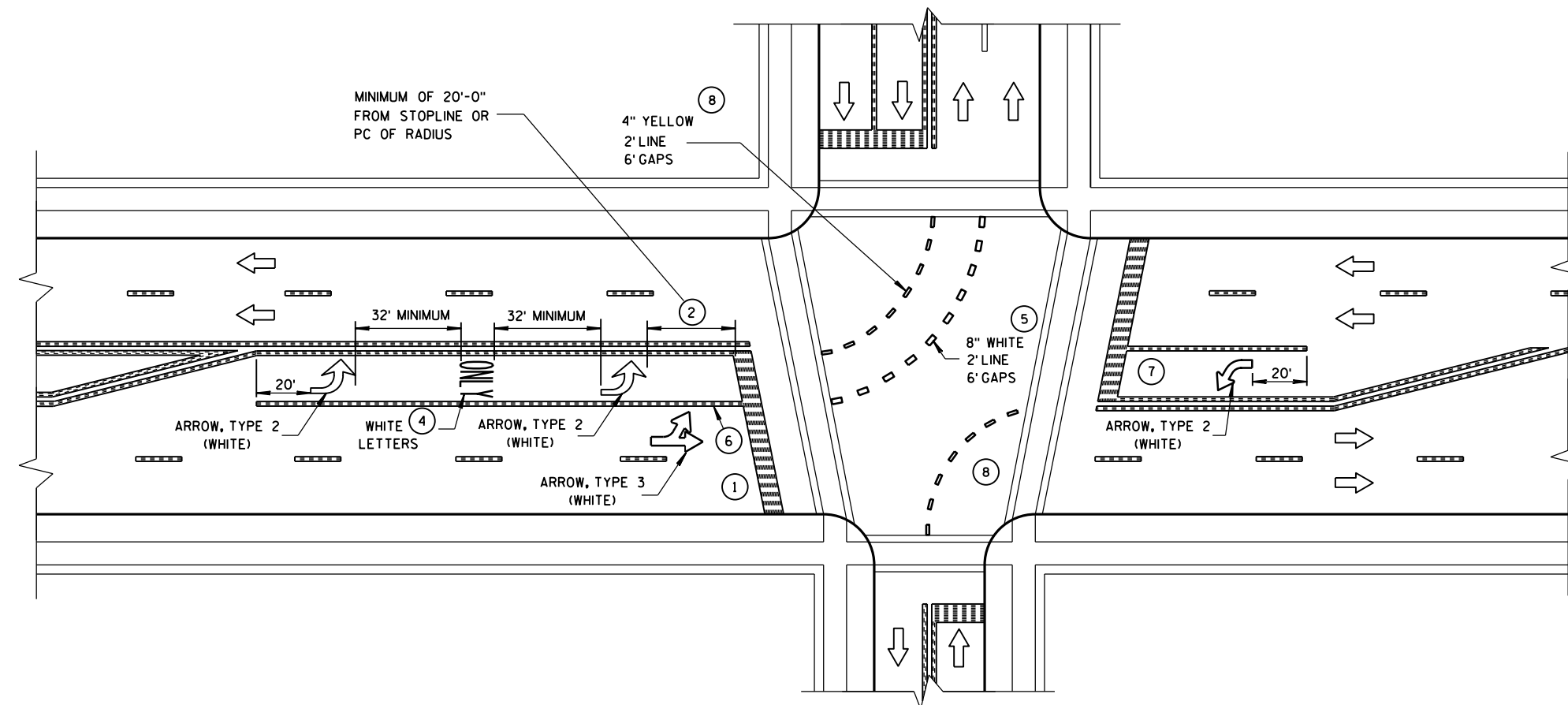
| POSTED OR 85th PERCENTILE SPEED | DISTANCE "A" |
|---------------------------------|--------------|
| 45 | 750 |
| 50 | 850 |
| 55 | 950 |



**DASHED YELLOW CENTERLINE
(THROUGHOUT SINGLE LANE)**

**PAVEMENT MARKING & SIGNING
(CLIMBING LANE &
PASSING LANE)**

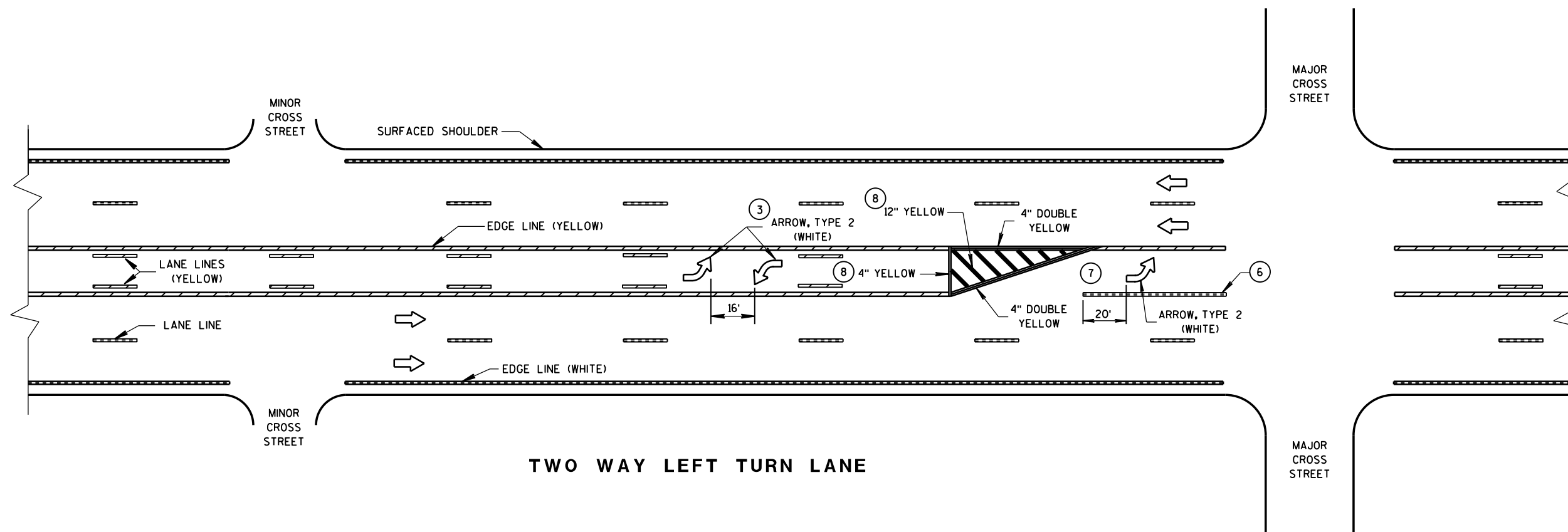
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



GENERAL NOTES

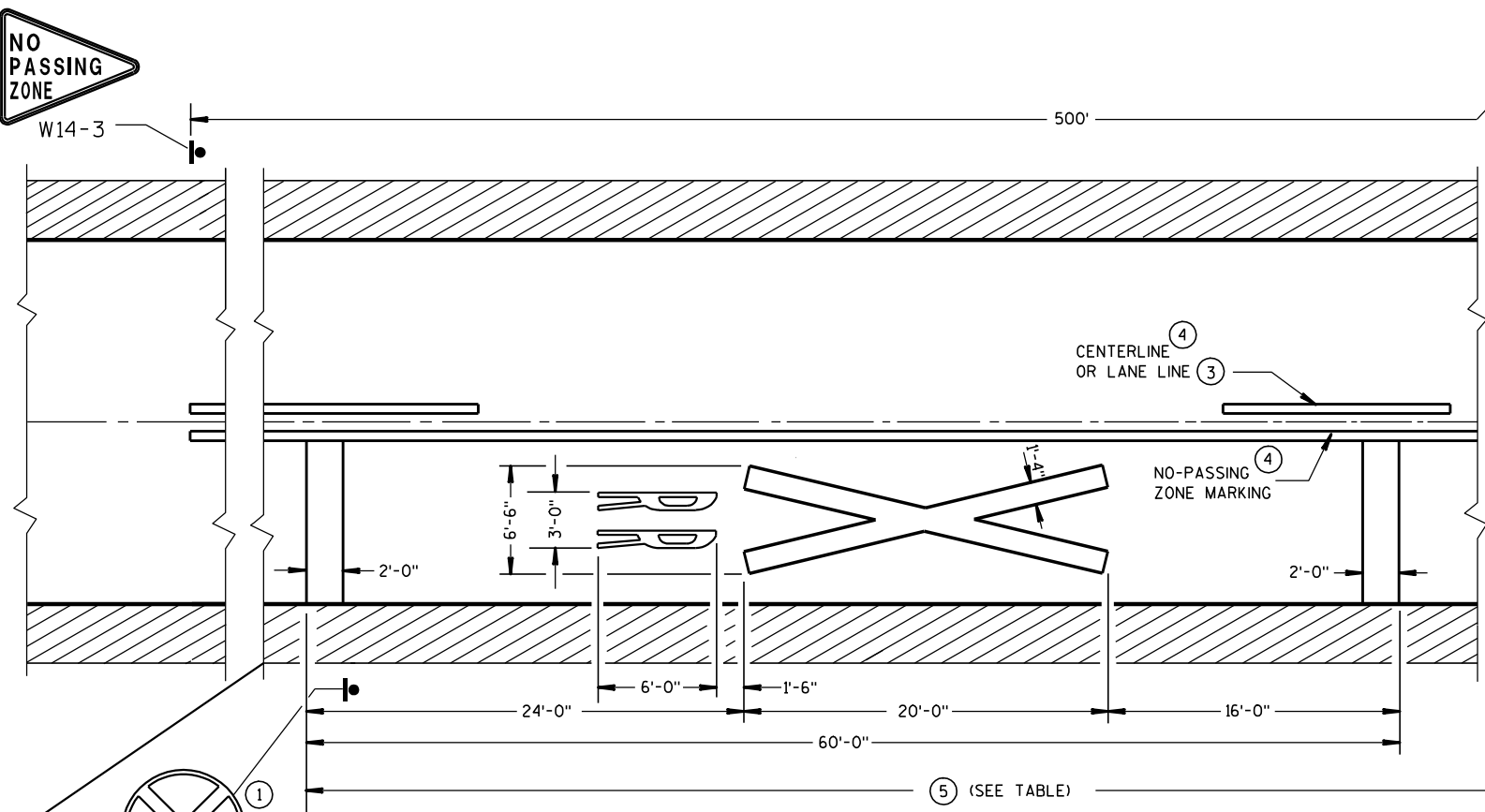
- ① STOP BAR IS REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.
- ② DISTANCE MAY BE ADJUSTED TO ACCOMMODATE SHORT LEFT TURN LANES, AS APPROVED BY THE ENGINEER.
- ③ A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ④ ADD EXTRA SETS OF ONE ARROW AND ONE ONLY PER 160 FEET OR WHEN ON A CURVE.
- ⑤ 8" WHITE WITH 2' LINE 6' GAPS FOR DUAL TURN LANE.
- ⑥ 8" WHITE
- ⑦ ADD SECOND ARROW WHEN TURN BAY IS GREATER THAN OR EQUAL TO 108 FEET.
- ⑧ REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.

