

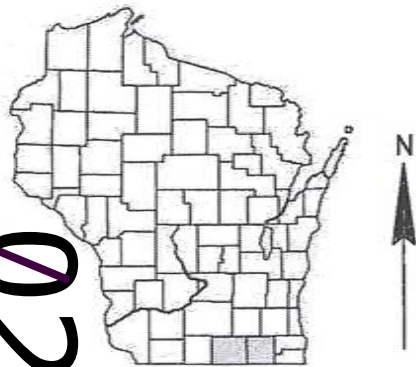
MAD JUNE 2013

PROJECT ID: 3663-00-61
WITH:

ORDER OF SHEETS

Section No.	Title
1	Section No. 1
2	Typical Sections and Details
3	Estimate of Quantities
3	Miscellaneous Quantities
5	Plan
6	Standard Detail Drawings

TOTAL SHEETS = 56



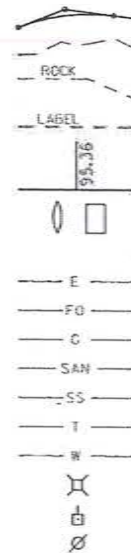
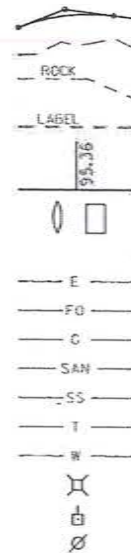
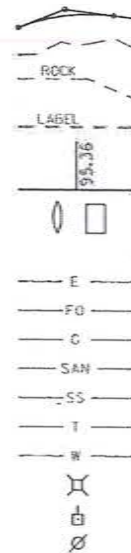
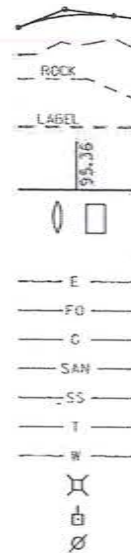
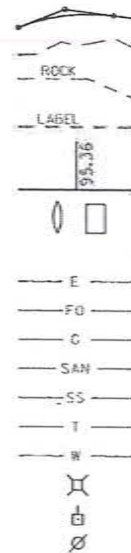
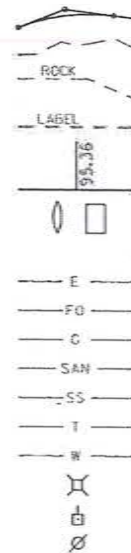
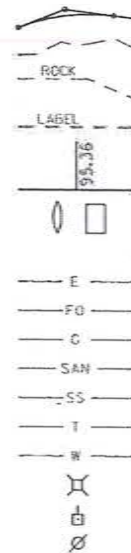
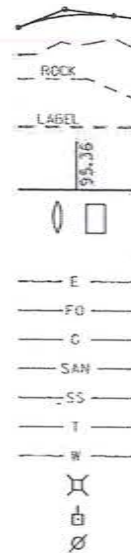
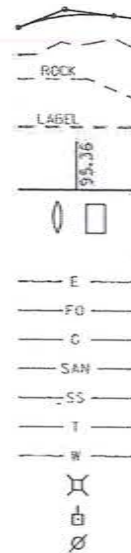
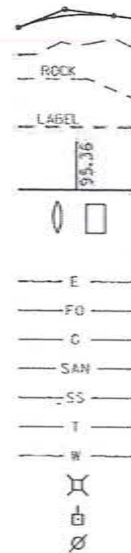
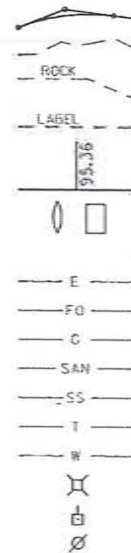
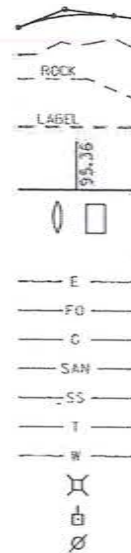
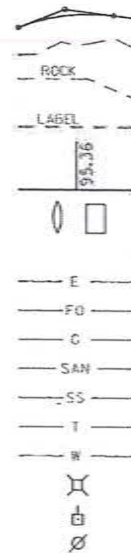
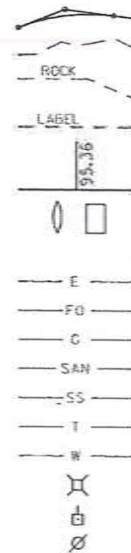
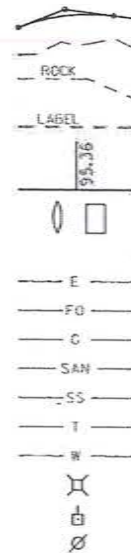
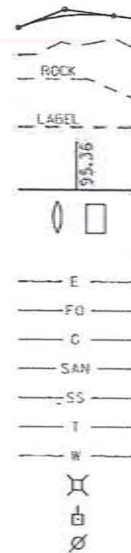
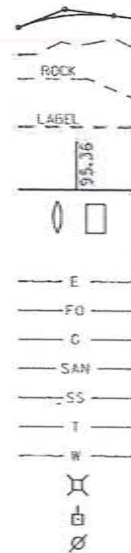
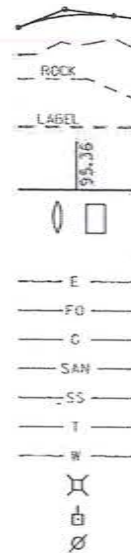
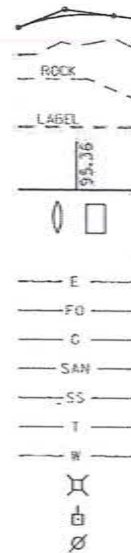
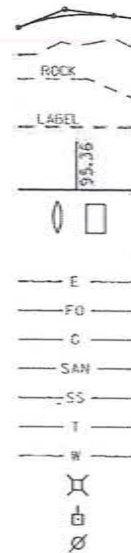
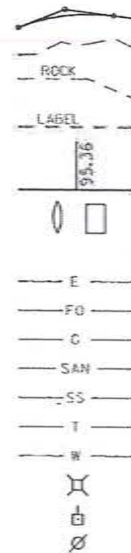
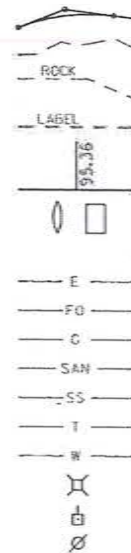
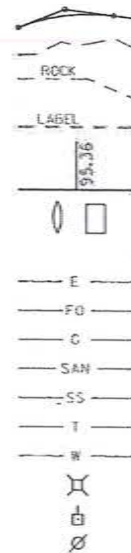
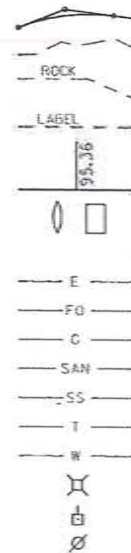
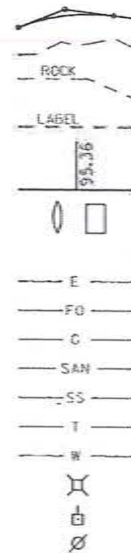
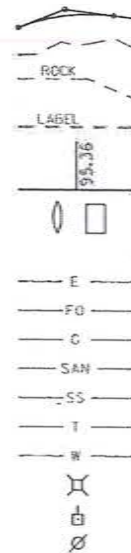
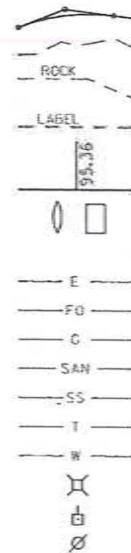
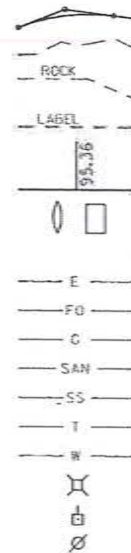
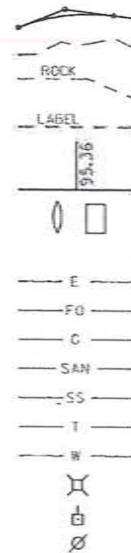
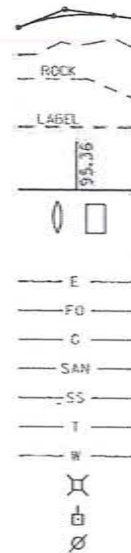
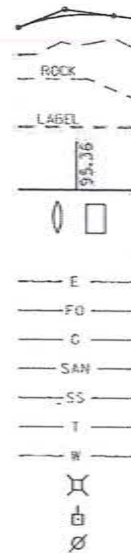
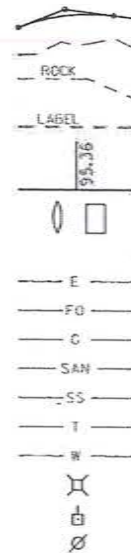
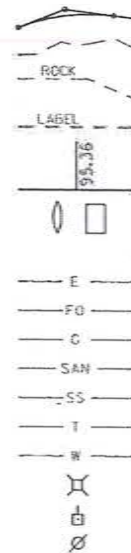
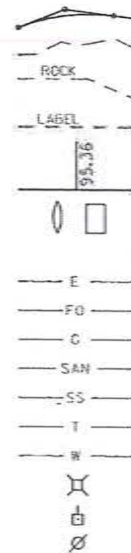
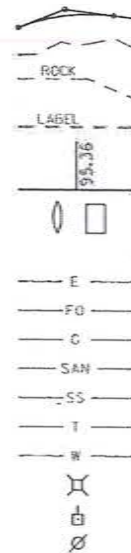
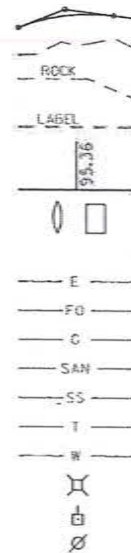
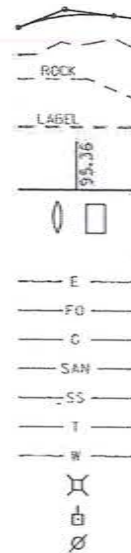
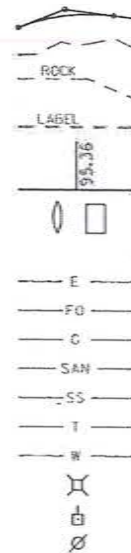
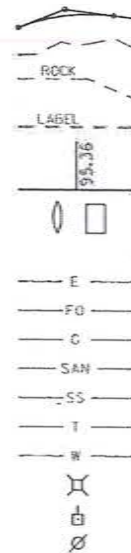
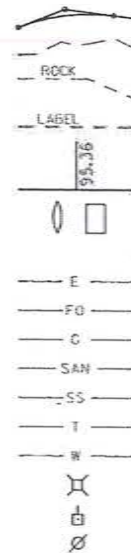
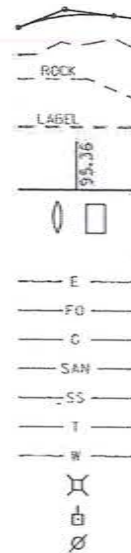
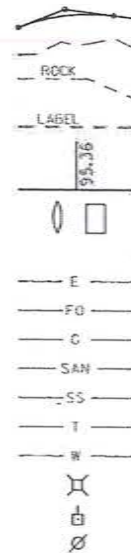
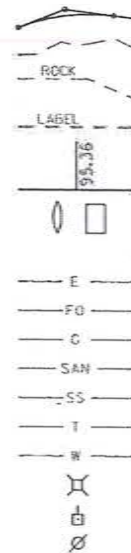
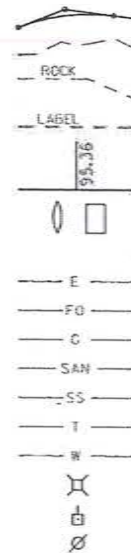
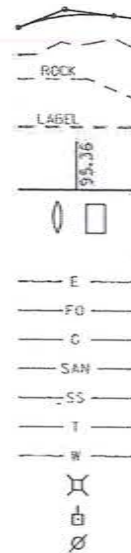
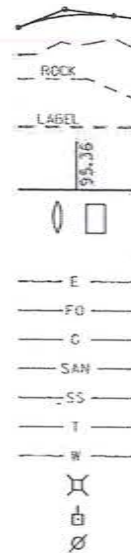
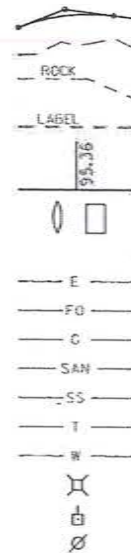
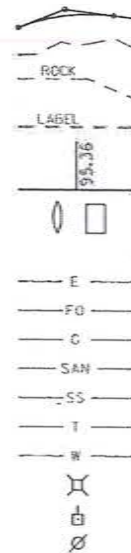
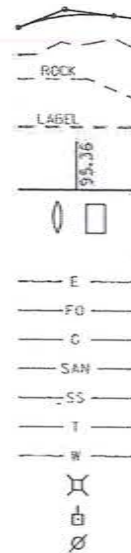
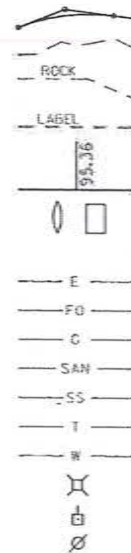
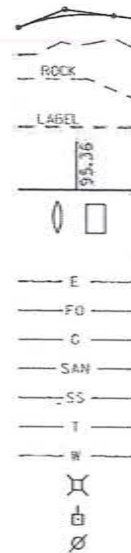
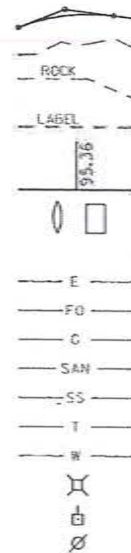
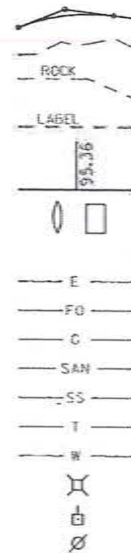
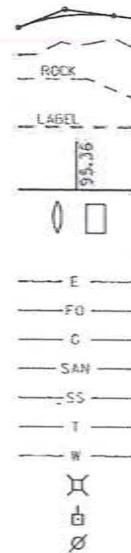
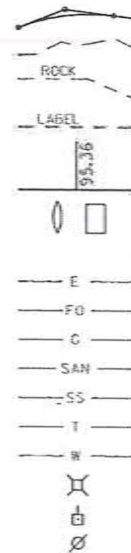
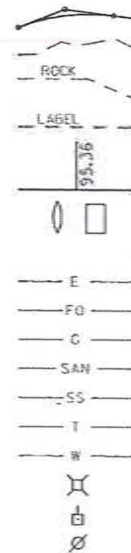
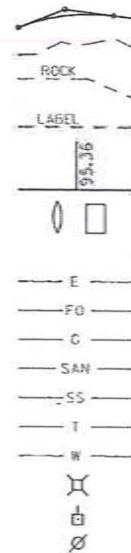
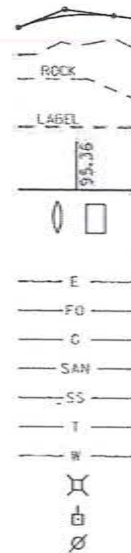
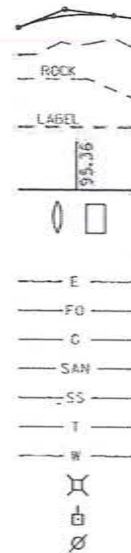
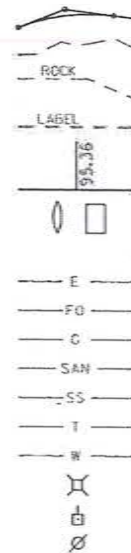
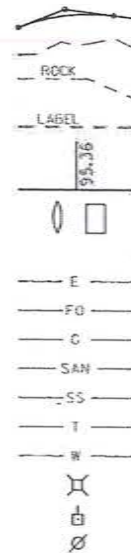
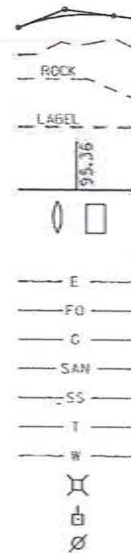
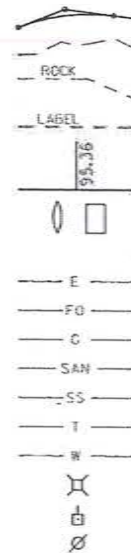
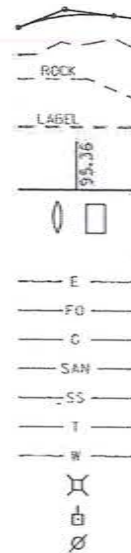
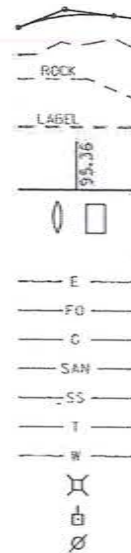
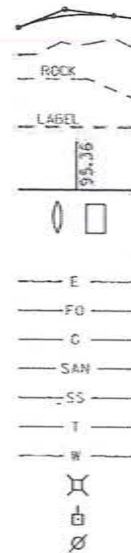
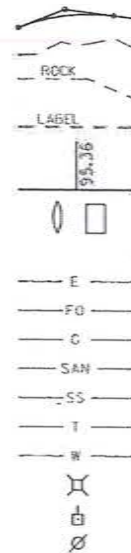
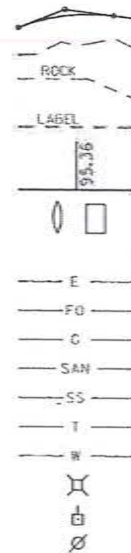
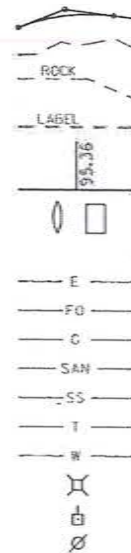
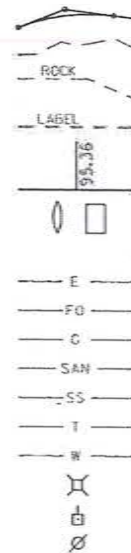
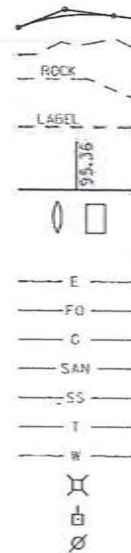
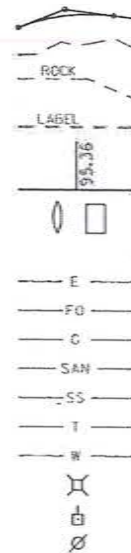
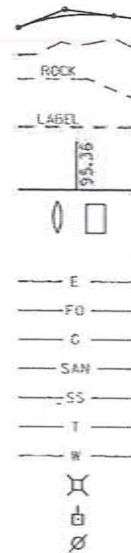
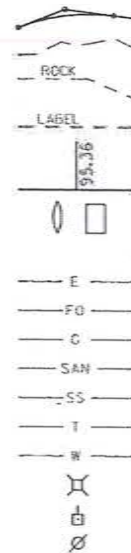
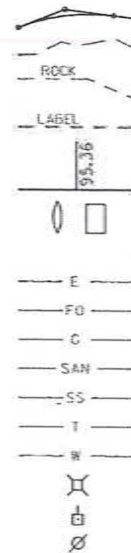
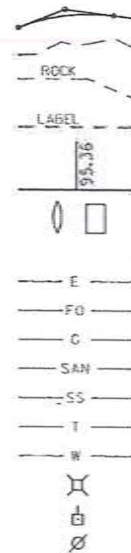
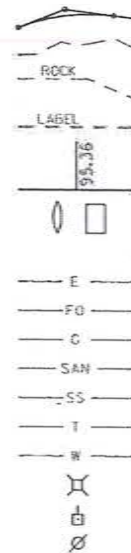
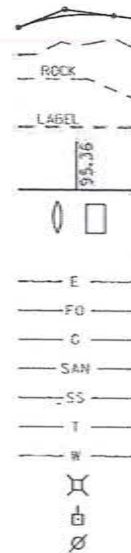
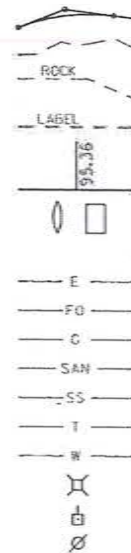
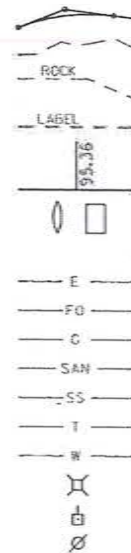
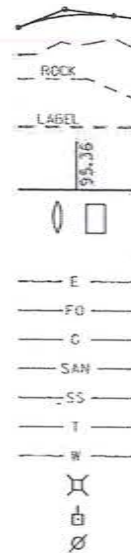
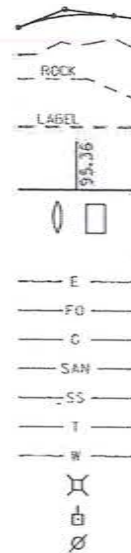
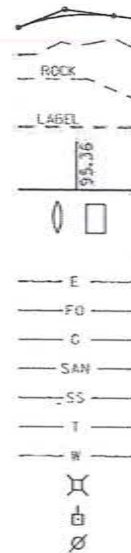
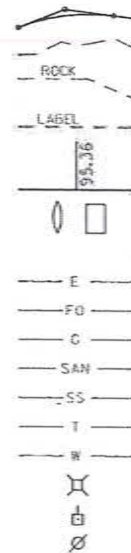
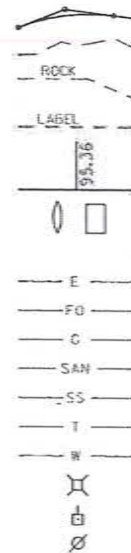
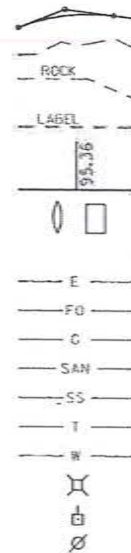
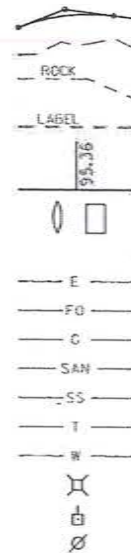
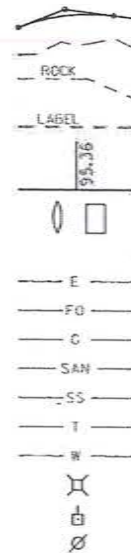
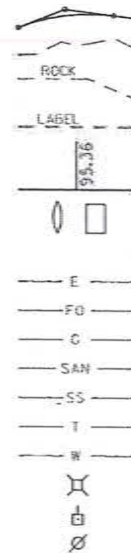
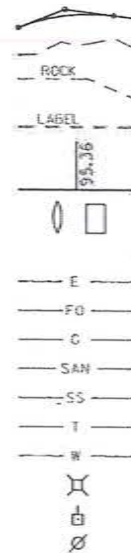
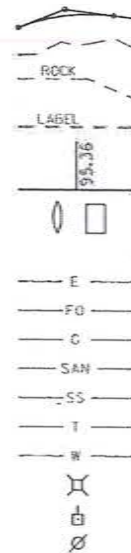
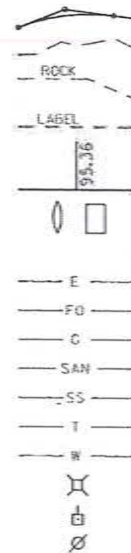
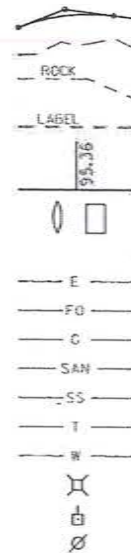
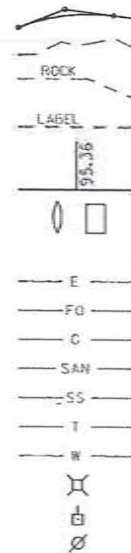
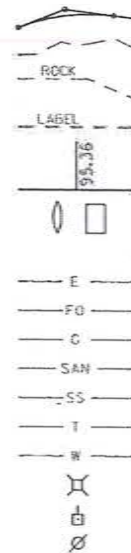
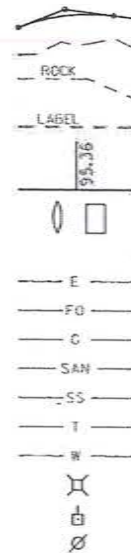
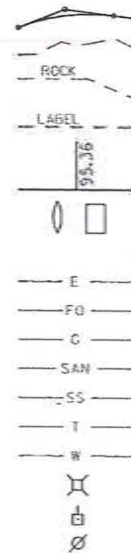
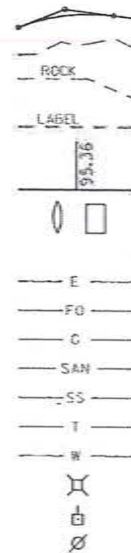
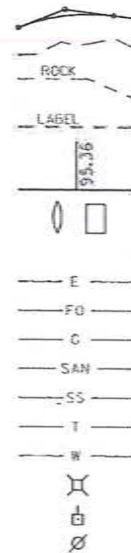
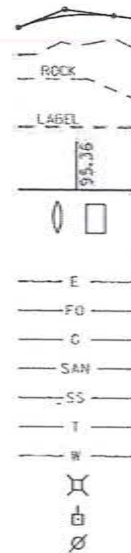
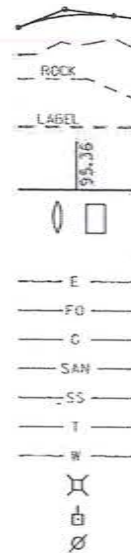
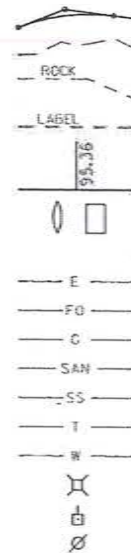
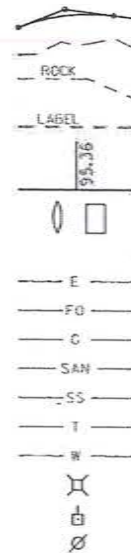
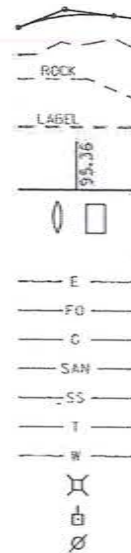
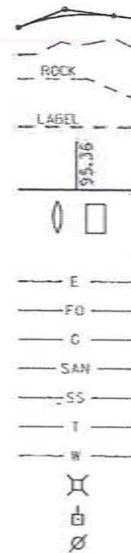
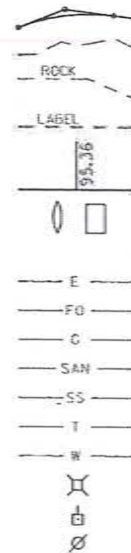
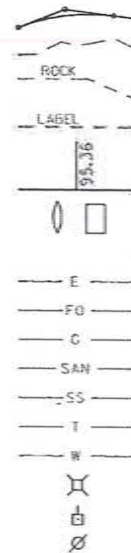
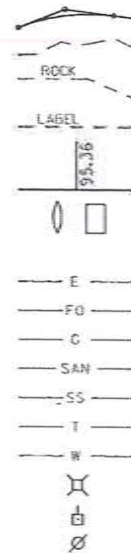
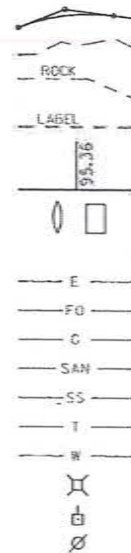
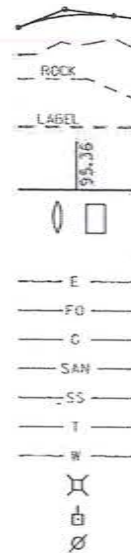
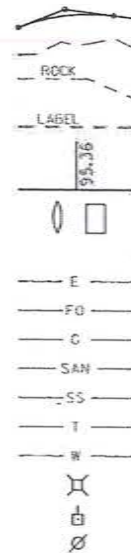
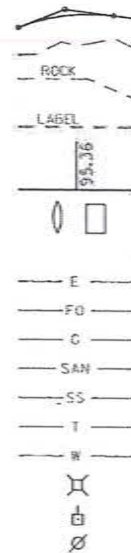
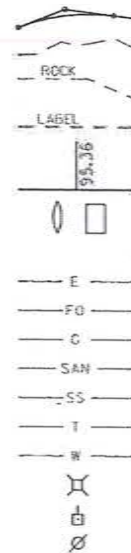
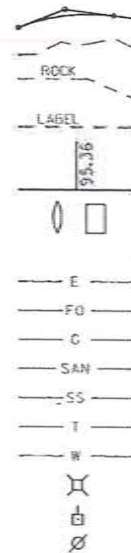
DESIGN DESIGNATION	STH 67
A.A.D.T. (2011)	= 1,000
A.A.D.T. (2030)	= 1,300
D.H.V.	=
D.D.	= 60/40
T.	= 12.9
DESIGN SPEED	= 55 MPH
ESALS	= 379,600

CONVENTIONAL SYMBOLS

PLAN	///
CORPORATE LIMITS	---
PROPERTY LINE	---
LOT LINE	---
LIMITED HIGHWAY EASEMENT	---
EXISTING RIGHT OF WAY	---
PROPOSED OR NEW R/W LINE	---
SLOPE INTERCEPT	---
REFERENCE LINE	---
EXISTING CULVERT	---
PROPOSED CULVERT (Box or Pipe)	---
COMBUSTIBLE FLUIDS	CAUTION
MARSH AREA	---
WOODED OR SHRUB AREA	---

PROFILE	---
GRADE LINE	---
ORIGINAL GROUND	---
MARSH OR ROCK PROFILE (To be noted as such)	---
SPECIAL DITCH	---
GRADE ELEVATION	---
CULVERT (Profile View)	---
UTILITIES	---
ELECTRIC	---
FIBER OPTIC	---
GAS	---
SANITARY SEWER	---
STORM SEWER	---
TELEPHONE	---
WATER	---
UTILITY PEDESTAL	---
POWER POLE	---
TELEPHONE POLE	---

BEGIN PROJECT
STA. 50+25
X=535931.73
Y=205443.59



UTILITY COMPANIES

ALLIANT ENERGY (ELECTRICITY)
JASON HOGAN
SUITE 1000
4902 N. Baltimore Lane
MADISON, WI 53718
p: 608-458-4871
jasonhogan@alliantenergy.com

BERGEN/SHARON TELEPHONE COMPANY-COMMUNCATION LINE
BRAD ELLEFSON/JOHN COWGER
105 PLAIN ST.
P.O. BOX 400
SHARON, WI 53585-0400
p: 262-736-9981
bellef@elknet.net
P: 262-215-6206
john@sharontelephone.com

VILLAGE OF SHARON
KEVIN DAY
P.O. BOX 379
SHARON, WI 53589
p: 262-736-4888
info@villageofsharon.com

We ENERGIES - GAS/PETROLEUM
DAN SANDE
333 W. EVERETT ST, A299
MILWAUKEE, WI 53203
p: 414-221-4578
Dan.Sande@we-energies.com

Rock Energy Coop
Lynn Maier
Janesville, WI 53545
P: 608-752-4550
lynnm@rock.coop

GENERAL NOTES

HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN
12.5 mm MIX GRADATION TO BE USED IN BOTH SCRATCH AND UPPER LAYERS.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN
TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS
FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING OR PARKING LANE.

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STATE OF
WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE
CONSTRUCTION" CURRENT EDITION, THE DETAILS IN THE PLANS, AND THE
SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

PRIOR TO THE INSTALLATION OF TRAFFIC CONTROL DEVICES THE CONTRACTOR
SHALL PROVIDE THE ENGINEER AND THE STATE HIGHWAY PATROL WITH THE NAME
AND TELEPHONE NUMBER OF A LOCAL PERSON RESPONSIBLE FOR THE EMERGENCY
MAINTENANCE OF TRAFFIC CONTROL.

THE EXACT LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES
SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE CONTRACTOR SHALL PLACE ALL TRAFFIC CONTROL SIGNS IN ADVANCE OF THE
PROJECT TERMINI BEFORE BEGINNING ANY WORK ON THE ROADWAY. THESE SIGNS
SHALL REMAIN IN PLACE UNTIL ALL WORK IS COMPLETED AND PERMANENT SIGNING
IS IN PLACE.

ALL EXISTING SIGNS SHALL REMAIN IN PLACE UNTIL PROPOSED SIGNS ARE
INSTALLED UNLESS THE ENGINEER APPROVES THEIR REMOVAL.

THE LOCATION OF STOP LINES SHALL BE DETERMINED IN THE FIELD BY THE
ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT
ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON
SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, ALL SIGNS
RELATING TO THE OPERATION SHALL BE COVERED OR REMOVED AND FACILITY
RESTORED TO NORMAL OPERATION.

THE EXACT LOCATION AND LIMITS OF PRIVATE ENTRANCES AND FIELD ENTRANCES
SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

WHEN THE QUANTITY OF THE ITEM OF AGGREGATE BASE DENSE IS MEASURED BY
PAYMENT OF THE TON, THE DEPTH OR THICKNESS OF THE COURSE IS APPROXIMATE
AND THE ACTUAL THICKNESS WILL DEPEND ON DISTRIBUTION OF MATERIAL AS
DIRECTED BY THE ENGINEER.

REMOVE AND REPLACE ALL SIGNS AND POSTS WITH THE NEW SIGNS AND POSTS IN
THE SAME GENERAL LOCATION UNLESS OTHERWISE NOTED IN THE PLANS.

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT LIMITS THAT MAY NEED TO
BE RELOCATED. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES
WITH A CALL TO DIGGERS HOTLINE AND A DIRECT CALL TO THE UTILITIES
THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF
DIGGERS HOTLINE.

CONTRACTOR SHALL VERIFY LOCATION OF EXISTING UTILITIES IN THE PROJECT
AREA PRIOR TO CONSTRUCTION.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF
THE ENGINEER.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS
DIRECTED BY THE ENGINEER.

A SAWED JOINT IS REQUIRED WHERE NEW HMA PAVEMENT MEETS EXISTING
PAVEMENT.

CONTRACTOR IS RESPONSIBLE FOR RESHAPING AND FINISHING ANY PREVIOUSLY
GRASSED AREAS WHICH ARE DISTURBED BY CONSTRUCTION OPERATIONS
OUTSIDE THE NORMAL CONSTRUCTION LIMITS.

