PLAN

MARSH AREA

WOODED OR SHRUB AREA

EAU JANUARY 2014 ORDER OF SHEETS PROJECT Section No. 2 Section No. 3 ₽ 8210-07-Section No. 9 Cross Sections

Section No. 1 Title

Typical Sections and Details Estimate of Quantities

Miscellaneous Quantities

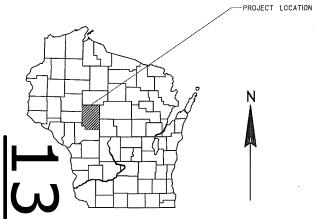
Section No. 4 Right of Way Plat

Section No. 5 Plan and Profile

Section No. 6 Standard Detail Drawings

Section No. 7 Sign Plates Section No. 8 Structure Plans Section No. 9 Computer Earthwork Data

TOTAL SHEETS = 82



DESIGN DESIGNATION

A.A.D.T. = 6400 A.A.D.T. = 8100 D.H.V. D.D. = 60/40

= 10.0 DESIGN SPEED = 25 & 35 MPH = 1,715,500

CONVENTIONAL SYMBOLS

CORPORATE LIMITS PROPERTY LINE LOT LINE LIMITED HIGHWAY EASEMENT EXISTING RIGHT OF WAY PROPOSED OR NEW R/W LINE SLOPE INTERCEPT REFERENCE LINE EXISTING CULVERT PROPOSED CULVERT (Box or Pipe) COMBUSTIBLE FLUIDS

PROFILE GRADE LINE ORIGINAL GROUND MARSH OR ROCK PROFILE (To be noted as such) SPECIAL DITCH

GRADE ELEVATION CULVERT (Profile View) UTILITIES ELECTRIC FIBER OPTIC

SANITARY SEWER STORM SEWER TELEPHONE WATER UTILITY PEDESTAL POWER POLE

TELEPHONE POLE

BEGIN PROJECT

STA 1+00 X=632181.788 Y=493505.826

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

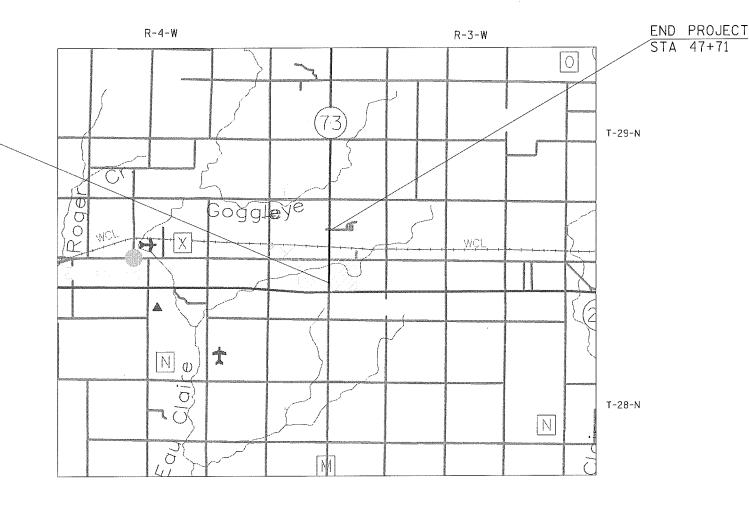
C THORP, WASHINGTON STREET

KRYCH STREET - NYE STREET

S.T.H. 73

CLARK

STATE PROJECT NUMBER 8210-07-73



LAYOUT

TOTAL NET LENGTH OF CENTERLINE = 0.885 MI.

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT

CONTRACT

PROJECT

WISC 2014005

STATE PROJECT

8210-07-73

PREPARED BY N₩ REGION - EAU CLAIRE Surveyor KOU THAO Designer CHAD HINES Project Manager DAN OJIBWAY Regional Examiner RICHARD SHERMO Regional Supervisor. C.O. Examiner

DATE: 7/30/13 Ruc S

PLOT DATE: 9/9/2008 8:17 AM

PLOT NAME : _____

FILE NAME: N:\PDS\C3D\82100730\DESIGN\PLANSPRODUCTION\010102_TI_STH73_82100730.DWG

PLOT BY : THAO, KOU

WIEDOT 40 - DDE - CUEET - 10

LIST OF STANDARD ABBREVIATIONS

ABUTMENT AGGREGATE AGG. AHEAD AH. APPROX. APPROXIMATE APRON ENDWALL AFW ASPH. **ASPHALTIC** AVERAGE DAILY TRAFFIC A.D.T.

AZIMUTH BACK BK. REGIN BEG. BENCH MARK B.M. CENTER LINE C/L CONC CONCRETE CONST. CONSTRUCTION ČÕ.

COUNTY
COUNTY TRUNK HIGHWAY
CROSS SECTION X-SEC. CR. CFS CRUSHED CUBIC FEET/SECOND CUBIC YARD

Č.Y., CU. YD. CULV. CULVERT CULVERT PIPE

D.O.T. DEPARTMENT OF TRANSPORTATION DESIGN HOUR VOLUME D.H.V. DIA. DIRECTIONAL DISTRIBUTION

DISCHARGE EACH DISCH. OR DIS. ELECTRIC FLECT. EL. OR ELEV. ELEVATION **EMBANKMENT**

EXCAVATION BELOW SUBGRADE E.B.S. EXISTING

EXIST FERT. F.E. FERTILIZE FIELD ENTRANCE FIN. FINISHED FOOT FLOW LINE F.L. GA. HORIZ. GAUGE HORIZONTAL HUNDREDWEIGHT CWT.

INL. L.H.F. LEFT-HAND FORWARD

LINEAR LINEAR FOOT LUMP SUM LIN. LIN. FT. MAX. MAXIMUM MISCELLANEOUS MISC. NORTH EAST NORTH WEST N.E. N.W. PAV'T

PAVEMENT
POINT OF CURVATURE
POINT OF INTERSECTION
POINT OF TANGENCY P.O.T. POINT ON TANGENT LB. PF POUND PRIVATE ENTRANCE PROJ. **PROJECT**

RANGE REQ'D REQUIRED RIGHT RHF RIGHT-HAND FORWARD

R/W RD. RIGHT OF WAY ROAD SHR. SHRINKAGE SLOPE STANDARD

S.D.D. STANDARD DETAIL DRAWINGS S.T.H STATE TRUNK HIGHWAY STA S.P.P.A STRUCTURAL PLATE PIPE ARCH

STRUCT STRUCTURE SURF. SURFACE TEL. TELEPHONE

TN. TOWN TRUCKS (PERCENT OF) UNCL.

U.G. UNDERGROUND VELOCITY OR DESIGN SPEED

V.C. VERTICAL CURVE

GENERAL NOTES

WHEN THE QUANTITY OF THE ITEMS OF SUBBASE OR SURFACE COURSE IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF MATERIAL AS DIRECTED BY THE ENGINEER.

DISTURBED AREAS WITHIN THE RIGHT OF WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, ARE TO BE TOPSOILED, FERTILIZED, SEEDED, AND E-MAT AS DIRECTED BY THE ENGINEER.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

THE QUANTITY OF THE ITEMS FOR EROSION PROTECTION INCLUDES UNDISTRIBUTED AMOUNTS. THE DISTRUBUTION AND LOCATION OF THESE MATERIALS ARE TO BE DETERMINED BY THE ENGINEER.

THE LOCATION OF EXISTING UTILITY LOCATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE SOME OTHER UTILITY INSTALLATIONS WITHIN THE AREA THAT ARE NOT SHOWN. IT IS THE CONCRACTOR'S RESPONSIBILITY TO CALL DIGGERS HOTLINE PRIOR TO BEGINNING WORK OPERATIONS. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMEBER OF DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

THE ITEM OF "ASPHALTIC SURFACE" SHALL BE USE FOR SPOT REPAIRS AND MINOR REPAIRS AFTER SURFACE MILL; INCLUDING BUT NOT LIMITED TO, FILLING POT HOLES, POP OUTS THAT OCCUR UNDER TRAFFIC, OR RAMPING AT JOINTS OR BETWEEN UNEVEN SURFACES.



1-800-242-8511 HEARING IMPAIRED TDD 1-800-542-2289 CALL 3 WORK DAYS BEFORE YOU DIG WWW.DIGGERSHOTLINE.COM

DEPARTMENT OF NATURAL RESOURCES

WEST CENTRAL REGION HEADQUARTERS 1300 WEST CLAIREMONT AVENUE EAU CLAIRE, WI 54701 TELEPHONE: 715-839-1609 ATTENTION: NICK SCHAFF

UTILITIES

CENTURYLINK-COMMUNICATION LINE 20 S WILSON AVE RICE LAKE, WI 54868 TELEPHONE: 715-234-5528 ATTENTION: JAMES A. ARQUETTE

THORP MUNICIPLE WATER & SEWER UTILITY P.O. BOX 334 THORP, WI 54771-0334 TELEPHONE: 715-669-5808 ATTENTION: TIMOTHY J. MCCREDDEN

WE ENERGIES-GAS/PETROLEUM 333 W EVERETT ST, A299 MILWAUKEE, WI 53203 TELEPHONE: 414-221-4578 ATTENTION: THOMAS J. KROSTAY

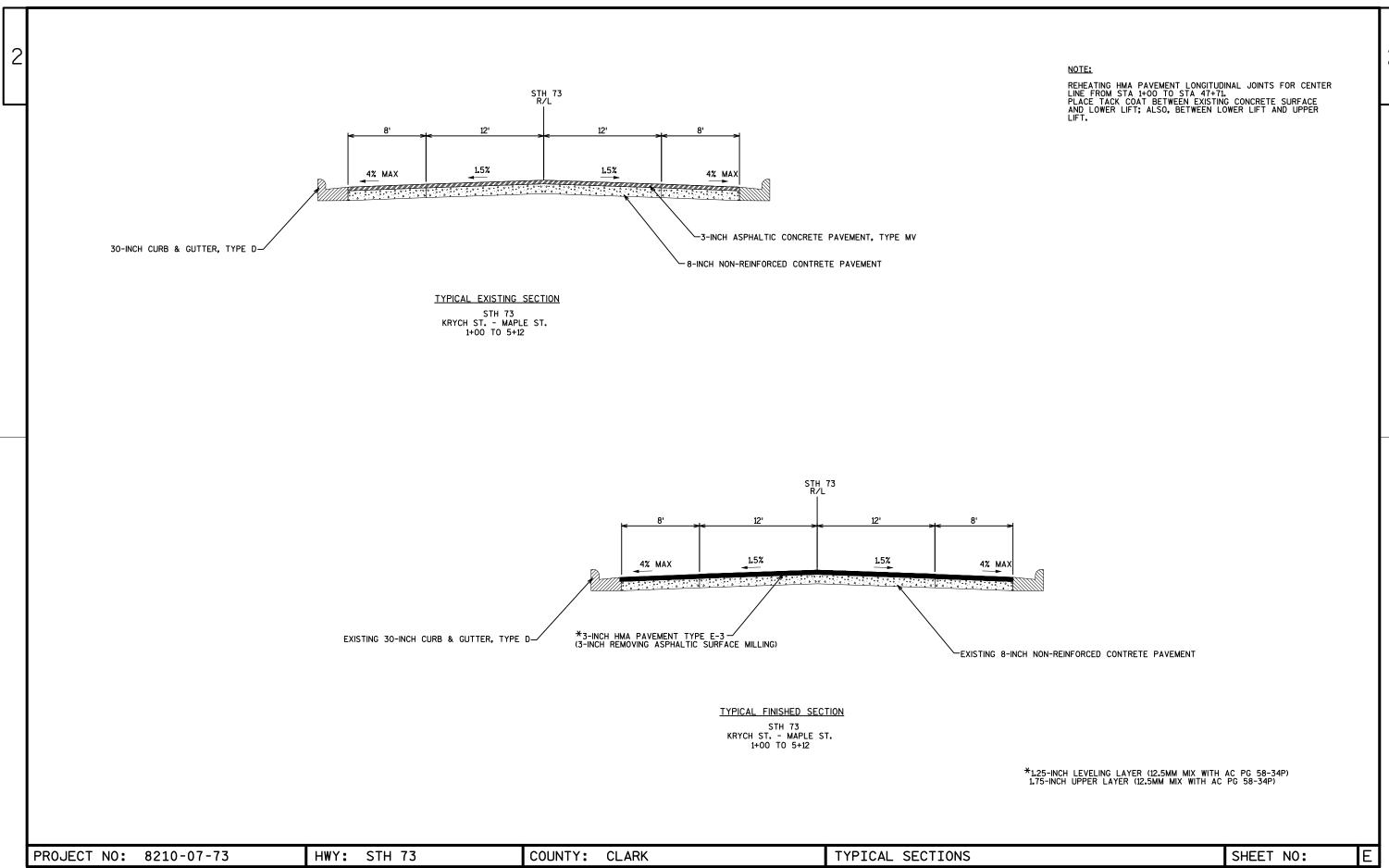
XCEL ENERGY-DISTRIBUTION 1414 W HAMILTON AVE P.O. BOX 8 EAU CLAIRE, WI 54702-0008 TELEPHONE: 715-236-5721 ATTENTION: STACIE HAUGEN

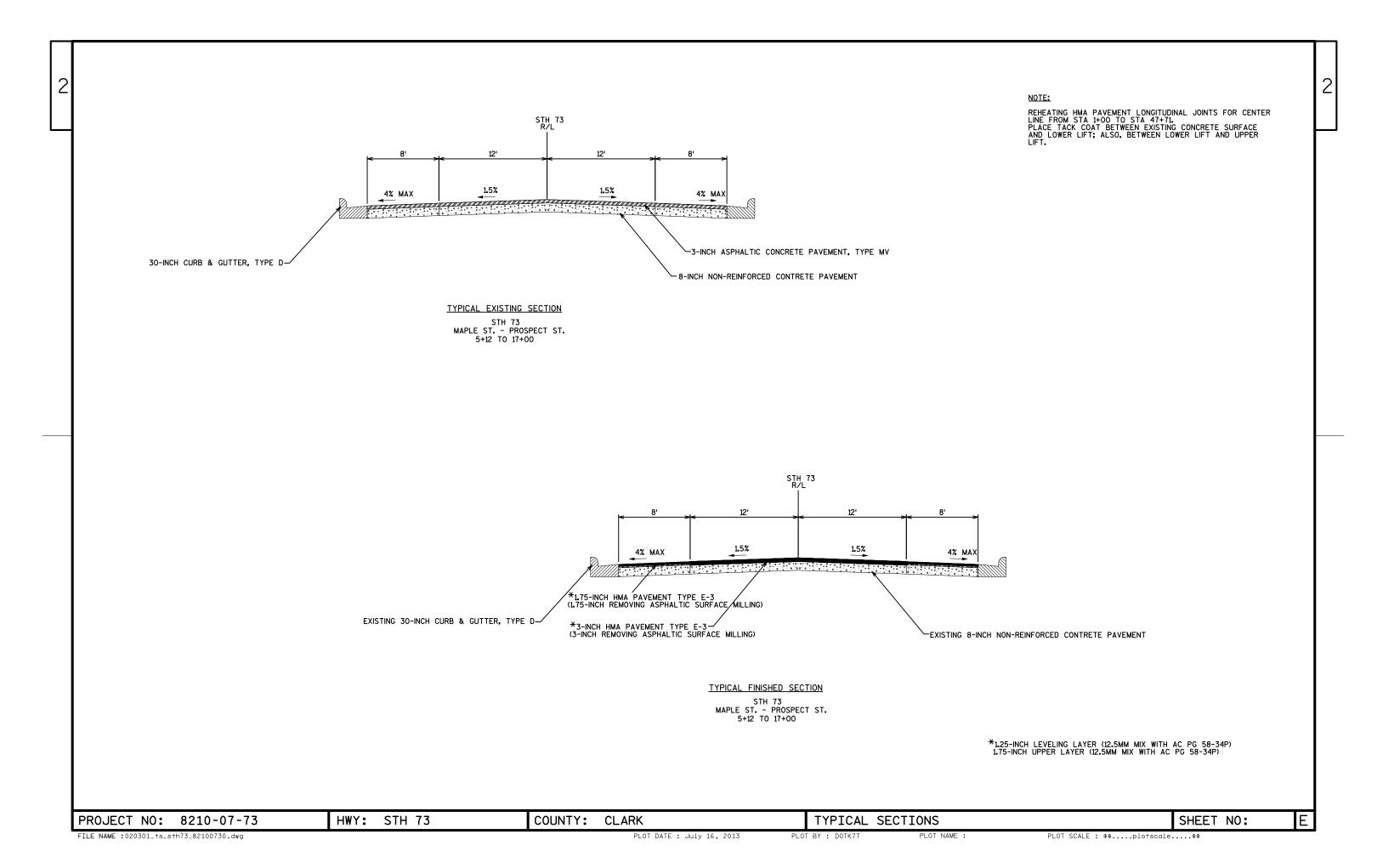
RAILROAD

WISCONSIN CENTRAL LTD (CN) 1625 DEPOT STREET STEVENS POINT, WI 54481 TELEPHONE: 715-345-2503 FAX: 715-345-2507 ATTENTION: JACKIE MACEWICZ EMAIL: JACKIE.MACEWICZ@CN.CA

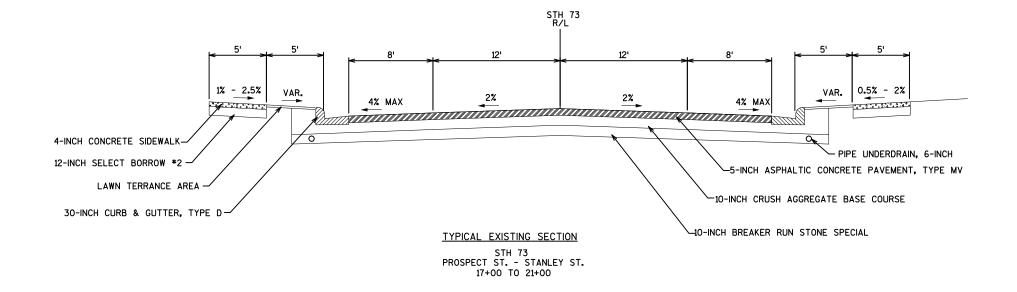
SHEET PROJECT NO: 8210-07-73 HWY: STH 73 COUNTY: CLARK GENERAL NOTES

PLOT SCALE: \$\$.....plotscale.....\$\$ FILE NAME: \$\$....designfile....\$\$ PLOT DATE: OCTOBER 18, 2012 PLOT BY : DOTK7T PLOT NAME WISDOT/CADDS SHEET 44



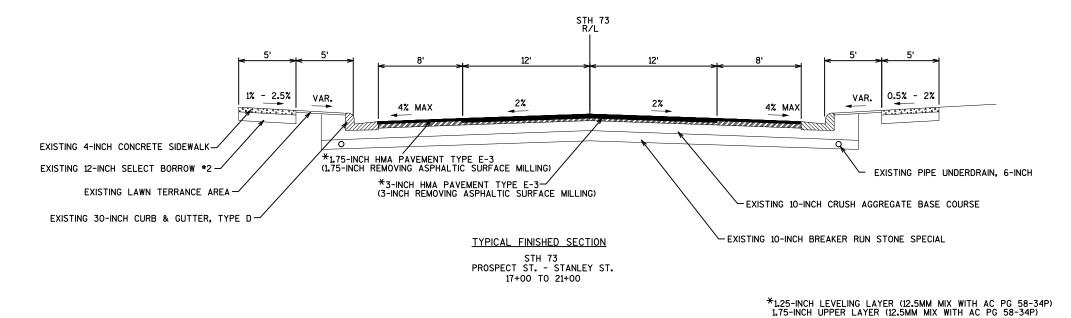






NOTE:

REHEATING HMA PAVEMENT LONGITUDINAL JOINTS FOR CENTER LINE FROM STA 1+00 TO STA 47+71. PLACE TACK COAT BETWEEN EXISTING CONCRETE SURFACE AND LOWER LIFT; ALSO, BETWEEN LOWER LIFT AND UPPER LIFT.



TYPICAL SECTIONS

SHEET NO:

FILE NAME :020301_ts_sth73_82100730.dwg PLOT BY: DOTK7T PLOT NAME: PLOT SCALE: \$\$....plotscale....\$\$

COUNTY: CLARK

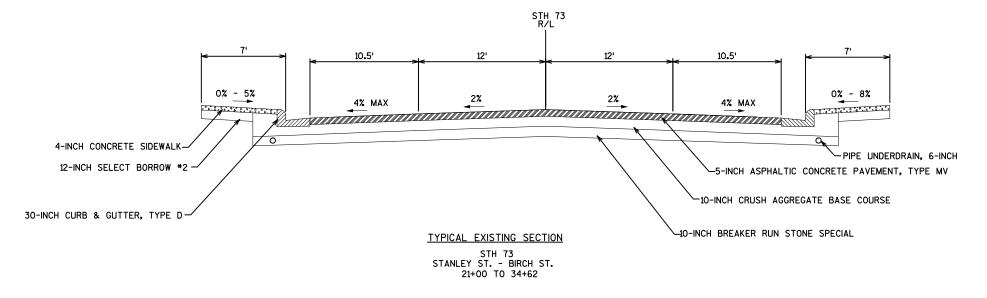
PROJECT NO: 8210-07-73

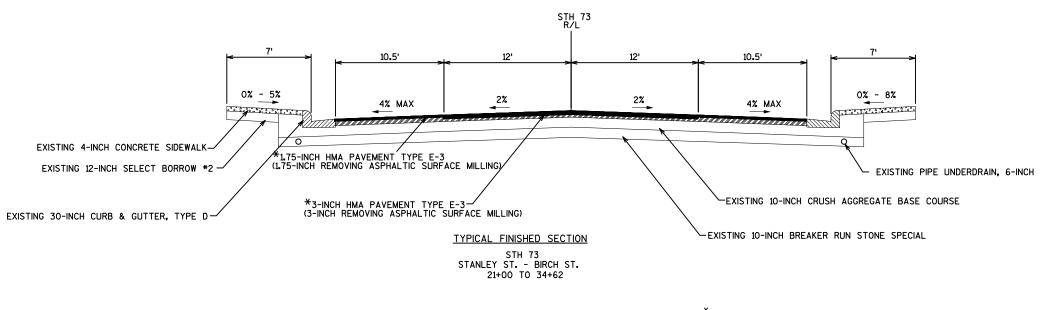
HWY: STH 73



NOTE:

REHEATING HMA PAVEMENT LONGITUDINAL JOINTS FOR CENTER LINE FROM STA 1+00 TO STA 47+71. PLACE TACK COAT BETWEEN EXISTING CONCRETE SURFACE AND LOWER LIFT; ALSO, BETWEEN LOWER LIFT AND UPPER LIFT.





*1.25-INCH LEVELING LAYER (12.5MM MIX WITH AC PG 58-34P) 1.75-INCH UPPER LAYER (12.5MM MIX WITH AC PG 58-34P)

PROJECT NO: 8210-07-73 HWY: STH 73 COUNTY: CLARK TYPICAL SECTIONS SHEET NO: E

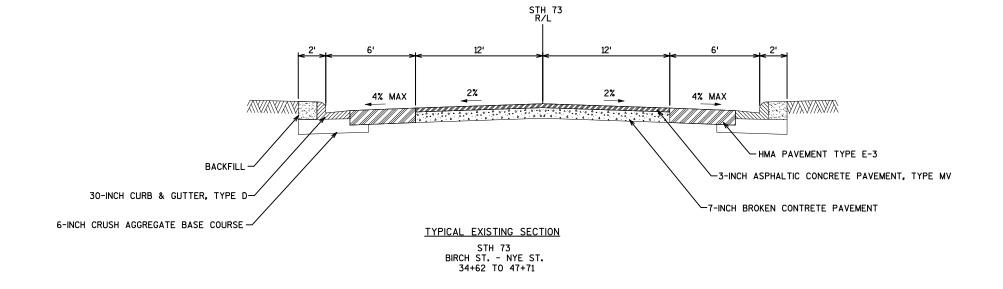
FILE NAME :020301_ts_sth73_82100730.dwg

PLOT DATE : July 16, 2013

PLOT BY : DOTK7T PLOT NAME :

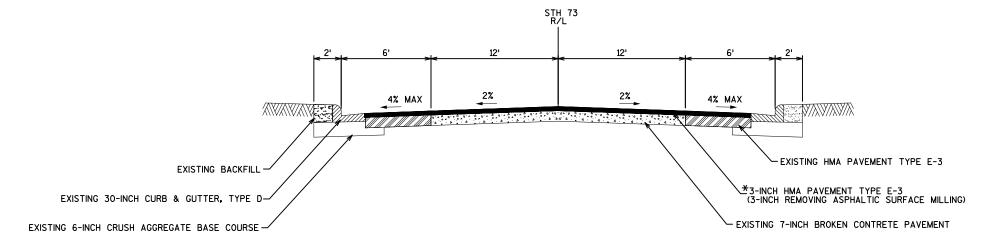
PLOT SCALE: \$\$.....plotscale.....\$\$





NOT

REHEATING HMA PAVEMENT LONGITUDINAL JOINTS FOR CENTER LINE FROM STA 1+00 TO STA 47+71. PLACE TACK COAT BETWEEN EXISTING CONCRETE SURFACE AND LOWER LIFT; ALSO, BETWEEN LOWER LIFT AND UPPER LIFT.



TYPICAL FINISHED SECTION

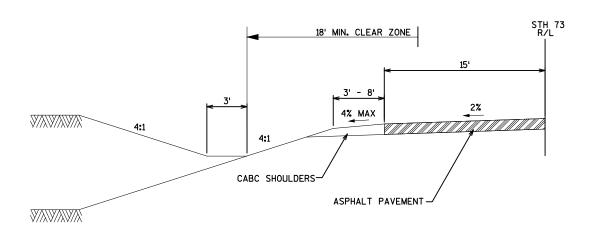
STH 73 BIRCH ST. - NYE ST. 34+62 TO 47+71

> *1.25-INCH LEVELING LAYER (12.5MM MIX WITH AC PG 58-34P) 1.75-INCH UPPER LAYER (12.5MM MIX WITH AC PG 58-34P)

PROJECT NO: 8210-07-73 HWY: STH 73 COUNTY: CLARK TYPICAL SECTIONS SHEET NO: E

FILE NAME :020301_ts_sth73_82100730.dwg PLOT BY: DOTK7T PLOT NAME: PLOT SCALE: \$\$.....plotscale.....\$\$

REHEATING HMA PAVEMENT LONGITUDINAL JOINTS FOR CENTER LINE FROM STA 1+00 TO STA 47+71.
PLACE TACK COAT BETWEEN EXISTING CONCRETE SURFACE AND LOWER LIFT; ALSO, BETWEEN LOWER LIFT AND UPPER LIFT.

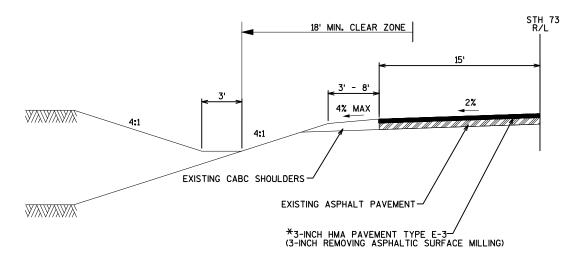


TYPICAL EXISTING HALF SECTION

STH 73 NORTHBOUND, LT

AT NYE ST.

46+00 TO 47+71



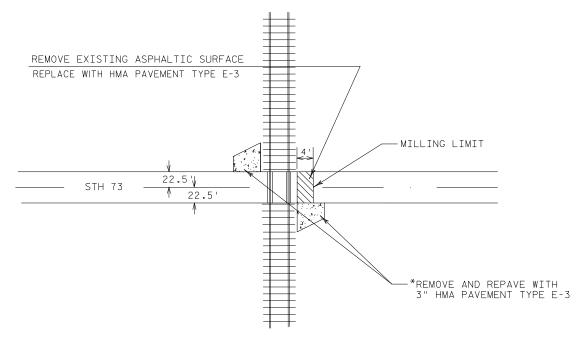
*1.25-INCH LEVELING LAYER (12.5MM MIX WITH AC PG 58-34P) 1.75-INCH UPPER LAYER (12.5MM MIX WITH AC PG 58-34P)

TYPICAL FINISHED HALF SECTION
STH 73 NORTHBOUND, LT

AT NYE ST. 46+00 TO 47+71

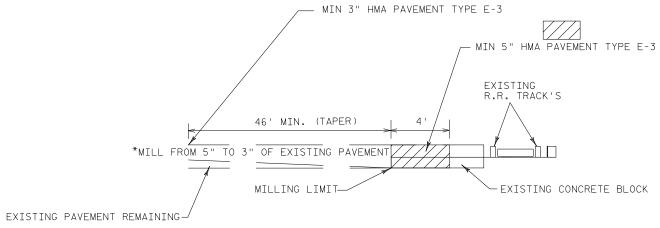
PROJECT NO: 8210-07-73 HWY: STH 73 COUNTY: CLARK TYPICAL SECTIONS SHEET NO: E

FILE NAME :020301_ts_sth73_82100730.dwg PLOT BY: DOTK7T PLOT NAME: PLOT SCALE: \$\$.....plotscale.....\$\$



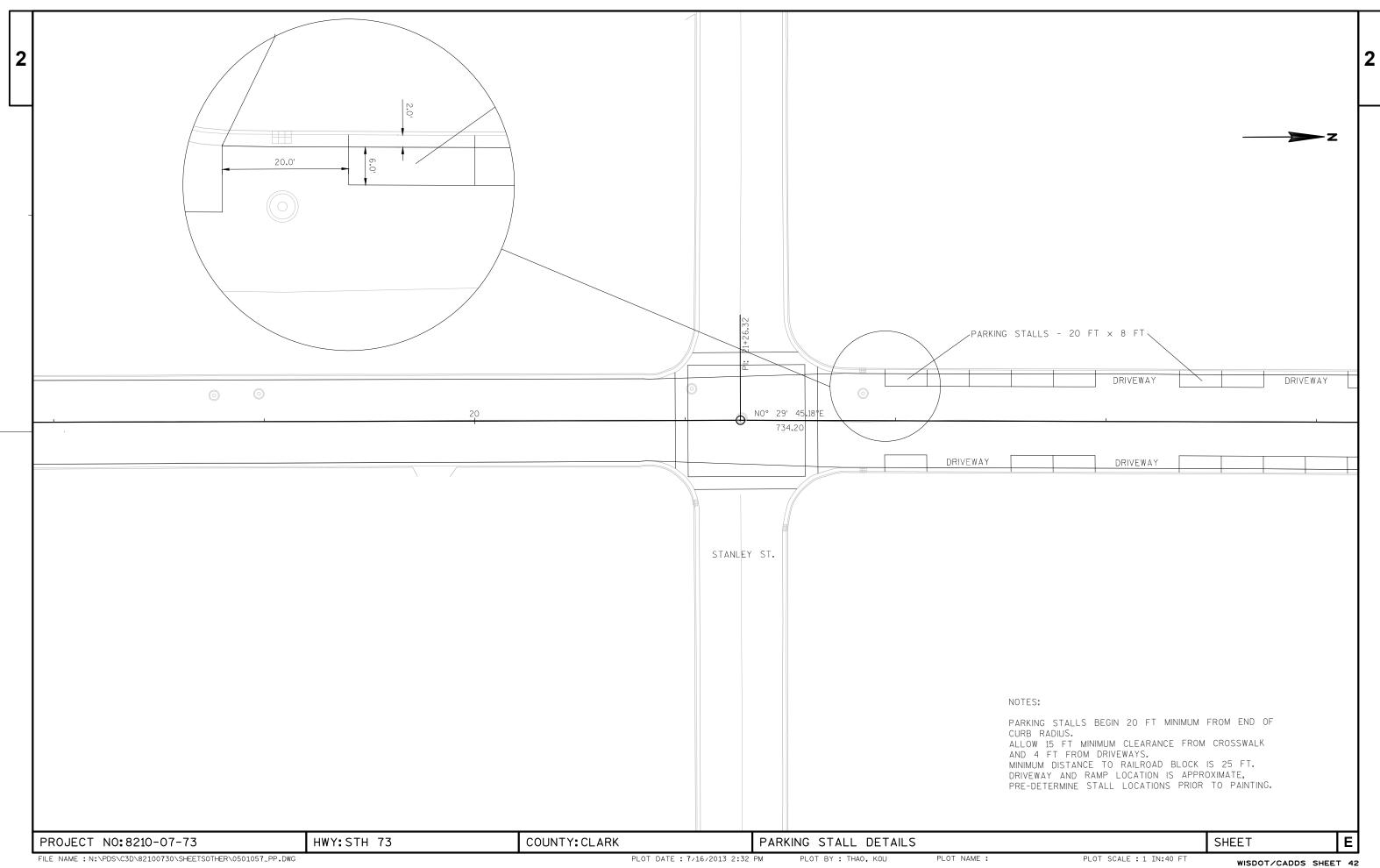
*FOR REPLACEMENT PANELS, TAPER ASPHALTIC SURFACE FROM END OF SIDEWALK TO END OF CONCRETE PANEL.