NOTE:
ARROW SYMBOL (➡)
SHOWS DIRECTION OF TRAVEL

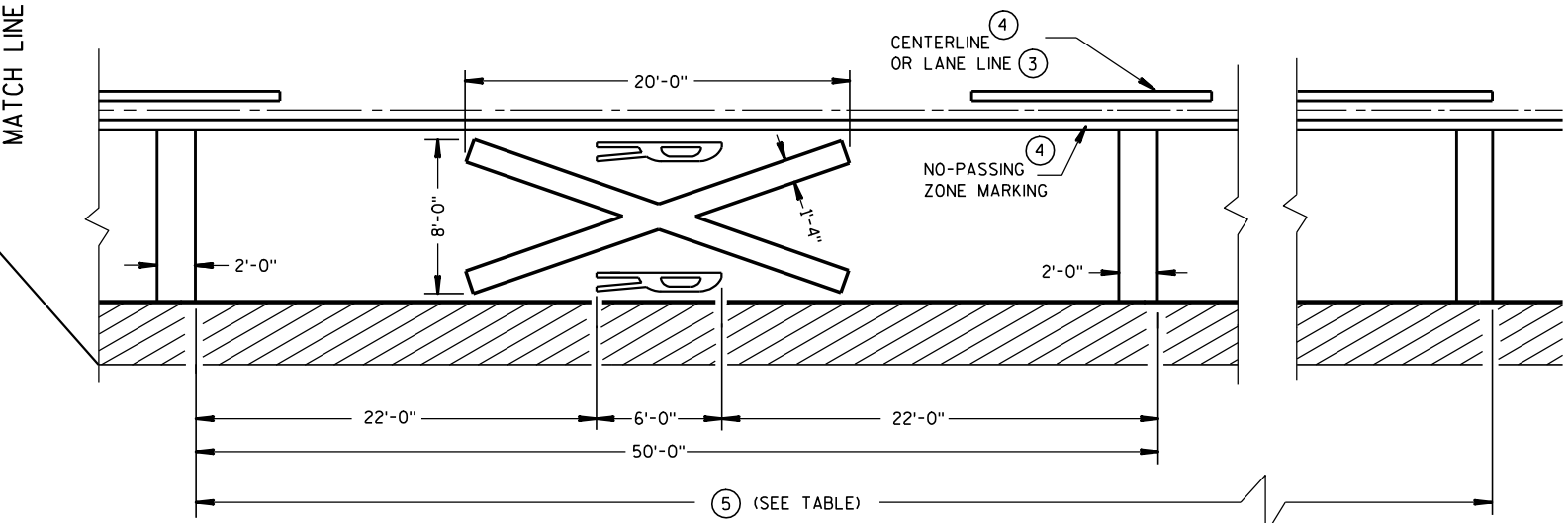


PAVEMENT MARKING
(LEFT TURN LANE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



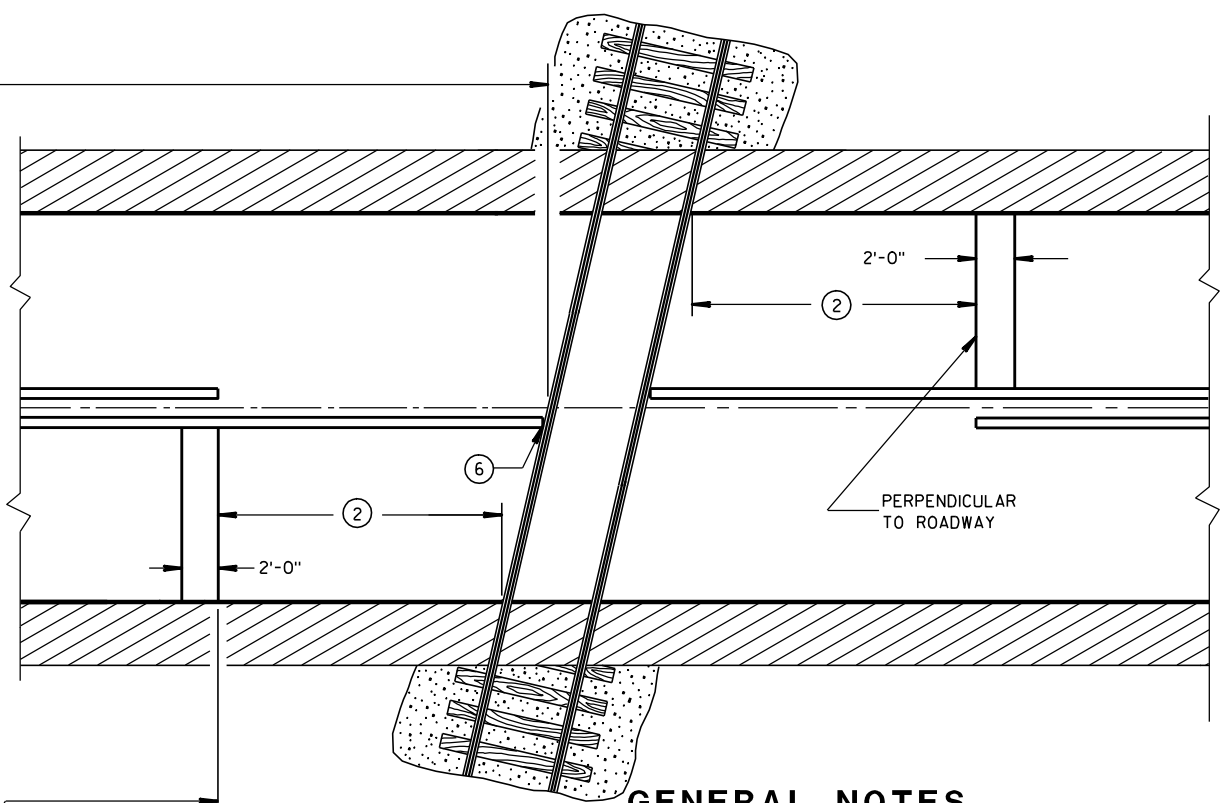
PREFERRED PAVEMENT MARKING



ALTERNATE PAVEMENT MARKING

| Posted Speed (M.P.H.) | Dimension Range (Feet) |
|-----------------------|------------------------|
| 25 | 150*- 250 |
| 30 | 200*- 300 |
| 35 | 250*- 450 |
| 40 | 300*- 500 |
| 45 | 400*- 650 |
| 50 | 550*- 800 |
| 55 | 750*- 1000 |
| 60 | 1000*- 1250 |
| 65 | 1000*- 1250 |

* THE MINIMUM DISTANCES IN THE TABLE ARE DESIRABLE AND SHOULD BE USED. THE DISTANCES MAY BE INCREASED UP TO THE MAXIMUM TO ALLOW FOR FIELD CONDITIONS SUCH AS THE CLOSE PROXIMITY OF DRIVEWAYS, BRIDGES, SIDEROADS OR OTHER FEATURES THAT WOULD PROHIBIT THE MINIMUM DISTANCES FROM BEING USED.



GENERAL NOTES

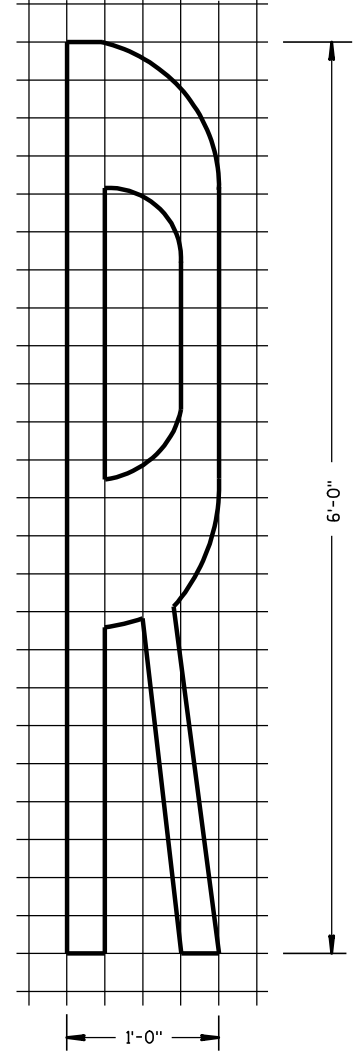
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

A THREE-LANE ROADWAY SHOULD BE MARKED WITH A CENTERLINE FOR TWO-LANE APPROACH OPERATION ON THE APPROACH TO A CROSSING.

ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE. ALL LETTERS AND SYMBOLS SHALL BE IN CONFORMANCE WITH THE "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS" (ADOPTED BY THE FEDERAL HIGHWAY ADMINISTRATION).

CENTER OR LANE LINES AND NO-PASSING ZONE MARKINGS SHOWN ON THIS DRAWING ARE REQUIRED AND PAID FOR UNDER OTHER ITEMS IN THE CONTRACT.

- ① A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).
- ② MINIMUM 8' FROM ANY RAILROAD WARNING DEVICES (SIGNALS, GATES, ETC.) OR 25' FROM THE NEAREST RAIL, WHICHEVER DISTANCE IS GREATER.
- ③ REFLECTIVE WHITE.
- ④ REFLECTIVE YELLOW 500' MINIMUM. MARKING LIMITS MAY BE EXTENDED AS DIRECTED BY THE ENGINEER TO MEET ADJACENT NO-PASSING ZONE MARKINGS.
- ⑤ TABLE BASED UPON 2C-4 WISCONSIN SUPPLEMENT OF MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- ⑥ FOR MULTIPLE TRACK CROSSINGS, THE BARRIER LINE SHALL EXTEND TO THE NEAR RAIL OF THE FURTHEST TRACK IN THE DIRECTION OF HIGHWAY TRAVEL.







SIGNING AND PAVEMENT MARKING
DETAILS FOR RAILROAD-HIGHWAY
GRADE CROSSINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
1-9-2012 /S/ Thomas N. Notbohm
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

LEGEND

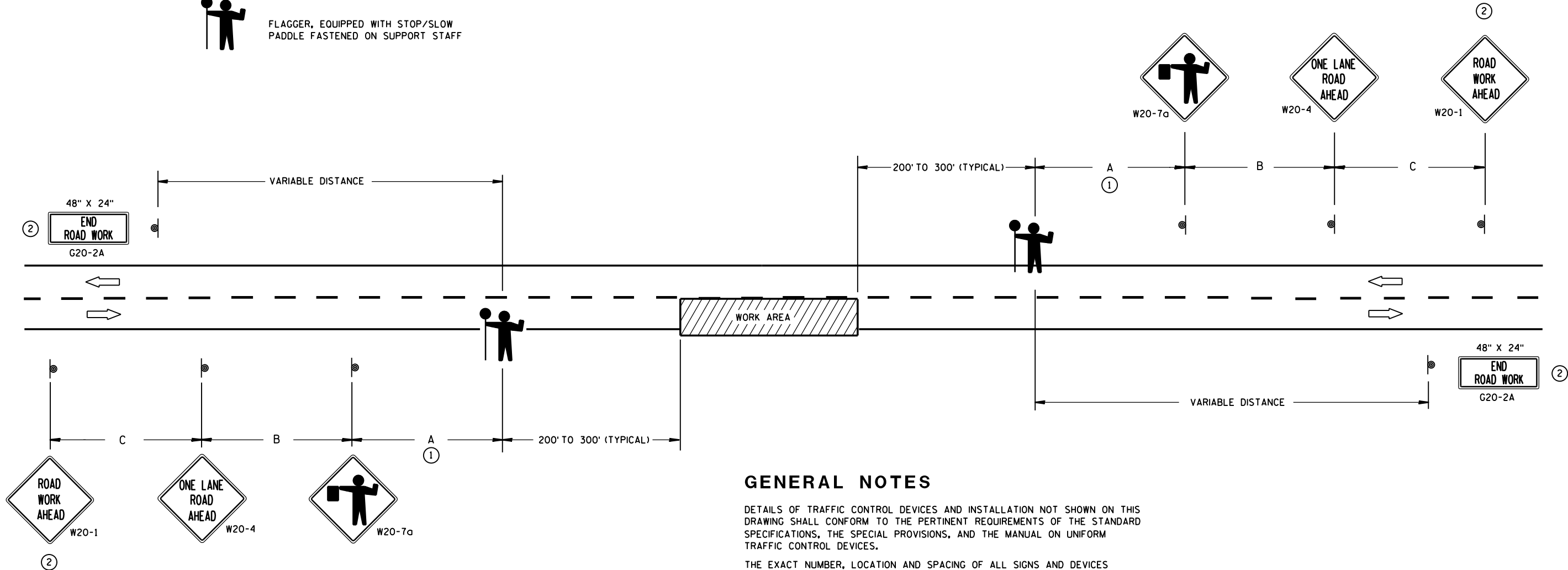
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

| SPEED LIMIT | SIGN SPACING A,B,C |
|-------------|-----------------------|
| 25-35 MPH | 200' |
| 35-40 MPH | 350' |
| 45-55 MPH | 500' |



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

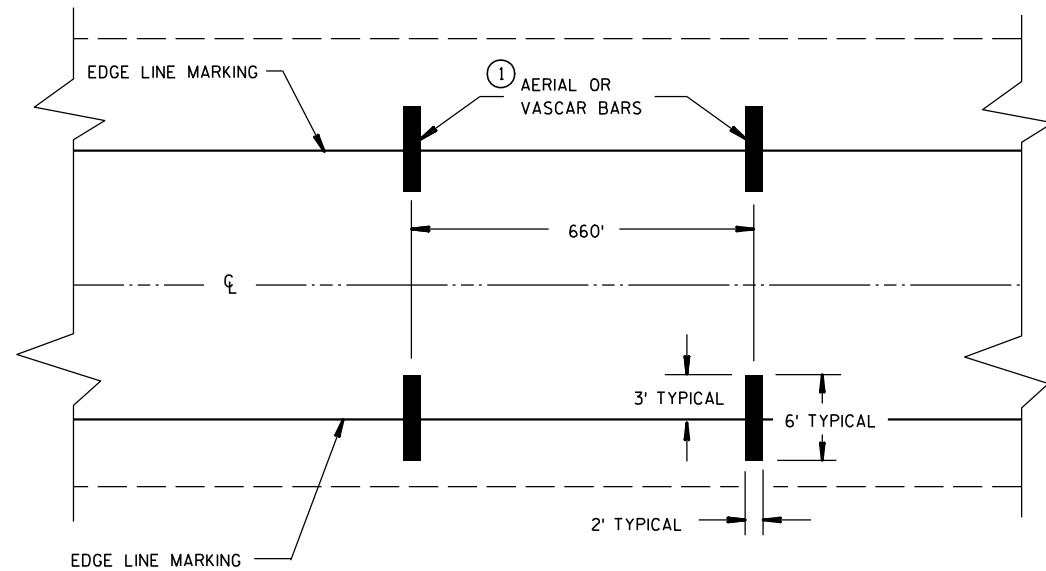
① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

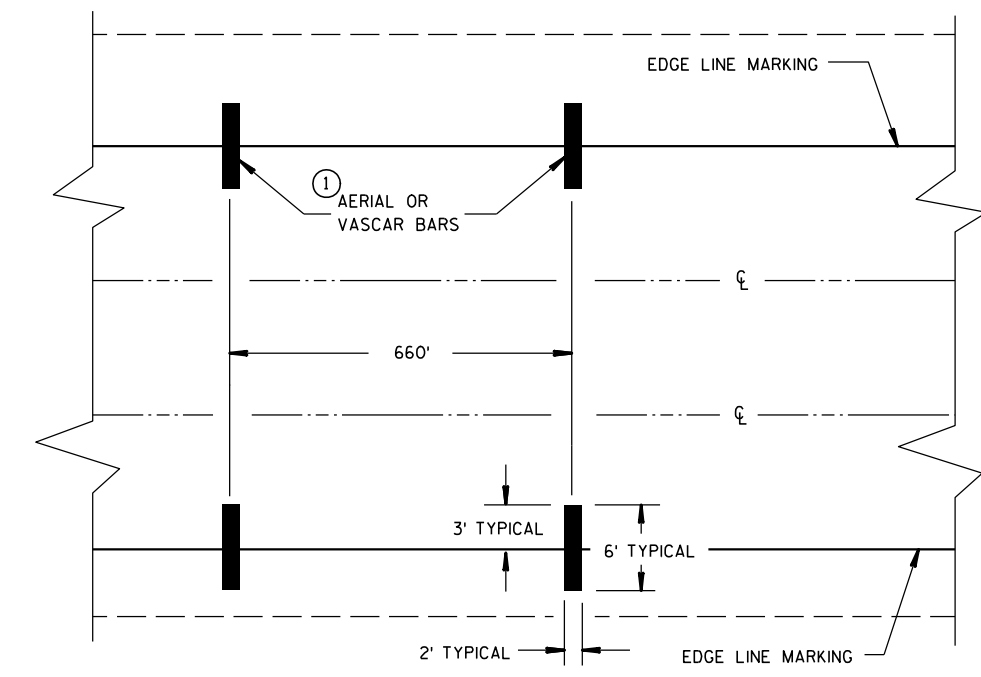
TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