LOCAL CONTACTS

DESIGN CONTACT

BOLLINGER, LACH & ASSOCIATES, INC.
CINDY FLOWER, P.E.
N 1977 SCHAITEI RD. - UNIT 300
LAKE GENEVA, WI 53147
p: 262-249-0900
C: 262-378-2200
cflower@bollingerlach.com

WDNR

AMANDA CUSHMAN
DNR SOUTH REGION HQ.
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
amanda.cushman@dnr.wi.gov

WISDOT CONTACT

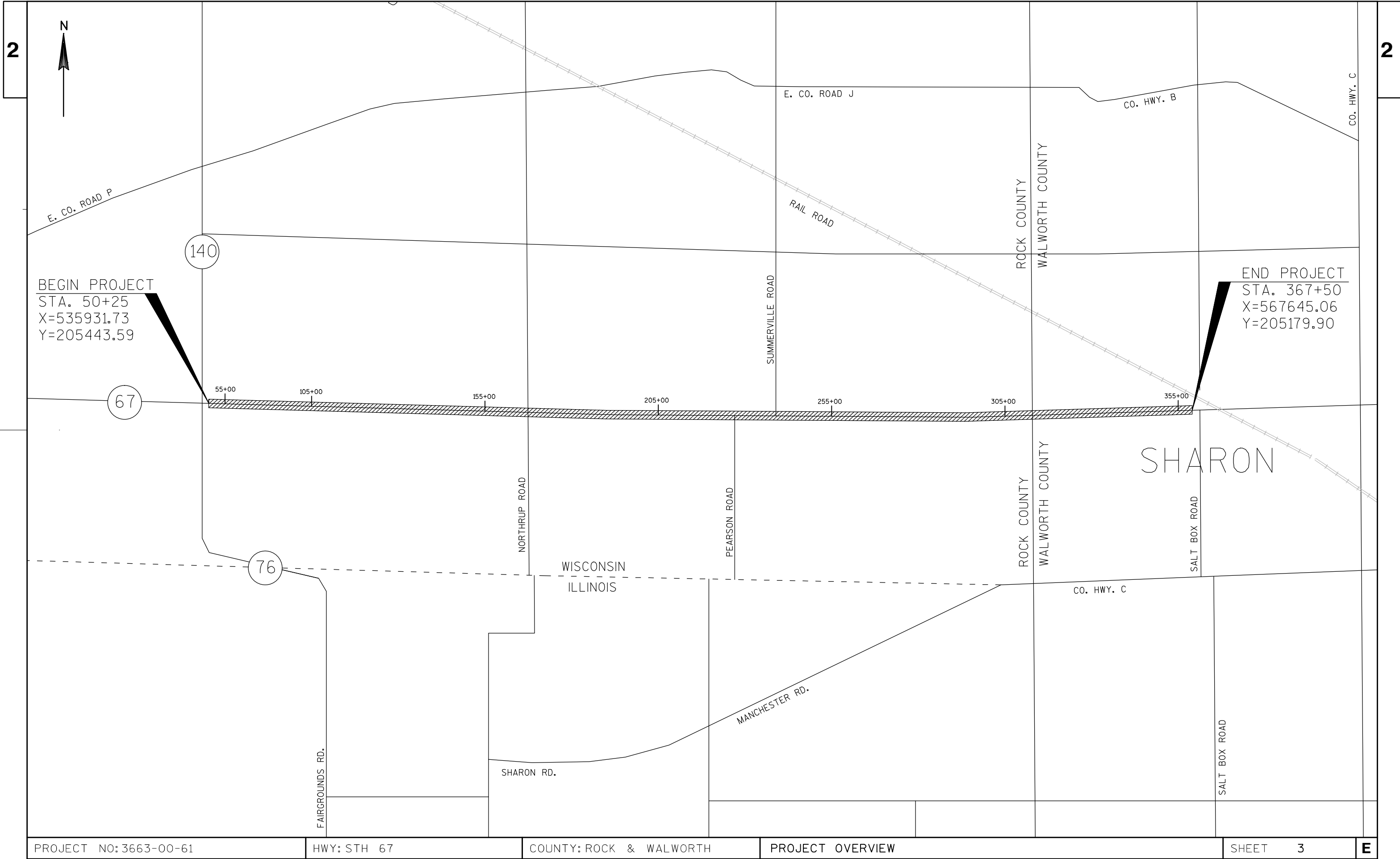
WISCONSIN DEPARTMENT OF TRANSPORTATION
WISDOT SOUTHWEST REGION
WILLIAM STROBEL
2101 WRIGHT ST.
MADISON, WI 53704
p: 608-242-8009
william.l.strobeledot.wi.gov

ACOE

STACY MARSHALL
U.S. ARMY CORPS OF ENGINEERS
20711 WATERTOWN ROAD, SUITE F
WAUKESHA, WI 53186
P: 262-717-9540, EXT. 5
stacy.l.marshall@usace.army.mil



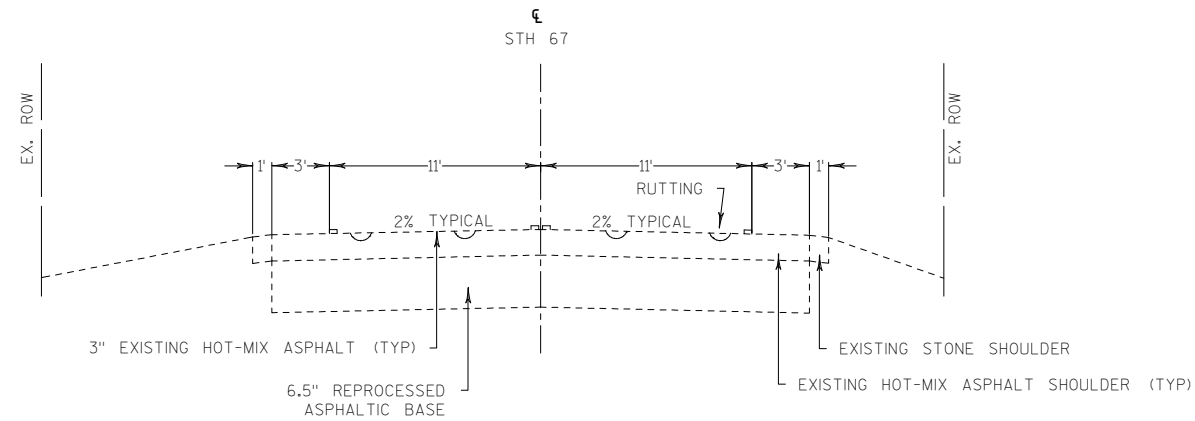
Call 811 3 Work Days Before You Dig
Or Toll Free (800) 242-8511
Hearing Impaired TDD (800) 542-2289
www.DiggersHotline.com



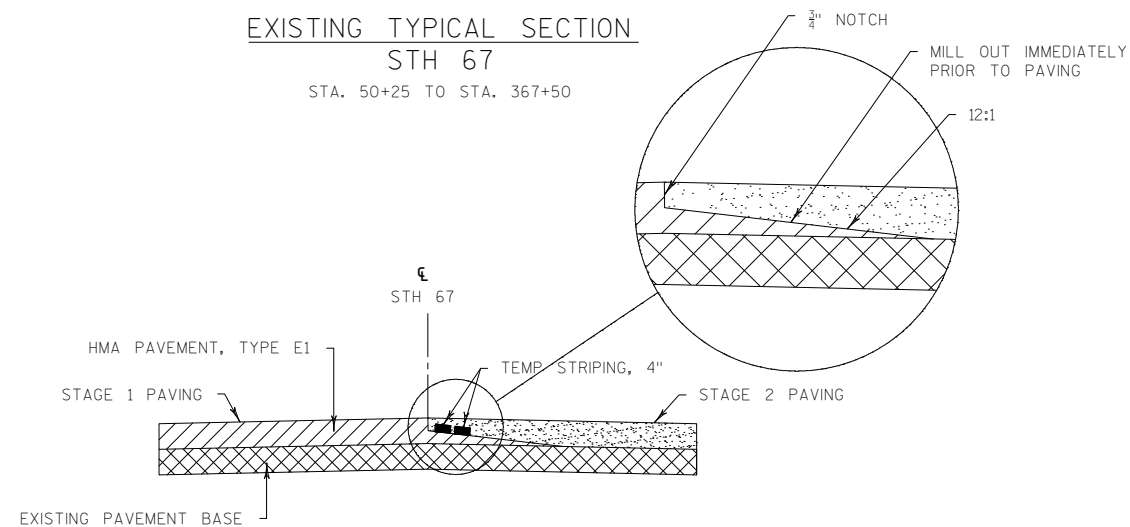
BEGIN PROJECT
STA. 50+25
X=535931.73
Y=205443.59

END PROJECT
STA. 367+50
X=567645.06
Y=205179.90

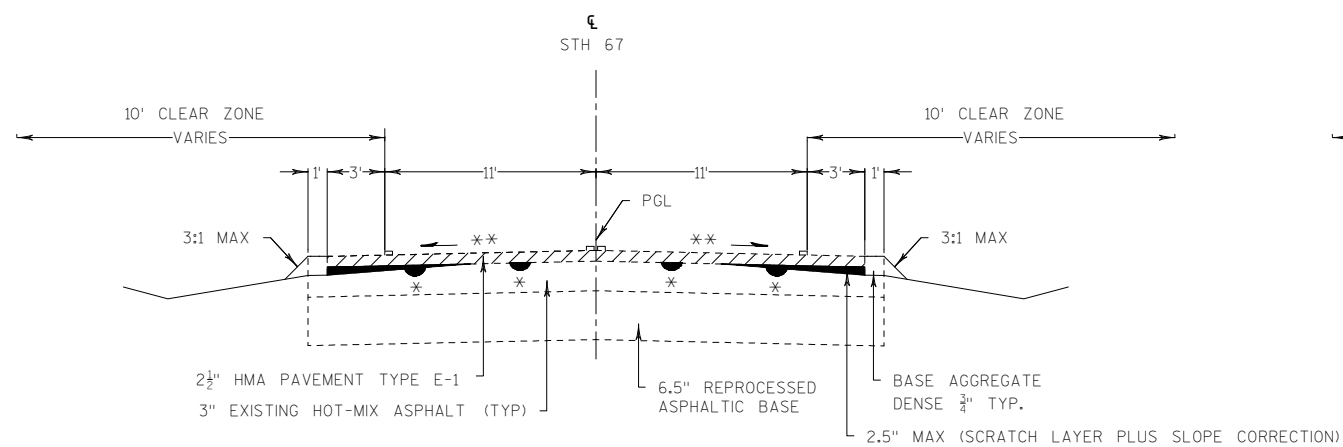
PROJECT NO: 3663-00-61	HWY: STH 67	COUNTY: ROCK & WALWORTH	PROJECT OVERVIEW	SHEET 3	E
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EXISTING TYPICAL SECTION
STH 67
STA. 50+25 TO STA. 367+50

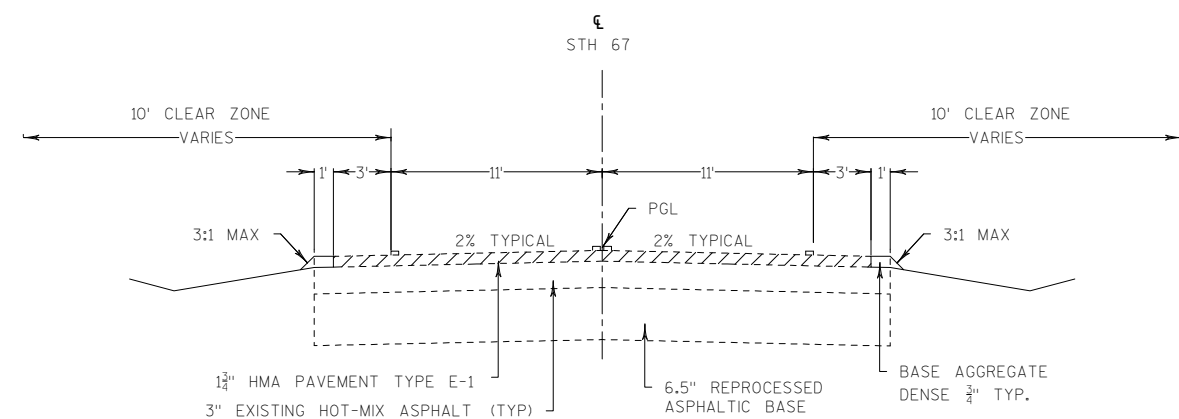


TYPICAL PAVEMENT CROSS SECTION
OF NOTCHED WEDGE LONGITUDINAL JOINTS



FINISHED TYPICAL SECTION
STH 67

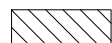
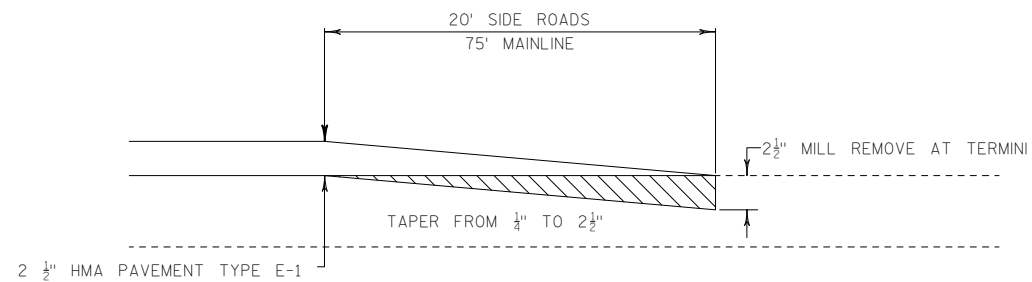
STA. 50+25 TO STA. 233+00 ** 1.5% MIN
STA. 235+00 TO STA. 307+50 2.0% MAX



FINISHED TYPICAL SECTION
STH 67

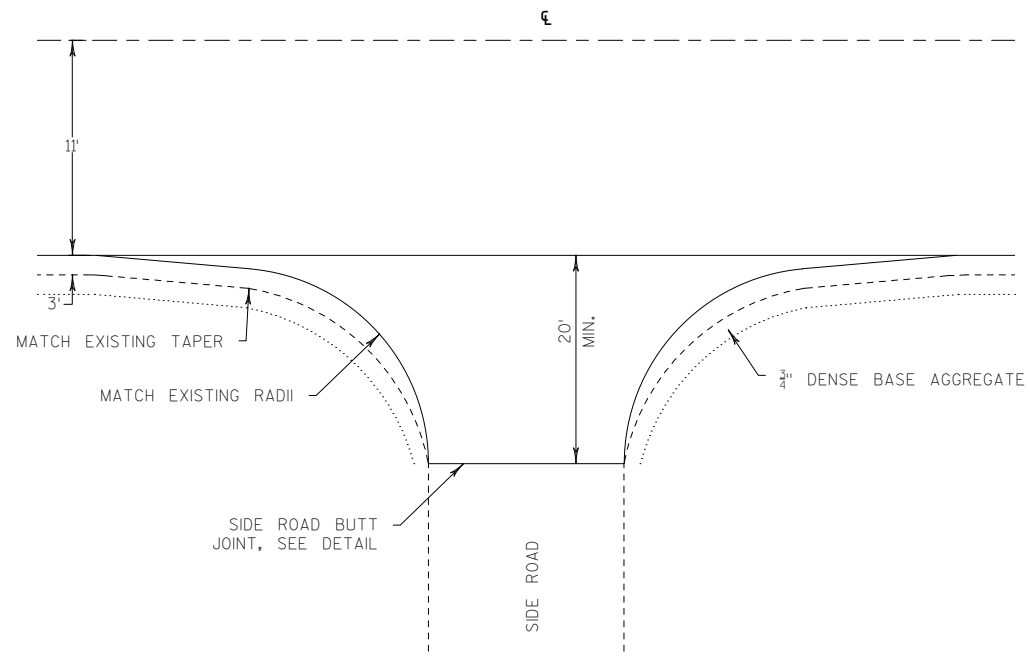
STA. 233+00 TO STA. 235+00

* SCRATCH LAYER HMA PAVEMENT TYPE E-1, SCRATCH LAYER SHALL BE PLACED PRIOR TO THE SURFACE LAYER. SEE MQ FOR NOTES AND LOCATIONS