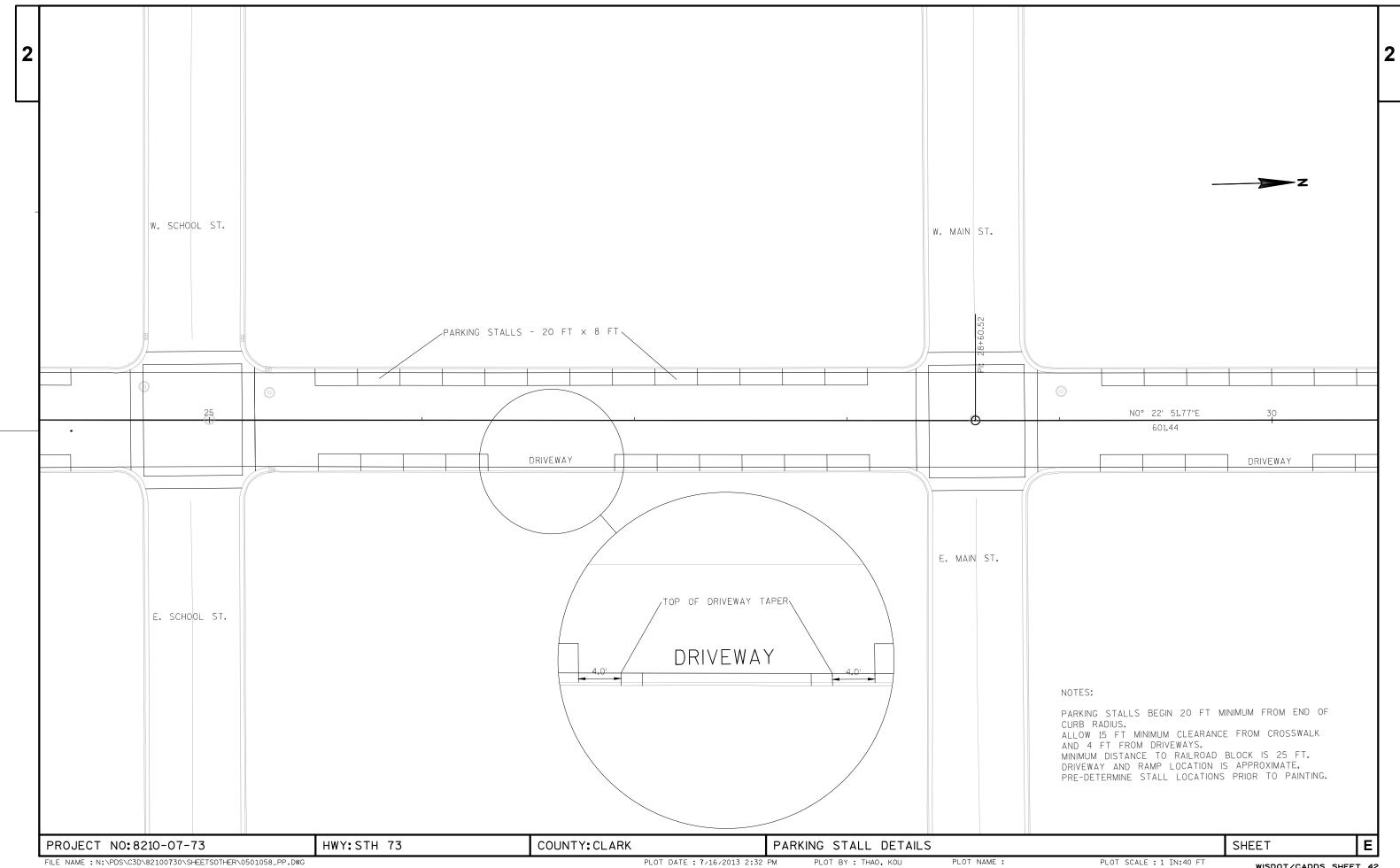
DETAIL FOR RAILROAD CROSSING ASPHALT REPLACEMENT

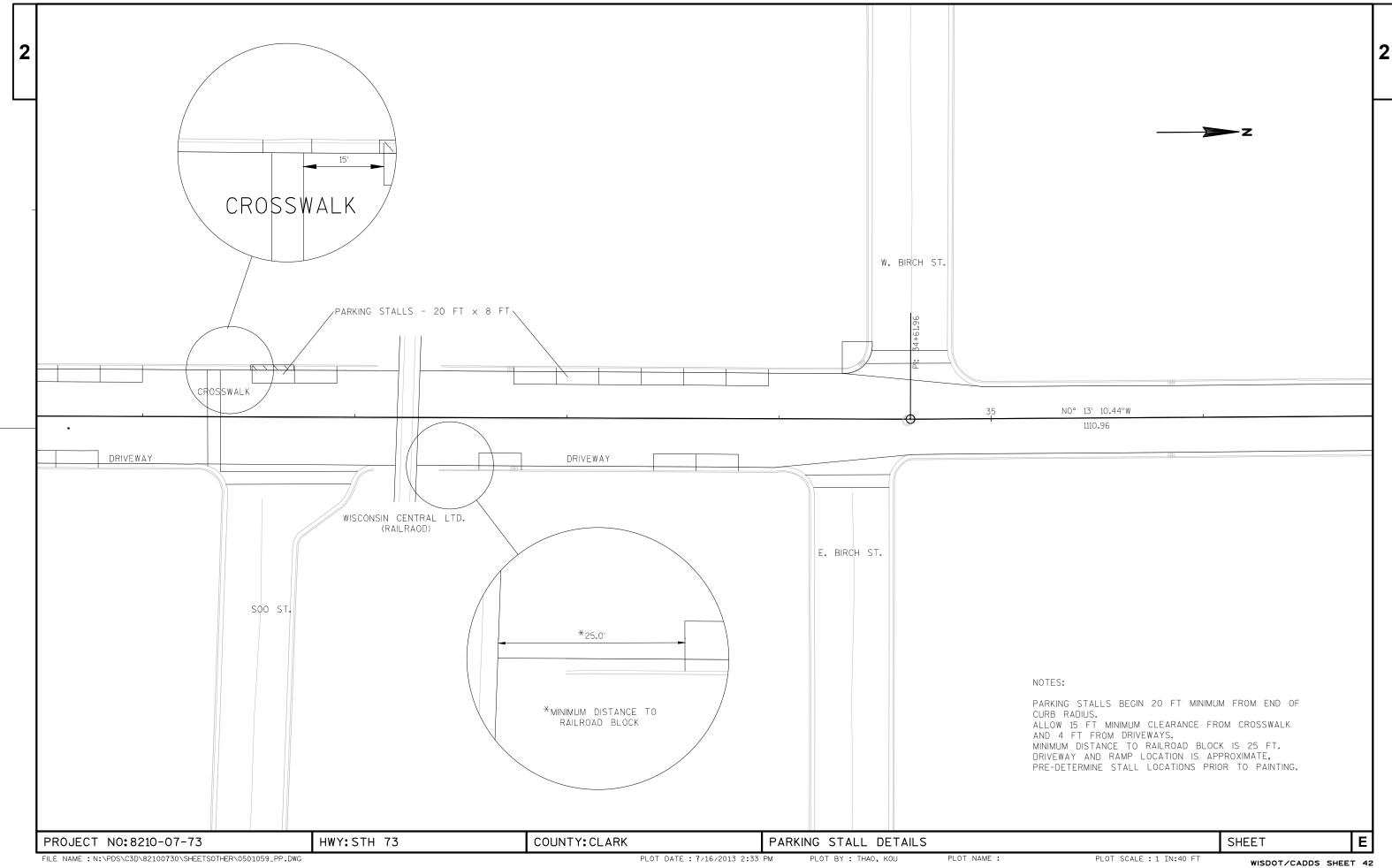


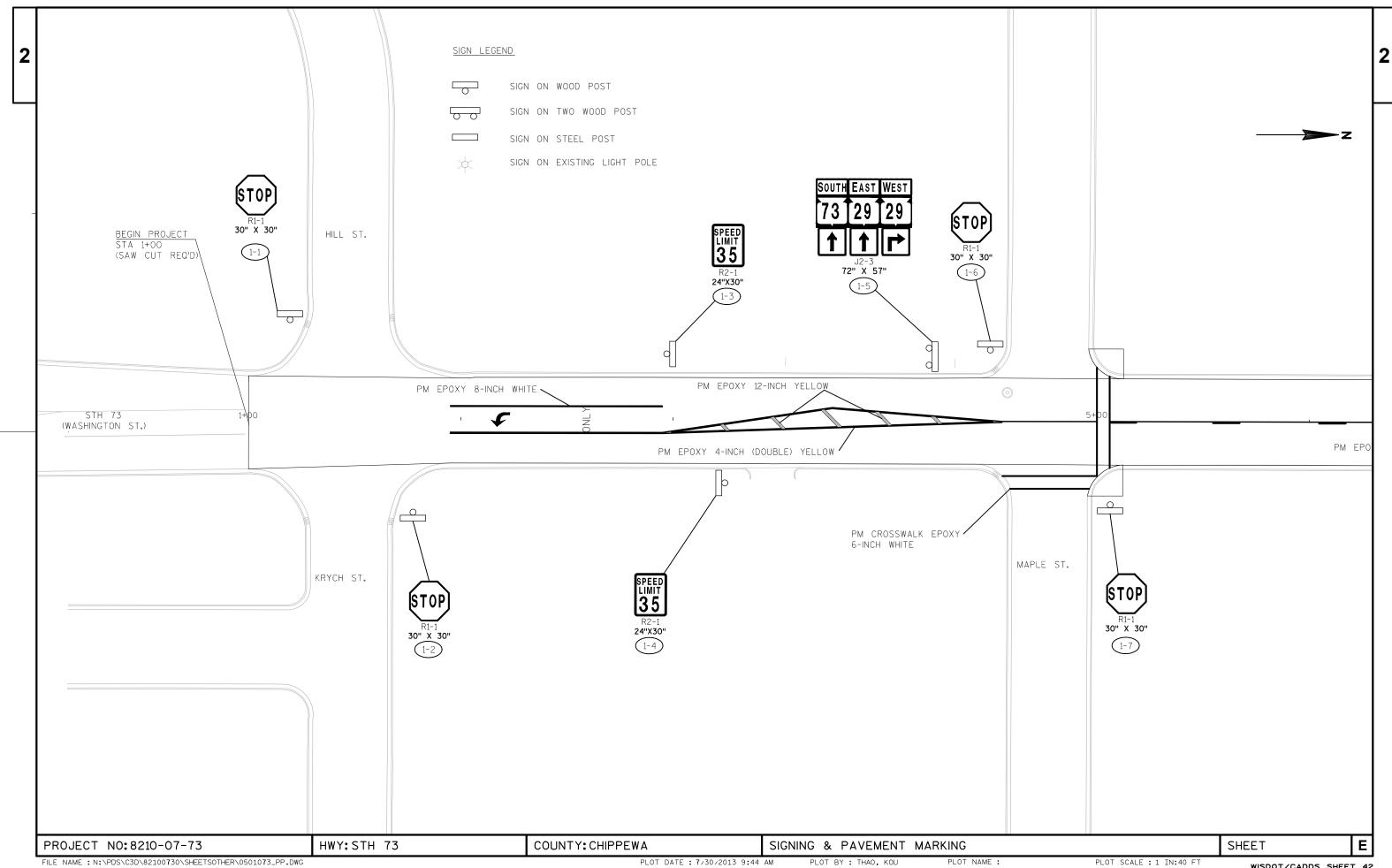
DETAIL FOR RAILROAD CROSSING BUTT JOINT

*REMOVE MATERIAL UNDER ITEM "REMOVING ASPHALT SURFACE BUTT JOINT"