TYPICAL FOR TWO WAY OR ONE WAY TRAFFIC



TYPICAL FOR MULTILANE TRAFFIC

SPEED ENFORCEMENT ZONE WITH AERIAL OR VASCAR BARS

GENERAL NOTES

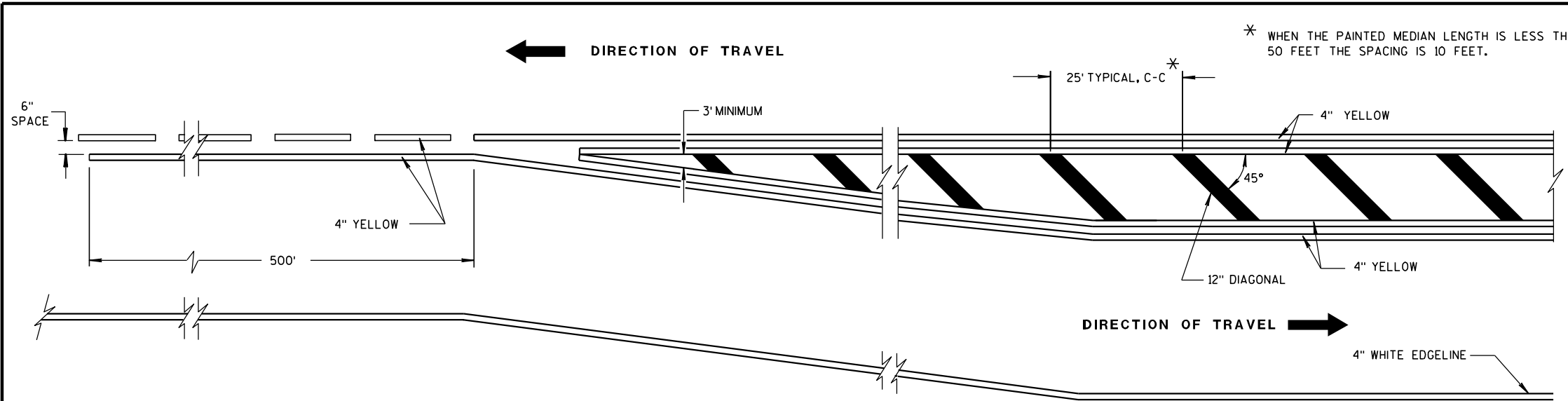
① NUMBER OF VASCAR OR AERIAL BARS SHALL BE A MINIMUM OF 2 OR A MAXIMUM OF 5 AT 660' SPACING.

A CAR CAN BE PROVIDED BY THE WISCONSIN STATE PATROL FOR TRAFFIC CONTROL.

AERIAL ENFORCEMENT BARS
PAVEMENT MARKING DETAILS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

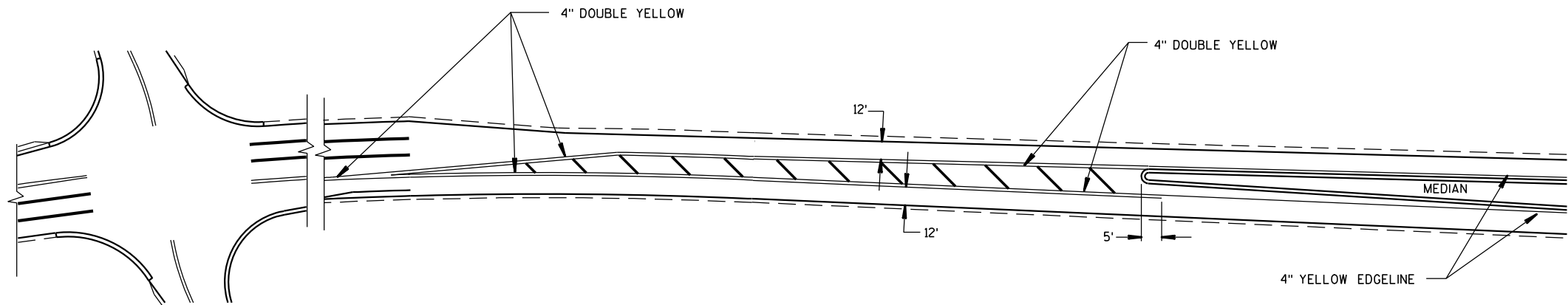
APPROVED
4/23/01 DATE /S/ Chester J. Spang CHIEF SIGNS AND MARKING ENGINEER
FHWA



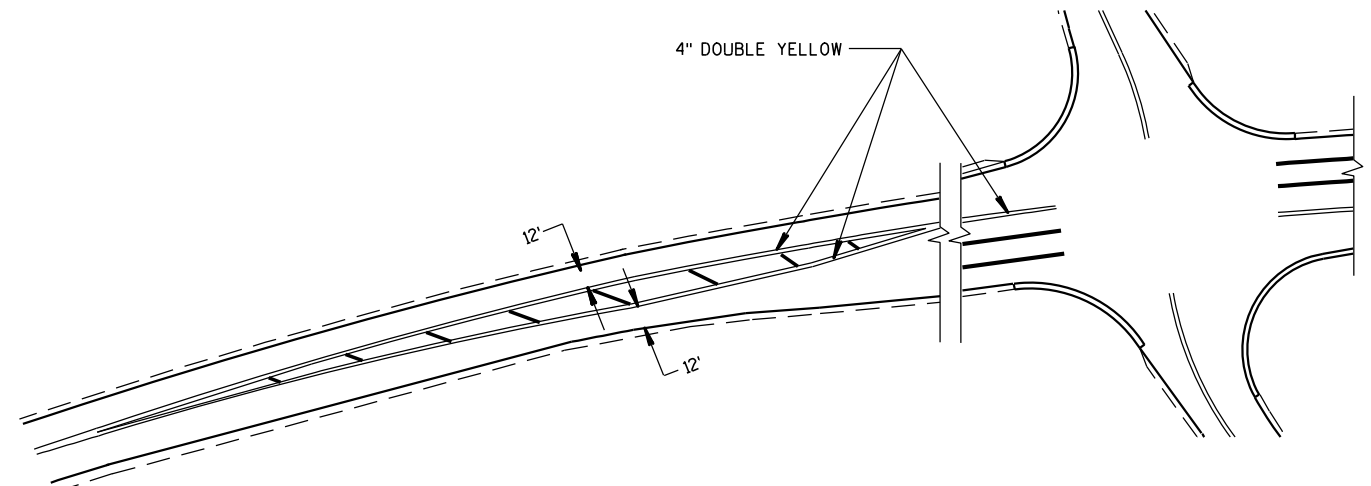
MEDIAN ISLAND DETAIL

GENERAL NOTE

DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT WIDEST POINT.

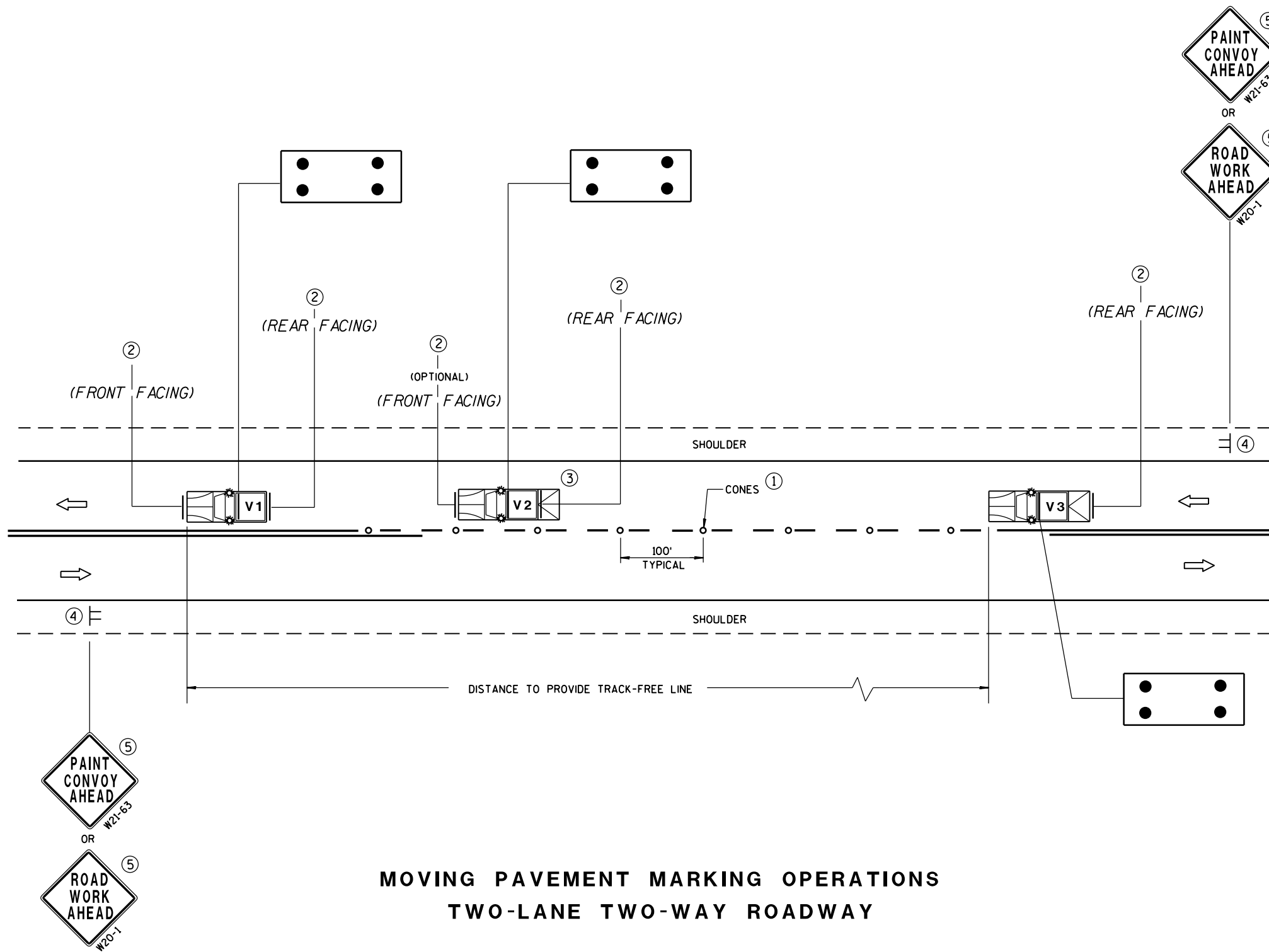


APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON APPROACH MARKINGS

| | |
|--|---|
| MEDIAN ISLAND MARKING | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED 2-5-09 DATE | /S/ Thomas N. Notbohm STATE TRAFFIC ENGINEER OF DESIGN |
| FHWA | |



MOVING PAVEMENT MARKING OPERATIONS TWO-LANE TWO-WAY ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

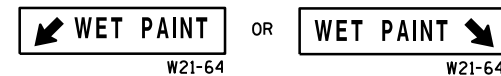
THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.