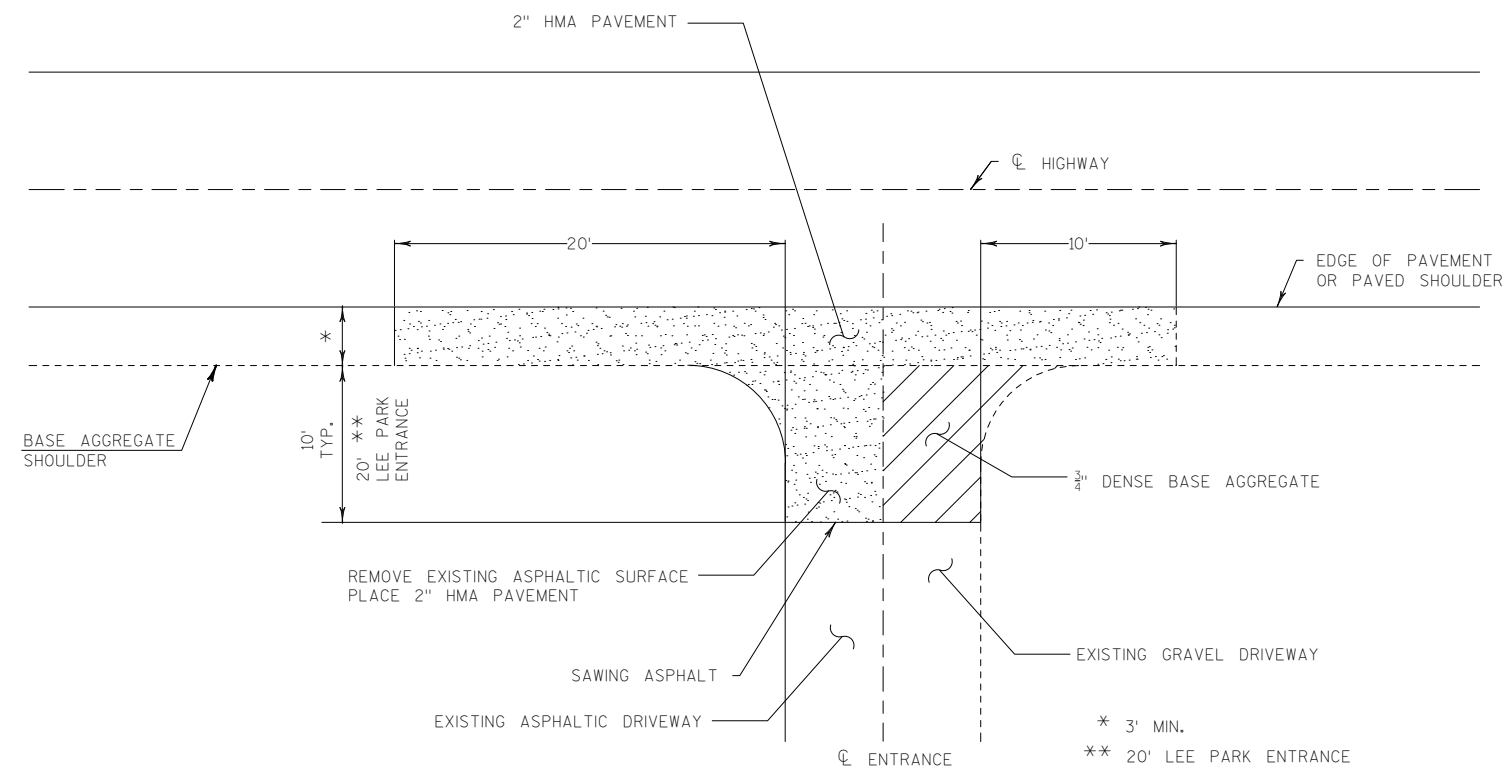


REMOVE ASPHALTIC SURFACE BUTT JOINTS

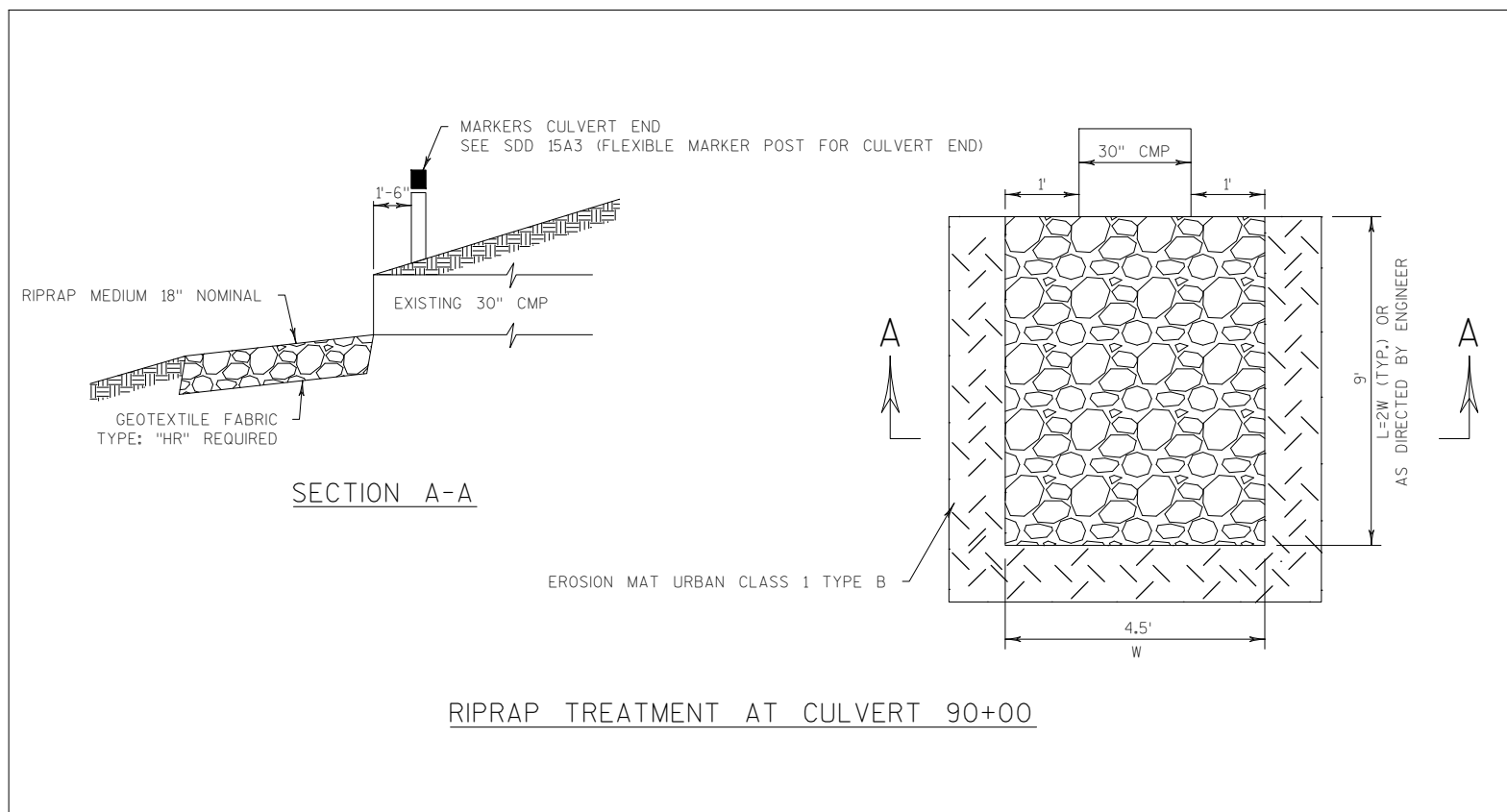
BUTT JOINT DETAIL



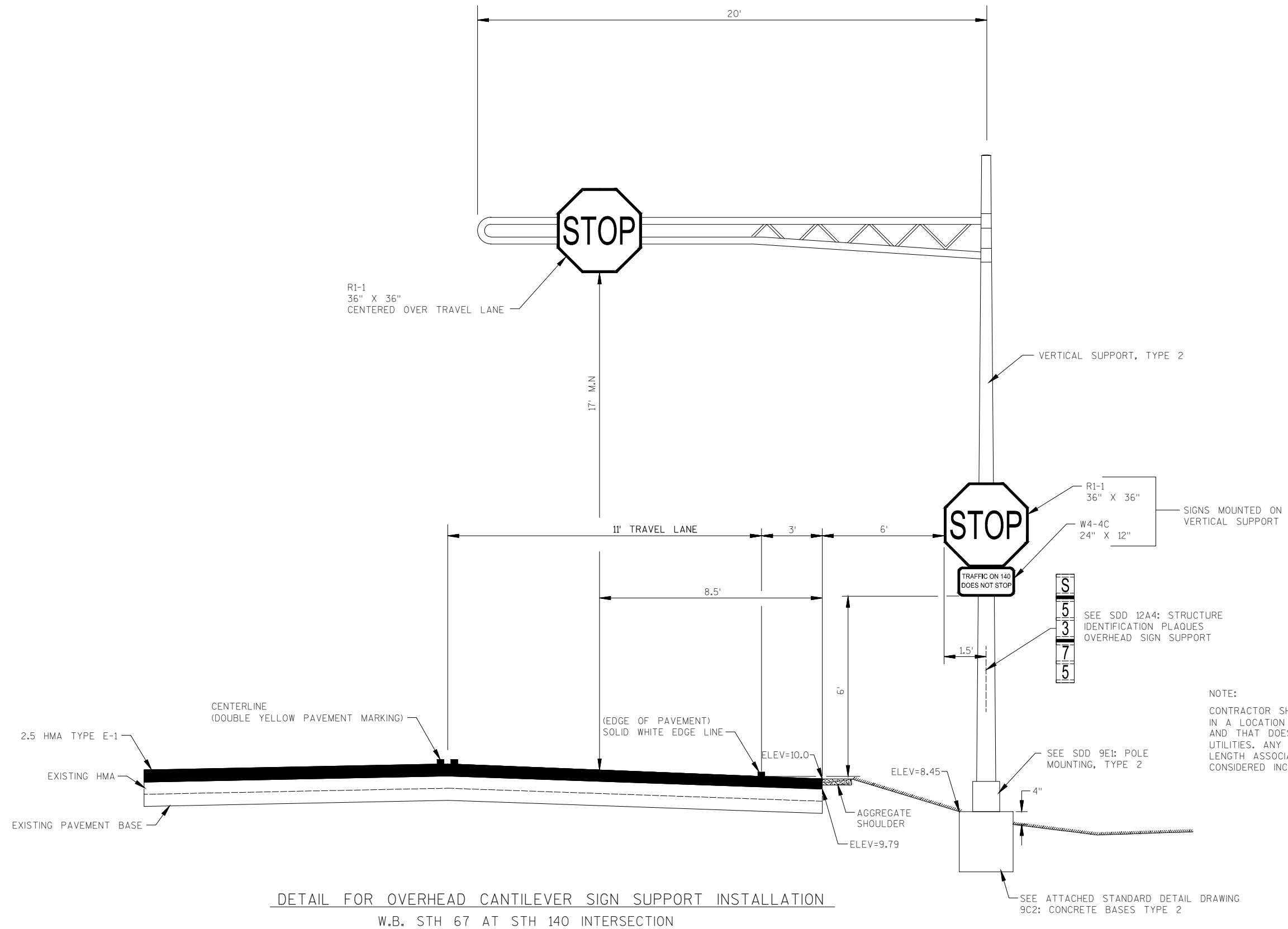
TYPICAL SIDE ROAD DETAIL

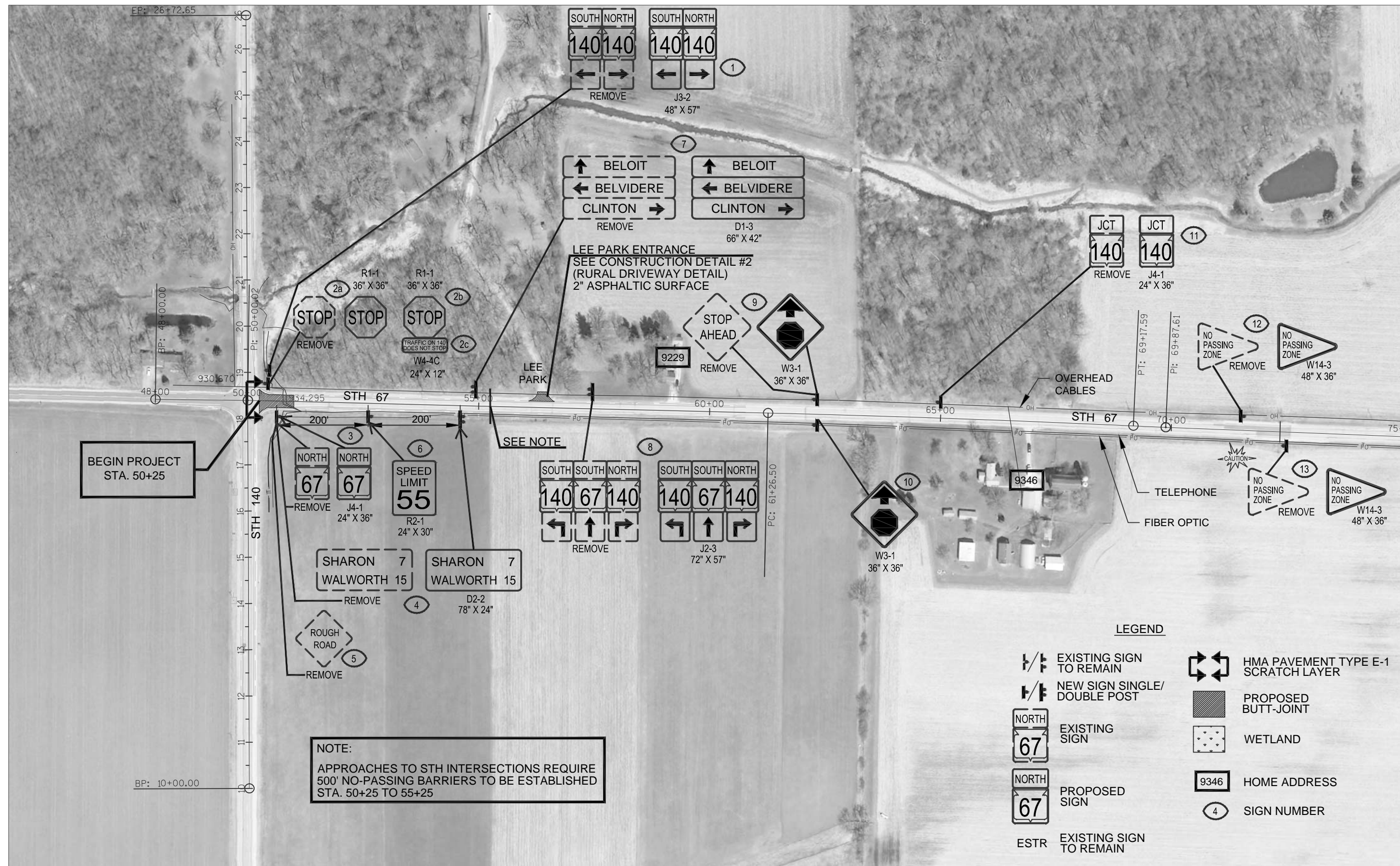


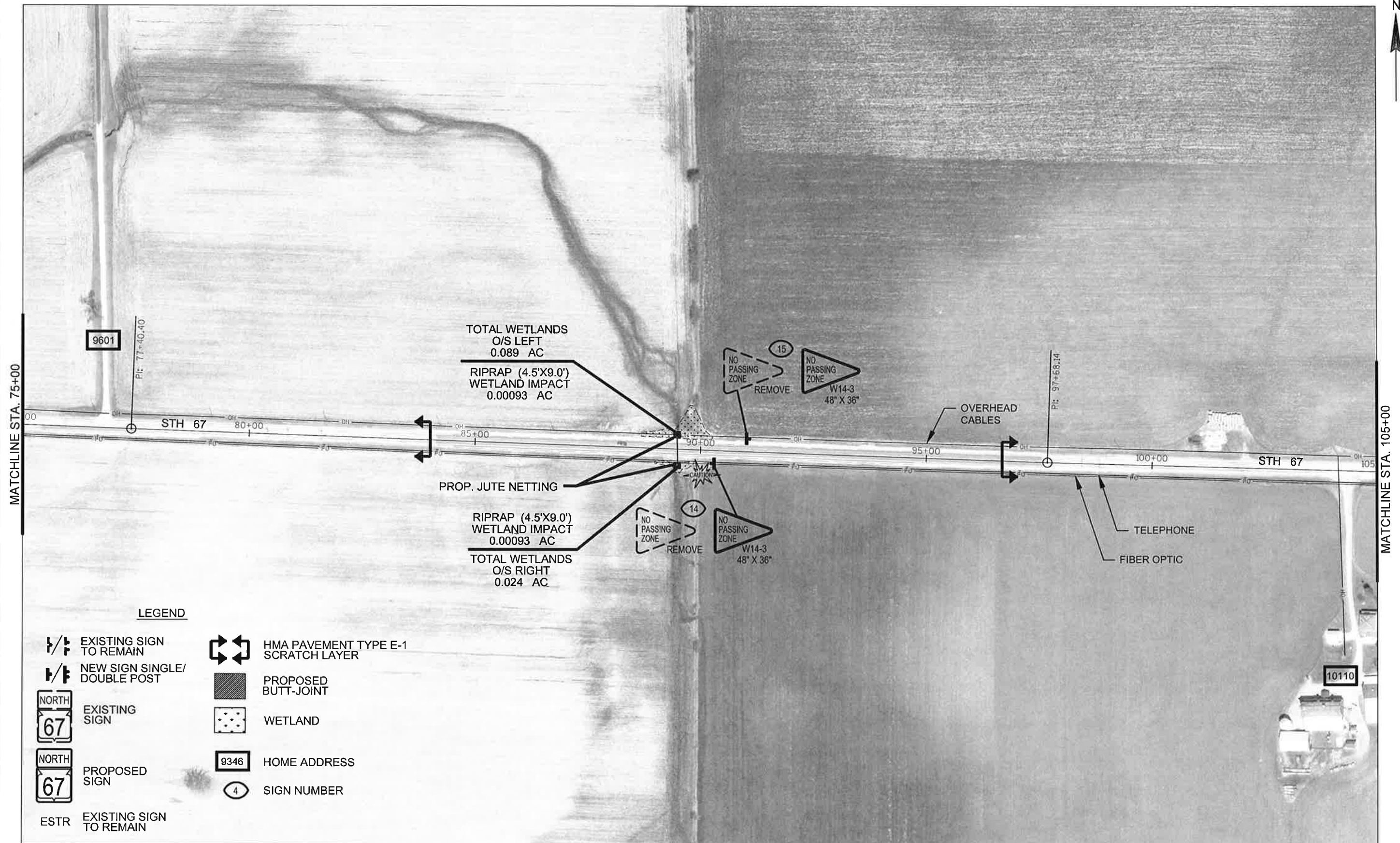
RURAL DRIVEWAY DETAIL



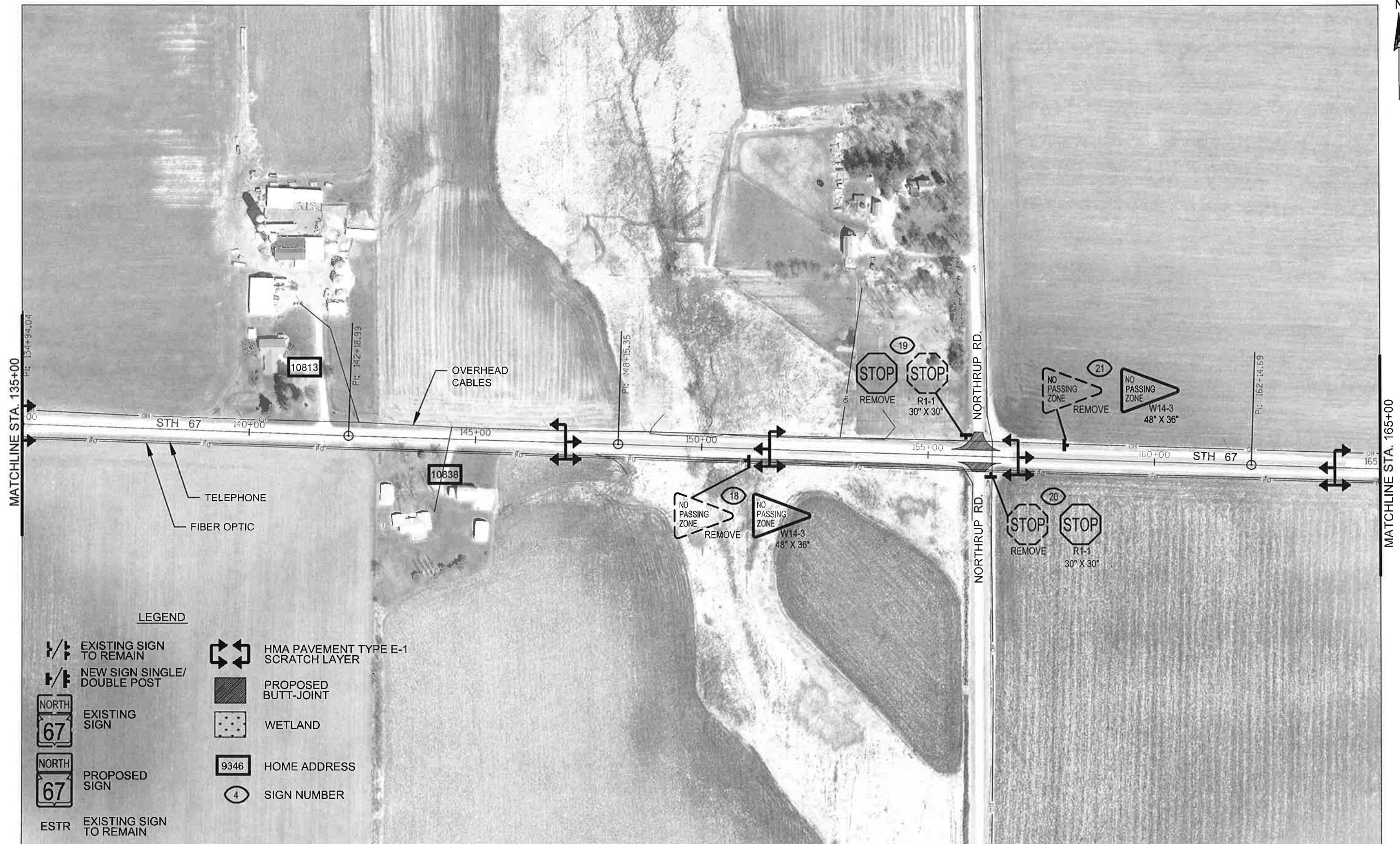
RIPRAP TREATMENT AT CULVERT 90+00





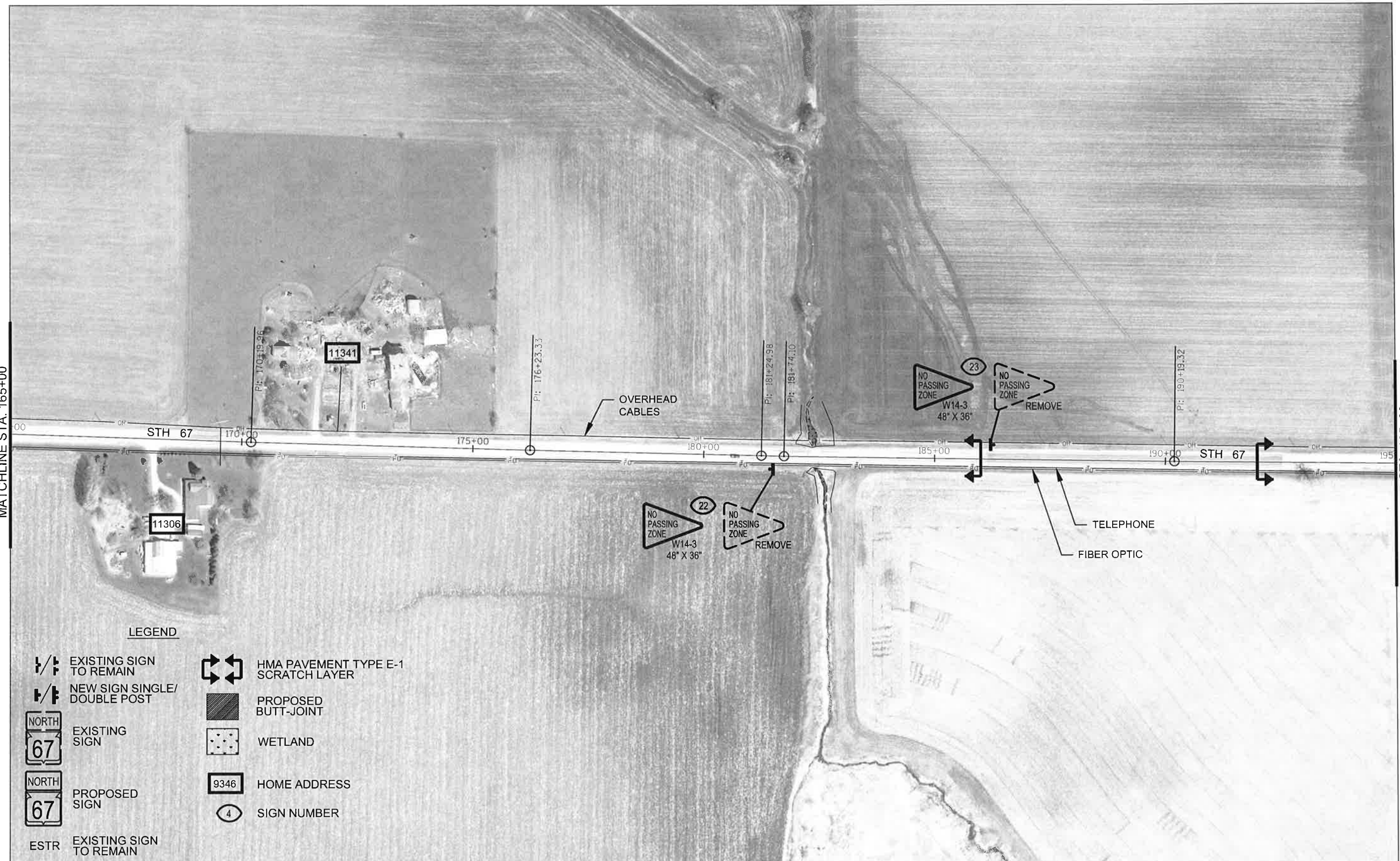


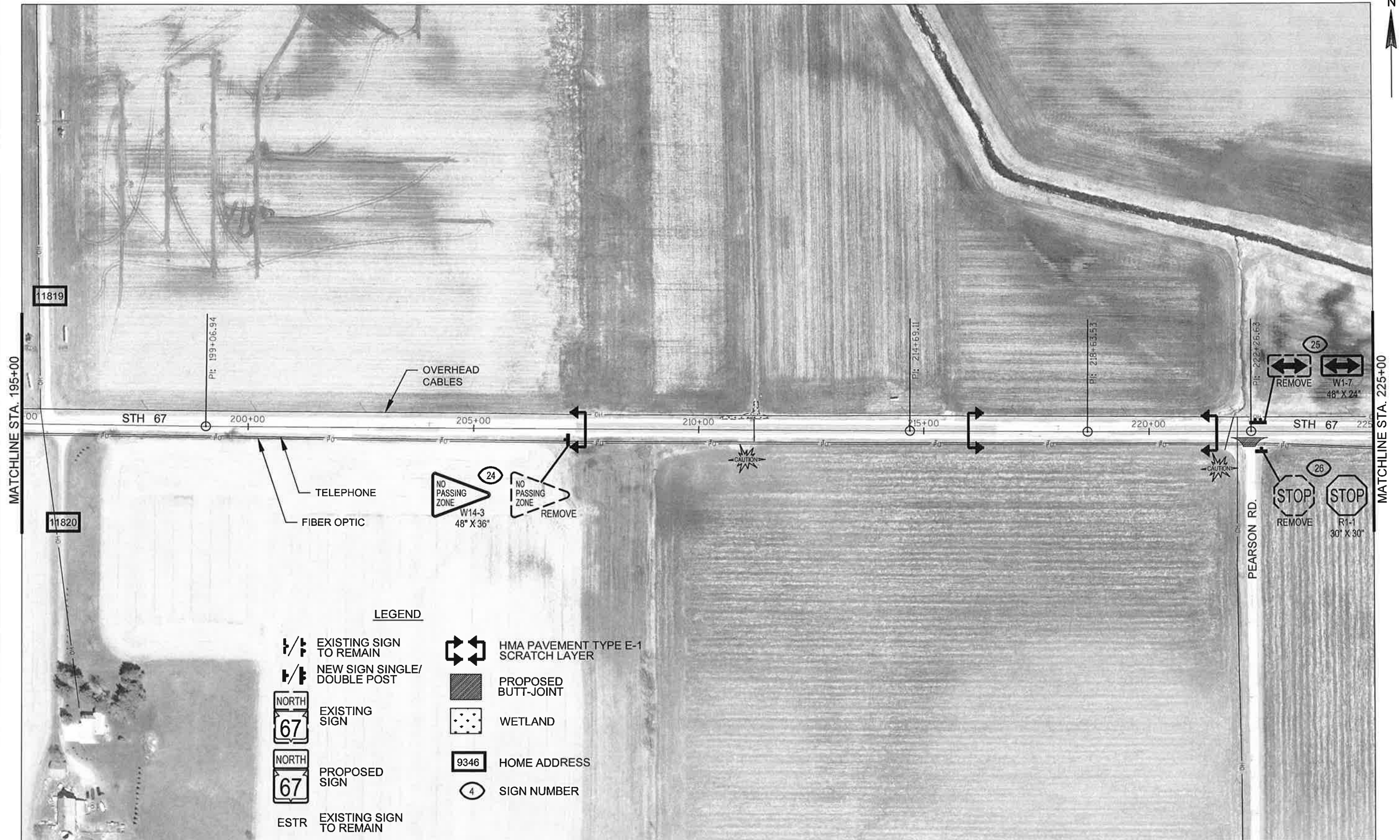




MATCHLINE STA. 165+00

MATCHLINE STA. 195+00

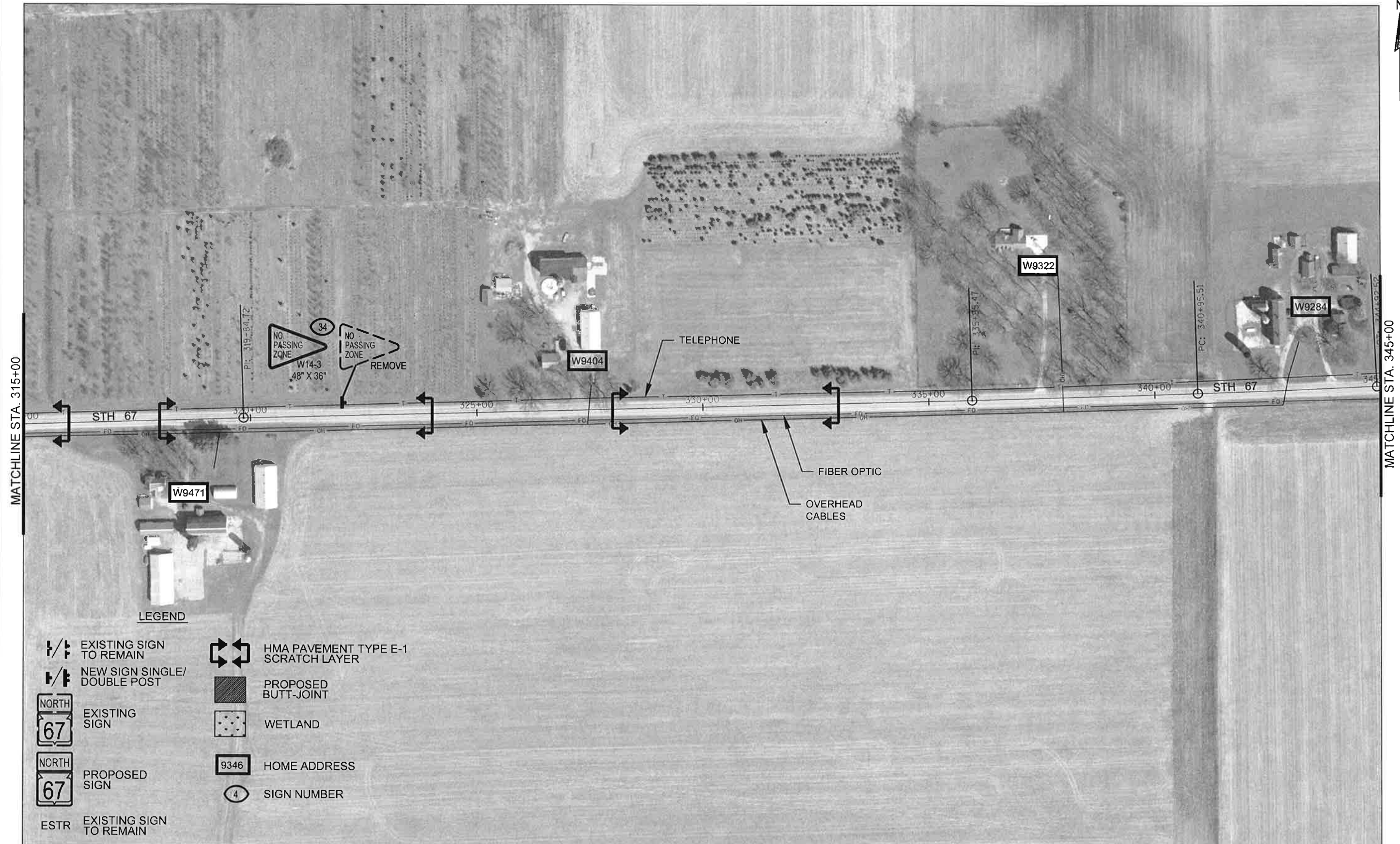








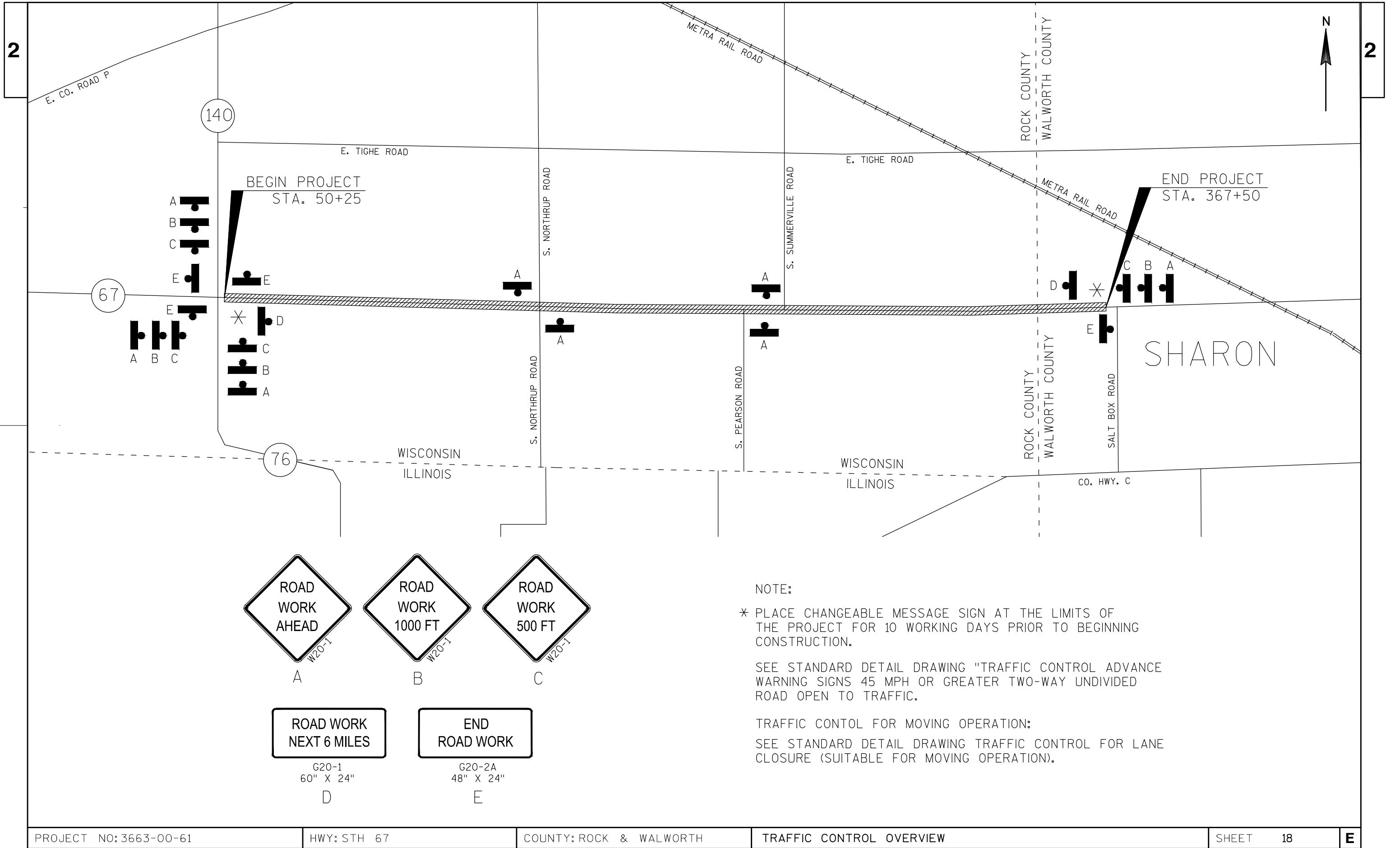






LEGEND

	EXISTING SIGN TO REMAIN		HMA PAVEMENT TYPE E-1 SCRATCH LAYER
	NEW SIGN SINGLE/DOUBLE POST		PROPOSED BUTT-JOINT
	EXISTING SIGN		WETLAND
	PROPOSED SIGN		HOME ADDRESS
	EXISTING SIGN TO REMAIN		SIGN NUMBER



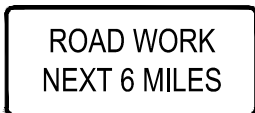
A



B

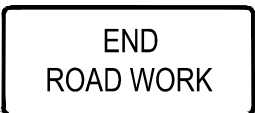


C



G20-1
60" X 24"

D



G20-2A
48" X 24"

E

NOTE:

* PLACE CHANGEABLE MESSAGE SIGN AT THE LIMITS OF THE PROJECT FOR 10 WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION.

SEE STANDARD DETAIL DRAWING "TRAFFIC CONTROL ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC.

TRAFFIC CONTOL FOR MOVING OPERATION:

SEE STANDARD DETAIL DRAWING TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATION).

DATE 27MAR13		E S T I M A T E O F Q U A N T I T I E S			
LINE					3663-00-61
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0010	204.0115	REMOVING ASPHALTIC SURFACE BUTT JOINTS	SY	1,020.000	1,020.000
0020	213.0100	FINISHING ROADWAY (PROJECT) 01.	EACH	1.000	1.000
		3663-00-61			
0030	305.0110	BASE AGGREGATE DENSE 3/4-INCH	TON	2,000.000	2,000.000
0040	440.4410.S	INCENTIVE IRI RIDE	DOL	24,040.000	24,040.000
0050	455.0140	ASPHALTIC MATERIAL PG64-28P	TON	980.000	980.000
0060	455.0605	TACK COAT	GAL	4,200.000	4,200.000
0070	460.1101	HMA PAVEMENT TYPE E-1	TON	17,850.000	17,850.000
0080	460.2000	INCENTIVE DENSITY HMA PAVEMENT	DOL	11,430.000	11,430.000
0090	606.0200	RI PRAP MEDIUM	CY	5.000	5.000
0100	618.0100	MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) 01. 3663-00-61	EACH	1.000	1.000
0110	619.1000	MOBILIZATION	EACH	1.000	1.000
0120	628.1905	MOBILIZATIONS EROSION CONTROL	EACH	1.000	1.000
0130	628.1910	MOBILIZATIONS EMERGENCY EROSION CONTROL	EACH	1.000	1.000
0140	628.2008	EROSION MAT URBAN CLASS I TYPE B	SY	70.000	70.000
0150	633.5200	MARKERS CULVERT END	EACH	2.000	2.000
0160	634.0612	POSTS WOOD 4X6-INCH X 12-FT	EACH	1.000	1.000
0170	634.0614	POSTS WOOD 4X6-INCH X 14-FT	EACH	7.000	7.000
0180	634.0616	POSTS WOOD 4X6-INCH X 16-FT	EACH	26.000	26.000
0190	634.0618	POSTS WOOD 4X6-INCH X 18-FT	EACH	5.000	5.000
0200	637.0202	SIGNS REFLECTIVE TYPE II	SF	293.320	293.320
0210	638.2602	REMOVING SIGNS TYPE II	EACH	36.000	36.000
0220	638.3000	REMOVING SMALL SIGN SUPPORTS	EACH	40.000	40.000
0230	642.5001	FIELD OFFICE TYPE B	EACH	1.000	1.000
0240	643.0100	TRAFFIC CONTROL (PROJECT) 01. 3663-00-61	EACH	1.000	1.000
0250	643.0900	TRAFFIC CONTROL SIGNS	DAY	1,188.000	1,188.000
0260	643.1050	TRAFFIC CONTROL SIGNS PCMS	DAY	136.000	136.000
0270	645.0120	GEOTEXTILE FABRIC TYPE HR	SY	9.000	9.000
0280	646.0106	PAVEMENT MARKING EPOXY 4-INCH	LF	63,450.000	63,450.000
0290	646.0406	PAVEMENT MARKING SAME DAY EPOXY 4-INCH	LF	30,750.000	30,750.000
0300	647.0566	PAVEMENT MARKING STOP LINE EPOXY 18-INCH	LF	55.000	55.000
0310	648.0100	LOCATING NO-PASSING ZONES	MI	6.010	6.010
0320	649.0100	TEMPORARY PAVEMENT MARKING 4-INCH	LF	42,400.000	42,400.000
0330	649.1100	TEMPORARY PAVEMENT MARKING STOP LINE 18-INCH	LF	55.000	55.000
0340	654.0102	CONCRETE BASES TYPE 2	EACH	1.000	1.000
0350	657.0255	TRANSFORMER BASES BREAKAWAY 11 1/2-INCH BOLT CIRCLE	EACH	1.000	1.000
0360	657.0305	POLES TYPE 2	EACH	1.000	1.000
0370	657.0590	TROMBONE ARMS 20-FT	EACH	1.000	1.000
0380	690.0150	SAWING ASPHALT	LF	200.000	200.000
0390	SPV.0090	SPECIAL 01. REMOVING HMA PAVEMENT NOTCHED WEDGE LONGITUDINAL JOINT MILLING	LF	31,725.000	31,725.000

REMOVAL ITEMS		
(204.0115) REMOVING ASPHALTIC SURFACE BUTT JOINTS		(690.0150) SAWING ASPHALT
LOCATION	(SY)	(LF)
50+25 TO 51+00	255	76
N. NORTHRUP	190	24
S. NORTHRUP	105	24
S. PEARSON	95	24
N. SUMMERVILLE	140	24
366+75 TO 367+50	235	28
TOTAL	1,020	200

BASE AGGREGATE DENSE 3/4 - INCH		
		305.0110 BASE AGGREGATE DENSE 3/4-INCH (TON)
STATION	LOCATION	
50+25 TO 367+50	LT	750
50+25 TO 367+50	RT	750
DRIVEWAYS		500
TOTAL		2,000

HMA PAVEMENT ITEMS						
(440.4410.S) INCENTIVE DENSITY HMA PAVEMENT (DOL)		(455.0140) ASPHALTIC MATERIAL PG 58-28 (TON)	(455.0605) TACK COAT (GAL)	(460.1101) HMA PAVEMENT TYPE E-1 (TON)	(460.2000) INCENTIVE DENSITY HMA PAVEMENT (DOL)	(SPV.0090.01) REMOVING HMA PAVEMENT NOTCHED WEDGE LONGITUDINAL JOINT MILLING (LF)
DESCRIPTION						
SCRATCH LAYER	-	160		2,865	1,840	-
1% SLOPE CORRECTION	-	45	1,710	805	510	-
2.5" OVERLAY	-	760		13,875	8,890	-
NOTCHED WEDGE	-	15	2,490	305	190	31,725
TOTAL	24,040	980	4,200	17,850	11,430	31,725

PAVEMENT MARKING					
(646.0106) PAVEMENT MARKING EPOXY 4-INCH (EDGELINE) (LF)		(647.0406) PAVEMENT MARKING SAME DAY EPOXY 4-INCH (LF)	(647.0566) PAVEMENT MARKING STOP LINE EPOXY 18-INCH (LF)	(649.0100) TEMPORARY PAVEMENT MARKING 4-INCH (LF)	(649.1100) TEMPORARY PAVEMENT MARKING STOP LINE 18-INCH (LF)
LOCATION					
SCRATCH LAYER/SLOPE CORRECTION	-	-	-	16,230	-
2.5' OVERLAY - 1st PASS	-	-	-	26,170	55
2.5' OVERLAY - 1st PASS	63,450	30,750	55	-	-
TOTAL	63,450	30,750	55	42,400	55

EROSION CONTROL					
(606.0200) RIPRAP MEDIUM (CY)		(628.1905) MOBILIZATIONS EROSION CONTROL (EACH)	(628.1910) MOBILIZATIONS EMERGENCY EROSION CONTROL (EACH)	(628.2008) EROSION MAT URBAN CLASS I TYPE B (SY)	(645.0120) GEOTEXTILE FABRIC TYPE HR (SY)
STATION					
90+00	5	1	1	70	9
TOTAL	5	1	1	70	9

TRAFFIC CONTROL			
(643.0100) TRAFFIC CONTROL PROJECT (EACH)		(643.0900) TRAFFIC CONTROL SIGNS (DAY)	(643.1050) TRAFFIC CONTROL PCMS (DAY)
DAYS			
68	1	-	136
54	-	1,188	-
TOTAL	1	1,188	136

3

3

SIGN REPAIRS																	
SIGN NO.	LOCATION	SIGN CODE	SIGN DESCRIPTION	(634.0612) Post Wood 4x6-INCH x 12-FT (EACH)	(634.0614) Post Wood 4x6-INCH x 14-FT (EACH)	(634.0616) Post Wood 4x6-INCH x 16-FT (EACH)	(634.0618) Post Wood 4x6-INCH x 18-FT (EACH)	SIZE (IN)	AREA (SF)	(637.0202) SIGN REFLECTIVE TYPE II	(638.2602) REMOVING SIGNS TYPE II (EACH)	(638.3000) REMOVING SMALL SIGN SUPPORTS (EACH)	(657.0305) POLES TYPE 2 (EACH)	(654.0102) CONCRETE BASES TYPE II (EACH)	(657.0255) TRANSFORMER BASES BREAKAWAY 11 1/2 - INCH BOLT CIRCLE (EACH)	(657.0590) TROMBONE ARMS 20-FT (EACH)	REMARKS
1	LT	J3-2	DIRECTIONAL ASSEMBLY (2 HEAD ROUTE PANEL)				1	48	57	19.00	1	1					
2a	LT	R1-1	STOP					36	36	7.46	1	1	1	1	1	1	1
2b	LT	R1-1	STOP					24	12	2.00							
2c	LT	W4-4C	TRAFFIC ON 140 DOES NOT STOP					36	36	7.46							
3	RT	J4-1	REASSURANCE ASSEMBLY (1 HEADED ROUTE PANEL)		1			24	36	6.00	1	2					
4	RT	D2-2	DESTINATION/DISTANCE (TWO) WITH DIE CUT LETTERS		2			78	24	13.00	1						
5	RT	W8-8	ROUGH ROAD					00	00	0.00	1						
6	RT	R2-1	SPEED LIMIT 55 MPH		1			24	30	5.00							
7	LT	D1-3	TRIPLE DESTINATION/ARROW			2		66	42	19.25	1	2					
8	LT	J2-3	ROUTE TURN ASSEMBLY (3 HEAD ROUTE PANEL)				2	72	57	28.50	1	2					
9	LT	W3-1	STOP AHEAD			1		36	36	9.00	1	1					
10	RT	W3-1	STOP AHEAD			1		36	36	9.00							
11	LT	J4-1	REASSURANCE ASSEMBLY (1 HEADED ROUTE PANEL)			1		24	36	6.00	1	1					
12	LT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
13	RT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
14	RT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
15	LT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
16	RT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
17	LT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
18	RT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
19	LT	R1-1	STOP			1		30	30	5.18	1	1					
20	RT	R1-1	STOP			1		30	30	5.18	1	1					
21	LT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
22	RT	W14-3	NO PASSING ZONE				1	48	36	6.00	1	1					
23	LT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
24	RT	W14-3	NO PASSING ZONE		1			48	36	6.00	1	1					
25	LT	W1-7	NIGHT ARROW (DOUBLE)			1		48	24	8.00	1	2					
26	RT	R1-1	STOP			1		30	30	5.18	1	1					
27	LT	R1-1	STOP			1		30	30	5.18	1	1					
28	RT	W1-7	NIGHT ARROW (DOUBLE)			1		48	24	8.00	1	2					
29	LT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
30	RT	W14-3	NO PASSING ZONE				1	48	36	6.00	1	1					
31	LT	I2-2	INFORMATION (ONE) WITH DIE CUT LETTERS			1		48	15	5.00	1	1					ROCK CO
32	RT	I2-2	INFORMATION (ONE) WITH DIE CUT LETTERS		2			72	15	7.50	1	2					WALWORTH CO
33	RT	I-55-56	ADOPT-A-HIGHWAY [SPONSOR]					00	00	0.00	0	0					
34	LT	W14-3	NO PASSING ZONE			1		48	36	6.00	1	1					
35a	LT	S3-1	SCHOOL BUS STOP AHEAD			1		36	36	9.00	1	1					
35b	LT	W8-8	ROUGH ROAD					00	00	0.00	1						
36	RT	W2-2	SIDE ROAD (90 DEGREES)			1		30	30	6.25	1	1					
37	LT	W1-7	NIGHT ARROW (DOUBLE)	1				48	24	8.00	1	2					
38	RT	R1-1	STOP			1		30	30	5.18	1	1					
TOTAL				1	7	26	5			293.32	36	40	1	1	1	1	

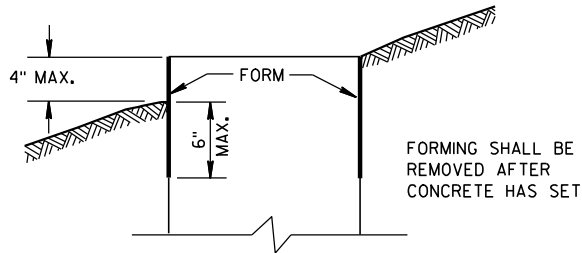
Standard Detail Drawing List

09C02-06	CONCRETE BASES, TYPES 1, 2 & 5
09E01-11A	POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2
12A04-03	STRUCTURE IDENTIFICATION PLAQUES, RAMP GATES, SIGN BRIDGES & OVERHEAD SIGN SUPPORTS & TRAFFIC SIGNALS
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C04-01	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C08-15A	PAVEMENT MARKING (MAINLINE)
15C12-03	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C19-01A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY

Standard Detail Drawing List

09C02-06	CONCRETE BASES, TYPES 1, 2 & 5
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15C19-01A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY

FORM DEPTH SHALL BE NO MORE THAN 6" BELOW GRADE ON THE LOWER SIDE OF BASE



FORMING DETAIL

QUANTITY REQUIREMENTS	CONCRETE BASE TYPE		
	1	2	5
APPROX. CUBIC YARDS OF CONCRETE	0.40	0.57	0.40
LBS. OF HOOP BAR STEEL	NONE	23	16
LBS. OF VERTICAL BAR STEEL	NONE	60	18

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE AS SHOWN ON THE PLANS.

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN AT THE ENTRANCE OF THE BASE.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6 X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NONMETALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

GENERAL NOTES (CONTINUED)

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION OF CABLE OR WIRE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FOR FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NONMETALLIC.

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG, STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 2 AND TYPE 5 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER THE BASE OF THE TYPE 2 AND TYPE 5 BASES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD, ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 AND 641.2.2 OF THE STANDARD SPECIFICATIONS, ASTM A-449, OR ASTM A-687 (GRADE 105).

WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4" "L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND END SHALL NOT BE THREADED.

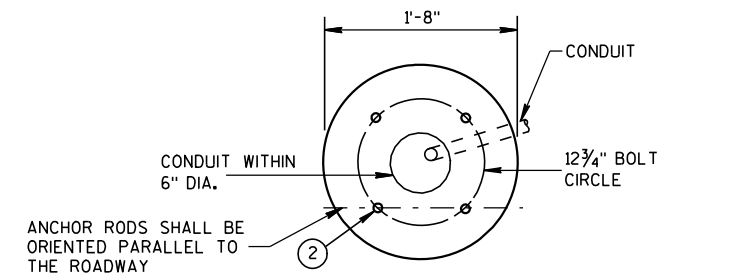
ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

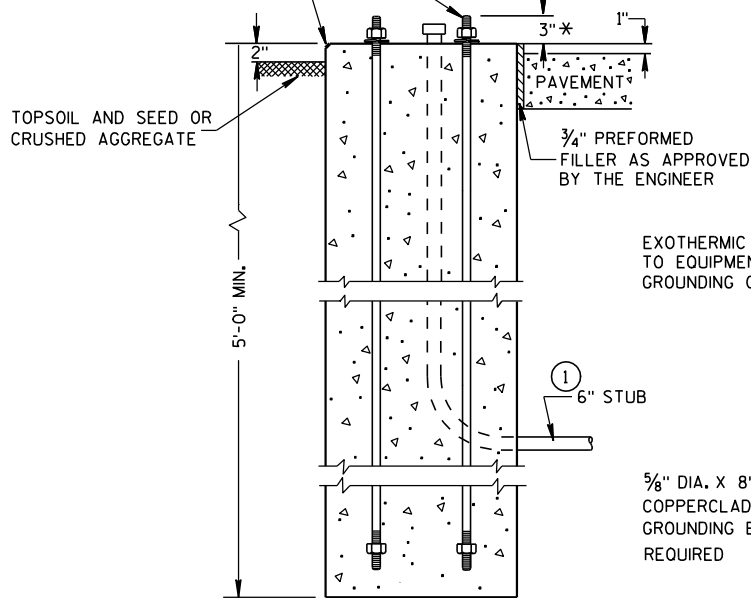
BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).

1 THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL BY THE ENGINEER.

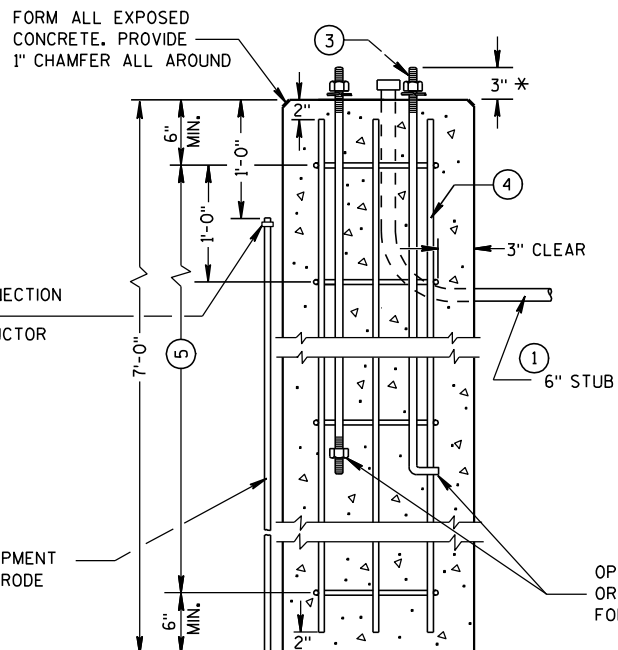
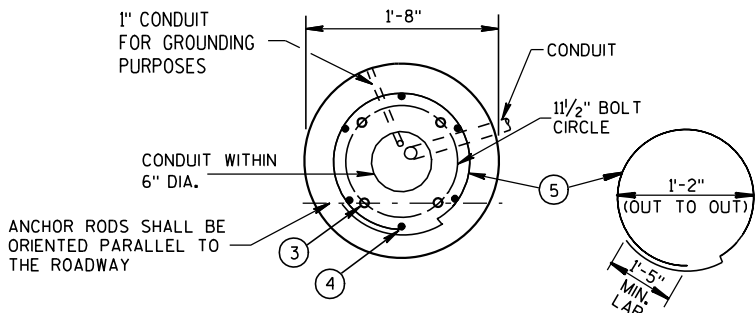
- 2 (4) 1" DIA. X 3'-6" ANCHOR RODS.
- 3 (4) 1" DIA. X 5'-0" ANCHOR RODS.
- 4 (6) NO. 6 X 6'-8" BAR STEEL REINFORCEMENT.
- 5 (7) NO. 4 X 5'-1" BAR STEEL REINFORCEMENT @ 1'-0" C-C.
- 6 (4) 1" DIA. X 3'-6" ANCHOR RODS.
- 7 (6) NO. 4 X 4'-8" BAR STEEL REINFORCEMENT.
- 8 (5) NO. 4 X 5'-1" BAR STEEL REINFORCEMENT @ 1'-0" C-C.