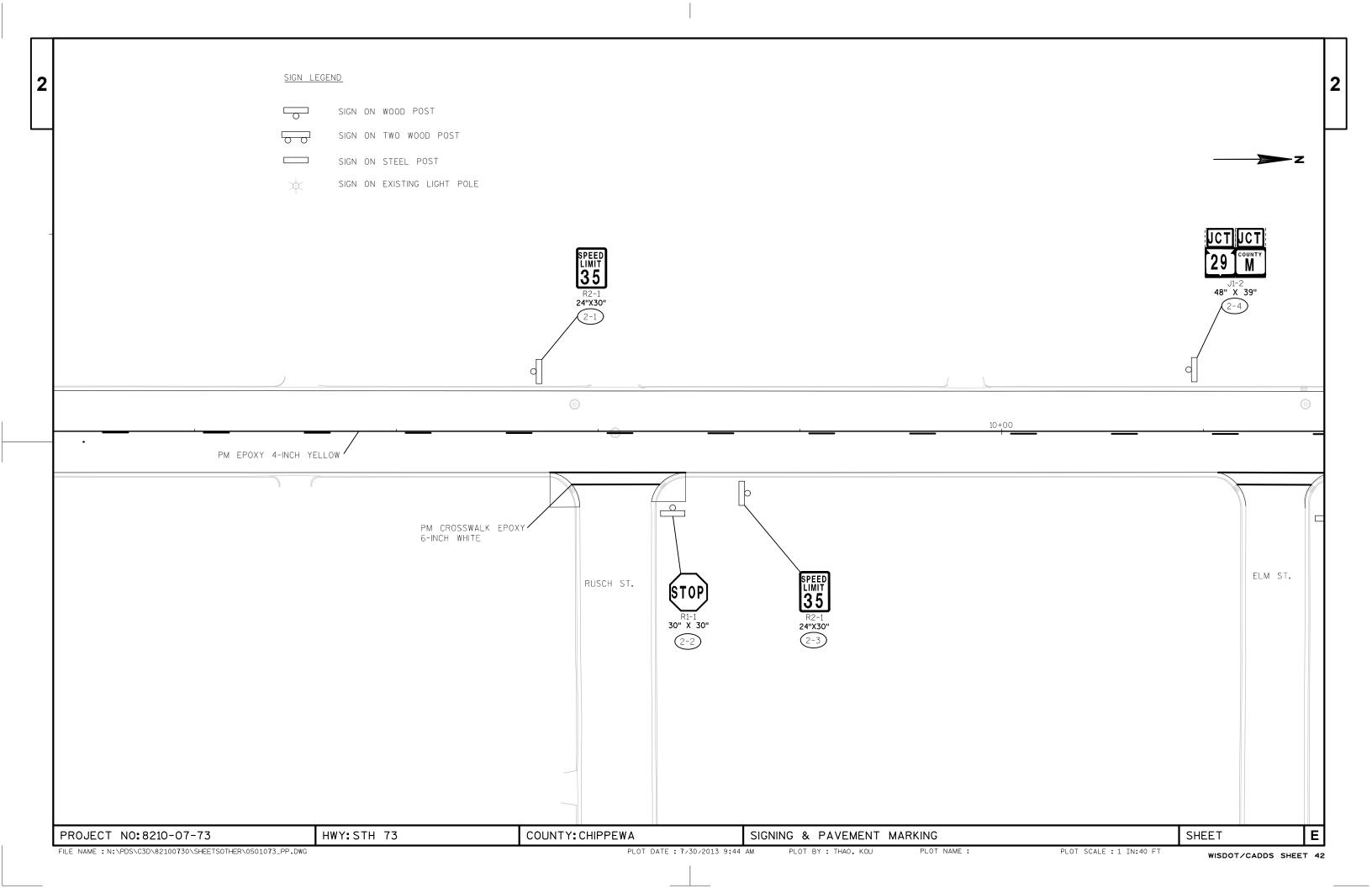


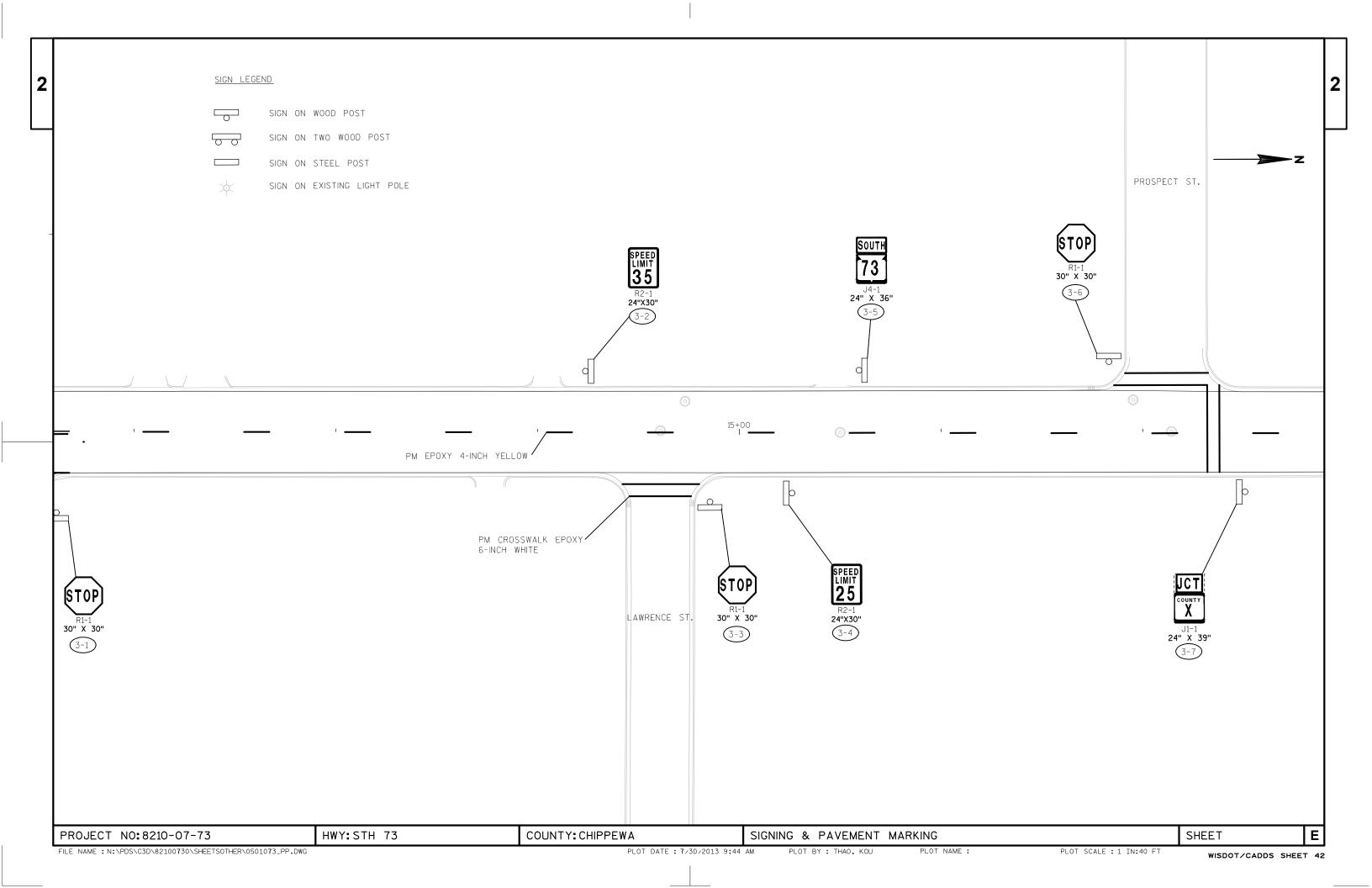


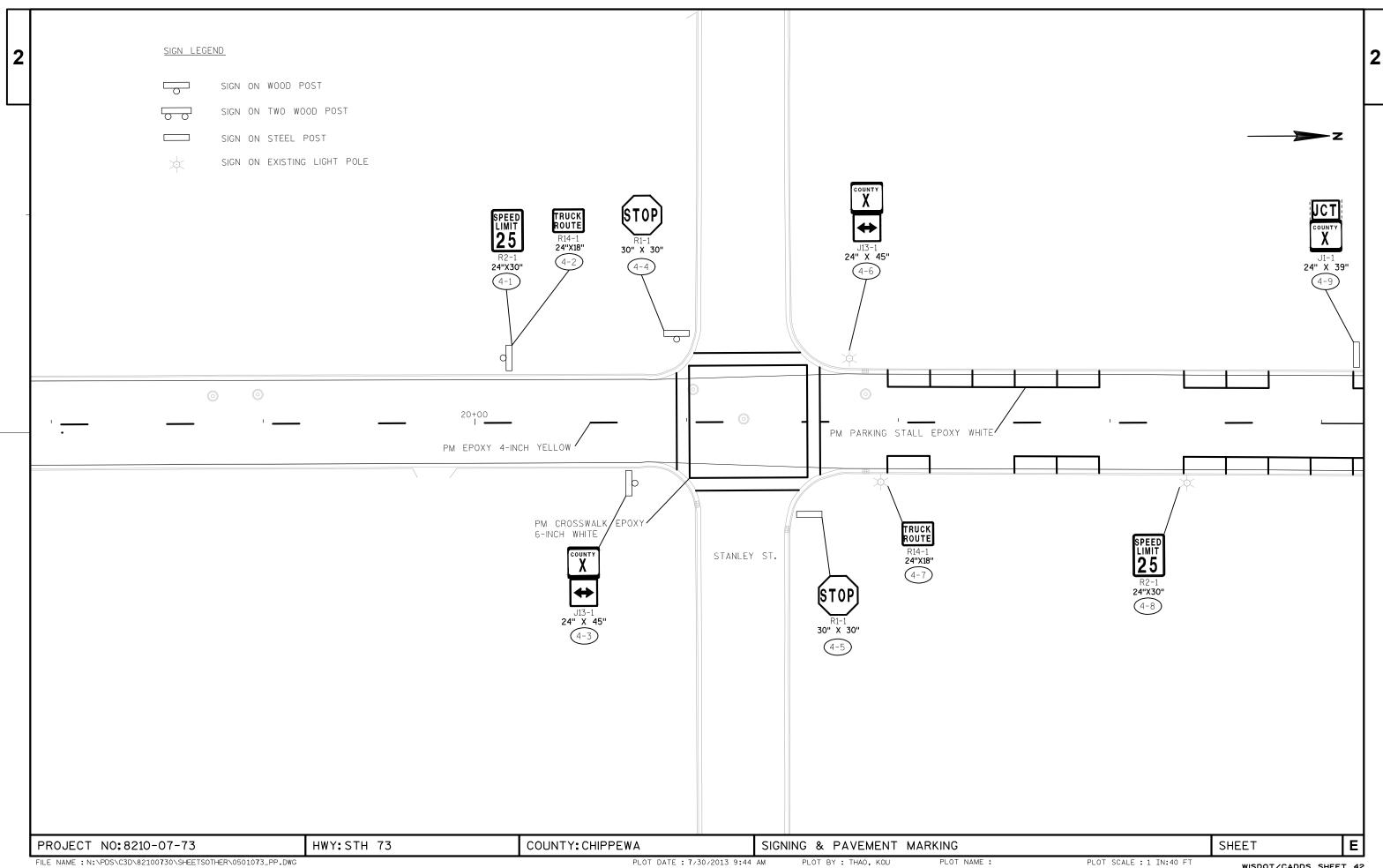


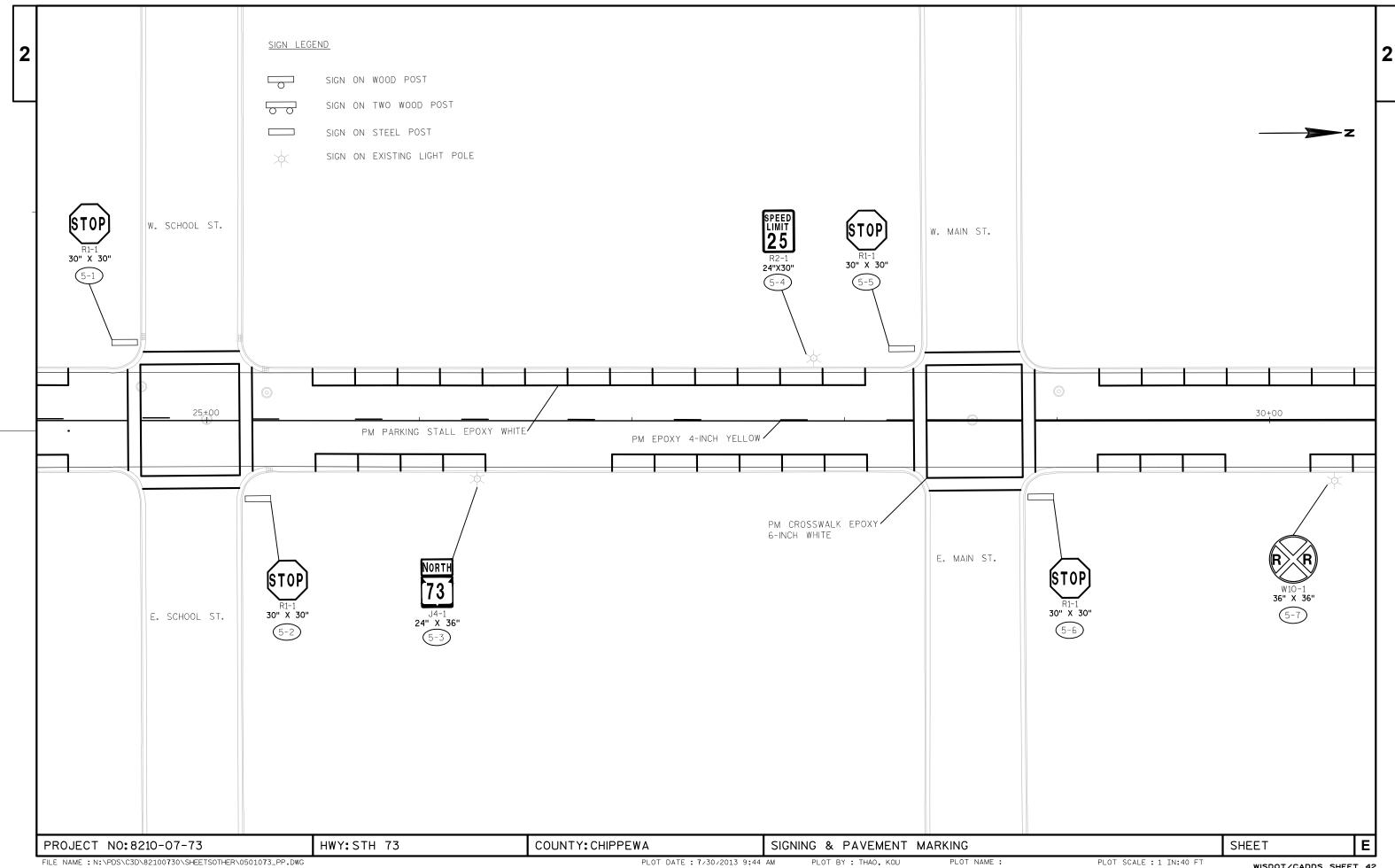


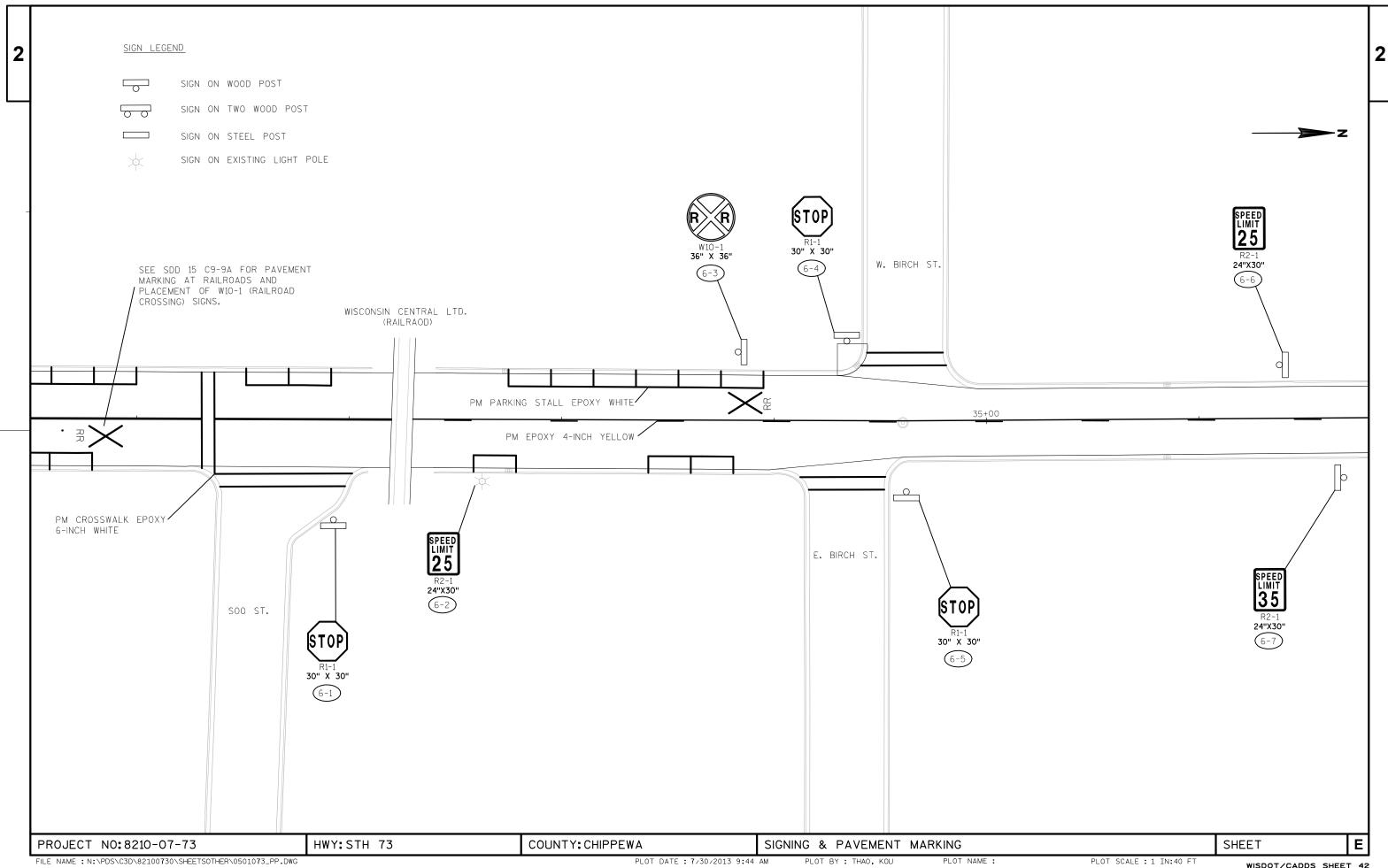
PLOT SCALE : 1 IN:40 FT

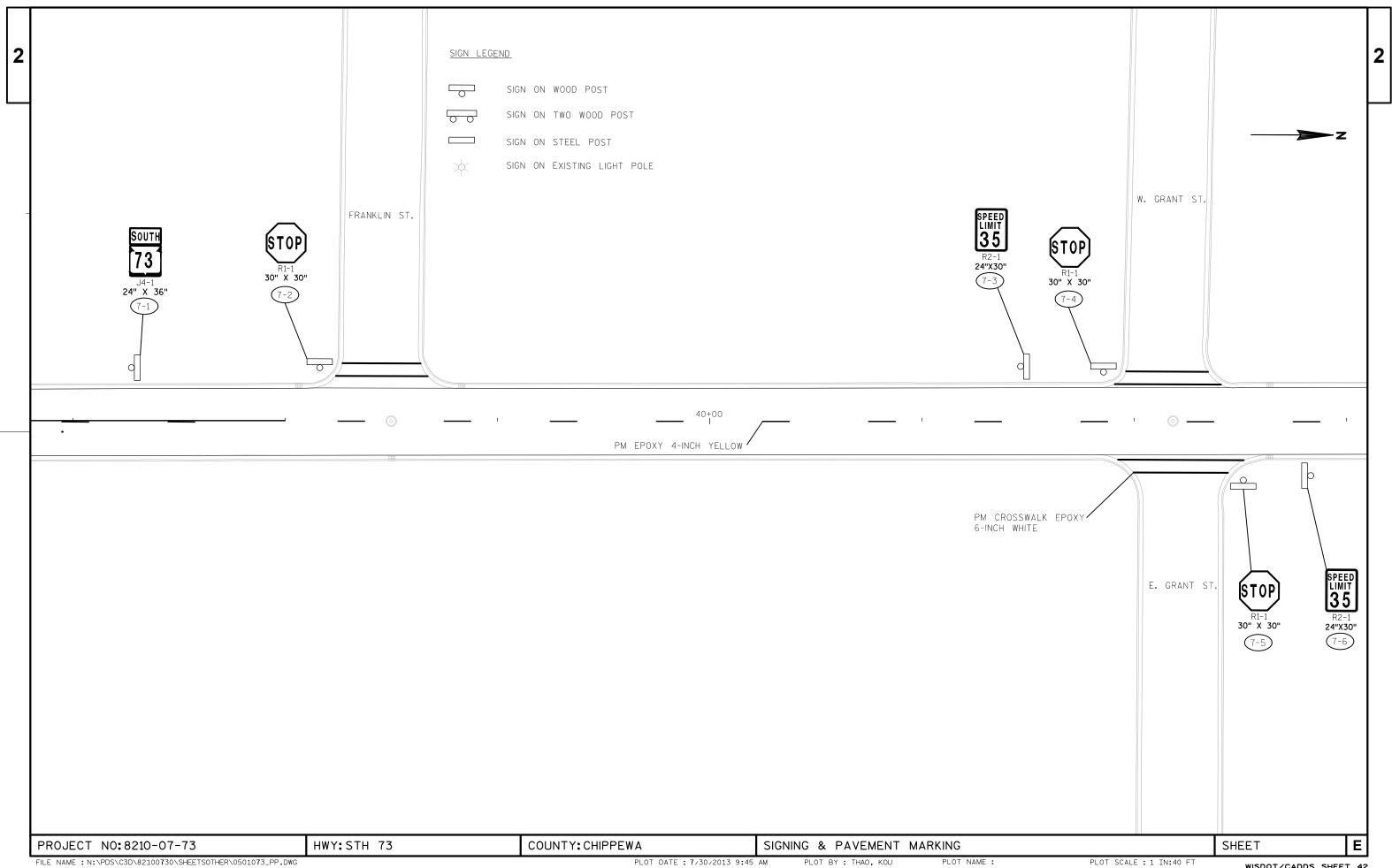


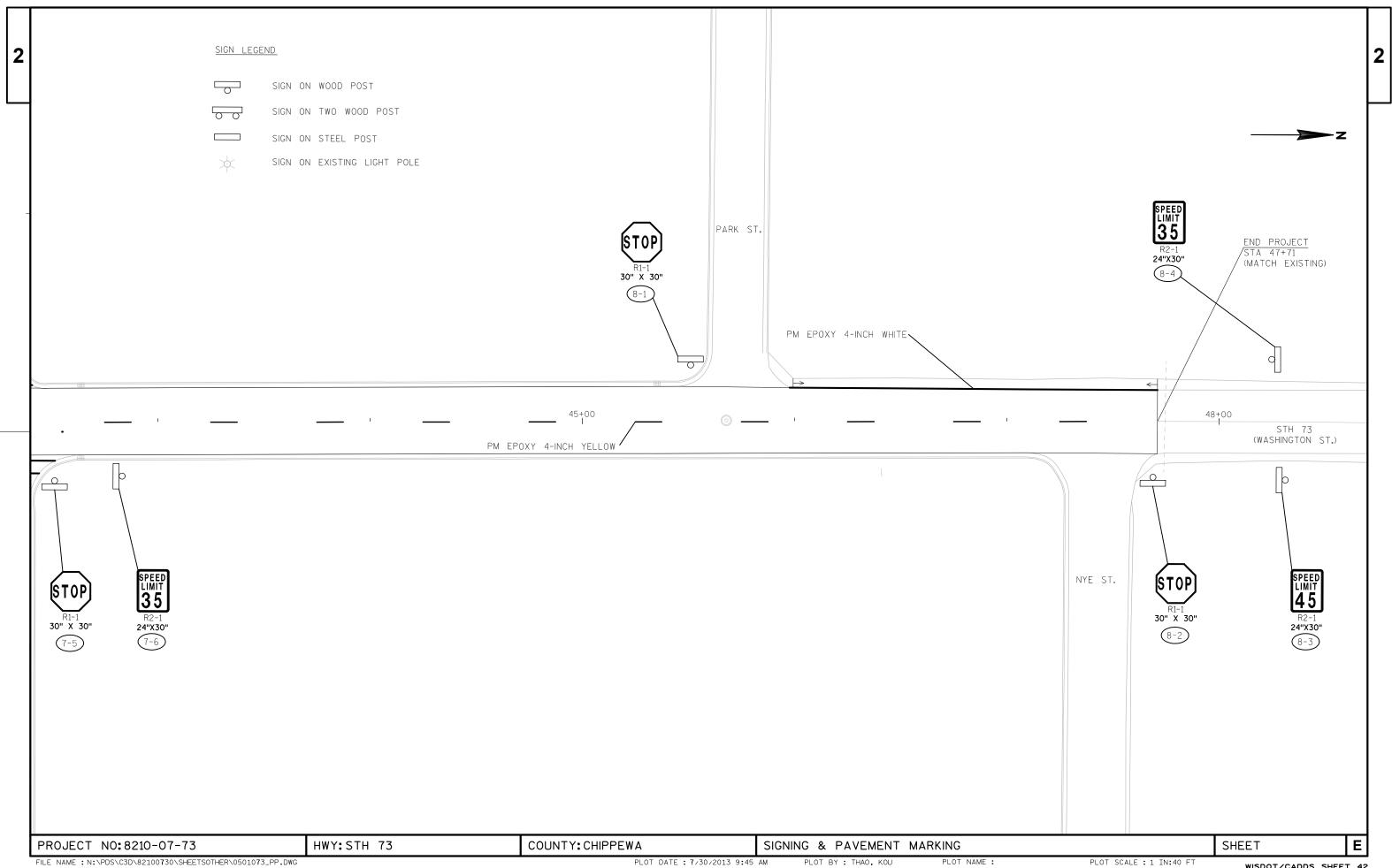


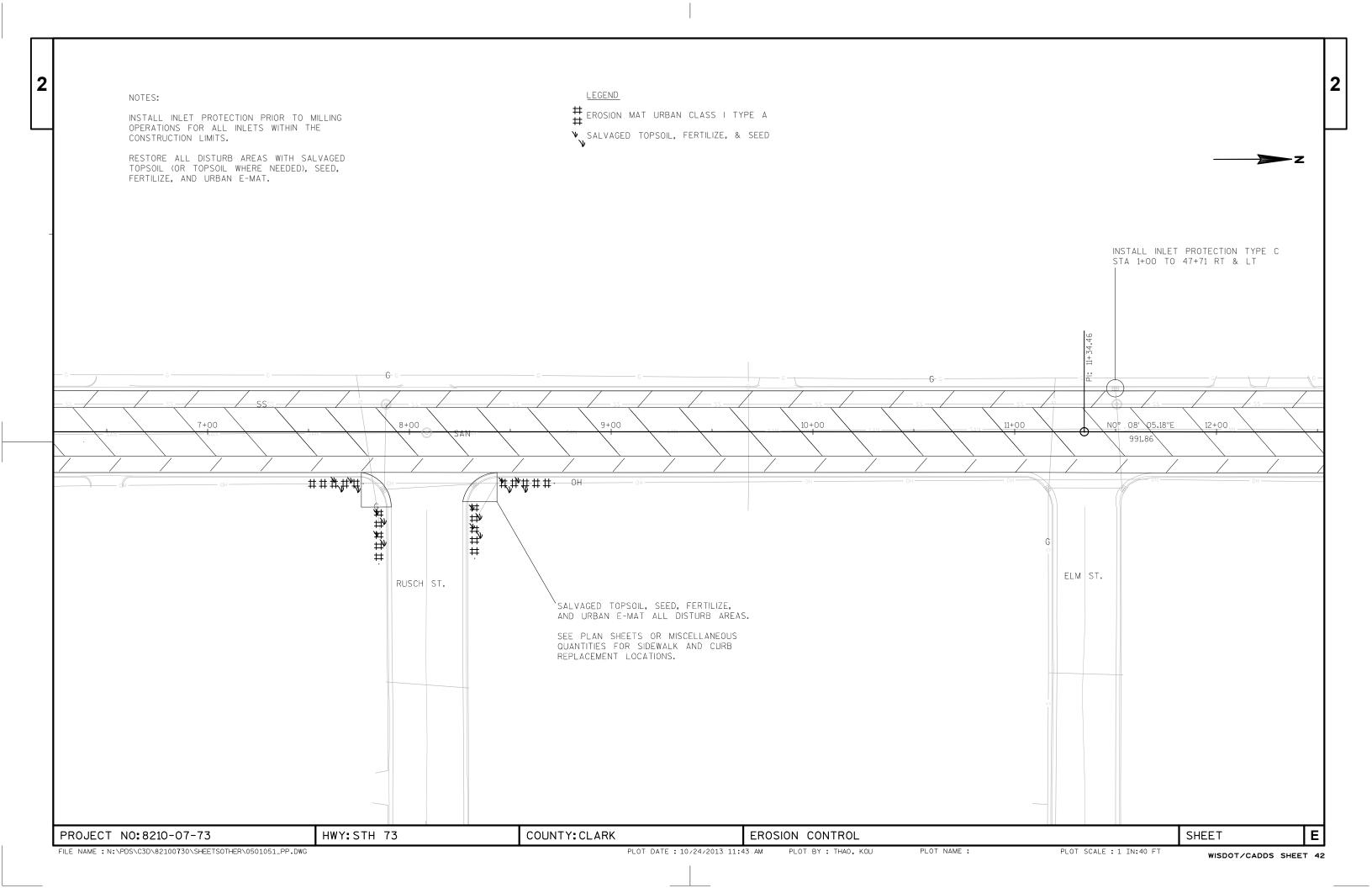


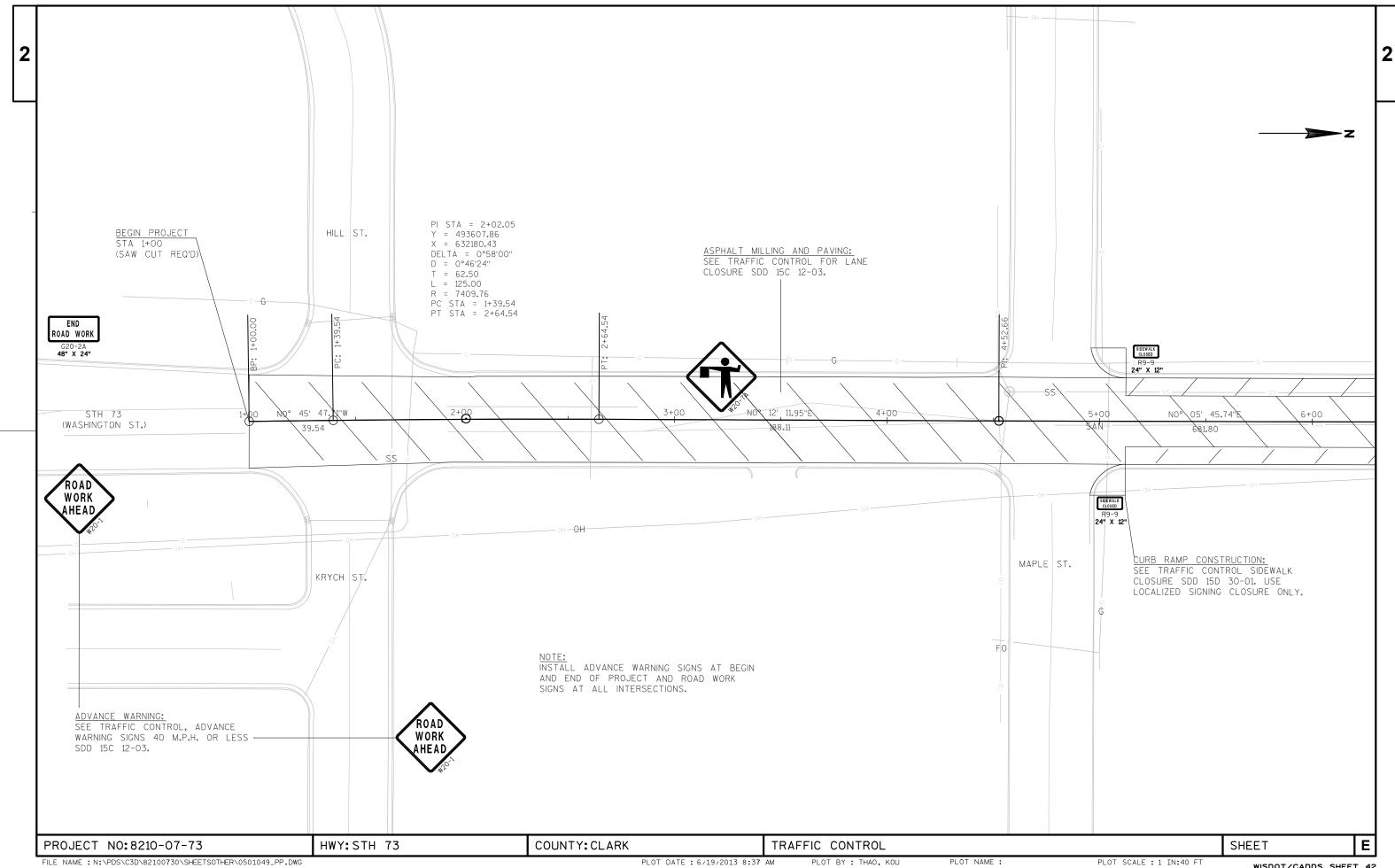




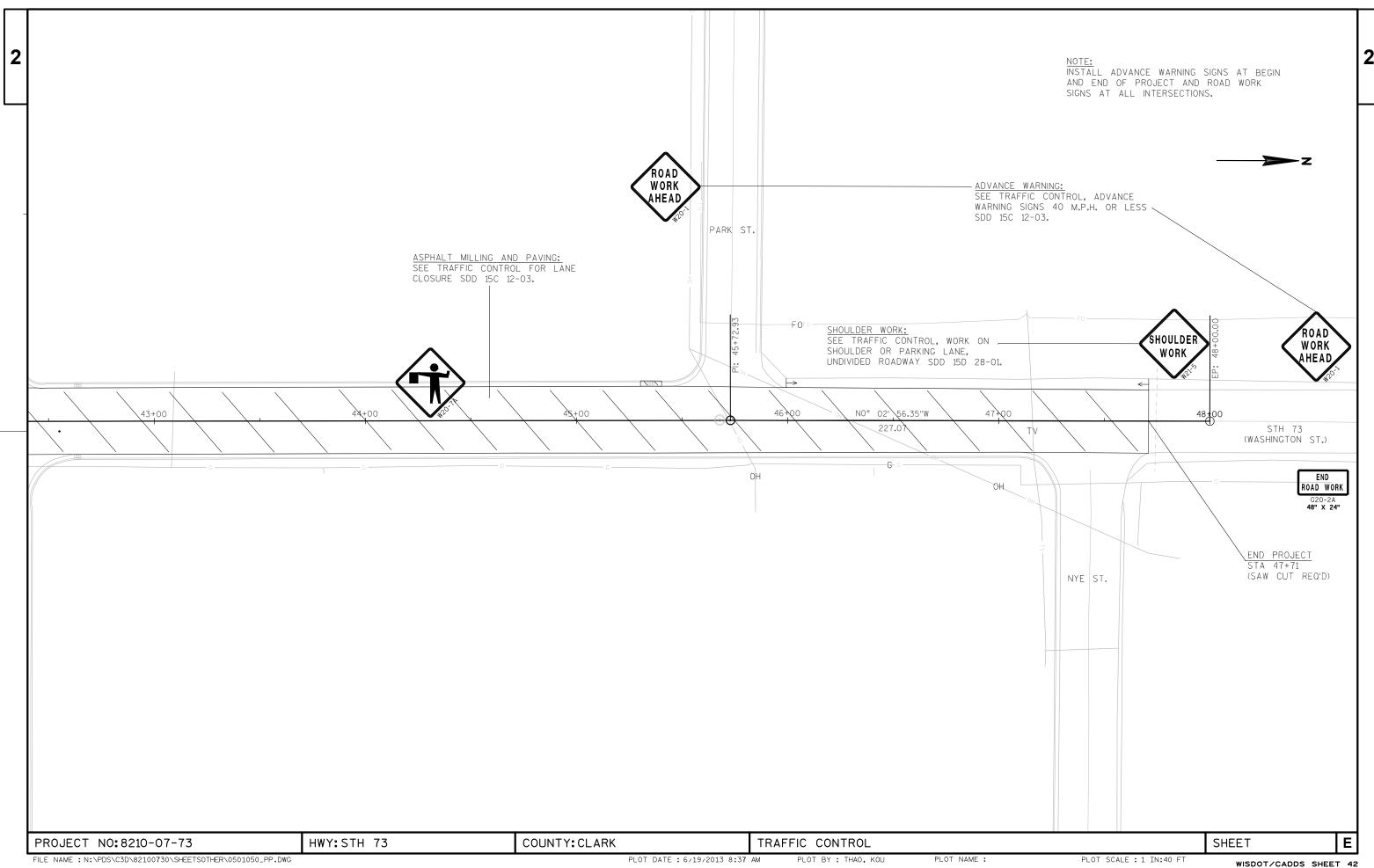








PLOT SCALE : 1 IN:40 FT WISDOT/CADDS SHEET 42



ITEM DESCRIPTION UNIT	DATE 05	5NOV13	E S	TIMATE	OF QUAN	T I T I E S 8210-07-73
0202	NUMBER					QUANTI TY
DOGS	0010					
DOSD 204.0155 REMOVING CONCRETE SI DEWALK SY 75.000 75.000	0030		REMOVING ASPHALTIC SURFACE MILLING			
213.0100						
B210-07-73 BASE ACOREGATE DENSE 3/4-INCH TON 15.000 15.0						
0070 0070	0060	213. 0100		EACH	1. 000	1. 000
Main	0070				15. 000	
1010 460, 1103 HMA PAYEMENT TYPE E-3	0800					
10120 465, 0105 ASPHALTIC SURFACE TON 225, 000 225, 000 20130 601, 0411 CONCRETE CUBB & GUTTER 30-INCH TYPE D LF 222, 000 222, 000 20150 602, 0405 CONCRETE SIDEWALK 4-INCH SF 675, 000 675, 000 675, 000 602, 0505 CUBR RAMP DETECTABLE WARNING FIELD SF 40, 000 40, 000 40, 000 602, 0505 CUBR RAMP DETECTABLE WARNING FIELD SF 40, 000 40, 000 602, 000 611, 0624 INLET COVERS TYPE A EACH 1, 000 1, 000 611, 0611, 0624 INLET COVERS TYPE A EACH 1, 000 1, 000 611, 0613, 0100 MINIET COVERS TYPE A EACH 1, 000 1, 000 611, 0613, 0100 MINIET ROWNERS EACH 1, 000 1, 000 619, 016, 0100 MINIET ROWNERS EACH 1, 000 1, 000 619, 0100 MINIET LYBING EACH 1, 000 1, 000 619, 0100 MINIET LYBING EACH 1, 000 1, 000 619, 0100 619, 1000 MINIET LYBING EACH 1, 000 1, 000 610, 000 619, 1000 619,	0100					
10120 405, 0105 ASPHALTIC SURFACE TON 225, 000 225, 000 20130 601, 0411 CONCRETE CUBR & GUTTER 30-INCH TYPE D LF 222, 000 222, 000 225, 000 20150 602, 0405 CONCRETE SIDEWALK 4-INCH SF 675, 000 675, 000 40, 000 7ELLOW CONCRETE SIDEWALK 4-INCH SF 40, 000 40, 000 40, 000 7ELLOW CONCRETE SIDEWALK 4-INCH SF 40, 000 40	0110	4/0.2000	LNCENTLYE DENCLTY LIMA DAVEMENT	DOI	2 020 000	2 020 000
0130 001.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D						
Color	0130			LF	222.000	222.000
VELLOW V	0140					
10170 611.0624 INLET COVERS TYPE H	0130	602.0303		эг	40.000	40.000
0180 611. 8115 ADJUSTING INLET COVERS EACH 1.000 1.000	0160	611. 0600	INLET COVERS TYPE A	EACH	2. 000	2. 000
0190	0170	611. 0624	INLET COVERS TYPE H		1. 000	1. 000
(PROJECT) 01.8210-07-73 (PROJECT) 01.000 (PROJECT) 01.8210-07-73 (PROJECT) 01.000 (
10210 625.0100 TOPSOIL SY 100.000 100.000 100.000 1020			(PROJECT) 01.8210-07-73			
0220 625. 0500 SALVAGED TOPSOIL SY 130. 000 130. 000 230 628. 1905 MOBIL L2ATI ONS EROSI ON CONTROL EACH 2. 000 2. 000 2000 20240 628. 1901 MOBIL L2ATI ONS EMERGENCY EROSI ON CONTROL EACH 1. 000 1. 000 1. 000 0250 628. 2006 EROSI ON MAT URBAN CLASS I TYPE A SY 100. 000 100	0200	619. 1000	MOBI LI ZATI ON	EACH	1. 000	1. 000
0230 628, 1995 MOBILIZATIONS EROSION CONTROL EACH 2,000 2,000	0210					
0240 628. 1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH 1. 000 1. 000 0250 628. 2006 EROSION MAT URBAN CLASS I TYPE A SY 100. 000 100. 000 100. 000 0250 628. 7015 INLET PROTECTION TYPE C EACH 36. 000 36. 000 0. 500 0280 630. 0120 SEEDING MIXTURE NO. 20 LB 50. 000 50. 000 0290 634. 0616 POSTS WOOD 4X6-INCH X 16-FT EACH 31. 000 31. 000 0300 634. 0618 POSTS WOOD 4X6-INCH X 18-FT EACH 7. 000 7. 000 000 0300 634. 0618 POSTS WOOD 4X6-INCH X 18-FT EACH 7. 000 7. 000 000 0300 634. 0618 POSTS WOOD 4X6-INCH X 18-FT EACH 7. 000 7. 000 000 0300 634. 0618 POSTS WOOD 4X6-INCH X 18-FT EACH 7. 000 7. 000 000 0300 634. 0618 POSTS WOOD 4X6-INCH X 18-FT EACH 7. 000 7. 000 000 0300 637. 2210 SIGNS TYPE II REFLECTIVE F SF 287. 460						
0260 628. 7015 INLET PROTECTION TYPE C EACH 36.000 36.000 0270 629.0205 FERTILIZER TYPE A CWT 0.500 0.500 0280 630.0120 SEEDING MIXTURE NO. 20 LB 50.000 50.000 0290 634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH 31.000 31.000 0300 634.0618 POSTS WOOD 4X6-INCH X 18-FT EACH 31.000 7.000 0310 634.0618 POSTS WOOD 4X6-INCH X 18-FT EACH 7.000 7.000 0310 634.0814 POSTS TUBULAR STEEL 2X2-INCH X 14-FT EACH 7.000 7.000 0310 634.0814 POSTS TUBULAR STEEL 2X2-INCH X 14-FT EACH 6.000 6.000 0320 637.2210 SIGNS TYPE II REFLECTIVE H SF 287.460 287.460 0330 637.2210 SIGNS TYPE II REFLECTIVE F SF 14.140 14.140 0340 638.2602 REMOVING SIGNS TYPE II EACH 69.000 69.000 0350 638.3000 REMOVING SIGNS TYPE II EACH 69.000 69.000 0350 638.3000 REMOVING SMALL SIGN SUPPORTS EACH 44.000 44.000 0360 642.5201 FIELD OFFICE TYPE C 0370 643.0100 TRAFFIC CONTROL (PROJECT) 01.8210-07-73 EACH 1.000 1.000 0380 643.0410 TRAFFIC CONTROL BARRICADES TYPE II DAY 210.000 210.000 0390 643.0410 TRAFFIC CONTROL BARRICADES TYPE II DAY 210.000 210.000 0390 643.0410 TRAFFIC CONTROL BARRICADES TYPE II DAY 210.000 210.000 0400 646.0106 PAVEMENT MARKING EPOXY 4-INCH LF 4,378.000 4,378.000 0410 646.0126 PAVEMENT MARKING EPOXY 4-INCH LF 38.000 38.000 0430 647.0136 PAVEMENT MARKING EPOXY 12-INCH LF 38.000 38.000 0400 647.0166 PAVEMENT MARKING RAILROAD CROSSINGS EACH 2.000 2.000 0400 647.0166 PAVEMENT MARKING RAILROAD CROSSINGS EACH 1.000 1.000 0400 647.0166 PAVEMENT MARKING RAILROAD CROSSINGS EACH 1.000 1.000 0400 647.0166 PAVEMENT MARKING PARKING STALL EPOXY LF 1,786.000 1,786.000 0470 647.0766 PAVEMENT MARKING PARKING STALL EPOXY LF 1,786.000 1,786.000 0470 647.0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF 2,550.000 2,550.000 0400 648.0100 LOCATING NO-PASSING ZONES MII 1.000 1.000	0230					
0270 629.0205 FERTILIZER TYPE A CWT 0.500 0.500 0280 630.0120 SEEDING MIXTURE NO. 20 LB 50.000 50.000 0290 634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH 31.000 31.000 0300 634.0618 POSTS WOOD 4X6-INCH X 18-FT EACH 7.000 7.000 0310 634.0618 POSTS WOOD 4X6-INCH X 18-FT EACH 7.000 7.000 0310 634.0814 POSTS TUBULAR STEEL ZX2-INCH X 14-FT EACH 6.000 6.000 0320 637.2210 SIGNS TYPE II REFLECTIVE H SF 287.460 287.460 0330 637.2230 SIGNS TYPE II REFLECTIVE F SF 14.140 14.140 0340 638.2602 REMOVING SIGNS TYPE II EACH 69.000 69.000 0350 638.3000 REMOVING SMALL SIGN SUPPORTS EACH 44.000 44.000 0360 642.5201 FIELD OFFICE TYPE C EACH 1.000 1.000 0370 643.0100 TRAFFIC CONTROL (PROJECT) 01.8210-07-73 EACH 1.000 1.000 0390 643.0410 TRAFFIC CONTROL BARRICADES TYPE II DAY 210.000 210.000 0400 646.0106 PAVEMENT MARKING EPOXY 4-INCH LF 4,378.000 4,378.000 0410 646.0126 PAVEMENT MARKING EPOXY 12-INCH LF 38.000 38.000 0410 647.0136 PAVEMENT MARKING RAILROAD CROSSINGS EACH 1.000 1.000 0420 647.0136 PAVEMENT MARKING RAILROAD CROSSINGS EACH 1.000 1.000 0440 647.0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1.000 1.000 0400 647.0366 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1.000 1.000 0400 647.0366 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1.000 1.000 0400 647.0366 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1.000 1.000 0400 647.0766 PAVEMENT MARKING PARKING STALL EPOXY EACH 1.000 1,786.000 0470 647.0766 PAVEMENT MARKING PARKING STALL EPOXY EACH 1.000 1,786.000 0470 647.0766 PAVEMENT MARKING PARKING STALL EPOXY EACH 1.000 1,000 0480 648.0100 LOCATING NO-PASSING ZONES MI 1.000 1,000 0490 650.5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222.000 222.000	0250	628. 2006	EROSION MAT URBAN CLASS I TYPE A	SY	100.000	100.000
0280 630.0120 SEEDING MIXTURE NO. 20 LB 50.000 50.000 0290 634.0616 POSTS WODD 4X6-INCH X 16-FT EACH 31.000 31.000 0300 634.0618 POSTS WODD 4X6-INCH X 18-FT EACH 7.000 7.000 0310 634.0814 POSTS TUBULAR STEEL 2X2-INCH X 14-FT EACH 6.000 6.000 0320 637.2210 SIGNS TYPE II REFLECTIVE H SF 287.460 287.460 0330 637.2230 SIGNS TYPE II REFLECTIVE F SF 14.140 14.140 0340 638.2602 REMOVIN RG SIGNS TYPE II EACH 69.000 69.000 0350 638.3000 REMOVING SMALL SIGN SUPPORTS EACH 44.000 44.000 0360 642.5201 FIELD OFFICE TYPE C EACH 1.000 1.000 0370 643.0100 TRAFFIC CONTROL (PROJECT) 01.8210-07-73 EACH 1.000 1.000 0380 643.0410 TRAFFIC CONTROL SARRICADES TYPE II DAY 210.000 210.000 0390 643.0900 TRAFFIC CONTROL SIGNS DAY 940.000 940.000 0400 646.0106 PAVEMENT MARKING EPOXY 4-INCH LF 4,378.000 4,378.000 0410 646.0126 PAVEMENT MARKING EPOXY 8-INCH LF 38.000 38.000 0430 647.0100 PAVEMENT MARKING RAILROAD CROSSINGS EACH 2.000 2.000 0400 647.0166 PAVEMENT MARKING RAILROAD CROSSINGS EACH 1.000 1.000 0400 647.0166 PAVEMENT MARKING RAILROAD CROSSINGS EACH 1.000 1.000 0400 647.0166 PAVEMENT MARKING RAILROAD CROSSINGS EACH 1.000 1.000 0400 647.0166 PAVEMENT MARKING RAILROAD CROSSINGS EACH 1.000 1.000 0400 647.0766 PAVEMENT MARKING STALL EPOXY EACH 1.000 1.000 0480 648.0100 LOCATING NO-PASSING ZONES MI 1.000 1.000 0490 650.5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222.000 222.000	0260	628. 7015	INLET PROTECTION TYPE C	EACH	36.000	36.000
0290 634, 0616 POSTS WOOD 4X6-INCH X 16-FT EACH 7, 000 31, 000 634, 0618 POSTS WOOD 4X6-INCH X 18-FT EACH 7, 000 7	0270					
0300 634. 0618 POSTS WOOD 4X6-INCH X 18-FT EACH 7. 000 7. 000 0310 634. 0814 POSTS TUBULAR STEEL 2X2-INCH X 14-FT EACH 6. 000 6. 000 0320 637. 2210 SI GNS TYPE II REFLECTIVE H SF 287. 460 287. 460 0330 637. 2230 SI GNS TYPE II REFLECTIVE F SF 14. 140 14. 140 0340 638. 2602 REMOVI NG SI GNS TYPE II EACH 69. 000 69. 000 0350 638. 3000 REMOVI NG SIGNS TYPE II EACH 69. 000 69. 000 0360 642. 5201 FI ELD OFFI CE TYPE C EACH 44. 000 44. 000 0360 643. 0410 TRAFFI C CONTROL (PROJECT) 01. 8210-07-73 EACH 1. 000 1. 000 0370 643. 0100 TRAFFI C CONTROL BARRI CADES TYPE II DAY 210. 000 210. 000 0380 643. 0410 TRAFFI C CONTROL SI GNS DAY 940. 000 940. 000 0400 646. 0106 PAVEMENT MARKI NG EPOXY 4-I NCH LF 4, 378. 000 4, 378. 000 0410 646. 0126 PAVEMENT MARKI NG EPOXY 12-I NICH LF 38. 000 38. 000 0420 647. 0136 PAVEMENT MARKI NG RAIL ROAD CROSSI NGS EACH 2. 000 2. 000 0400 647. 0136 PAVEMENT MARKI NG ARROWS EPOXY TYPE 2 EACH 1. 000 1. 000 0460 647. 0356 PAVEMENT MARKI NG PARKI NG STALL EPOXY EACH 1. 000 1. 000 0470 647. 0766 PAVEMENT MARKI NG PARKI NG STALL EPOXY EACH 1. 000 1. 786. 000 0470 647. 0766 PAVEMENT MARKI NG PARKI NG STALL EPOXY EACH 1. 000 1. 786. 000 0480 648. 0100 LOCATI NG NO-PASSI NG ZONES MI 1. 000 2. 550. 000 0480 648. 0100 LOCATI NG NO-PASSI NG ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTI ON STAKI NG CUTBE GUTTER AND LF 222. 000 22. 000						
0320 637. 2210 SI GNS TYPE II REFLECTIVE H 0330 637. 2230 SI GNS TYPE II REFLECTIVE F 0340 638. 2602 REMOVI NG SI GNS TYPE II EACH 69. 000 69. 000 0350 638. 3000 REMOVI NG SI GNS TYPE II 0350 638. 3000 REMOVI NG SMALL SI GN SUPPORTS 0360 642. 5201 FI ELD OFFI CE TYPE C 0370 643. 0100 TRAFFI C CONTROL (PROJECT) 01. 8210-07-73 EACH 1. 000 1. 000 0380 643. 0410 TRAFFI C CONTROL SI GNS DAY 940. 000 940. 000 0400 646. 0106 PAVEMENT MARKI NG EPOXY 4-I NCH LF 38. 000 38. 000 0410 646. 0126 PAVEMENT MARKI NG EPOXY 12-I NCH LF 38. 000 38. 000 0430 647. 0110 PAVEMENT MARKI NG RAI LROAD CROSSI NGS EACH 2. 000 2. 000 0400 647. 0166 PAVEMENT MARKI NG ARROWS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKI NG PARKI NG STALL EPOXY EACH 1. 000 1. 000 0460 647. 0356 PAVEMENT MARKI NG PARKI NG STALL EPOXY EACH 1. 000 1. 000 0470 647. 0766 PAVEMENT MARKI NG PARKI NG STALL EPOXY LF 1, 786. 000 2, 550. 000 0470 647. 0766 PAVEMENT MARKI NG CROSSWALK EPOXY 6-I NCH LF 2, 550. 000 2, 550. 000 0480 648. 0100 LOCATI NG NO-PASSI NG CORS WALK EPOXY 6-I NCH LF 2, 550. 000 2, 550. 000 0490 650. 5500 CONSTRUCTI ON STAKI NG CURB GUTTER AND LF 222. 000 222. 000	0300					
0330 637. 2230 SIGNS TYPE II REFLECTIVE F SF 14. 140 14. 140 0340 638. 2602 REMOVING SIGNS TYPE II EACH 69. 000 69. 000 0350 638. 3000 REMOVING SMALL SIGN SUPPORTS EACH 44. 000 44. 000 44. 000 0350 638. 3000 REMOVING SMALL SIGN SUPPORTS EACH 1. 000 1. 000 0370 643. 0100 TRAFFIC CONTROL (PROJECT) 01. 8210-07-73 EACH 1. 000 1. 000 0380 643. 0410 TRAFFIC CONTROL BARRICADES TYPE II DAY 210. 000 210. 000 0390 643. 0900 TRAFFIC CONTROL SIGNS DAY 940. 000 940. 000 0400 646. 0106 PAVEMENT MARKING EPOXY 4-INCH LF 4, 378. 000 4, 378. 000 0400 646. 0136 PAVEMENT MARKING EPOXY 12-INCH LF 38. 000 38. 000 0430 647. 0110 PAVEMENT MARKING RAILROAD CROSSINGS EACH 2. 000 2. 000 EPOXY 0440 647. 0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKING PAKING SPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKING PAKING STALL EPOXY EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKING PARKING STALL EPOXY EACH 1. 000 1. 000 0450 647. 0766 PAVEMENT MARKING STALL EPOXY EACH 1. 000 1. 000 0450 647. 0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF 2, 550. 000 2, 550. 000 0480 648. 0100 LOCATING NO-PASSING ZONES MI 1. 000 222. 000 0450 650. 5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222. 000 222. 000	0310	634. 0814	POSTS TUBULAR STEEL 2X2-INCH X 14-FT	EACH	6. 000	6. 000
0340 638. 2602 REMOVING SIGNS TYPE II EACH 69. 000 69. 000 0350 638. 3000 REMOVING SMALL SIGN SUPPORTS EACH 44. 000 44. 000 0360 642. 5201 FIELD OFFICE TYPE C EACH 1. 000 1. 000 0370 643. 0100 TRAFFIC CONTROL (PROJECT) 01. 8210-07-73 EACH 1. 000 1. 000 0380 643. 0410 TRAFFIC CONTROL BARRI CADES TYPE II DAY 210. 000 210. 000 0390 643. 0900 TRAFFIC CONTROL SIGNS DAY 940. 000 940. 000 0400 646. 0106 PAVEMENT MARKI NG EPOXY 4-I NCH LF 4, 378. 000 4, 378. 000 0410 646. 0126 PAVEMENT MARKI NG EPOXY 8-I NCH LF 38. 000 38. 000 0420 646. 0136 PAVEMENT MARKI NG ROSSY 12-I NCH LF 38. 000 38. 000 0430 647. 0110 PAVEMENT MARKI NG RAI LROAD CROSSI NGS EACH 2. 000 2. 000 0440 647. 0166 PAVEMENT MARKI NG ROSS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKI NG WORDS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKI NG STALL EPOXY EACH 1. 000 1. 000 0460 647. 0656 PAVEMENT MARKI NG STALL EPOXY EACH 1. 000 1. 000 0460 647. 0766 PAVEMENT MARKI NG STALL EPOXY EACH 1. 000 1. 000 0480 648. 0100 LOCATI NG NO-PASSI NG ZONES MI 1. 000 2, 550. 000 0480 648. 0100 LOCATI NG NO-PASSI NG ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTI ON STAKI NG CURB GUTTER AND LF 222. 000 222. 000	0320					
0350 638.3000 REMOVING SMALL SIGN SUPPORTS EACH 44.000 44.000 0360 642.5201 FIELD OFFICE TYPE C EACH 1.000 1.000 0370 643.0100 TRAFFIC CONTROL (PROJECT) 01.8210-07-73 EACH 1.000 1.000 0380 643.0410 TRAFFIC CONTROL BARRICADES TYPE II DAY 210.000 210.000 0390 643.0900 TRAFFIC CONTROL SIGNS DAY 940.000 940.000 0400 646.0106 PAVEMENT MARKING EPOXY 4-INCH LF 4,378.000 4,378.000 0410 646.0126 PAVEMENT MARKING EPOXY 8-INCH LF 100.000 100.000 0420 646.0136 PAVEMENT MARKING EPOXY 12-INCH LF 38.000 38.000 0430 647.0110 PAVEMENT MARKING RAILROAD CROSSINGS EACH 2.000 2.000 EPOXY 0440 647.0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1.000 1.000 0450 647.0356 PAVEMENT MARKING WORDS EPOXY EACH 1.000 1.000 0460 647.0656 PAVEMENT MARKING STALL EPOXY LF 1,786.000 1,786.000 0470 647.0766 PAVEMENT MARKING STALL EPOXY LF 1,786.000 1,786.000 0470 647.0766 PAVEMENT MARKING STALL EPOXY LF 1,786.000 2,550.000 0470 647.0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF 2,550.000 2,550.000 0480 648.0100 LOCATING NO-PASSING ZONES MI 1.000 1.000 0490 650.5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222.000 222.000	0330 0340					
0370 643. 0100 TRAFFIC CONTROL (PROJECT) 01. 8210-07-73 EACH 1. 000 1. 000 0380 643. 0410 TRAFFIC CONTROL BARRICADES TYPE II DAY 210. 000 210. 000 0390 643. 0900 TRAFFIC CONTROL SIGNS DAY 940. 000 940. 000 0400 646. 0106 PAVEMENT MARKING EPOXY 4-INCH LF 4, 378. 000 4, 378. 000 0410 646. 0126 PAVEMENT MARKING EPOXY 8-INCH LF 38. 000 38. 000 0420 646. 0136 PAVEMENT MARKING EPOXY 12-INCH LF 38. 000 38. 000 0430 647. 0110 PAVEMENT MARKING RAILROAD CROSSINGS EACH 2. 000 2. 000 EPOXY 0440 647. 0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKING WORDS EPOXY EACH 1. 000 1. 000 0460 647. 0656 PAVEMENT MARKING STALL EPOXY EACH 1. 000 1. 000 0460 647. 0766 PAVEMENT MARKING STALL EPOXY EACH 1. 000 1. 000 0470 647. 0766 PAVEMENT MARKING STALL EPOXY EACH 1. 000 2. 550. 000 0480 648. 0100 LOCATING NO-PASSING ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222. 000 222. 000	0350					
0370 643. 0100 TRAFFIC CONTROL (PROJECT) 01. 8210-07-73 EACH 1. 000 1. 000 0380 643. 0410 TRAFFIC CONTROL BARRICADES TYPE II DAY 210. 000 210. 000 0390 643. 0900 TRAFFIC CONTROL SIGNS DAY 940. 000 940. 000 0400 646. 0106 PAVEMENT MARKING EPOXY 4-INCH LF 4, 378. 000 4, 378. 000 0410 646. 0126 PAVEMENT MARKING EPOXY 8-INCH LF 38. 000 38. 000 0420 646. 0136 PAVEMENT MARKING EPOXY 12-INCH LF 38. 000 38. 000 0430 647. 0110 PAVEMENT MARKING RAILROAD CROSSINGS EACH 2. 000 2. 000 EPOXY 0440 647. 0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKING WORDS EPOXY EACH 1. 000 1. 000 0460 647. 0656 PAVEMENT MARKING STALL EPOXY EACH 1. 000 1. 000 0460 647. 0766 PAVEMENT MARKING STALL EPOXY EACH 1. 000 1. 000 0470 647. 0766 PAVEMENT MARKING STALL EPOXY EACH 1. 000 2. 550. 000 0480 648. 0100 LOCATING NO-PASSING ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222. 000 222. 000	0360	642, 5201	FIELD OFFICE TYPE C	EACH	1. 000	1. 000
0390 643.0900 TRAFFIC CONTROL SIGNS DAY 940.000 940.000 0400 646.0106 PAVEMENT MARKING EPOXY 4-INCH LF 4,378.000 4,378.000 0410 646.0126 PAVEMENT MARKING EPOXY 8-INCH LF 100.000 100.000 0420 646.0136 PAVEMENT MARKING EPOXY 12-INCH LF 38.000 38.000 0430 647.0110 PAVEMENT MARKING RAILROAD CROSSINGS EACH 2.000 2.000 EPOXY 0440 647.0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1.000 1.000 0450 647.0356 PAVEMENT MARKING WORDS EPOXY EACH 1.000 1.000 0460 647.0656 PAVEMENT MARKING STALL EPOXY EACH 1.000 1.786.000 0470 647.0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF 2,550.000 2,550.000 0480 648.0100 LOCATING NO-PASSING ZONES MI 1.000 1.000 0490 650.5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222.000 222.000	0370	643.0100	TRAFFIC CONTROL (PROJECT) 01.8210-07-73	EACH	1.000	1. 000
0400 646. 0106 PAVEMENT MARKING EPOXY 4-INCH LF 4, 378. 000 4, 378. 000 0410 646. 0126 PAVEMENT MARKING EPOXY 8-INCH LF 100. 000 100. 000 0420 646. 0136 PAVEMENT MARKING EPOXY 12-INCH LF 38. 000 38. 000 0430 647. 0110 PAVEMENT MARKING RAILROAD CROSSINGS EACH 2. 000 2. 000 EPOXY 0440 647. 0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKING WORDS EPOXY EACH 1. 000 1. 000 0460 647. 0656 PAVEMENT MARKING PARKING STALL EPOXY LF 1, 786. 000 1, 786. 000 0470 647. 0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF 2, 550. 000 2, 550. 000 0480 648. 0100 LOCATING NO-PASSING ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222. 000 222. 000	0380					
0420 646. 0136 PAVEMENT MARKING EPOXY 12-INCH LF 38. 000 38. 000 0430 647. 0110 PAVEMENT MARKING RAILROAD CROSSINGS EACH 2. 000 2. 000 EPOXY 0440 647. 0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKING WORDS EPOXY EACH 1. 000 1. 000 0460 647. 0656 PAVEMENT MARKING PARKING STALL EPOXY LF 1, 786. 000 1, 786. 000 0470 647. 0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF 2, 550. 000 2, 550. 000 0480 648. 0100 LOCATING NO-PASSING ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222. 000 222. 000	0390 0400					
0420 646. 0136 PAVEMENT MARKING EPOXY 12-INCH LF 38. 000 38. 000 0430 647. 0110 PAVEMENT MARKING RAILROAD CROSSINGS EACH 2. 000 2. 000 EPOXY 0440 647. 0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKING WORDS EPOXY EACH 1. 000 1. 000 0460 647. 0656 PAVEMENT MARKING PARKING STALL EPOXY LF 1, 786. 000 1, 786. 000 0470 647. 0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF 2, 550. 000 2, 550. 000 0480 648. 0100 LOCATING NO-PASSING ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222. 000 222. 000	0410	646, 0126	PAVEMENT MARKING FPOXY 8-INCH	 LF	100,000	100,000
EPOXY 0440 647. 0166 PAVEMENT MARKI NG ARROWS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKI NG WORDS EPOXY EACH 1. 000 1. 000 0460 647. 0656 PAVEMENT MARKI NG PARKI NG STALL EPOXY LF 1, 786. 000 1, 786. 000 0470 647. 0766 PAVEMENT MARKI NG CROSSWALK EPOXY 6-I NCH LF 2, 550. 000 2, 550. 000 0480 648. 0100 LOCATI NG NO-PASSI NG ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTI ON STAKI NG CURB GUTTER AND LF 222. 000 222. 000	0410	646. 0136	PAVEMENT MARKING EPOXY 12-INCH		38.000	38.000
0440 647. 0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EACH 1. 000 1. 000 0450 647. 0356 PAVEMENT MARKING WORDS EPOXY EACH 1. 000 1. 000 0460 647. 0656 PAVEMENT MARKING PARKING STALL EPOXY LF 1, 786. 000 1, 786. 000 0470 647. 0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF 2, 550. 000 2, 550. 000 0480 648. 0100 LOCATING NO-PASSING ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222. 000 222. 000	0430	647. 0110		EACH	2. 000	2. 000
0460 647. 0656 PAVEMENT MARKING PARKING STALL EPOXY LF 1, 786. 000 1, 786. 000 0470 647. 0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF 2, 550. 000 2, 550. 000 0480 648. 0100 LOCATING NO-PASSING ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222. 000 222. 000	0440	647. 0166		EACH	1. 000	1. 000
0470 647. 0766 PAVEMENT MARKING CROSSWALK EPOXY 6-INCH LF 2,550. 000 2,550. 000 0480 648. 0100 LOCATING NO-PASSING ZONES MI 1. 000 1. 000 0490 650. 5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222. 000 222. 000	0450	647. 0356	PAVEMENT MARKING WORDS EPOXY	EACH	1. 000	1. 000
0480 648.0100 LOCATING NO-PASSING ZONES MI 1.000 1.000 0490 650.5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222.000 222.000	0460					
0490 650. 5500 CONSTRUCTION STAKING CURB GUTTER AND LF 222. 000 222. 000						
	0480					
			CURB & GUTTER			