③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.

④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

V1 LEAD VEHICLE


V2 SHADOW VEHICLE

V3 TRAIL VEHICLE WITH TMA

TMA TRUCK-MOUNTED ATTENUATOR

 SIGN ON TEMPORARY SUPPORT

 DIRECTION OF TRAFFIC

 CONES

 FLASHING ARROW PANEL (CAUTION)

MOVING PAVEMENT MARKING
OPERATION
TWO-LANE TWO-WAY ROADWAY

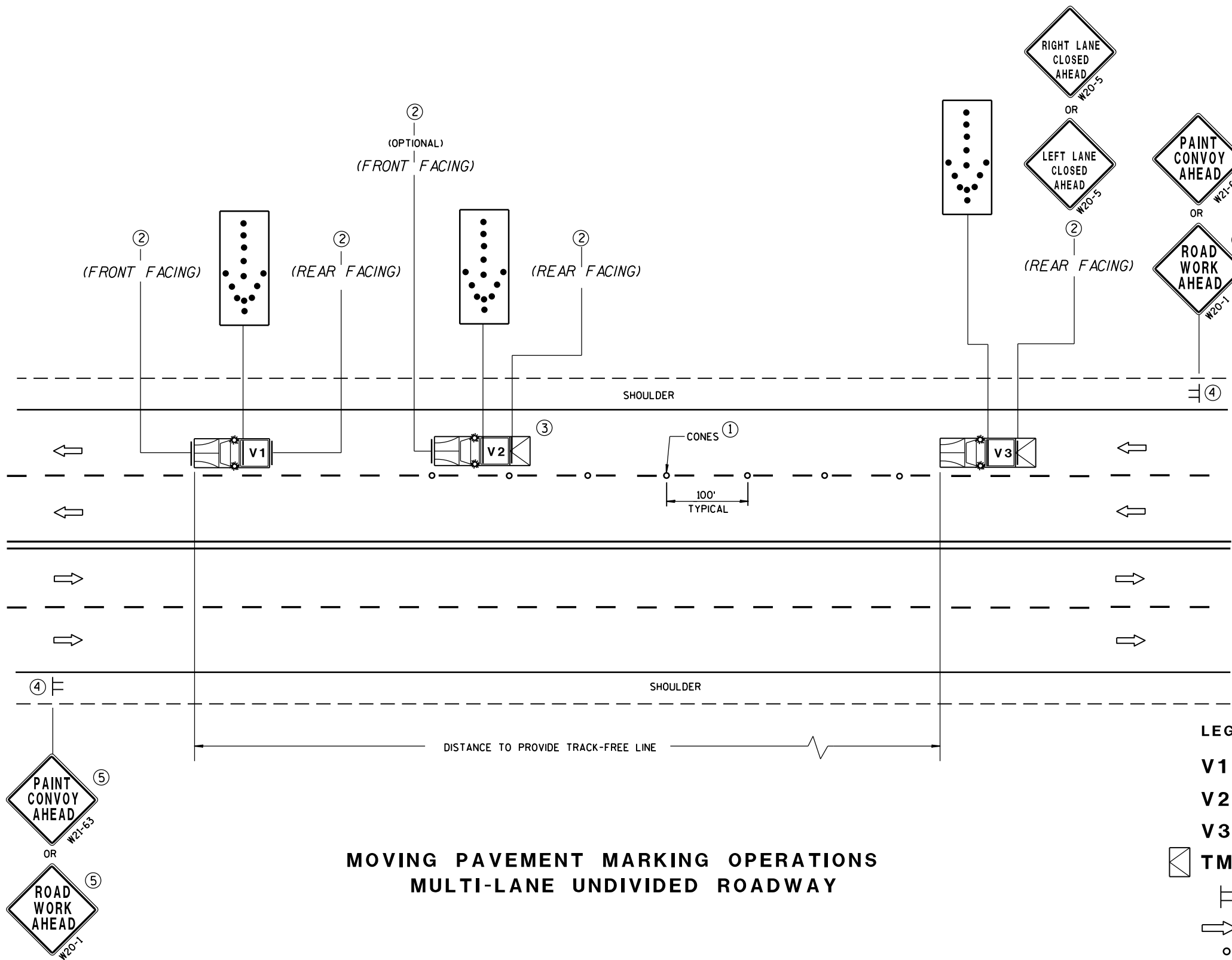
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

5/3/2013
DATE

/S/ Travis Feltes
STATE TRAFFIC ENGINEER

FHWA

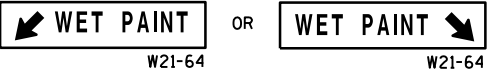


MOVING PAVEMENT MARKING OPERATIONS
MULTI-LANE UNDIVIDED ROADWAY

GENERAL NOTES

- ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.
- ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.
- IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.
- ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.
- DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.
- WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.
- USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.
- FOR EDGELINE MARKING OR IF CONES ARE NOT USED, POSITION THE REARMOST SHADOW VEHICLE ON THE SHOULDER AS SHOWN IN THE MUTCD IF THE SHOULDER HAS ADEQUATE WIDTH.
- WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.
- THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE OR LANELINE MARKING FOR MULTILANE UNDIVIDED ROADWAYS.

- ① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- ② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.
- ③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.
- ④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.



LEGEND

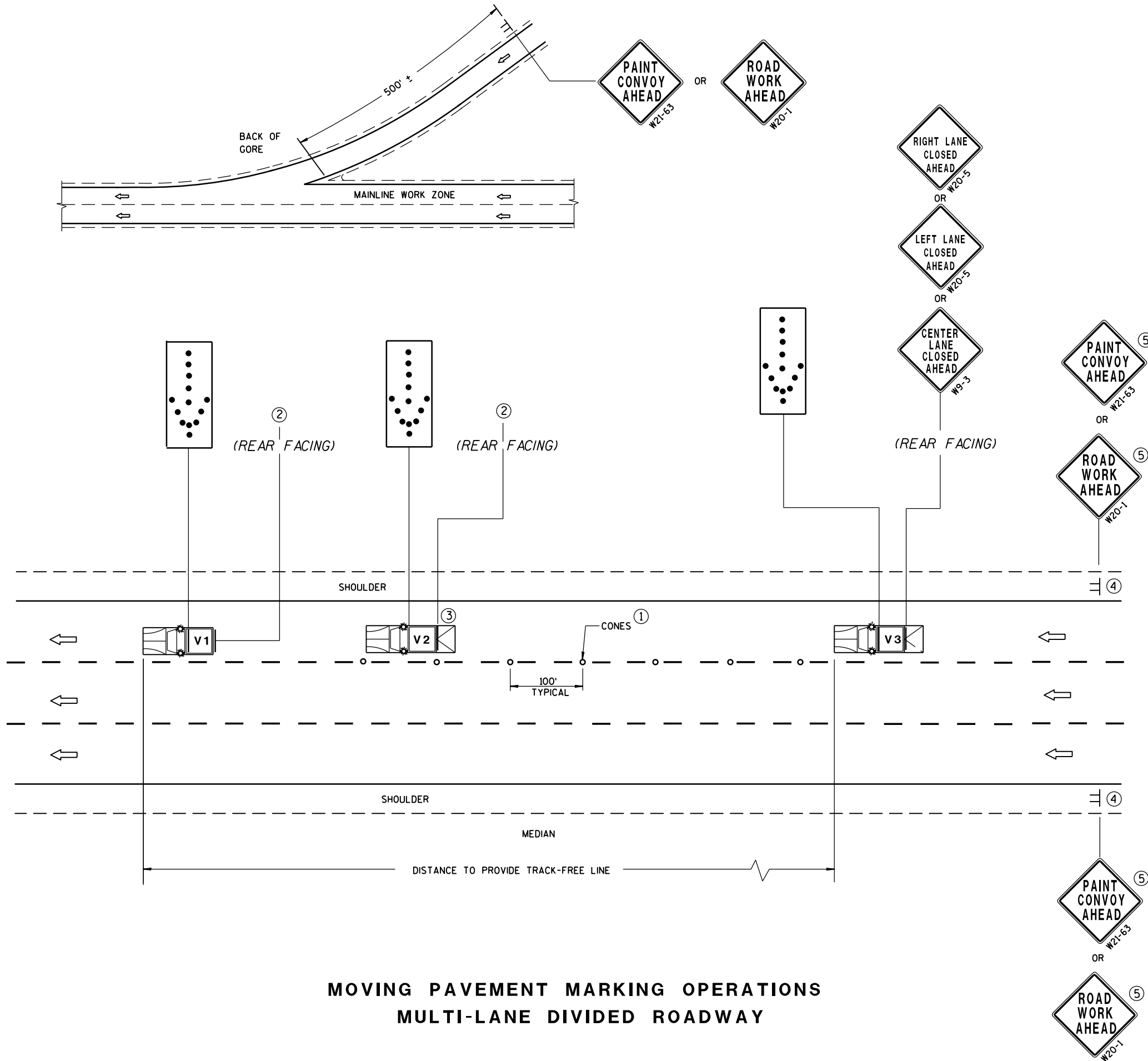
- V1 LEAD VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE WITH TMA
- TMA TRUCK-MOUNTED ATTENUATOR
- Sign on temporary support
- Direction of traffic
- Cones
- Flashing arrow panel (merge)

MOVING PAVEMENT MARKING
OPERATION
MULTI-LANE UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5/3/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER

FHWA



GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

FOR EDGE LINE MARKING OR IF CONES ARE NOT USED, POSITION THE REARMOST SHADOW VEHICLE ON THE SHOULDER AS SHOWN IN THE MUTCD IF THE SHOULDER HAS ADEQUATE WIDTH. USE DOUBLE ARROWS WHEN CONVOY IS IN CENTER LANE ONLY.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

THIS DRAWING SHALL BE USED FOR EDGE LINE OR LANELINE MARKING FOR MULTILANE DIVIDED ROADWAYS.

- ① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- ② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.
- ③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.
- ④ SIGNS SHALL BE REPEATED AFTER EVERY ON RAMP OR EVERY THREE MILES.
- ⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

- V1 LEAD VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE WITH TMA
- TMA TRUCK-MOUNTED ATTENUATOR
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- CONES
- FLASHING ARROW PANEL (MERGE)

MOVING PAVEMENT MARKING
OPERATION
MULTI-LANE DIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5/3/2013 DATE /S/ Travis Feltes
STATE TRAFFIC ENGINEER
FHWA

GENERAL NOTES

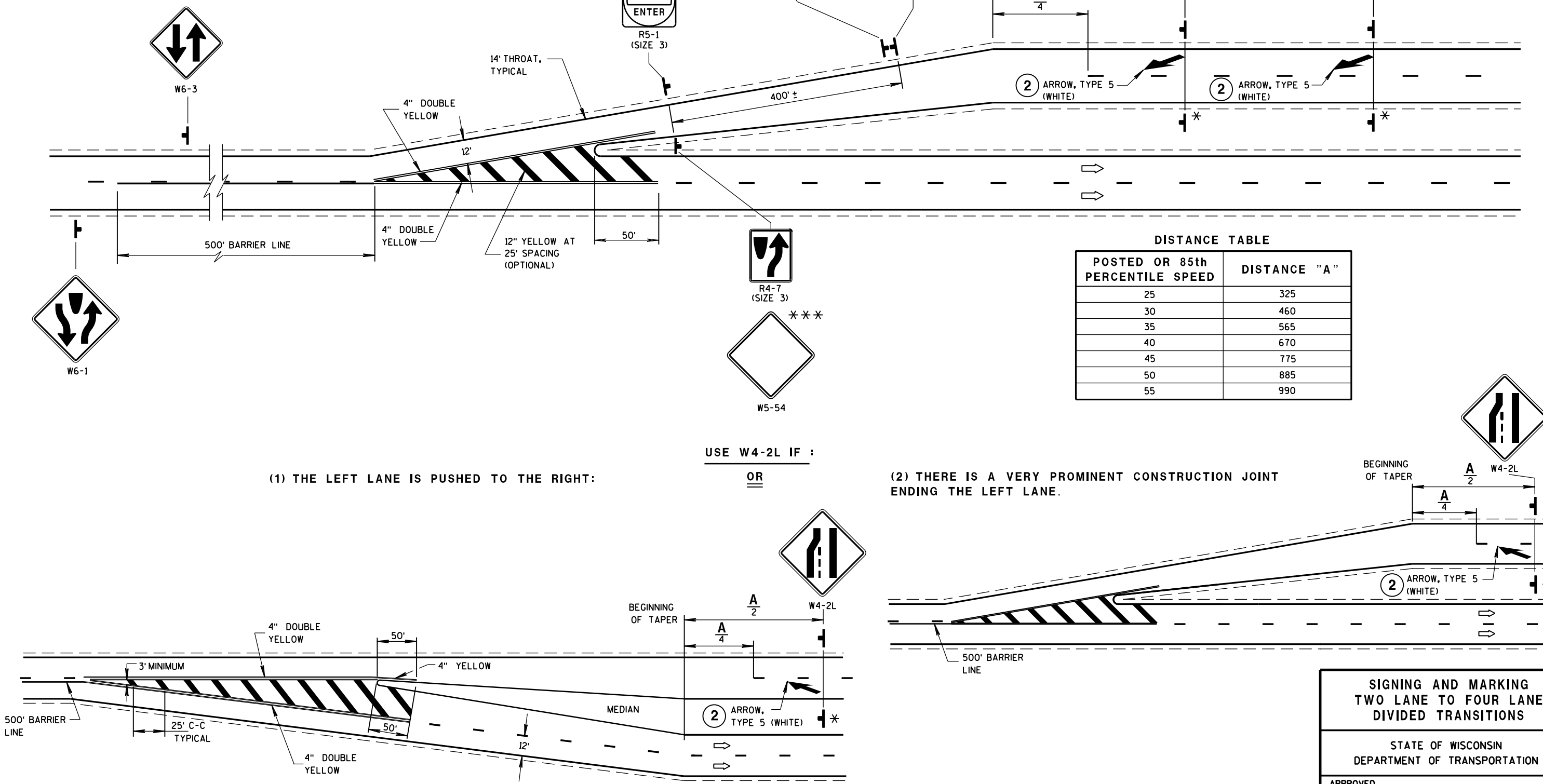
SIGNING AND MARKING IS SHOWN AS TYPICAL PLACEMENT. FIELD CONDITIONS MAY DICTATE CHANGES IN SIGNING AND MARKING PLACEMENT.

A DISTANCE DEPENDENT ON SPEED (SEE TABLE)

- 1 WRONG WAY PREVENTION SIGNS ARE DISCUSSED IN T G M 2-15-12
- 2 REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT

SYMBOLS

- * OPTIONAL SIGNS
- ** SIGNS MAY BE OMITTED IF SPACE DOES NOT PERMIT
- *** IF POSTED SPEED 45 MPH OR GREATER, PLACE W5-54 SIGN UNDER R4-7 SIGN. MOUNT W5-54 SIGN AT 4' MOUNTING HEIGHT (TOP OF ROADWAY TO BOTTOM OF SIGN)
- POST MOUNTED SIGN
- DIRECTION OF TRAFFIC FLOW



(1) THE LEFT LANE IS PUSHED TO THE RIGHT:

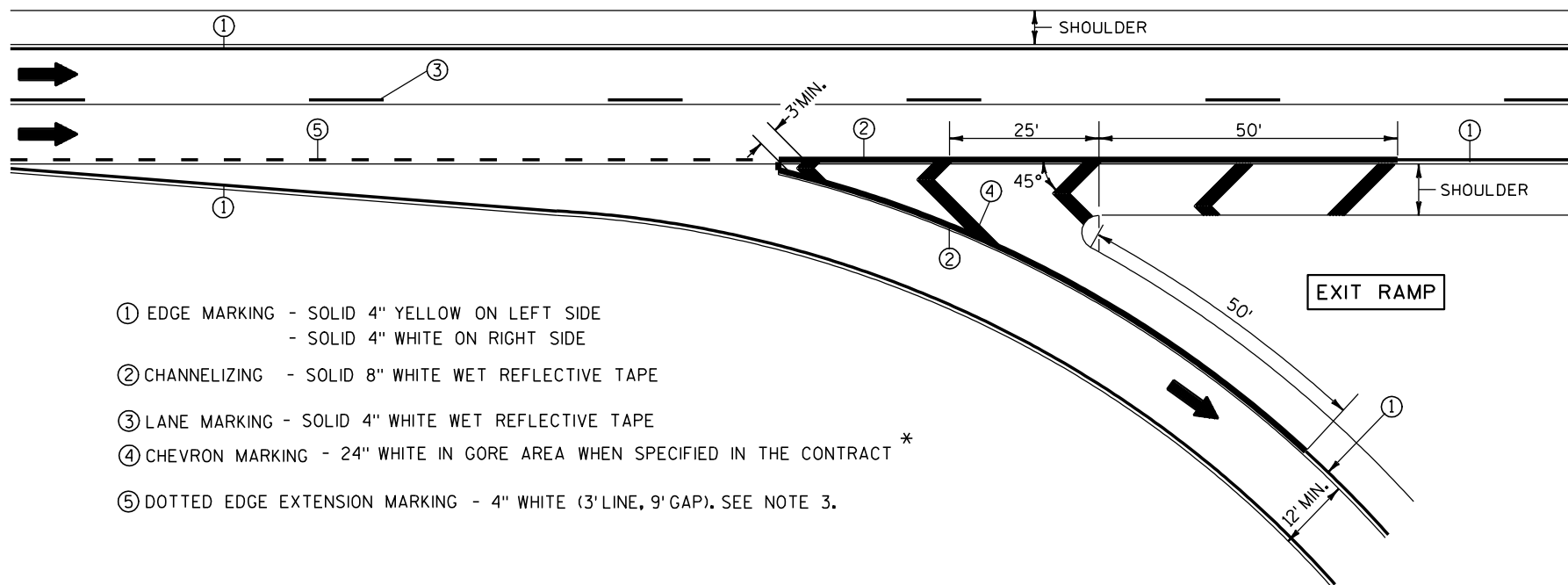
USE W4-2L IF :
OR

(2) THERE IS A VERY PROMINENT CONSTRUCTION JOINT
ENDING THE LEFT LANE.