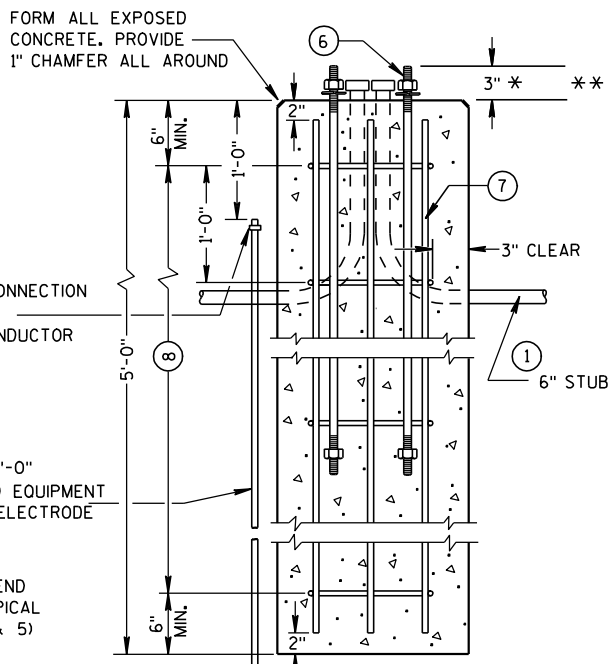
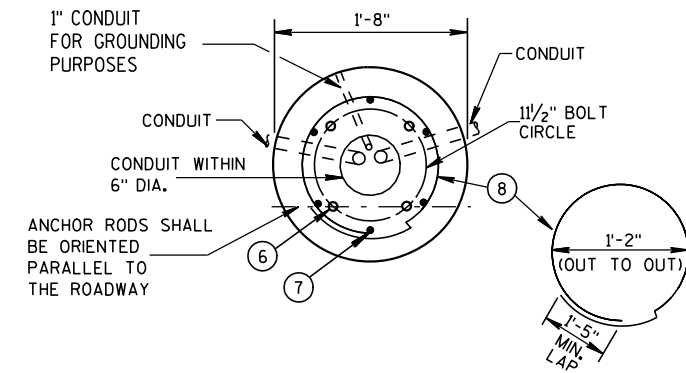
HALF SECTION IN UNPAVED AREA (TYPICAL FOR TYPES 1, 2 & 5)



TYPE 1



TYPE 2



TYPE 5

CONCRETE BASES

* ANY ANCHOR ROD PROJECTION SHORTER THAN 2 3/4" OR LONGER THAN 3 1/4" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

** FOR NONBREAKAWAY INSTALLATIONS, 4 1/2" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.

CONCRETE BASES, TYPES 1, 2 & 5

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

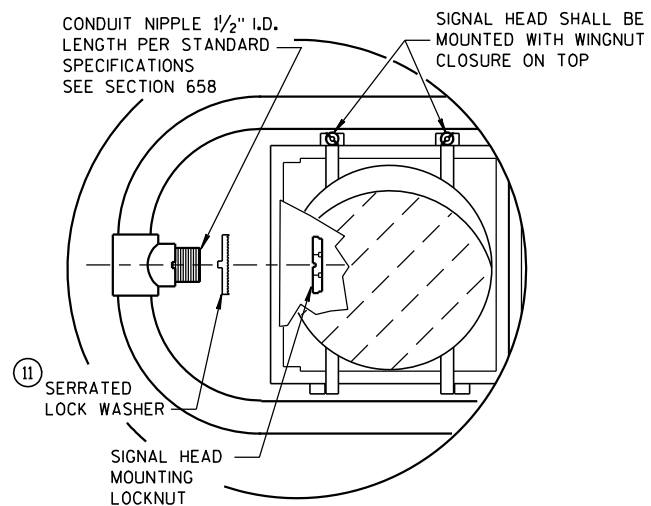
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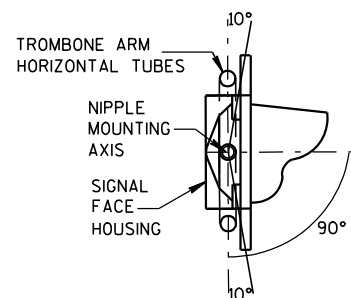
FHWA

/S/ Joanna L. Bush
STATE ELECTRICAL ENGINEER FOR HWYS



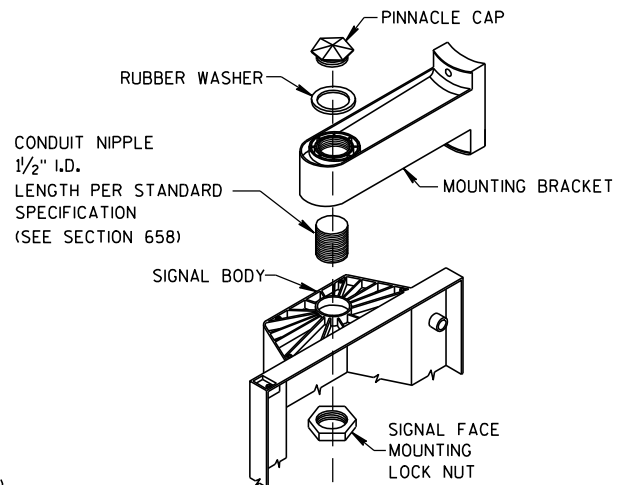
HORIZONTAL SIGNAL HEAD MOUNTING DETAIL *

* SIGNAL HEAD ATTACHMENT ALSO APPLYS TO MOUNTING AT CROSS BAR

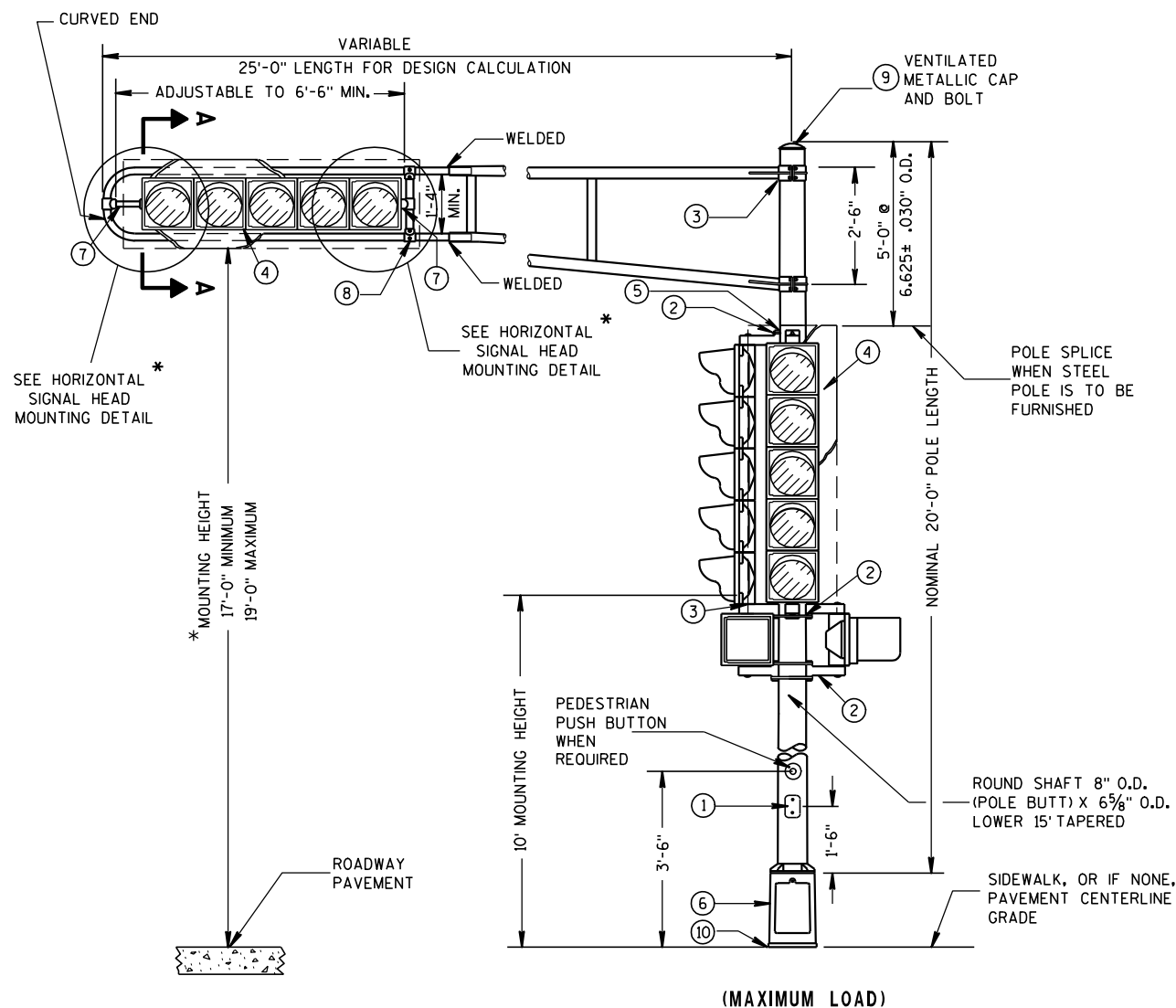


SECTION A-A

(10 DEGREES TILT REQUIREMENT OF FACE(S) IN THE TROMBONE MOUNTING)



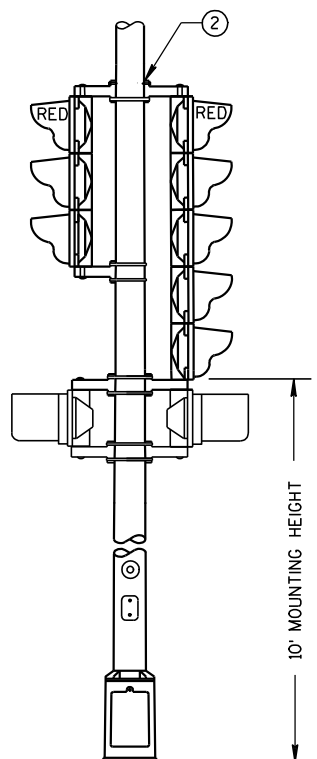
SIGNAL FACE MOUNTING DETAIL (BANDED)



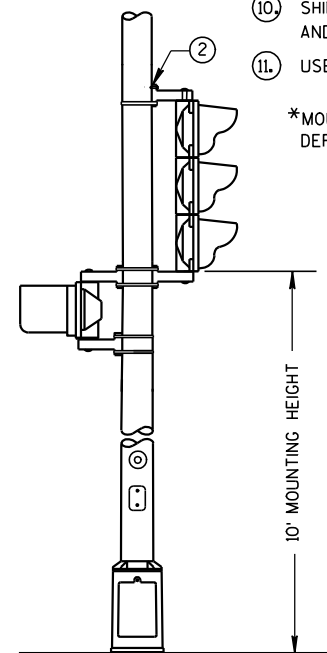
(MAXIMUM LOAD)

TYPICAL MOUNTING OF BACK TO BACK
3 AND 5 SECTION SIGNAL FACES

TYPE 2 POLE MOUNTING CONFIGURATION



TYPICAL MOUNTING OF 3 SECTION
SIGNAL FACE



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

POLES SHALL BE EITHER ALUMINUM OR GALVANIZED STEEL AS CALLED FOR IN THE CONTRACT.

SECTION 657, POLES, OF THE STANDARD SPECIFICATIONS SHALL APPLY TO THIS DRAWING.

A PULL WIRE/ROPE IN ACCORDANCE WITH STANDARD SPECIFICATION 652 SHALL BE INSTALLED IN EACH TROMBONE ARM RACEWAY DURING THE MANUFACTURING PROCESS.

TYPE 2 ALUMINUM POLES SHALL BE CONSTRUCTED OF 6063-T6 ALUMINUM ALLOY. SLEEVEING INSIDE THE POLE IS NOT ACCEPTABLE.

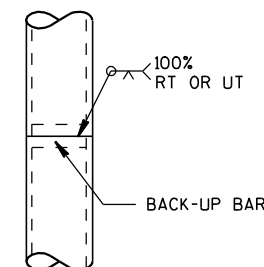
WHEN TRANSFORMER BASES ARE USED, WIRE CONNECTIONS SHALL BE MADE IN THE TRANSFORMER BASE.

- (1) 4" X 6" REINFORCED HANDHOLE & COVER ASSEMBLY WITH 2 (TWO) 1/4" X 3/4" - 20 TPI HEX HEAD STAINLESS STEEL BOLTS.
- (2) SIGNAL FACE MOUNTING BRACKETS. MOUNT WITH CAP SCREWS AND BANDING. (SEE STANDARD SPECIFICATIONS - SEC. 658)
- (3) GROMMETS, 1" CHASE NIPPLES OR 1" CLOSE CONDUIT NIPPLES WITH BUSHINGS SHALL BE PROVIDED FOR 1 3/8" HOLE IN POLE SHAFT FOR WIRING.
- (4) SECURELY MOUNT DULL BLACK POLYCARBONATE BACKPLATES, PROJECTING 5" BEYOND ALL SIDES OF THE SIGNAL FACE HOUSING, PER MANUFACTURER'S RECOMMENDATIONS.
- (5) POLE MOUNTED SIGNAL FACES SHALL REQUIRE 1 OR MORE MOUNTING SPACERS UNDER THE TOP MOUNTING BRACKET(S) AS REQUIRED, TO PLUMB THE SIGNAL FACES.
- (6) CAST ALUMINUM TRANSFORMER BASE, WHEN REQUIRED.
- (7) MOUNTING BRACKET NIPPLES FOR THE SIGNAL FACE(S) SHALL BE 2 INCHES IN LENGTH AND 1/2 INCHES IN DIAMETER. (SEE STANDARD SPECIFICATION - SECTION 658).
- (8) VERTICAL STRUT (ADJUSTABLE). ONE (1) SET SCREW (1/4" X 3/4" LONG-20 TPI, STAINLESS STEEL, HEX HEAD) INTO EACH ARM MEMBER IF STRUT IS THE SLIDING TYPE.
- (9) FURNISH AND INSTALL VENTILATED, CAST, METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH ONE (1) 1/4" X 3/4" - 20 TPI STAINLESS STEEL, HEX HEAD BOLT.
- (10) SHIMMING, IF NEEDED, SHALL BE LOCATED BETWEEN THE CONCRETE FOUNDATION AND THE TRANSFORMER BASE.
- (11) USE SERRATED LOCK WASHERS WITH NOTCHES BETWEEN END TEE AND SIGNAL HEAD.

*MOUNTING HEIGHT LIMITATION DIMENSIONS OF THE TROMBONE MAST ARM WILL BE DEPENDENT UPON THE USE/NON-USE OF A TRANSFORMER BASE.

FOR MANUFACTURERS USE ONLY

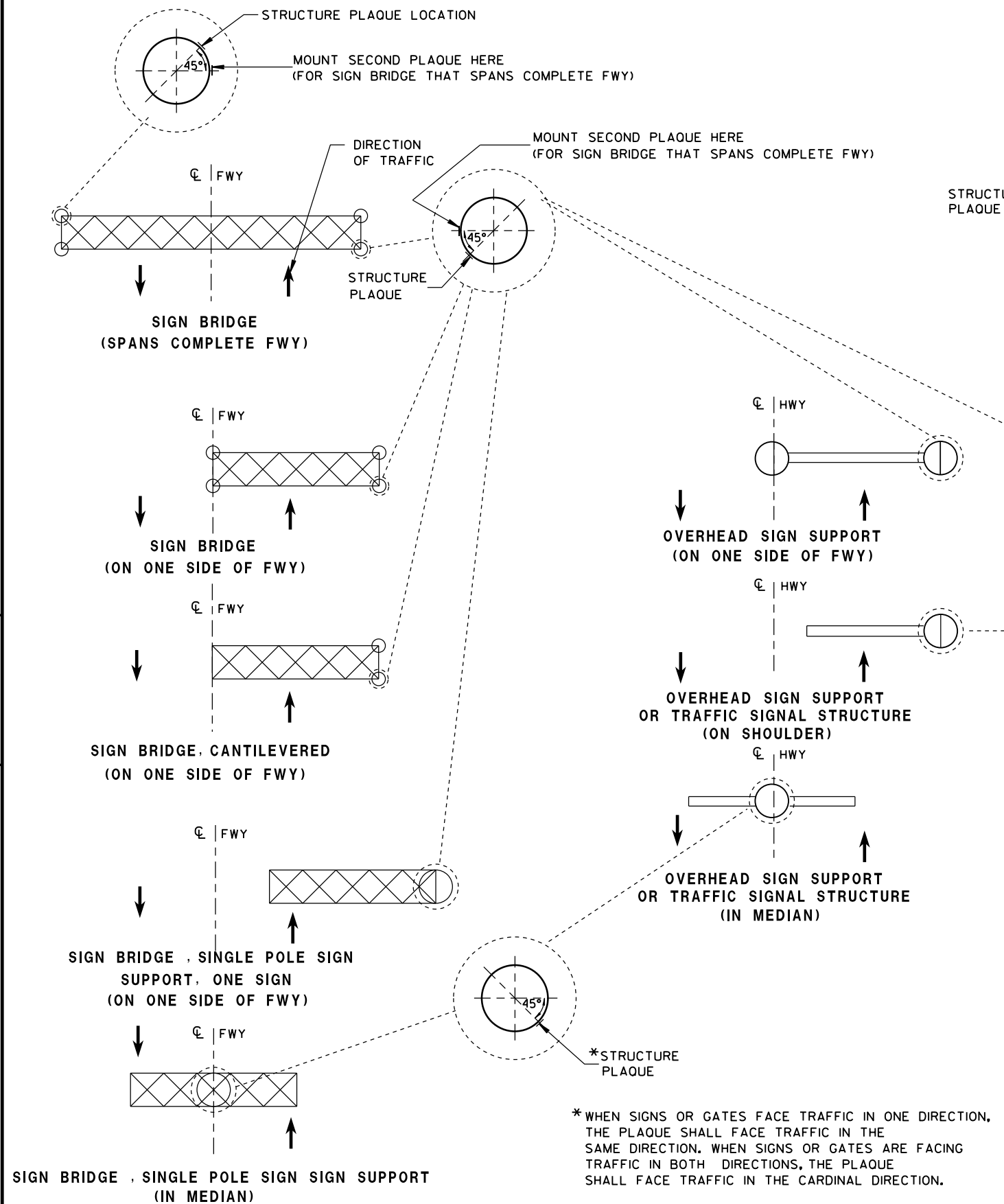
WELD TO BE 100% R.T. OR U.T. TESTED AS PER THE REQUIREMENTS OF AWS D 1.5-88. RECORDS OF COMPLIANCE OF SUCH TESTING SHALL BE FURNISHED TO THE OFFICE OF DESIGN/BRIDGE FOR VERIFICATION AND APPROVAL.



POLE SPLICE DETAIL

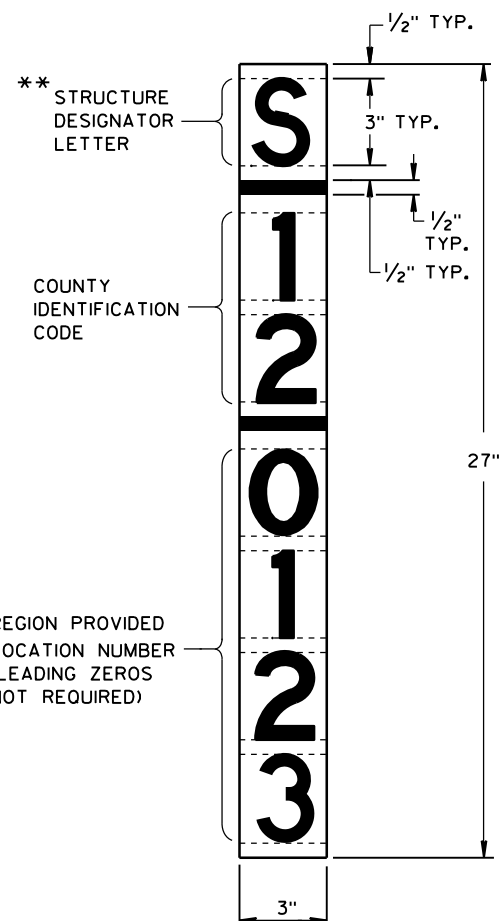
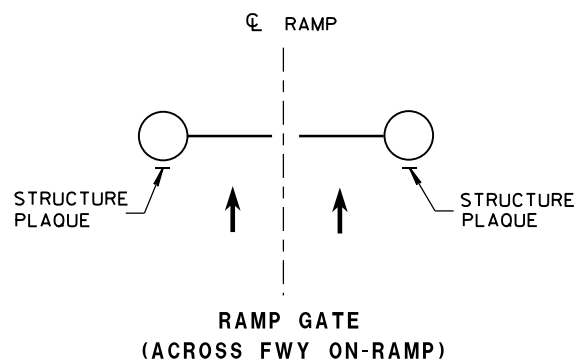
POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



LOCATION OF RAMP GATE, SIGN BRIDGE, OVERHEAD SIGN SUPPORT & TRAFFIC SIGNAL STRUCTURE PLAQUES

RAMP GATE, SIGN BRIDGE, OVERHEAD SIGN SUPPORT AND TRAFFIC SIGNAL STRUCTURE PLAQUE FOR SIGN BRIDGES AND OVERHEAD SIGN SUPPORT WHICH ARE NOT STRUCTURE MOUNTED



GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN IN THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PLAQUES SHALL BE INCIDENTAL TO ALL NEW INSTALLATIONS.

IF THE PROPOSED SIGN BRIDGE OR OVERHEAD SIGN SUPPORT IS REPLACING AN EXISTING SIGN BRIDGE OR OVERHEAD SIGN SUPPORT, A NEW IDENTIFICATION PLAQUE WILL BE REQUIRED.

FASTEN TOP, CENTER AND BOTTOM OF PLAQUE TO POLE OR OTHER LOCATION AS FOLLOWS:

GALVANIZED STEEL SHAFT - 3 STAINLESS STEEL POP RIVETS

A588 STEEL SHAFT - SHIM FOR DRAINAGE WITH STAINLESS WASHERS;
FASTEN WITH STAINLESS SELF-TAPPING SCREWS

ALUMINUM SHAFTS - 3 ALUMINUM POP RIVETS

MOUNTING HEIGHT SHALL BE APPROXIMATELY 5.0' ABOVE CURB OR SHOULDER. ADJUST IF IT IS KNOWN THAT REQUIRED TRAFFIC SIGNS WILL OBSTRUCT.

PLAQUE MATERIALS:

BASE - SHEET ALUMINUM, 0.060" THICK.

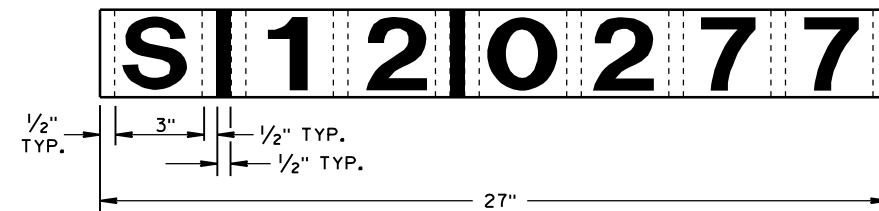
FACE - WHITE, SELF-ADHESIVE VINYL SHEETING, NON-RETROREFLECTIVE

LINES - BLACK, 1/2" WIDE, SELF-ADHESIVE

CHARACTERS:- BLACK, SELF ADHESIVE, SERIES "D", SIZE AS SHOWN.

FOR SIGN BRIDGES, STRUCTURE MOUNTED, THE STRUCTURE PLAQUE SHALL BE MOUNTED HORIZONTALLY AS SHOWN ON THE DRAWING. THE STRUCTURE PLAQUE SHALL BE MOUNTED HORIZONTALLY TO THE BACK OF THE SIGN, BETWEEN THE ALUMINUM EXTRUSIONS, NEAR THE TOP LEFT HAND CORNER OF THE SIGN. THE BASE MATERIAL SHALL BE OMITTED AND THE FACE ADHERED DIRECTLY TO THE ALUMINUM SURFACE. PRIOR TO ADHERING THE MATERIAL, THE ALUMINUM SURFACE SHALL BE SMOOTH, CLEAN AND DRY.

WHERE SIGN BRIDGE ILLUMINATION IS PROVIDED, THE STRUCTURE MUST ALSO HAVE A SIGN BRIDGE CIRCUIT PLAQUE AS SHOWN IN THE ELECTRICAL DETAILS.



IDENTIFICATION PLAQUE FOR SIGN BRIDGE, STRUCTURE MOUNTED

** LETTER "G" UTILIZED FOR RAMP GATES. LETTER "S" UTILIZED FOR SIGN BRIDGES, OVERHEAD SIGN SUPPORTS, AND TRAFFIC SIGNALS.

STRUCTURE IDENTIFICATION PLAQUES,
RAMP GATES, SIGN BRIDGES, OVERHEAD
SIGN SUPPORTS, & TRAFFIC SIGNALS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

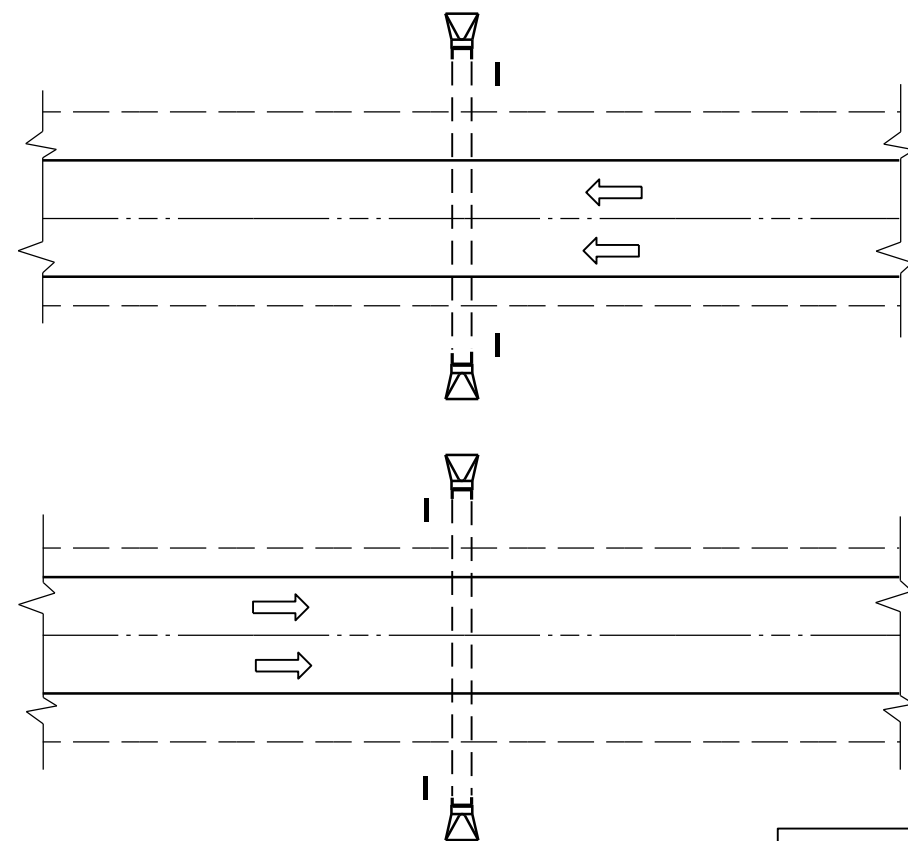
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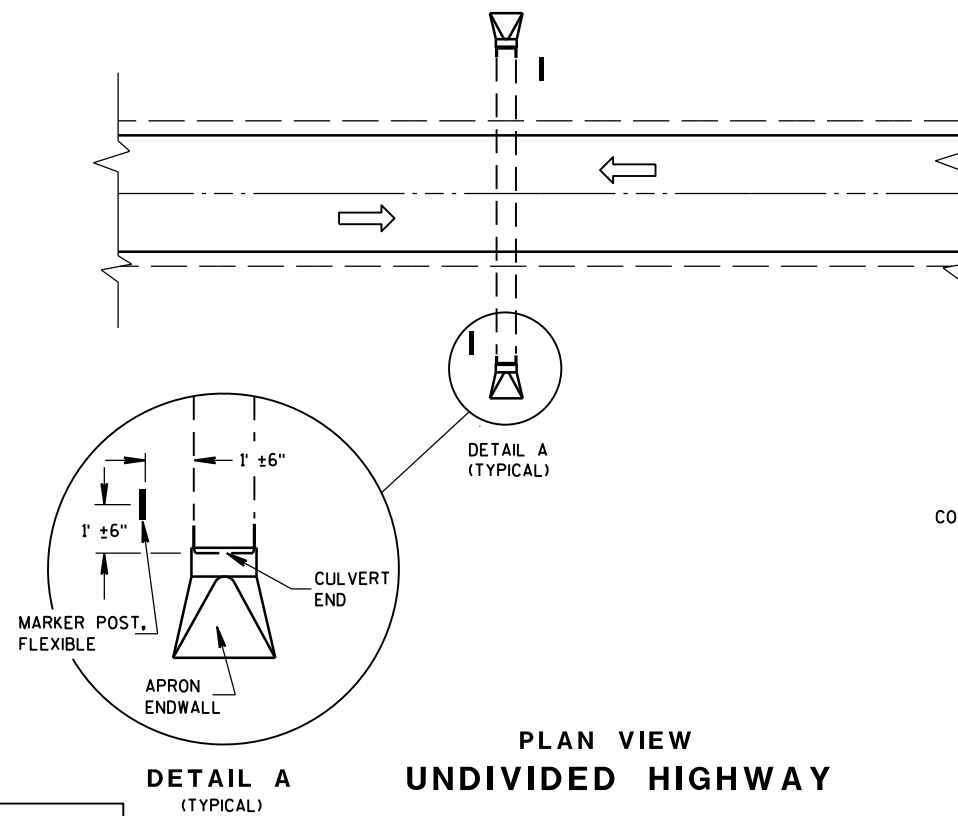
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FHWA

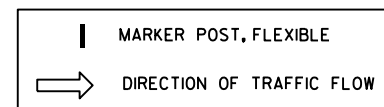
/S/ Travis Feltes
STATE TRAFFIC ENGINEER OF DESIGN



PLAN VIEW
DIVIDED HIGHWAY



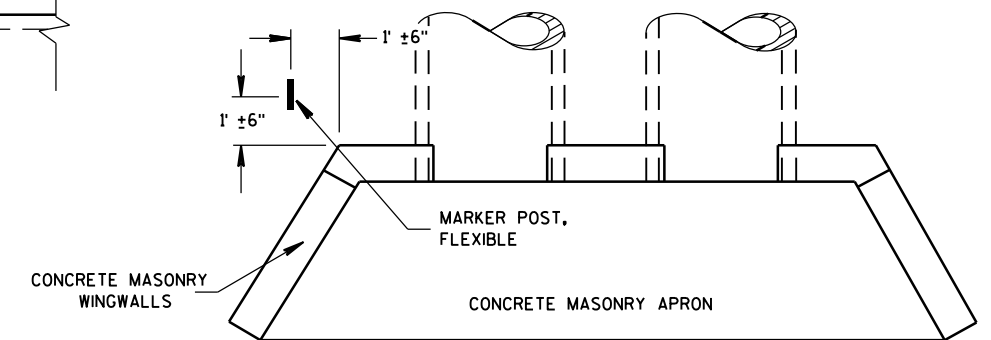
PLAN VIEW
UNDIVIDED HIGHWAY



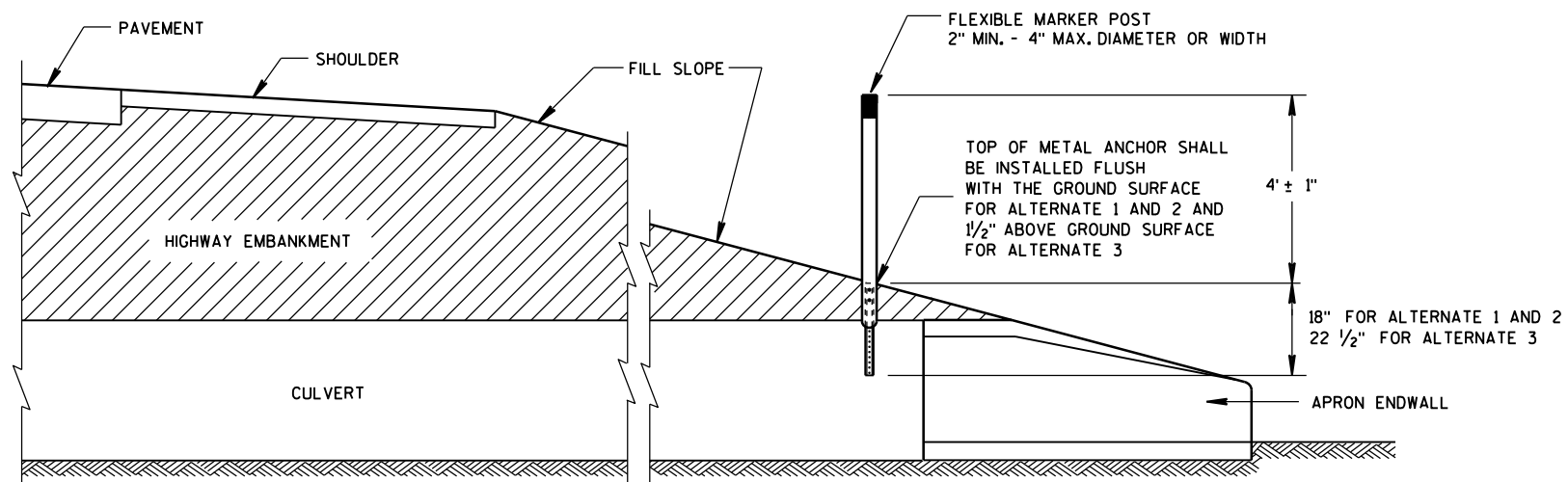
FLEXIBLE MARKER POST LOCATION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



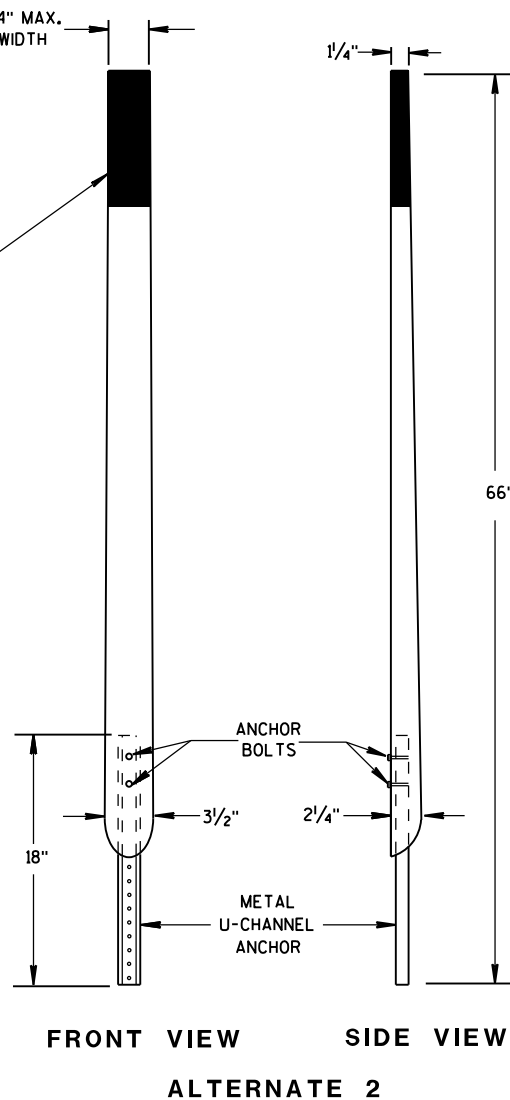
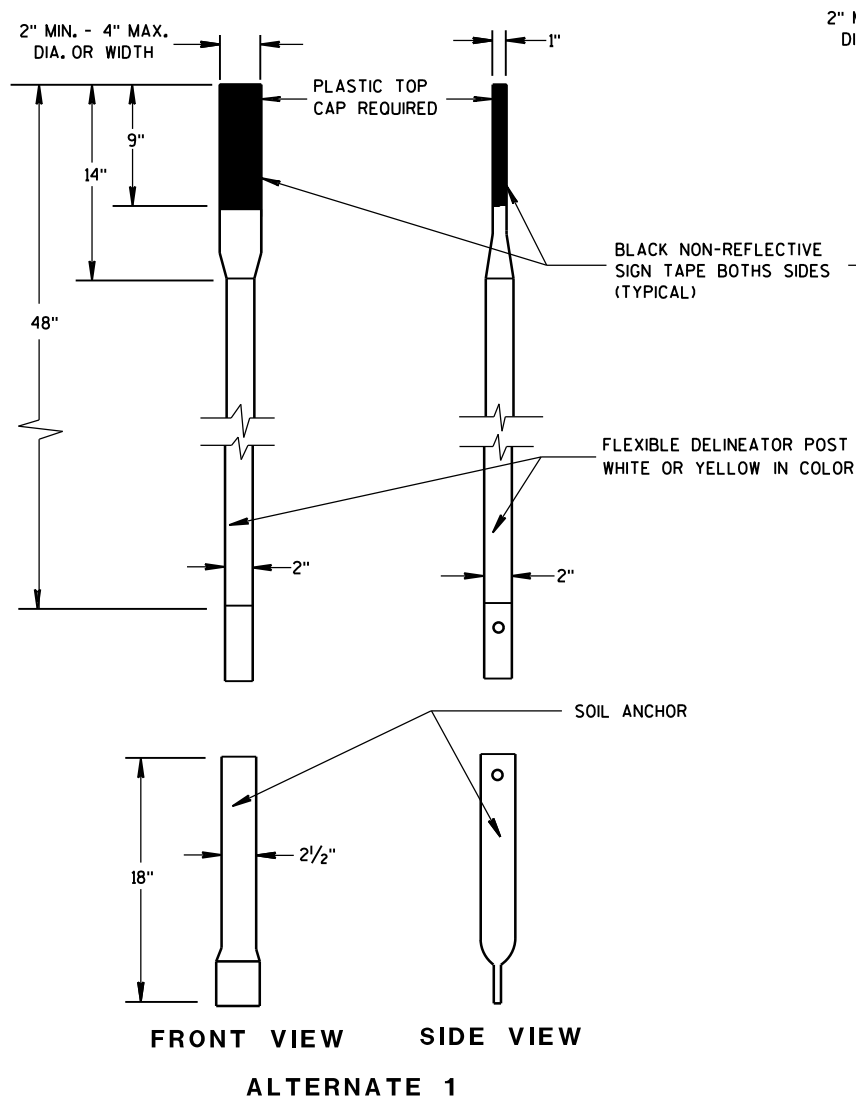
PLAN VIEW
CONCRETE MASONRY ENDWALLS FOR
CULVERT PIPE AND PIPE ARCH



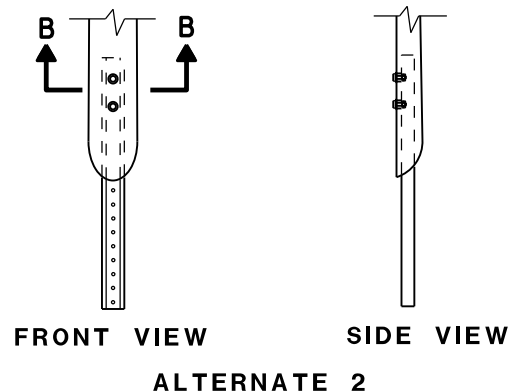
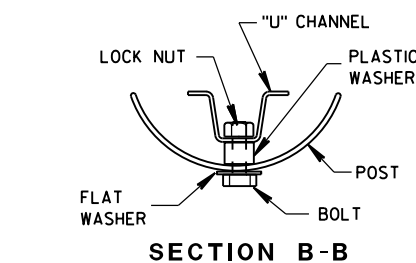
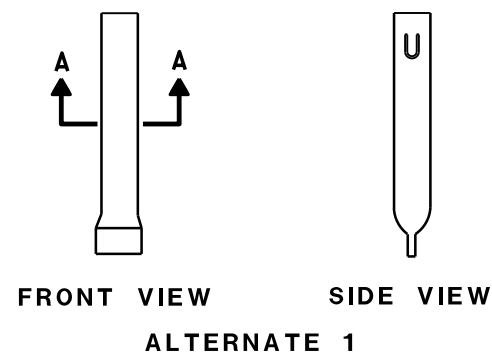
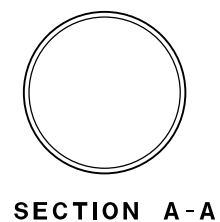
CROSS SECTION
FLEXIBLE MARKER POST