DATE 05 LINE	5NOV13	E S T	IMATE	OF QUAN	T I T I E S 8210-07-73	
NUMBER 0500	ITEM 650.8000	ITEM DESCRIPTION CONSTRUCTION STAKING RESURFACING REFERENCE	UNI T LF	TOTAL 4, 800. 000	QUANTI TY 4, 800. 000	
0510	650. 9910	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) 01.8210-07-73	LS	1.000	1. 000	
0520	690. 0150	SAWING ASPHALT	LF	75.000	75.000	
0530	690. 0250	SAWI NG CONCRETE	LF	55.000	55.000	
0540	ASP. 1TOA	ON-THE-JOB TRAINING APPRENTICE AT \$5.	HRS	1, 200. 000	1, 200. 000	
0550	ASP. 1TOG	ON-THE-JOB TRAINING GRADUATE AT \$5.00/HR	HRS	300.000	300.000	
0560	SPV. 0090	SPECIAL O1. CONCRETE CURB AND GUTTER CURE AND SEAL TREATMENT	LF	222. 000	222. 000	
0570	SPV. 0105	SPECIAL 01. PREPARE FOUNDATION FOR ASPHALTIC PAVING SPECIAL	LS	1. 000	1. 000	
0580	SPV. 0165	SPECIAL 01. CONCRETE SIDEWALK CURE AND SEAL TREATMENT	SF	675. 000	675. 000	
0590	SPV. 0170	SPECIAL 01. REHEATING HMA PAVEMENT LONGITUDINAL JOINTS SPECIAL	STA	47. 000	47. 000	

REMOVING ASPHALTIC SURFACE

			204.0110	
CATEGORY	STATION	LOCATION	SY	REMARKS
0010	5+00	STH 73, RT & LT	16	C & G REPLACEMENT
0010	7+80	STH 73, RT	8	C & G REPLACEMENT
0010	8+35	STH 73, RT	8	C & G REPLACEMENT
0010	16+75	STH 73, LT	8	C & G REPLACEMENT
0010	31+60	STH 73, LT	10	C & G REPLACEMENT
0010	32+15	STH 73, LT	10	PANEL AT RAILROAD
0010	32+18	STH 73	20	4-FT AT RR BLOCK
0010	32+32	STH 73	20	4-FT AT RR BLOCK
0010	32+35	STH 73, RT	10	PANEL AT RAILROAD
0010	34+40	STH 73, LT	8	C & G REPLACEMENT
0010	38+85	STH 73, LT	8	C & G REPLACEMENT
0010	45+35	STH 73, LT	8	C & G REPLACEMENT
		TOTAL 0010	134	

REMOVING ASPHALTIC SURFACE BUTT JOINTS

CATEGORY	STATION TO STATION	LOCATION	204.0115 SY	REMARKS
CATEGORT	STATION TO STATION	LOCATION	31	REMARKS
0010	31+70 - 32+16	STH 73	230	RAILROAD BUTT JOINT (SOUTH)
0010	32+34 - 32+80	STH 73	230	RAILROAD BUTT JOINT (NORTH)
	52.00	TOTAL 0010	460	

REMOVING ASPHALTIC SURFACE MILLING

COUNTY: CLARK

					204.0120	
CATEGORY	STATION	TO	STATION	LOCATION	SY	REMARKS
0040	4 00		2 22	70	4.0.5	
0010			2+00	STH 73	465	
0010	2+00	-	5+12	STH 73	1400	
0010	5+12	-	21+75	STH 73	5590	
0010	21+75	-	31+70	STH 73	3900	
0010	32+80	-	34+33	STH 73	550	
0010	34+33	-	45+50	STH 73	3910	
0010	45+50	-	47+71	STH 73	760	
				TOTAL 0010	16575	
0020	5+27	_	20+75	STH 73, RT	830	6-FT SHLD
0020	5+27	-	20+75	STH 73, LT	970	6-FT SHLD
0020	21+75	_	31+70	STH 73, RT & LT	1020	6-FT SHLD
0020	32+80	-	34+00	STH 73, RT & LT	205	6-FT SHLD
				TOTAL 0020	3025	
				PROJECT TOTAL	19600	

HWY: STH 73

REMOVING CURB & GUTTER

			204.0150	
CATEGORY	STATION	LOCATION	LF	REMARKS
0010	5+00	STH 73, RT & LT	48	C & G REPLACEMENT
0010	7+80	STH 73, RT	24	C & G REPLACEMENT
0010	8+35	STH 73, RT	24	C & G REPLACEMENT
0010	16+75	STH 73, LT	24	C & G REPLACEMENT
0010	31+60	STH 73, LT	30	C & G REPLACEMENT
0010	34+40	STH 73, LT	24	C & G REPLACEMENT
0010	38+85	STH 73, LT	24	C & G REPLACEMENT
0010	45+35	STH 73, LT	24	C & G REPLACEMENT
		TOTAL 0010	222	

REMOVING CONCRETE SIDEWALK

			204.0155	
CATEGORY	STATION	LOCATION	SY	REMARKS
0010	5+00	STH 73, RT & LT	30	CURB RAMP REPLACEMENT
0010	7+80	STH 73, RT	15	CURB RAMP REPLACEMENT
0010	8+35	STH 73, RT	15	CURB RAMP REPLACEMENT
0010	34+40	STH 73, LT	15	CURB RAMP REPLACEMENT
		TOTAL 0010	75	

FINISHING ROADWAY (8210-07-73)

			213.0100	
CATEGORY	STATION TO STATION	LOCATION	EACH	REMARKS
0010	1+00 - 47+71	PROJECT	1	
		TOTAL 0010	1	
	<u>B</u> ,	ASE AGGREGATE DE	NSE 3/4-INCH	
			305.0110	
CATEGORY	STATION TO STATION	LOCATION	TON	REMARKS
0010	46+00 - 47+70	STH 73, LT	15	SHOULDERS
		TOTAL 0010	15	

Е

3

	Δ	ASPHALTIC PAVEMENT								ASPHALTIC S	<u>SURFACE</u>		
			ASPHALTIC		НМА						465.0105		
			MATERIAL	TACK	PAVEMENT		CATEGORY	STATIO	ON TO STATION	LOCATION	TON	REMA	RKS
			PG58-34P	COAT	TYPE E-3		·						<u>.</u>
			455.0135	455.0605	460.1103		0010	1+0	00 - 47+71	PROJECT	200	PREPARE FO	UNDATION
CATEGORY	STATION TO STATION	LOCATION	TON	GAL	TON	REMARKS	0010	1+0	00 - 47+71	PROJECT	25	RAMPING, P	POTHOLES
						_							
0010	1+00 - 2+00	STH 73	5	25	80					TOTAL 0010	225		
0010	2+00 - 5+12	STH 73	13	70	235								
0010	5+12 - 21+75	STH 73	47	280	860								
0010	21+75 - 34+33	STH 73	42	255	760	INCL PANELS @ RR			<u>INLETS</u>				
0010	34+33 - 45+50	STH 73	37	200	660				INCLIO	INLET	INLET	ADJUSTING	
0010	45+50 - 47+71	STH 73	8	40	130					COVERS	COVERS	INLET	
										TYPE A	TYPE H	COVERS	
		TOTAL 0010	152	870	2725					611.0600	611.0624	611.8115	
							CATEGORY	STATION	LOCATION	EACH	EACH	EACH	REMARKS
0020	5+27 - 20+75	STH 73, RT	5	22	85	6-FT SHLD	CATEGORI	3171112011	200/111011	<u> </u>		L/ (CIT	REP WAY
0020	5+27 - 20+75	STH 73, LT	6	25	95	6-FT SHLD	0010	16+75	STH 73, LT	_	_	1	
0020	21+75 - 34+12	STH 73, RT & LT	7	33	125	6-FT SHLD	0010	34+40	STH 73, LT	1	_	_	
							0010	38+85	STH 73, LT	1	_	_	
		TOTAL 0020	18	80	305		0010	45+35	STH 73, LT	_	1	_	
		TOTAL 0020	10	00	505		0010	43+33	3111 73, E1		_		
		PROJECT TOTAL	170	950	3030				TOTAL 0010	2	1	1	
	CURB &	GUTTER											
			CONCRET	E			CONCRETE	SIDEWALK					
		CONCRETE								CURB			
		CURB &								RAMP	CONCRETE		
		GUTTER								DETECTABLE	SIDEWALK		
		30-INCH							CONCRETE	WARNING	CURE &		
		TYPE D							SIDEWALK	FIELD	SEAL		
		601.0411							4-INCH	YELLOW	TREATMENT		
CATECORY	STATION LOCATION				DVC				602.0405	602.0505	SPV.0165		

			CONCRETE	CURB &						CURB		
			CURB &	GUTTER						RAMP	CONCRETE	
			GUTTER	CURE &						DETECTABLE	SIDEWALK	
									CONCRETE	WARNING	CURE &	
			30-INCH	SEAL					SIDEWALK	FIELD	SEAL	
			TYPE D	TREATMENT					4-INCH	YELLOW	TREATMENT	
			601.0411	SPV.0090					602.0405	602.0505	SPV.0165	
CATEGORY	STATION	LOCATION	<u>LF</u>	LF	REMARKS	CATEGORY	STATION	LOCATION	SF	SF	SF	REMARKS
						CATEGORI	SIAITON	LOCATION				KLMAKKS
0010	5+00	STH 73, RT & LT	48	48	C & G REPLACEMENT	0010	F 00	72 0	270	1.0	270	
0010	7+80	STH 73, RT	24	24	C & G REPLACEMENT	0010	5+00	STH 73, RT & LT	270	16	270	CURB RAMP REPLACEMENT
0010	8+35	STH 73, RT	24	24	C & G REPLACEMENT	0010	7+80	STH 73, RT	135	8	135	CURB RAMP REPLACEMENT
0010	16+75	STH 73, LT	24	24	C & G REPLACEMENT	0010	8+35	STH 73, RT	135	8	135	CURB RAMP REPLACEMENT
0010	31+60	STH 73, LT	30	30	C & G REPLACEMENT	0010	34+40	STH 73, LT	135	8	135	CURB RAMP REPLACEMENT
		·										
0010	34+40	STH 73, LT	24	24	C & G REPLACEMENT			TOTAL 0010	675	40	675	
0010	38+85	STH 73, LT	24	24	C & G REPLACEMENT			TOTAL OUTO	07.3	40	073	
0010	45+35	STH 73, LT	24	24	C & G REPLACEMENT							

TOTAL 0010 222 222

PROJECT NO: 8210-07-73 HWY: STH 73 COUNTY: CLARK MISCELLANEOUS QUANTITIES SHEET E

FILE NAME: \$\$....designfile....\$\$

PLOT DATE: \$\$...plottingdate...\$\$ PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:

PLOT SCALE : \$\$.....plotscale.....\$\$ WISDOT/CADDS SHEET 43

	MAINTENANCE AND REPAIR	OF HAUL ROADS (82	10-07-73)				MOBIL:	IZATION				MOBILIZATION
CATEGORY	STATION TO STATION	LOCATION	618.0100 EACH	REMARKS_					MOBILIZ		ILIZATION EROSION CONTROL	EMERGENCY EROSION CONTROL
0010	1+00 - 47+71	PROJECT	1		CATEGORY	STATION TO	STATION	LOCATION	619	.1000 EACH	628.1905 EACH	628.1910 EACH REMARK
		TOTAL 0010	1		0010	1+00 -	47+71	PROJECT		1	2	1
		INLET PROTECTION	TYPE C					TOTAL 0010)	1	2	1
			628.70									
	STATION TO STATION	LOCATION	EA				TRAFFIC CON	NTROL				
010	1+00 - 47+71	STH 73, RT & LT		36						TRAFFIC CONTROL	TRAFFIC	
		TOTAL 00		36					TRAFFIC CONTROL	BARRICADES TYPE II	CONTROL SIGNS	
	ETELD OF	FEECE TYPE 6 (0310	07 72)		CATEGORY	STATION TO ST	ATTON L	OCATION	643.0100 EACH	643.0410 DAYS	643.0900 DAYS	
	FIELD OF	FICE TYPE C (8210-	<u>-07-73)</u>		CATEGORY	STATION TO STA	ATTON L	OCATION	EACH	DAYS	DAYS	REMARKS
C.T. COD. (642.5201	D = 144 D // G	0010	1+00 - 47+7		ROJECT	1	-	- 140	
CATEGORY	STATION TO STATION	LOCATION	EACH	REMARKS	0010 0010	5+00 - 34+4 1+00 - 47+7		тн 73 тн 73	-	210	140 800	
0010	1+00 - 47+7	1 PROJECT	1					_				=
		TOTAL 0010	1				Т	OTAL 0010	1	210	940	
]	RESTORATION		FROSTON						
						EROSION MAT						
						URBAN		SEEDIN	IG			
				TORSOTI	SALVAGED	CLASS I	FERTILIZE					
				TOPSOIL 625.0100		TYPE A 628.2006	TYPE 629.020					
	CATE	GORY STATION	LOCATION	SY		SY	CW			MARKS		
	003		STH 73, RT &	LT -	30	30	0.0			_K & CURB		
	003		STH 73, RT	_	15	15 15	0.0			_K & CURB		
	003 003		STH 73, RT STH 73, RT	-	15 15	15 15	0.0 0.0			_K & CURB _K & CURB		
	003		STH 73, RT	_	15	15	0.0			LK & CURB		
	003		STH 73, LT	_	10	10	0.0			_K & CURB		
	003		STH 73, LT	_	10	10	0.0			_K & CURB		
	003		STH 73, LT	_	10	10	0.0		4 CURB			
	003		STH 73, LT	-		10	0.0)4	4 CURB			
	003	LO PROJECT	STH 73	100	-	-		-	- UNDISTI	RIBUTED		
			TOTAL C	0010 100	130	130	0.					

MISCELLANEOUS QUANTITIES

PLOT DATE: \$\$...plottingdate...\$\$ PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:

COUNTY: CLARK

HWY: STH 73

PROJECT NO: 8210-07-73

FILE NAME: \$\$....designfile....\$\$

Ε

SHEET

PLOT SCALE : \$\$.....plotscale.....\$\$ wiSDOT/CADDS SHEET 43

PERMANENT SIGNING

NOTE:	CTCN		^{A}DE	THEIRE	IINDED	CTMTI ATD	CTCN	CDOLID N	IIMDED
NOIF:	20 L CHN	REMOVALS	AKE	TINCL ODED	UNIJEK	SIMILAIR	20 I CHN	GROUP N	UMBER

SIGN GROUP NUMBER	SIGN CODE	SIGN MESSAGE	SIGN SIZE W x H (INCHES)	637.2210 SIGNS TYPE II REFLECTIVE H	637.2230 SIGNS TYPE II REFLECTIVE F	634.0616 POSTS WOOD 4x6-INCH x 16-FT EACH	634.0618 POSTS WOOD 4x6-INCH x 18-FT EACH	634.0814 POSTS TUBULAR STEEL 2x2-INCH x 14-FT EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3 REMOVING SMALL SIGN SUPPORT EACH	REMARKS
1-1	R1-1	STOP	30 x 30	5.18	-	1			1	1	
1-2	R1-1	STOP	30 x 30	5.18	=	1	=	=	1	1	
1-3	R2-1	SPEED LIMIT 35	24 x 30	5.00	=	1	=	=	1	1	
1-4	R2-1	SPEED LIMIT 35	24 x 30	5.00	_	1	_	_	1	1	
1-5	J2-3		72 x 57	28.50	_	_	2	_	_	2	
	M3-3	SOUTH Cardinal Route Marker	24 x 12	_	_	_	_	-	1	_	
	M1-6	STATE ROUTE MARKER 73	24 x 24	_	_	_	_	-	1	_	
	M6-1	ARROW AHEAD	21 x 21	-	_	-	_	-	1	_	
	M3-2	EAST <i>Cardinal Route Marker</i>	24 x 12	_	_	_	_	_	1	_	
	M1-6	STATE ROUTE MARKER 29	24 x 24	_	_	_	_	-	1	_	
	M6-1	ARROW AHEAD	21 x 21	-	_	-	_	-	1	_	
	M3-4	WEST <i>Cardinal Route Marker</i>	24 x 12	_	_	-	_	-	1	_	
	M1-6	STATE ROUTE MARKER 29	24 x 24	_	_	_	_	_	1	_	
	M5-1R	ADVANCE ARROW RT TURN	21 x 21	_	_	_	_	-	1	_	
1-6	R1-1	STOP	30 x 30	5.18	=	1	=	=	1	1	
1-7	R1-1	STOP	30 x 30	5.18	_	1	=	-	1	1	
2-1	R2-1	SPEED LIMIT 35	24 x 30	5.00	_	1	_	_	1	1	
2-2	R1-1	STOP	30 x 30	5.18	_	1	_	_	1	1	
2-3	R2-1	SPEED LIMIT 35	24 x 30	5.00	=	1	=	-	1	1	
2-4	J1-2		48 x 39	13.00	_	_	1	_	_	1	
	M2-1	JCT	21 x 15	_	_	_	_	_	1	_	
	M1-6	STATE ROUTE MARKER 29	24 x 24	_	_	_	_	_	1	_	
	M2-1	JCT	21 x 15	_	_	_	_	=	1	_	
	M1-5A	COUNTY MARKER M	24 x 24	_	_	_	_	_	1	_	
3-1	R1-1	STOP	30 x 30	5.18	_	1	_	_	1	1	
3-2	R2-1	SPEED LIMIT 35	24 x 30	5.00	_	1	_	_	1	1	
3-3	R1-1	STOP	30 x 30	5.18	_	1	_	-	1	1	
3-4	R2-1	SPEED LIMIT 25	24 x 30	5.00	_	1	_	_	1	1	
3-5	J4-1		24 x 36	6.00	_	-	1	=	_	1	
	M3-3	SOUTH Cardinal Route Marker	24 x 12	-	_	_	_ _	_	1	_	
	M1-6	STATE ROUTE MARKER 73	24 x 24	_	_	_	_	_	1	_	
3-6	R1-1	STOP	30 x 30	5.18	_	1	_	_	1	1	
3-7	J1-1		24 x 39	6.50	_	_	1	_	_	1	
_	M2-1	JCT	21 x 15	-	_	_	_	_	1	_	
	M1-5A	COUNTY MARKER X	24 x 24	_	_	_	_	_	1	_	
4-1	R2-1	SPEED LIMIT 25	24 x 30	5.00	_	1	=	=	1	1	
4-2	R14-1	TRUCK ROUTE	24 x 18	3.00	_	_	_	_	1	_	SAME POST AS 4-1
4-3	J13-1	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	24 x 45	7.50	_	_	1	_	_	1	
. 3	M1-5A	COUNTY MARKER X	24 x 24	-	_	_	-	_	1	_	
	M6-4	DIRECTIONAL ARROW LEFT-RIGHT	21 x 21	_	_	_	_	-	1	_	
4-4	R1-1	STOP	30 x 30	5.18	_	1	_	_	_ 1	1	
4-5	R1-1	STOP	30 x 30	5.18	_	-	_	1	1	1	
4-6	J13-1	2.0.	24 x 45	7.50	_	_	_	-	_	_	MOUNT ON EXISTING LIGHT POLE
. 0	M1-5A	COUNTY MARKER X	24 x 24	-	_	_	_	_	1	_	
	M6-4	DIRECTIONAL ARROW LEFT-RIGHT	21 x 21	_	_	_	_	_	1	_	
4-7	R14-1	TRUCK ROUTE	24 x 18	3.00	_	_	_	_	1	_	MOUNT ON EXISTING LIGHT POLE
	N-7 -	SPEED LIMIT 25	24 x 30	5.00					-		LISSIA ON EXISTING LIGHT FOLL

Ε HWY: STH 73 COUNTY: CLARK PROJECT NO: 8210-07-73 SHEET MISCELLANEOUS QUANTITIES

PERMANENT SIGNING

NOTE:	CTCN		4 D E	THEFT	CTMTI ATD	CTCN	GROUP NUMBER	
N() F -		REMINAL	$\Delta R =$				GROUP NUMBER	,

SIGN GROUP NUMBER	SIGN CODE	SIGN MESSAGE	SIGN SIZE W x H (INCHES)	637.2210 SIGNS TYPE II REFLECTIVE H	637.2230 SIGNS TYPE II REFLECTIVE F	634.0616 POSTS WOOD 4x6-INCH x 16-FT EACH	634.0618 POSTS WOOD 4x6-INCH x 18-FT EACH	634.0814 POSTS TUBULAR STEEL 2x2-INCH x 14-FT EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3 REMOVING SMALL SIGN SUPPORT EACH	REMARKS
4-9	J1-1		24 x 39	6.50	_	-	-	1	_	1	
	M2-1	JCT	21 x 15	-	_	_	-	_	1	_	
	M1-5A	COUNTY MARKER X	24 x 24	-	_	-	-	_	1	_	
5-1	R1-1	STOP	30 x 30	5.18	_	_	-	1	1	1	
5-2	R1-1	STOP	30 x 30	5.18	_	_	-	1	1	1	
5-3	J4-1		24 x 36	6.00	_	-	-	_	_	_	MOUNT ON EXISTING LIGHT POLE
	M3-1	NORTH Cardinal Route Marker	24 x 12	-	_	_	-	_	1	_	
	M1-6	STATE ROUTE MARKER 73	24 x 24	-	_	_	-	_	1	_	
5-4	R2-1	SPEED LIMIT 25	24 x 30	5.00	_	-	-	-	1	-	MOUNT ON EXISTING LIGHT POLE
5-5	R1-1	STOP	30 x 30	5.18	_	_	-	1	1	1	
5-6	R1-1	STOP	30 x 30	5.18	_	-	-	1	1	1	
5-7	W10-1	RAILROAD CROSSING	36 x 36	-	7.07	_	-	_	1	_	MOUNT ON EXISTING LIGHT POLE
6-1	R1-1	STOP	30 x 30	5.18	_	1	-	_	1	1	
6-2	R2-1	SPEED LIMIT 25	24 x 30	5.00	_	_	-	_	1	_	MOUNT ON EXISTING LIGHT POLE
6-3	W10-1	RAILROAD CROSSING	36 x 36	-	7.07	1	-	-	1	1	
6-4	R1-1	STOP	30 x 30	5.18	-	1	-	-	1	1	
6-5	R1-1	STOP	30 x 30	5.18	_	1	-	-	1	1	
6-6	R2-1	SPEED LIMIT 25	24 x 30	5.00	_	1	-	-	1	1	
6-7	R2-1	SPEED LIMIT 35	24 x 30	5.00	_	1	-	-	1	1	
7-1	J4-1		24 x 36	6.00	_	_	1	_	_	1	
	M3-3	SOUTH <i>Cardinal Route Marker</i>	24 x 12	-	_	_	-	_	1	_	
	M1-6	STATE ROUTE MARKER 73	24 x 24	-	_	_	-	-	1	_	
7-2	R1-1	STOP	30 x 30	5.18	_	1	-	-	1	1	
7-3	R2-1	SPEED LIMIT 35	24 x 30	5.00	_	1	-	-	1	1	
7-4	R1-1	STOP	30 x 30	5.18	_	1	-	-	1	1	
7-5	R1-1	STOP	30 x 30	5.18	-	1	-	-	1	1	
7-6	R2-1	SPEED LIMIT 35	24 x 30	5.00	_	1	-	-	1	1	
8-1	R1-1	STOP	30 x 30	5.18	_	1	-	-	1	1	
8-2	R1-1	STOP	30 x 30	5.18	_	1	-	-	1	1	
8-3	R2-1	SPEED LIMIT 45	24 x 30	5.00	-	1	-	-	1	1	
8-4	R2-1	SPEED LIMIT 35	24 x 30	5.00	-	1	-	_	1	1	
			TOTAL 0010	287.46	14.14	31.00	7.00	6.00	69.00	44.00	

PROJECT NO: 8210-07-73 HWY: STH 73 COUNTY: CLARK MISCELLANEOUS QUANTITIES SHEET E

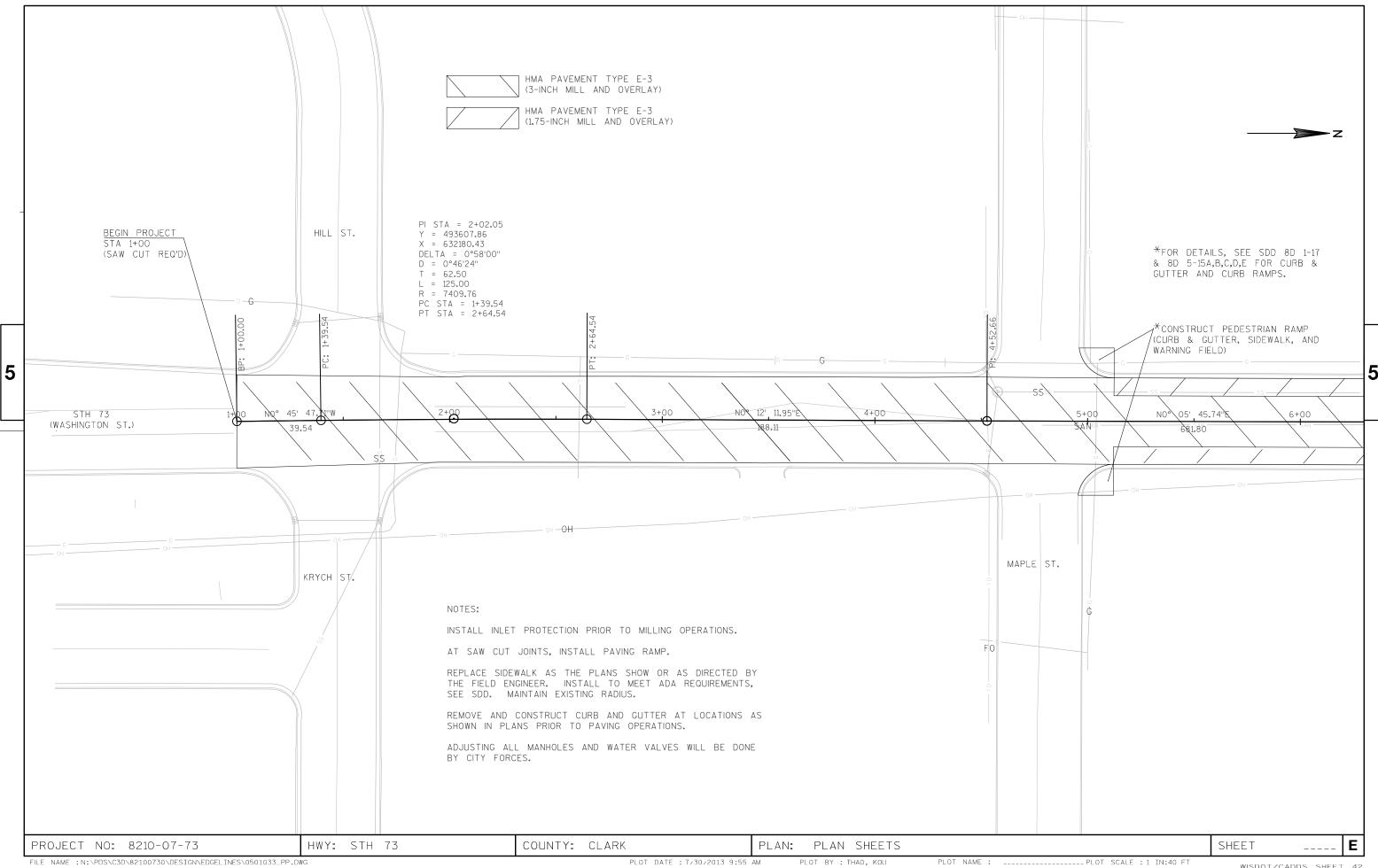
JT	DAVEMENT	DA\/FMENT	DAV/EMENT	

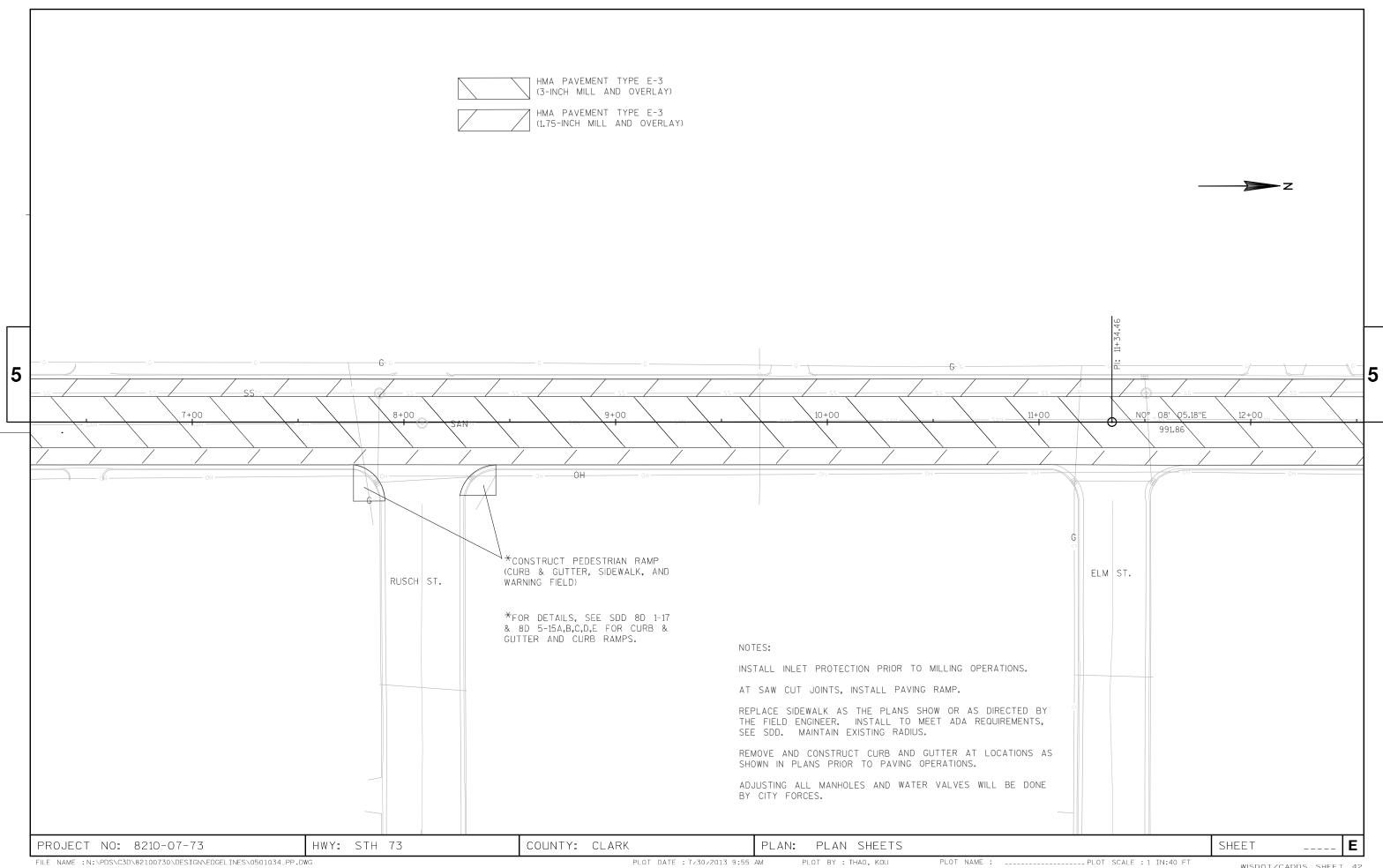
CATEGORY	CTATTON TO CTATTON	LOCATION	PAVEMENT MARKING EPOXY 4-INCH 646.0106	PAVEMENT MARKING EPOXY 8-INCH 646.0126	PAVEMENT MARKING EPOXY 12-INCH 646.0136	PAVEMENT MARKING RAILROAD CROSSINGS EPOXY 647.0110	PAVEMENT MARKING ARROWS EPOXY TYPE 2 647.0166	PAVEMENT MARKING WORDS EPOXY 647.0356	PAVEMENT MARKING PARKING STALL EPOXY 647.0656	PAVEMENT MARKING CROSSWALK EPOXY 6-INCH 647.0766	DEMOKS
CATEGORY	STATION TO STATION	LOCATION	LF 070	LF	LF	EACH	EACH	EACH	<u>LF</u>	LF	REMARKS
0010 0010	1+95 - 5+20 5+20 - 11+68	STH 73 STH 73	970		-	7.	-	-	-		CL - YELLOW
0010	11+68 - 24+00	STH 73	811 275	_	_		_	_	_	_	CL - YELLOW CL - YELLOW
0010	24+00 - 28+33	STH 73	533	_	_	_	_	_	_	_	CL - YELLOW
0010	28+90 - 32+20	STH 73	660	_	_	_	_	_	_	_	CL - YELLOW
0010	32+30 - 38+00	STH 73	708	_	_	_	_	_	_	_	CL - YELLOW
0010	38+00 - 47+71	STH 73	250	JI	_	_	_	_	_	_	CL - YELLOW
0010	46+00 - 47+71	STH 73	171	_	_	_	_	_	_	0.757	EL - WHITE
0010	1+95 - 2+95	STH 73		100	_	_	_	_	_	1	CHANNELIZE - WHITE
0010	3+25	STH 73	_	100	4	_	_	_			MEDIAN - YELLOW
0010	3+50	STH 73	_	_	8	_	_	_	_	1.50	MEDIAN - YELLOW
0010	3+75	STH 73	_	_	12	_	_	_	_	-	MEDIAN - YELLOW
0010	4+00	STH 73	_	_	9	_	_	_	_	1 2	MEDIAN - YELLOW
0010	4+25	STH 73	_	_	5	_	_	_	_	_	MEDIAN - YELLOW
0010	31+50	STH 73	_	_	_	1	_	_	_	2	SEE SDD
0010	33+00	STH 73	_	_	_	1	_	_	_	- 12	SEE SDD
0010	2+18	STH 73	_	_	_	_	1	_	_	-	LEFT TURN LANE
0010	2+59	STH 73	_	_	_	_	_	1	_	-	"ONLY" LT TURN
0010	1+74	KRYCH	_	_	_	_	_	_	_	125	WHITE
0010	4+55 - 5+06	Maple & STH 73	_	_	_	_	_	_	_	175	WHITE
0010	8+10	RUSCH	_	_	_	_	_	_	_	109	WHITE
0010	11+40	ELM	_	_	_	_	_	_	_	92	WHITE
0010	14+60	LAWRENCE	_	_	_	_	_	_	_	68	WHITE
0010	16+86 - 17+38	PROSPECT & STH 73	_	_	_	_	_	_	_	173	WHITE
0010	20+96 - 21+63	STANLEY & STH 73	_	_	_	_	_	_	_	409	WHITE
0010	24+62 - 25+22	SCHOOL & STH 73	_	_	_	_	_	_	_	381	WHITE
0010	28+33 - 28+90	MAIN & STH 73	_	_	_	_	_	_	_	379	WHITE
0010	31+31 - 32+00	SOO & STH 73	1.5	_	_	_	_	_	_	217	WHITE
0010	34+12 - 34+81	BIRCH	(4)	_	_	_	_	_	_	151	WHITE
0010	38+46	FRANKLIN	12	_	_	_	_	_	_	80	WHITE
0010	41+91 - 42+52	GRANT	-	_	-	-	-	-	-	191	WHITE
		TOTAL 0010	4378	100	38	2	1	1	0	2550	
0020	21+75 - 24+42	STH 73, RT & LT	() <u>=</u> ,	-		_	_	-	452	_	WHITE
0020	25+42 - 28+11	STH 73, RT & LT	4.2	_		-	-	-	616	-	WHITE
0020	29+11 - 34+12	STH 73, RT & LT	(*)	-		-	-	-	718	-	WHITE
		TOTAL 0020	0	0	0	0	0	0	1786	0	
		PROJECT TOTAL	4378	100	38	2	1	1	1786	2550	