SIGNING AND MARKING
TWO LANE TO FOUR LANE
DIVIDED TRANSITIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5/3/2013 DATE /S/ Travis Feltes
STATE TRAFFIC ENGINEER
FHWA

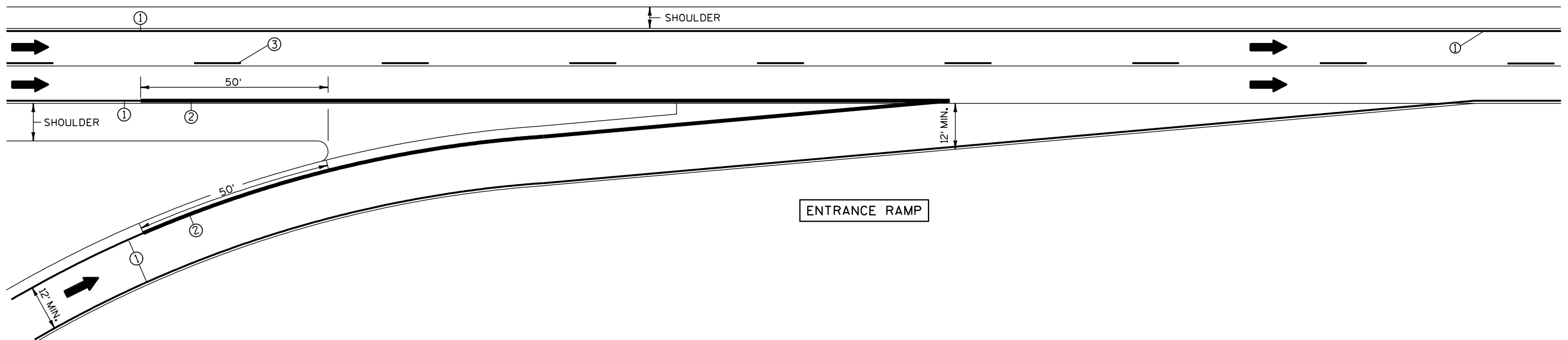


- ① EDGE MARKING - SOLID 4" YELLOW ON LEFT SIDE
- SOLID 4" WHITE ON RIGHT SIDE
- ② CHANNELIZING - SOLID 8" WHITE WET REFLECTIVE TAPE
- ③ LANE MARKING - SOLID 4" WHITE WET REFLECTIVE TAPE
- ④ CHEVRON MARKING - 24" WHITE IN GORE AREA WHEN SPECIFIED IN THE CONTRACT *
- ⑤ DOTTED EDGE EXTENSION MARKING - 4" WHITE (3' LINE, 9' GAP). SEE NOTE 3.

NOTES:

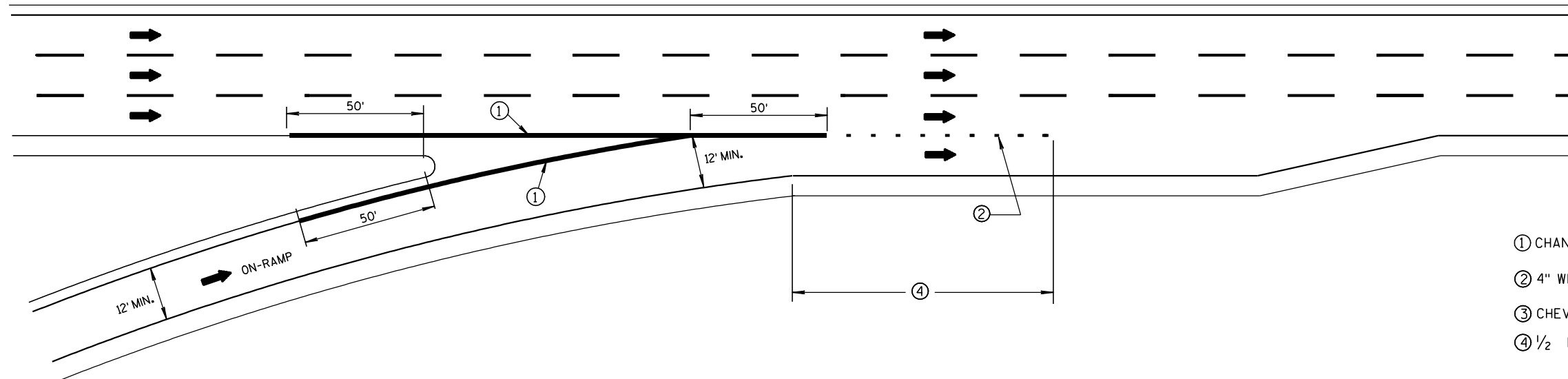
- 1. ARROWS SHOWN ON THIS MARKING PLAN DESIGNATE TRAFFIC FLOW, AND SHALL NOT BE TAKEN AS PROPOSED PAVEMENT MARKINGS.
- 2. PLACE WHITE EDGE OF TAPE 6" LEFT FROM JOINT.
- 3. 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- 4. RETRACE EXISTING DIAGONAL MARKINGS.

* REFER TO DESIGN NOTES.



PAVEMENT MARKING
(RAMPS AND GORES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

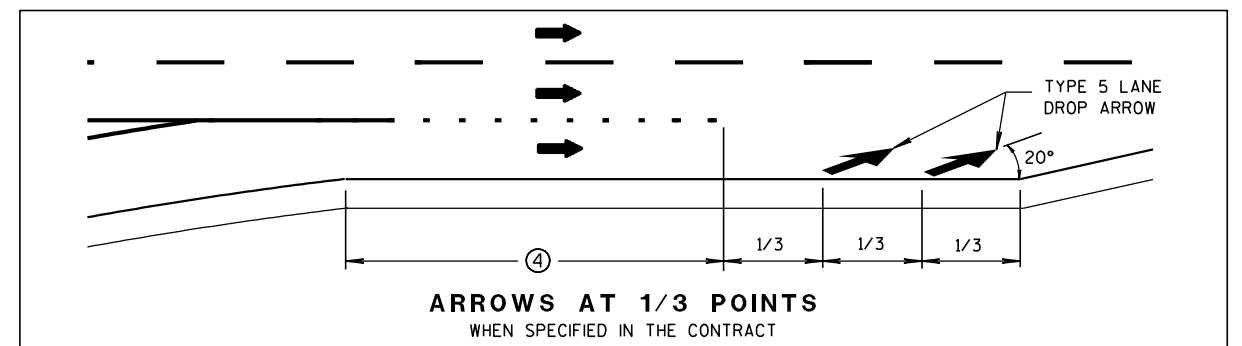
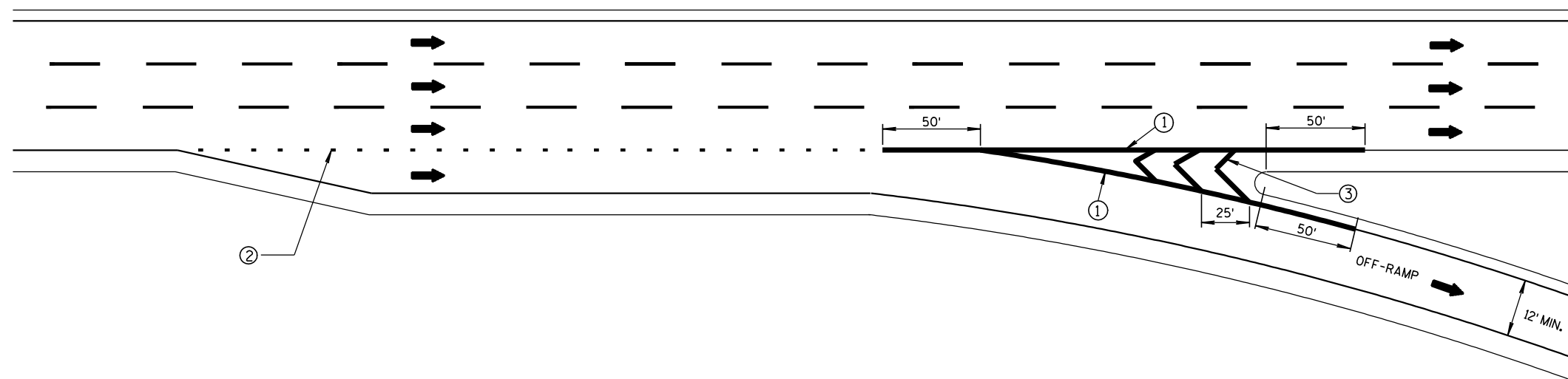


SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL ENTRANCE-RAMP

NOTES:

1. AHEAD ARROWS SHOWN ON THIS MARKING PLAN DESIGNATE TRAFFIC FLOW, AND SHALL NOT BE TAKEN AS PROPOSED PAVEMENT MARKINGS.
2. PLACE WHITE EDGE OF TAPE 6" LEFT FROM JOINT.
3. RETRACE EXISTING DIAGONAL MARKINGS.

- ① CHANNELIZING - SOLID 8" WHITE WET REFLECTIVE TAPE IN GORE AREA.
- ② 4" WHITE (3' LINE, 9' GAP).
- ③ CHEVRON MARKING - 24" WHITE WHEN SPECIFIED IN THE CONTRACT.
- ④ ½ LENGTH OF FULL WIDTH ACCELERATION LANE.