FLEXIBLE MARKER POST
FOR CULVERT END

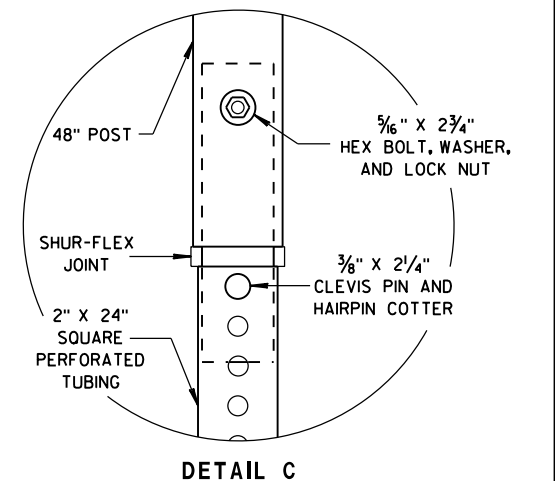
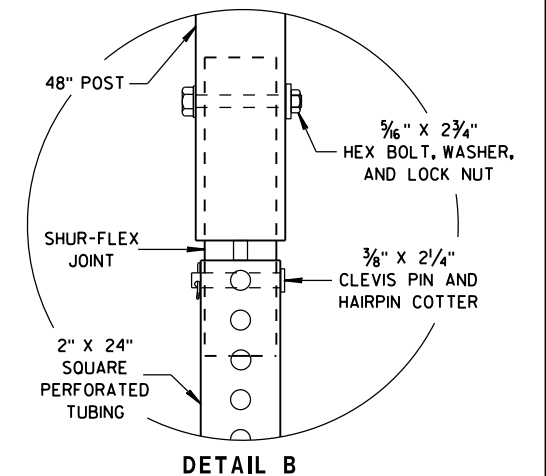
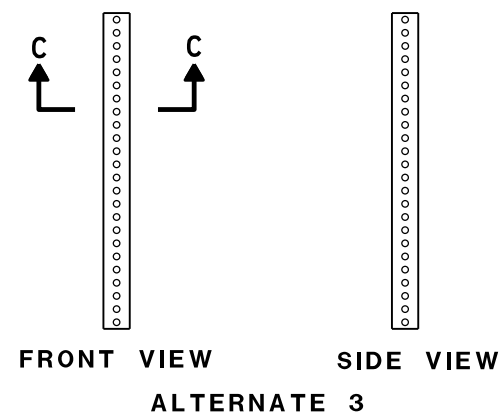
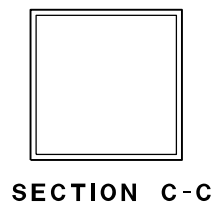
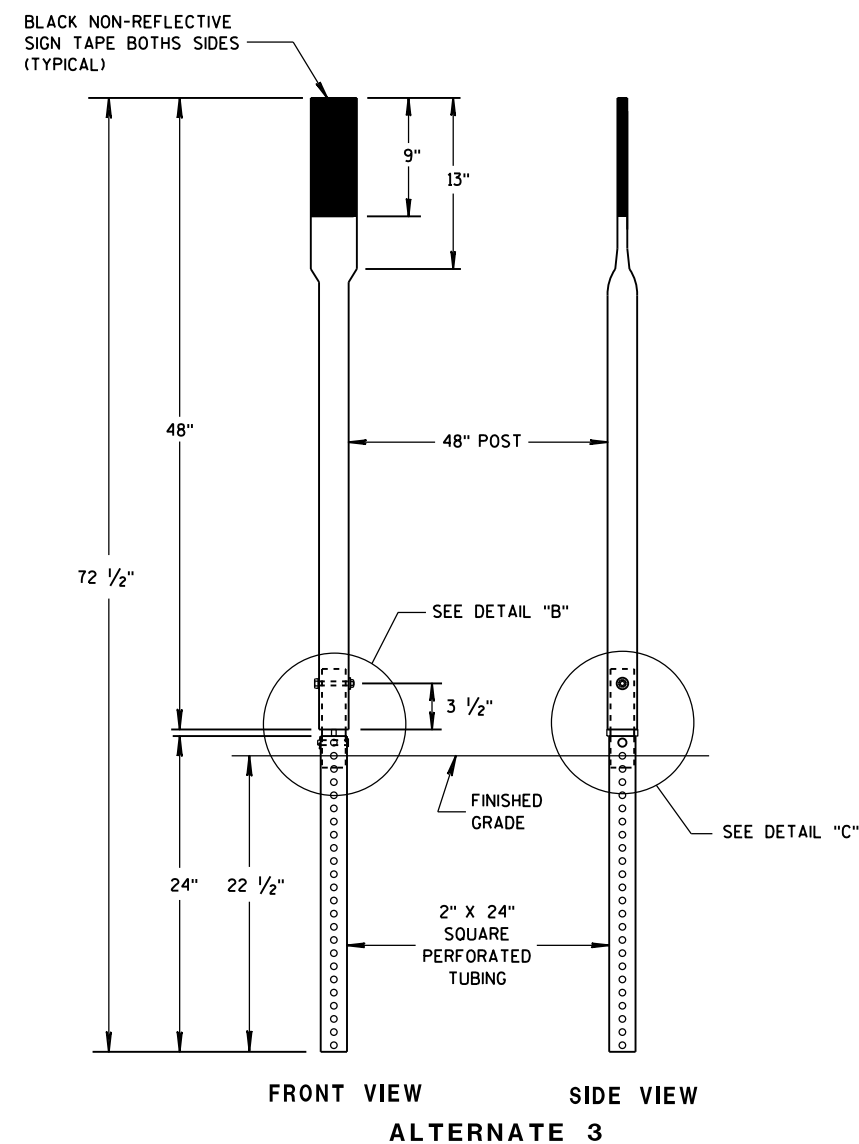
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



FLEXIBLE MARKER POSTS



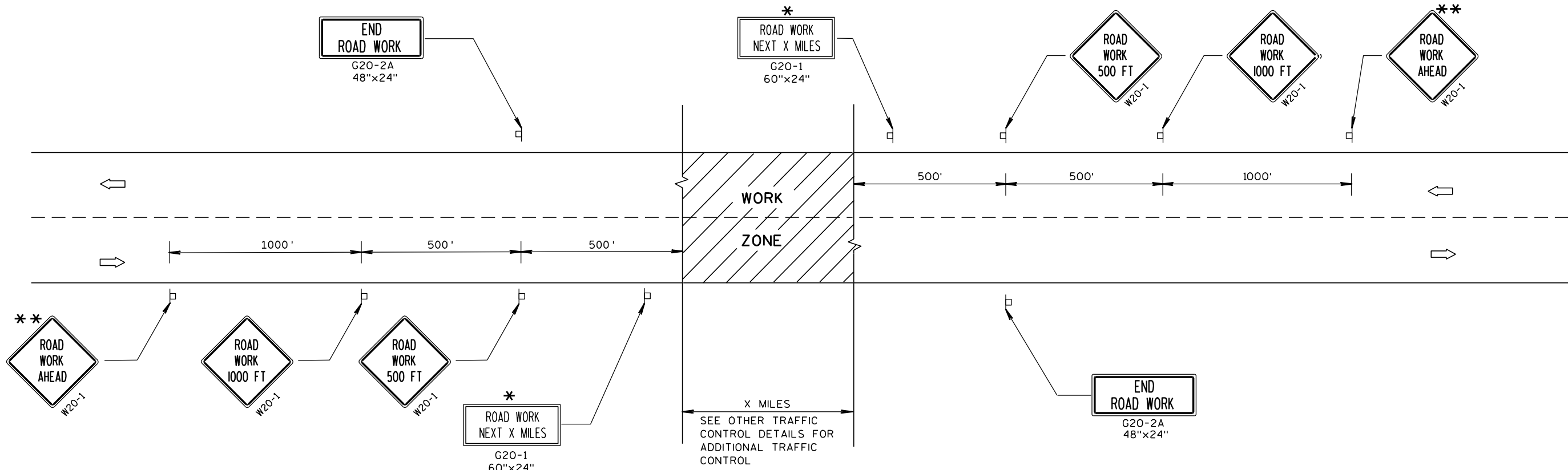
FLEXIBLE MARKER POST ANCHORS



FLEXIBLE MARKER POST FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012 DATE /S/ Travis Feltes
STATE TRAFFIC ENGINEER OF DESIGN
FHWA



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

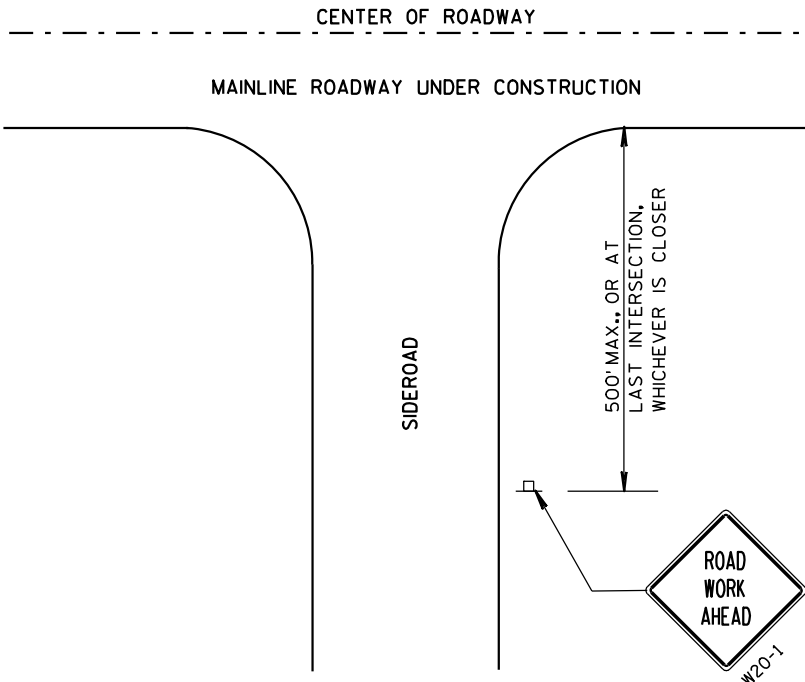
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- ** PLACE ADDITIONAL W20-1 "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA OR SIGNING.



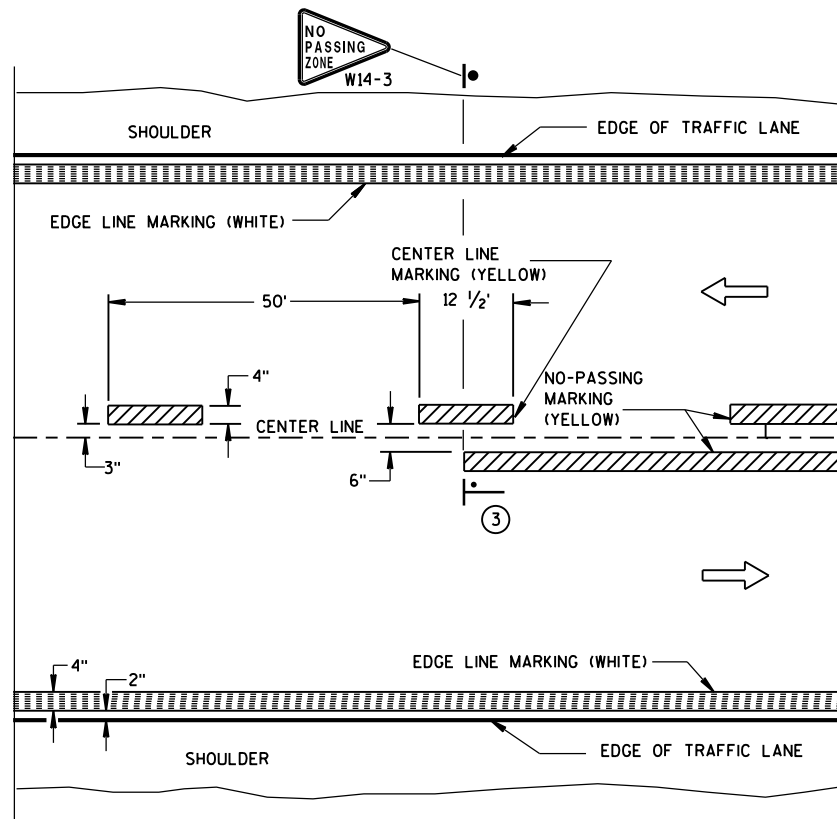
LEGEND

- ▮ POST MOUNTED SIGN
- ➡ DIRECTION OF TRAFFIC FLOW

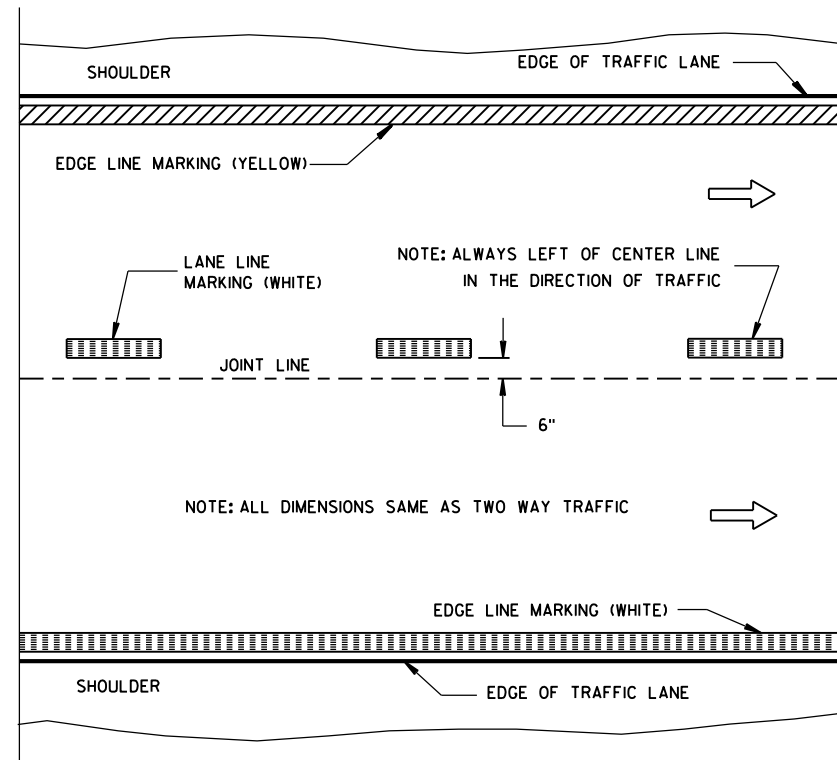
TRAFFIC CONTROL, ADVANCE
WARNING SIGNS 45 M.P.H.
OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5/23/00 /S/ Chester J. Spang
DATE CHIEF SIGNS AND MARKING ENGINEER
FHWA

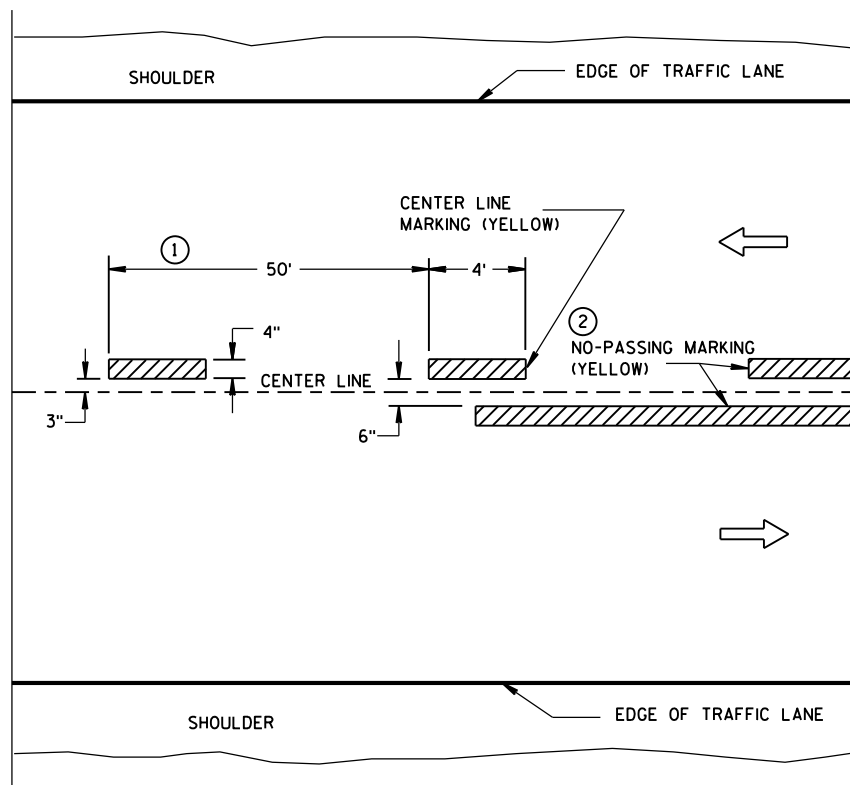


TWO WAY TRAFFIC

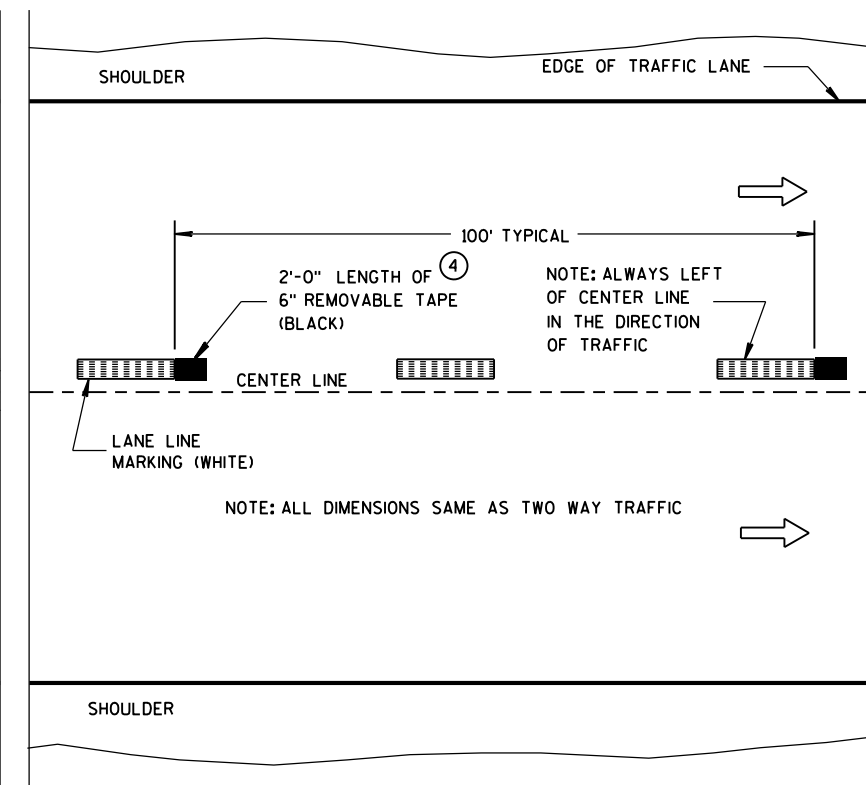


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING
(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

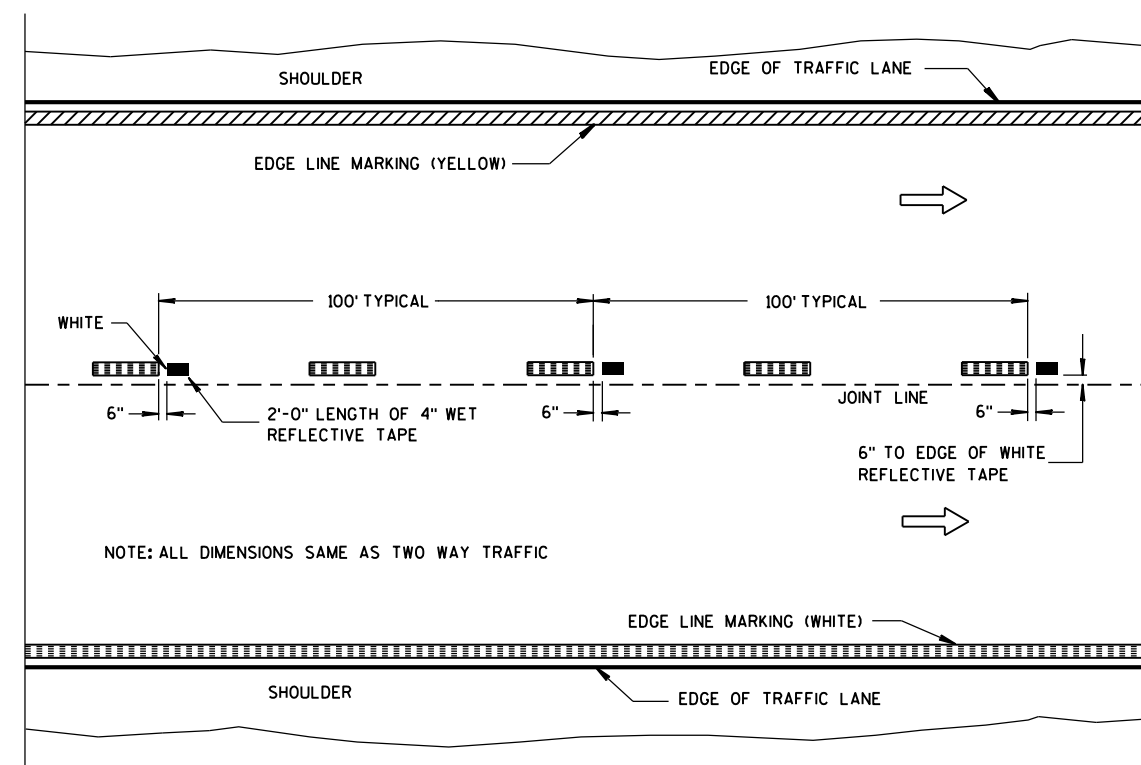
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- 1 HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- 2 NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- 3 NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
- 4 CONCRETE ONLY.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL



WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE

LEGEND

- "T" MARKING
- POST MOUNTED SIGN


PAVEMENT MARKING
(MAINLINE)


STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION


APPROVED
10-1-2012 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

TWO-LANE ROADWAY

SYMBOLS

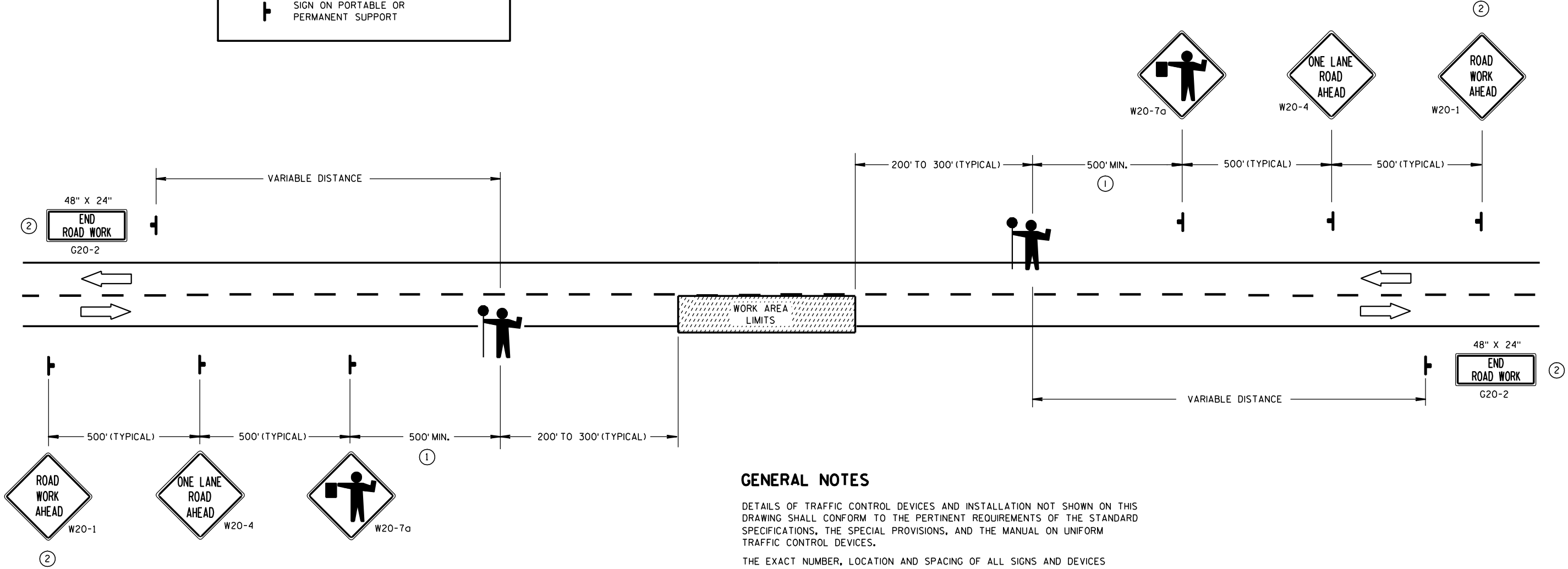
WORK AREA

FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN ON PORTABLE OR PERMANENT SUPPORT



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



GENERAL NOTES

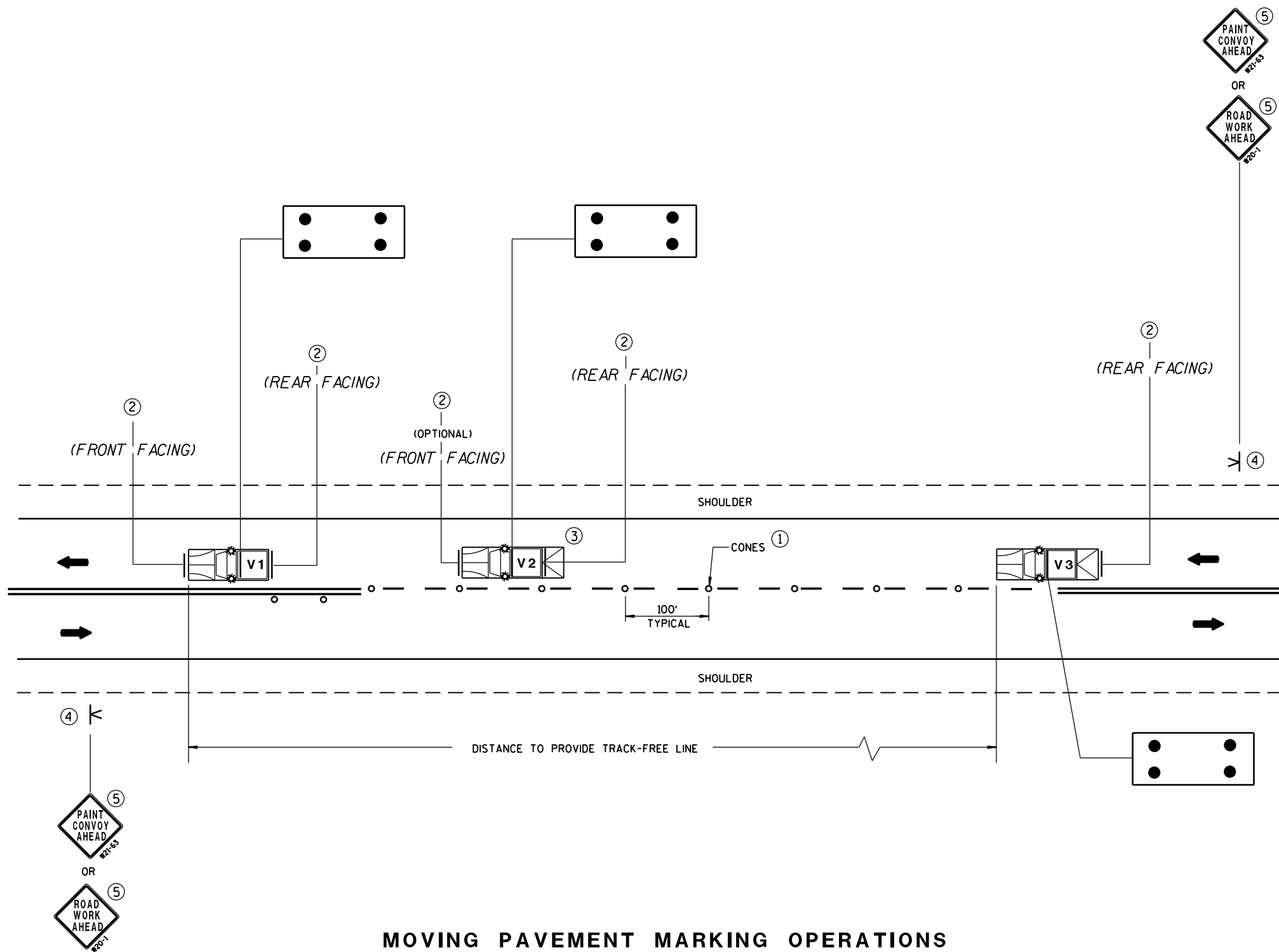
- DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.
- WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.
- FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, THE "FLAGGER AHEAD", THE "ROAD WORK AHEAD" AND THE ONE LANE ROAD AHEAD" SIGNS SHALL BE COVERED OR REMOVED AND THE HIGHWAY RESTORED TO NORMAL OPERATION.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

- ① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
9/5/06 /S/ Thomas N. Notbohm
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



MOVING PAVEMENT MARKING OPERATIONS TWO-LANE TWO-WAY ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

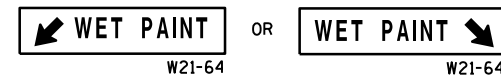
THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.



③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.

④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

V1 LEAD VEHICLE

V2 SHADOW VEHICLE

V3 TRAIL VEHICLE WITH TMA

TMA TRUCK-MOUNTED ATTENUATOR

K SIGN ON PORTABLE SUPPORT

→ DIRECTION OF TRAVEL

o CONES

⋮ FLASHING ARROW PANEL (CAUTION)

MOVING PAVEMENT MARKING
OPERATION
TWO-LANE TWO-WAY ROADWAY

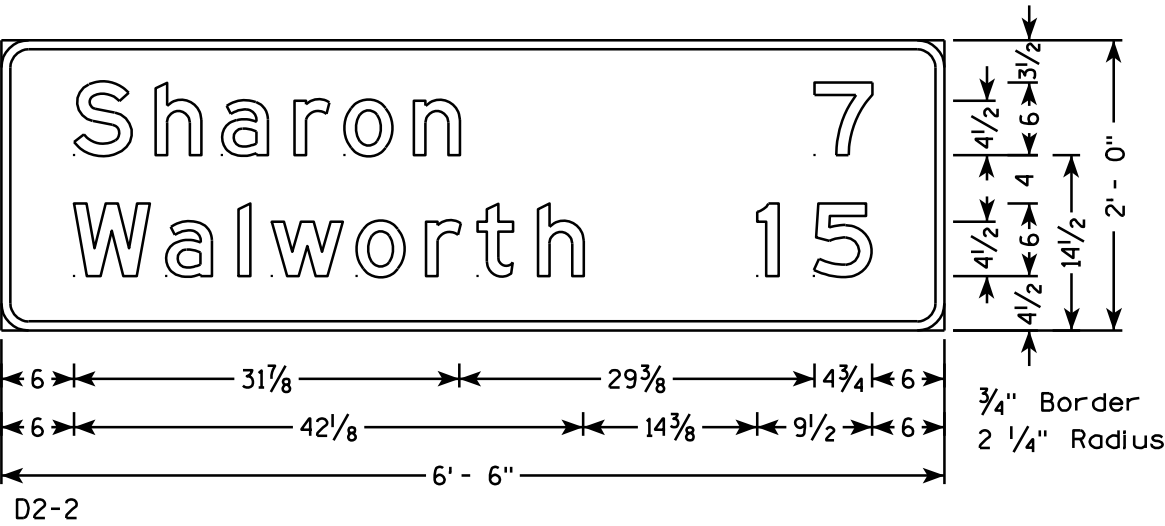
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

12/6/2011
DATE

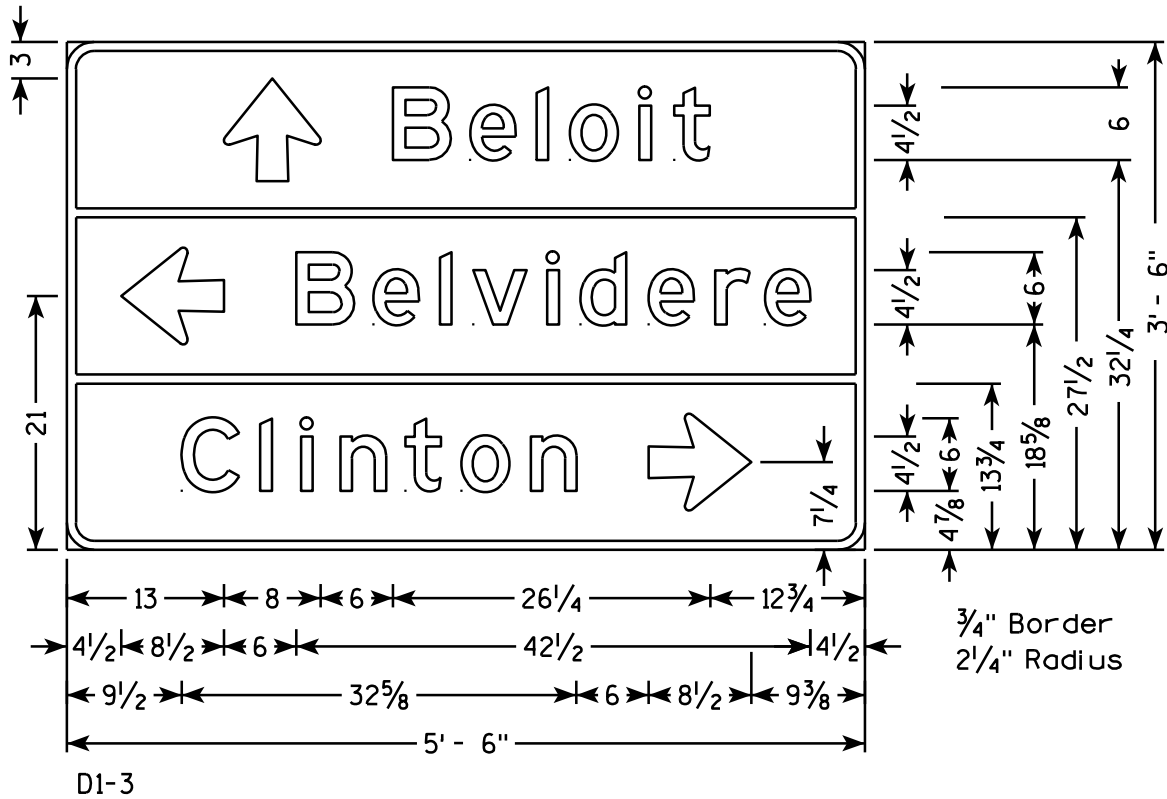
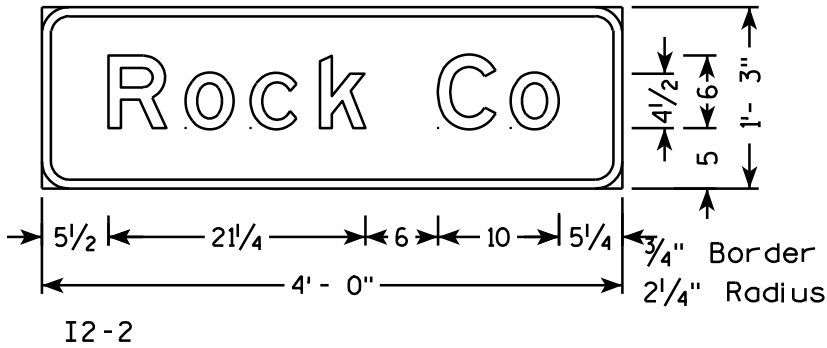
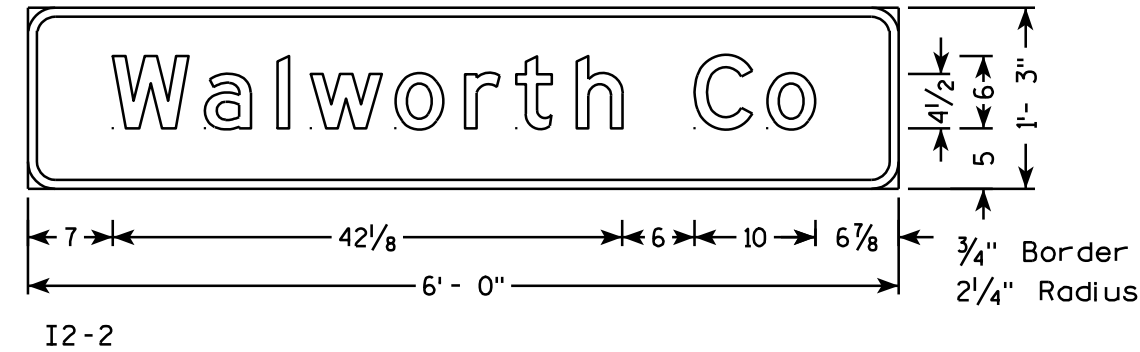
FHWA