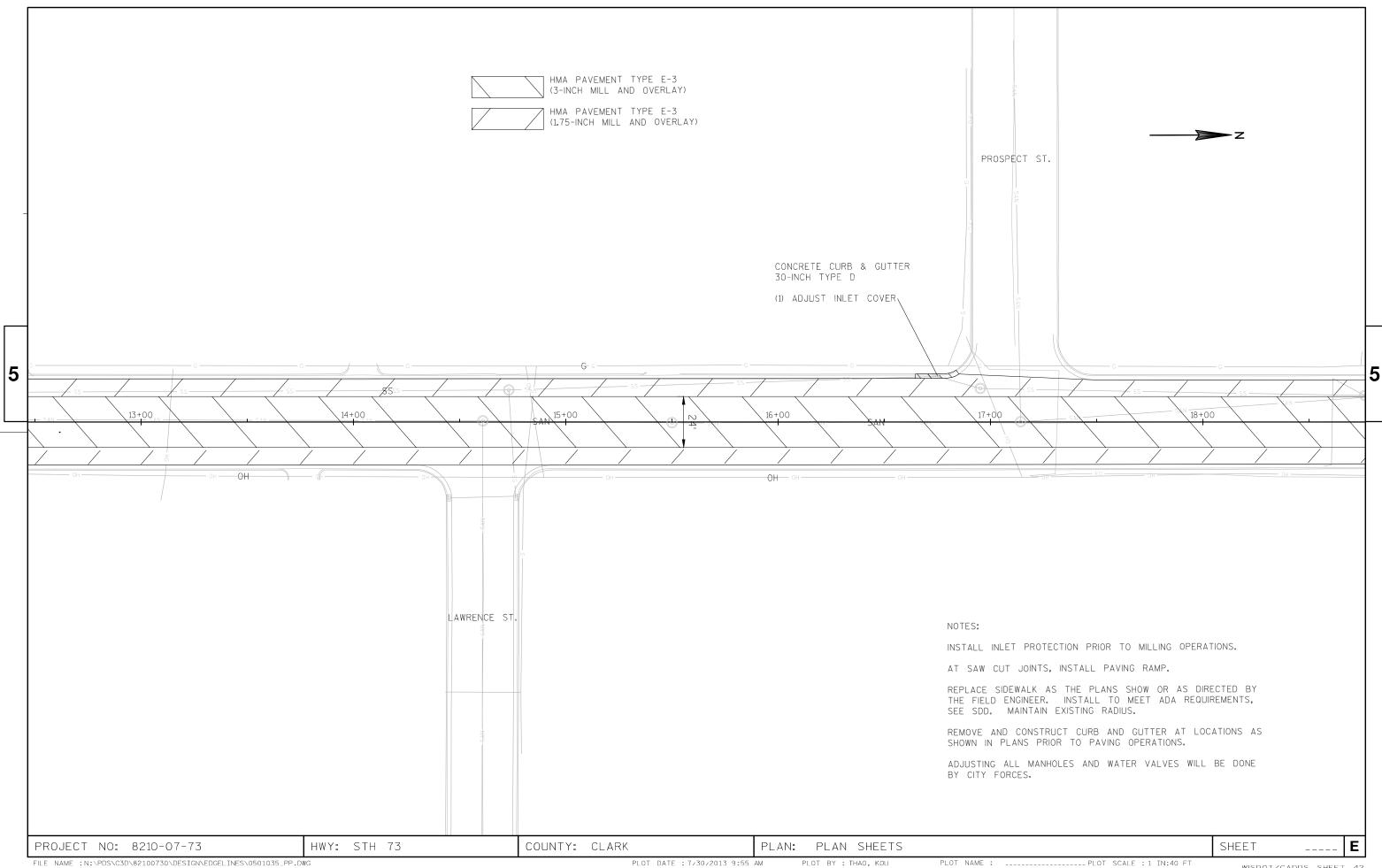
PAVEMENT MARKING

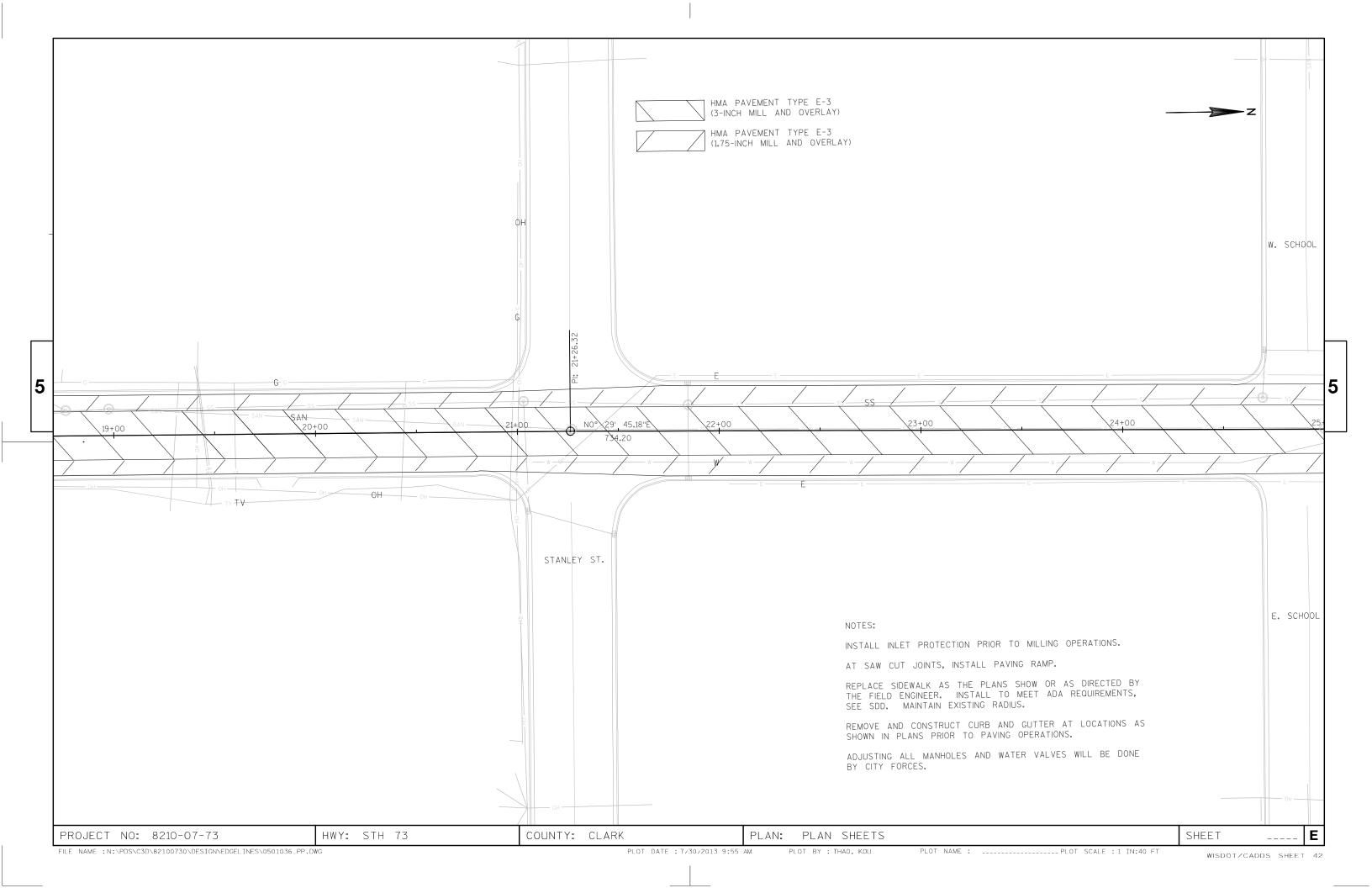
PROJECT NO: 8210-07-73 HWY: STH 73 COUNTY: CLARK MISCELLANEOUS QUANTITIES SHEET E

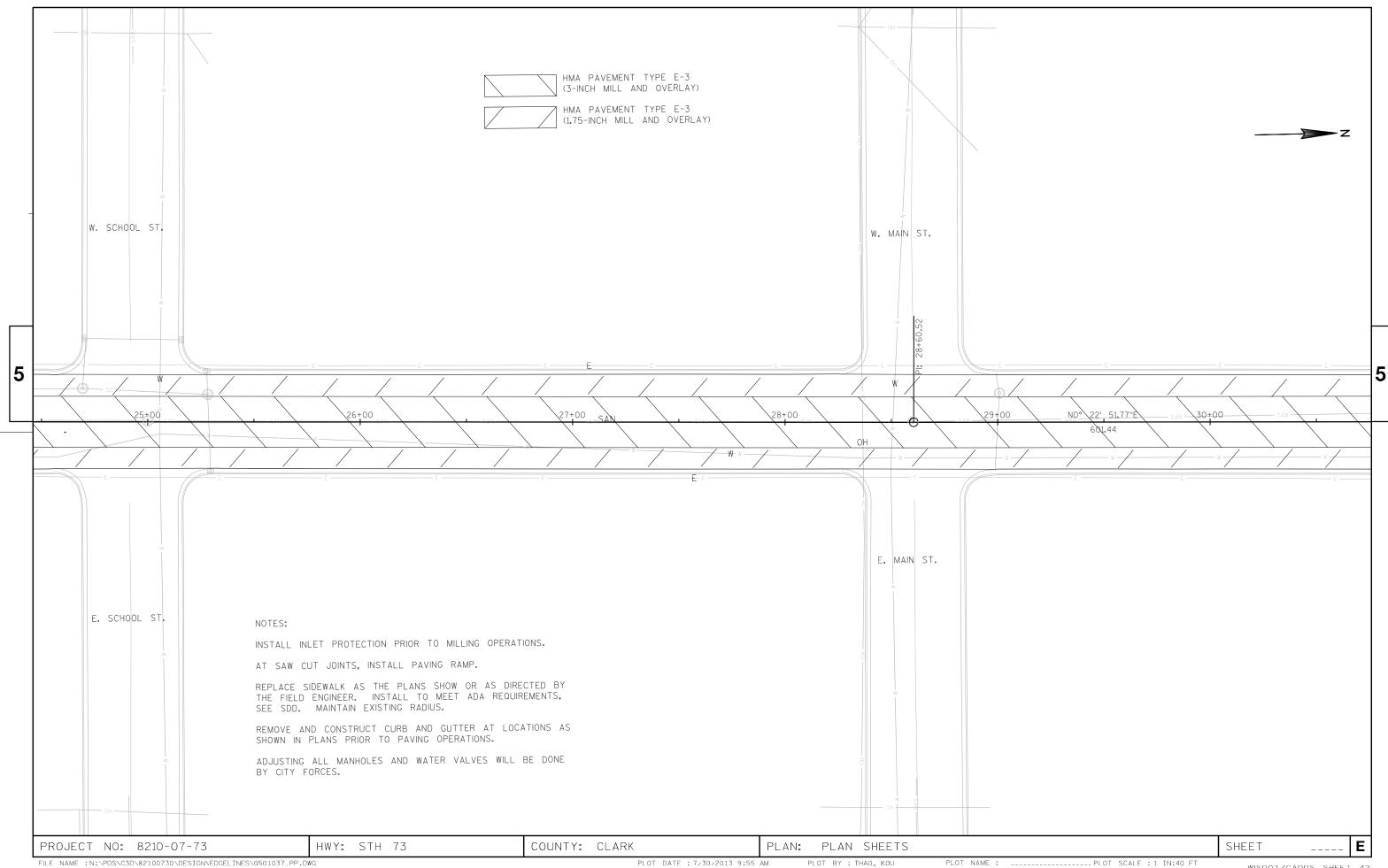
CAT				STAKING CURB GUTTER AND CURB & GUTTER 650.5500	CONSTRUCTION STAKING RESURFACING REFERENCE 650.8000	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL 650.9910					
	EGORY	STATION TO STATION	LOCATION	<u>LF</u>	LF	LS	REMA	ARKS			
ſ	010	5+00	STH 73, RT & LT	48	_	_	C & G REP	LACEMENT			
	010	7+80	STH 73, RT	24	_	_	C & G REP				
	010	8+35	STH 73, RT	24	_	-	C & G REP				
	010	16+75	STH 73, LT	24	_	-	C & G REP				
	010	31+60	STH 73, LT	30	_	-	C & G REP				
	010	34+40	STH 73, LT	24	-	-	C & G REP				
	010	38+85	STH 73, LT	24	_	-	C & G REP	LACEMENT			
	010	45+35	STH 73, LT	24	-	-	C & G REP	LACEMENT			
C	010	1+00 - 47+71	PROJECT		4800	1					
			TOTAL 0010	222	4800	1					
CATEGORY STATION TO STATION TO 0010 1+00 - 4		LOCATION PROJECT	1 REMARKS		CATEGORY 0010	STATION TO		LOCATION PROJECT TOTAL 0010		SPV.0105 LS REMARKS 1 1	5
	<u>:</u>	TOTAL 0010 SAWING ASPHALT	1				SAWING CO			-	
		690.015	0					690.	0250		
CATEGORY STAT	ION	LOCATION L	F REMARKS		CATEGOR	RY STATION	LOCATION		LF	REMARKS	
0010 1+	00 9	STH 73 4	4		0010	F : 00	CTU 72 P	т 0 іт	10	C 9 C DEDI ACEMENT	
0010 47-		STH 73 3:			0010 0010	5+00 7+80	STH 73, R STH 73, R		10 5	C & G REPLACEMENT C & G REPLACEMENT	
			_		0010	8+35	STH 73, R		5	C & G REPLACEMENT	
		TOTAL 0010 7	 5		0010	11+15	STH 73, R		5	C & G REPLACEMENT	
					0010	11+60	STH 73, R		5	C & G REPLACEMENT	
					0010	16+75	STH 73, L		5	C & G REPLACEMENT	
SPECIAL 01. RE	HEATING F	HMA PAVEMENT LONGITUDINA	AL JOINTS SPECIAL		0010	31+60	STH 73, L		5	C & G REPLACEMENT	
			SDV 0170		0010	34+40	STH 73, L		5	C & G REPLACEMENT	
CATEGORY STATION TO	STATION	LOCATION	SPV.0170 STA REMARKS		0010	38+85	STH 73, L		5	C & G REPLACEMENT	
				-	0010	45+35	STH 73, L	Т	5	C & G REPLACEMENT	
0010 1+00 -	47+71		47 				TOTAL 00)10	55		
		TOTAL 0010	47								







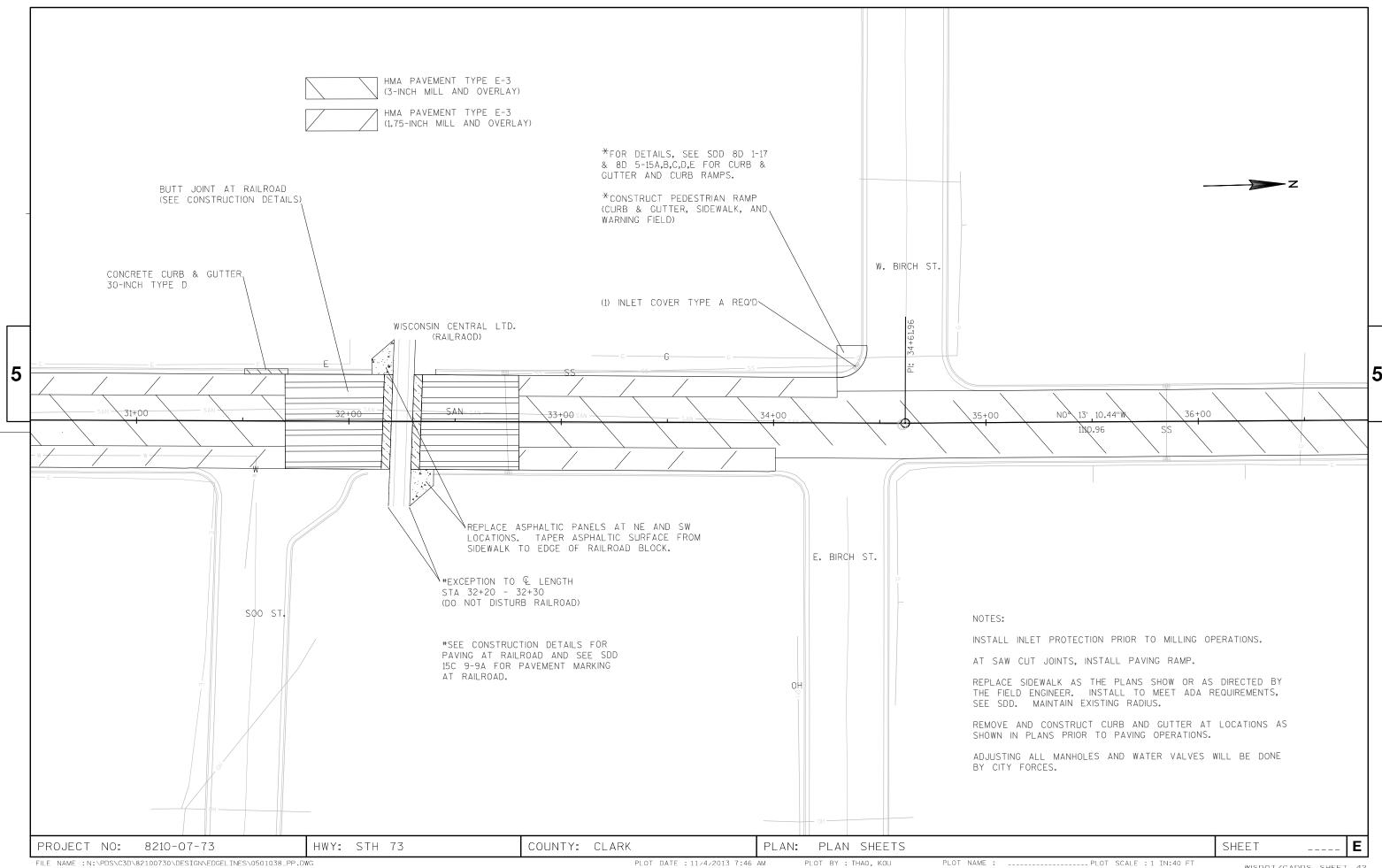




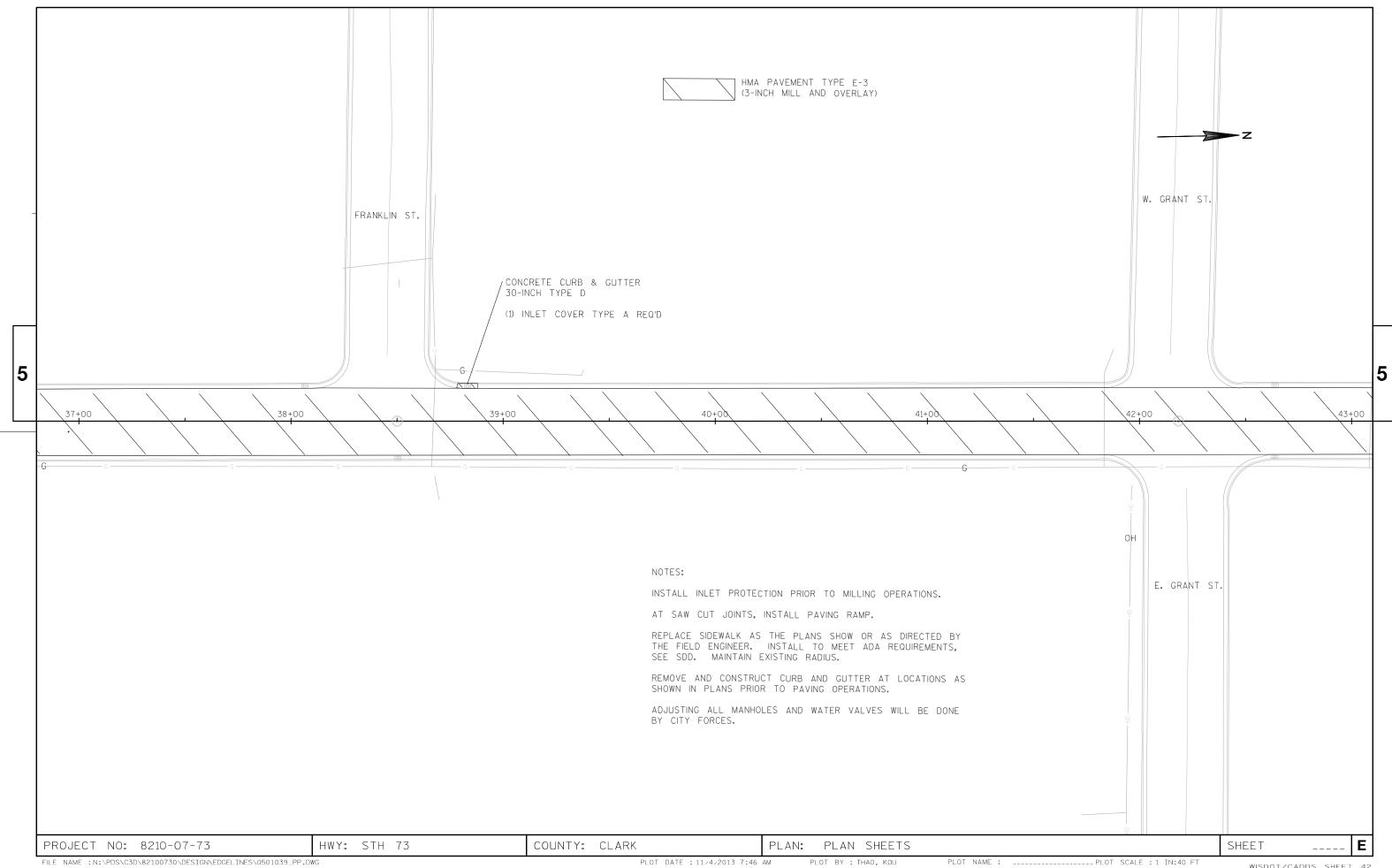
FILE NAME :N:\PDS\C3D\82100730\DESIGN\EDGELINES\0501037_PP.DWG