ARROWS AT 1/3 POINTS
WHEN SPECIFIED IN THE CONTRACT

SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL EXIT-RAMP

PAVEMENT MARKING FOR
PARALLEL ON-RAMP AND
PARALLEL OFF-RAMPSTATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

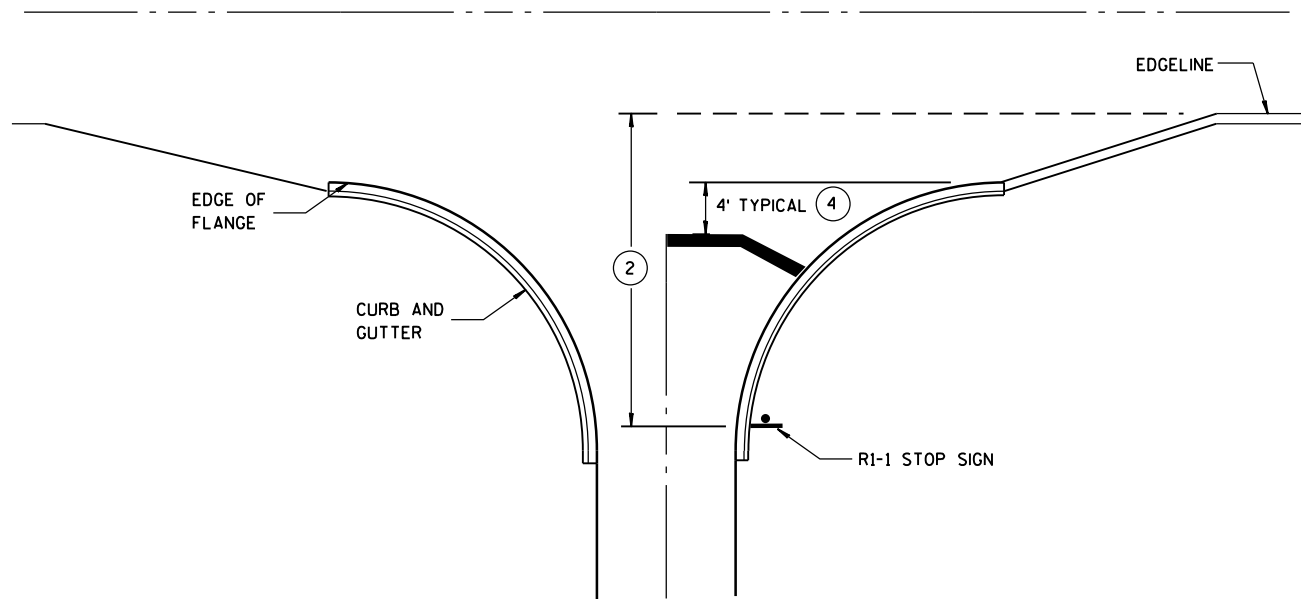
APPROVED

6/23/2011

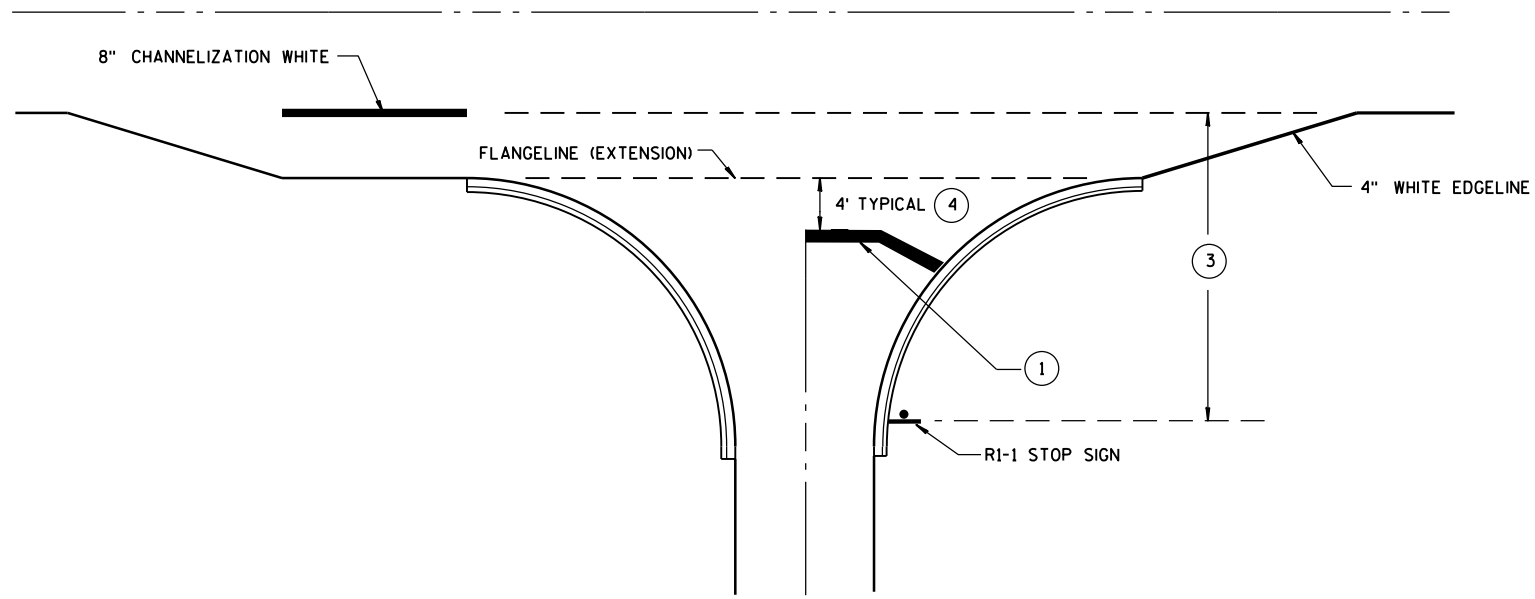
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FHWA

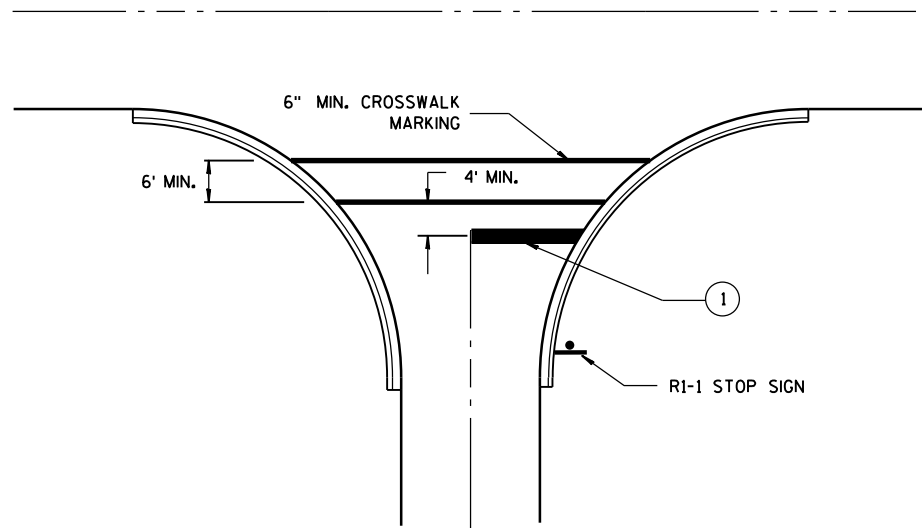
/S/ Thomas N. Notbohm
STATE TRAFFIC ENGINEER OF DESIGN



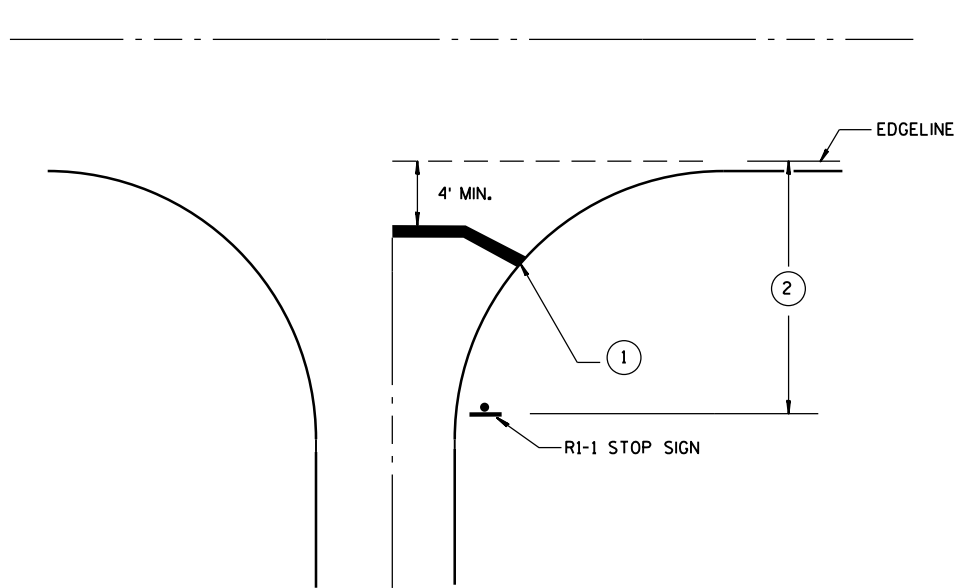
TYPICAL STOP LINE PAVEMENT MARKING
WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING
WITHOUT CURB AND GUTTER

GENERAL NOTES

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGE LINE THAN NO STOP LINE IS REQUIRED.
- ③ IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- ④ MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES.

STOP LINE AND CROSSWALK
PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4/30/2013 DATE /S/ Travis Feltz
STATE TRAFFIC ENGINEER
FHWA



Wisconsin Department of Transportation

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through innovation and exceptional service.

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