/S/ Thomas N. Notbohm
STATE TRAFFIC ENGINEER OF DESIGN

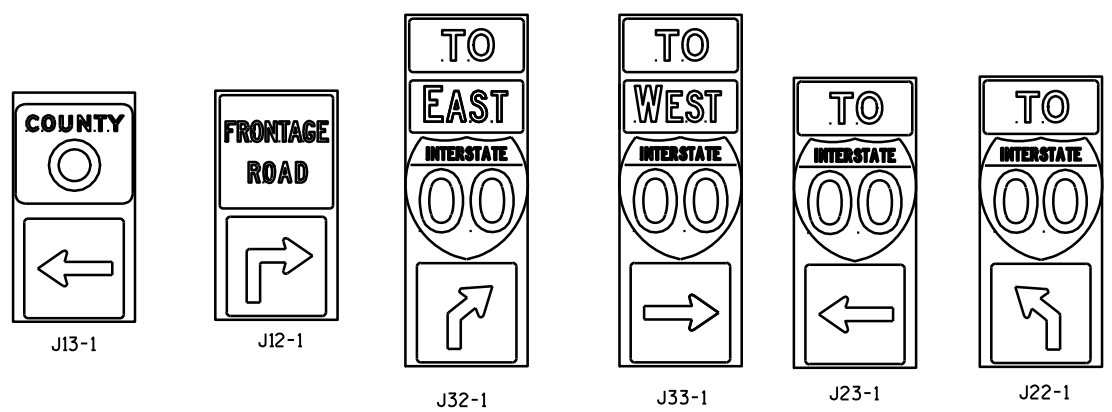
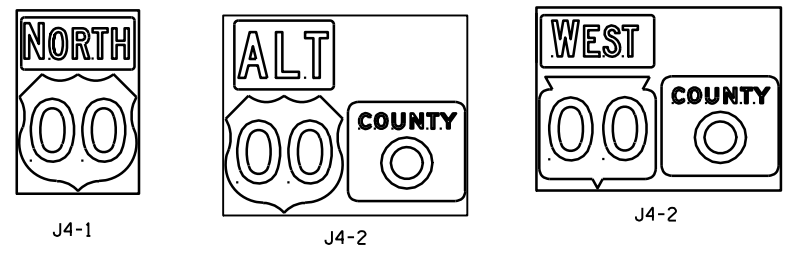
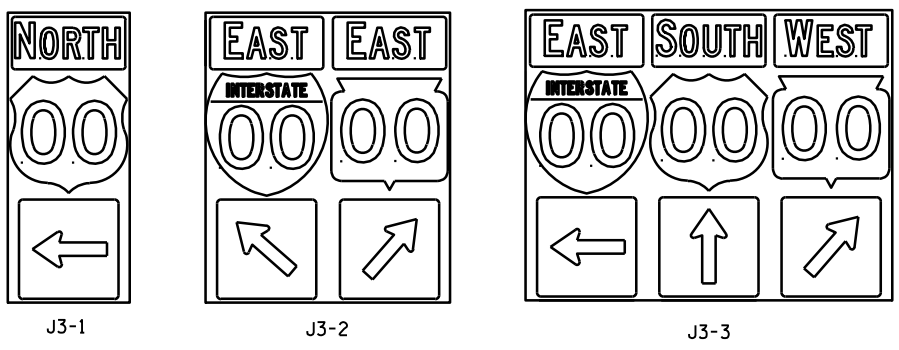
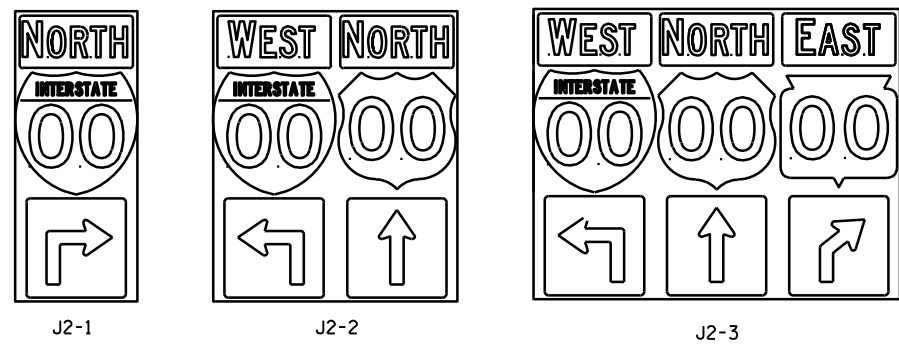
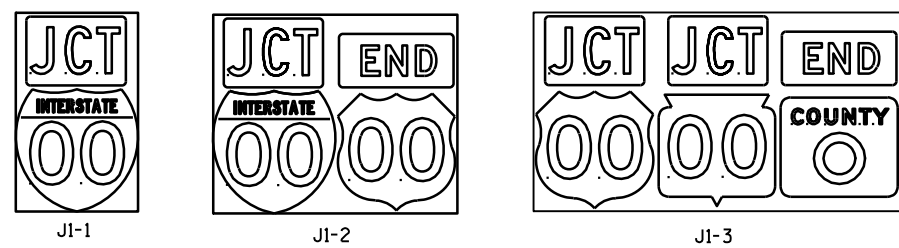


NOTES

1. All Signs Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - GREEN
Message - WHITE
3. Message Series - E



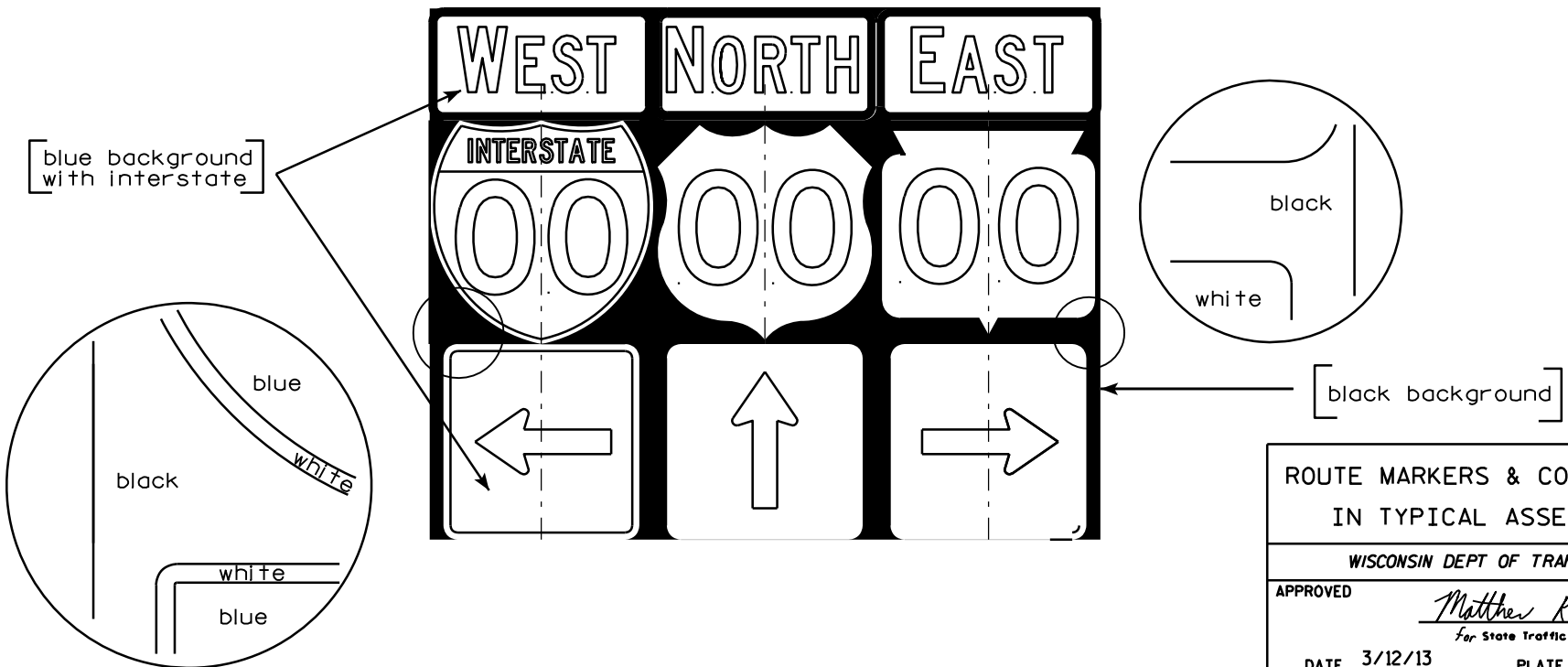
TYPICAL ASSEMBLIES



JV
(Typical Vertical J-Assembly
See Note 10)

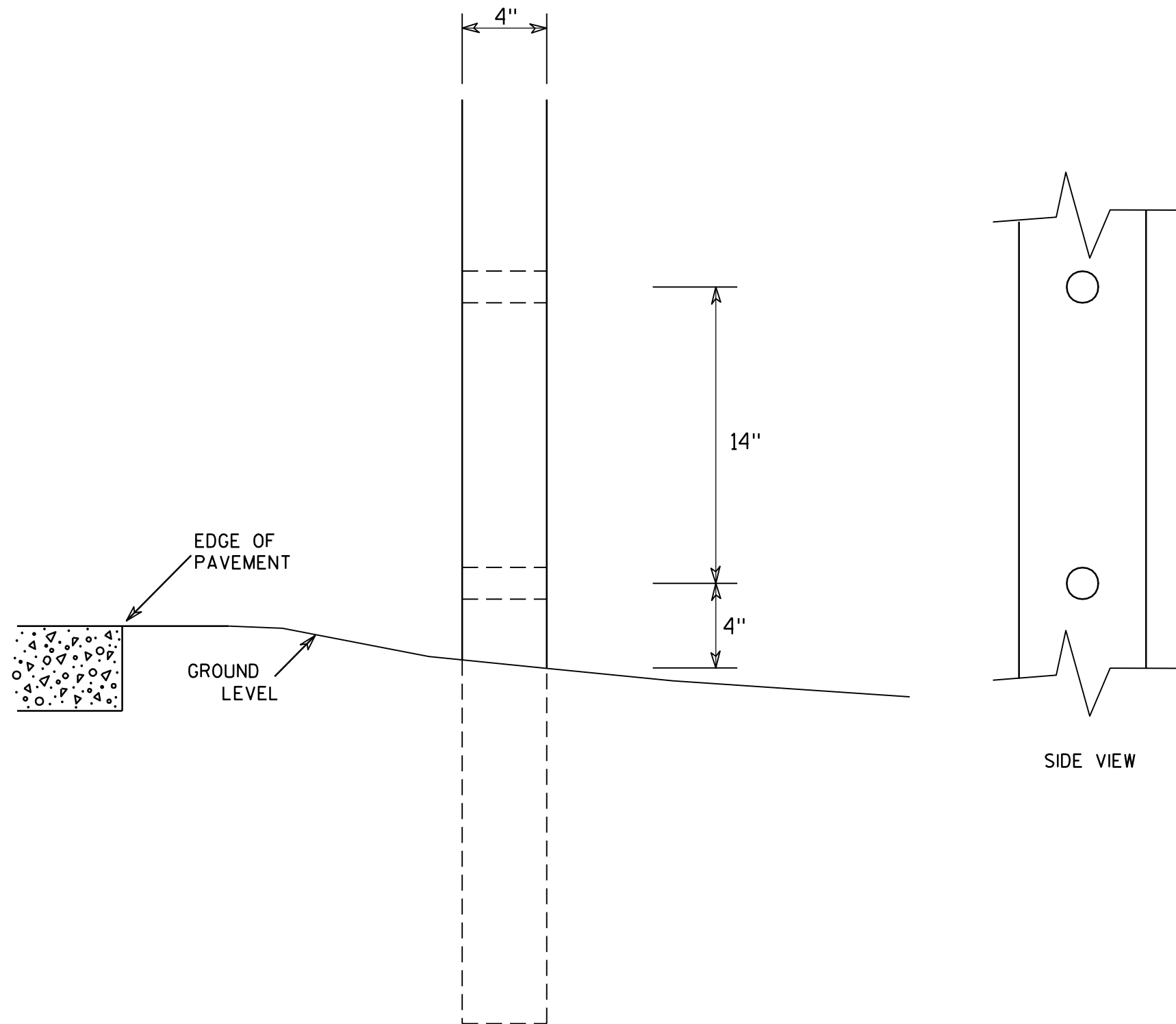
NOTES

1. Signs are Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Black Non-reflective
Message - see Note 5
3. Message Series - See Note 5
4. Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
5. The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
6. Certain marker heads require the component pieces to be the same color. As an example, all the components used with an M1-1 Interstate marker shall be blue.
7. Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
8. Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
9. Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
10. All Vertical J Assemblies are given a Sign Code of JV



ROUTE MARKERS & COMPONENTS IN TYPICAL ASSEMBLIES	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 3/12/13	PLATE NO. A2-1S.7

7



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

4 X 6 WOOD POST
MODIFICATIONS

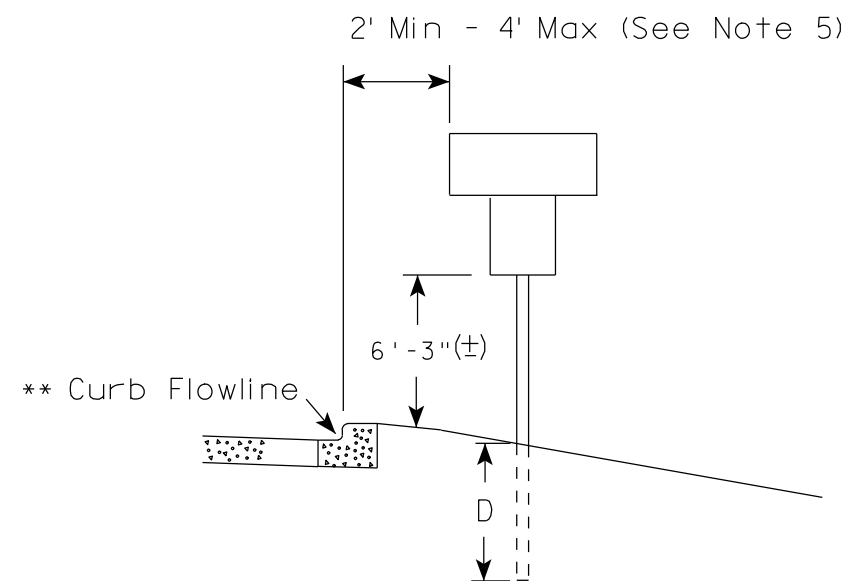
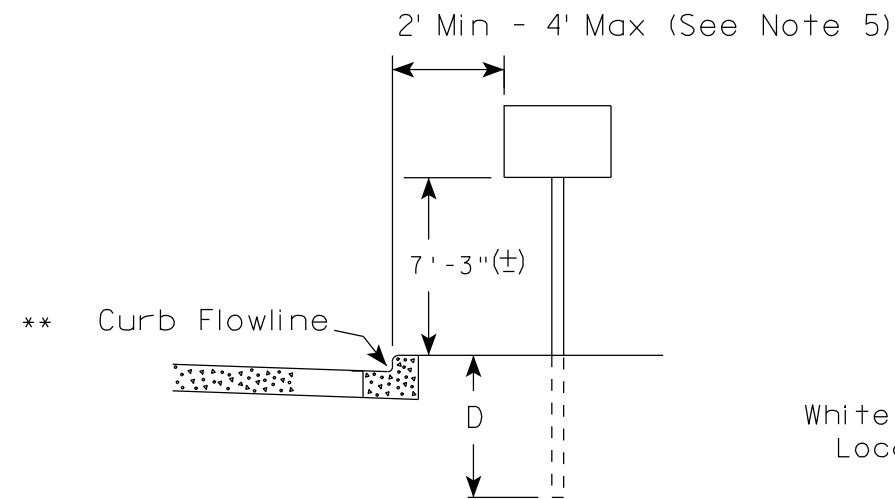
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Chester J. Spang*
for State Traffic Engineer

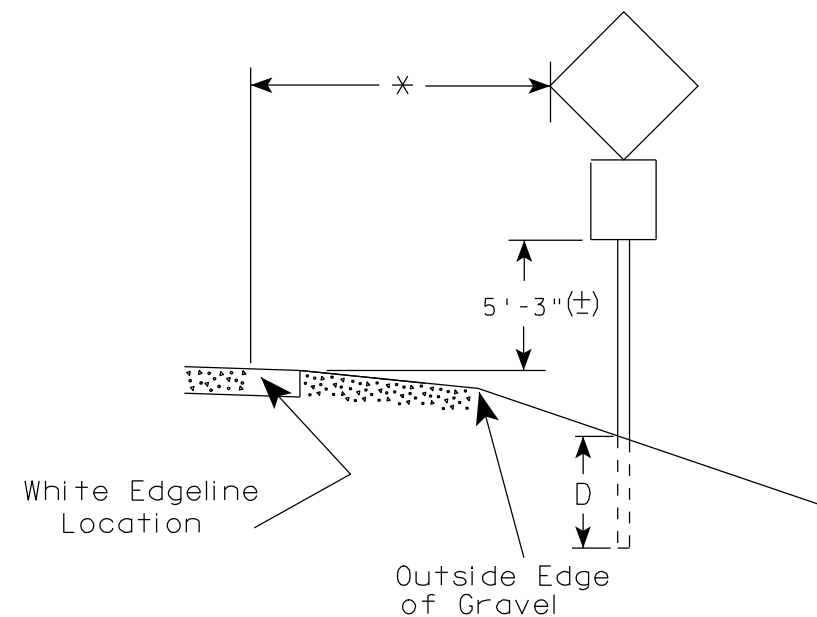
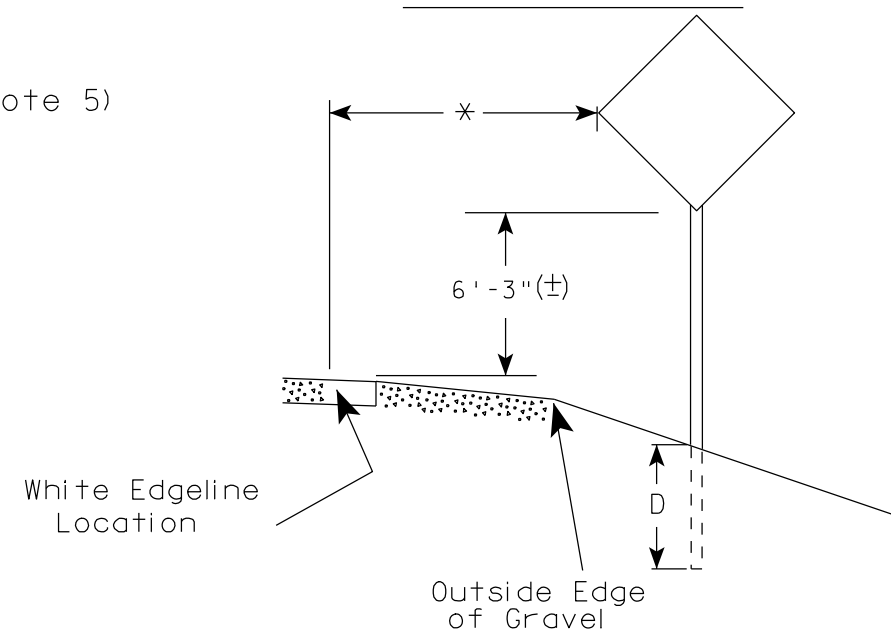
DATE 3/27/97 PLATE NO. A4-11.2

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet or larger than 20 sq. ft. shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'-3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series) & End of Road Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (±).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

✱✱ The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 9/21/2011 PLATE NO. A4-3.16

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

GENERAL NOTES

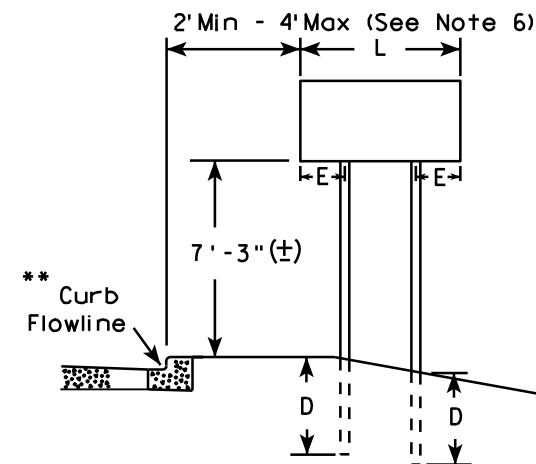
- For multiple post installations, individual post spacing shall be greater than 3'-6".
- See tables below for required number of posts.
- For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
- The (±) tolerance for mounting height is 3 inches.
- Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
- Offset distance shall be consistent with existing signs or consistent throughout length of project.
- Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
- The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series) & End of Road Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

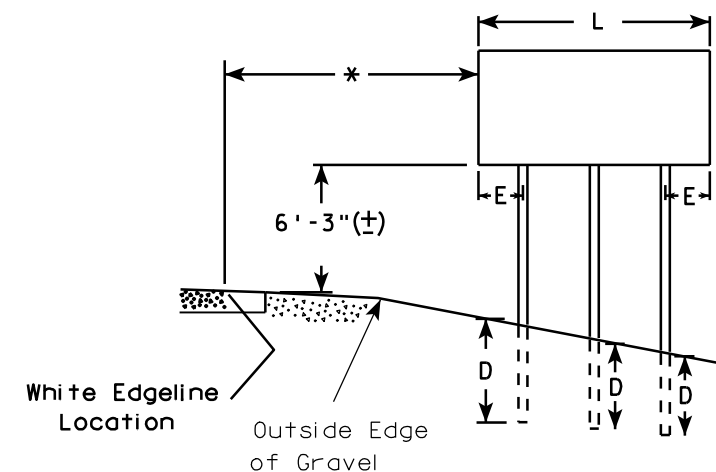
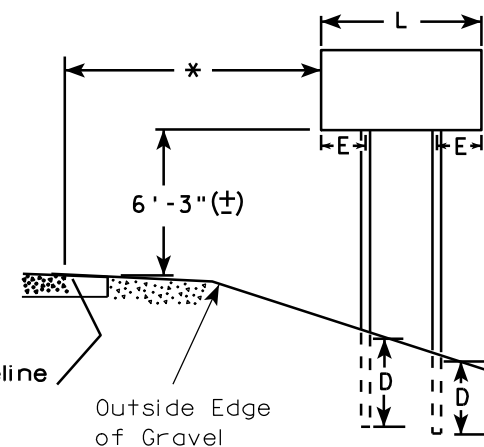
** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width or 20 S.F. or less in area.

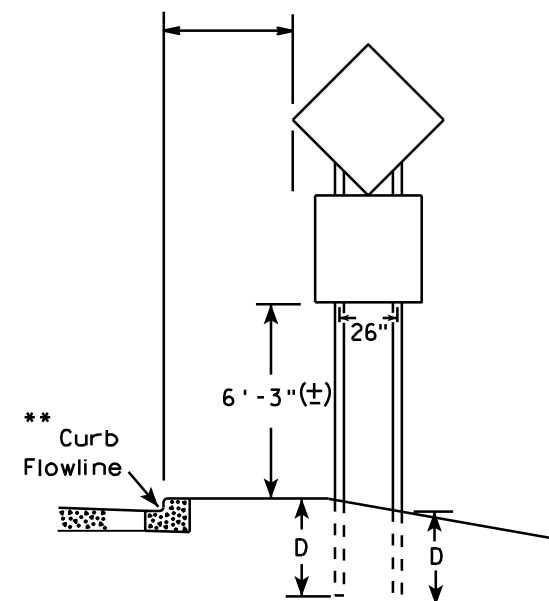
URBAN AREA



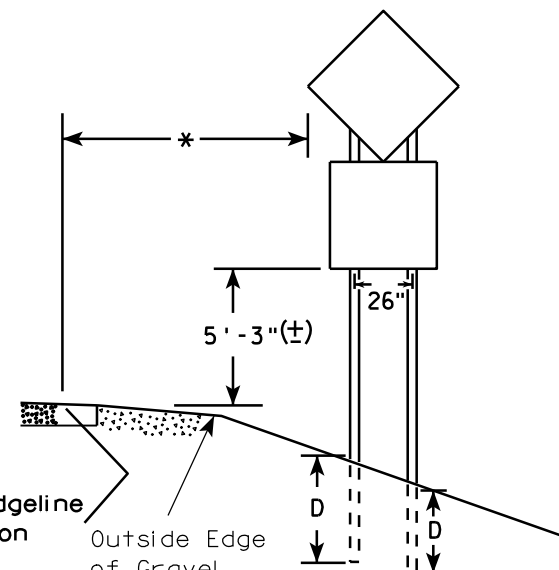
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)

L	E
Greater than 48" Less than 60"	12"
60" to 120"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)

L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)

L	E
168" and greater	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
For State Traffic Engineer
DATE 9/21/2011 PLATE NO. A4-4.11

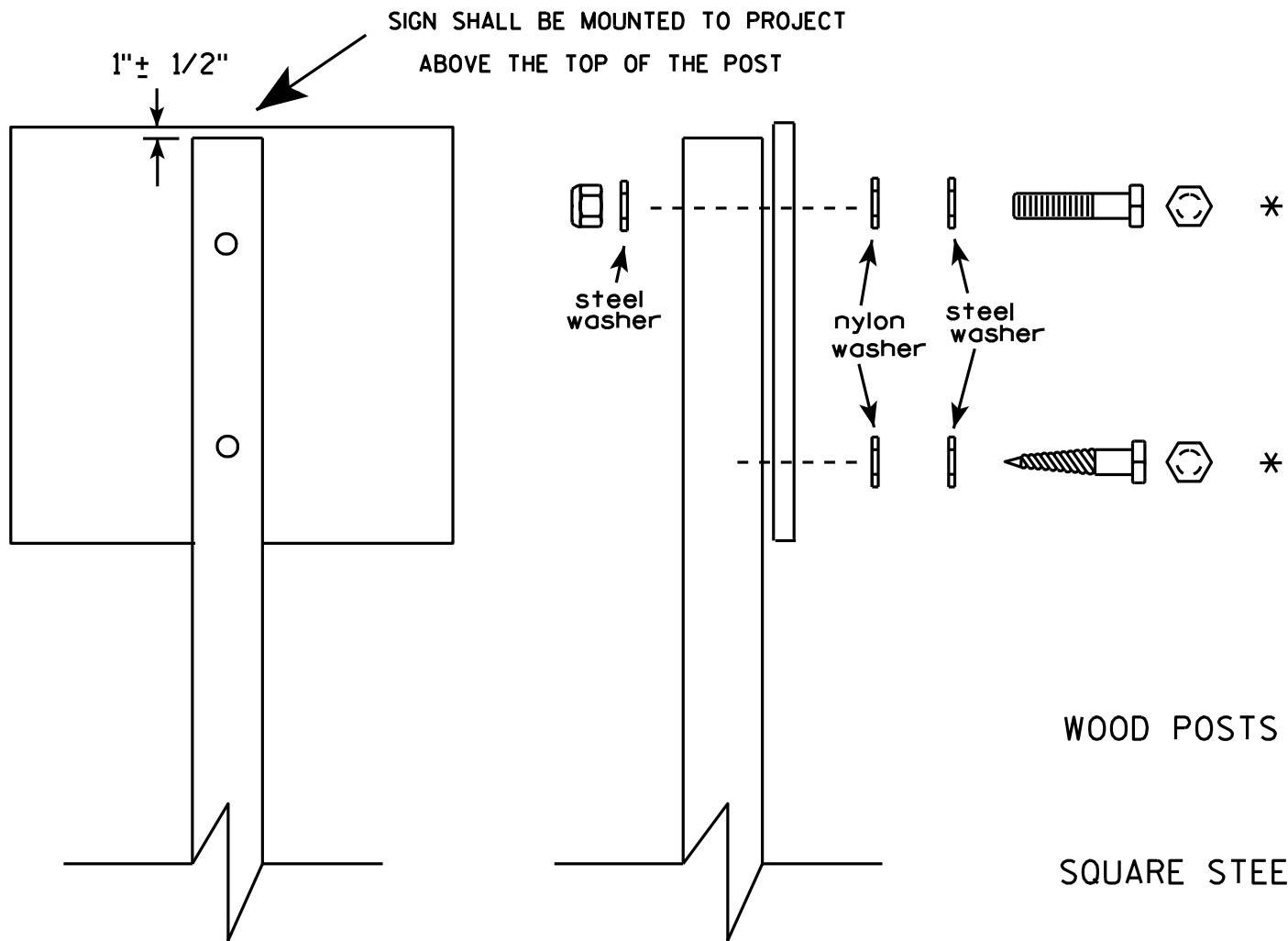
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

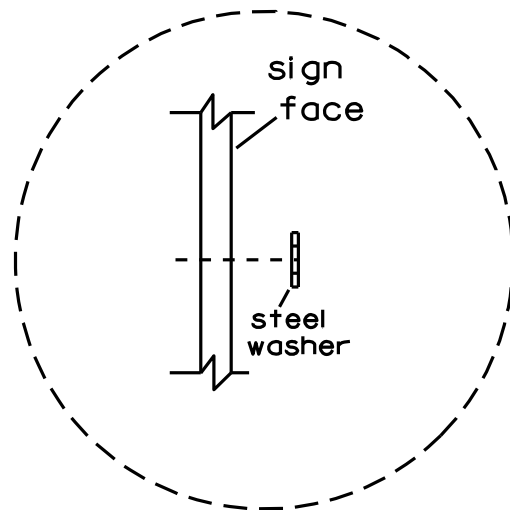


Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- WOOD POSTS (4" x 4" or 4" x 6")
LAG SCREWS - 3/8" X 3"
MACHINE BOLTS - 5/16" X 6-1/2" or 7" Length w/ nuts
- SQUARE STEEL POSTS (2" x 2")
MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts
RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON for all Type H signs.

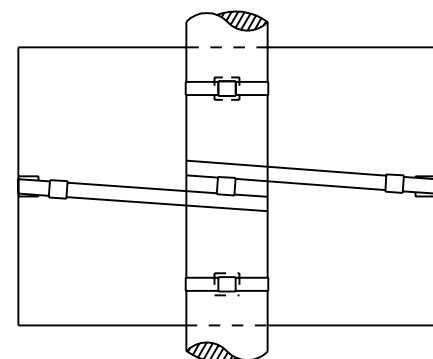
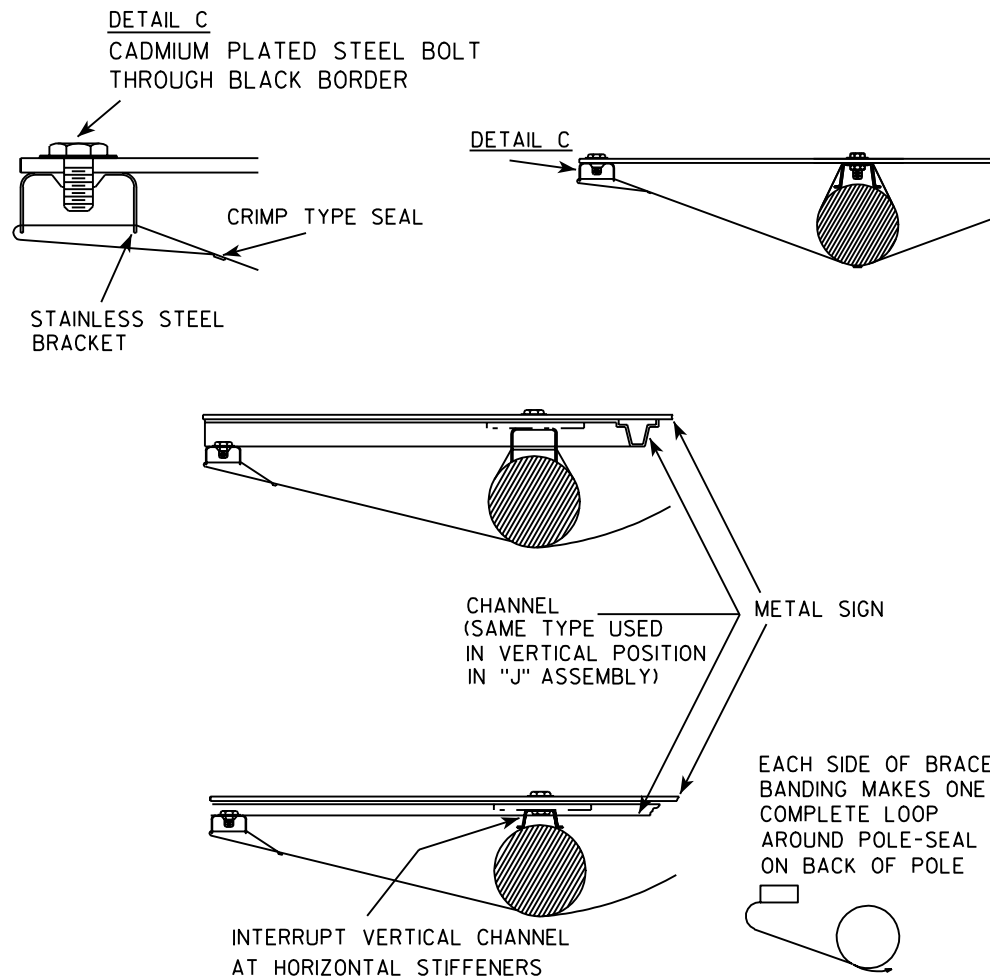


Washer Placement when Sign Has Other Than Type H or Type F Face

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

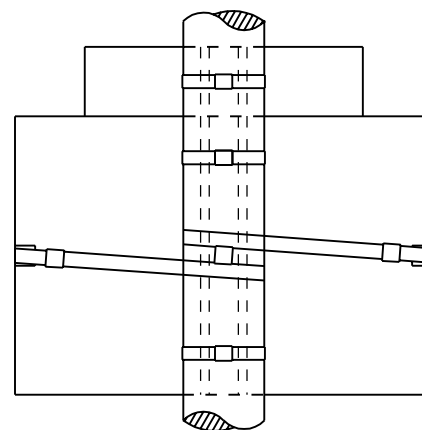
ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 3/23/10	PLATE NO. A4-8.7

BRACE BANDING



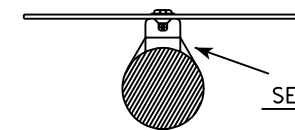
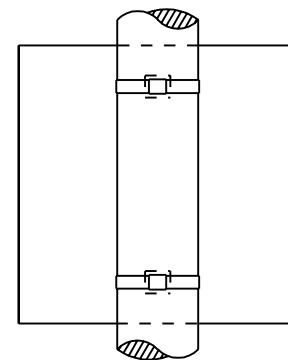
BRACE BANDING

BRACE BANDING SHALL BE TIGHTENED FIRMLY
BUT NOT SO TIGHT AS TO APPRECIABLY
CURVE FACE OF SIGN.

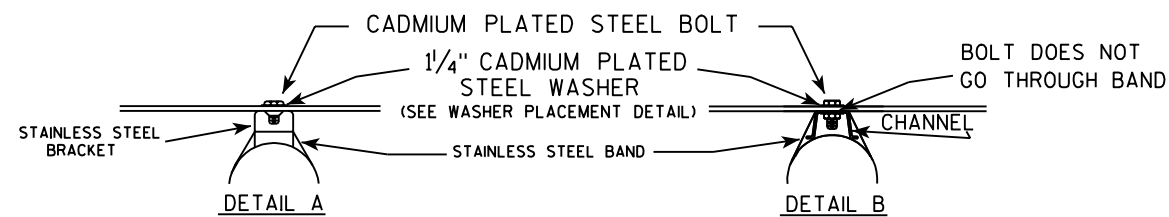


BRACKET BANDING

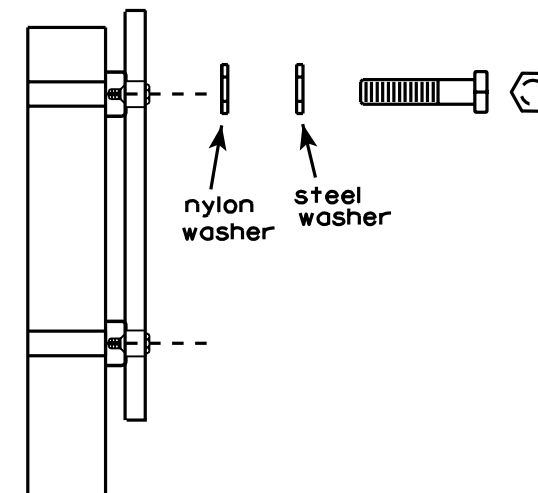
SINGLE SIGN



SEE DETAIL A



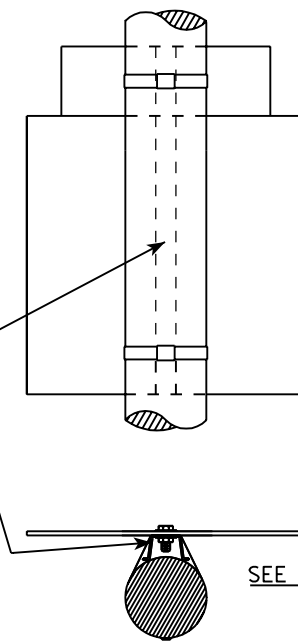
WASHER PLACEMENT



WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

"J" ASSEMBLY



SEE DETAIL B

GENERAL NOTES

1. Signs 4' or greater in width shall have one brace band installed at the center of the sign.
2. Signs 3' or greater in height shall have three bracket bands installed. Signs less than 3' in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.