PLOT DATE : 7/30/2013 9:55 AM

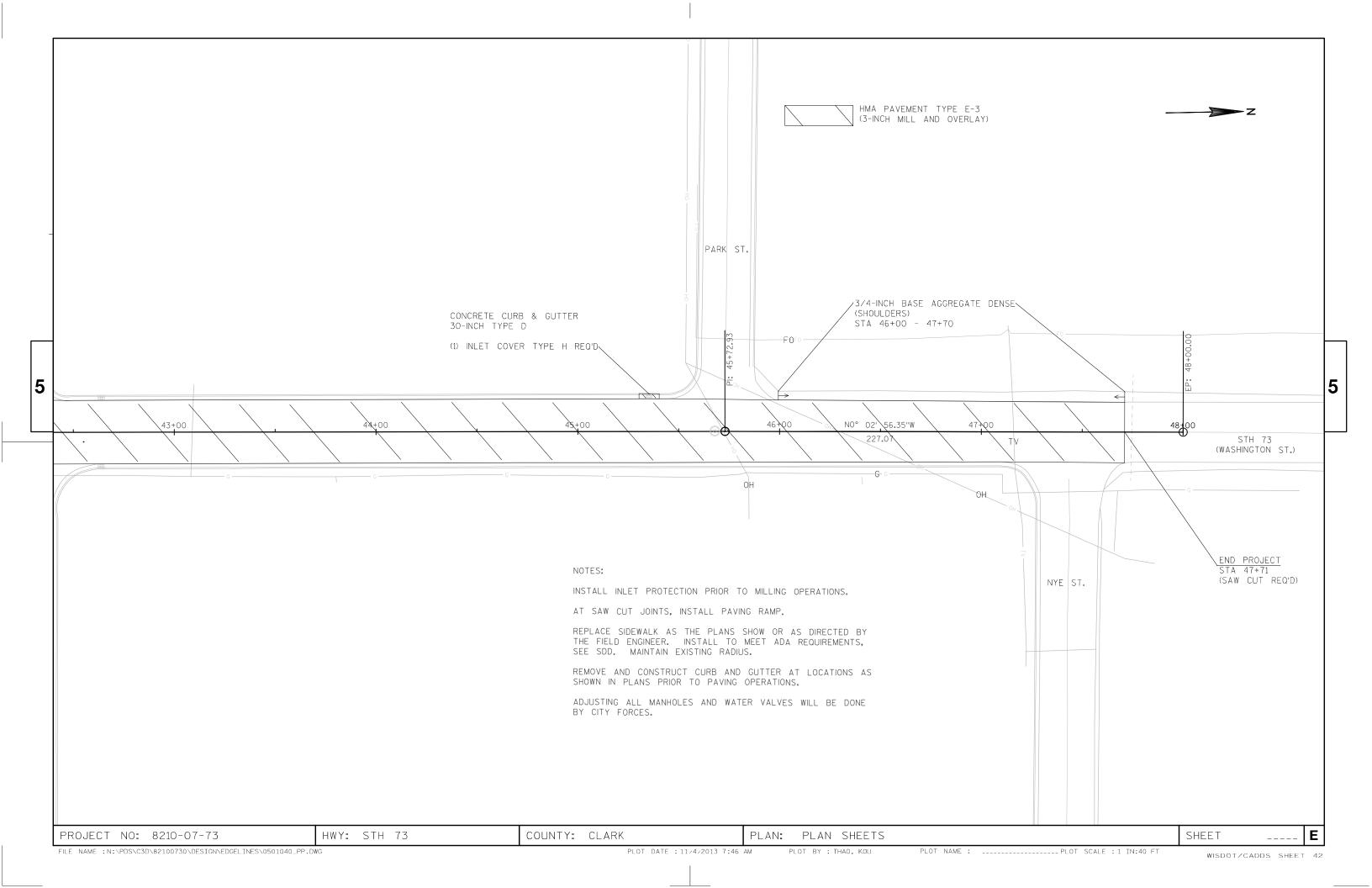
WISDOT/CADDS SHEET 42



FILE NAME ; N:\PDS\C3D\82100730\DESIGN\EDGELINES\0501038_PP.DWG



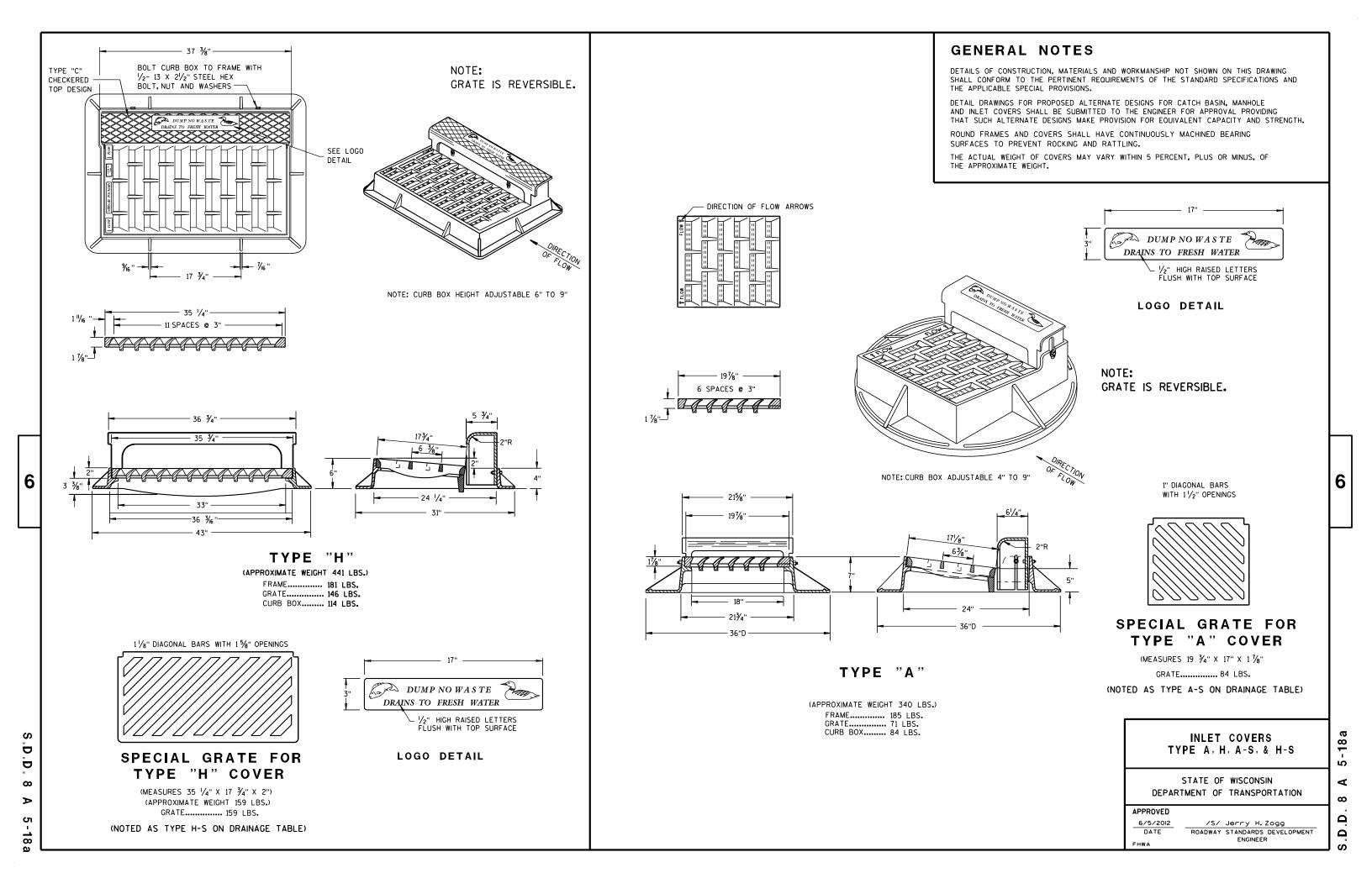
WISDOT/CADDS SHEET 42

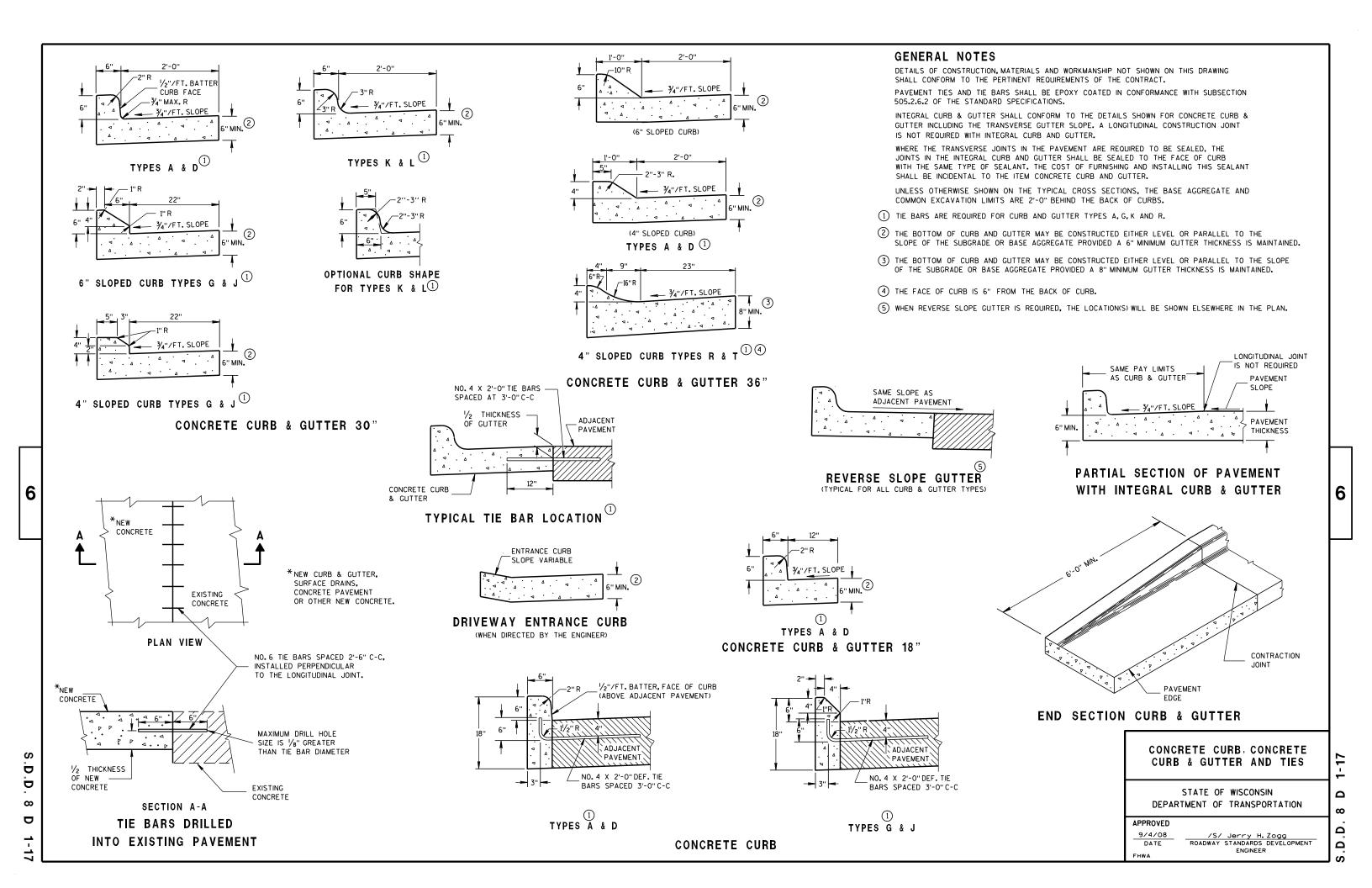


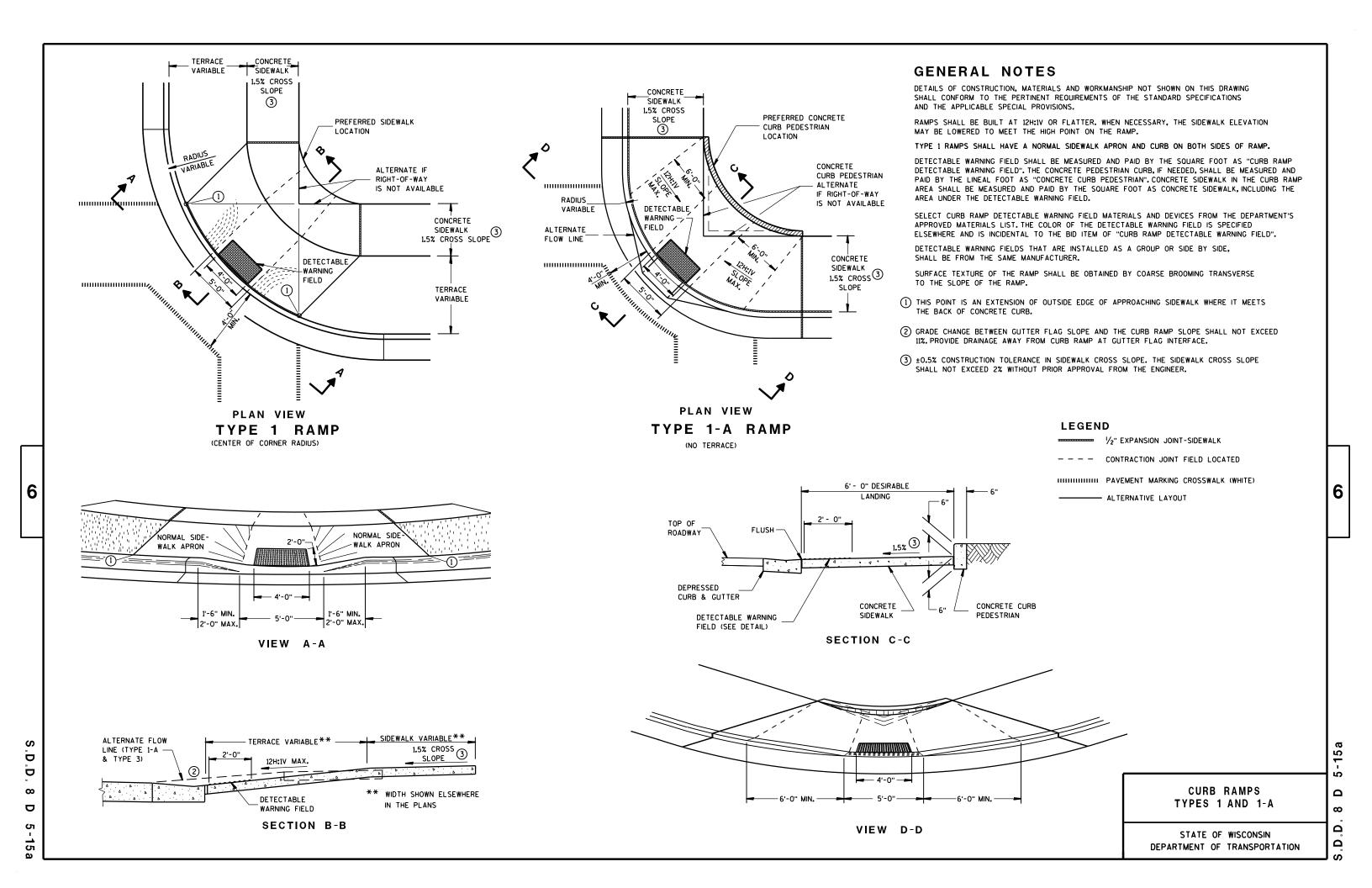
Standard Detail Drawing List

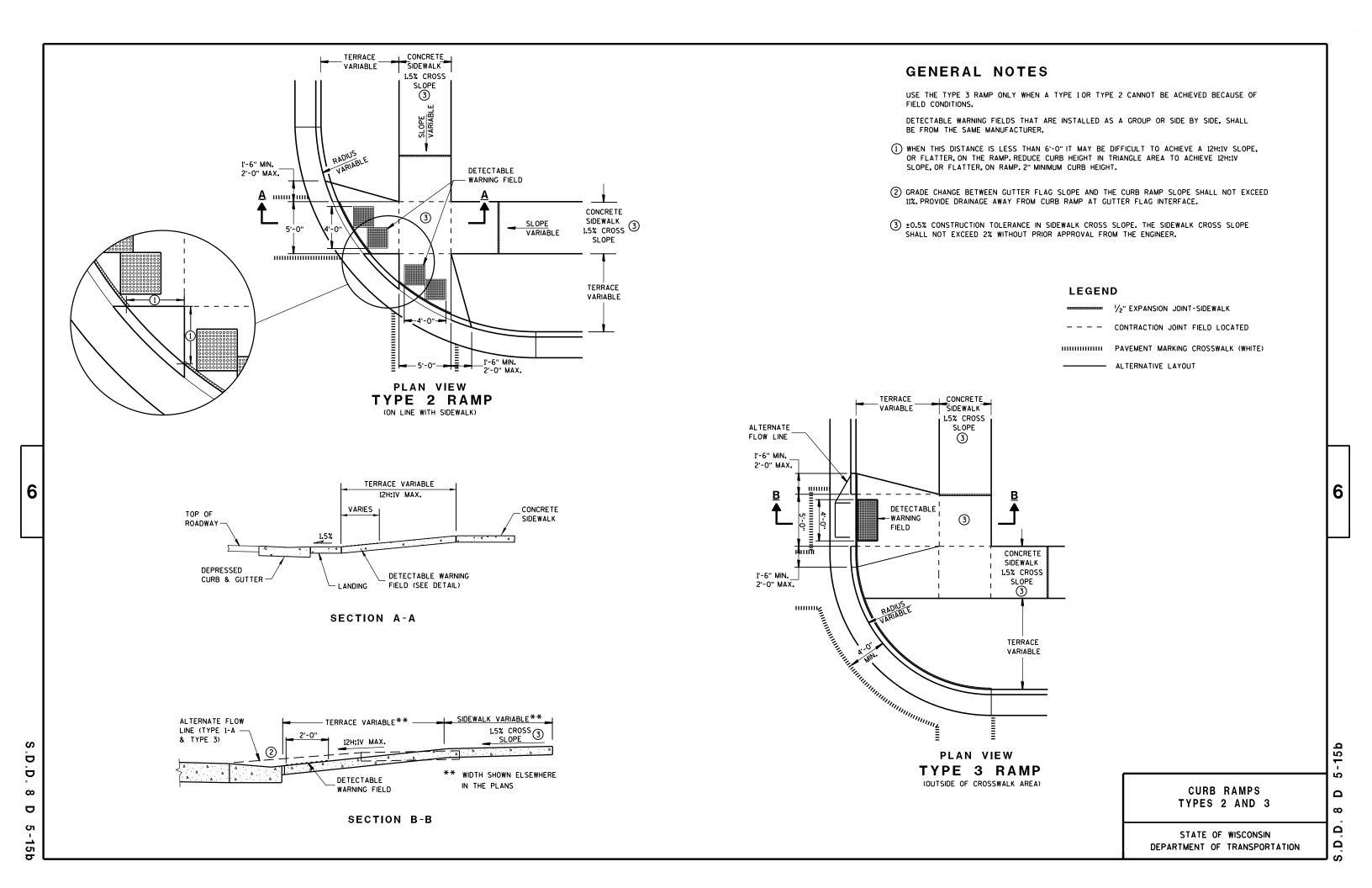
08A05-18A	INLET COVERS TYPE A, H, A-S, & H-S
08D01-17	CONCRETE CURB, CONCRETE CURB AND GUTTER AND TIES
08D05-15A	CURB RAMPS TYPES 1 AND 1-A
08D05-15B	CURB RAMPS TYPES 2 AND 3
08D05-15C	CURB RAMPS TYPES 4A AND 4A1
08D05-15D	CURB RAMPS TYPE 4B AND 4B1
08D05-15E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08E10-02	INLET PROTECTION TYPE A, B, C AND D
15C05-01	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C07-12B	PAVEMENT MARKING WORDS
15C07-12C	PAVEMENT MARKING ARROWS
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C08-16B	PAVEMENT MARKING (INTERSECTIONS)
15C08-16C	PAVEMENT MARKING (CLIMBING LANE & PASSING LANE)
15C08-16D	PAVEMENT MARKING (CLIMBING LANE & PASSING LANE)
15C08-16E	PAVEMENT MARKING (LEFT TURN LANE)
15C08-16F	PAVEMENT MARKING (ISLANDS)
15C09-09A	SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD-HIGHWAY GRADE CROSSINGS
15C12-03	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15D28-01	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-01	TRAFFIC CONTROL. SIDEWALK CLOSURE

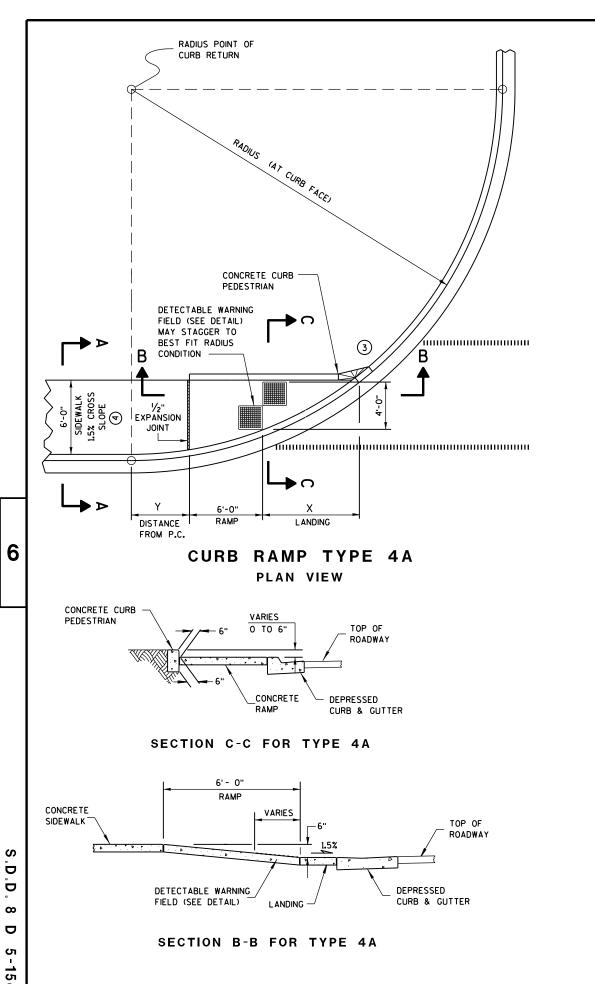
6

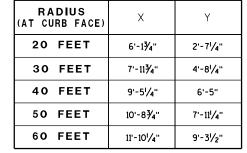












GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE.

4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS

SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

ISOMETRIC VIEW FOR TYPE 4A

ISOMETRIC VIEW FOR TYPE 4A1

₩ 1/2" EXPANSION JOINT-SIDEWALK

HIHIHIHIH PAVEMENT MARKING CROSSWALK (WHITE)

CONTRACTION JOINT FIELD LOCATED

CURB RAMPS

TYPES 4A AND 4A1

STATE OF WISCONSIN

DEPARTMENT OF TRANSPORTATION

LEGEND

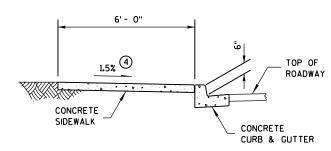
OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

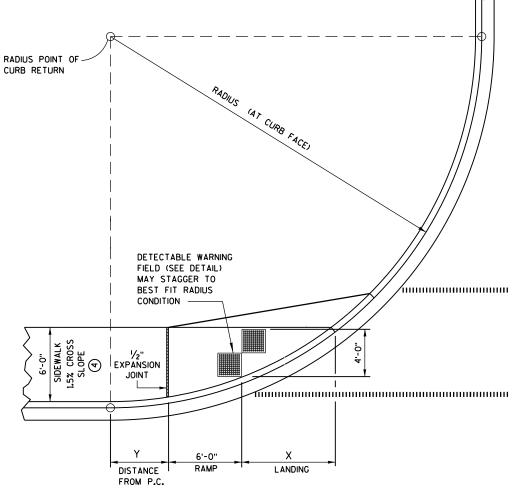
(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

SHALL BE FROM THE SAME MANUFACTURER.

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A-A FOR TYPE 4A

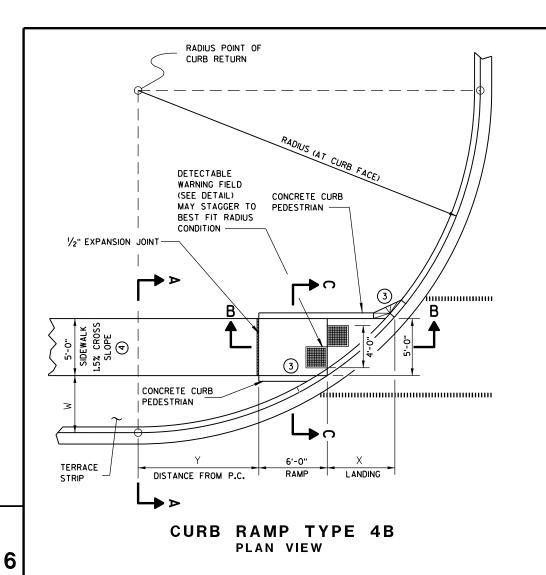


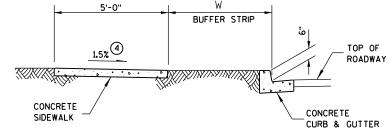
CURB RAMP TYPE 4A1
PLAN VIEW

15c

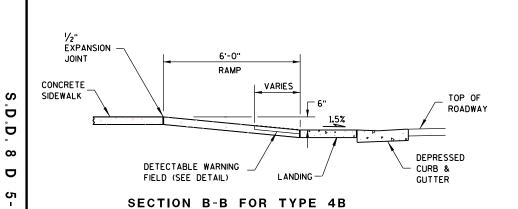
6

D.D. 8 D 5





SECTION A-A FOR TYPE 4B



LEGEND

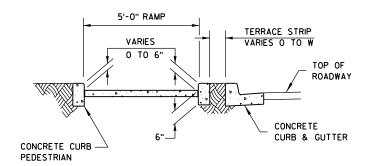
1/2" EXPANSION JOINT-SIDEWALK

---- CONTRACTION JOINT FIELD LOCATED

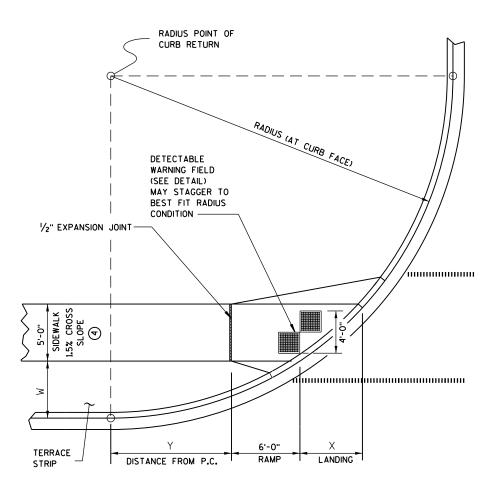
HIHIHIHIH PAVEMENT MARKING CROSSWALK (WHITE)

RADIUS	W =	3' - 0"	W =	4' - Ø"	W =	5′ - 0"	W =	6′ - Ø"	W = 7' - ∅"		
(AT CURB FACE)	Х	Y	X	Y	Х	Y	X	Y	X	Y	
20 FEET	5'-51/2"	4'-6'/2"	4'-81/2"	6'-0"	4'-1"	7'-2¾"	3'-7"	8'-3 ¹ /2"	3'-11/2"	9'-21/2"	
30 FEET	7'-3¾"	7'-1"	6'-51/2"	8'-11'/2"	5'-91/4"	10'-7"	5'-21/2"	12'-0"	4'-8¾"	13'-3'/4"	
40 FEET	8'-91/2"	9'-21/2"	7'-10"	11'-5'/4"	7'-1"	13'-41/2"	6'-5¾"	15'-¾"	5'-111/2"	16'-7'/4"	
50 FEET	10'-¾"	11'-¾"	9'-1/4"	13'-7'/4"	8'-21/2"	15'-91/2"	7'-61/2"	17'-9"	6'-11¾"	19'-6'/4"	
60 FEET	11'-2'/2"	12'-8¾"	10'-¾"	15'-61/2"	9'-21/4"	17'-11¾"	8'-5 ¾ "	20'-1¾"	7'-101/2"	22'-11/2"	

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION C-C FOR TYPE 4B



CURB RAMP TYPE 4B1 **PLAN VIEW**

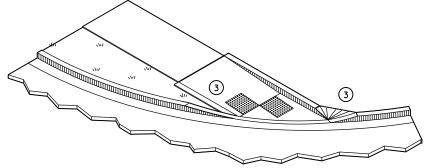
GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

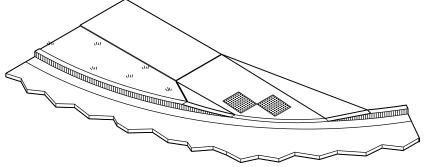
RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE. SHALL BE FROM THE SAME MANUFACTURER.

- (3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

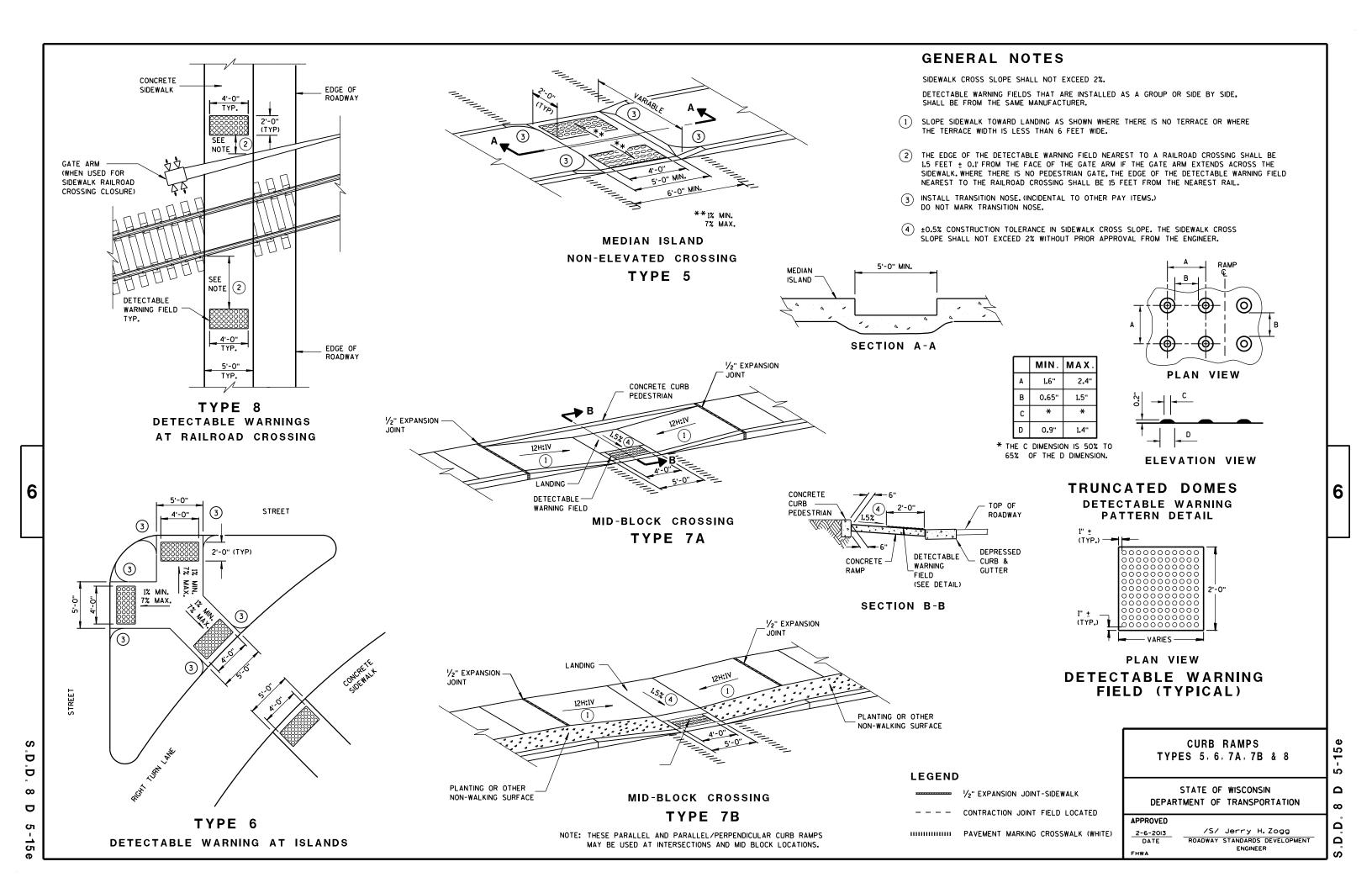
CURB RAMPS TYPE 4B AND 4B1

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION 6

-15d 2 Ω ∞

Ω

Ω







INLET PROTECTION, TYPE A

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- 1) FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- (2) FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- (3) FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE, THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION TYPE A, B, C, AND D

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

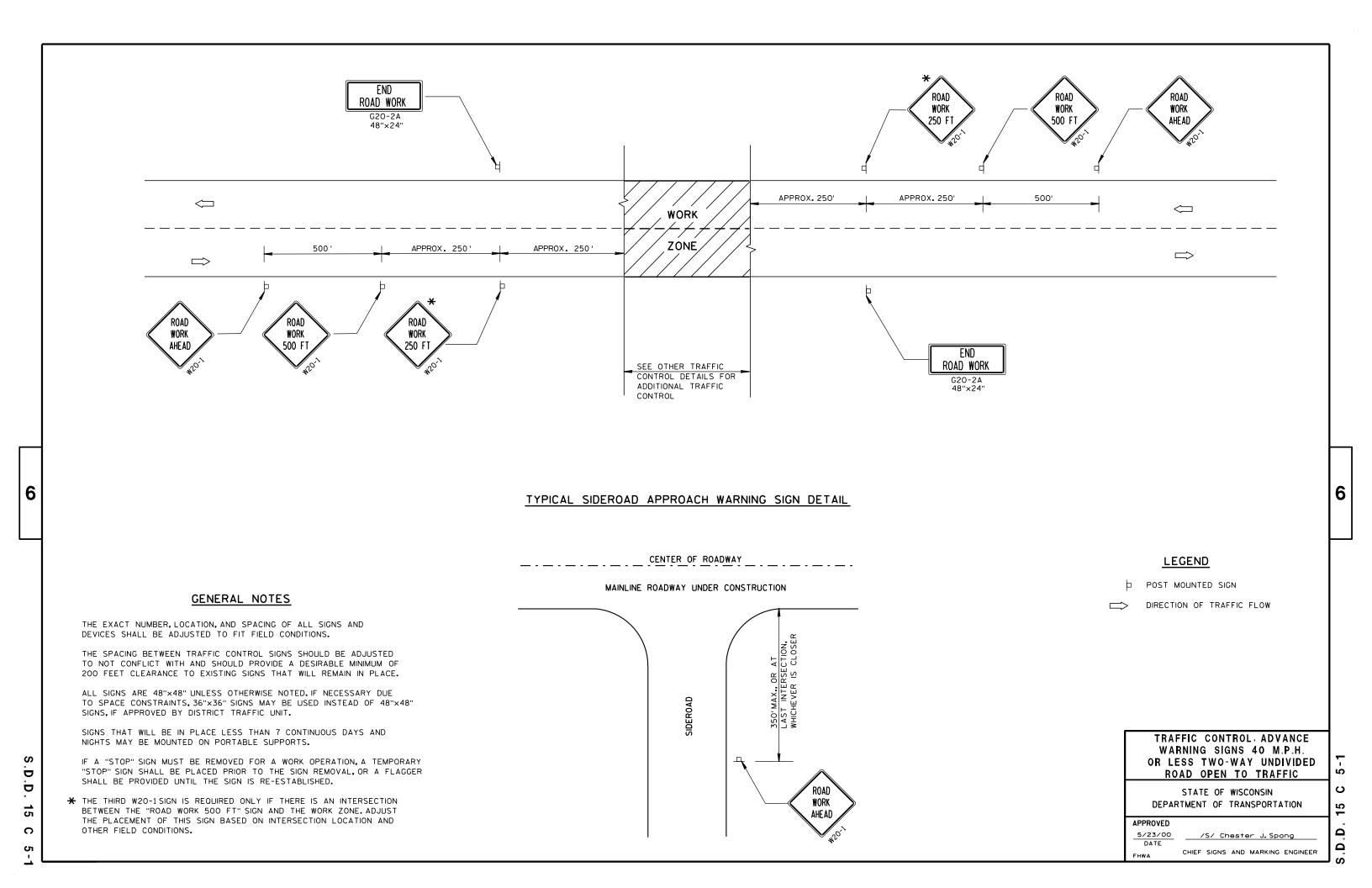
10/16/02

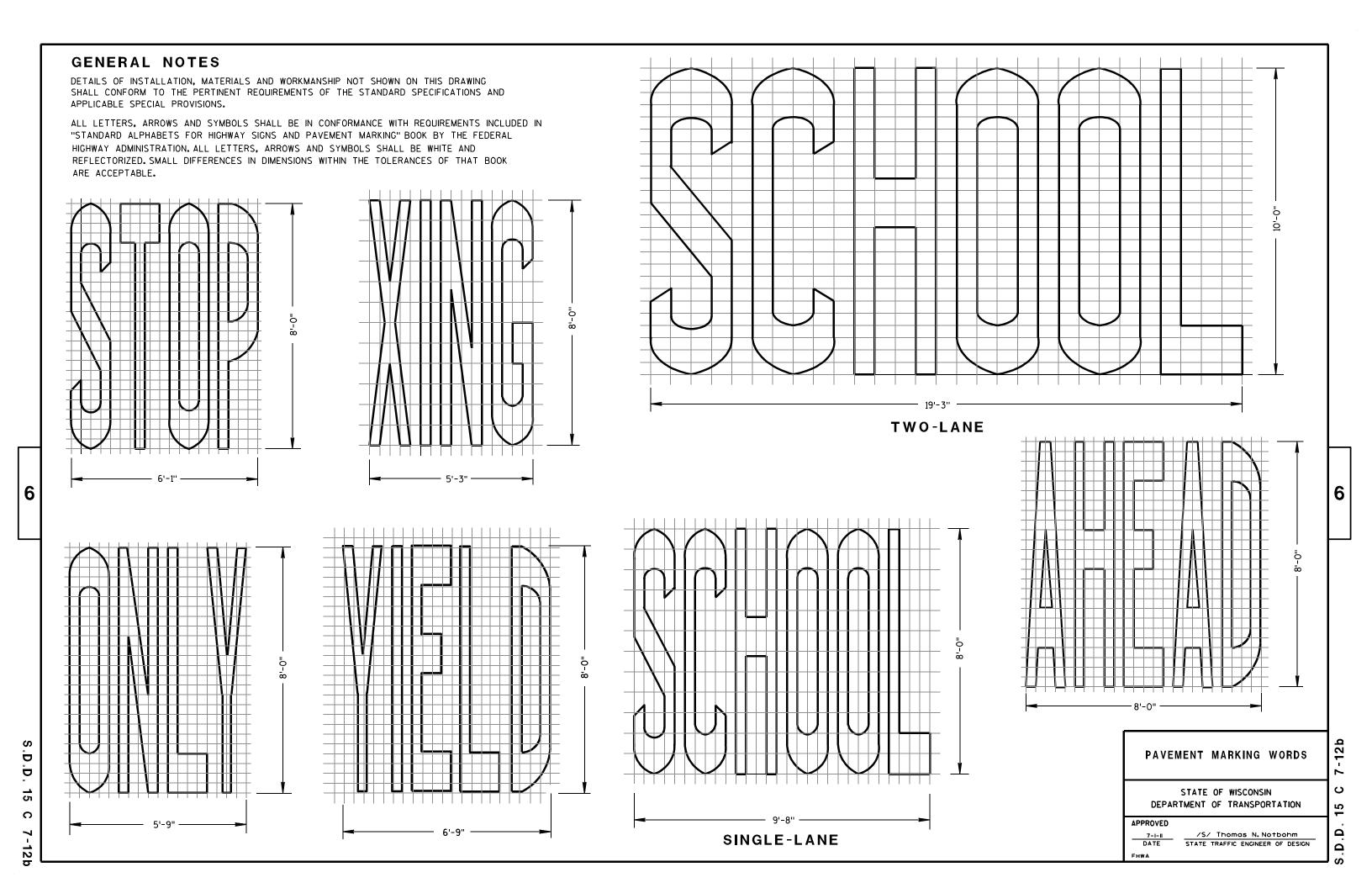
/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER 6

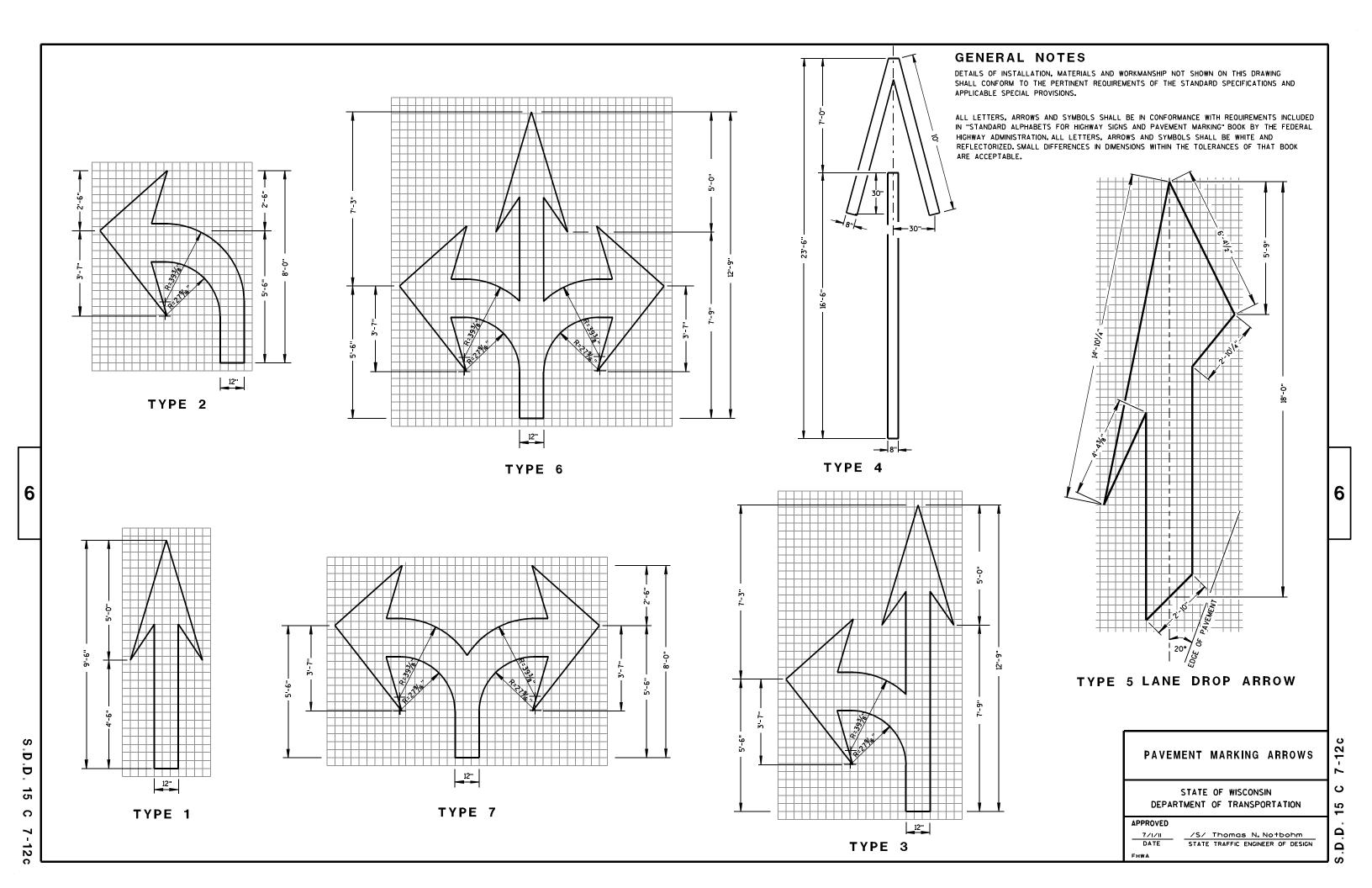
0

ш

 ∞

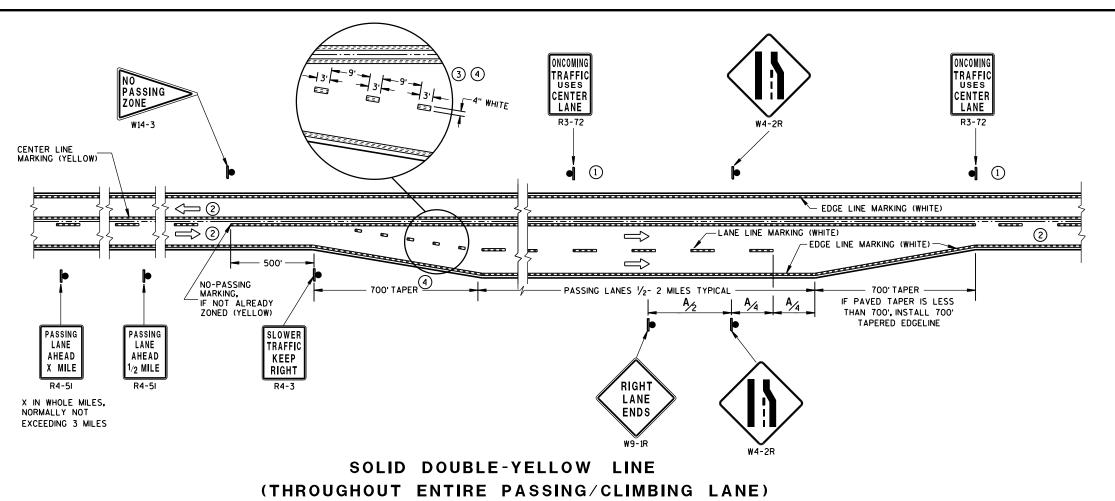










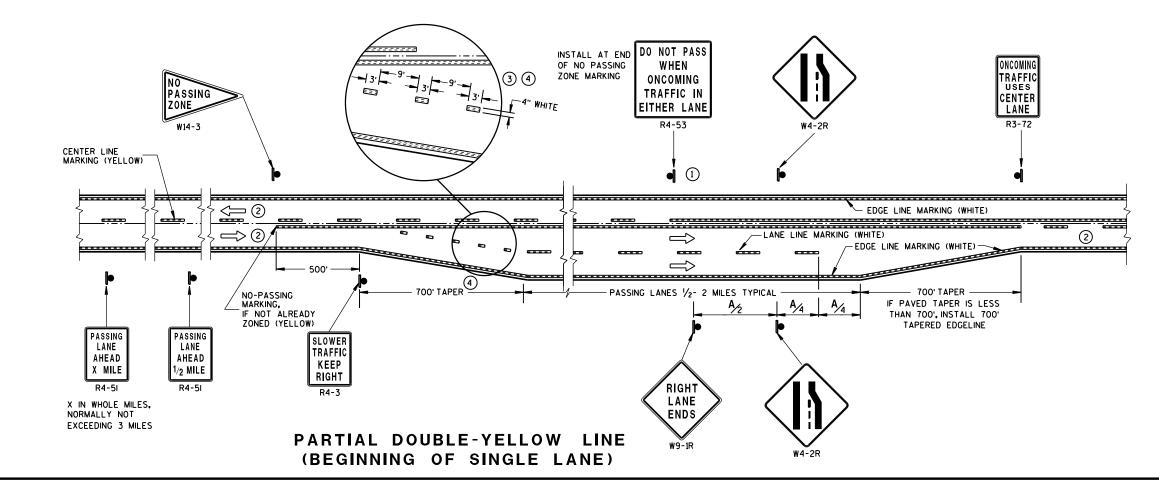


6

D

Ö

15



GENERAL NOTES

- $\ensuremath{\bigcirc}$ Sign shall be repeated at 1 mile increments or at the discretion of the regional traffic engineer.
- (2) THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- 3 THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3'LINE 9'GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL (>>) SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	750
50	850
55	950