STANDARD SIGN SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 11/08/05 PLATE NO. A5-9.2

PROJECT NO:

HWY:

COUNTY:

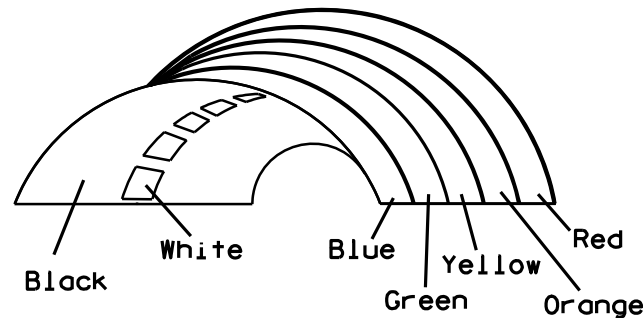
SHEET NO:

E



* VARIES

Background Colors of Symbol*



*1/4" Black Border between each color of rainbow and border of rainbow

NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - White
Message - (See Note 5)
- 3. Message Series - (See Note 6)
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Border - Blue
Line 1 - Red
Line 2 - Black
Line 3-5 - Blue
- 6. Line 1 - Dutch 8011L
Line 2 - Series E
Line 3-5 - Series C
- 7. Contractor shall provide and install a new post bracket in accordance with the I55-56B sign detail.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	36	1 1/2	1/2	5/8	3	2	3 1/2	2 7/8	1	8	2 1/8	11 1/4	11 1/8	9 3/8	1 1/4		3/4	12 5/8	7 1/2							7.5
3																											
4																											
5																											

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

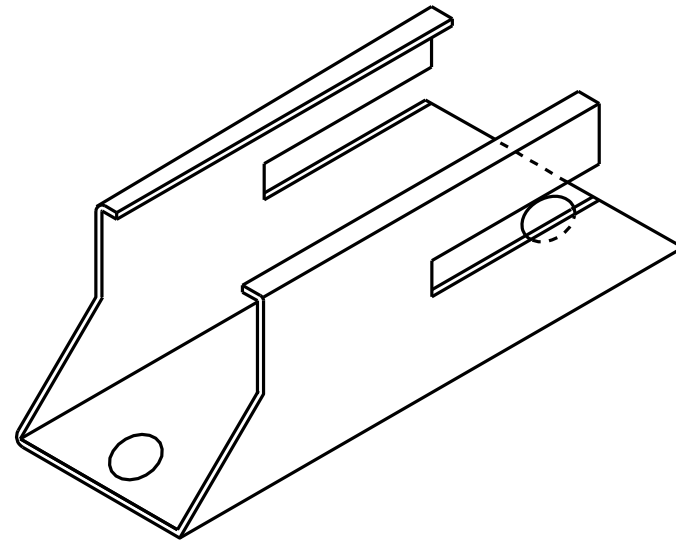
STANDARD SIGN
I55-56

WISCONSIN DEPT OF TRANSPORTATION

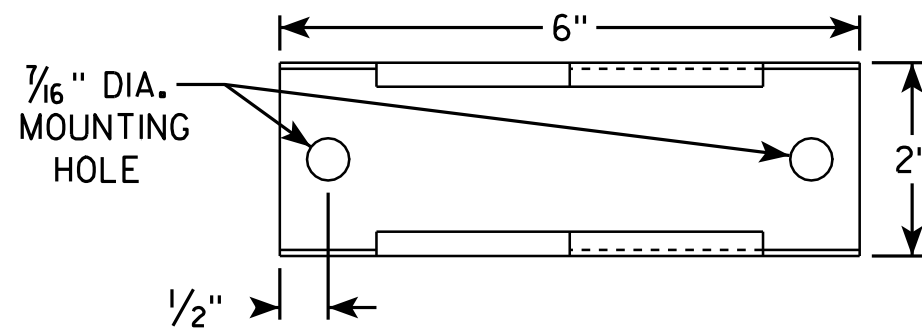
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 4/27/11 PLATE NO. I55-56.3

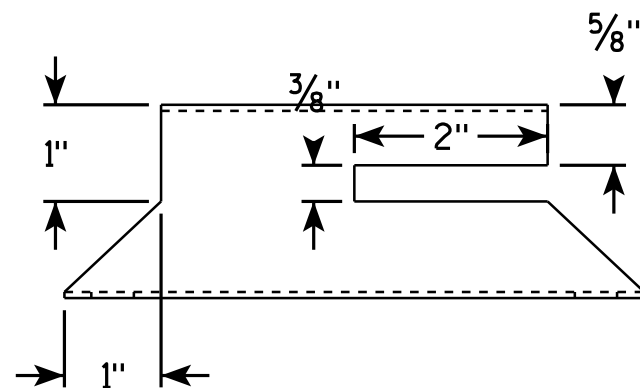
ISOMETRIC VIEW



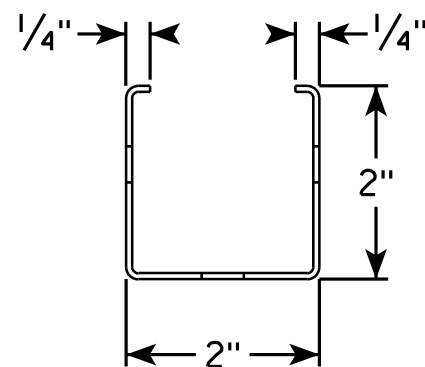
TOP VIEW



SIDE VIEW



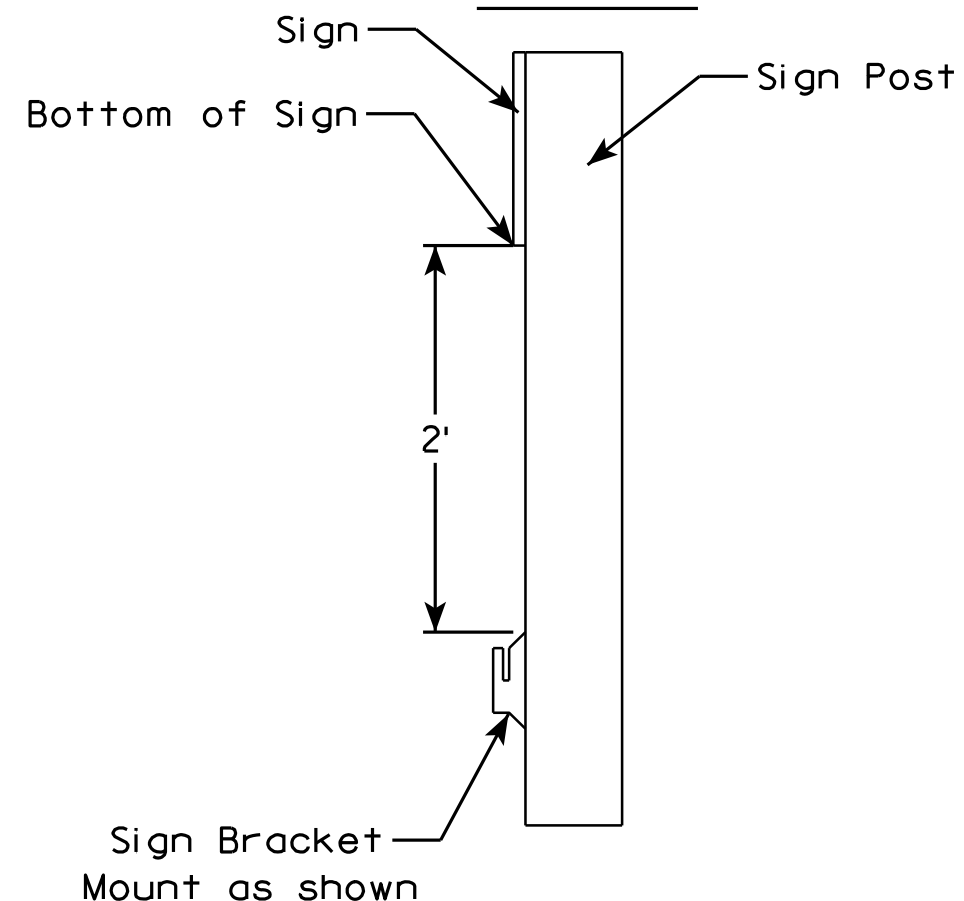
END VIEW



NOTES

1. Must be capable of permanent attachment to a wood or steel channel sign post utilizing the fastening hardware specified on the A4-8 sign plate.
2. Shall be entirely primed and painted with two coats of a black powder coated enamel paint.
3. Shall be made with 12 gauge steel, and incorporate no welds, no hinged components, no threaded lock-type components, and no parts which are loose or can be separated from the main body.
4. Shall have rounded edges with at least 1/8" radii.
5. Shall not have unrounded and uncoated metal edges which can contact the back surface of the roll-up sign.
6. Top of bracket shall be mounted 2' below the bottom of the I55-56 sign.
7. Cost of bracket and fastening hardware shall be incidental to the I55-56 sign.

SIDE VIEW



ROLLUP SIGN BRACKET
I55-56B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/5/10 PLATE NO. I55-56B.1

PROJECT NO:

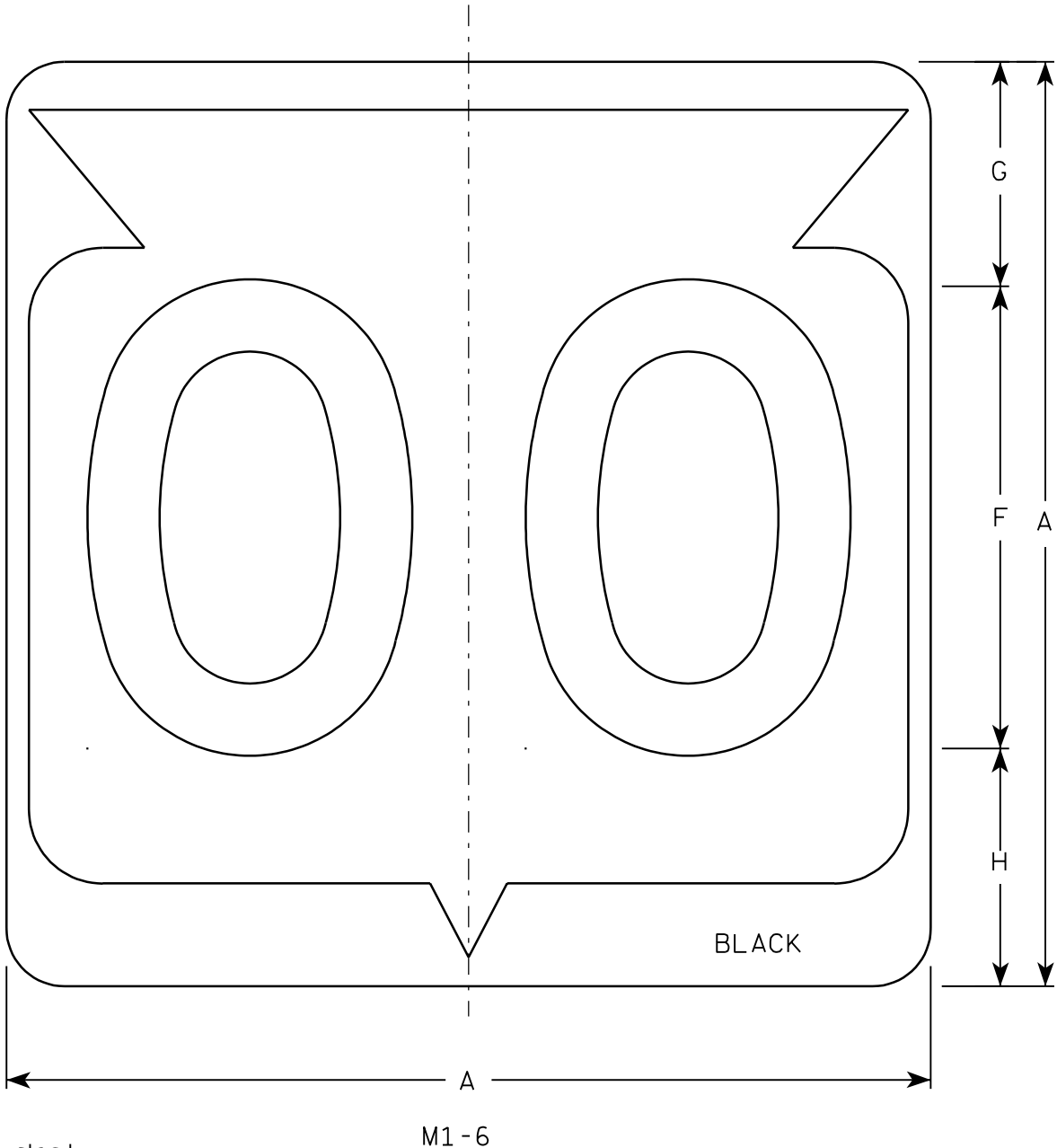
HWY:

COUNTY:

SHEET NO:

E

7



Metric equivalent
for this sign is:

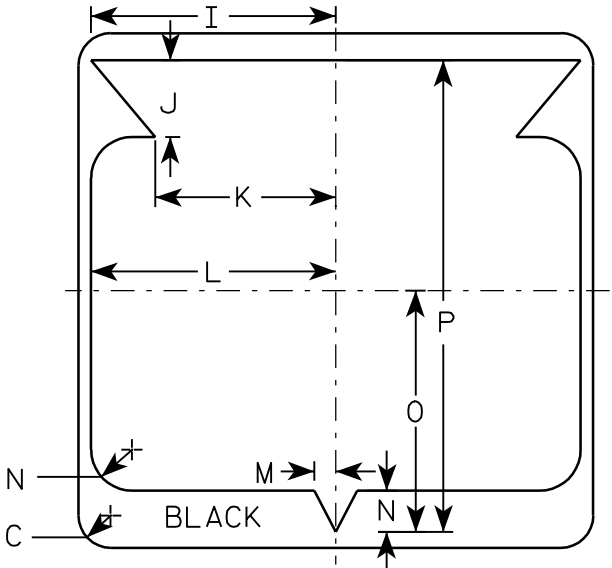
SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0	.36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81

PROJECT NO:				HWY:				COUNTY:																SHEET NO:				E
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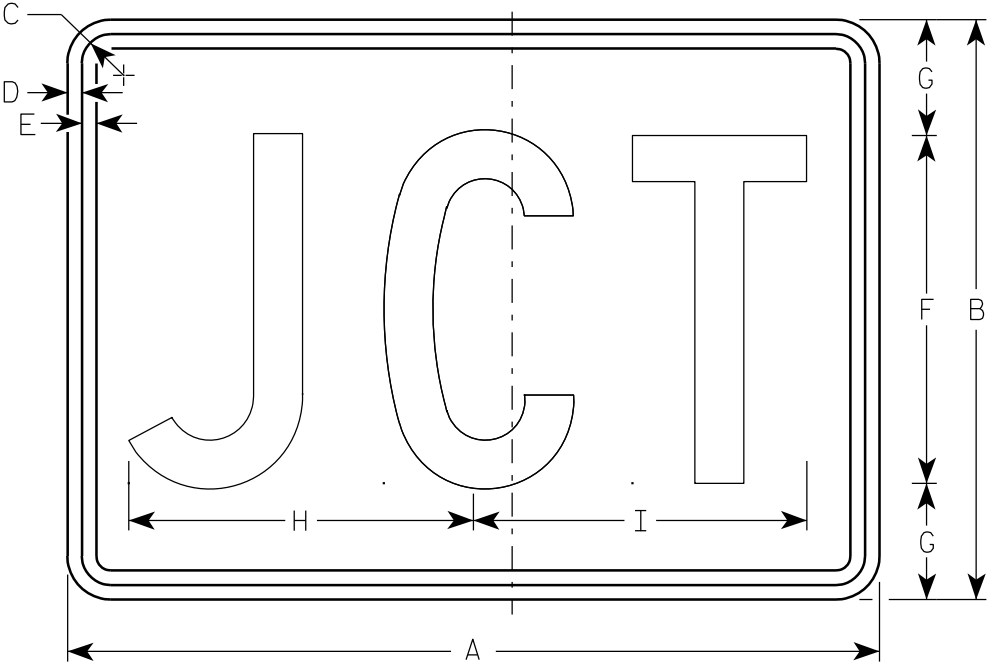
NOTES

- Sign is Type II - See Note 6 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - White & Black - See Note 6
Message - Black
- Message Series - See note 5
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- Substitute appropriate Series numerals and adjust spacing as per plate A10-1.
- Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective



7

7



M2-1
MK2-1
MM2-1
MR2-1

Metric equivalent
for this sign is:

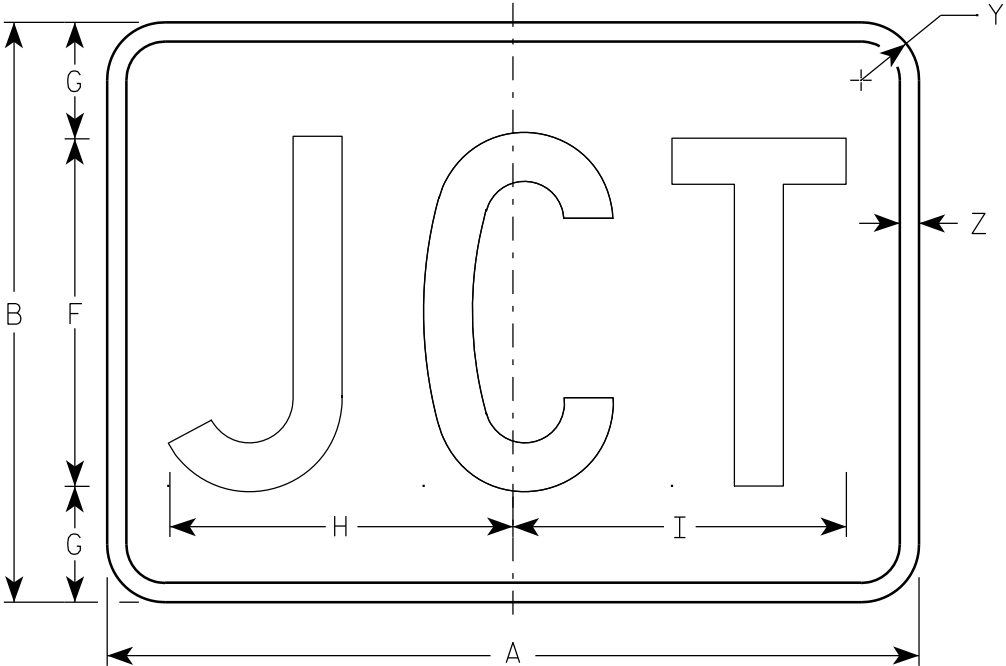
SIZE	
1	
2	525 mm X 375 mm
3	750 mm X 525 mm
4	750 mm X 525 mm
5	750 mm X 525 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20	0.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40	0.20

PROJECT NO:				HWY:				COUNTY:								SHEET NO:				E
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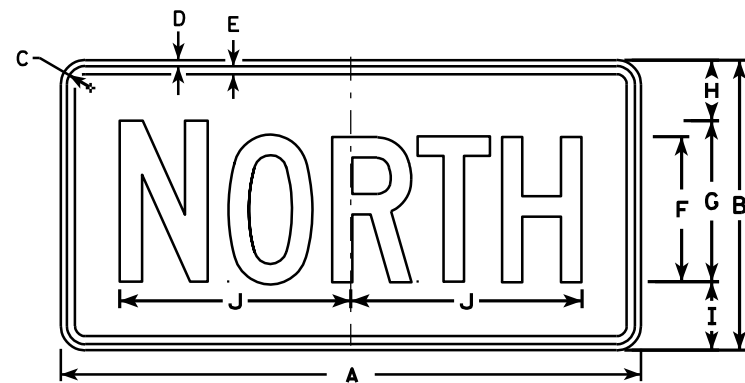
NOTES

1. Sign is Type II - See Note 5 - reference
WIS DOT Standard Specification for HIGHWAY
and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base
material is plywood but borders shall be rounded
as shown. When base material is metal, the
corners and borders shall be rounded.
5. M2-1 Background - White - Type H Reflective
(Detour or temporary Signs - Reflective)
Message - Black
MB2-1 Background - Blue
Message - White - Type H Reflective
(Detour or temporary Signs - Reflective)
MG2-1 Background - Green
Message - White - Type H Reflective
MK2-1 Background - Green
Message - White - Type H Reflective
MM2-1 Background - White - Type H Reflective
Message - Green
MN2-1 Background - Brown
Message - White - Type H Reflective
MR2-1 Background - Brown
Message - Yellow - Type H Reflective



MB2-1
MG2-1
MN2-1

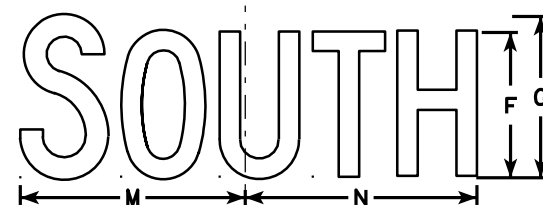
7



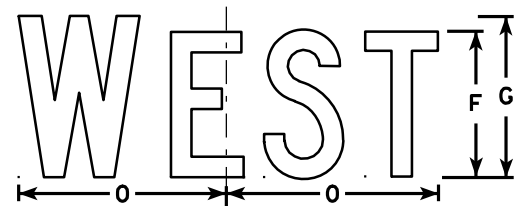
M3-1
MK3-1
M03-1



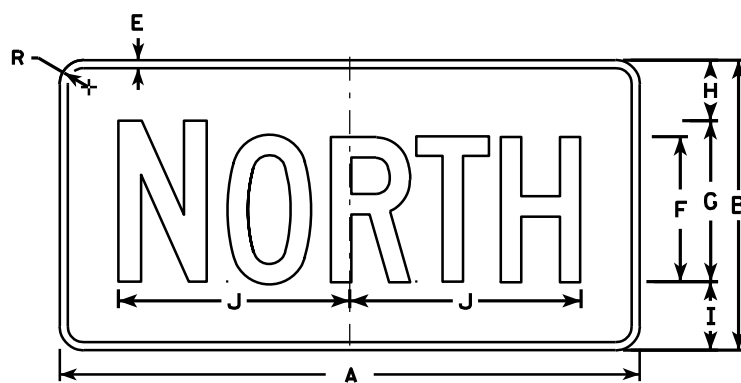
M3-2
MK3-2
M03-2



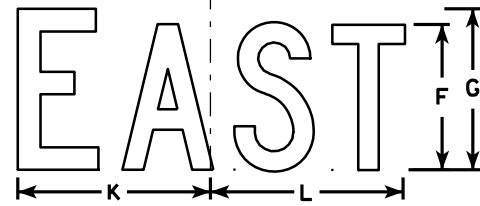
M3-3
MK3-3
M03-3



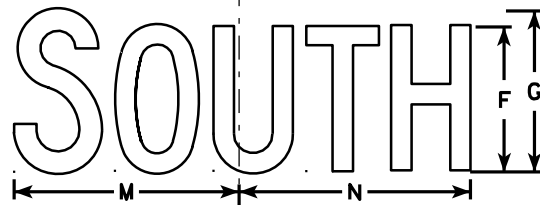
M3-4
MK3-4
M03-4



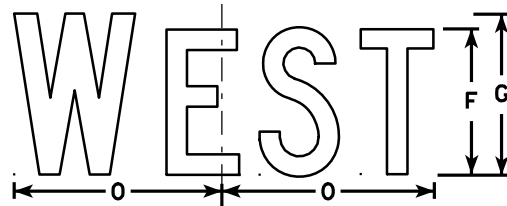
MB3-1
MG3-1
MM3-1
MN3-1



MB3-2
MG3-2
MM3-2
MN3-2



MB3-3
MG3-3
MM3-3
MN3-3



MB3-4
MG3-4
MM3-4
MN3-4

NOTES

1. All Signs Type II - See Note 5 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White - Type H Reflective (Detour or temporary signs - Reflective)
Message - Black
MB3-1 thru MB3-4 Background - Blue
Message - White - Type H Reflective (Detour or temporary signs - Reflective)
MG3-1 thru MG3-4 Background - Green
Message - White - Type H Reflective
MK3-1 thru MK3-4 Background - Green
Message - White - Type H Reflective
MM3-1 thru MM3-4 Background - White - Type H Reflective
Message - Green
MN3-1 thru MN3-4 Background - Brown
Message - White - Type H Reflective
M03-1 thru M03-4 Background - Orange - Reflective
Message - Black
6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

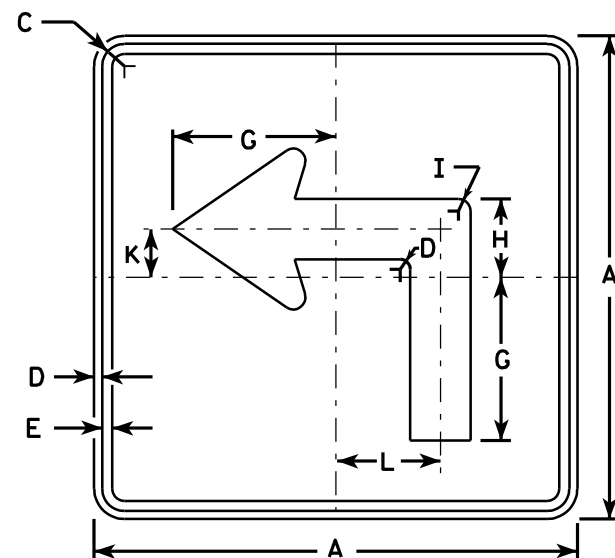
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

STANDARD SIGNS M3-1 thru M3-4 SERIES

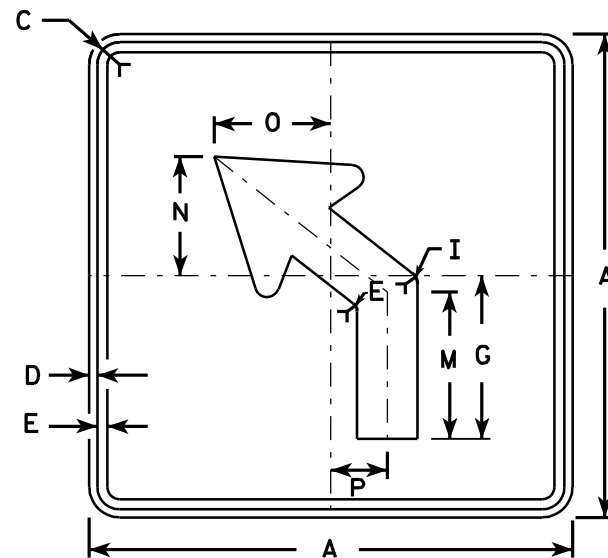
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

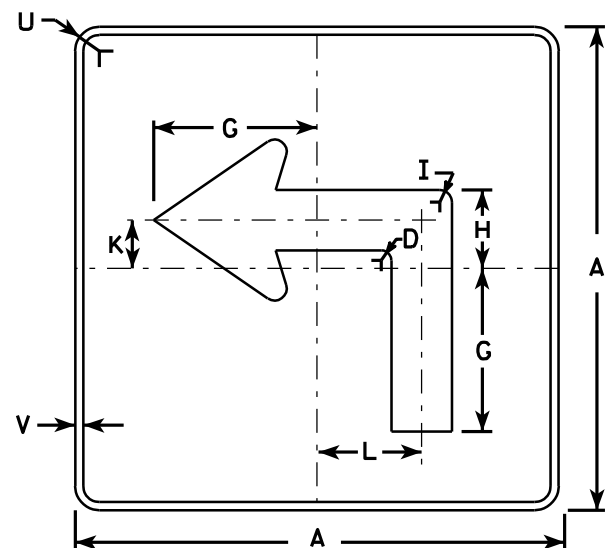
DATE 11/10/10 PLATE NO. M3-1.12



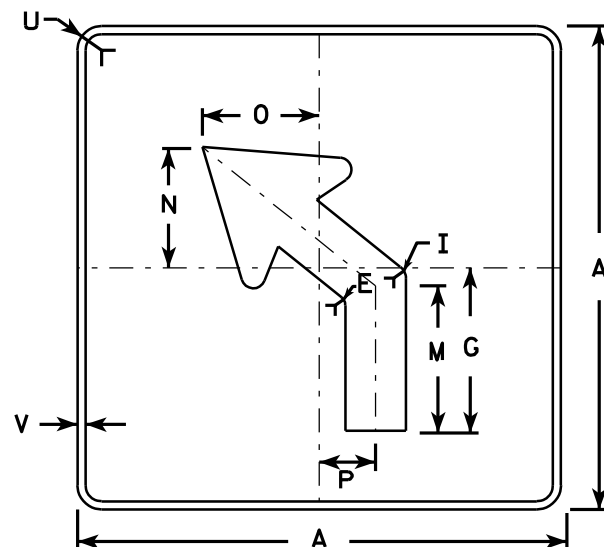
M5-1L
MK5-1L
MM5-1L
MO5-1L
MR5-1L



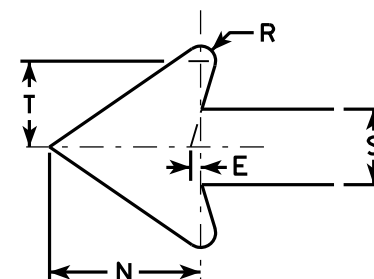
M5-2L
MK5-2L
MM5-2L
MO5-2L
MR5-2L



MB5-1L
MG5-1L
MN5-1L



MB5-2L
MG5-2L
MN5-2L



Metric equivalent
for this sign is:

SIZE	
1	
2	525 mm X 525 mm
3	750 mm X 750 mm
4	750 mm X 750 mm
5	750 mm X 750 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06	0.28
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25	0.56
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25	0.56
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25	0.56

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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NOTES

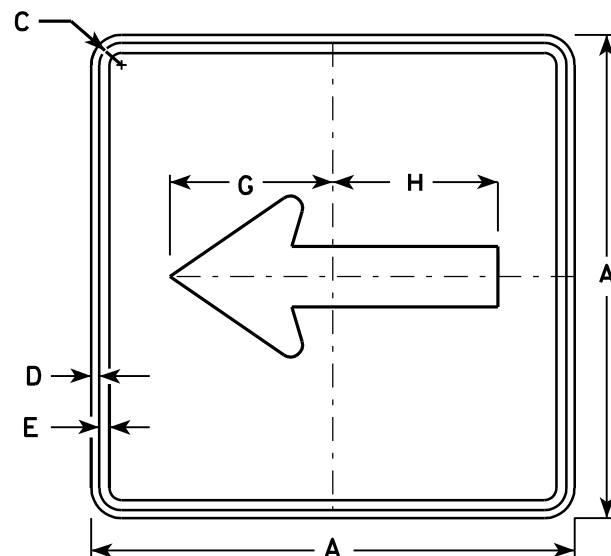
- Signs are Type II - See Note 4 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M5-1 and M5-2 Background - White - Type H Reflective (Detour or temporary Signs - Reflective)
Message - Black
MB5-1 and MB5-2 Background - Blue
Message - White - Type H Reflective (Detour or temporary Signs - Reflective)
MG5-1 and MG5-2 Background - Green
Message - White - Type H Reflective
MK5-1 and MK5-2 Background - Green
Message - White Type H Reflective
MM5-1 and MM5-2 Background - White - Type H Reflective
Message - Green
MN5-1 and MN5-2 Background - Brown
Message - White - Type H Reflective
MO5-1 and MO5-2 Background - Orange - Reflective
Message - Black
MR5-1 and MR5-2 Background - Brown
Message - Yellow - Type H Reflective
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

STANDARD SIGN
M5-1 & M5-2

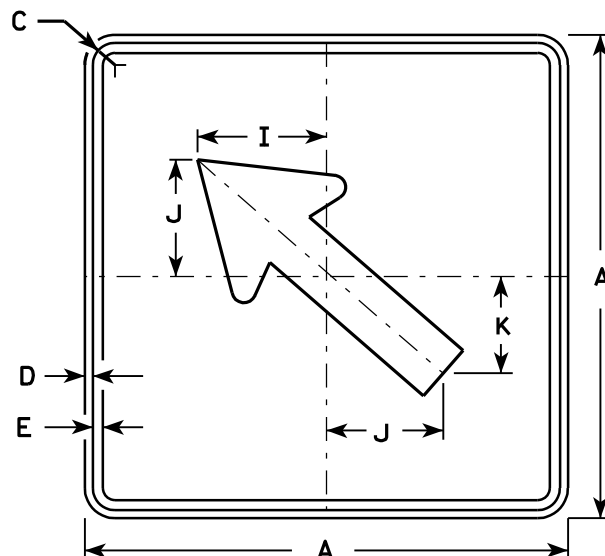
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

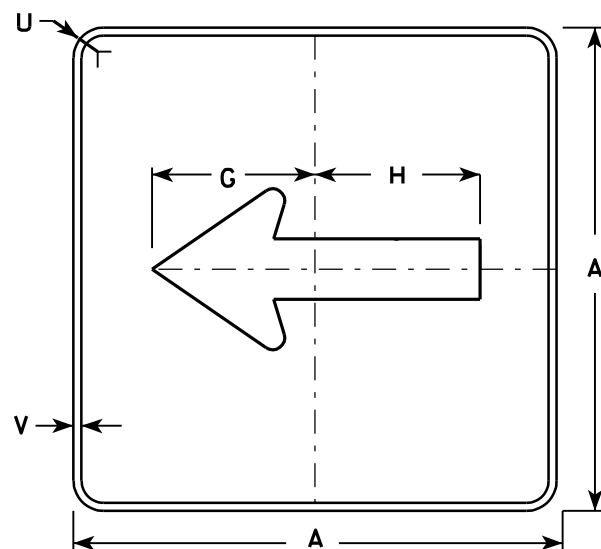
DATE 3/16/10 PLATE NO. M5-1.11



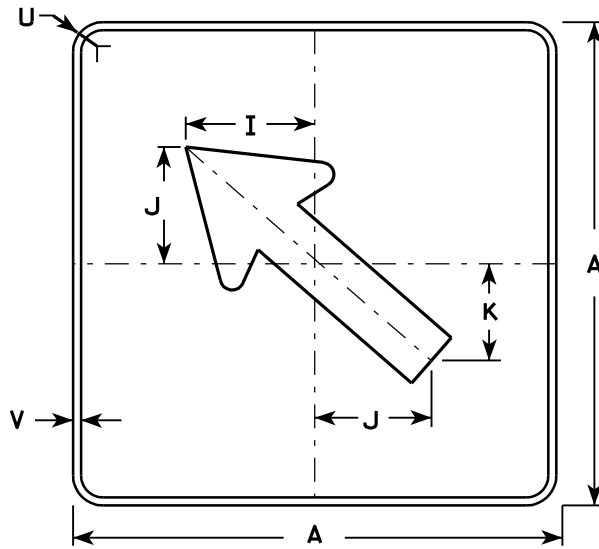
M6-1
MK6-1
MM6-1
MO6-1
MR6-1



M6-2
MK6-2
MM6-2
MO6-2
MR6-2



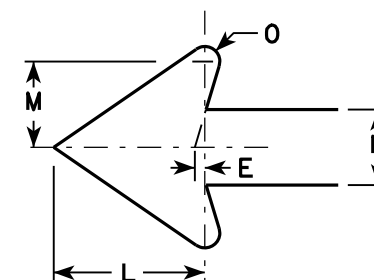
MB6-1
MG6-1
MN6-1



MB6-2
MG6-2
MN6-2

NOTES

- Signs are Type II - See Note 4 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White - Type H Reflective
(Detour or temporary Signs - Reflective)
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White - Type H Reflective
(Detour or temporary Signs - Reflective)
MG6-1 and MG6-2 Background - Green
Message - White - Type H Reflective
MK6-1 and MK6-2 Background - Green
Message - White - Type H Reflective
MM6-1 and MM6-2 Background - White - Type H Reflective
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White - Type H Reflective
MO6-1 and MO6-2 Background - Orange - Reflective
Message - Black
MR6-1 and MR6-2 Background - Brown
Message - Yellow - Type H Reflective