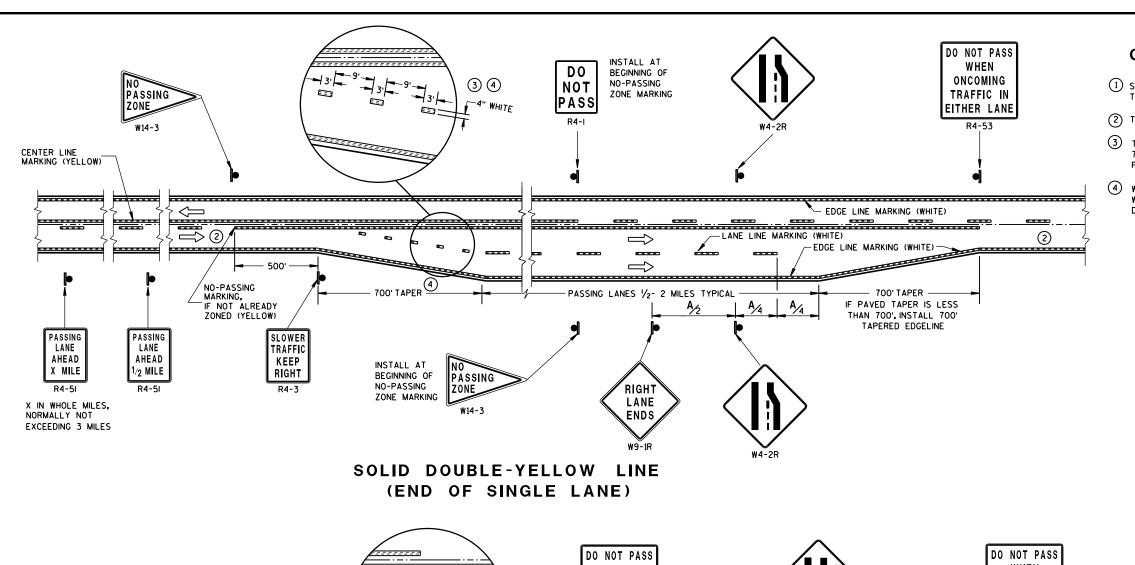
PAVEMENT MARKING & SIGNING
(CLIMBING LANE &
PASSING LANE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

5 C 8-16c

.D.D. 15 C



6

Ö

 $^{\circ}$

C

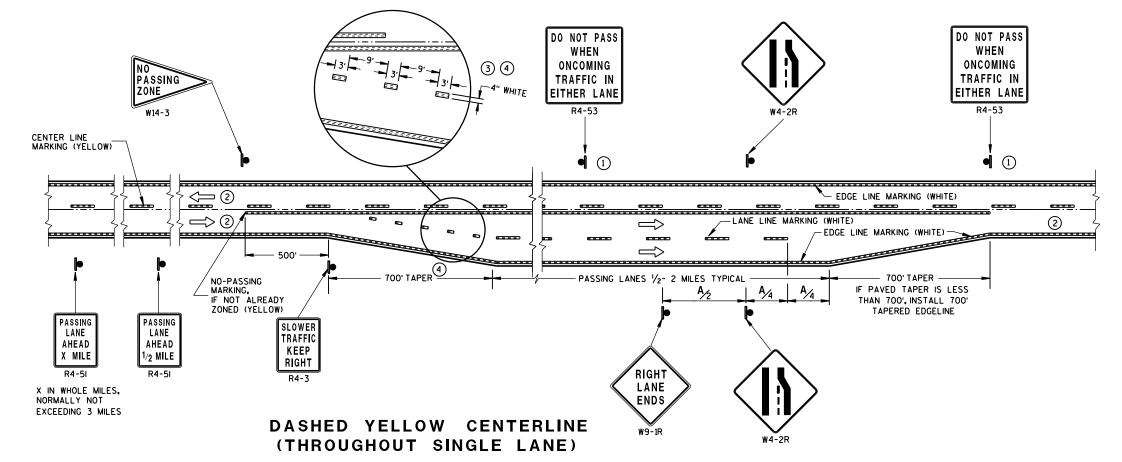
GENERAL NOTES

- \bigodot Sign shall be repeated at $\ensuremath{\mathcal{V}}_2$ mile increments or at the discretion of the regional traffic engineer.
- (2) THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3'LINE 9'GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL () SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	750
50	850
55	950

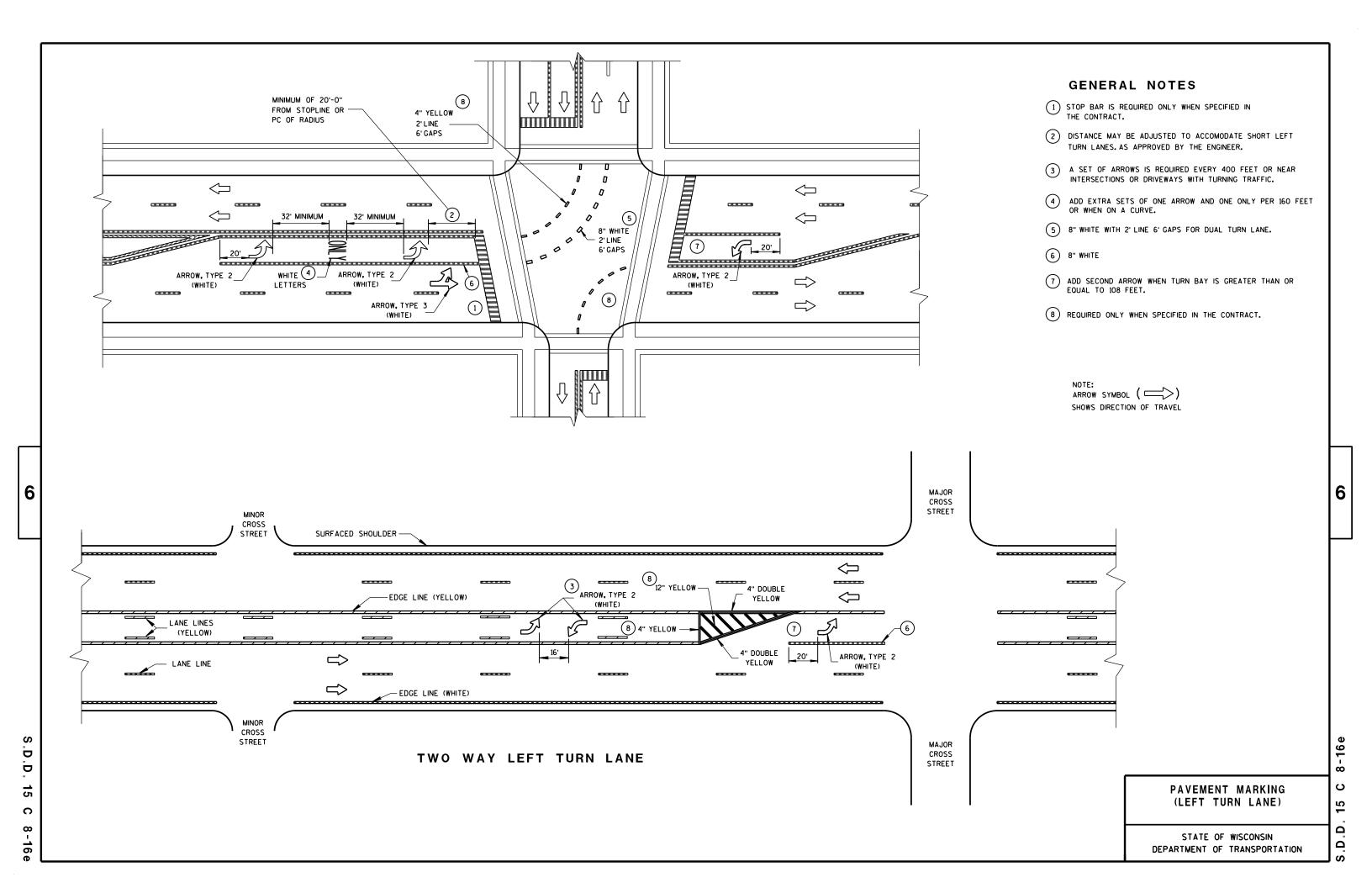


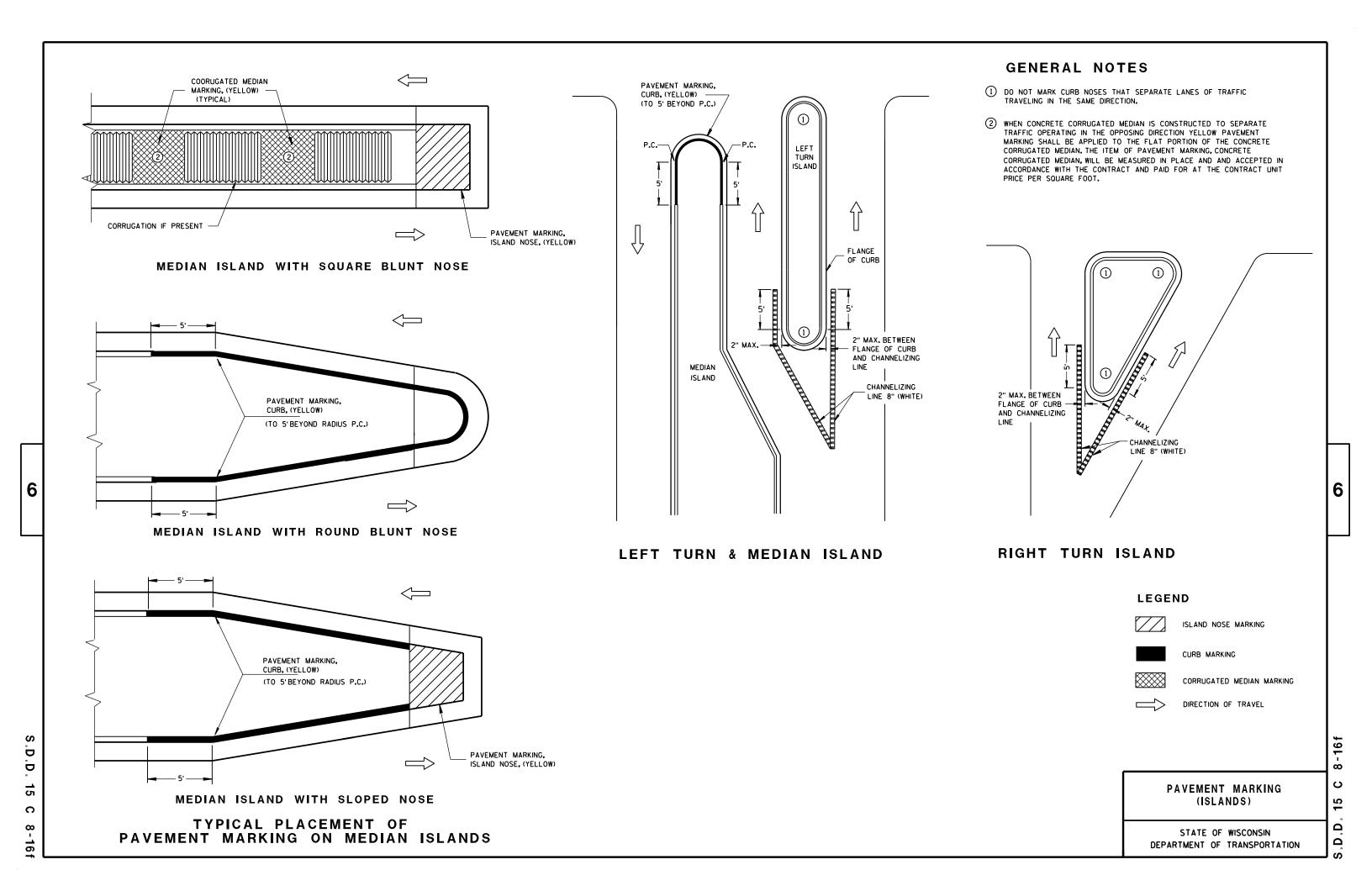
PAVEMENT MARKING & SIGNING (CLIMBING LANE & PASSING LANE)

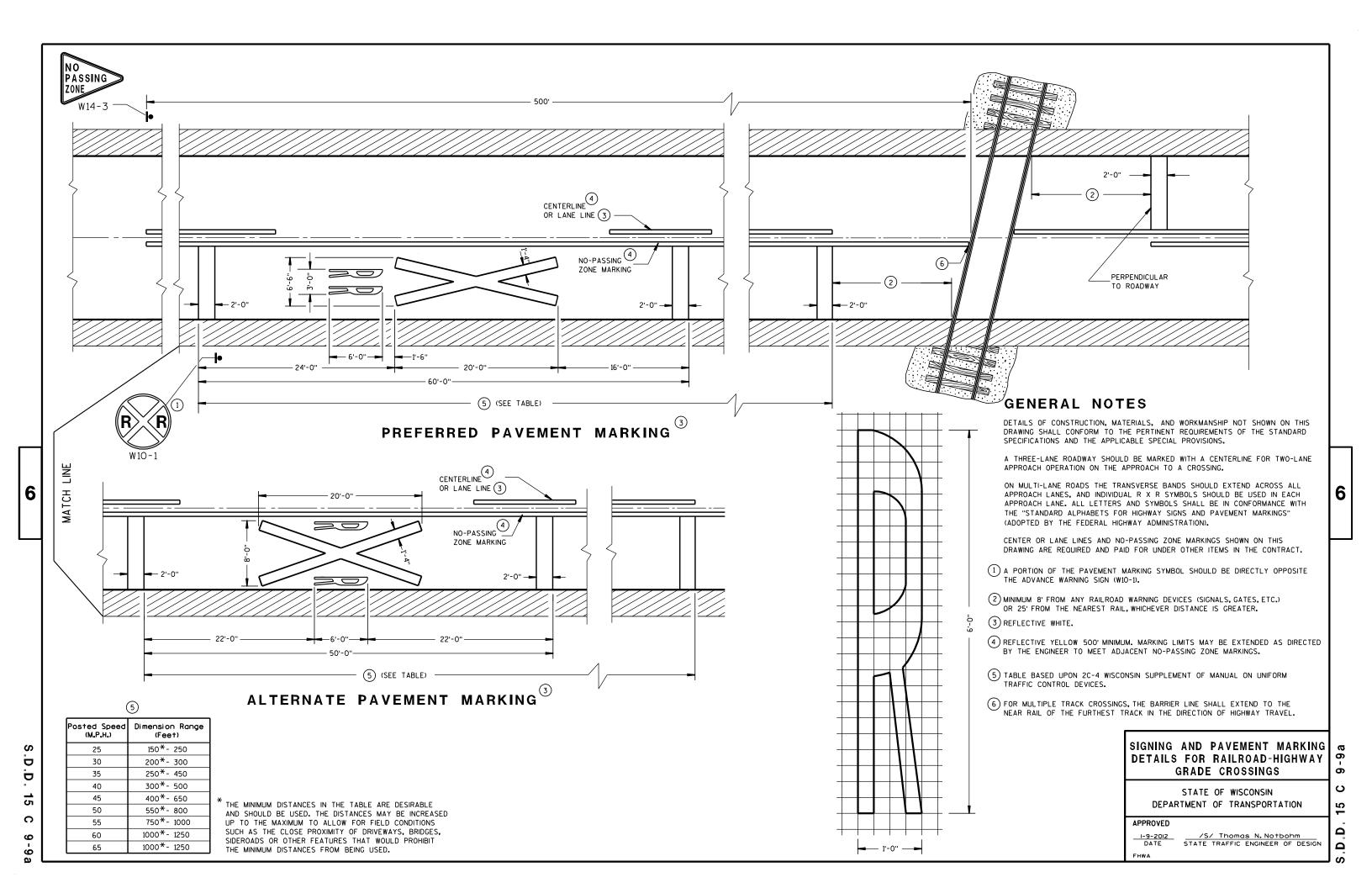
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

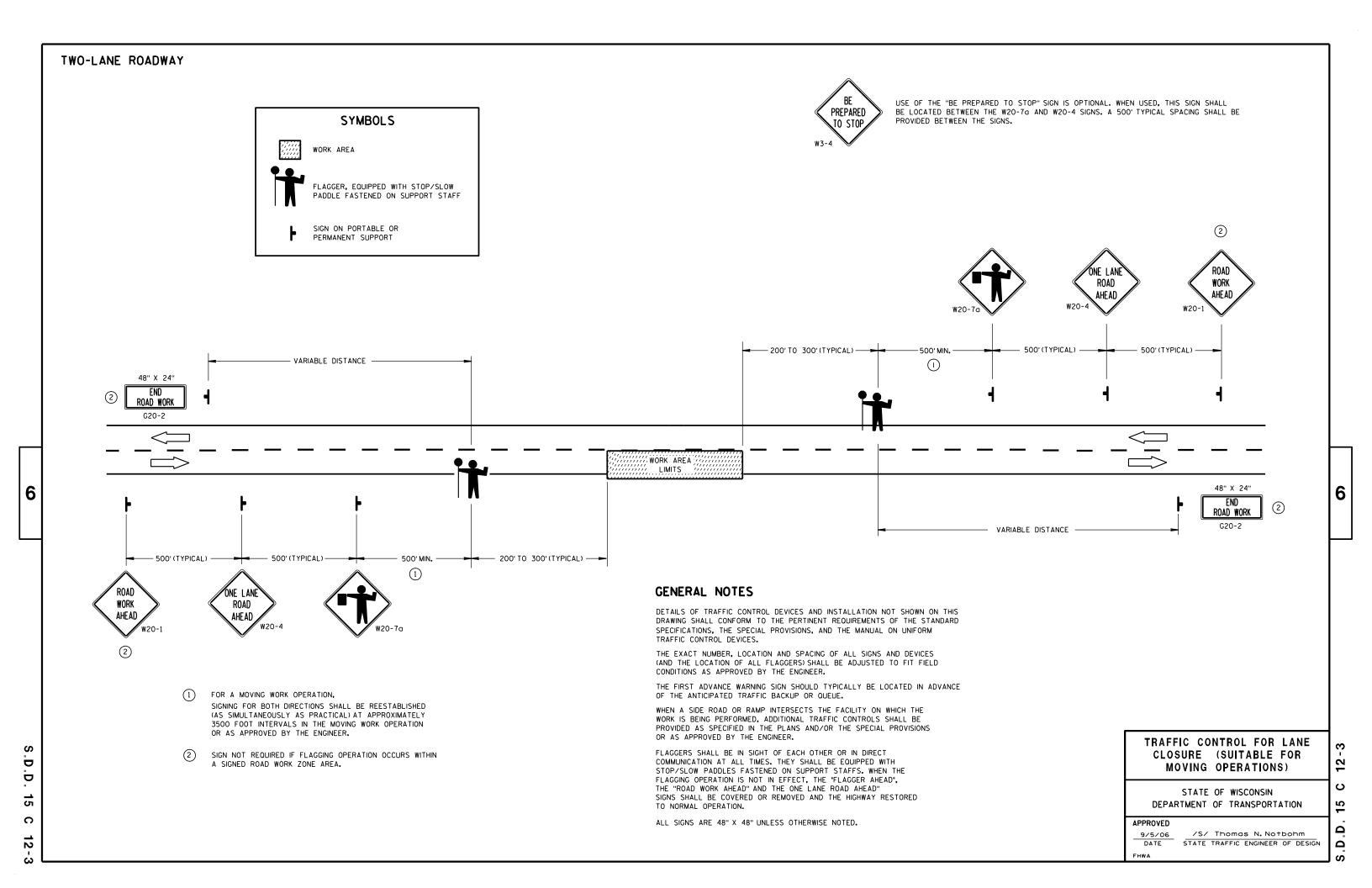
16 d

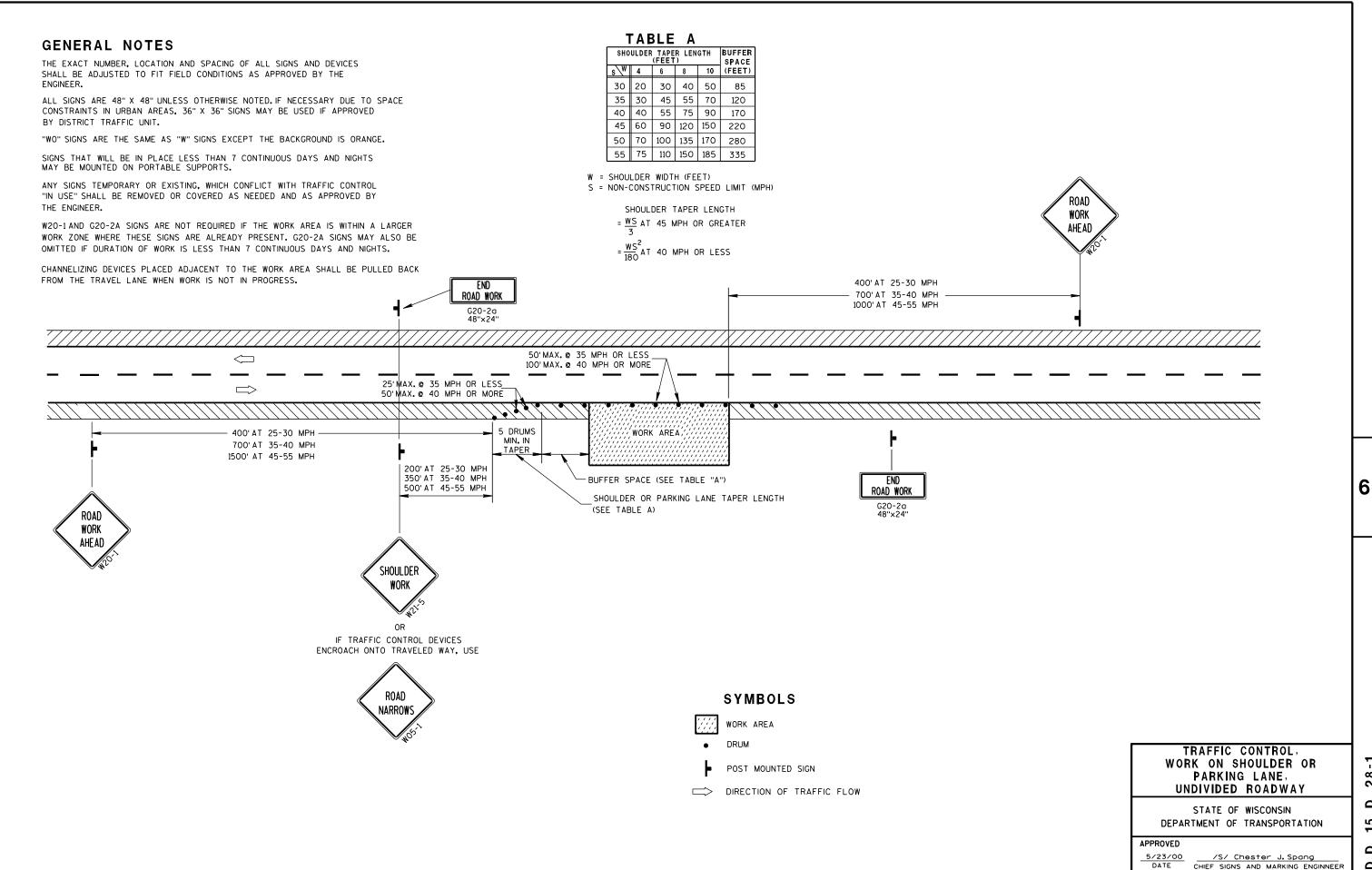
Ω Ω











6

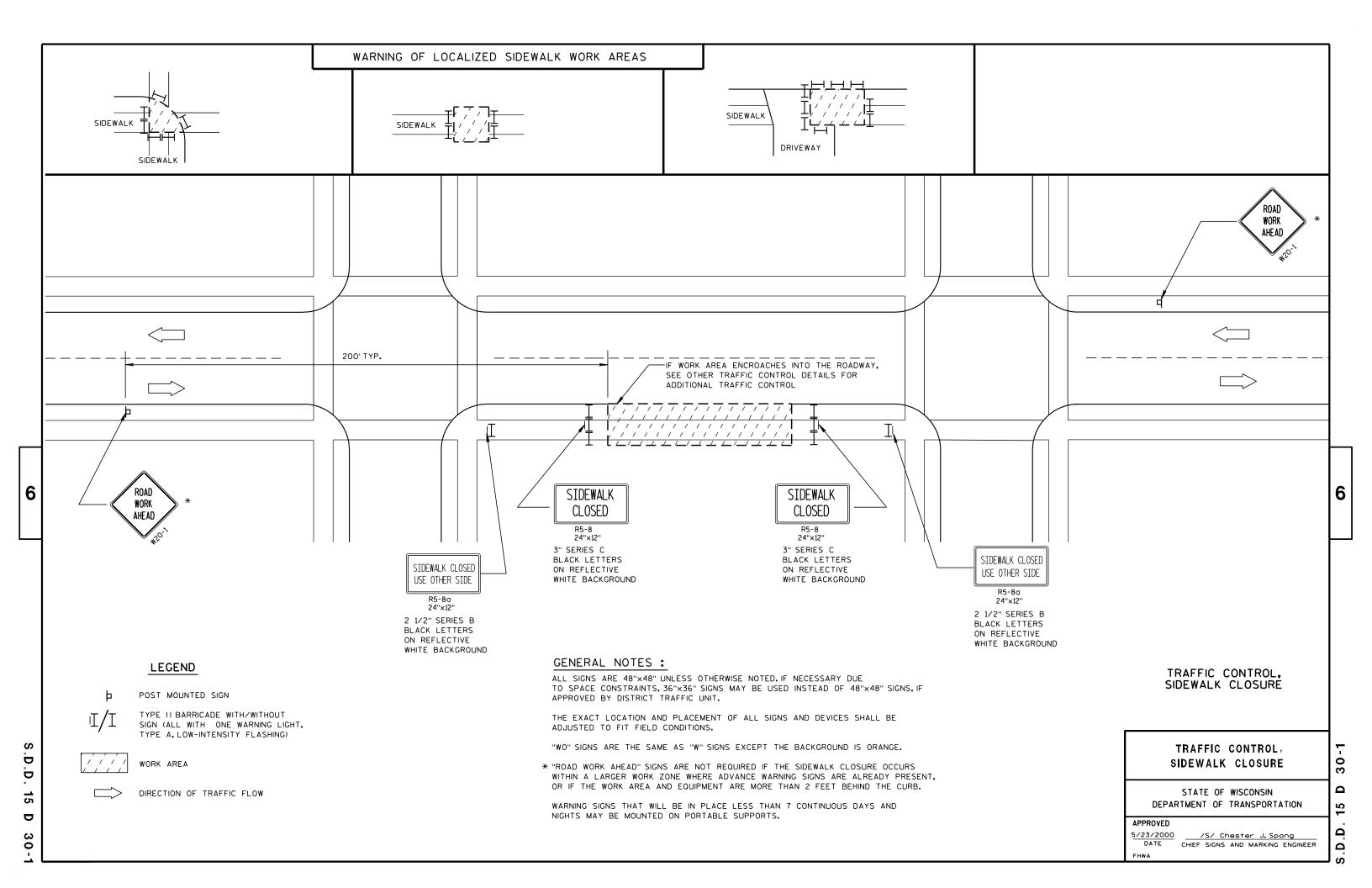
Ö

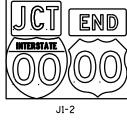
Ō

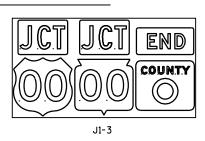
D

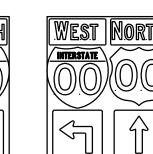
Ω S

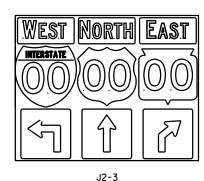
N Ω













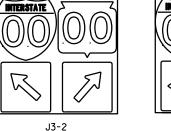
(Typical Vertical J-Assembly See Note 10 and 11)

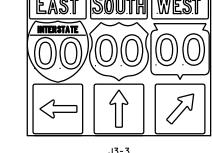


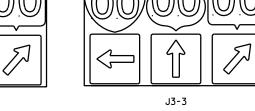
J3-1

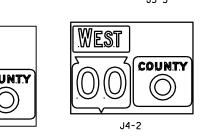


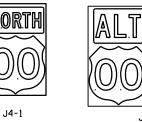
INTERSTATE

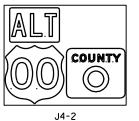


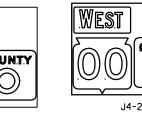










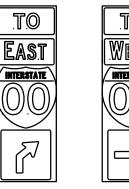




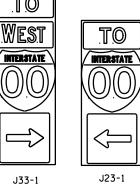
J13-1

PROJECT NO:

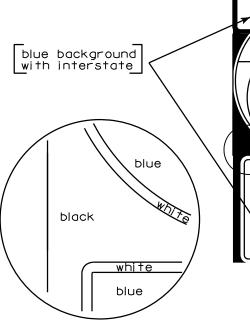
FRONTAGE ROAD



J32-1





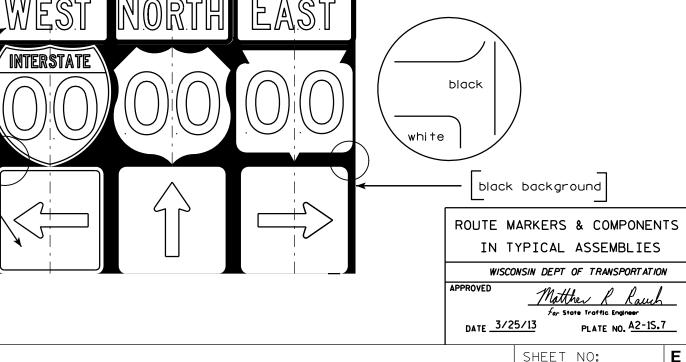


NOTES

- 1. Signs are Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

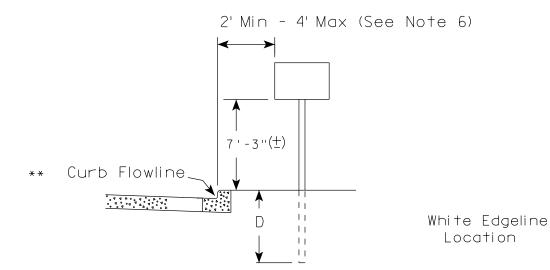
Background - Black Non-reflective Message - see Note 5

- 3. Message Series See Note 5
- 4. Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
- 5. The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
- 6. Certain marker heads require the component pieces to be the same color. As an example, all the components used with an M1-1 Interstate marker shall be blue.
- 7. Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
- 8. Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- 9. Route assemblies that have 36 inch shields and have dimensions greater than 48 inchs (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- 10. All Vertical J Assemblies are given a Sign Code of JV
- 11. For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

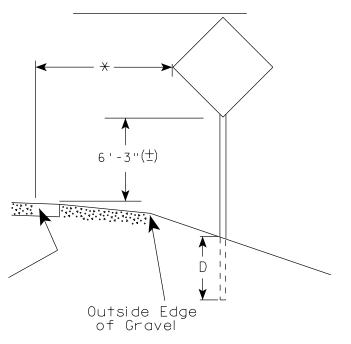




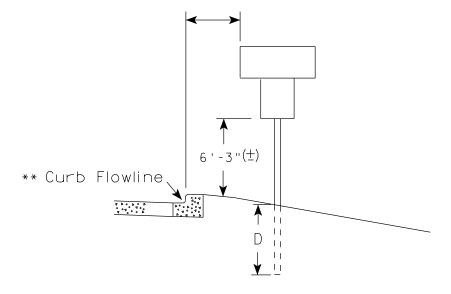
urban area



RURAL AREA (See Note 2)



2' Min - 4' Max (See Note 6)



White Edgeline
Location

Outside Edge
of Gravel

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated.

That height is typically measured where

there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

HWY:

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

PLOT BY : mscj9h

GENERAL NOTES

- 1. Signs wider than 4 feet, 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- 2. If signs are mounted on barrier wall, see A4-10 sign plate.
- 3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
- 4. Minimum mounting height for J assemblies (A4-5) is 7'-3'' (\pm) or 6'-3'' (\pm) per urban or rural detail respectively.
- 5. Minimum mounting height for signs mounted on traffic signal poles is 5' 3" (\pm) .
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. The (\pm) tolerance for mounting height is 3 inches.
- 8. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (\pm) or as directed by the Engineer.
- 9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series) & End of Rod Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (+).

POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther & Rauh
For State Traffic Engineer

DATE 9/30/13

SHEET NO:

COUNTY:

JN I Y:

PLOT DATE: 30-SEP-2013 13:25

PLOT NAME :

PLOT SCALE: 99.237937:1.000000

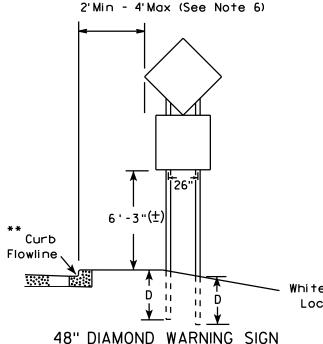
WISDOT/CADDS SHEET 42

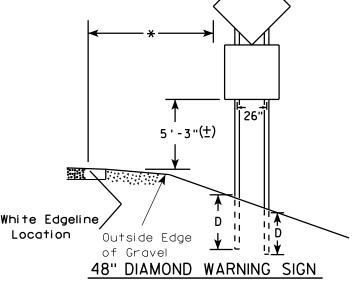
PROJECT NO:

GENERAL NOTES

- 1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- 2. See tables below for required number of posts.
- 3. For expressways and freeways, mounting height is 7'-3'' (±) or 6'-3'' (±) depending upon existence of sub-sign.
- 4. The (±) tolerance for mounting height is 3 inches.
- 5. Minimum mounting height for J assemblies (A4-5) is 7'-3" (\pm) or 6'-3" (\pm) per urban or rural detail respectively.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
- 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B). Clearance Markers (W5-52). Mile Markers (D10 series) & End of Road Markers (W5-56 & W5-56A) shall be mounted at a height of 4"-3" (\pm).
- * 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- ** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- ** See A4-3 sign plate for signs 4' or less in width or 20 S.F. or less in area.

URBAN AREA RURAL AREA (See Note 3) 2' Min - 4' Max (See Note 6) ₩E# FF# 6'-3"(±) 6'-3"(±) 7'-3"(±) ** Curb ********\ Flowline D 700 M White Edgeline D 11 White Edgeline, Location Outside Edae Location Outside Edge of Gravel





COUNTY:

of Gravel

	SIGN SHAPE OTHER THAN (TWO POSTS REQUIRED	
	L	E
* * *	Greater than 48" Less than 60"	12"
	60" to 120"	L/5

SIGN SHAPE OTHER THAN (THREE POSTS REQUIR	
L	E
Greater than 120" less than 168"	12"

HWY:

SIGN SHAPE OTHER THAN (FOUR POSTS REQUIRE	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matther For State Traffic Engineer

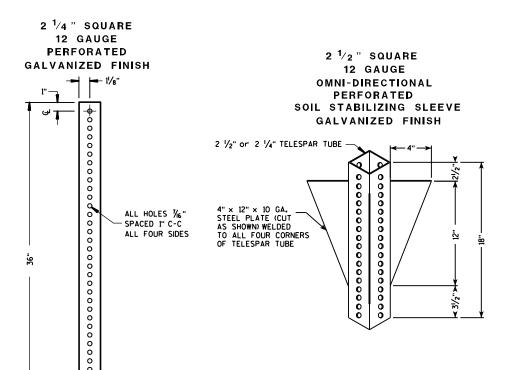
PLATE NO. A4-4.12 DATE 9/30/13

SHEET NO: PLOT BY: mscj9h

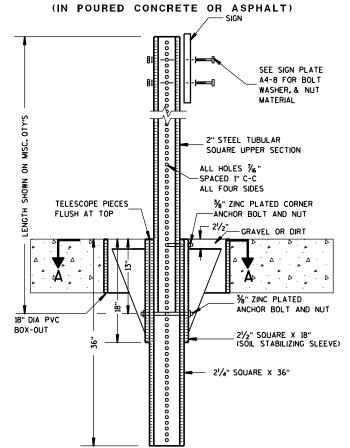
PROJECT NO:



TELESCOPIC TUBING ANCHORS TWO PIECE SYSTEM



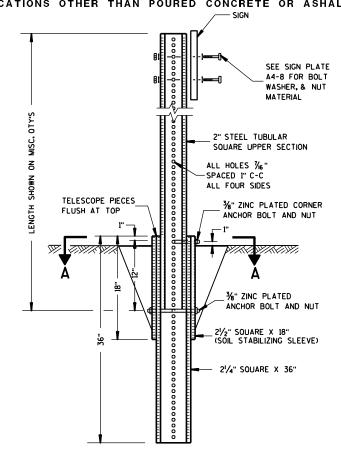
HWY:

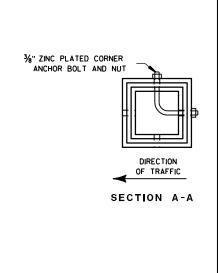


DETAIL OF TUBULAR STEEL SIGN POST

DETAIL OF TUBULAR STEEL SIGN POST

(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASHALT)





Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL SIGN POST A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matther

For State Traffic Engineer DATE <u>5/30/1</u>2 PLATE NO. <u>A4-9.7</u>

SHEET NO:

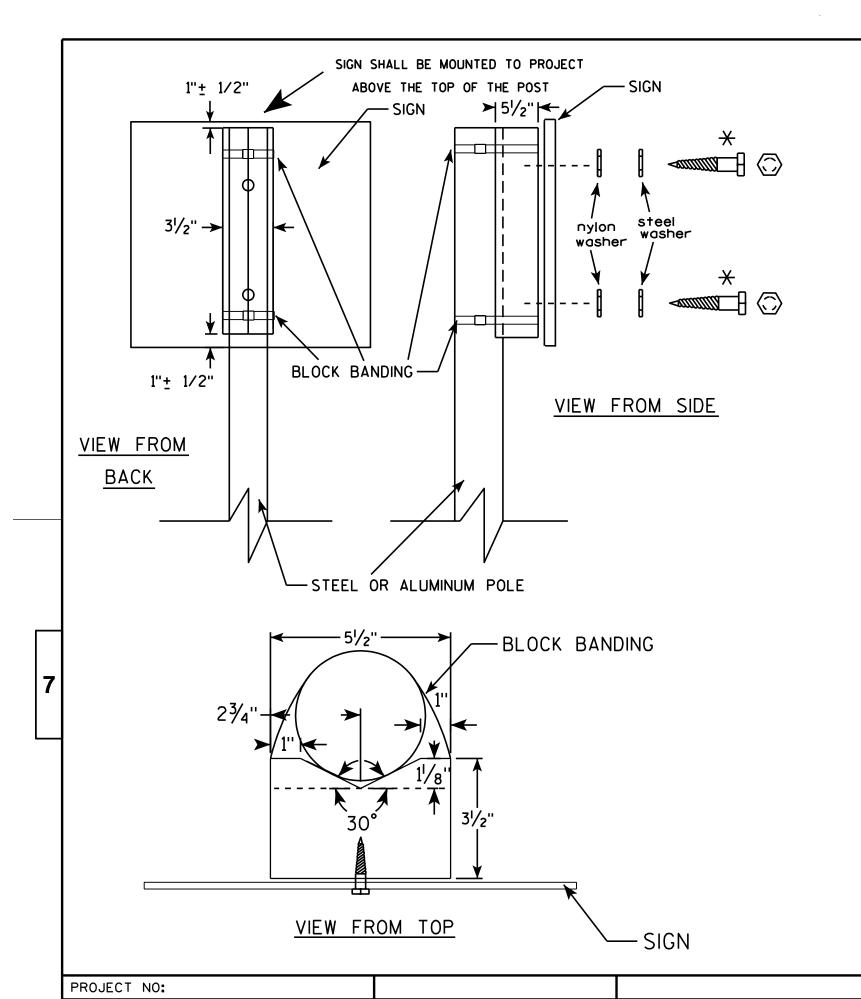
PROJECT NO: FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN COUNTY:

PLOT DATE: 30-MAY-2012 14:04

PLOT BY: mscj9h

PLOT NAME :

PLOT SCALE : 13.933009:1.000000



GENERAL NOTES

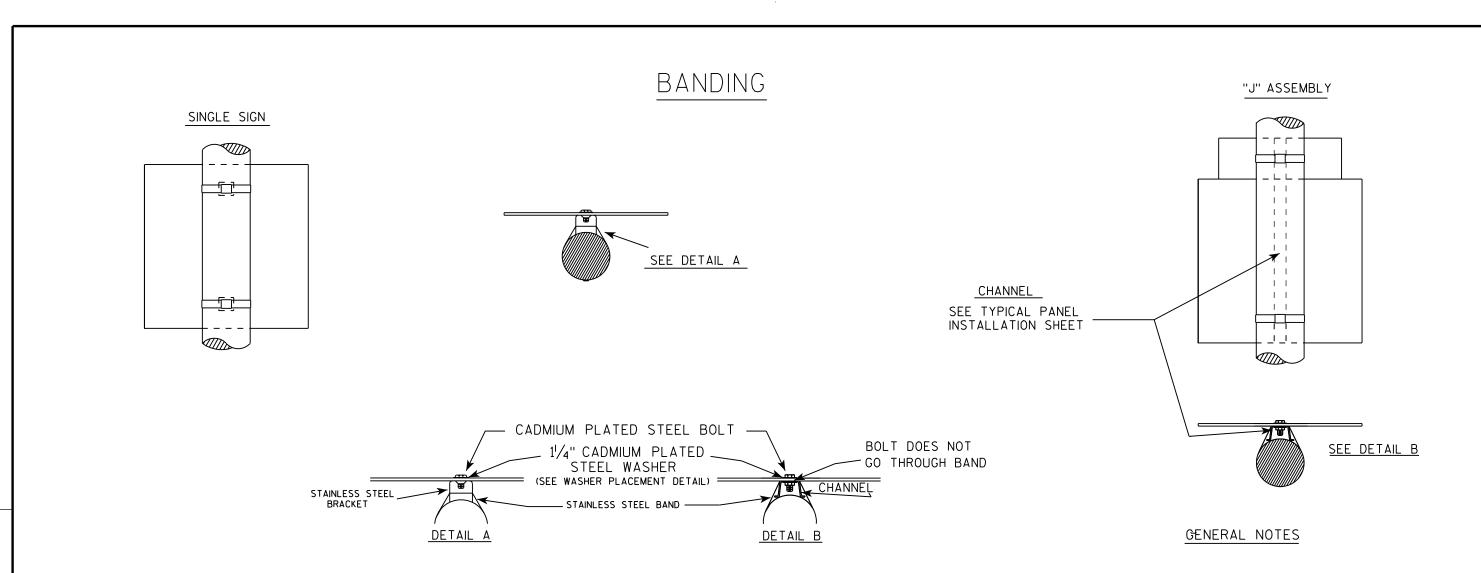
- 1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
- 2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
- 3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
- 4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
- 5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D, or
 - b. Cadmium plated in accordance with ASTM Designation: B 766 TYPE 3, Class 12, or
 - c. Electro-galvanized in accordance with ASTM Designation: B 633, TYPE III, SC 3.
- 6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
- 7. STEEL WASHERS SHALL BE 11/4" O.D. X 3/8" I.D. X 1/16"
- 8. NYLON WASHERS SHALL BE $1^{1}/_{4}$ " O.D. X $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

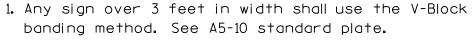
X LAG BOLTS SHALL BE 3/8" X 21/2"

BLOCK BANDING DETAIL (V-BLOCK OPTION) WISCONSIN DEPT OF TRANSPORTATION APPROVED For State Traffic Engineer PLATE NO. <u>A5-10.1</u>

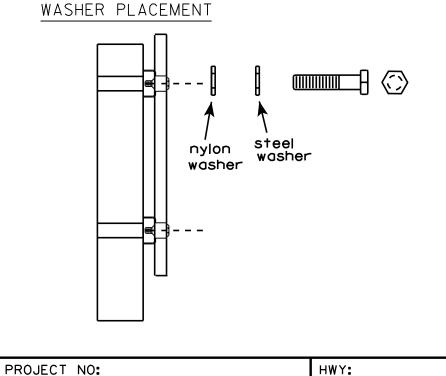
DATE 7/12/07

SHEET NO:





- 2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
- 3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.



WASHERS (ALL POSTS) -

COUNTY:

1-1/4" O.D. X3/8" I.D. X1/16" STEEL 1-1/4" O.D. X3/8" I.D. X .080 NYLON FOR ALL TYPE H SIGNS

STANDARD SIGN SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

State Traffic Engineer DATE 8/16/13

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A59.DGN

PLOT DATE: 16-AUG-2013 13:27

PLOT BY: mscsja

PLOT NAME :

PLOT SCALE: 33.740899:1.000000

WISDOT/CADDS SHEET 42

PLATE NO. A5-9.3

- Sign is Type II see Note 7 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

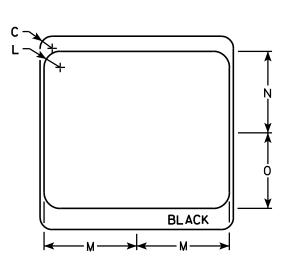
Background - White & Black - See Note 7 Message - Black

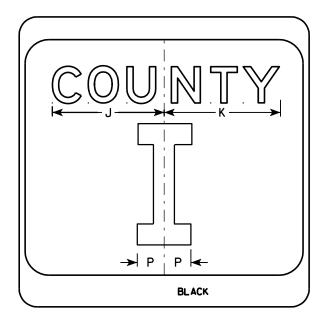
- 3. Message Series see Note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Message Series E for 1 letter.

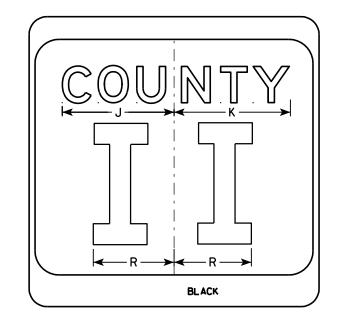
 Message Series D for 2 letters unless
 message is too big then Series C.

 Message Series C for 3 letters unless
 message is too big then Series B.
- 6. Substitute appropriate letters & optically center to achieve proper balance.
- 7. Permanent Signs

Background - Type H Reflective Detour or temporary Signs Background - Reflective







PLOT NAME :

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	0	R	S	Т	U	V	W	Х	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 %									4.0
3	36		2 1/4			16	4	7 %	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 %	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 %		10									9.0
5	36		2 1/4			16	4	7 5/8	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
DDO	IECT	NO.					111	/V.					COUN	TV.													
FRU	JECT	NO.					HV	V I .						I I .					I								

CTH MARKER
M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

PROVED

Matthew Rauch

Forstate Traffic Engineer

MATE 9/27/11 PLATE NO. M1-5A.8

DATE 9/27/11

SHEET NO:

BLACK

M1-5A

- 1. Sign is Type II See Note 6 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White & Black - See Note 6 Message - Black

- 3. Message Series See note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate Series numerals and adjust spacing as per plate A10-1.
- 6. Permanent Signs
 Background Type H Reflective
 Detour or temporary Signs
 Background Reflective

BLACK	↑ G → ↑ F → → ↑ → → → → → → → → → →
Metric equivalent for this sign is:	

HWY:

900 mm X 900 mm

5 900 mm X 900 mm

PROJECT NO:

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	Т	U	V	w	Х	Y	Z	Area sq. ft.	Area m2
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 1/8	11 1/2	1	1 1/8	11 1/4	21 1/8											4.0	. 36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	. 81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	. 81
ب			- / -			1 .0	74	74	78	78	12 /8	/8	- /2	_ /8	10 78				<u> </u>			1	<u> </u>				J.0	_

COUNTY:

STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

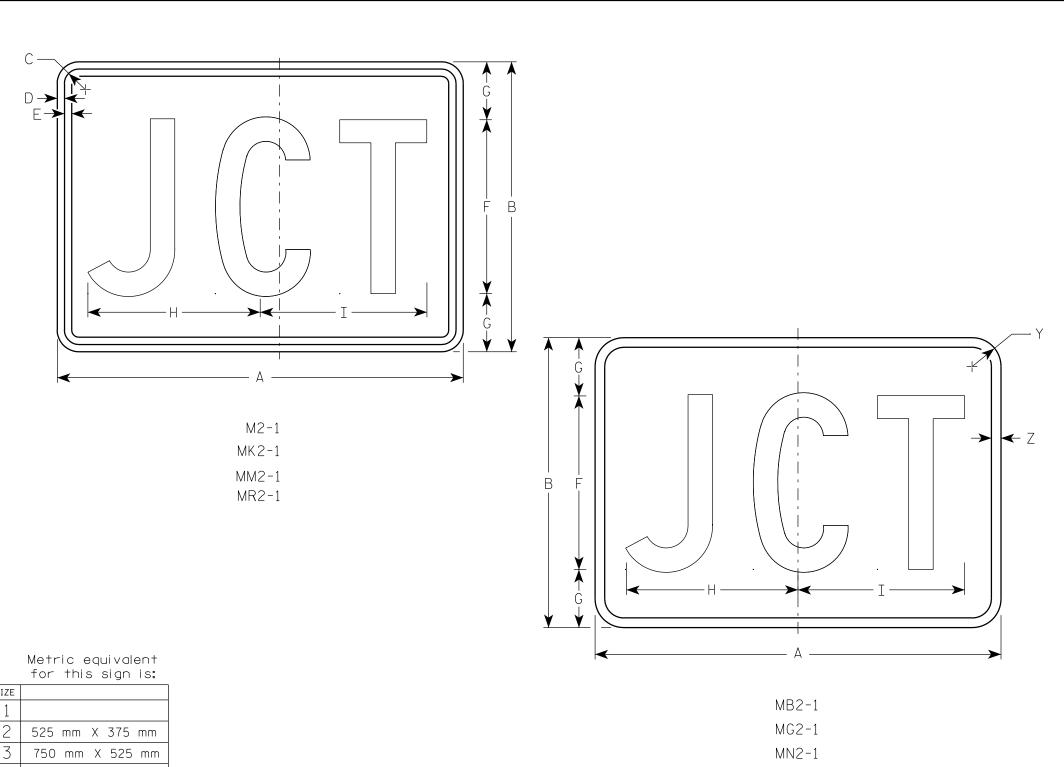
APPROVED

The state Traffic Engineer

DATE 3/20/02 PLATE NO. M1-6.9

SHEET NO:

PLOT NAME :



- 1. Sign is Type II See Note 5 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. M2-1 Background White Type H Reflective (Detour or temporary Signs - Reflective) Message - Black
 - MB2-1 Background Blue Message - White - Type H Reflective (Detour or temporary Signs - Reflective)
 - MG2-1 Background Green Message - White - Type H Reflective
 - MK2-1 Background Green Message - White - Type H Reflective
 - MM2-1 Background White Type H Reflective Message - Green
 - MN2-1 Background Brown Message - White - Type H Reflective
 - MR2-1 Background Brown Message - Yellow - Type H Reflective

750 mm X 525 mm 750 mm X 525 mm

PROJECT NO:

SIZE	Ξ[Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	Т	U	٧	W	X	Y	Z	Area sq. ft.	Area m2
1																													
2	2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 %																1 1/2	1/2	2.20	0.20
3	3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20
4	-	30	21	1 1/8	3/8	3/8	13	4	12 1/8	12 3/8																1 1/2	1/2	4.40	0.20
5		30	21	1 1/8	3/8	3/8	13	4	12 1/8	12 3/8																1 1/2	1/2	4.40	0.20

COUNTY:

STANDARD SIGN

M2 - 1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

 f_{or} State Traffic Engineer

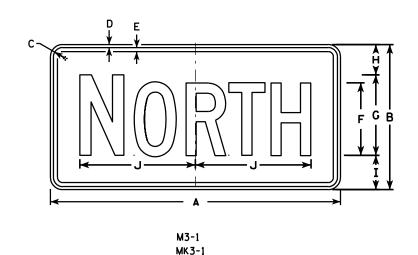
DATE 3/16/10

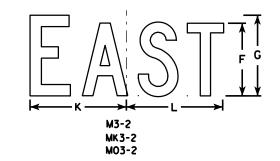
PLATE NO. M2-1.10 SHEET NO:

WISDOT/CADDS SHEET 42

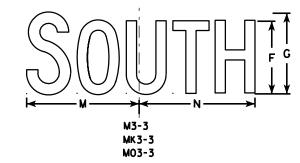
PLOT NAME : PLOT DATE: 16-MAR-2010 09:49 PLOT SCALE: 4.965868:1.000000 PLOT BY: dotsja

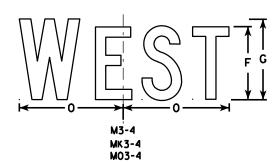
HWY:



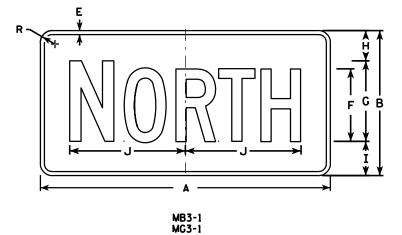


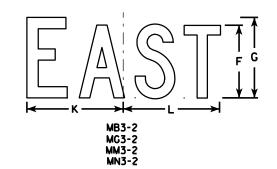
MO3-1





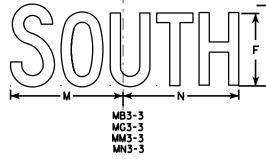
HWY:

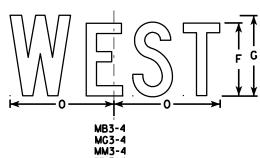




MM3-1

MN3-1





<u>NOTES</u>

- 1. All Signs Type II See Note 5 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. M3-1 thru M3-4 Background White Type H Reflective (Detour or temporary signs Reflective) Message Black
 - MB3-1 thru MB3-4 Background Blue Message - White - Type H Reflective (Detour or temporary signs - Reflective)
 - MG3-1 thru MG3-4 Background Green

 Message White Type H Reflective
 - MK3-1 thru MK3-4 Background Green

 Message White Type H Reflective
 - MM3-1 thru MM3-4 Background White Type H Reflective Message Green
 - MN3-1 thru MN3-4 Background Brown
 Message White Type H Reflective
 - M03-1 thru M03-4 Background Orange Reflective Message Black
- 6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	Т	כ	٧	W	X	Y	Z	Areg sq. ft.
1																											
2	24	12	1 1/8	3/8	3⁄8	6	7	2 1/4	2 3/4	10 1/4	7 1/8	8 3/8	10 1/4	9 3/4	8 ¾			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

COUNTY:

STANDARD SIGNS M3-1 thur M3-4 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE 11/10/10 PLATE NO. M3-1.12

SHEET NO: E

PROJECT NO:

PLOT NAME :

- Signs are Type II See Note 4 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - See note 4 Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M5-1 and M5-2 Background White Type H Reflective Message Black
 - MB5-1 and MB5-2 Background Blue

 Message White Type H Reflective
 - MG5-1 and MG5-2 Background Green

 Message White Type H Reflective
 - MK5-1 and MK5-2 Background Green
 - Message White Type H Reflective
 - MM5-1 and MM5-2 Background White Type H Reflective Message Green
- MN5-1 and MN5-2 Background Brown

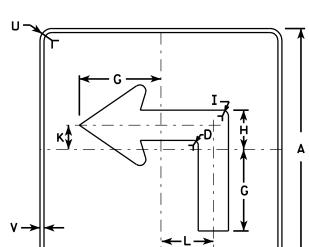
Message - White - Type H Reflective

- M05-1 and M05-2 Background Orange Type F Reflective Message - Black
- MP5-1 and MP5-2 Background White Type H Reflective Message Blue
- MR5-1 and MR5-2 Background Brown
 - Message Yellow Type H Reflective
- 5. M5-1R same as M5-1L except arrow points right.
- 6. M5-2R same as M5-2L except arrow tilts right.

c —	
D → E →	
Į.	←
·	M5-2L
	MK5-2L

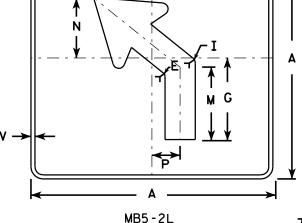
MK5-1L MM5-1L MO5-1L MP5-1L MR5-1L

M5-1L



MB5-1L MG5-1L MN5-1L

HWY:



MG5-2L

MN5-2L

MM5-2L

M05-2L

MP5-2L

MR5-2L

T A S

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	₩	Х	Y	Z	Areo sq. ft
1																											
2	21		1 1/8	3%	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 %	5 1/4	5	2 1/2		1/2	2 %	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 1/8	7 /8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 1/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 1/8	7 /8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 1/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 1/8	½		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 1/8	1/2					6.25

COUNTY:

STANDARD SIGN M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

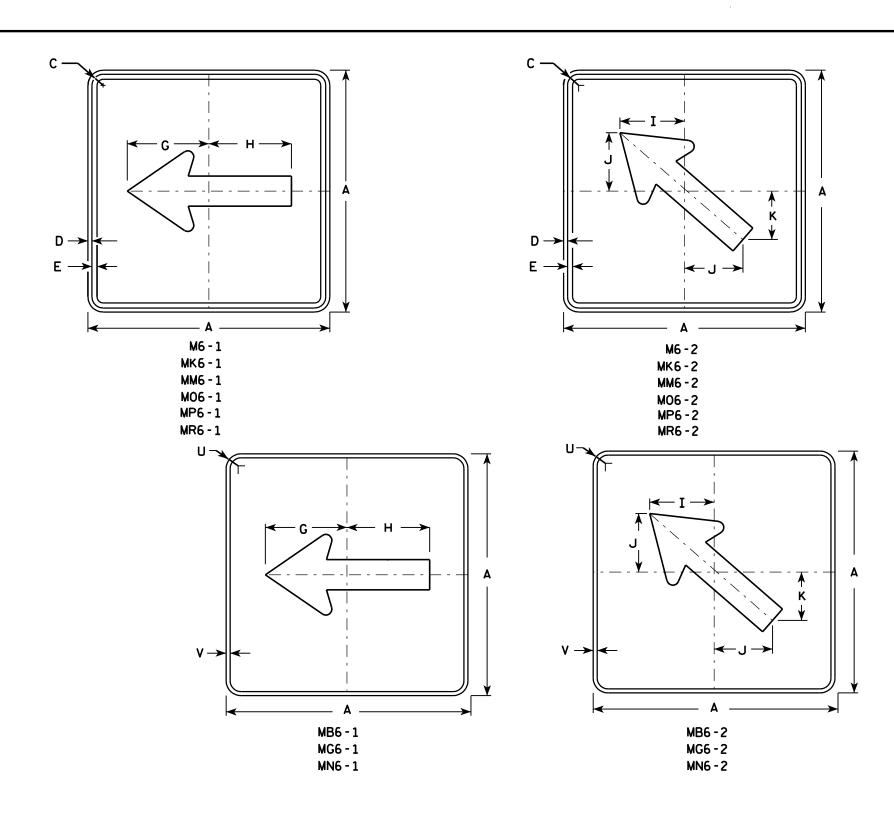
APPROVED

For State Traffic Engineer
DATE 7/29/13 PLATE NO. M5-1.12

SHEET NO:

PROJECT NO:

PLOT NAME :



- 1. Signs are Type II See Note 4 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

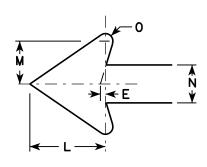
Background - See note 4 Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M6-1 and M6-2 Background White Type H Reflective Message Black
 - MB6-1 and MB6-2 Background Blue Message - White - Type H Reflective
 - MG6-1 and MG6-2 Background Green
 Message White Type H Reflective
 - MK6-1 and MK6-2 Background Green

 Message White Type H Reflective
 - MM6-1 and MM6-2 Background White Type H Reflective Message Green
 - MN6-1 and MN6-2 Background Brown

 Message White Type H Reflective
 - M06-1 and M06-2 Background Orange Type F Reflective Message - Black
 - MP6-1 and MP6-2 Background White Type H Reflective Message Blue
 - MR6-1 and MR6-2 Background Brown

 Message Yellow Type H Reflective



PLOT NAME :

SIZE	Α	В	С	D	Е	F	G	Н	I	J	К	L	М	N	0	Р	0	R	S	T	U	٧	₩	Х	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 %	5	4 1/4	5 1/4	3	2 %	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 ¾	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25

COUNTY:

STANDARD SIGN M6-1 & M6-2 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

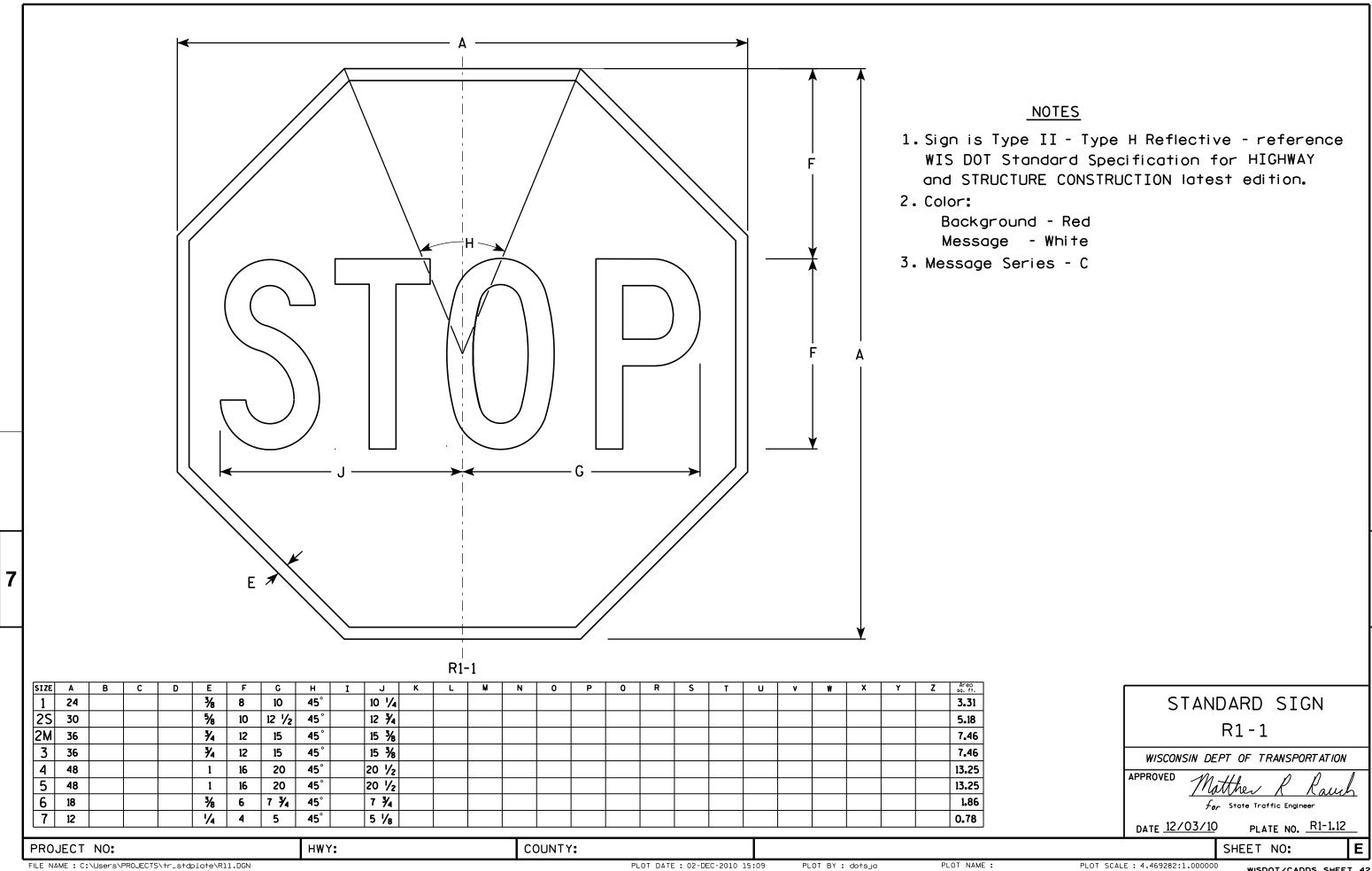
Matther R Rauch

DATE 7/29/13 PLATE NO. M6-1.13

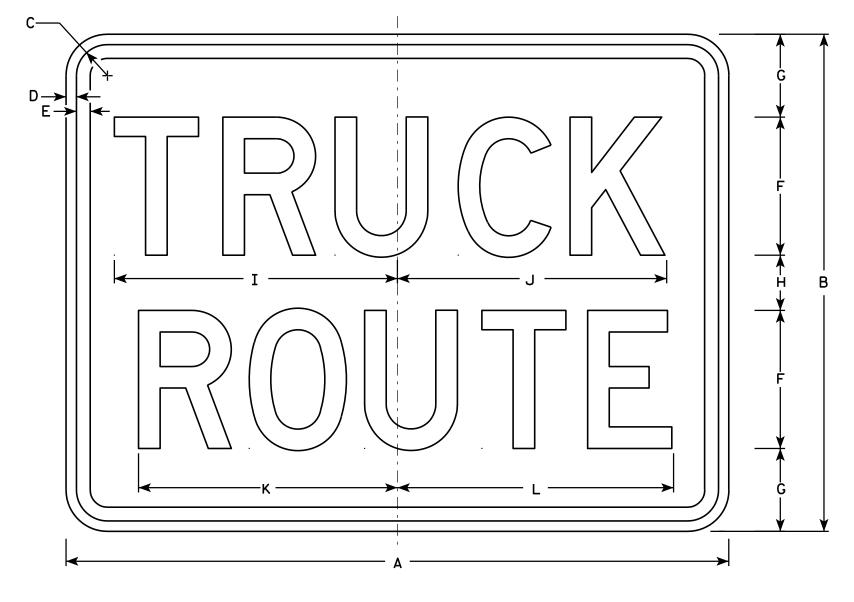
SHEET NO:

HWY:

PROJECT NO:



WISDOT/CADDS SHEET 42



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

R14-1

HWY:

SIZE	Α	В	С	D	Е	F	G	H	I	J	K	L	М	Z	0	Р	0	R	S	Т	U	٧	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	5	3	2	10 1/4	9 3/4	9 3%	10															3.0
2	24	18	1 1/8	3∕8	1/2	5	3	2	10 1/4	9 3/4	9 3/8	10															3.0
3	30	24	1 1/8	3∕8	1/2	6	4	4	12 1/4	11 ¾	11 1/4	12															5.0
4																											
5													·														

COUNTY:

STANDARD SIGN R14-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

ROVED Matthew & Ray

DATE 4/1/11

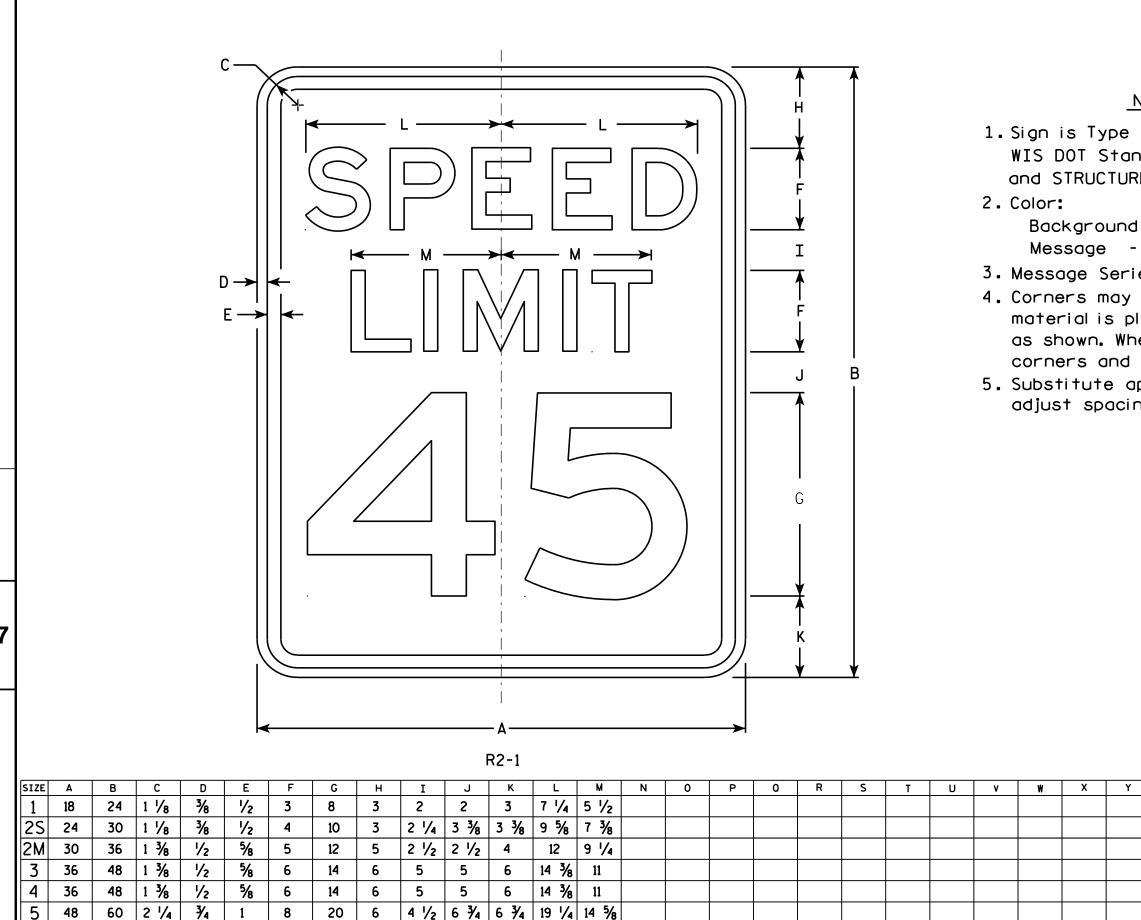
 f_{or} State Traffic Engineer

11 PLATE NO. R14-1.6

SHEET NO:

PROJECT NO:

PLOT NAME :



COUNTY:

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

Background - White Message - Black

- 3. Message Series E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal. the corners and borders shall be rounded.
- 5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

3.0

5.0

7.5

12.0

12.0

20.0

STANDARD SIGN R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matther R Raus

For State Traffic Engineer DATE <u>5/26/1</u>0 PLATE NO. R2-1.13

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R21.DGN

PROJECT NO:

HWY:

PLOT DATE: 28-MAY-2010 08:32

PLOT BY : ditjph

PLOT NAME :

PLOT SCALE: 4.717577:1.000000

WISDOT/CADDS SHEET 42

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

3. Message Series - E

D K F G WID-1	

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1	30			3/8	5/8	7	3 1/2	45°	12 3/8	7 1/8	3	1 1/2															4.91
2S	36			5/8	3/4	8	4	45°	14 3/8	8 %	4	2															7.07
2M	36			5/8	3/4	8	4	45°	14 3/8	8 %	4	2															7.07
3																											
4	48			3/4	1 1/4	10	5	45°	18 3/8	11 %	5	2 1/2															12.57
5																											

COUNTY:

STANDARD SIGN W10-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 3/13/13 State Traffic Engineer PLATE NO. W10-1.8

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W101.DGN

PROJECT NO:

HWY:

PLOT DATE: 13-MAR-2013 11:06

PLOT BY : mscj9h

PLOT NAME :

PLOT SCALE: 6.946657:1.000000

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

http://www.dot.wisconsin.gov