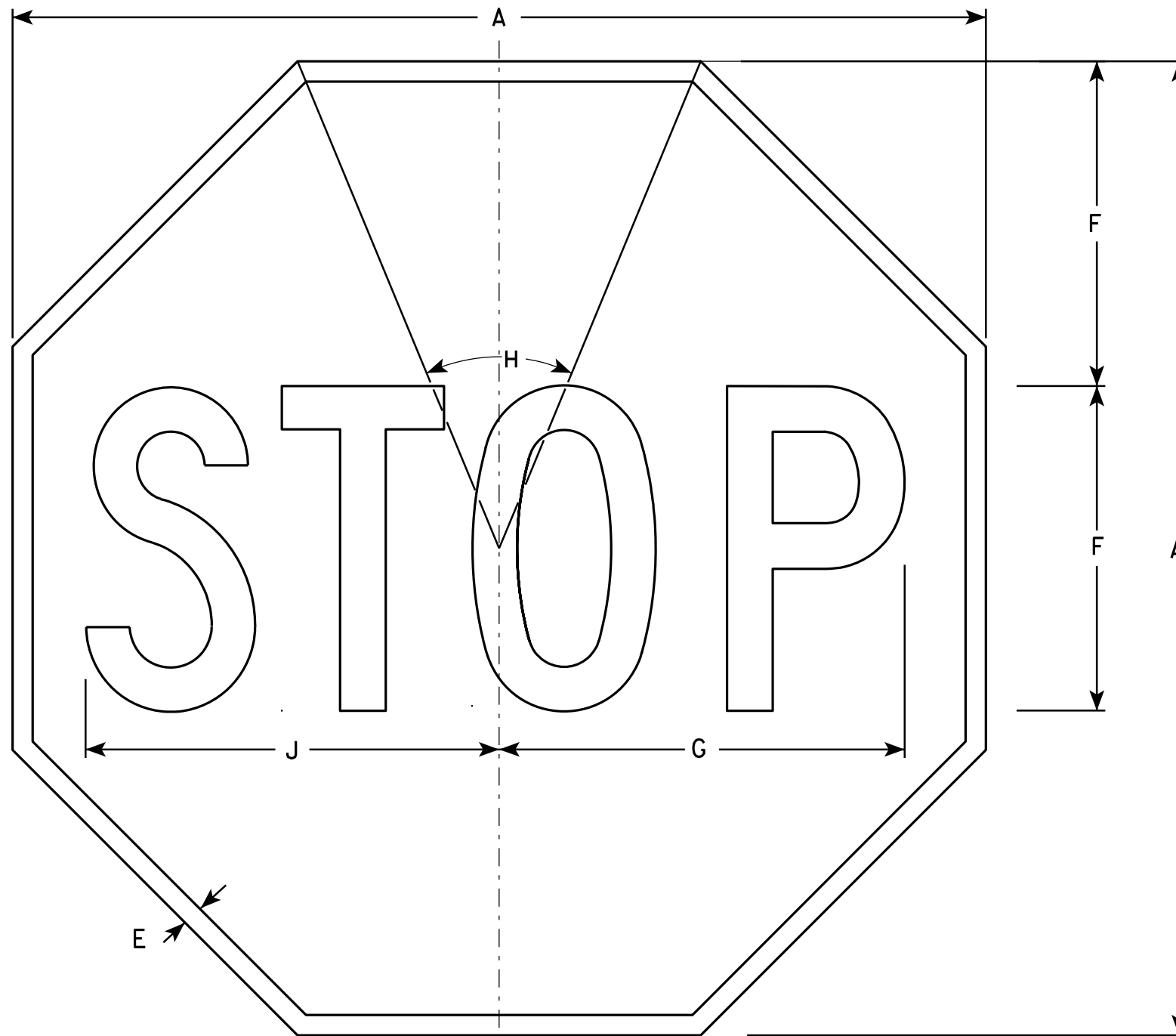


Metric equivalent
for this sign is:

SIZE	
1	
2	525 mm X 525 mm
3	750 mm X 750 mm
4	750 mm X 750 mm
5	750 mm X 750 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1																												
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06	0.28
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25	0.56
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25	0.56
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25	0.56

PROJECT NO: HWY: COUNTY: SHEET NO: E



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C

R1-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24				3/8	8	10	45°		10 1/4																	3.31
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

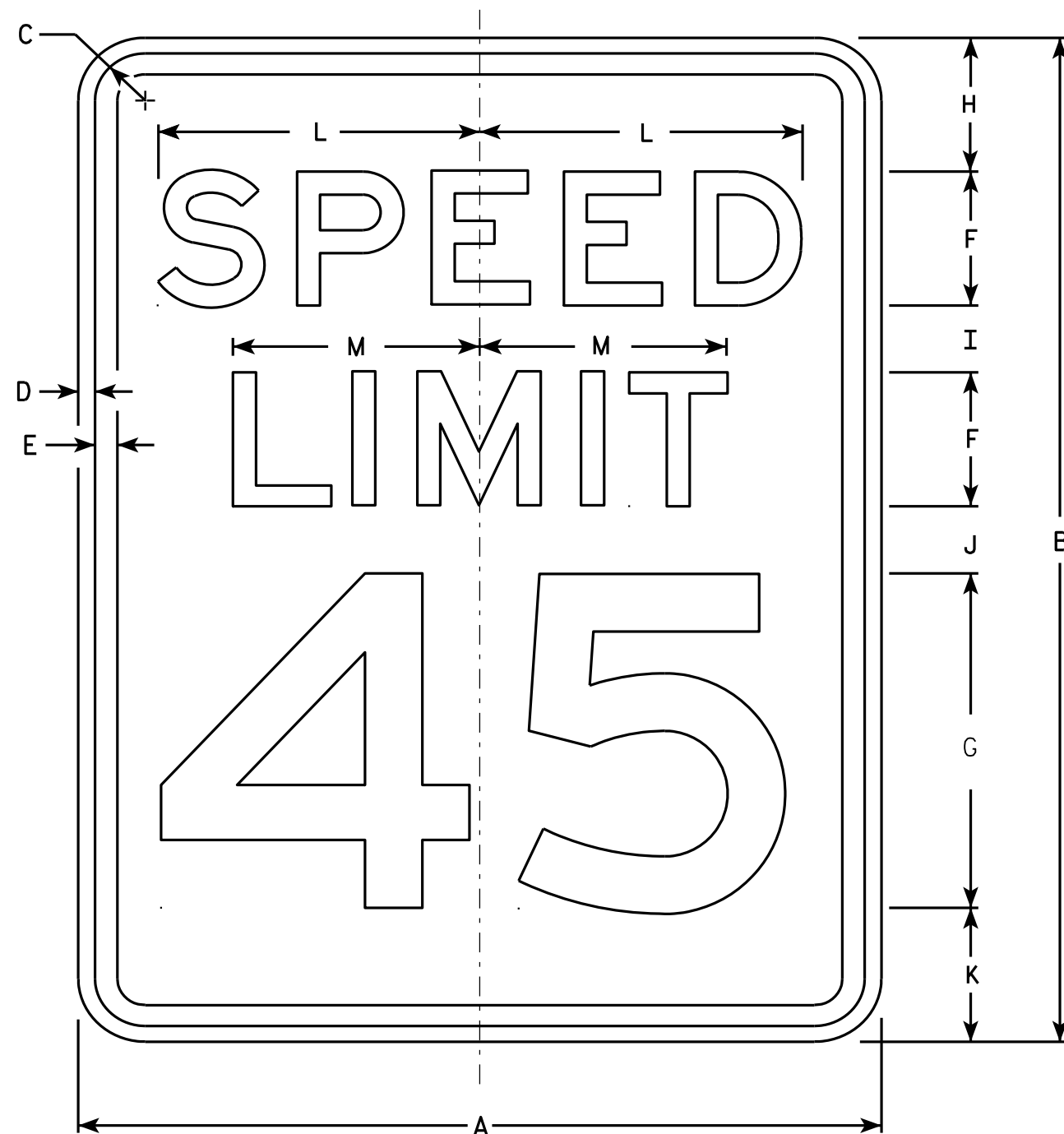
STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1.12

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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R2-1

NOTES

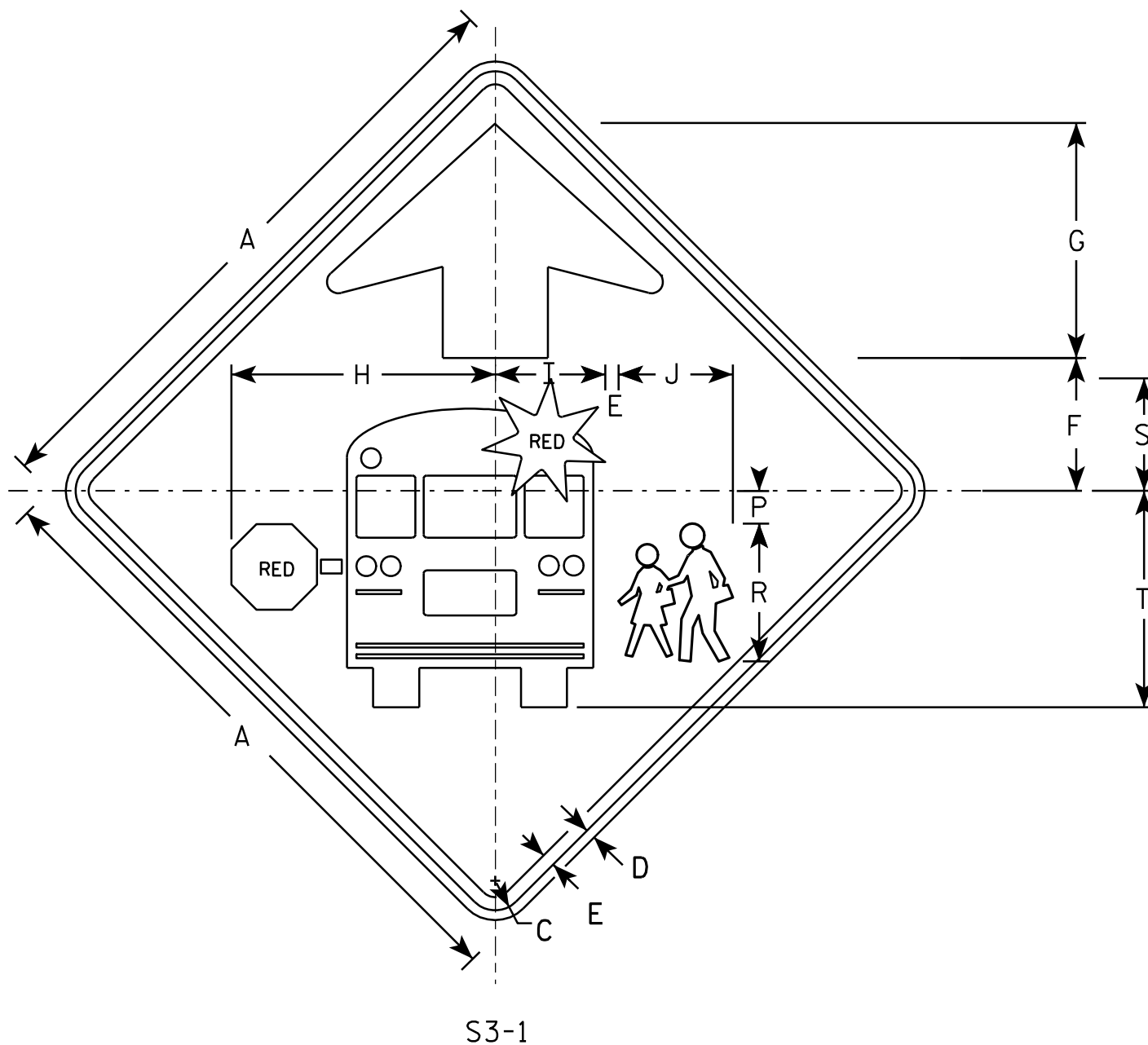
1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN
R2-1

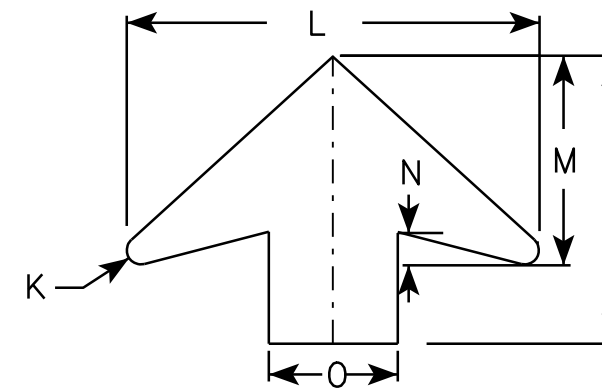
WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
For State Traffic Engineer
DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: HWY: COUNTY: SHEET NO: E



NOTES

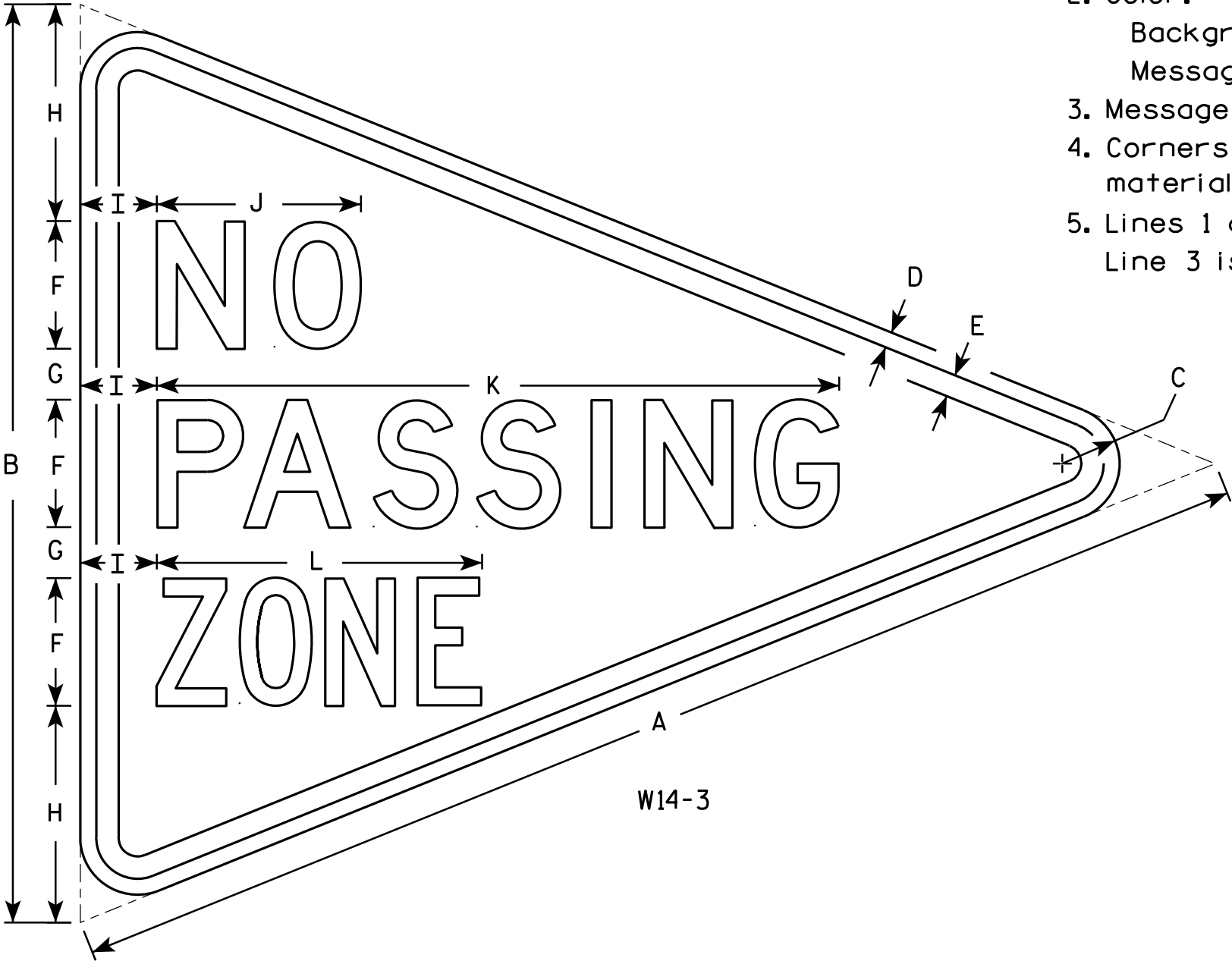
1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
 - Background - YELLOW-GREEN
 - Message - BLACK except as noted
 - Circles except PEDS- RED BACKGROUND
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

[illegible]

STANDARD SIGN	
S3-1	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<u>Matthew R. Rauch</u> for State Traffic Engineer
DATE <u>6/8/10</u>	PLATE NO. <u>S3-16</u>

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Message Series - See note 5
- 4. Corners and borders shall be rounded on all base materials for this sign.
- 5. Lines 1 and 2 are Series D.
Line 3 is series C.



W14-3

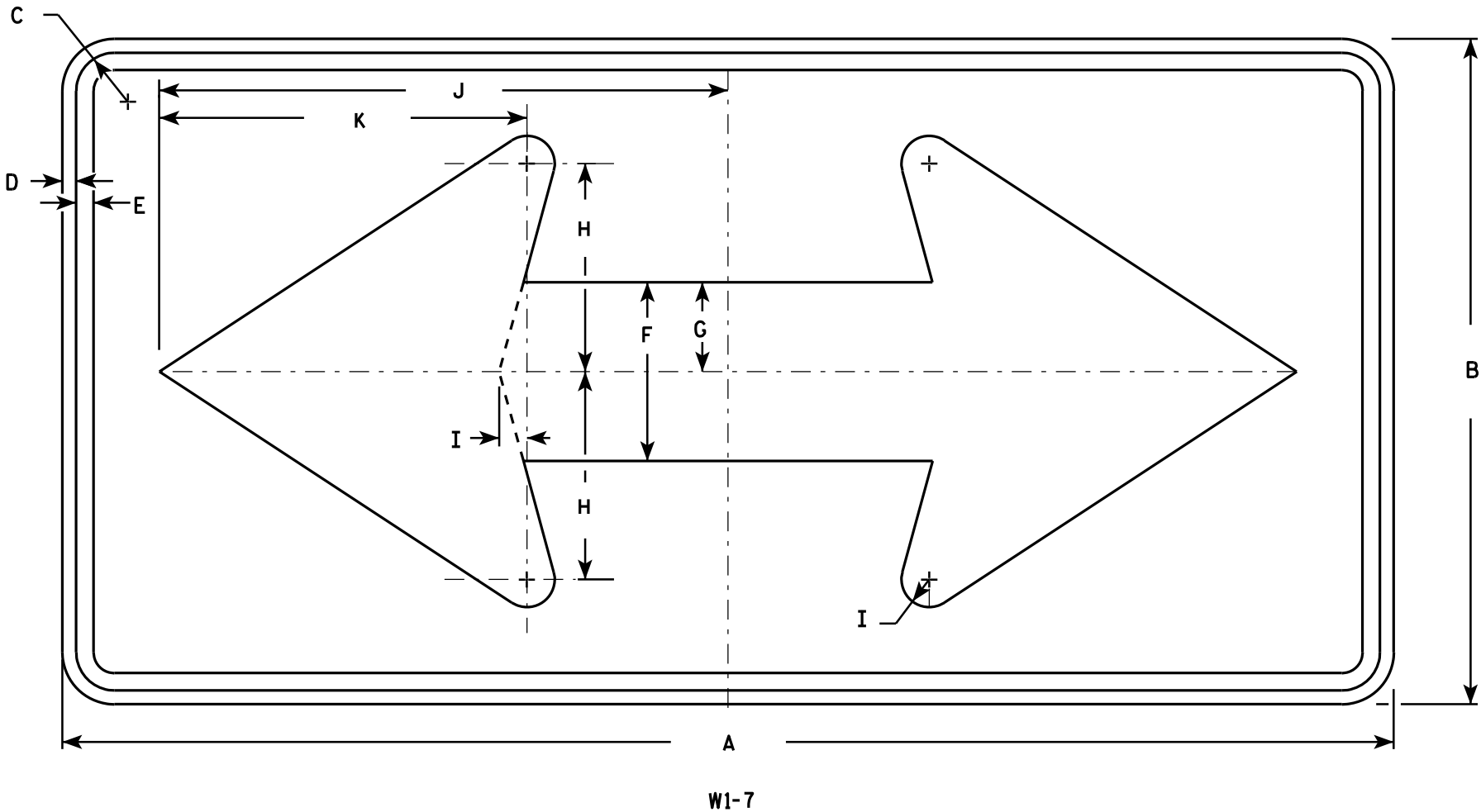
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
2M	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
3	64	48	3	3/4	1 1/4	6	3	12	4	10 3/4	33 5/8	16 1/2															10.7
4																											
5																											

STANDARD SIGN
W14-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 6/7/10 PLATE NO. W14-3.9

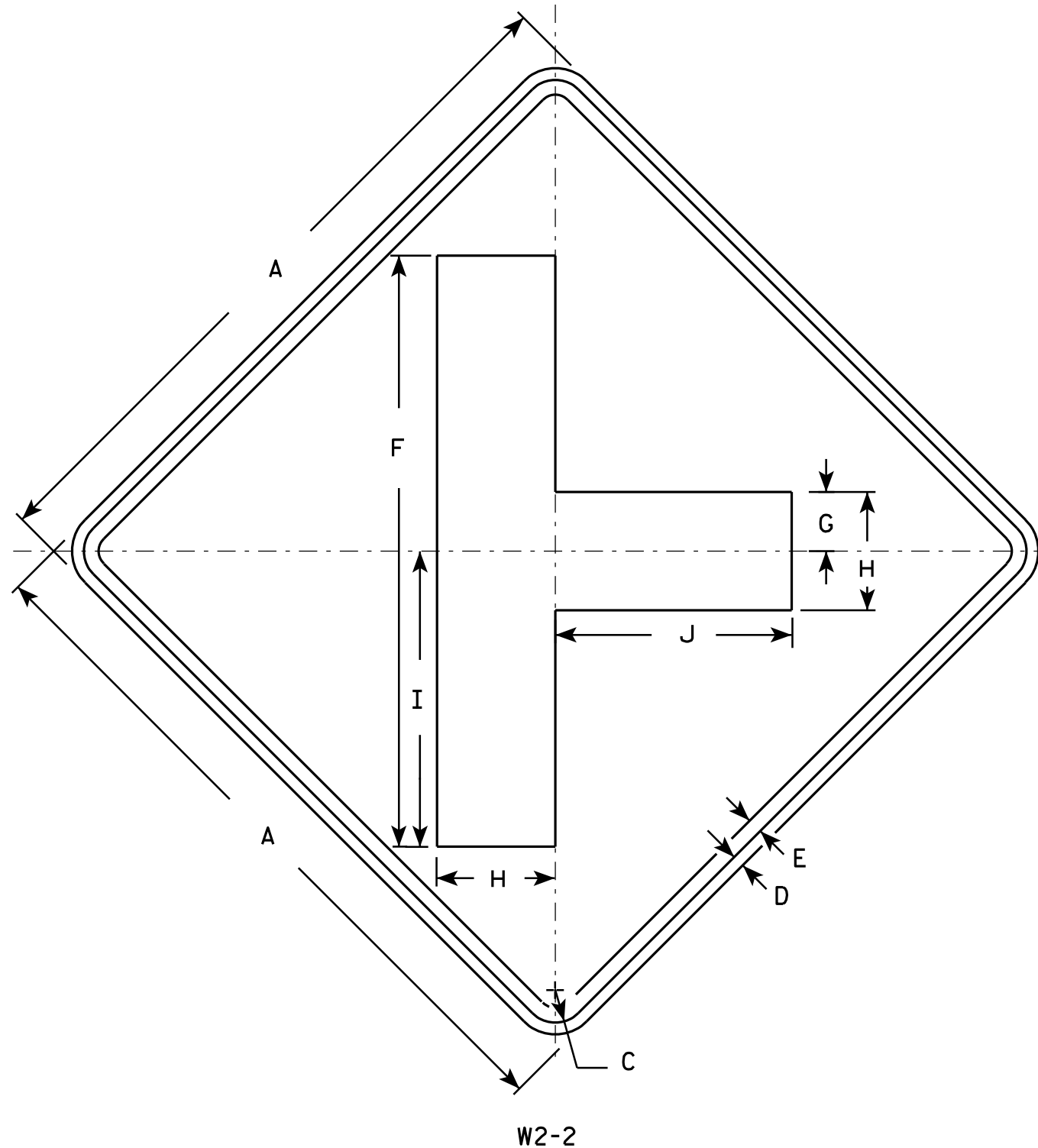


NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	1/2	5	2 1/2	5 3/4	3/4	15 5/8	10 1/8																4.5
2S	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
2M	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
3	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
4	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
5	96	48	2 1/4	3/4	1	13	6 1/2	15	2	41	26 1/2																32.0

STANDARD SIGN	
W1 - 7	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 6/7/10	PLATE NO. W1-7.7



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

W2-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	20	2	4	10	8																	4.0
2S	30		1 3/8	1/2	5/8	25	2 1/2	5	12 1/2	10																	6.25
2M	30		1 3/8	1/2	5/8	25	2 1/2	5	12 1/2	10																	6.25
3	36		1 5/8	5/8	3/4	30	3	6	15	12																	9.0
4	48		2 1/4	3/4	1	40	4	8	20	16																	16.0
5																											

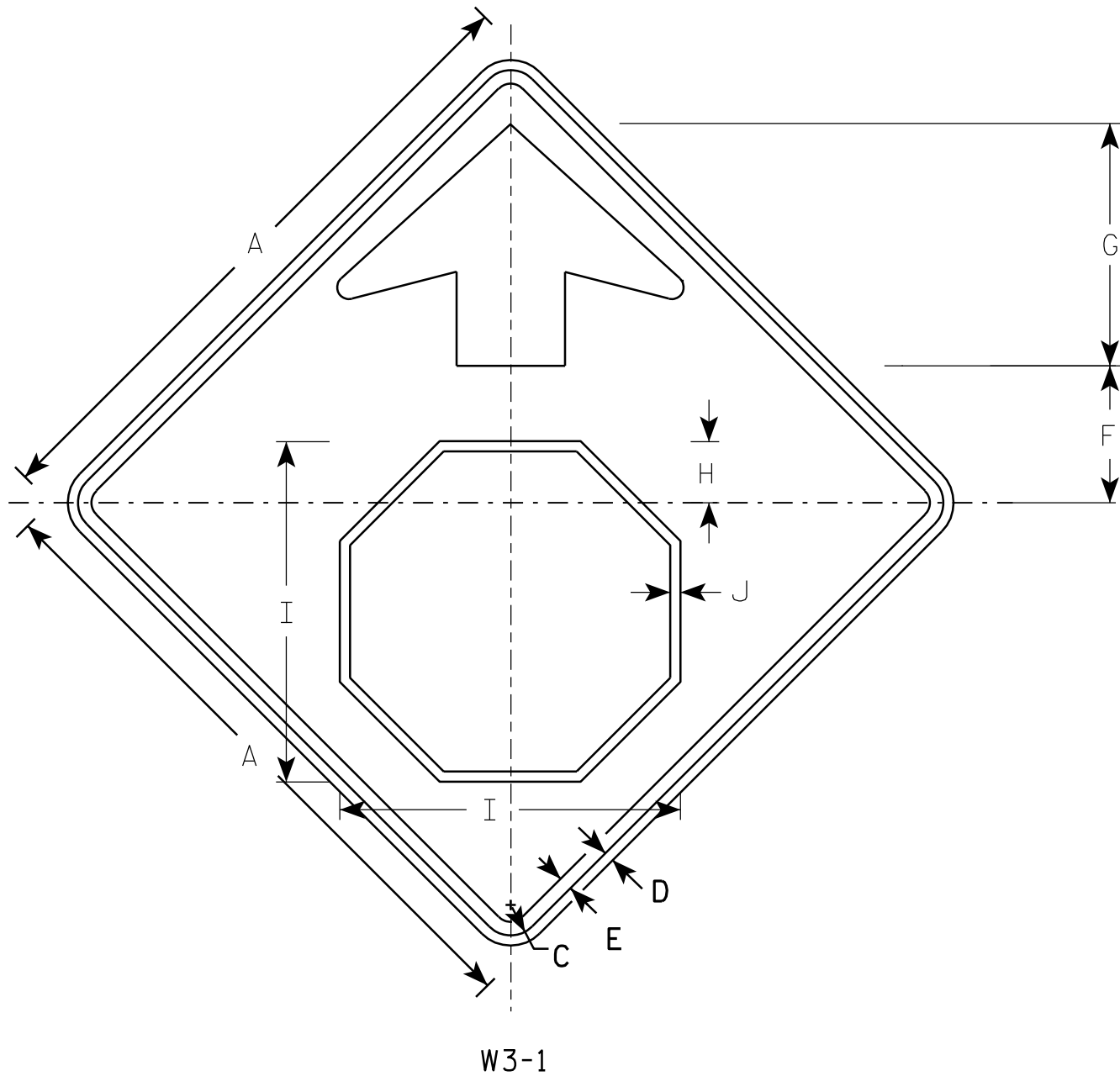
STANDARD SIGN W2-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

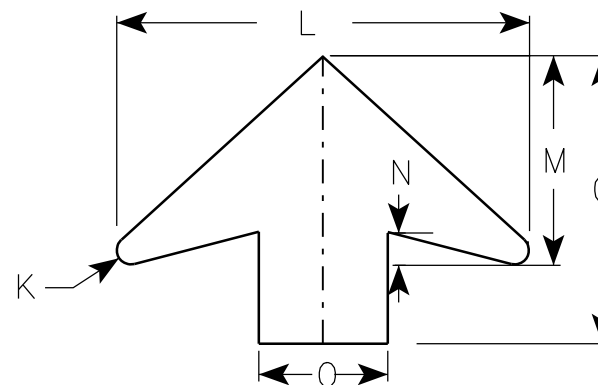
DATE 5/29/12 PLATE NO. W2-2.6

PROJECT NO: HWY: COUNTY: SHEET NO: E



NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - YELLOW
Arrow & Border - BLACK
Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	2 7/8	15 3/4	1/2	1/2	16	8	1 1/4	5												6.25
2S	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
2M	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
3	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
4	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
5	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0

PROJECT NO:

STANDARD SIGN
W3-1

WISCONSIN DEPT OF TRANSPORTATION

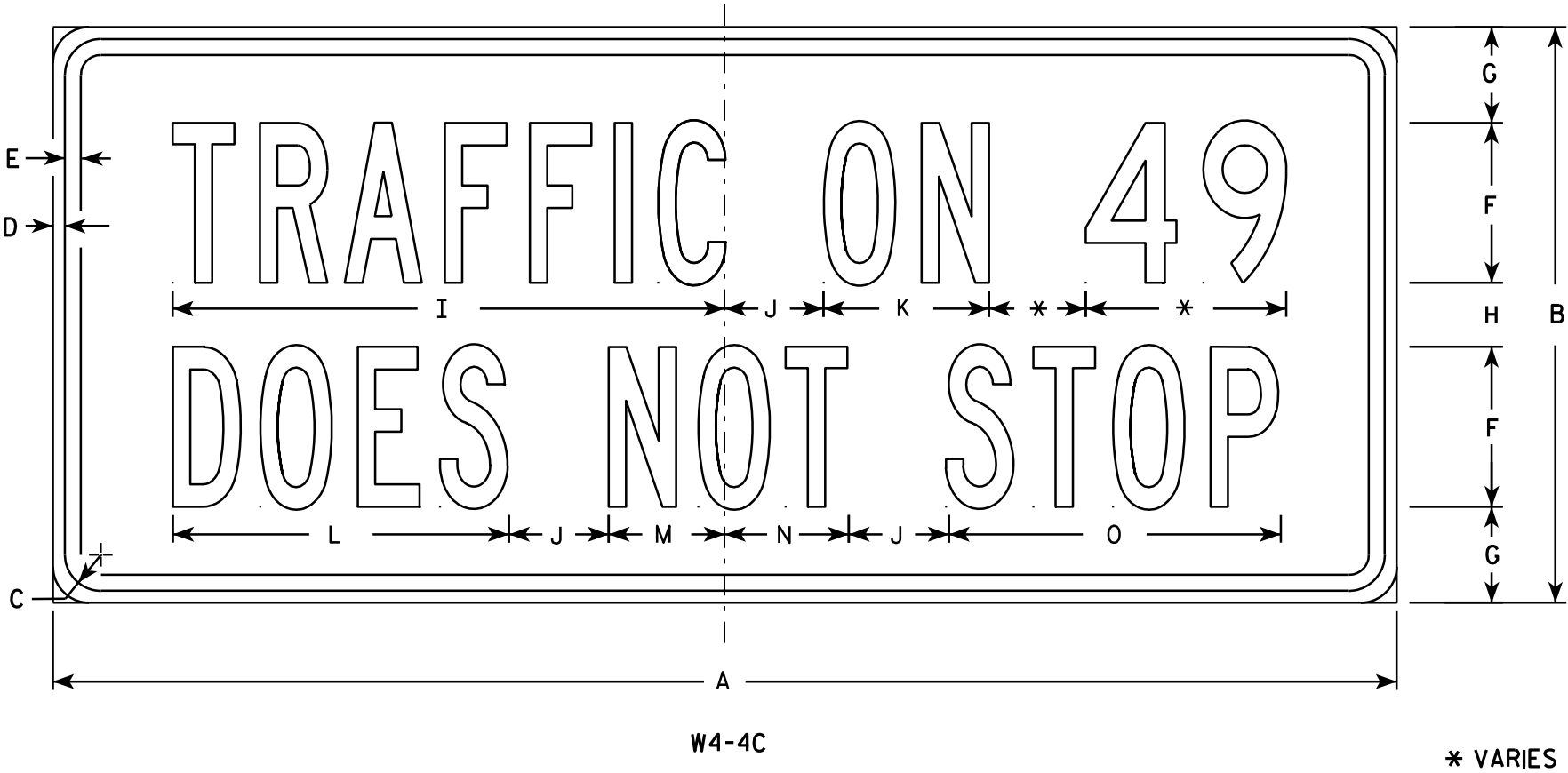
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/7/10 PLATE NO. W3-1.12

SHEET NO:

E

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
 - Background - Yellow
 - Message - Black
- 3. Message Series - B (See note 5)
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Highway Number usually Series D. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

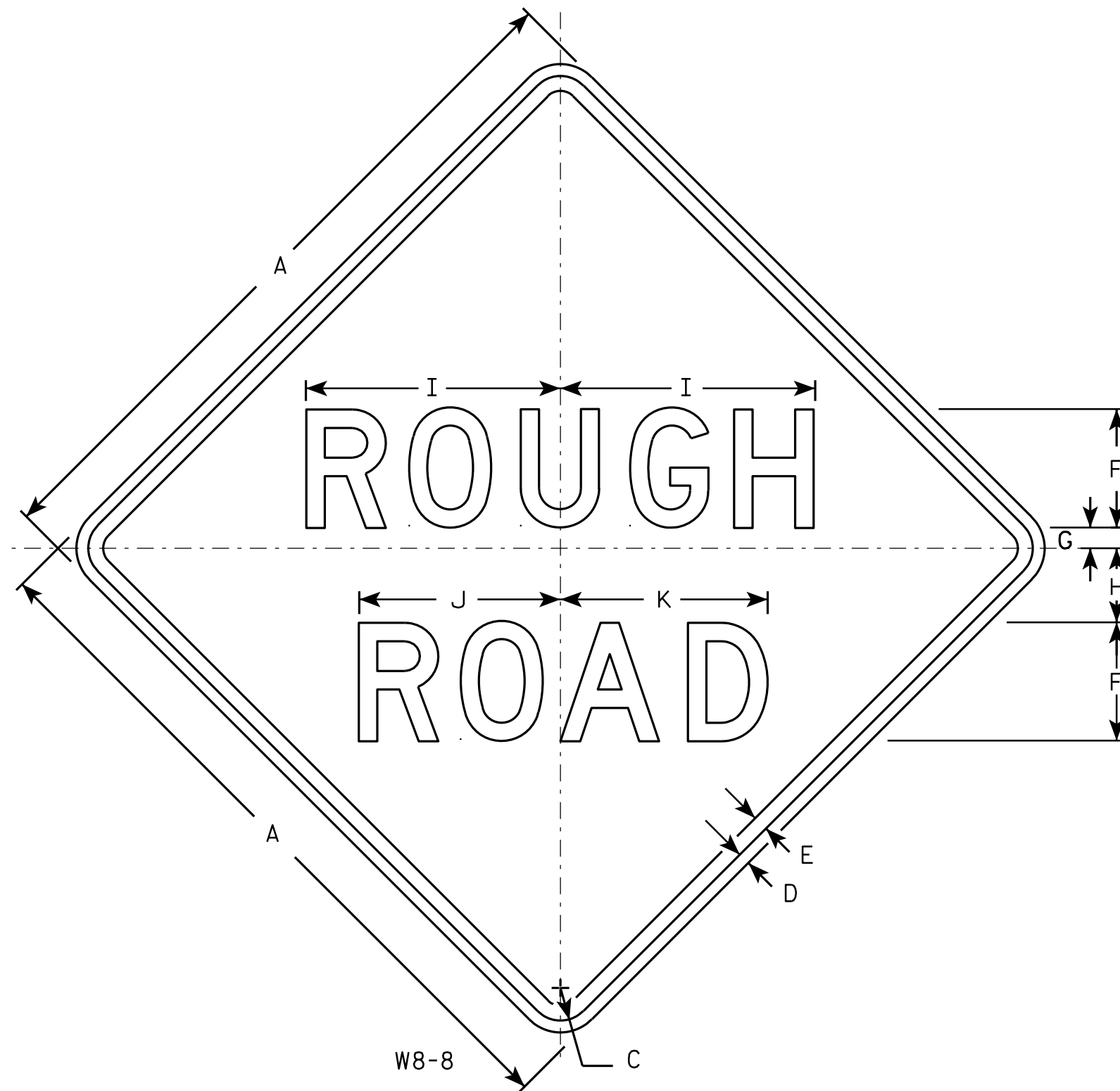


STANDARD SIGN
W4-4C

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 03/12/13 PLATE NO. W4-4.2



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	4	3/4	3	8 5/8	6 3/4	7																4.0
2S	36		1 5/8	5/8	3/4	6	1	5	12 7/8	10 1/8	10 1/2																9.0
2M	36		1 5/8	5/8	3/4	6	1	5	12 7/8	10 1/8	10 1/2																9.0
3	36		1 5/8	5/8	3/4	6	1	5	12 7/8	10 1/8	10 1/2																9.0
4	36		1 5/8	5/8	3/4	6	1	5	12 7/8	10 1/8	10 1/2																9.0
5	48		2 1/4	3/4	1	8	1 1/2	6	17 1/8	13 1/2	14																16.0

STANDARD SIGN W8-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 03/14/13 PLATE NO. W8-8.9

PROJECT NO: HWY: COUNTY: SHEET NO: E



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